

UTILITIES (G102A)

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

COMMUNICATION ACD TELECOMMUNICATION 1800 GRAND RIVER AVENUE LANSING, MI 48906 517.999.9999	COMMUNICATION FRONTIER COM 83 TOWNSEND AVENUE NORWALK, OH 44857 419.744.3613
COMMUNICATION MEDINA COUNTY FIBER NETWORKS 144 NORTH BROADWAY STREET MEDINA, OH 44256 216.832.7059	COMMUNICATION WINDSTREAM 560 TERNES AVENUE ELYRIA, OH 44035 440.329.4245
CABLE CHARTER COMMUNICATIONS 5520 WHIPPLE AVENUE NW NORTH CANTON, OH 44720 330.494.9200	COUNTY MEDINA COUNTY SANITARY ENGINEER 791 W. SMITH ROAD MEDINA, OH 44256 330.723.9581
ELECTRIC OHIO EDISON 1717 ASHLAND ROAD MANSFIELD, OH 44905 419.521.6214	GAS DOMINION 1201 E. 55TH STREET CLEVELAND, OH 44103 216.769.6788
GAS ASPIRE ENERGY 300 TRACY BRIDGE ROAD ORRVILLE, OH 44667 330.682.7726	GAS COLUMBIA GAS OF OHIO 780 FRY ROAD MIDDLEBURG HEIGHTS, OH 44130 440.891.2428

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

EXISTING PLANS (G103)

EXISTING PLANS ENTITLED MED-606-0.00 (2016) MAY BE INSPECTED IN THE ODOT DISTRICT THREE OFFICE IN ASHLAND.

WORK LIMITS (G106)

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. (PREVIOUS CONSTRUCTION PLANS SHOWING THE ORIGINAL ALIGNMENT AND PROFILE, ARE AVAILABLE FOR INSPECTION AT THE ODOT DISTRICT 3 OFFICE). PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

INTERSECTIONS, DRIVES, AND MAILBOX APPROACHES

DO NOT TREAT THE APRONS OR RADIUS RETURNS OF INTERSECTING ROADWAYS WITH ITEM 422 WITH THIS PROJECT. THE INTENT IS TO MAINTAIN THE MAINLINE TREATMENT WIDTH THROUGH THE INTERSECTIONS ON STATE ROUTE 606 WITHOUT TREATING ANY INTERSECTION APRONS.

TREAT ALL EXISTING PAVED DRIVE APRONS WITH THE SAME TREATMENT AS THE ADJACENT MAINLINE. ONLY TREAT PAVED DRIVE APRONS WHERE THE EXISTING APRON IS PAVED WITH CONTINUOUS ASPHALT, ORIGINALLY PLACED CONGRUENTLY WITH THE ADJACENT MAINLINE PAVEMENT.

TREAT ALL EXISTING PAVED MAILBOX APPROACHES WITH THE SAME TREATMENT AS THE ADJACENT MAINLINE.

PERFORM NO TREATMENT OUTSIDE OF THE EXISTING PAVEMENT. NO ADDED AGGREGATE IS TO BE PLACED ON AGGREGATE DRIVES WITHIN THE PROJECT LIMITS.

EXTRA QUANTITIES OF ALL MATERIALS NEEDED TO COMPLETE THE ABOVE LISTED WORK IS LISTED ON THE PAVEMENT AND SHOULDER DATA SHEET INCLUDED IN THESE PLANS AND IS CARRIED TO THE GENERAL SUMMARY.

ANY HAZARD OR UNSAFE CONDITION RESULTING FROM THE ABOVE WORK MUST BE CORRECTED IMMEDIATELY. THE CONTRACTOR IS REMINDED OF SECTIONS 105.01, 107.07 & 614.02A OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

TRAFFIC CONTROL

IT IS THE CONTRACTOR'S RESPONSIBILITY TO CREATE A PAVEMENT MARKING LOG PRIOR TO THE REMOVAL, COVERING, OR OTHER DESTRUCTION OF THE EXISTING MARKINGS. THIS LOG, PRIOR TO REMOVAL, COVERING, OR OTHER DESTRUCTION OF THE EXISTING MARKINGS, IS TO BE SUBMITTED TO THE ENGINEER FOR APPROVAL. ENSURE ALL EXISTING MARKINGS REMAIN IN PLACE UNTIL SUCH APPROVAL IS OBTAINED.

PRIOR TO PERFORMING THE SINGLE CHIP SEAL TREATMENT, REMOVE ALL EXISTING PAVEMENT MARKINGS USING AN APPROVED METHOD TO THE SATISFACTION OF THE ENGINEER. REMOVE PAVEMENT MARKINGS NO MORE THAN SEVEN CALENDAR DAYS PRIOR TO THE ANTICIPATED APPLICATION OF THE CHIP SEAL PAVEMENT TREATMENT. AT A MINIMUM, REINSTALL THE CENTERLINE AT THE END OF EACH WORKING DAY FOR THE LOCATION WHERE WORK HAS BEEN COMPLETED. THIS CENTER LINE MAY BE A WORK ZONE CENTER LINE OR THE FINAL PAVEMENT MARKING.

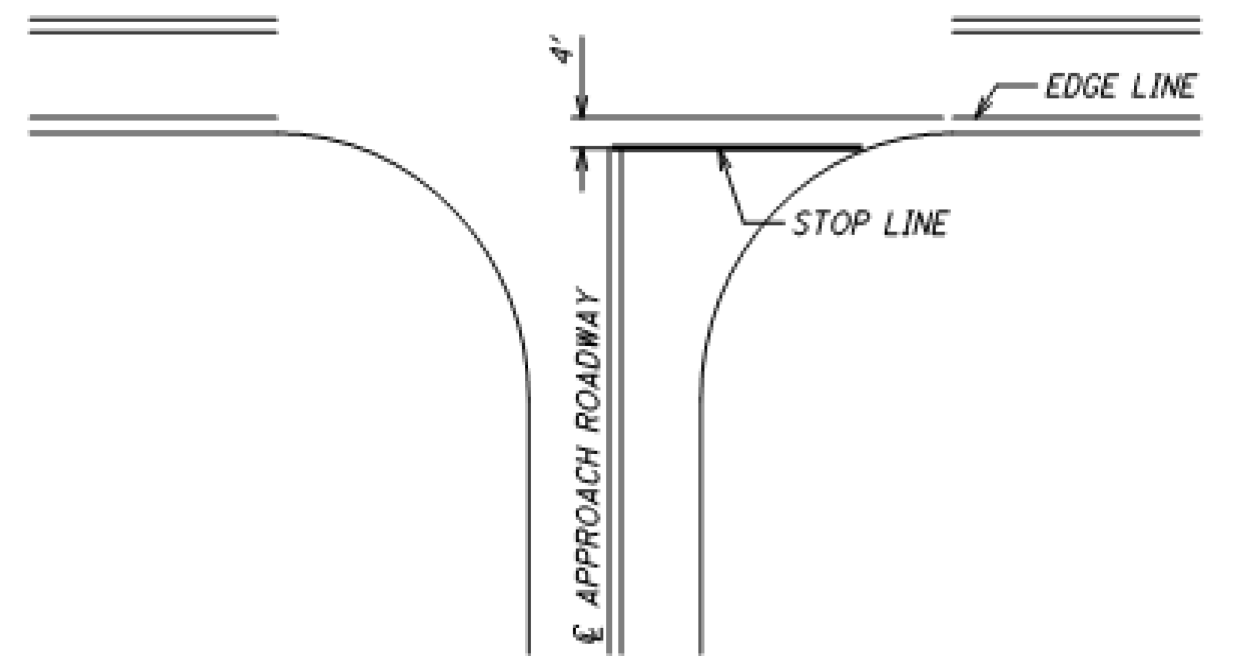
AS IT IS ANTICIPATED THAT THE FIRST APPLICATION OF PERMANENT PAVEMENT MARKINGS WILL HAVE A SHORT USEFUL LIFE DUE TO THE LOOSE NATURE OF THE CHIP SEAL TREATMENT, 30 TO 45 DAYS AFTER THE INSTALLATION OF THE FIRST PERMANENT PAVEMENT MARKINGS, REINSTALL THE PERMANENT PAVEMENT MARKINGS AGAIN, FOR A TOTAL OF TWO APPLICATIONS. DO NOT REMOVE THE FIRST APPLICATION PRIOR TO INSTALLING THE SECOND.

STRIPE THE LANES USING THE EXISTING LANE WIDTH.

- INSTALL WORK ZONE STOP LINES AT THE FOLLOWING LOCATIONS:
- ON STATE ROUTE 606 SOUTHBOUND AT STATE ROUTE 3
 - ON STATE ROUTE 606 NORTHBOUND AT STATE ROUTE 94/303

STOP BAR PLACEMENT

IN ORDER TO ACHIEVE MAXIMUM INTERSECTION SIGHT DISTANCE, AT NORMAL STOP CONTROLLED RURAL INTERSECTIONS WITHOUT CROSSWALK, PLACE THE STOP BAR FOUR FEET FROM THE EDGE LINE OF THE INTERSECTING ROADWAY, OR IN LINE WITH THE OUTSIDE EDGE OF THE PAVED SHOULDER, WHICHEVER IS WIDER.



ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR (442)
ITEM 253 – PAVEMENT REPAIR

ITEM 251:
~~THIS ITEM SHALL BE USED WHEN THE REPAIR IS A MAXIMUM OF 4.00" IN DEPTH. THE REPLACEMENT MATERIAL SHALL BE ITEM 442 19 MM, AS PER PLAN MATERIAL AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. ITEM 442 SHALL USE A PG64-22 BINDER.~~

ITEM 253:
THIS ITEM SHALL BE USED WHEN THE REPAIR IS GREATER THAN 4.00" IN DEPTH. THE REPLACEMENT MATERIAL SHALL BE ITEM 301, OR ITEM 442 19MM, AS PER PLAN MATERIAL AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. ITEM 301 ASPHALT CONCRETE CAN BE USED WHEN THE DEPTH OF THE REPAIR IS GREATER THAN 4", WITH A MAXIMUM PAVEMENT LIFT OF 6". THE TOP 1.5" OF EVERY REPAIR SHALL BE ITEM 442 19MM, AS PER PLAN REGARDLESS OF DEPTH. PG64-22 BINDER SHALL BE USED FOR ITEM 301.

FOR THE ITEM 442 19MM, AS PER PLAN MATERIAL, REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:
~~MIX DESIGN: FOR NIDES USE 50 GYRATIONS, FOR NMAX USE 75 GYRATIONS.~~
~~USE A PG64-22 BINDER.~~
~~MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT IS 30 PERCENT.~~
APPLY 703.05 FOR COARSE AND FINE AGGREGATE EXCEPT GRADATION FOR FINE AGGREGATE DOES NOT APPLY.

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE.

PAVEMENT REPAIR SHALL BE PERFORMED BEFORE PLACEMENT OF THE SINGLE CHIP SEAL TREATMENT. THE DEPTH OF THE REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR AND WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, (BY TICKET WEIGHT CONVERSION), OF ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR (442) OR ITEM 253 – PAVEMENT REPAIR. ESTIMATED QUANTITIES ARE PROVIDED ON THE PAVEMENT AND SHOULDER DATA SHEET AND ARE CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

PROTECTION OF CASTINGS

PROVIDE PROTECTION FROM CONTAMINATION WITH CHIP SEAL MATERIAL FOR ALL EXISTING CASTINGS WITHIN THE PROJECT LIMITS, INCLUDING CATCH BASINS, MONUMENT BOXES, MANHOLES, ETC. SHOULD ANY CHIP SEAL MATERIAL, INCLUDING THE EMULSION, BE PLACED ERRANTLY ON ANY CASTING, CLEAN THE CASTING TO A CONDITION EQUAL TO OR BETTER THAN ORIGINAL USING A METHOD APPROVED BY, AND TO THE SATISFACTION OF, THE ENGINEER.

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS (G118B)

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED THE LIMITS IN THE HEIGHT LIMITS IN THE TABLE SHOWN BELOW. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA) AND THE DISTRICT THREE FAA COORDINATOR WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO SUBMIT FORM 7460-1 TO THE FAA VIA THE FAA OBSTRUCTION EVALUATION GROUP'S ONLINE PROJECT FILING PROCESS. NOTIFY THE ODOT DISTRICT THREE FAA COORDINATOR WHEN SUBMITTING FAA FORM 7460-1.

NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND THE ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER AND THE DISTRICT FAA COORDINATOR VIA EMAIL AT Ethan.Caudill@dot.ohio.gov.

FEDERAL AVIATION ADMINISTRATION SOUTHWEST REGIONAL OFFICE OBSTRUCTION EVALUATION GROUP 10101 HILLWOOD PARKWAY FORT WORTH, TX 76177 FAX: 817.222.5920 http://oeaaa.faa.gov	ODOT DISTRICT THREE DISTRICT FAA COORDINATOR 906 CLARK AVENUE ASHLAND, OHIO 44805 419.207.7132 ethan.caudill@dot.ohio.gov
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COUNTY	ROUTE	BEGINNING SLM	ENDING SLM	HEIGHT LIMIT (FEET)
MED	606	0.00	4.26	25 FT