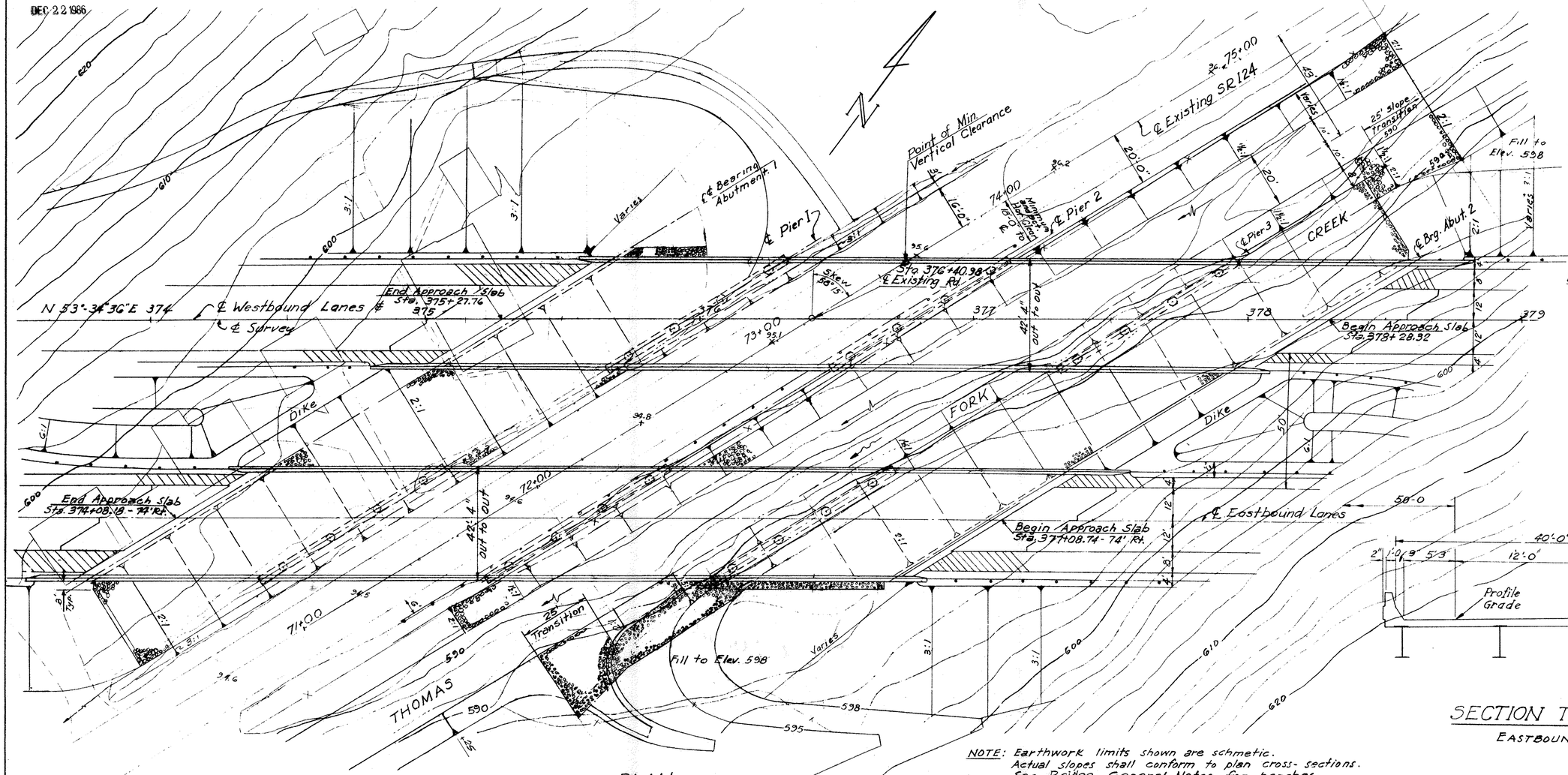
FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

393
467

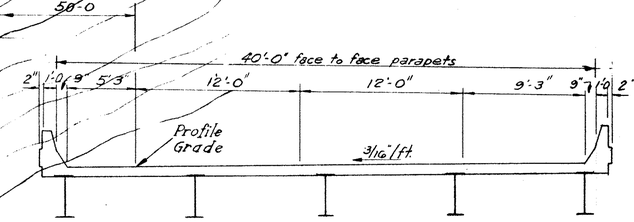
MEG-7-6.18



DETAIL OF ROCK CHANNEL PROTECTION

HYDRAULIC DATA

Drainage Area = 10.19 Sq.M
Q50 = 2475 cfs.
50 year high water Elev. 595.6
Velocity 3.8 ft. per sec.

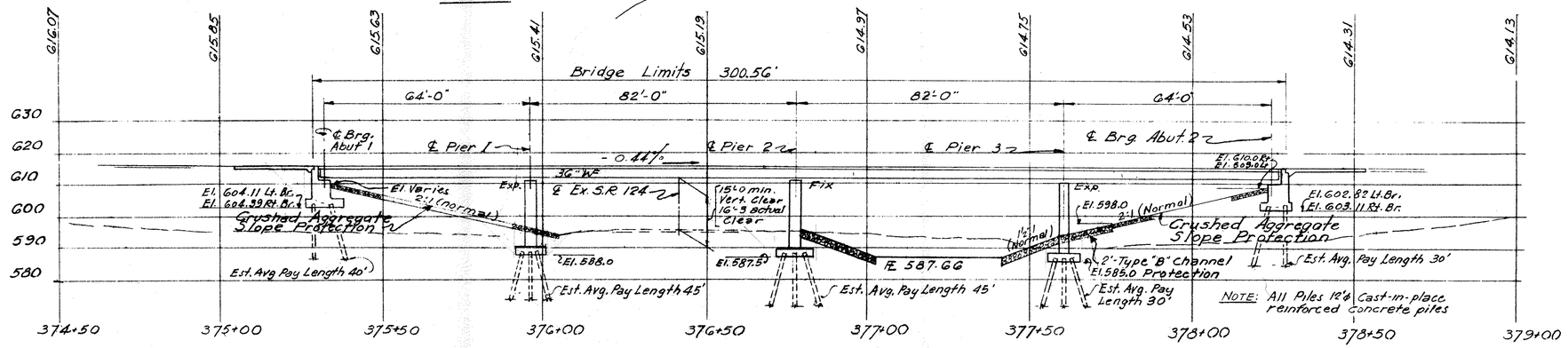


SECTION THRU BRIDGE

EASTBOUND LANES

NOTE: Earthwork limits shown are schmetic.
Actual slopes shall conform to plan cross-sections.
See Bridge General Notes for benches.

PLAN



NOTE: All Piles 12"ϕ Cast-in-place reinforced concrete piles

ELEVATION ON ℄ WESTBOUND LANES

ADT - 1990 - 5030

PROPOSED STRUCTURE

TYPE : Continuous Rolled Beam with reinforced concrete deck and substructure
SPANS : 64'-0", 82'-0", 82'-0", 64'-0"
ROADWAY : 2 with 40'-0" face to face of parapets
WEARING SURFACE : 1" Monolithic Concrete
LOADING : AASHO H S 20-44
APPROACH SLABS : AS1-67 (25'-0" Long)
ALIGNMENT : Tangent
SKEW : 58°15" L.F

AUBLE - MITCHELL - BURGESS & ASSOC
ENGINEERS & ARCHITECT | 1 | 13

SITE PLAN

BRIDGE NO. MEG-7-07.13-L/R
S.R. 7 over S.R. 124
MEIGS COUNTY
STA 375+27.76 STA 378+28.32

PROPOSED WORK

DESIGNED	DRAWN	TRACED	CHECKED	REVISION
			BRB	

FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

394
467

MEG-7-6.15

ESTIMATED QUANTITIES (TOTAL FOR TWO BRIDGES)

ITEM	TOTAL	UNIT	DESCRIPTION	ABUTMENTS	PIERS	SUPER-STRUCTURE	GENERAL	CHECKED BY & DATE	
503	1285	Cu. Yd	Unclassified Excavation	692	593			LEN	9-70
505	Lump	Lump Sum	Test Pile				Lump		
507	9860	Lin. Ft.	12" Cast-in-place reinforced concrete piles	2660	7200			LEN	9-70
509	393,456	Lb.	Reinforcing Steel	40,861	120,738	231 857		LEN	9-70
511	805	Cu. Yd.	Class C Concrete, superstructure			805		LEN	9-70
511	318	Cu. Yd.	Class C Concrete, pier caps and columns		318			LEN	9-70
511	333	Cu. Yd.	Class C Concrete, abutments above footings	333				LEN	9-70
511	418	Cu. Yd.	Class C Concrete, footings	258	160			LEN	9-70
512	30	Lin. Ft.	Premolded sealing strip	30				LEN	9-70
513	822 000	Lb.	Structural Steel			822 000		LEN	9-70
514	822,000	Lb.	Field painting of structural steel			822 000			
518	152	Cu. Yd	Porous backfill	152				LEN	9-70
518	274	Lin. Ft	6" Perforated Helical C.M.P. including specials, 707.01	274				LEN	9-70
518	165	Lin. Ft.	6" non-perforated helical C.M.P. 707.01	165				LEN	9-70
518	18	Each	Scuppers, including supports					LEN	9-70
601	1795	Sq. Yd	Crushed aggregate slope protection		18		1795	LEN	9-70
808	805	Units	Chemical admixture for concrete Type A, B or D			805			
503	Lump	Lump Sum	Cofferdams, cribs, and sheeting				Lump		

GENERAL NOTES

REFERENCE SHALL BE MADE TO THE FOLLOWING:

STANDARD DRAWINGS BR-1-67 DATED 1-1-71
SD-1-69 DATED 6-12-69
RB-1-55 REVISED 2-2-59
AS-1-67 REVISED 6-12-69

SUPPLEMENTAL SPECIFICATIONS 808 DATED 1-1-71
836 DATED 1-1-71

DESIGN SPECIFICATIONS: THIS STRUCTURE CONFORMS TO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY OFFICIALS, 1965, INCLUDING THE OHIO "SUPPLEMENT" TO THESE SPECIFICATIONS.

DESIGN DATA:

DESIGN LOADING - HS 20-44

CONCRETE CLASS C - UNIT STRESS 1200 P.S.I. FOR SUPERSTRUCTURE
- UNIT STRESS 1333 P.S.I. FOR SUBSTRUCTURE

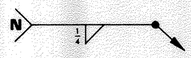
STRUCTURAL STEEL - ASTM A36 - UNIT STRESS 20,000 P.S.I.

REINFORCING STEEL - ASTM A615, A616, OR A617 - UNIT STRESS 20,000 P.S.I. SPIRAL REINFORCEMENT may BE PLAIN BARS ASTM A306, A499, A 615, A82

PILES SHALL BE DRIVEN TO A MINIMUM BEARING CAPACITY OF 35 TONS FOR ABUTMENTS AND PIERS.

MAINTENANCE OF TRAFFIC: TWO LANES OF TRAFFIC WITH A MINIMUM HORIZONTAL WIDTH OF 24'-0" AND A MINIMUM VERTICAL CLEARANCE OF 12'-9" SHALL BE MAINTAINED ON S.R. 124 AT ALL TIMES.

WELDS ON NON-STRESS CARRYING MEMBERS ARE SHOWN THUS:



EMBANKMENT CONSTRUCTION: THE EMBANKMENTS SHALL BE CONSTRUCTED TO THE LEVEL OF THE SUBGRADE FOR A MINIMUM DISTANCE OF 200 FEET BACK OF THE ABUTMENTS. EXCAVATION SHALL THEN BE MADE FOR THE ABUTMENTS AND PIERS 1 AND 3.
PROVIDE A 30 FT. WIDE BENCH PARALLEL TO THE 600' CONTOUR BETWEEN STA. 372+00 AND STA. 375+00 AT THE REAR APPROACH FILL, AND A 30 FT. WIDE BENCH PARALLEL TO THE 594' CONTOUR BETWEEN STA. 376+50 AND STA. 379+50 AT THE FORWARD APPROACH FILL.

AUBLE-MITCHELL-BURGESS & ASSOC. 2/13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

ESTIMATED QUANTITIES & GENERAL NOTES
BRIDGE NO. MEG.-7-0713 L/R
S.R. 7 OVER S.R. 124
MEIGS COUNTY STA. 375+27.76 TO STA. 375+28.30

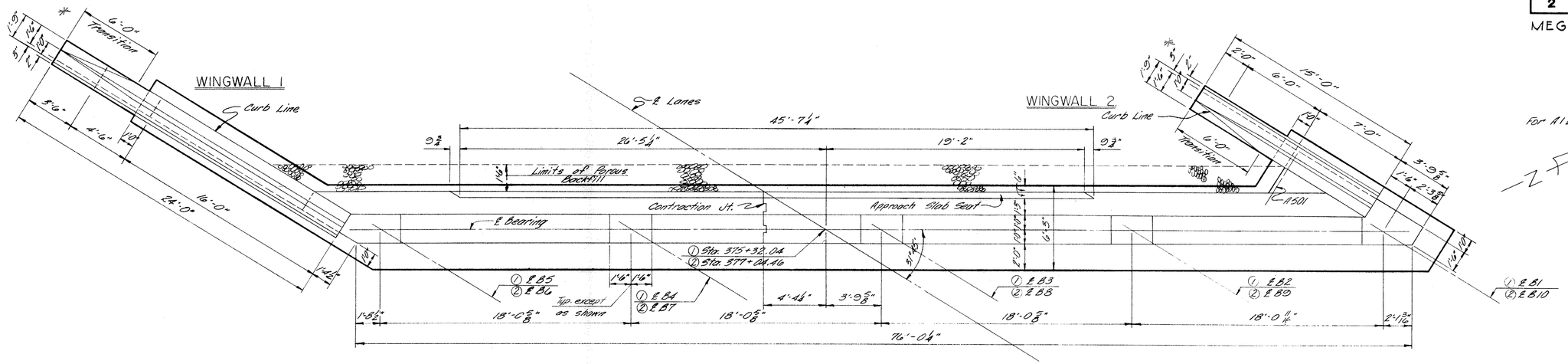
Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
				LEN	10-70	

For parapet transition note see sheet 4/13

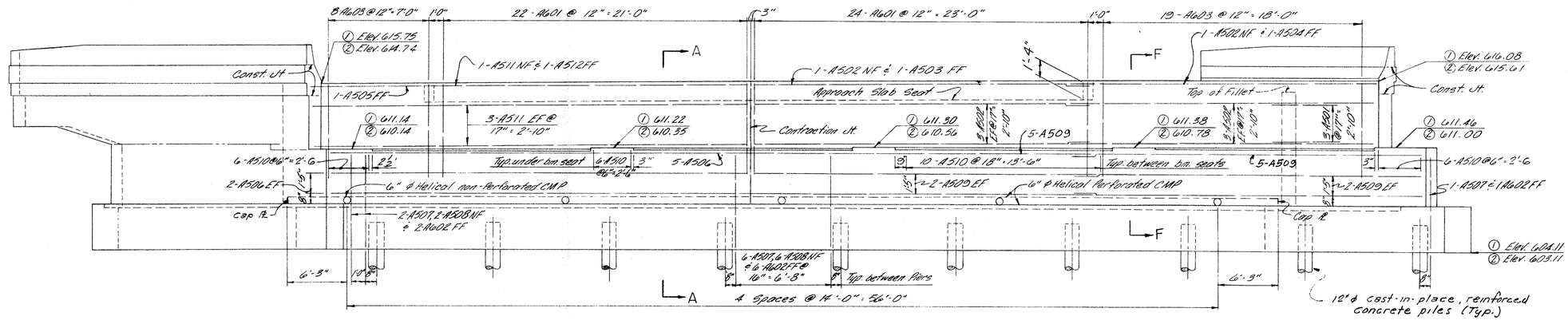
FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

MEG-7-6.15

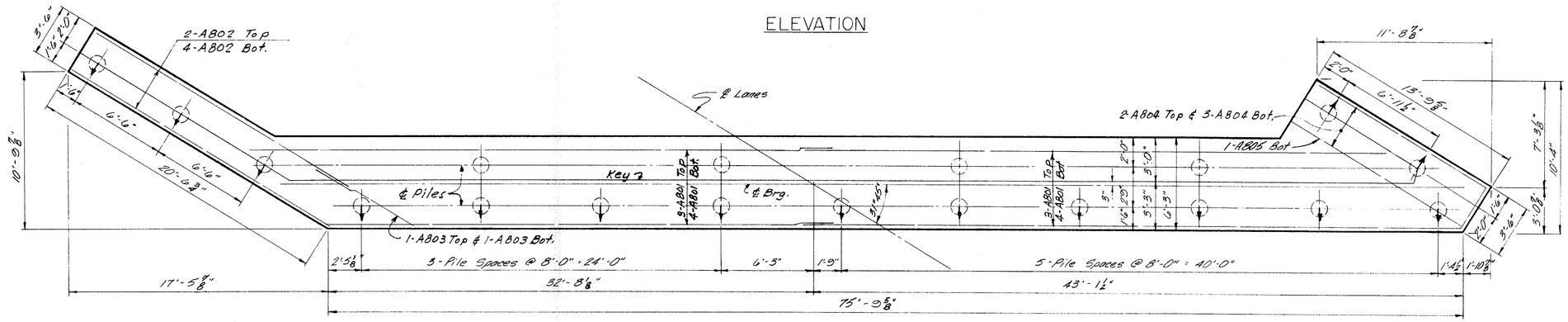
395
467



PLAN



ELEVATION



FOOTING PLAN

NOTES:
1. Porous backfill 1'-6" thick full length of abutment and wings, shall extend up to the subgrade or to the finished ground surface
2. For end dam details see Std. Dwg SD-1-69 Sh. 1&2 of 4
3. For wingwalls see Sh. 4
4. For Sections A-A & F-F, and Contraction Joint Detail see Sh. 4
5. For Reinforcing Steel List see Sh. 12

LEGEND
EF = Each Face
NF = Near Face
FF = Far Face
ϕ indicates pile bottered 1:4 in direction shown.
① Abutment A1L
② Abutment A2R

AUBLE-MITCHELL-BURGESS & ASSOC. 3/13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

ABUTMENTS
A1L & A2R

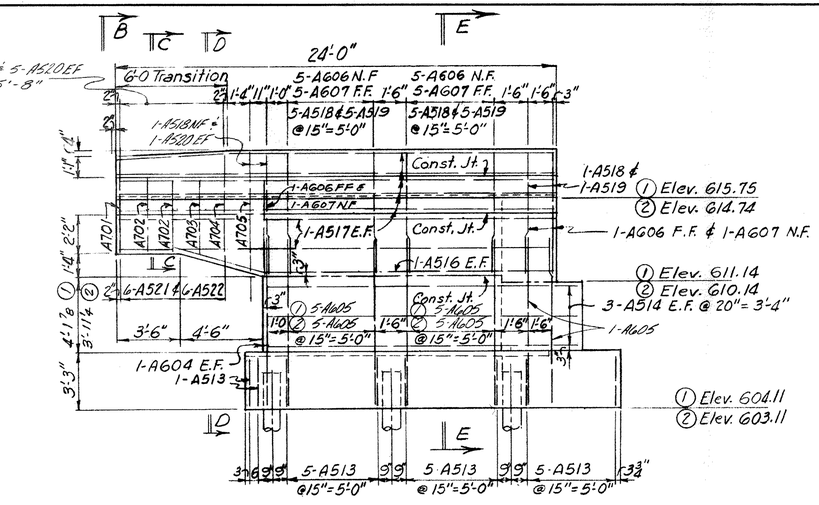
BRIDGE NO MEG-7-0713 L/R
S.R. 7 OVER S.R. 124
MEIGS COUNTY
STA. 375+27.76 TO STA. 378+28.32

Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
PAR	DAC		LPH	LEN	10-70	

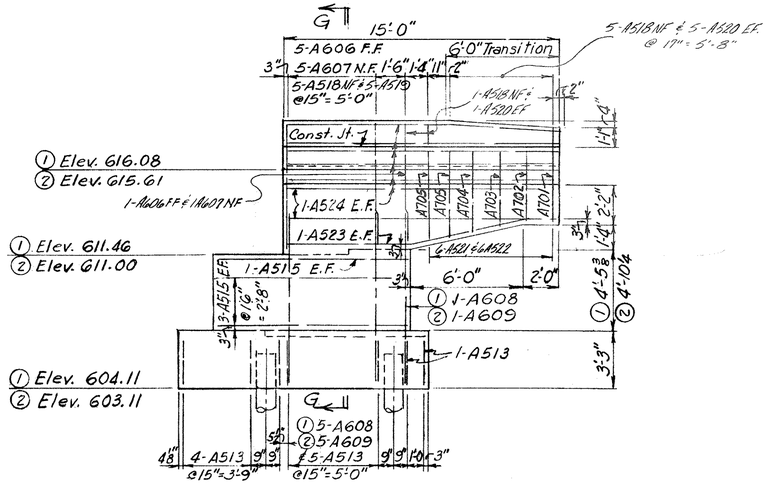
FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

396
467

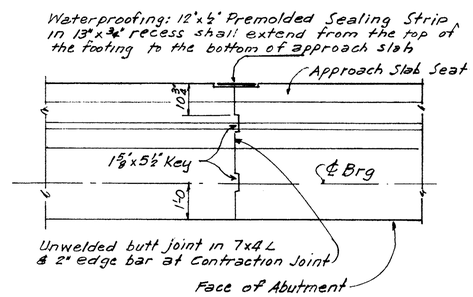
MEG-7-6.15



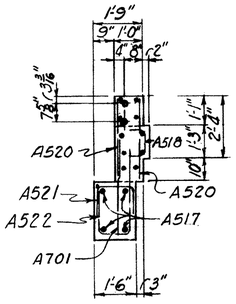
ELEVATION - WINGWALL 1



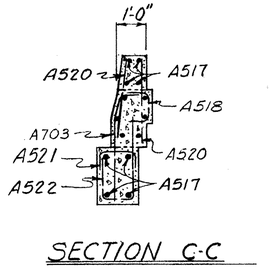
ELEVATION - WINGWALL 2



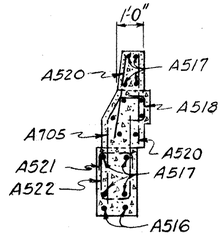
CONTRACTION JOINT DETAIL



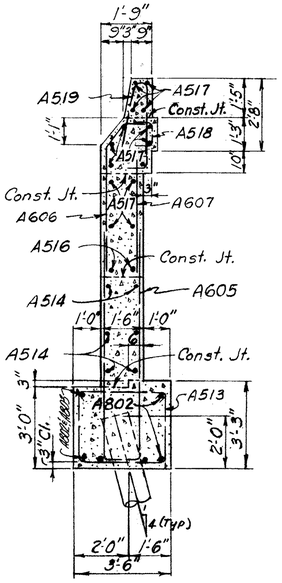
VIEW B-B



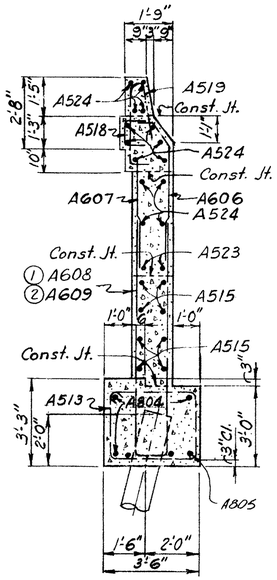
SECTION C-C



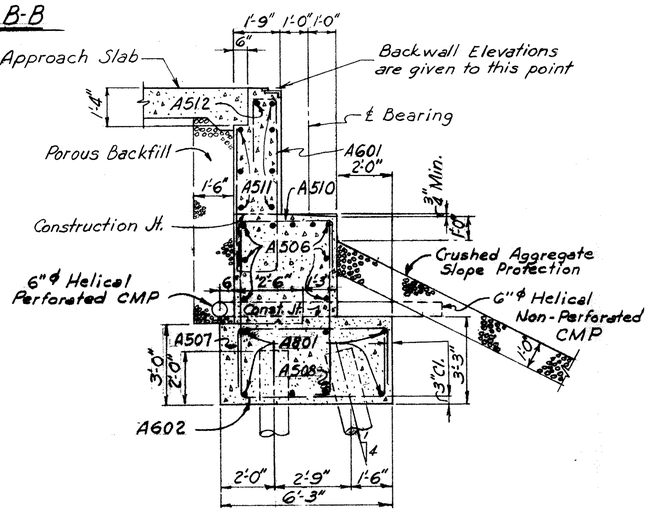
SECTION D-D



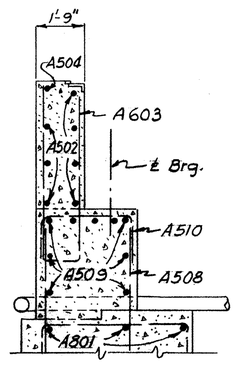
SECTION E-E



SECTION G-G



SECTION A-A



SECTION F-F

NOTES:
1. For additional railing details see Std. Dwg. BR-1-67 sh. 1 of 3.
2. For location of sections A-A, & F-F see Sh. 3
3. For reinforcing steel list see Sh. 12

PARAPET TRANSITION AND WINGWALL ENDS shall be as shown on Std. Drwg. BR-1-67 revised 1-1-71. Reinforcing steel shall be field bent or cut to fit the revised shape.

LEGEND
E.F. = Each Face
N.F. = Near Face
F.F. = Far Face
① Abutment A1L
② Abutment A2R

AUBLE-MITCHELL-BURGESS & ASSOC. 4/13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

WINGWALLS
1 & 2
BRIDGE NO. MEG-7-0713 L/R
S.R. 7 OVER S.R. 124
STA. 375 + 27.76 TO
MEIGS COUNTY STA. 378 + 28.32

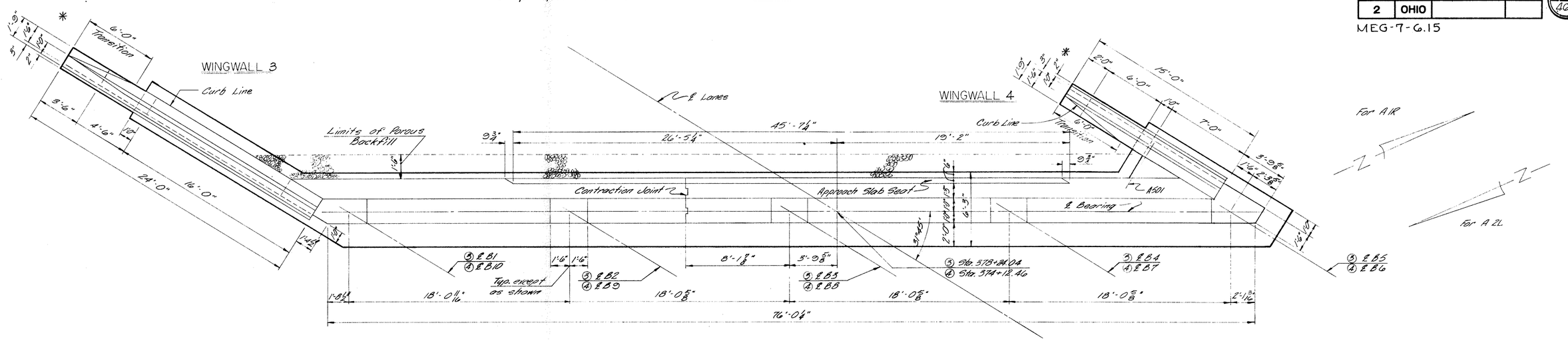
Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
PAR	PAR		LPH	LEN	10-70	

For parapet transition note see sheet 4/13

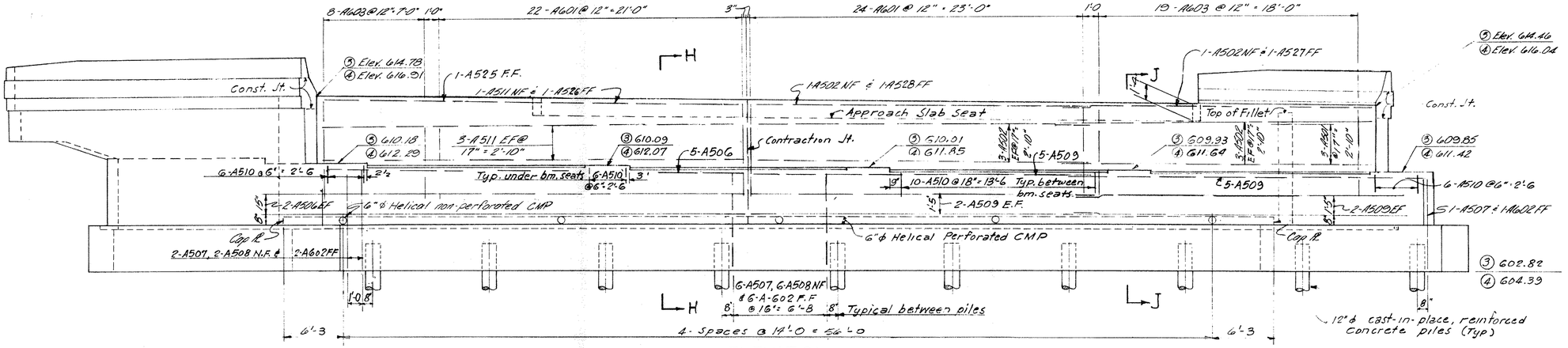
FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

397/467

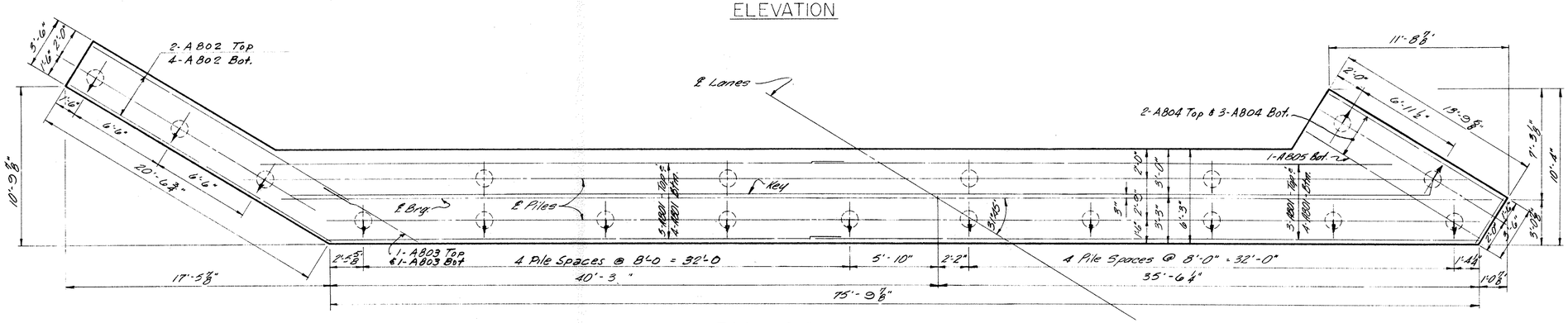
MEG-7-6.15



PLAN



ELEVATION



FOOTING PLAN

NOTES:
1. Porous backfill 1'-6" thick full length of abutment shall extend up to the subgrade or to the finished ground surface.
2. For end dam details see Std. Dwg SD-1-69 Sh. 1 & 2 of 4
3. For wingwalls see Sh. 6
4. For sections H-H & J-J see Sh 6
5. For Contraction Joint Detail see Sh. 4
6. For Reinforcing Steel List see Sh. 12

LEGEND:
E.F = Each Face
N.F = Near Face
F.F = Far Face
(1) indicates pile battered 1:4 in direction shown.
(3) Abutment A2L
(4) Abutment A1R

AUBLE-MITCHELL-BURGESS & ASSOC. 5/13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

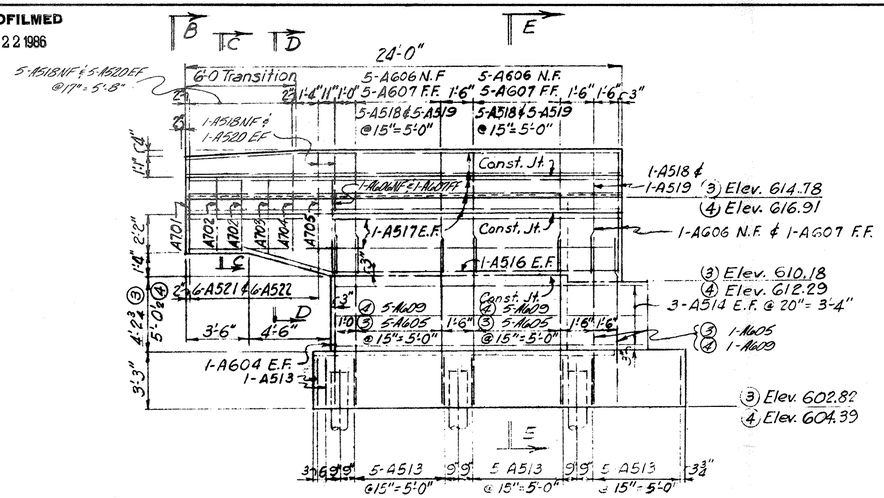
ABUTMENTS
A1R & A2L
BRIDGE NO MEG-7-0713 L/R
S.R. 7 OVER S.R. 124
MEIGS COUNTY
STA. 375+27.76 10
STA. 378+28.32

Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
PAR	DAC		LPH	LEN	10-70	

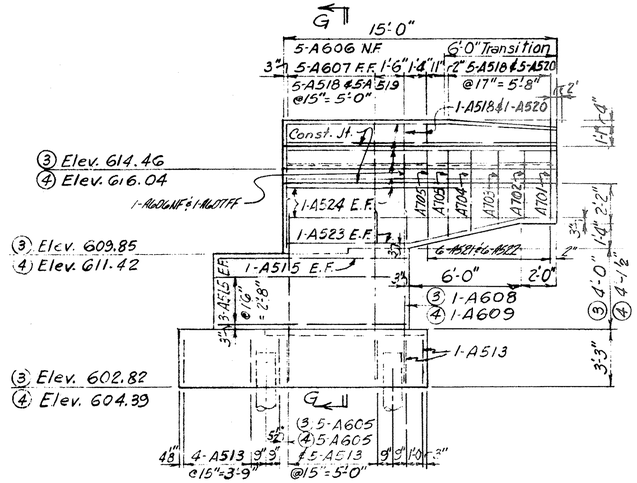
FED. ROAD DIVISION	STATE	PROJECT		
2	OHIO			

398
407

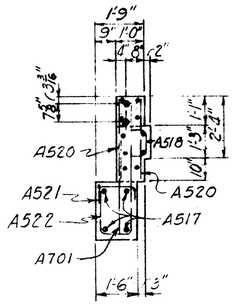
MEG-7-6.15

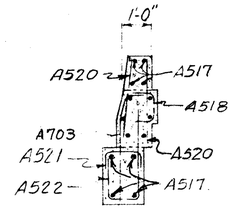


ELEVATION - WINGWALL 3

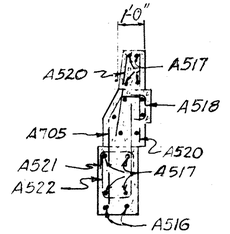


ELEVATION - WINGWALL 4

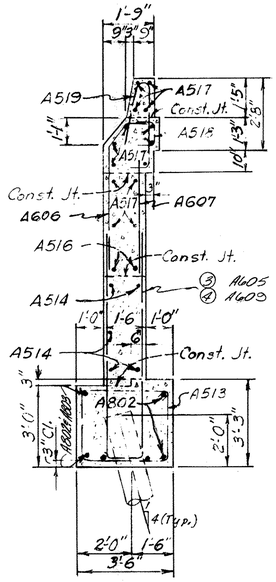




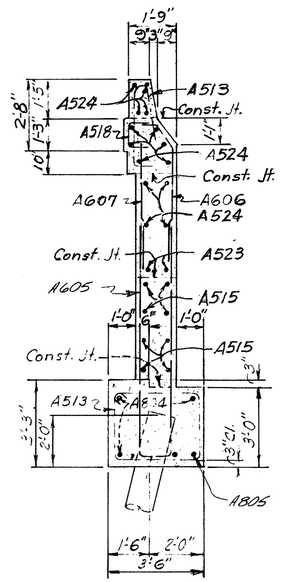
SECTION C-C



SECTION D-D

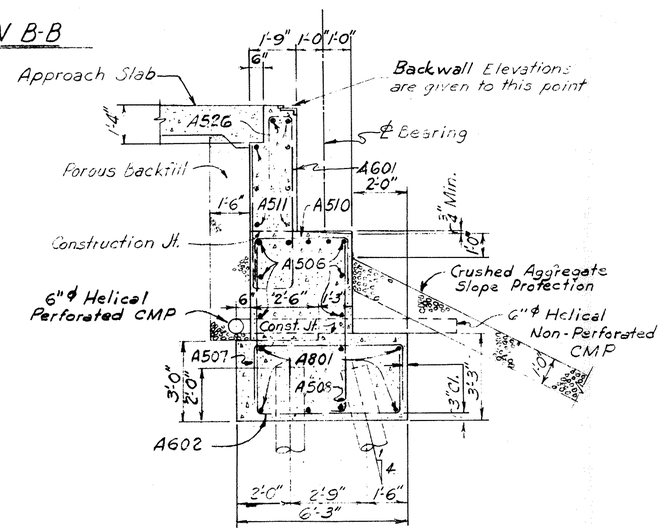


SECTION E-E

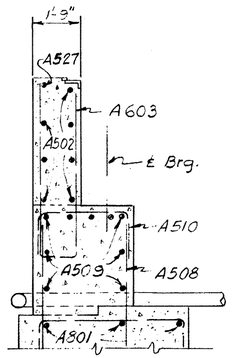


SECTION G-G

VIEW B-B



SECTION H-H



SECTION J-J

Notes:
1. For additional railing details see Std. Dwg BR-1-67 Sh. 1 of 3
2. For location of Sections H-H & J-J see Sh. 5
3. For reinforcing steel list see Sh. 12
4. For parapet transition note see sheet 4/13

LEGEND:
E.F. = Each Face
N.F. = Near Face
F.F. = Far Face
3 Abut. A2L
4 Abut. A1R

AUBLE-MITCHELL-BURGESS & ASSOC.
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO
6/13

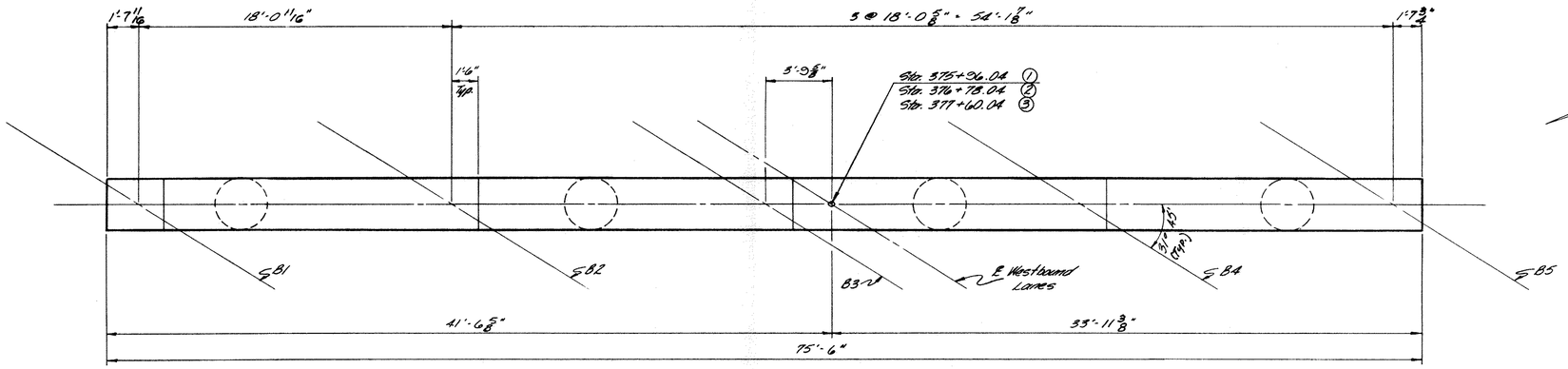
WINGWALLS
3 & 4

BRIDGE NO. MEG-7-0713 L/R
S.R.7 OVER S.R. 124
STA. 375 + 27.76 TO
MEIGS COUNTY STA. 378 + 28.32

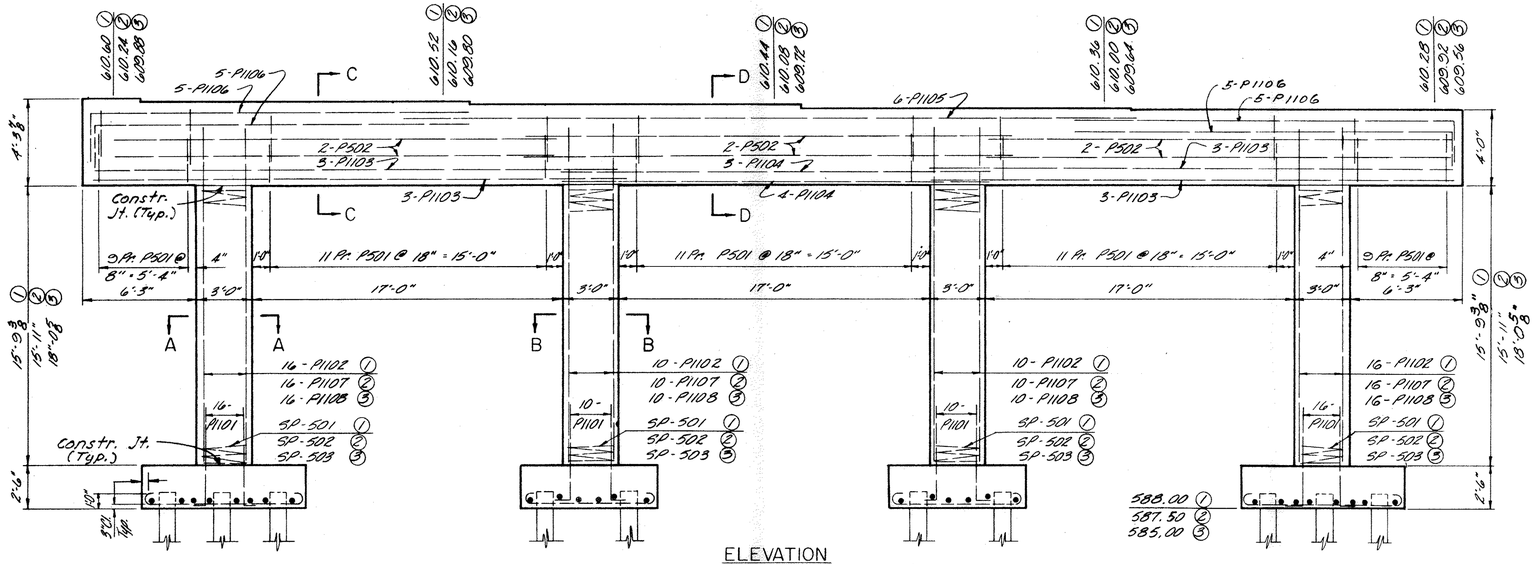
Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
PAR	DAC		LPH	LEN	10-70	

FED. AID PROJECT	STATE	PROJECT	
2	OHIO		

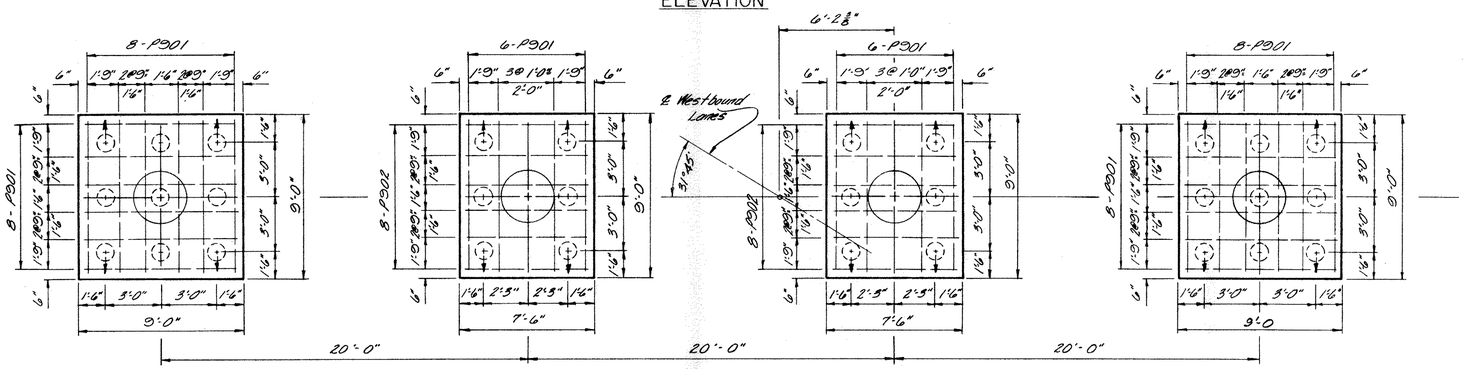
MEG-7-6.15



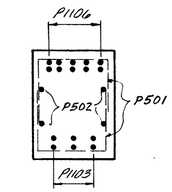
PLAN PIERS 1L, 2L, 3L



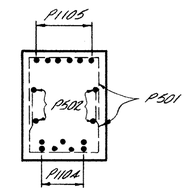
ELEVATION



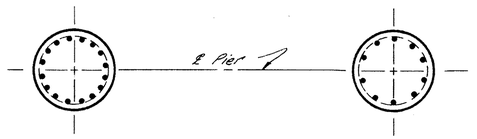
FOOTING PLAN



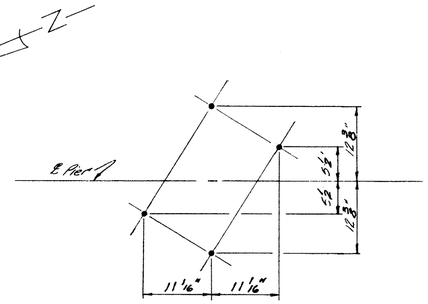
SECTION C-C



SECTION D-D



SECTION A-A SECTION B-B



TYPICAL ANCHOR BAR LAYOUT
PIER 2 ONLY

LEGEND:
① Pier 1L
② Pier 2L
③ Pier 3L
Indicates pile battered 1:4 in direction shown.

NOTES:
1. Special care shall be taken in placing reinforcing steel in the vicinity of the bridge seat so as to avoid interference with the drilling of Anchor Bar Holes.
2. For Reinforcing Steel List, see Sheet 12

AUBLE-MITCHELL-BURGESS & ASSOC. 7/13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

PIERS 1L, 2L, 3L

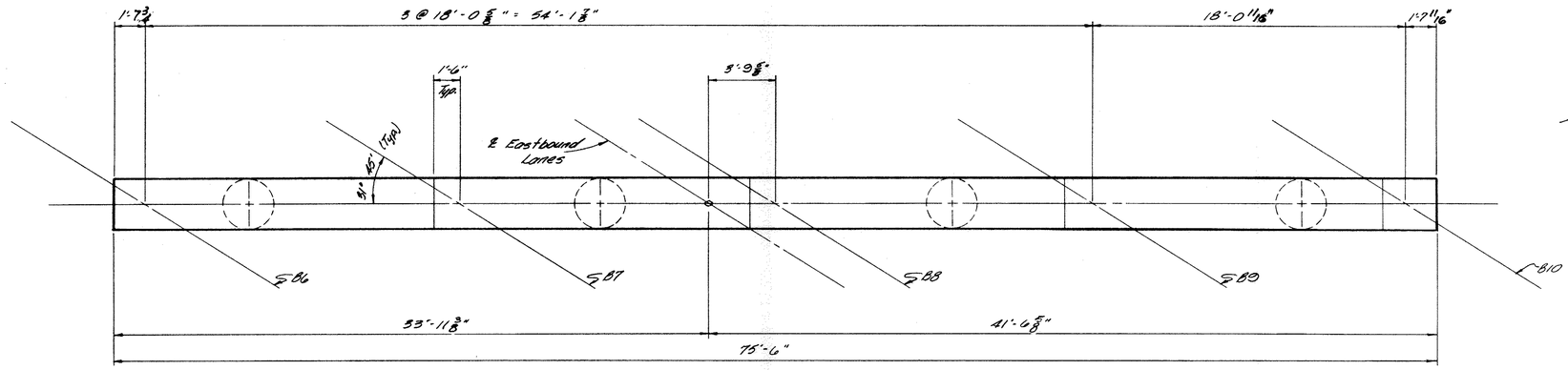
BRIDGE NO. MEG.-7-0713 L/R
S.R. 7 OVER S.R. 124
MEIGS COUNTY
STA. 375+27.76 TO
STA. 378+28.32

Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
ERB	DAC		ERB	LEN	10-70	

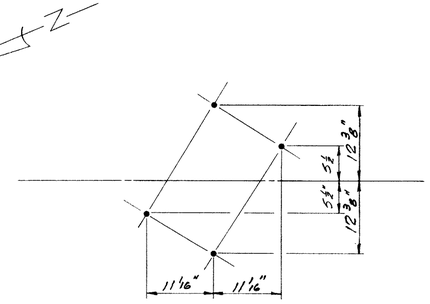
FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

400
467

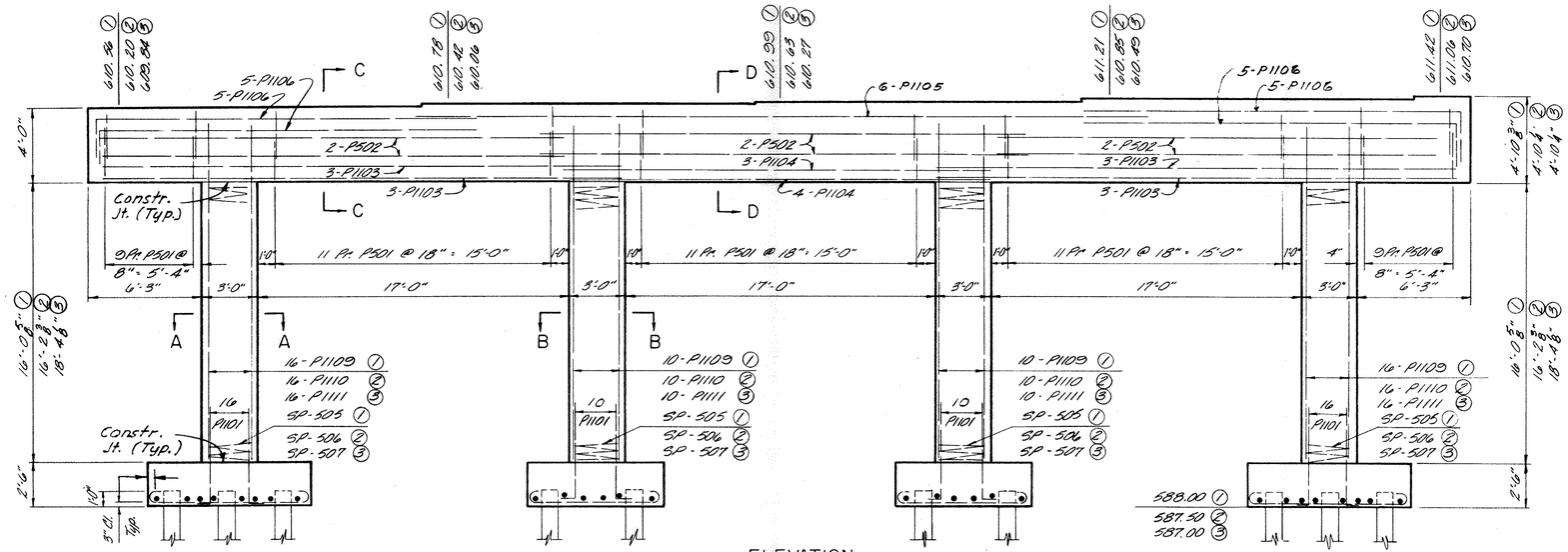
MEG-7-6.15



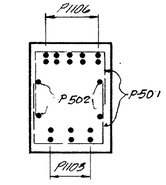
PLAN PIERS 1R, 2R, 3R



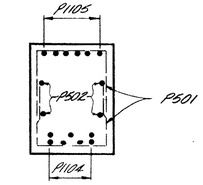
TYPICAL ANCHOR BAR LAYOUT
PIER 2 ONLY



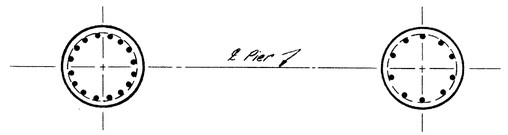
ELEVATION



SECTION C-C

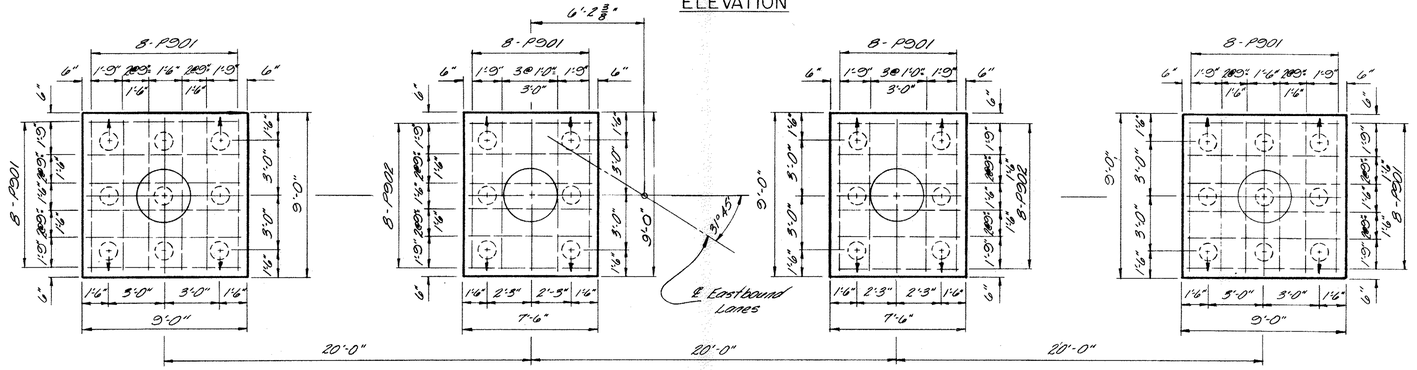


SECTION D-D



SECTION A-A

SECTION B-B



FOOTING PLAN

NOTES:
1. Special care shall be taken in placing reinforcing steel in the vicinity of the bridge seat so as to avoid interference with the drilling of Anchor Bar Holes.
2. For Reinforcing Steel List, see Sheet 12

LEGEND:
(1) Pier 1R
(2) Pier 2R
(3) Pier 3R
Indicates pile battered 1:4 in direction shown.

AUBLE-MITCHELL-BURGESS & ASSOC. 8 | 13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

PIERS
1R 2R. 3R

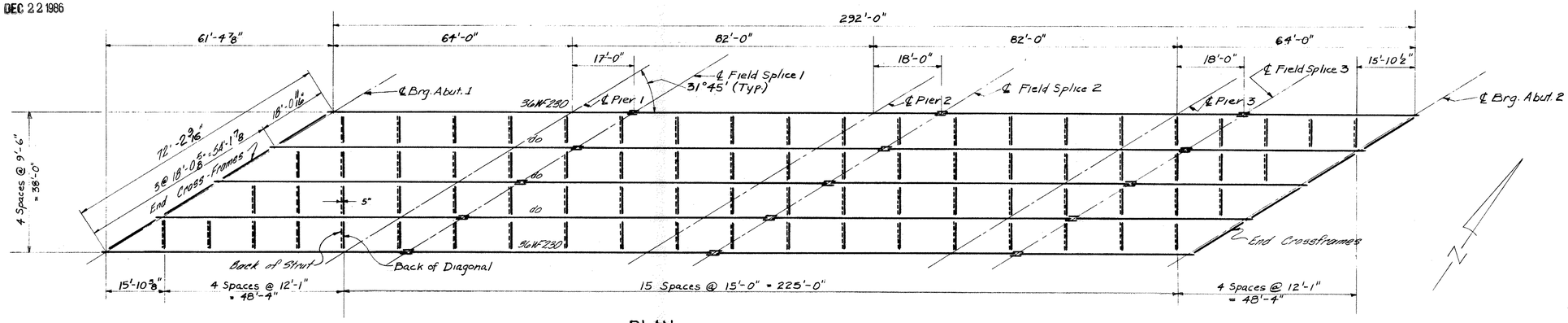
BRIDGE NO. MEG.-7-0713 L/R
S.R. 7 OVER S.R. 124
STA. 375+27.76 TO
STA. 378+28.32
MEIGS COUNTY

Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
ERB	DAC		ERB	LEN	10-70	

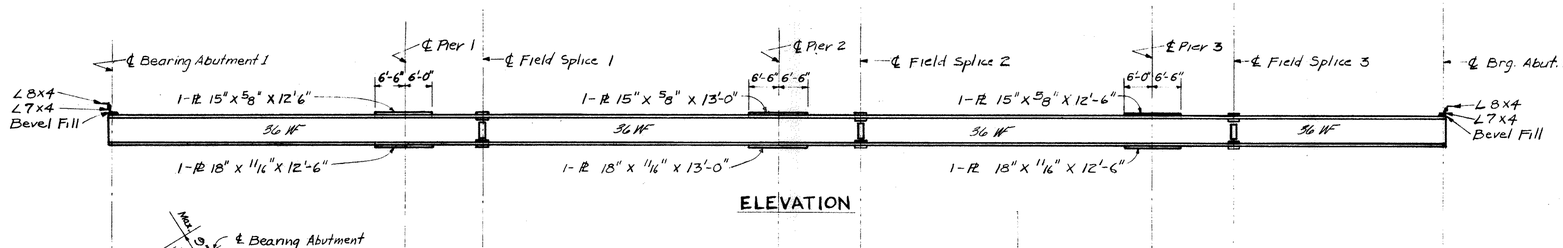
FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

401/467

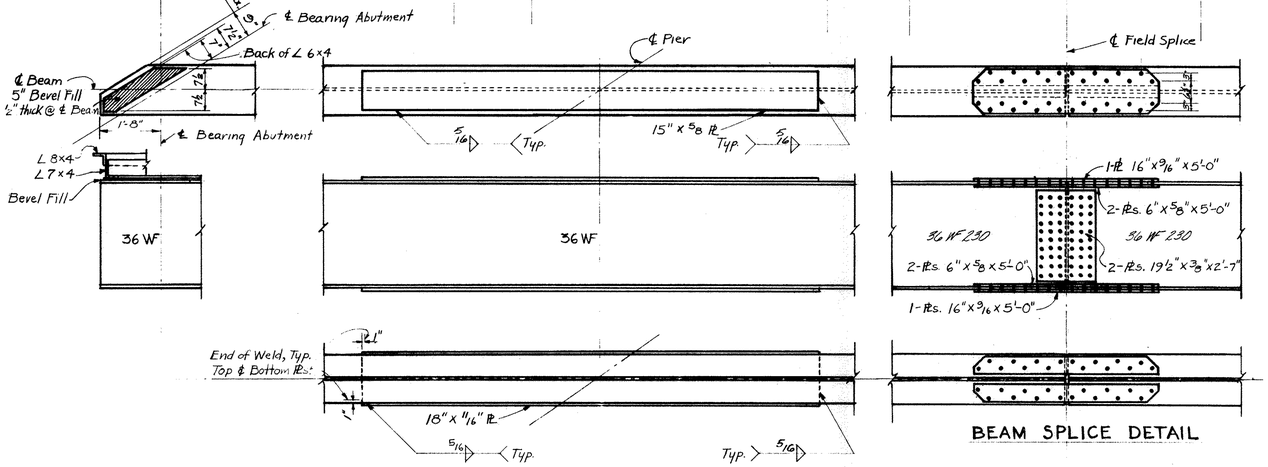
MEG-7-6.15



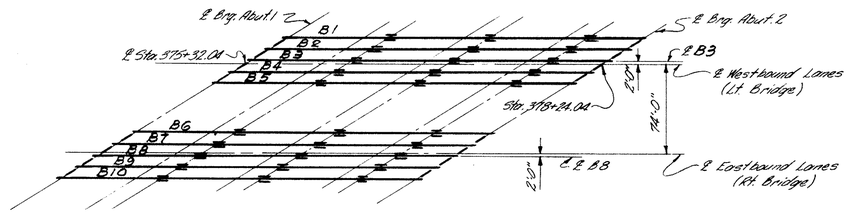
PLAN



ELEVATION

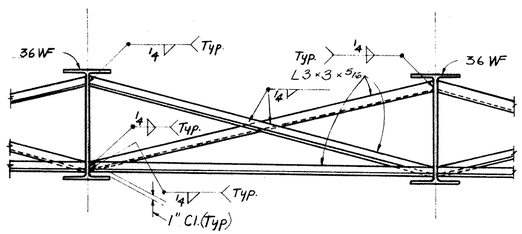


BEAM SPLICE DETAIL

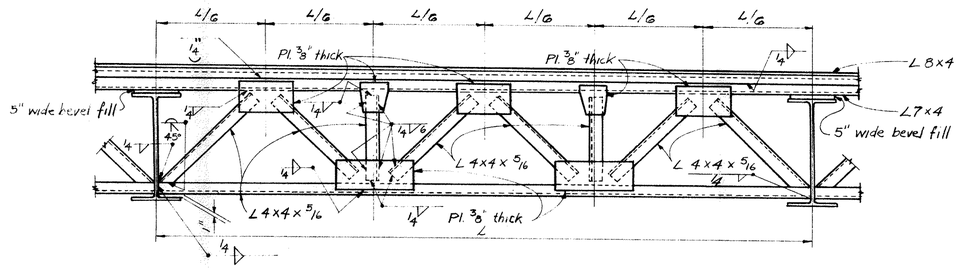


KEY PLAN

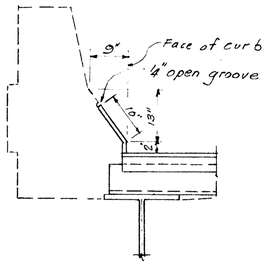
NOTES;
1. For End Crossframe & End Dam Details, see Std. Dwg. SD-1-69, Sheet 1 of 4.
2. For Curb Plate Details at End Dam, see Std. Dwg. SD-1-69, Sheet 2 of 4.
3. For Rocker & Bolster Details, see Std. Dwg. RB-1-55.
4. For additional details of Field Splices, see Std. Dwg. SD-1-69, Sheet 4 of 4.
5. For Screed Elevations, see Sheet 11
6. For Scupper Location, see Sheet 11
7. For Standard Scupper Details, see Std. Dwg. SD-1-69, Sheet 3 of 4.
8. All centerline bearings, field splices & crossframes are normal to baseline.
9. For Field Splices, all nuts shall be ½" ϕ for 1" ϕ High Strength Bolts.
10. For Elevation Layout & Deflection & Camber Table see Sheet 10



TYPICAL INTERMEDIATE CROSSFRAME



END CROSSFRAME



END DAM DETAIL AT CURB

AUBLE-MITCHELL-BURGESS & ASSOC. 9/13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

FRAMING PLAN

BRIDGE NO MEG-7-0713 L/R
S.R. 7 OVER S.R. 124
MEIGS COUNTY
STA. 375+27.76 TO
STA. 378+28.32

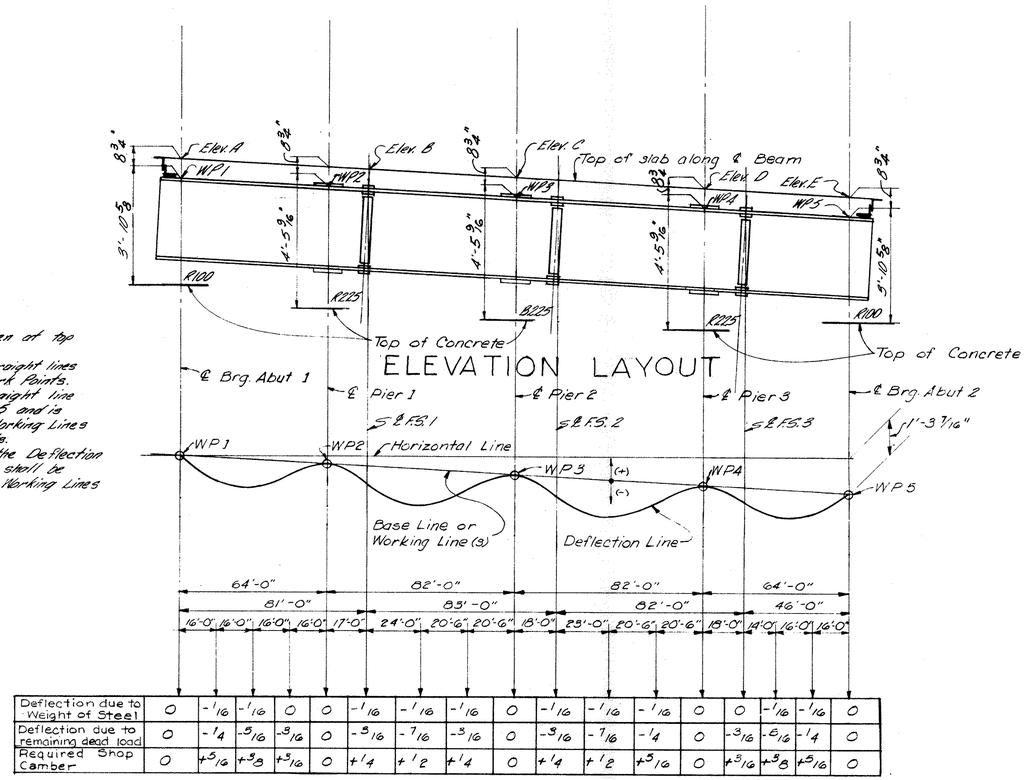
Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
ERB	JDR		ERB	LEN	10-70	

FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

402
A67

MEG-7-6.15

NOTES;
1. Work Points are given at top of beam (W.P.)
2. Working Lines are straight lines between indicated Work Points.
3. Base Line is a straight line between W.P.1 & W.P.5 and is coincidental with Working Lines between Work Points.
4. Tabulated Values in the Deflection and Camber Table shall be measured from the Working Lines.



Deflection due to Weight of Steel	0	-1/16	-1/16	0	0	-1/16	-1/16	-1/16	0	-1/16	-1/16	-1/16	0	0	-1/16	-1/16	0
Deflection due to remaining dead load	0	-1/4	-5/16	-3/16	0	-5/16	-7/16	-5/16	0	-5/16	-7/16	-1/4	0	-3/16	-6/16	-1/4	0
Required Shop Camber	0	+5/16	+3/8	+5/16	0	+1/4	+1/2	+1/4	0	+1/4	+1/2	+5/16	0	+5/16	+3/8	+5/16	0

DEFLECTION & CAMBER

TABLE OF ELEVATIONS

BEAM	ELEV. A	ELEV. B	ELEV. C	ELEV. D	ELEV. E
B1	616.075	615.783	615.482	615.071	614.790
B2	615.934	615.712	615.352	614.991	614.709
B3	615.913	615.631	615.270	614.909	614.628
B4	615.833	615.551	615.190	614.829	614.548
B5	615.752	615.470	615.110	614.749	614.467
B6	616.036	615.755	615.394	615.033	614.752
B7	616.252	615.971	615.610	615.249	614.967
B8	616.464	616.183	615.822	615.461	615.180
B9	616.684	616.402	616.041	615.681	615.399
B10	616.899	616.618	616.257	615.896	615.615

NOTE:
For Framing Plan and additional notes see sh. 9

AUBLE-MITCHELL-BURGESS & ASSOC. 10 | 13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

FRAMING PLAN DETAILS

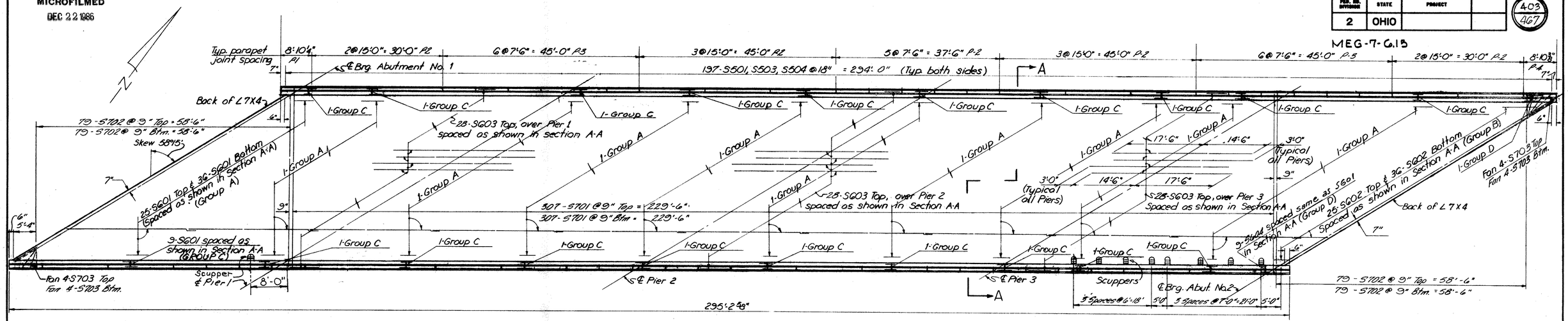
BRIDGE NO. MEG-7-0713 L/R
S.R. 7 OVER S.R. 124
MEIGS COUNTY
STA. 375+27.76 TO STA. 378+28.32

Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
ERB	DAC		ERB	LEN	10-70	

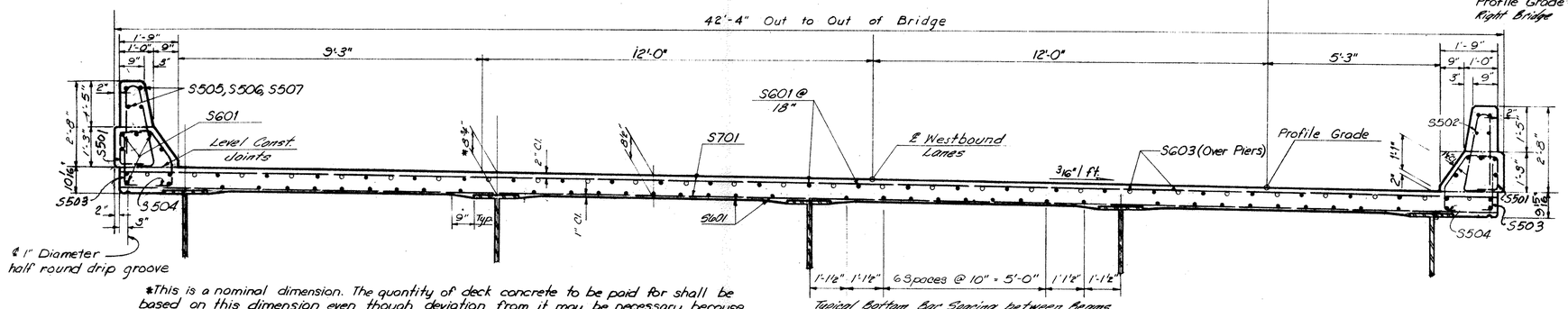
FED. RD. SYSTEM	STATE	PROJECT	
2	OHIO		

403
467

MEG-7-6.13



SLAB PLAN



*This is a nominal dimension. The quantity of deck concrete to be paid for shall be based on this dimension even though deviation from it may be necessary because the top flange of the girder may not have the exact Camber or Conformation required to place it parallel to the finished grade. Deduction shall be made for volume of encased steel plates as per Sec. 511.18 of the Construction and Material Specifications.

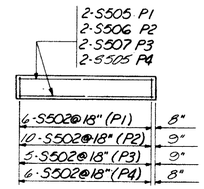
STATION	Profile Grade	LEFT BRIDGE		RIGHT BRIDGE	
		Left Curb Line	Right Curb Line	Left Curb Line	Right Curb Line
373+75	616.40	–	–	–	–
374+00	616.29	–	–	–	616.81
+25	616.18	–	–	–	616.70
+50	616.07	–	–	615.99	616.59
+75	615.96	–	–	615.88	616.48
375+00	615.85	–	–	615.77	616.37
+25	615.74	–	615.66	615.66	616.26
+50	615.63	–	615.55	615.55	616.15
+75	615.52	616.04	615.44	615.44	616.04
376+00	615.41	615.93	615.33	615.33	615.93
+25	615.30	615.82	615.22	615.22	615.82
+50	615.19	615.71	615.11	615.11	615.71
+75	615.08	615.60	615.00	615.00	–
377+00	614.97	615.49	614.89	614.89	–
+25	614.86	615.38	614.78	614.78	–
+50	614.75	615.27	614.67	–	–
+75	614.64	615.16	614.56	–	–
378+00	614.53	615.05	–	–	–
+25	614.42	614.94	–	–	–
+50	614.31	614.83	–	–	–

SECTION A-A

TYPICAL SECTION

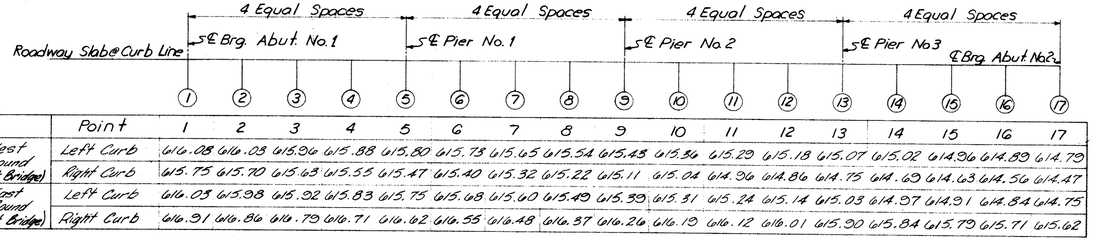
Westbound Lanes Shown

PANEL	NO. REQ'D	LENGTH
P1	2	8'-10¼"
P2	20	15'-0"
P3	34	7'-6"
P4	2	8'-10⅜"



PARAPET PANELS

NOTES:
1. Deck Slab Haunch: A typical haunch width of 9" shall be used for computing quantity of concrete. However the haunch width may vary between 6" & 12" provided that the slope shall be not more than 1:4 for a haunch less than 9" in width.
2. Spread or Cut Reinforcing Steel in slab to clear Scuppers.
3. Slab thickness includes 1" for monolithic wearing surfaces.
4. For End Dam & Curb Plate details, see Std. Dwg. SD-1-69, Sheets 1 & 2 of 4.
5. For Reinforcing Steel List & Bar Bending Details, see Sheet 12 & 13
6. For Scupper Details, see Std. Dwg. SD-1-69, Sheet 3 of 4.
7. For Railing & Parapet Joint Details, see Std. Dwg. BR-1-67, Sheet 1 of 3.
8. Scupper spacing applies to both bridges, place scuppers along low gutter



	Point	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17
West Bound (Lt. Bridge)	Left Curb	616.08	616.03	615.96	615.88	615.80	615.73	615.65	615.54	615.45	615.36	615.29	615.18	615.07	615.02	614.96	614.89	614.79
	Right Curb	615.75	615.70	615.63	615.55	615.47	615.40	615.32	615.22	615.11	615.04	614.96	614.86	614.75	614.69	614.63	614.56	614.47
East Bound (Rt. Bridge)	Left Curb	616.03	615.98	615.92	615.83	615.75	615.68	615.60	615.49	615.39	615.31	615.24	615.14	615.03	614.97	614.91	614.84	614.75
	Right Curb	616.91	616.86	616.79	616.71	616.62	616.55	616.48	616.37	616.26	616.18	616.12	616.01	615.90	615.84	615.79	615.71	615.62

SCREED ELEVATIONS

(Adjusted For Slab Deflections)

AUBLE-MITCHELL-BURGESS & ASSOC. 11/13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

SUPERSTRUCTURE
ROADWAY SLAB

BRIDGE NO MEG-7-0713 L/R
S.R. 7 OVER S.R. 124
MEIGS COUNTY
STA. 375+27.76 TO
STA. 378+28.32

Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
ERB	JHD		ERB	LEN	10-70	

MICROFILMED
DEC 22 1986

ABUTMENTS					PIERS					SUPERSTRUCTURE					REPLACEMENT BARS				
MARK	NO.	LENGTH	TYPE	WEIGHT	MARK	NO.	LENGTH	TYPE	WEIGHT	MARK	NO.	LENGTH	TYPE	WEIGHT	MARK	NO.	LENGTH	TYPE	WEIGHT
A501	24	4'-0	Str	100	P501	612	9 - 9	Bt	6224	S501	788	2' - 0"	Bt.	1644	RE5	3	6'-7	Str.	
A502	56	22'-10	Str	1334	P502	72	26' - 1"	Str.	1959	S502	788	5' - 4"	Bt.	4383	RE6	6	6'-11	Str.	
A503	2	24'-11	Str	52						S503	788	2' - 3"	Bt	1849	RE7	7	7'-3	Str.	
A504	2	20'-0	Str	42						S504	788	3 - 2	Bt.	2603	RE8	1	7'-6	Str.	
A505	2	12'-0	Str	25						S505	32	8' - 6"	Str.	284	RE9	1	7'-10	Str.	
A506	36	29'-4	Str	1101						S506	160	14' - 8	Str.	2448	RE11	5	8'-7	Str.	
A507	228	8'-3	Bt	1962						S507	272	7' - 2"	Str.	2033					
A508	224	7'-0	Bt	1635															
A509	72	23'-10	Str	1790															
A510	280	6'-4	Bt	1850															
A511	28	31'-10	Str	930	P901	264	11' - 0"	Bt.	9874										
A512	2	24'-4	Str	51	P902	96	9' - 6"	Bt.	3101										
A513	112	11'-7	Bt	1353															
A514	24	17'-0	Str	426															
A515	32	10'-7	Str	353						S601	1580	30' - 0"	Str.	71195					
A516	8	21'-10	Bt	182						S602	122	14' - 0"	Str.	2565					
A517	56	23'-8	Str	1382						S603	168	32' - 0	Str.	8075					
A518	124	2'-0	Bt	259						S604	36	15'- 0	Str.	811					
A519	68	6'-4	Bt	449															
A520	112	3'-0	Str	350															
A521	48	2'-3	Bt	113	P1101	312	7' - 0"	Bt.	11604										
A522	48	6'-2	Bt	309	P1102	52	19' - 4	Str.	5341										
A523	8	13'-8	Bt	114	P1103	72	29' - 4"	Str.	11221										
A524	56	14'-8	Str	857	P1104	42	23' - 7"	Str.	5263										
A525	2	39'-4	Str	82	P1105	36	40' - 0"	Str.	7651	S701	1228	41 - 8	Str.	104585					
A526	2	16'-10	Str	35	P1106	120	24' - 0"	Bt.	15301	S702	632	4-8 to 40-5 by 5½ 8 Ea.	Str.	29120					
A527	2	10'-0	Str	21	P1107	52	19' - 6"	Str.	5387	S703	32	4 - 0	Str.	262					
A528	2	32'-5	Str	68	P1108	52	21' - 8"	Str.	5986										
					P1109	52	19' - 8"	Str.	5433										
					P1110	52	19' - 9"	Str.	5456										
					P1111	52	21' - 11"	Str.	6055										
A601	184	15'-5	Bt	4261															
A602	228	14'-1	Bt	4823															
A603	108	13'-8	Bt	2217															
A604	8	9'-0	Str	108															
A605	48	19'-0	Bt	1370	SP501	4	15' - 9"	Bt.	2350										
A606	76	6'-2	Bt	704	SP502	4	15' - 11"	Bt.	2374										
A607	76	5'-7	Str	637	SP503	4	18' - 1"	Bt.	2643										
A608	6	19'-8	Bt	177	SP504	4	16' - 0"	Bt.	2386										
A609	18	20'-8	Bt.	559	SP505	4	16' - 2"	Bt.	2410										
					SP506	4	18' - 4"	Bt.	2719										
A701	8	5'-0	Str	82															
A702	12	4'-9	Bt	117															
A703	8	4'-11	Bt	80															
A704	8	5'-6	Bt	90															
A705	12	6'-1	Bt	149															
A801	56	40'-0	Str	5981															
A802	24	20'-3	Str	1298															
A803	8	7'-3	Str	155															
A804	20	13'-5	Str	716															
A805	4	10'-6	Str	112															

FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

404/467

MEG-7-6.15

NOTES

1. BAR SIZE IS INDICATED IN THE BAR MARK. THE FIRST DIGIT WHERE THREE DIGITS ARE USED AND THE FIRST TWO DIGITS WHERE FOUR ARE USED, INDICATE THE BAR SIZE NUMBER.
2. SPIRAL REINFORCING BARS: THE "LENGTH" SHOWN IN THE STEEL LIST FOR THE SPIRAL BARS IS THE DISTANCE FROM TOP OF THE FOOTING TO THE BOTTOM OF CAP, OR TO WITHIN 2" (±) OF THE TOP OF COLUMN FOR PIERS WITHOUT CAPS, TO THE NEAREST INCH.

   FOUR STEEL CHANNEL, TEE OR ANGLE SPACERS WEIGHING APPROXIMATELY 0.80 LB. PER LIN. FT. OF SPACER, SHALL BE PROVIDED FOR EACH SPIRAL UNIT. THEY SHALL BE EQUALLY SPACED ALONG THE PERIPHERY OF THE COIL. THE NUMBER OF POUNDS OF THESE SPACERS, BASED ON 0.80 LB. PER LIN. FT., WILL BE PAID FOR AS REINFORCING STEEL AND IS INCLUDED IN THE TABULATED QUANTITY OF SPIRAL BARS.
3. SEE SHEET 13 FOR BENDING DETAILS.

AUBLE-MITCHELL-BURGESS & ASSOC.
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

12/13

REINFORCING STEEL LIST

BRIDGE NO. MEG.-7-0713 L/R
S.R. 7 OVER S.R. 124
STA. 375 + 27.76 TO
STA. 378 + 28.32
MEIGS COUNTY

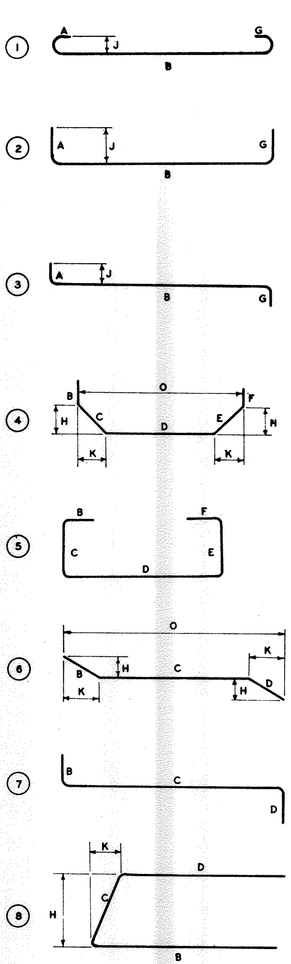
Designed	Drawn	Traced	Checked	Reviewed Date	Revised
				LEN 10-70	

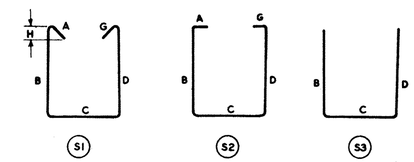
FED. RD. DIVISION	STATE	PROJECT	
2	OHIO		

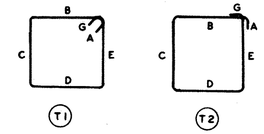
405
467

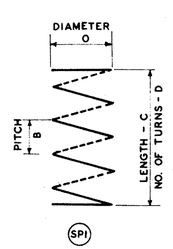
MEG-7-6.15

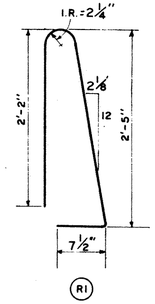
MARK	TYPE	DIMENSIONS FOR BENDING A	B	C	D	E	F	G	H	J	K	R	O
A507	5		-	1'-7	5'-4	1'-7	-						
A508	2	0'-6	6'-6					-					
A510	5		-	1'-7	3'-5	1'-7	-						
A513	T2	0'-5	3'-0	2'-6	3'-0	2'-6		0'-5					
A516	6	-	6'-0	15'-10	-				1'-8		5'-9		
A518	2	0'-6	1'-0					0'-6					
A519	R2	0'-6	3'-2										
A521	2	0'-6	1'-3					0'-6					
A522	5		1'-8	1'-3	3'-6	-	-						
A523	6	-	6'-10	6'-10	-				1'-6		6'-8		
A601	5		5'-2	1'-5	6'-3	0'-11	2'-0						
A602	5		2'-6	5'-4	6'-7	-	-						
A603	5		6'-4	1'-5	8'-3	-	-						
A605	5		3'-1	1'-2	9'-1	-	-						
A606	9	-	4'-3	1'-2	0'-9				0'-9		0'-11½		
A608	5		3'-5	1'-2	8'-5								
A609	5		3'-11	1'-2	9'-11								
A702	9	-	3'-0	1'-0	0'-9				0'-7½		0'-11½		
A703	9	-	3'-2	1'-0	0'-9				0'-4		0'-11½		
A704	9	-	3'-8	1'-1	0'-9				0'-6		0'-11½		
A705	9	-	4'-2	1'-2	0'-9				0'-9		0'-11½		
P501	S3		3' - 8"	2' - 8"	3'-8"								
P501	1	1'-5"	8' - 6"					1'-3"					
P502	1	1'-3"	7' - 0"					1'-3"					
P1101	2	1'-2"	5' - 10"					-					
P1106	5		-	3' - 2"	21' - 2	-	-						
SP501	SP1		0' - 3½"	15' - 9"	61								2' - 8"
SP502	SP1		0' - 3½"	15' - 11"	62								2' - 8"
SP503	SP1		0' - 3½"	18' - 1"	70								2' - 8"
SP504	SP1		0' - 3½"	16' - 0"	62								2' - 8"
SP505	SP1		0' - 3½"	16' - 2"	63								2' - 8"
SP506	SP1		0' - 3½"	18' - 4"	71								2' - 8"
S501	2	0'-6"	1' - 0"					0'-6"					
S502	R1							-					
S503	2	0'-6"	1' - 9"										
S504	9	0'-6"	0' - 10"	1' - 2½"	0' - 9"				0' - 9"		0'-11½"		

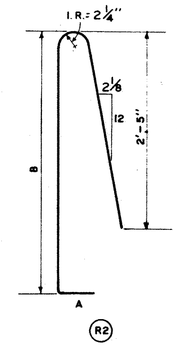












NOTES

1. FIGURES IN CIRCLES SHOW BAR TYPES.
2. ALL DIMENSIONS ARE OUT TO OUT OF BAR.
3. "J" DIMENSION ON HOOKS TO BE SHOWN ONLY WHERE NECESSARY TO RESTRICT HOOK SIZE, OTHERWISE STANDARD HOOKS ARE TO BE USED.
4. "H" DIMENSION ON STIRRUPS TO BE SHOWN WHERE NECESSARY TO RESTRICT HOOKS.
5. ALL BENDS SHOWN ARE BENT AROUND A STANDARD MANDREL, EXCEPT SPIRALS SPI, AND WHERE RADIUS "R" IS INDICATED.
6. RADIUS DIMENSION "R" IS TO OUTSIDE OF BAR.
7. THE LENGTH OF BENT BARS IS MEASURED ALONG THE CENTERLINE.
8. FOR STANDARD HOOK DIMENSIONS, SEE SECT. 509.05 OF THE SPECIFICATIONS.
9. FOR BAR TYPE SPI, THE NO OF TURNS "D" IS THE LENGTH "C" DIVIDED BY THE PITCH "B", PLUS 3 TURNS (TOTAL NUMBER OF CLOSED COILS), EXPRESSED AS THE NEAREST WHOLE NUMBER. 1½ CLOSED COILS SHALL BE PROVIDED AT THE ENDS OF EACH SPIRAL UNIT.

AUBLE-MITCHELL-BURGESS & ASSOC. 13 13
ENGINEERS AND ARCHITECTS
CINCINNATI, OHIO

BAR BENDING DETAILS

BRIDGE NO. MEG.-7-0713 L/R
S.R.7 OVER SR 124
MEIGS COUNTY
STA. 375+27.76 TO
STA. 378+28.32

Designed	Drawn	Traced	Checked	Reviewed	Date	Revised
				LEN	10-70	