

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

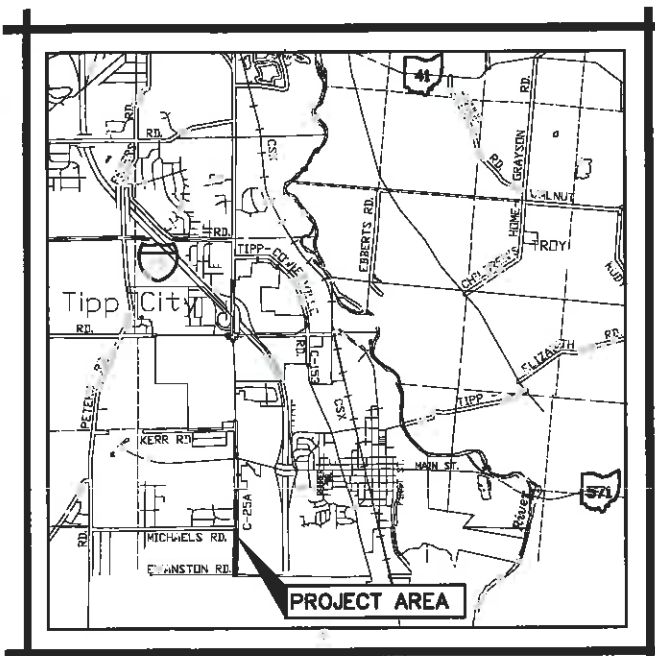
**MIA TIPP CITY 25A RECONSTRUCTION, PH. 2
PID #93245**

**SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
MIAMI COUNTY, OHIO**

INDEX OF SHEETS

PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF THE RECONSTRUCTION OF C.R. 25-A FROM MICHAELS RD. TO EVANSTON RD. CONSTRUCTION CONSISTS OF SIDEWALK, PAVEMENT, CURBING, THE INSTALLATION OF A NEW STORM SEWER SYSTEM, UTILITY RELOCATIONS, STREET LIGHTS AND RESTORATION OF TRAFFIC SIGNAGE. ALSO SIDEWALK WILL BE INSTALLED ALONG THE WEST ONLY FROM MICHAELS RD. TO MAIN ST. (SR 571).



LOCATION MAP

LATITUDE: N 39°-56'-53" LONGITUDE: W 84°-11'-56"



PORTION TO BE IMPROVED _____
STATE & FEDERAL ROUTES _____
OTHER ROADS _____

VICINITY MAP

DESIGN DESIGNATION MIA TIPP CITY 25A RECONSTRUCTION, PH. 2

CURRENT ADT (2017)	7,660
DESIGN YEAR ADT (2037)	8,300
DESIGN HOURLY VOLUME (2037)	830
DIRECTIONAL DISTRIBUTION	55%
TRUCKS (24 HOUR B&C)	4%
DESIGN SPEED	45 MPH
LEGAL SPEED	45 MPH

DESIGN FUNCTIONAL CLASSIFICATION - URBAN MINOR ARTERIAL
DESIGN EXCEPTIONS
NONE REQUIRED

**Addendum No.1 Revisions
Sheets Added**

DECEMBER 19, 2016

2016 SPECIFICATIONS

THE CONSTRUCTION STANDARDS AND DRAWINGS OF THE CITY OF TIPP CITY AND THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND GENERAL NOTES LISTED IN THE PLAN, SHALL GOVERN THIS IMPROVEMENT. THE MOST RESTRICTIVE SHALL APPLY.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA	= 5.39 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA	= 0.50 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA	= 5.89 ACRES

UNDERGROUND UTILITIES
CALL BEFORE YOU DIG
OHIO Utilities Protection SERVICE
1-800-382-2764
(Non-members must be called directly)
OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:

ChoiceOne Engineering
905 WISHER ROAD | SIDNEY, OHIO 45365 | 937.497.0200
16LAND AVENUE | LOVELAND, OHIO 45140 | 513.239.8554
7IAN STREET | PORTLAND, INDIANA 47371 | 260.766.2500
www.CHOICEONEENGINEERING.com

ENGINEER'S SEAL:

SIGNED: _____
DATE: _____

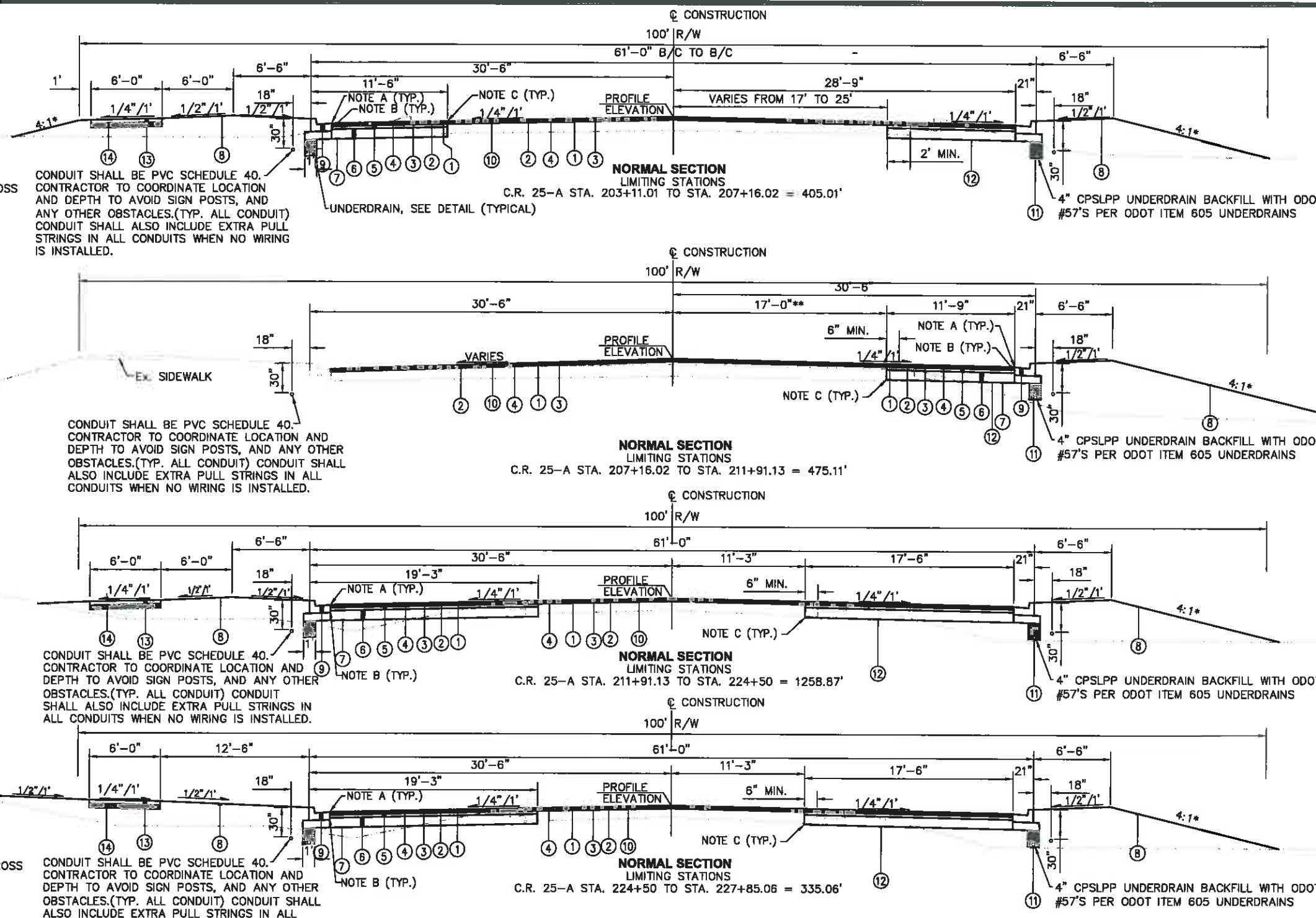
CITY OF TIPP CITY STANDARD CONSTRUCTION DRAWINGS				ODOT STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	
300-4	06-01-15	800-7	06-01-15			HL-20.11	01-16-15	800	10-21-16
300-5	06-01-15	800-8	06-01-15	BP 3.1	07-18-14	HL-30.11	01-16-15	809	04-15-16
300-6	06-01-15	800-10	06-01-15	BP 7.1	07-18-14	HL-30.21	01-17-14	821	04-20-12
300-8	06-01-15	800-11	06-01-15			HL-30.22	01-17-14	832	01-17-14
300-9	06-01-15	800-13	06-01-15			HL-60.11	01-15-16		
300-10	06-01-15			HW 2.2	01-15-16	HL-60.12	07-15-16		
300-12	06-01-15					MT-95.31	07-18-14		
500-4	06-01-15					MT-95.32	07-18-14		
600-2	06-01-15					MT-95.50	10-16-15	TC-41.20	10-18-13
600-8	06-01-15					MT-95.60	07-19-13	TC-42.20	10-18-13
600-10	06-01-15					MT-97.10	07-18-14	TC-52.10	10-18-13
800-1	06-01-15					MT-97.12	07-18-14	TC-52.20	07-18-14
800-2	06-01-15					MT-99.20	07-19-13	TC-71.10	01-17-14
800-5	06-01-15					MT-101.60	07-19-13	TC-82.10	10-18-13
800-6	06-01-15					MT-101.90	07-17-15	TC-84.20	10-18-13
						MT-105.10	07-19-13	TC-84.21	10-18-13

Approved: _____
Date: 12/19/16 CITY MANAGER

OEPA, STORM:	SUBMITTED 11/07/16	APPROVED 11/29/16
OEPA, WATER:	SUBMITTED 11/16/16	APPROVED 11/30/16
OEPA, SANITARY:	SUBMITTED N/A	APPROVED N/A

ChoiceOne Engineering
PID NO. 93245
FEDERAL NO. E120733
STATE NO. N/A
SOUTH CR 25-A RECONSTRUCTION, PH. 2
MIA TIPP CITY 25A RECONSTRUCTION, PH. 2
FILE NAME: MIA2C1406GTA
DRAWN BY: BHB
CHECKED BY: JSP
CITY OF: TIPP CITY
COUNTY: MIAMI
SHEET NUMBER: 1 OF 52

**CHOICE ONE ENGINEERING
RELEASE FOR BIDDING
JANUARY 25, 2017**



* UNLESS OTHERWISE SHOWN IN CROSS SECTIONS

CONDUIT SHALL BE PVC SCHEDULE 40. CONTRACTOR TO COORDINATE LOCATION AND DEPTH TO AVOID SIGN POSTS, AND ANY OTHER OBSTACLES. (TYP. ALL CONDUIT) CONDUIT SHALL ALSO INCLUDE EXTRA PULL STRINGS IN ALL CONDUITS WHEN NO WIRING IS INSTALLED.

* UNLESS OTHERWISE SHOWN IN CROSS SECTIONS

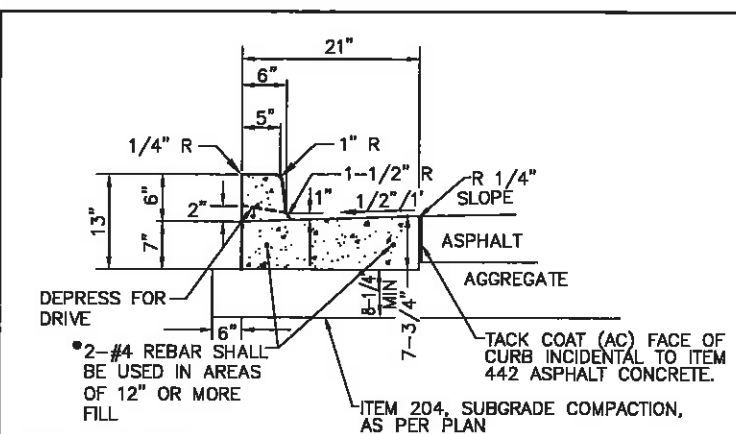
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TIPP CITY TYPE 1 COMBINATION CURB AND GUTTER, AS PER PLAN

GENERAL NOTES

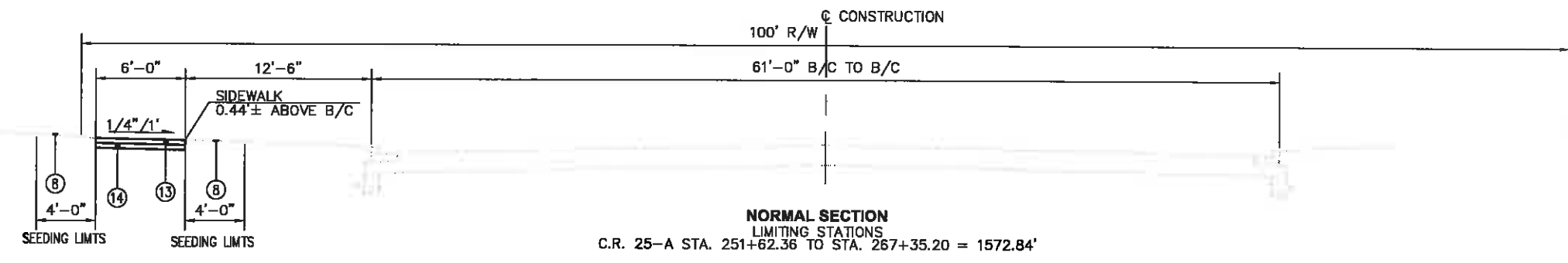
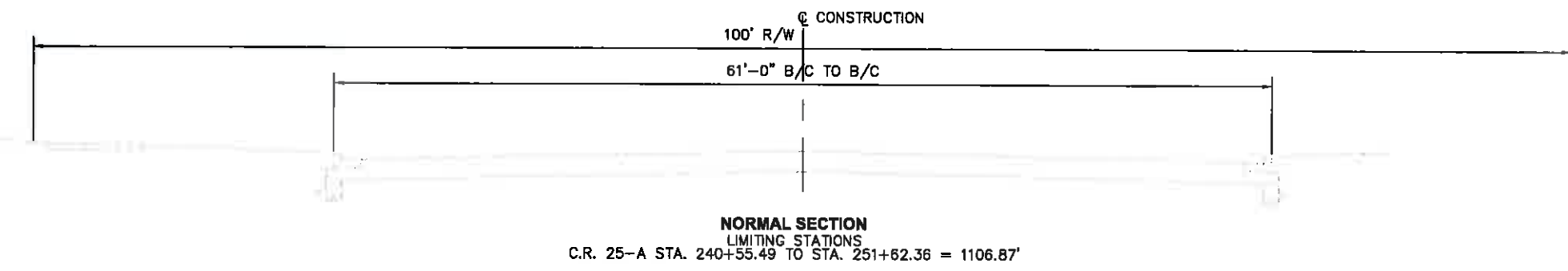
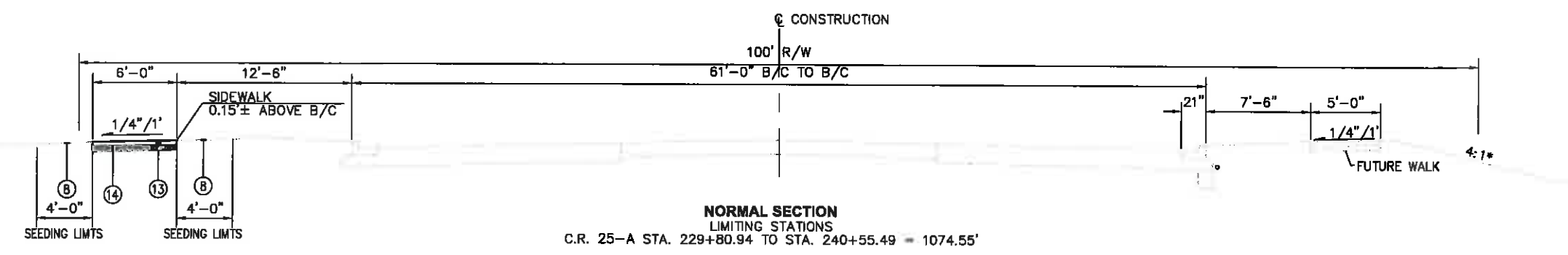
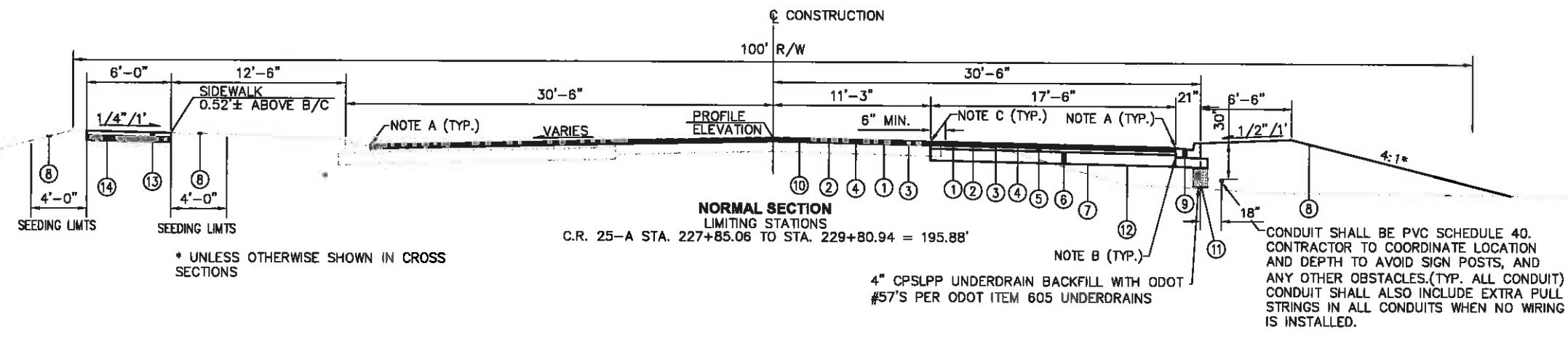
1. CONCRETE AND WORK SHALL MEET THE REQUIREMENTS SET FORTH IN ODOT QC MISC. (CEMENT ONLY - NO POZZOLAN MATERIAL) AND BE REINFORCED WITH CONCRETE FIBERS.
2. CURBING SHALL HAVE CONTRACTION JOINTS EVERY 10'.
3. MINIMUM OF 8-1/4" OF ODOT 304 SHALL BE PLACED UNDER CURBING.
4. CURBING SHALL BE BACKFILLED IMMEDIATELY AFTER FORMS ARE REMOVED.
5. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.
6. USE CLEAR CURING COMPOUND IMMEDIATELY AFTER FINISHING SURFACES. ANY OTHER METHODS OR TYPE OF CURING COMPOUND MUST BE PRE-APPROVED.
7. 1/2" PREMOLDED EXPANSION JOINTS SHALL BE CONSTRUCTED EACH SIDE OF DRIVE APPROACH SECTIONS AND AT P.C. POINTS, AT INTERSECTIONS, AND AT 100' MAX. INTERVALS.
8. ALL UNDERGROUND UTILITY LATERALS SHALL BE MARKED IN THE TOP OF ALL CURB WHILE IT IS BEING POURED AS FOLLOWS:
"S" - SANITARY LATERAL
"W" - WATER LATERAL

- NOTE A**
ITEM 442 ASPHALT SURFACE COURSE IS TO BE LEVEL OR 1/4" ABOVE GUTTER PLATE.
- NOTE B**
TACK COAT FACE OF CURB PRIOR TO ITEM 442 PLACEMENT, INCIDENTAL TO ITEM 442 ASPHALT CONCRETE.
- NOTE C**
PER C&MS 203.04E, LOCATE SOUND PAVEMENT EDGES, AND CURT AND TIM PAVEMENT TO A NEAT LINE. REPAIR AND RESTORE DAMAGE CAUSED BY THE EQUIPMENT OR METHODS. INCLUDE THE COST OF CUTTING, TRIMMING, AND DISPOSAL UNDER ITEM 203.

LEGEND

- ① ITEM 442, 1-1/2" ASPHALT CONCRETE, SURFACE COURSE, 12.5mm, TYPE A, (448)
- ② ITEM 407, TACK COAT (APPLIED AT THE RATE OF 0.06 GALLONS PER SQUARE YARD)
- ③ ITEM 442, 1-3/4" ASPHALT CONCRETE, INTERMEDIATE COURSE, 19 MM, TYPE A(448)
- ④ ITEM 407, TACK COAT (APPLIED AT THE RATE OF 0.08 GALLONS PER SQUARE YARD)
- ⑤ ITEM 301, 3" ASPHALT CONCRETE BASE, PG64-22
- ⑥ ITEM 304, 10" AGGREGATE BASE (IN TWO EQUAL LIFTS), AS PER PLAN
- ⑦ ITEM 204, SUBGRADE COMPACTION, AS PER PLAN
- ⑧ ITEM 659, SEEDING AND MULCHING, AS PER PLAN
- ⑨ ITEM 609, TIPP CITY TYPE 1 COMBINATION CURB AND GUTTER, AS PER PLAN
- ⑩ ITEM 254, 2"± PAVEMENT PLANING, ASPHALT CONCRETE
- ⑪ ITEM 605 - 4" BASE PIPE UNDERDRAINS, AS PER PLAN
- ⑫ ITEM 204, GEOGRID, AS PER PLAN (ONLY AS NEEDED PER DIRECTION OF CITY)
- ⑬ ITEM 608, 4" CONCRETE SIDEWALK, AS PER PLAN
- ⑭ ITEM 304, 4" AGGREGATE BASE, AS PER PLAN

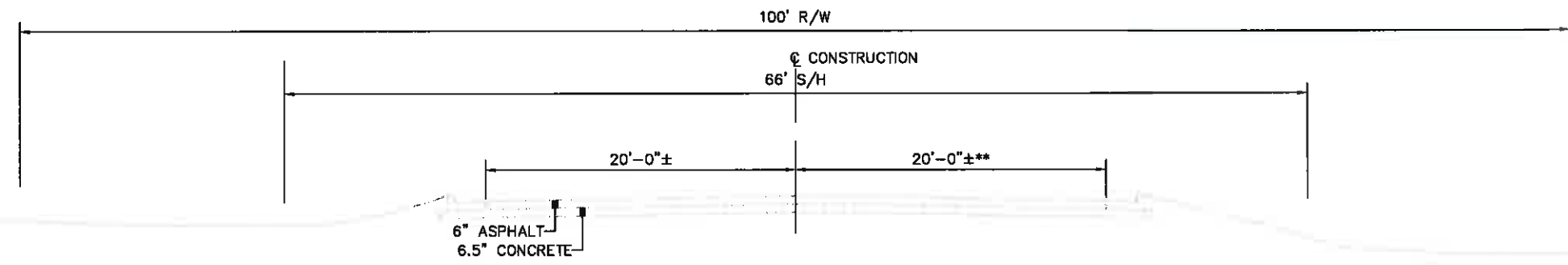
REVISIONS:
FILE NAME MIATC1406GYA
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATC1406
DATE 12-19-2016
SHEET NUMBER 2 OF 52



SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
TYPICAL SECTIONS

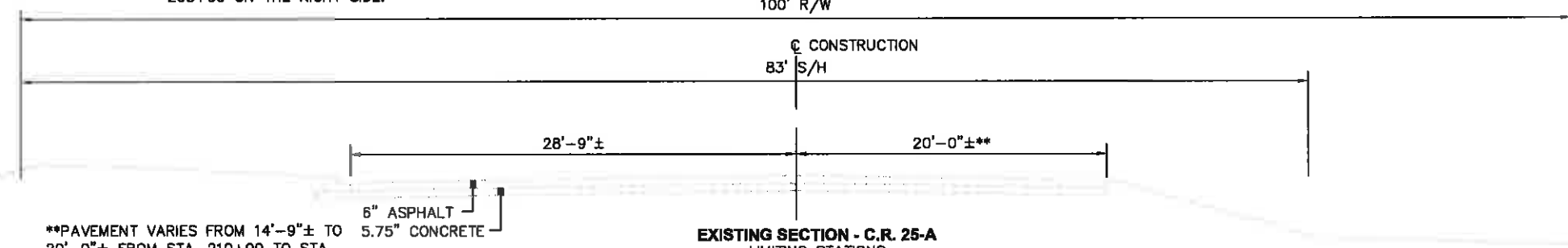
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FILE NAME MATCH1406CYB
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MATCH1406
DATE 12-19-2016
SHEET NUMBER 3 OF 52

SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
TYPICAL SECTIONS



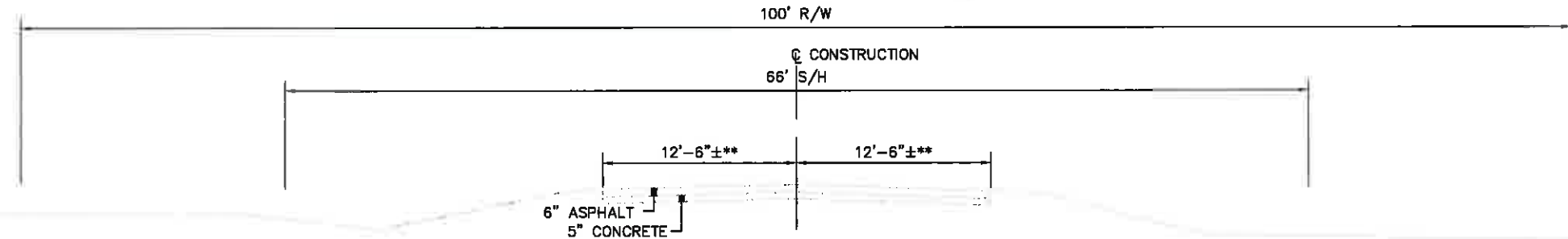
**PAVEMENT VARIES FROM 20'-0"± TO 30'-0"± FROM STA. 203+50 TO STA. 205+00 ON THE RIGHT SIDE.

EXISTING SECTION - C.R. 25-A
 LIMITING STATIONS
 C.R. 25-A STA. 203+50 TO STA. 207+15.56 = 365.56'



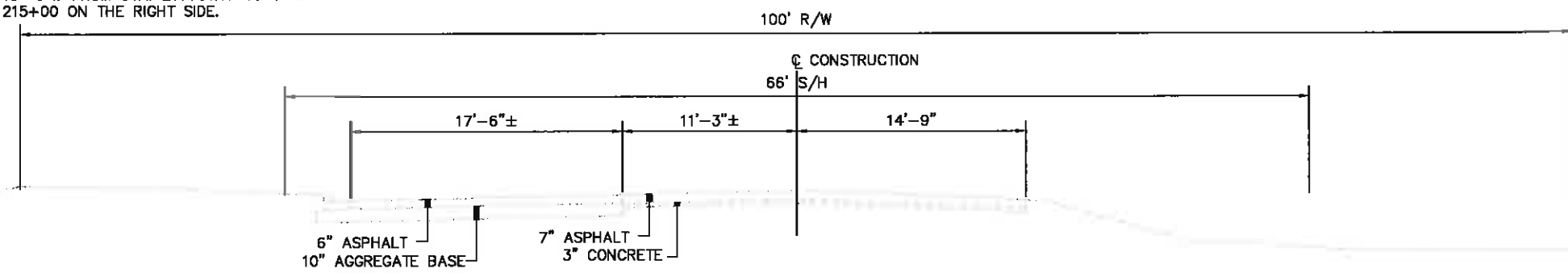
**PAVEMENT VARIES FROM 14'-9"± TO 20'-0"± FROM STA. 210+00 TO STA. 211+91.17 ON THE RIGHT SIDE.

EXISTING SECTION - C.R. 25-A
 LIMITING STATIONS
 C.R. 25-A STA. 207+15.56 TO STA. 211+91.13 = 475.57'



**PAVEMENT VARIES FROM 12'-0"± TO 15'-0"± FROM STA. 211+91.17 TO STA. 213+50 ON THE LEFT SIDE.
 PAVEMENT VARIES FROM 12'-6"± TO 15'-0"± FROM STA. 211+91.17 TO STA. 215+00 ON THE RIGHT SIDE.

EXISTING SECTION - C.R. 25-A
 LIMITING STATIONS
 C.R. 25-A STA. 211+91.13 TO STA. 227+85.06 = 1593.93'



EXISTING SECTION - C.R. 25-A
 LIMITING STATIONS
 C.R. 25-A STA. 227+85.06 TO STA. 229+17.06 = 132.00'

REVISIONS:

FILE NAME	MIATCH1406CYC
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	

ELEVATION DATUM

ALL ELEVATIONS ARE BASED ON U.S.G.S. AND CITY OF TIPP CITY DATUM.

GENERAL NOTES AND DETAILS

ALL CONSTRUCTION METHODS, MATERIALS, AND SPECIFICATIONS SHALL COMPLY WITH THE CITY OF TIPP CITY ENGINEERING STANDARDS AND SPECIFICATIONS OR OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION STANDARDS AND SPECIFICATIONS WHICHEVER IS MORE RESTRICTIVE AS DETERMINED BY THE CITY.

UNDERGROUND UTILITIES

THE LOCATIONS OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS OF THE UTILITY AS REQUIRED BY SECTION 153.64 ORC. EXISTING UTILITIES ARE SHOWN IN THEIR APPROXIMATE LOCATION ACCORDING TO THE BEST AVAILABLE DATA. THE CONTRACTOR WILL BE RESPONSIBLE FOR LOCATING THEM IN THE FIELD PRIOR TO CONSTRUCTION AND WILL BE RESPONSIBLE FOR ANY DAMAGE DONE TO THEM. CONTRACTOR TO CONTACT OHIO UTILITIES PROTECTION SERVICE (1-800-362-2764) 48 HOURS PRIOR TO CONSTRUCTION. NON-MEMBERS MUST BE CALLED DIRECTLY.

UTILITY OWNERSHIP

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

ELECTRIC, WATER CITY OF TIPP CITY
STORM & SANITARY MUNICIPAL BLDG.
260 S. GARBER ST.
TIPP CITY, OHIO 45371
937-667-6305

GAS VECTREN
6500 CLYO RD.
CENTERVILLE OHIO, 45459
937-312-2533
CONTACT: DON SPECHT

TELEPHONE FRONTIER COMMUNICATIONS
6464 WESTBROOK RD.
CLAYTON, OHIO 45315
541-390-3910
CONTACT: CHUCK BARNACCHI

CABLE TIME WARNER
3691 TURNER RD.
DAYTON, OH 45415
937-425-6850
CONTACT: TIM KUSS

ELECTRIC DAYTON POWER AND LIGHT CO
1900 DRYDEN ROAD
DAYTON, OHIO 45439
WILLIAM GOURLEY
937-331-4521

DP&L TRANSMISSION
1900 DRYDEN ROAD
DAYTON, OHIO 45439
GREG TOKAR
937-331-4647

OHIO UTILITIES PROTECTION SERVICE
2 WORKING DAYS BEFORE YOU DIG
CALL TOLL FREE 800-362-2764

UTILITY INTERFERENCE

IF, DURING THE CONSTRUCTION, INTERFERENCE ARISES WITH EXISTING UTILITIES IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE UTILITY COMPANY INVOLVED. ANY AND ALL WORK REQUIRED FOR PUBLIC OR PRIVATE UTILITIES WILL BE DONE BY AND AT THE EXPENSE OF THEIR RESPECTIVE OWNERS, UNLESS OTHERWISE NOTED ON THESE PLANS. THE CONTRACTOR SHALL NOTIFY, AT LEAST 7 DAYS BEFORE BREAKING GROUND, ALL PUBLIC SERVICE CORPORATIONS HAVING WIRES, POLES, PIPES, CONDUITS, MANHOLES, OR OTHER STRUCTURES THAT MAY BE AFFECTED BY THIS OPERATION, INCLUDING ALL STRUCTURES WHICH ARE AFFECTED AND NOT SHOWN ON THESE PLANS. THERE WILL BE NO DELAYS ALLOWED FOR UTILITY INTERFERENCES.

EXISTING TILE HOOKUPS

THE DRAINAGE TILE CURRENTLY CONNECTED TO THE EXISTING STORM SEWER SHALL BE CONNECTED TO THE PROPOSED STORM SEWER. ANY DRAINAGE TILE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED BY THE CONTRACTOR TO A CONDITION EQUAL TO OR BETTER THAN ITS ORIGINAL CONDITION. ALL TILE REMOVED, REPLACED AND/OR CONNECTED TO THE STORM SEWER SHALL BE NOTED ON THE RECORD DRAWINGS AND SHALL BE INSPECTED BY THE CITY BEFORE THEY ARE COVERED.

ALL FIELD OR STORM DRAINS WHICH ARE ENCOUNTERED DURING CONSTRUCTION SHALL BE PROVIDED WITH UNOBSTRUCTED OUTLETS OR PLUGGED AS APPROVED AND DIRECTED BY THE CITY. THE CITY WILL ASSIST IN ANY NECESSARY INVESTIGATION OR DYE TESTING PRIOR TO APPROVING PLUGGING A FIELD OR STORM TILE. CONNECTION OF INTERSECTING DRAIN TILES AND THE PROPOSED STORM SEWER SHALL BE THROUGH MANUFACTURED TEES, UNLESS OTHERWISE APPROVED BY THE CITY. COST OF ALL THE ITEMS ABOVE SHALL BE INCLUDED IN THE COST OF ITEM 611 STORM SEWERS.

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE CITY SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT ANY EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE CITY SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEM.

GRAFFITI AND VANDALISM

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL AND REPLACEMENT OF ANY CONCRETE WORK OR OTHER PAY ITEM UNDER THIS CONTRACT WHICH IS DEEMED UNACCEPTABLE BY THE CITY DUE TO GRAFFITI OR VANDALISM DAMAGE.

SUBCONTRACTOR SUPERVISION

THE CONTRACTOR IS REQUIRED TO HAVE A PROJECT SUPERVISOR ON-SITE TO SUPERVISE THE SUBCONTRACTOR FOR QUALITY CONTROL PURPOSES AND TO PROVIDE ANY NECESSARY ASSISTANCE TO THE SUBCONTRACTOR TO ENSURE QUALITY WORK.

COST OF THIS ITEM SHALL BE INCLUDED IN THE COST OF RELATED PAY ITEMS OF THIS PROJECT.

TESTING AND CERTIFICATION

CONTRACTOR TO PROVIDE TESTING OF OR CERTIFICATION THAT MATERIAL SUPPLIED FOR THIS PROJECT MEETS THE PROJECT SPECIFICATIONS, IF REQUIRED.

POTENTIAL ITEMS FOR TESTING CONSIST OF TRENCH BACKFILL, EMBANKMENT, AND CONCRETE, WHICH MAY BE PERFORMED BY THE CITY IF THEY SUSPECT DEFECTIVE WORK OR MATERIALS.

PAYMENT FOR CERTIFICATION OF WORK SHALL BE INCIDENTAL TO THE VARIOUS ITEMS IT IS RELATED TO AND SHALL INCLUDE ALL COST RELATED TO PROVIDING THE CERTIFICATION AS REQUIRED. COST OF ANY TESTING BY A TESTING LAB CONTRACTED BY THE CITY SHALL BE BORNE BY THE CITY, UNLESS THE TEST DOES NOT MEET PROJECT SPECIFICATIONS OR WORK IS FOUND TO BE DEFECTIVE, AT WHICH, THE CONTRACTOR SHALL PAY THE EXPENSES FOR TESTING.

PROPERTY POINTS AND SURVEY MONUMENTS

CARE SHALL BE TAKEN BY THE CONTRACTOR TO SAFEGUARD ANY PROPERTY POINTS OR OTHER SURVEY REFERENCE MARKS ENCOUNTERED DURING CONSTRUCTION OF THIS PROJECT. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RESET ANY PROPERTY POINT OR SURVEY MONUMENT WHICH IS DISTURBED AS A RESULT OF CONSTRUCTION OF THIS PROJECT. THE PROPERTY POINTS AND SURVEY MONUMENTS SHALL BE RESET UNDER THE SUPERVISION OF A REGISTERED PROFESSIONAL SURVEYOR.

PAYMENT FOR THIS ITEM SHALL BE INCIDENTAL TO THE OTHER ITEMS PAID FOR IN THIS PROJECT.

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE CITY, REPRESENTATIVES OF THE CITY, AND THE CONTRACTOR, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE CITY.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE CITY.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE CITY.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

MISCELLANEOUS

COMPENSATION FOR THE WORK AS SHOWN ON THE PLANS SHALL BE AT THE UNIT PRICES INCLUDED IN THE BID PROPOSAL. NO SEPARATE PAYMENT WILL BE MADE FOR TASKS (NOT ALL INCLUSIVE) INCLUDING SUCH ITEMS AS MOBILIZATION, RECORD DRAWINGS AND CONNECTIONS TO EXISTING FACILITIES.

WORK LIMITS

ALL WORK SHALL BE WITHIN EXISTING RIGHT-OF-WAY AND/OR CONSTRUCTION LIMITS UNLESS OTHERWISE INSTRUCTED BY THE CITY.

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

RECORD DRAWINGS

THE CONTRACTOR SHALL PROVIDE 2 COMPLETE SETS OF RECORD DRAWINGS TO THE CITY WITHIN 30 DAYS OF PROJECT COMPLETION. THESE DRAWINGS SHALL SHOW ALL CHANGES TO THE ORIGINAL DRAWINGS, ALL MANHOLE AND CATCH BASIN LOCATIONS AND INVERTS, ALL LATERAL LOCATIONS AND DEPTHS, ALL LOCATIONS AND DEPTHS OF EXISTING UTILITIES ENCOUNTERED DURING CONSTRUCTION AND ALL OTHER PERTINENT DATA TO THE IMPROVEMENTS. PAYMENT FOR THIS ITEM SHALL BE INCIDENTAL TO THE OTHER ITEMS PAID FOR IN THIS PROJECT.

SPECIAL CONDITIONS

ALL CONCRETE WORK MUST BE FINISHED TO THE SATISFACTION OF THE CITY. CONCRETE CURBING PLACED BY MACHINE MUST BE HAND TROWELED PRIOR TO BROOMING. NO WATER MAY BE USED TO ASSIST TROWELING.

ASPHALT MAY NOT OVERLAP CURBING. ANY EXCESS ASPHALT MUST BE REMOVED FROM CURBING BEFORE FINAL ROLLING.

MUD

THE TRACKING OR SPILLAGE OF MUD, DIRT, OR DEBRIS UPON CITY STREETS IS PROHIBITED AND ANY SUCH OCCURRENCE SHALL BE CLEANED UP IMMEDIATELY BY THE CONTRACTOR.

SAWCUT PAVEMENT JOINTS

SAWCUT PAVEMENT JOINTS SHALL BE INCLUDED IN THE PAYMENT OF ITEM 202, REMOVED, AS PER PLAN. MORE THAN ONE SAWCUT MAY BE NECESSARY TO ENSURE A CLEAN CUT JUST PRIOR TO ASPHALT OR CONCRETE PLACEMENT. ASPHALT MATERIAL SHALL BE PLACED ON THE VERTICAL FACE OF SAWCUT JOINTS PRIOR TO PAVING AS PER 401.14. AFTER THE ASPHALT WORK IS COMPLETED, THE TRANSVERSE JOINTS SHALL BE SEALED WITH LIQUID ASPHALT. THE JOINT PREPARATION AND SEALING SHALL BE INCLUDED IN THE PAYMENT FOR ASPHALT CONCRETE.

DEWATERING

ANY DEWATERING OR PUMPING NECESSARY FOR THE CONSTRUCTION OF ANY ITEMS SHALL BE INCIDENTAL TO THOSE PARTICULAR CONSTRUCTION ITEMS.

NOTE

ROOF DRAINS, FOUNDATION DRAINS, AND ALL OTHER CLEAN WATER CONNECTIONS TO THE SANITARY SYSTEM ARE PROHIBITED.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 3" BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. CONTRACTOR SHALL HAVE ON SITE STEEL PLATES FOR MAINTENANCE OF TRAFFIC AS DEEMED NECESSARY BY THE CITY IF THE 3" TOLERANCE IS NOT MET. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25' OR LESS) BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE CITY. CONTRACTOR SHALL ALSO PROVIDE ANY DRIVEWAY RAMPING NEEDED DURING PAVEMENT REMOVAL.

MASONRY COLLAR

A CONCRETE COLLAR SHALL BE PROVIDED WHERE PROPOSED STORM SEWER PIPE IS CONNECTED TO AN EXISTING PIPE. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR THE NEW CONDUIT.

FUNDING

DUE TO FUNDING, THE OWNER RESERVES THE RIGHT TO INCREASE, DECREASE, OR OMIT ANY ITEMS LISTED IN THE BID SCHEDULE.

NO FINAL UNIT PRICE ADJUSTMENTS PER ODOT 104.02 WILL BE MADE AT CONTRACT FINALIZATION.

SAFETY

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE, AND LOCAL SAFETY REQUIREMENTS, TOGETHER WITH EXERCISING PRECAUTIONS AT ALL TIMES FOR THE PROTECTION OF PERSONS (INCLUDING EMPLOYEES) AND PROPERTY. IT IS ALSO THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO INITIATE, MAINTAIN, AND SUPERVISE ALL SAFETY REQUIREMENTS, PRECAUTIONS, AND PROGRAMS IN CONNECTION WITH THE WORK.

MODIFICATIONS

ANY MODIFICATIONS TO THE SPECIFICATIONS OR CHANGES TO THE WORK AS SHOWN ON THE DRAWINGS MUST HAVE PRIOR WRITTEN APPROVAL BY THE CITY.

WATER MAIN CROSSING SEPARATION

WHENEVER A SANITARY SEWER OR STORM SEWER AND WATERLINE MUST CROSS, THE SEWER SHALL BE LAID AT SUCH AN ELEVATION THAT THE CROWN OF THE SEWER IS AT LEAST 18" BELOW THE BOTTOM OF THE WATERLINE. IF IT IS ABSOLUTELY IMPOSSIBLE TO MAINTAIN THE 18" VERTICAL SEPARATION, THE SANITARY SEWER SHALL BE CONSTRUCTED WITH WATERLINE TYPE MATERIALS WHICH WILL WITHSTAND A 150 PSI PRESSURE TEST. THESE REQUIREMENTS WILL EXTEND FOR A DISTANCE OF 10', MEASURED PERPENDICULAR, ON BOTH SIDES OF THE WATERLINE.

CITY DOES NOT PERMIT CHANGES IN MATERIALS MID SPAN. ENTIRE SPAN MUST BE CHANGED.

AT CROSSINGS, THE WATER MAIN SHALL HAVE A MINIMUM VERTICAL DISTANCE OF 18" FROM STORM AND SANITARY SEWERS. ALSO ONE FULL LENGTH OF WATER MAIN SHALL BE LOCATED SO THE JOINTS ARE AS FAR FROM THE STORM OR SANITARY SEWERS AS POSSIBLE.

RESIDENTIAL, COMMERCIAL, STORM DRAINAGE AND SEWER FACILITIES

EXISTING ROOF DRAINS, FOOTER DRAINS, STORM DRAINS, SANITARY LATERALS, OR YARD DRAINS, DISTURBED BY THE WORK, SHALL BE PROVIDED WITH UNOBSTRUCTED OUTLETS BY CONNECTING THE CONDUITS.

ALL CLEAN WATER CONNECTIONS (ROOF DRAINS, SUMP PUMPS, ETC.) ENCOUNTERED SHALL BE CONNECTED TO THE PROPOSED STORM SEWER. ALL SANITARY LATERALS SHALL BE RECONNECTED TO SANITARY SEWER PER THE CITY'S DIRECTION. THE LOCATION, TYPE, SIZE AND GRADE OF THE NEW CONDUIT REQUIRED TO REPLACE OR EXTEND THE EXISTING DRAIN WILL BE DETERMINED BY THE OWNER DURING CONSTRUCTION, AND PAYMENT SHALL BE MADE ON FINAL MEASUREMENTS AND INCLUDE ALL MATERIALS NECESSARY SUCH AS GRANULAR BEDDING, PROPER FITTINGS AND CONNECTIONS, AND PIPE. PERMISSION FROM PROPERTY OWNERS SHALL BE GRANTED TO THE CONTRACTOR PRIOR TO ANY CONSTRUCTION ON PRIVATE PROPERTY.

THE FOLLOWING CONTINGENT ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS FOR THE WORK NOTED ABOVE:

ITEM 611	4" PVC SDR-35 CONDUIT, TYPE B	100 FEET
ITEM 611	6" PVC SDR-35 CONDUIT, TYPE B	100 FEET
ITEM 611	8" PVC SDR-35 CONDUIT, TYPE B	100 FEET

CONCURRENT ACTIVITY

THE CONTRACTOR IS HEREBY ADVISED THAT ALL UTILITY COMPANIES AFFECTED BY THIS PROJECT MAY BE WORKING CONCURRENTLY WITHIN THE PROJECT LIMITS.

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. CHOICE ONE ENGINEERING CORPORATION MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA EITHER IN-SERVICE OR ABANDONED. FURTHER, CHOICE ONE ENGINEERING CORPORATION DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH CHOICE ONE ENGINEERING CORPORATION DID LOCATE AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. CHOICE ONE ENGINEERING CORPORATION HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

ASPHALT

ALL ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS SHALL APPLY TO THIS PROJECT EXCEPT FOR ODOT ITEM 401.20 ASPHALT BINDER PRICE ADJUSTMENT.

ALL ASPHALT DELIVERED SHALL BE ACCOMPANIED WITH A LOAD TICKET. ASPHALT CONCRETE BID ITEMS ARE NOT ELIGIBLE FOR ANY ASPHALT BINDER PRICE ADJUSTMENT.



SYDNEY, OHIO 937-497-0200
LORELAND, OHIO 912.295.8554
PORTLAND, INDIANA 260.766.2500
WWW.CHOICEONEENGINEERING.COM

SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
GENERAL NOTES

REVISIONS:

FILE NAME
MIATC1406GNA

DRAWN BY
BHB

CHECKED BY
JSP

PROJECT No.
MIATC1406

DATE
12-19-2016

SHEET NUMBER

5 OF 52

OWNERSHIP OF WOOD

ALL TREES REMOVED AS PART OF ITEM 201, CLEARING AND GRUBBING, SHALL BE CUT INTO 2' LENGTHS, SPLIT IF NECESSARY TO ALLOW THE PIECES TO BE MOVED BY HAND, STACKED BEYOND THE RIGHT-OF-WAY LINE, AND LEFT FOR THE RESPECTIVE PROPERTY OWNERS. IF THE PROPERTY OWNER REFUSES TO ACCEPT THE WOOD, THE CONTRACTOR SHALL DISPOSE OF THE WOOD AS DIRECTED BY THE CITY. PAYMENT FOR THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE LUMP SUM PRICE FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

CONTRACT WORK PERFORMED BY THE CITY

IF THE CONTRACTOR REFUSES OR FAILS TO PERFORM WORK OF ANY IMMEDIATE NATURE SUCH AS THE PLACEMENT OF BARRICADES, REPLACEMENT OF SIGNS, OR OTHER DEVICES REQUIRED BY THIS CONTRACT IN A REASONABLE TIME, THE CITY WILL PERFORM THE NECESSARY WORK. THE CONTRACTOR SHALL REIMBURSE THE CITY AT THE RATE OF 2.5 TIMES THE ACTUAL COST OF LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM SUCH WORK. THE CITY SHALL NOTIFY OR ATTEMPT TO NOTIFY THE CONTRACTOR OF THE NECESSITY TO PERFORM SUCH WORK. THE CITY SHALL BE REIMBURSED BY DEDUCTION FROM THE CONTRACTOR'S NEXT PAYMENT UNDER THE CONTRACT. REASONABLE TIME FOR ALL STREETS INVOLVED ON THIS CONTRACT IS 1 HOUR FROM THE TIME OF NOTIFICATION BY THE CITY.

NONRUBBER TIRE VEHICLES

NO NONRUBBER TIRE VEHICLES SHALL BE MOVED ON CITY STREETS. EXCEPTIONS MAY BE GRANTED BY THE CITY WHERE SHORT DISTANCES AND SPECIAL CIRCUMSTANCES ARE INVOLVED. GRANTING OF EXCEPTIONS MUST BE IN WRITING AND ANY RESULTING DAMAGE MUST BE REPAIRED TO THE SATISFACTION OF THE CITY. THE CONTRACTOR SHALL USE EXTREME CARE WHEN OPERATING NONRUBBER TIRE VEHICLES ON STREETS OR DRIVEWAYS TO AVOID MARKING OR DAMAGING THE PAVEMENT. PROTECTION OF THE PAVEMENT FROM DAMAGE RESULTING FROM THE TRACKS OF NONRUBBER TIRE VEHICLES UTILIZED IN TRENCH EXCAVATION SHALL BE REQUIRED. A WOOD PLANK SYSTEM, USED TIRES, RUBBER MATS, OR OTHER MEANS AS APPROVED BY THE CITY'S REPRESENTATIVE SHALL BE USED TO PROTECT THE PAVEMENT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

CONSTRUCTION NOISE

IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, ANY POWER-OPERATED CONSTRUCTION-TYPE DEVICES SHALL NOT BE OPERATED BETWEEN THE HOURS OF 9:00 PM AND 7:00 AM. IN ADDITION, ANY SUCH DEVICE SHALL NOT BE OPERATED AT ANY TIME IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

DEMOLITION NOTES

1. THE CONTRACTOR SHALL SAWCUT ALL EXISTING PAVEMENT, SIDEWALK AND CURB AGAINST WHICH ALL IMPROVEMENTS ARE PROPOSED. PAVEMENT, WALK, AND CURB SHALL BE SAWCUT IN NEAT, STRAIGHT LINES. COST FOR SAWCUTTING SHALL BE INCLUDED IN ITEM 202 REMOVED, AS PER PLAN.

2. THE EXISTING SIDEWALK IS EXPECTED TO HAVE LOW LOAD BEARING CAPACITY, EXERCISE CARE.

CONCRETE FIBERS

ALL CURB, DRIVEWAYS, WALKS, AND CURB RAMPS SHALL HAVE 3 LBS. OF 2.25" IN LENGTH FIBRILLATED MACROFIBERS.

CONTRACTOR IS TO NOTIFY THE FIBER MANUFACTURER'S SUPPLIER REPRESENTATIVE 48 HOURS PRIOR TO THE FIRST POUR OF THE CONCRETE UTILIZING EACH DIFFERENT FIBER TYPE TO ENSURE FIBERS ARE MIXED CORRECTLY, PLACEMENT, AND FINISHING. FIBER REPRESENTATIVE MUST BE ON SITE FOR THE FIRST POUR OF EACH FIBER TYPE.

CONTROL OF SPILLS

THIS PROJECT IS LOCATED WITHIN THE BOUNDARIES OF A DESIGNATED SOLE SOURCE AQUIFER. BEST CONSTRUCTION PRACTICES ARE TO BE IMPLEMENTED TO MINIMIZE WATER QUALITY IMPACTS. IDLE EQUIPMENT, PETROCHEMICALS, AND TOXIC/HAZARDOUS MATERIALS SHALL NOT BE STORED NEAR DRAINAGE WAYS, DITCHES OR STREAMS. REFUELING SHALL NOT BE UNDERTAKEN NEAR DRAINAGE WAYS, DITCHES OR STREAMS. A SPILL CONTAINMENT KIT IS TO BE MAINTAINED ON-SITE THROUGHOUT CONSTRUCTION ACTIVITIES. SPILLS OF FUELS, OILS, CHEMICALS, OR OTHER MATERIALS WHICH COULD POSE A THREAT TO GROUNDWATER SHALL BE CLEANED UP IMMEDIATELY. IF THE SPILL IS A REPORTABLE AMOUNT, THE TIPP CITY FIRE DEPARTMENT (937-669-8477) IS TO BE CONTACTED.

ENDANGERED SPECIES HABITAT

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY-LISTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVALS MUST OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVING, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE GROUND SURFACE AND WITH A MINIMUM HEIGHT OF 13 FEET.

NEPA ASSIGNMENT LANGUAGE

THE ENVIRONMENTAL REVIEW, CONSULTATION AND OTHER ACTIONS REQUIRED BY APPLICABLE FEDERAL ENVIRONMENTAL LAWS FOR THIS PROJECT ARE BEING, OR HAVE BEEN, CARRIED OUT BY ODOT PURSUANT TO 23 U.S.C. 327 AND A MEMORANDUM OF UNDERSTANDING DATED 12/11/2015, AND EXECUTED BY FHWA AND ODOT.

ITEM 201 CLEARING AND GRUBBING, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 201 CLEARING AND GRUBBING, EXCEPT AS HEREIN MODIFIED.

THIS WORK SHALL CONSIST OF REMOVING AND TRIMMING TREES, REMOVING STUMPS, TRIMMING AND/OR REMOVING BUSHES, REMOVING LANDSCAPE TIMBERS, ETC. AS NOTED ON THE PLANS, WORK SHALL BE COORDINATED WITH THE INDIVIDUAL PROPERTY OWNERS TO ENHANCE MAXIMUM POSSIBLE SATISFACTION.

ALL STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

REMOVE ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS UNDER THE EACH BID ITEM LISTED BELOW. THE FOLLOWING IS AN APPROXIMATE ESTIMATE OF THE NUMBER OF TREES AND STUMPS TO BE REMOVED, QUANTITIES MAY BE ADJUSTED IN FIELD:

SIZE	No. TREES	No. STUMPS	TOTAL
12"-24"	0	0	0
24"-36"	0	0	0
36"-60"	0	0	0

PAYMENT FOR ITEM 201 CLEARING AND GRUBBING, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE, SHALL BE AT THE CONTRACT LUMP SUM BID PRICE EXCEPT FOR ITEMS PAID FOR AS EACH AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 202 REMOVED, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 202 REMOVAL OF STRUCTURES AND OBSTRUCTIONS, EXCEPT AS HEREIN MODIFIED.

ITEM 202 REMOVED, AS PER PLAN CONSISTS OF SUCH ITEMS AS GUARDRAIL, CONCRETE PAVEMENT, HEADWALLS, ASPHALT PAVEMENT, CATCH BASINS, EXISTING PAVEMENT, ROCKS, HANDRAILS, LANDSCAPE TIMBERS/PAVERS, BRICK PAVERS, PULL BOXES, TRAFFIC SIGNAL CONDUIT, DETECTOR LOOPS, ETC., AS NECESSARY FOR THE CONSTRUCTION OF THE PROJECT, INCLUDING SAW CUTTING PAVEMENT.

THE REMOVAL OF EXISTING SIDEWALK, EXISTING PIPE, AND EXISTING CURB SHALL BE PAID UNDER THEIR RESPECTIVE ITEMS.

ANY ITEM THAT IS REMOVED UNDER OR WITHIN 5' OF PROPOSED CURB, PAVEMENT, SIDEWALKS, OR DRIVEWAY SHALL BE BACKFILLED AND COMPACTED USING ITEM 611 TYPE 1 OR 2 STRUCTURAL MATERIAL.

ANY PRIVATE PROPERTY SHALL BE REMOVED CAREFULLY AND STORED FOR THE PROPERTY OWNER AT A PLACE DESIGNATED BY THE OWNER ON THAT PARTICULAR PROPERTY. ANY SALVAGED PUBLIC ITEMS SHALL REMAIN CITY PROPERTY AND SHALL BE CAREFULLY REMOVED BY THE CONTRACTOR AND STORED AT A DESIGNATED LOCATION ON THE PROJECT SITE FOR CITY PICKUP.

PAYMENT FOR ITEM 202 REMOVED, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 203 EXCAVATION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 203 ROADWAY EXCAVATION AND EMBANKMENT, EXCEPT AS HEREIN MODIFIED.

ALL SURFACE MATERIAL, EXCAVATED UNSUITABLE MATERIAL AND ALL EXCESS EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY AND EXPENSE OUTSIDE OF THE RIGHT-OF-WAY AT A SITE APPROVED BY THE CITY. THIS ITEM SHALL ALSO INCLUDE THE PLACING OF A MINIMUM OF 6" OF TOPSOIL ON ALL DISTURBED AREAS DURING FINAL GRADING.

PAYMENT FOR ITEM 203 EXCAVATION, INCLUDING EMBANKMENT CONSTRUCTION, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT CUBIC YARD BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 204 EXCAVATION OF SUBGRADE AND STRUCTURAL EMBANKMENT, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 204 SUBGRADE COMPACTION AND PROOF ROLLING, EXCEPT AS HEREIN MODIFIED.

THIS WORK SHALL INCLUDE THE EXCAVATING OF UNSUITABLE SUBGRADE AND REPLACING WITH ODOT ITEM 304 AGGREGATE BASE, ODOT #1, ODOT #2, OR ANY COMBINATION AS DIRECTED BY THE CITY. LOCATION AND AMOUNT OF THIS ITEM SHALL BE LOCATED BY THE CITY AT THE TIME OF CONSTRUCTION ONLY AFTER THE CONTRACTOR HAS SATISFACTORILY ATTEMPTED TO DRY OUT AND WORK THE SUBGRADE. THIS ITEM INCLUDES EXCAVATION AND DISPOSAL OF UNSUITABLE MATERIAL, SUBGRADE COMPACTION, AND THE SUPPLY AND PLACEMENT OF THE ABOVE MENTIONED GRANULAR MATERIAL. THE ESTIMATED QUANTITY IS 4700 CUBIC YARDS, BUT THIS ITEM COULD BE NON-PERFORMED IF DEEMED UNNECESSARY OR EXCEEDS THE ESTIMATED QUANTITY, IF NECESSARY.

PAYMENT FOR ITEM 204 EXCAVATION OF SUBGRADE AND GRANULAR EMBANKMENT, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT CUBIC YARD BID PRICE (REGARDLESS OF QUANTITY) AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 204 SUBGRADE COMPACTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 204 SUBGRADE COMPACTION AND PROOF ROLLING, EXCEPT AS HEREIN MODIFIED.

THIS WORK SHALL INCLUDE "PROOF ROLLING" WITH A LOADED TANDEM DUMP TRUCK AS DIRECTED BY THE CITY UNTIL NO DEFLECTION OR TIRE INDENTATION IN SUBGRADE IS PRESENT.

PAYMENT FOR ITEM 204 SUBGRADE COMPACTION, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT SQUARE YARD BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 204 GEOGRID, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 204 SUBGRADE COMPACTION AND PROOF ROLLING, EXCEPT AS HEREIN MODIFIED.

THE WORK SHALL INCLUDE SUBGRADE STABILIZATION BY UTILIZING TENSAR TX5 GEOGRID, NAUE SECUGRID 60/20, COLBOND ENKAGRID PRO 40, OR AN APPROVED EQUIVALENT. LOCATION AND AMOUNT SHALL BE LOCATED BY THE CITY AT THE TIME OF CONSTRUCTION. TENSAR TX5 GEOGRID SHALL BE INSTALLED AS PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS. THIS WORK SHALL INCLUDE THE ADDITIONAL LABOR AND EQUIPMENT TO PROPERLY PLACE 304 AGGREGATE BASE OVER TENSAR AS RECOMMENDED BY THE MANUFACTURER.

THE ESTIMATED QUANTITY IS 9300 SQUARE YARDS, BUT THIS ITEM COULD BE NON-PERFORMED IF DEEMED UNNECESSARY OR EXCEED THE ESTIMATED QUANTITY, IF NECESSARY. THE METHOD OF MEASUREMENT SHALL BE THE SURFACE AREA COVERED BY STABILIZATION MEMBRANE, NOT INCLUDING OVERLAP OR WASTE.

PAYMENT FOR ITEM 204 GEOGRID, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT SQUARE YARD BID PRICE (REGARDLESS OF QUANTITY) AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 304 AGGREGATE BASE, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 304 AGGREGATE BASE, EXCEPT AS HEREIN MODIFIED.

THIS ITEM SHALL ALSO INCLUDE SATURATING THE AGGREGATE BASE WITH WATER DURING PLACEMENT OF EACH LIFT PRIOR TO COMPACTING.

THIS WORK SHALL INCLUDE "PROOF ROLLING" WITH LOADED TANDEM DUMP TRUCK AS DIRECTED BY THE CITY UNTIL NO DEFLECTION OR TIRE INDENTATION IN THE AGGREGATE BASE IS PRESENT.

PAYMENT FOR ITEM 304 AGGREGATE BASE, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT CUBIC YARD BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 452 NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 452 NON-REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT, EXCEPT AS HEREIN MODIFIED.

CONCRETE DRIVE APPROACHES SHALL HAVE A MINIMUM THICKNESS OF 6" FOR RESIDENTIAL AND 8" FOR COMMERCIAL DRIVES. 6" AGGREGATE BASE SATURATED W/ WATER PRIOR TO COMPACTION, THIS SHALL BE PAID UNDER THE RESPECTIVE ITEM. CONCRETE SHALL BE ODOT QUMISC. (CEMENT ONLY - NO POZZOLAN MATERIAL) AND REINFORCED WITH CONCRETE FIBERS. PERFORM A BROOM FINISH AND TOOLED EDGES ON ALL SURFACES AND APPLY CURING COMPOUND IMMEDIATELY. PROVIDE 1/2" PREMOLDED EXPANSION JOINT TO ISOLATE APPROACHES AND WALKS.

DRIVES SHALL BE INSTALLED TO MATCH INTO EXISTING.

PAYMENT FOR ITEM 452 NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT SQUARE YARD BID PRICE AND SHALL INCLUDE ALL MATERIAL, LABOR, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

Old Bid sheet, see next with Addendum No.1 Revisions

REVISIONS:

FILE NAME
MIATG1406GNB

DRAWN BY
BHB

CHECKED BY
JSP

PROJECT No.
MIATG1406

DATE
12-19-2016

SHEET NUMBER

6 Of 52

OWNERSHIP OF WOOD

ALL TREES REMOVED AS PART OF ITEM 201, CLEARING AND GRUBBING, SHALL BE CUT INTO 2' LENGTHS, SPLIT IF NECESSARY TO ALLOW THE PIECES TO BE MOVED BY HAND, STACKED BEYOND THE RIGHT-OF-WAY LINE, AND LEFT FOR THE RESPECTIVE PROPERTY OWNERS. IF THE PROPERTY OWNER REFUSES TO ACCEPT THE WOOD, THE CONTRACTOR SHALL DISPOSE OF THE WOOD AS DIRECTED BY THE CITY. PAYMENT FOR THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE LUMP SUM PRICE FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

CONTRACT WORK PERFORMED BY THE CITY

IF THE CONTRACTOR REFUSES OR FAILS TO PERFORM WORK OF ANY IMMEDIATE NATURE SUCH AS THE PLACEMENT OF BARRICADES, REPLACEMENT OF SIGNS, OR OTHER DEVICES REQUIRED BY THIS CONTRACT IN A REASONABLE TIME, THE CITY WILL PERFORM THE NECESSARY WORK. THE CONTRACTOR SHALL REIMBURSE THE CITY AT THE RATE OF 2.5 TIMES THE ACTUAL COST OF LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM SUCH WORK. THE CITY SHALL NOTIFY OR ATTEMPT TO NOTIFY THE CONTRACTOR OF THE NECESSITY TO PERFORM SUCH WORK. THE CITY SHALL BE REIMBURSED BY A DEDUCTION FROM THE CONTRACTOR'S NEXT PAYMENT UNDER THE CONTRACT. REASONABLE TIME FOR ALL STREETS INVOLVED ON THIS CONTRACT IS 1 HOUR FROM THE TIME OF NOTIFICATION BY THE CITY.

NONRUBBER TIRE VEHICLES

NO NONRUBBER TIRE VEHICLES SHALL BE MOVED ON CITY STREETS. EXCEPTIONS MAY BE GRANTED BY THE CITY WHERE SHORT DISTANCES AND SPECIAL CIRCUMSTANCES ARE INVOLVED. GRANTING OF EXCEPTIONS MUST BE IN WRITING AND ANY RESULTING DAMAGE MUST BE REPAIRED TO THE SATISFACTION OF THE CITY. THE CONTRACTOR SHALL USE EXTREME CARE WHEN OPERATING NONRUBBER TIRE VEHICLES ON STREETS OR DRIVEWAYS TO AVOID MARKING OR DAMAGING THE PAVEMENT. PROTECTION OF THE PAVEMENT FROM DAMAGE RESULTING FROM THE TRACKS OF NONRUBBER TIRE VEHICLES UTILIZED IN TRENCH EXCAVATION SHALL BE REQUIRED. A WOOD PLANK SYSTEM, USED TIRES, RUBBER MATS, OR OTHER MEANS AS APPROVED BY THE CITY'S REPRESENTATIVE SHALL BE USED TO PROTECT THE PAVEMENT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

CONSTRUCTION NOISE

IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, ANY POWER-OPERATED CONSTRUCTION-TYPE DEVICES SHALL NOT BE OPERATED BETWEEN THE HOURS OF 9:00 PM AND 7:00 AM. IN ADDITION, ANY SUCH DEVICE SHALL NOT BE OPERATED AT ANY TIME IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

DEMOLITION NOTES

1. THE CONTRACTOR SHALL SAWCUT ALL EXISTING PAVEMENT, SIDEWALK AND CURB AGAINST WHICH ALL IMPROVEMENTS ARE PROPOSED. PAVEMENT, WALK, AND CURB SHALL BE SAWCUT IN NEAT, STRAIGHT LINES. COST FOR SAWCUTTING SHALL BE INCLUDED IN ITEM 202 REMOVED, AS PER PLAN.

2. THE EXISTING SIDEWALK IS EXPECTED TO HAVE LOW LOAD BEARING CAPACITY, EXERCISE CARE.

CONCRETE FIBERS

ALL CURB, DRIVEWAYS, WALKS, AND CURB RAMPS SHALL HAVE 3 LBS. OF 2.25" IN LENGTH FIBRILLATED MACROFIBERS.

CONTRACTOR IS TO NOTIFY THE FIBER MANUFACTURER'S SUPPLIER REPRESENTATIVE 48 HOURS PRIOR TO THE FIRST POUR OF THE CONCRETE UTILIZING EACH DIFFERENT FIBER TYPE TO ENSURE FIBERS ARE MIXED CORRECTLY, PLACEMENT, AND FINISHING. FIBER REPRESENTATIVE MUST BE ON SITE FOR THE FIRST POUR OF EACH FIBER TYPE.

CONTROL OF SPILLS

THIS PROJECT IS LOCATED WITHIN THE BOUNDARIES OF A DESIGNATED SOLE SOURCE AQUIFER. BEST CONSTRUCTION PRACTICES ARE TO BE IMPLEMENTED TO MINIMIZE WATER QUALITY IMPACTS. IDLE EQUIPMENT, PETROCHEMICALS, AND TOXIC/HAZARDOUS MATERIALS SHALL NOT BE STORED NEAR DRAINAGE WAYS, DITCHES OR STREAMS. REFUELING SHALL NOT BE UNDERTAKEN NEAR DRAINAGE WAYS, DITCHES OR STREAMS. A SPILL CONTAINMENT KIT IS TO BE MAINTAINED ON-SITE THROUGHOUT CONSTRUCTION ACTIVITIES. SPILLS OF FUELS, OILS, CHEMICALS, OR OTHER MATERIALS WHICH COULD POSE A THREAT TO GROUNDWATER SHALL BE CLEANED UP IMMEDIATELY. IF THE SPILL IS A REPORTABLE AMOUNT, THE TIPP CITY FIRE DEPARTMENT (937-669-8477) IS TO BE CONTACTED.

ENDANGERED SPECIES HABITAT

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY-LISTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVALS MUST OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVING, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE GROUND SURFACE AND WITH A MINIMUM HEIGHT OF 13 FEET.

NEPA ASSIGNMENT LANGUAGE

THE ENVIRONMENTAL REVIEW, CONSULTATION AND OTHER ACTIONS REQUIRED BY APPLICABLE FEDERAL ENVIRONMENTAL LAWS FOR THIS PROJECT ARE BEING, OR HAVE BEEN, CARRIED OUT BY ODOT PURSUANT TO 23 U.S.C. 327 AND A MEMORANDUM OF UNDERSTANDING DATED 12/11/2015, AND EXECUTED BY FHWA AND ODOT.

ITEM 201 CLEARING AND GRUBBING, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 201 CLEARING AND GRUBBING, EXCEPT AS HEREIN MODIFIED.

THIS WORK SHALL CONSIST OF REMOVING AND TRIMMING TREES, REMOVING STUMPS, TRIMMING AND/OR REMOVING BUSHES, REMOVING LANDSCAPE TIMBERS, ETC. AS NOTED ON THE PLANS, WORK SHALL BE COORDINATED WITH THE INDIVIDUAL PROPERTY OWNERS TO ENHANCE MAXIMUM POSSIBLE SATISFACTION.

ALL STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

REMOVE ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS UNDER THE EACH BID ITEM LISTED BELOW. THE FOLLOWING IS AN APPROXIMATE ESTIMATE OF THE NUMBER OF TREES AND STUMPS TO BE REMOVED, QUANTITIES MAY BE ADJUSTED IN FIELD:

SIZE	No. TREES	No. STUMPS	TOTAL
12"-24"	0	0	0
24"-36"	0	0	0
36"-60"	0	0	0

PAYMENT FOR ITEM 201 CLEARING AND GRUBBING, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE, SHALL BE AT THE CONTRACT LUMP SUM BID PRICE EXCEPT FOR ITEMS PAID FOR AS EACH AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 202 REMOVED, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 202 REMOVAL OF STRUCTURES AND OBSTRUCTIONS, EXCEPT AS HEREIN MODIFIED.

ITEM 202 REMOVED, AS PER PLAN CONSISTS OF SUCH ITEMS AS GUARDRAIL, CONCRETE PAVEMENT, HEADWALLS, ASPHALT PAVEMENT, CATCH BASINS, EXISTING PAVEMENT, ROCKS, HANDRAILS, LANDSCAPE TIMBERS/PAVERS, BRICK PAVERS, PULL BOXES, TRAFFIC SIGNAL CONDUIT, DETECTOR LOOPS, ETC., AS NECESSARY FOR THE CONSTRUCTION OF THE PROJECT, INCLUDING SAW CUTTING PAVEMENT.

THE REMOVAL OF EXISTING SIDEWALK, EXISTING PIPE, AND EXISTING CURB SHALL BE PAID UNDER THEIR RESPECTIVE ITEMS.

ANY ITEM THAT IS REMOVED UNDER OR WITHIN 5' OF PROPOSED CURB, PAVEMENT, SIDEWALKS, OR DRIVEWAY SHALL BE BACKFILLED AND COMPACTED USING ITEM 611 TYPE 1 OR 2 STRUCTURAL MATERIAL.

ANY PRIVATE PROPERTY SHALL BE REMOVED CAREFULLY AND STORED FOR THE PROPERTY OWNER AT A PLACE DESIGNATED BY THE OWNER ON THAT PARTICULAR PROPERTY. ANY SALVAGED PUBLIC ITEMS SHALL REMAIN CITY PROPERTY AND SHALL BE CAREFULLY REMOVED BY THE CONTRACTOR AND STORED AT A DESIGNATED LOCATION ON THE PROJECT SITE FOR CITY PICKUP.

PAYMENT FOR ITEM 202 REMOVED, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

△ ITEM 203 EXCAVATION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 203 ROADWAY EXCAVATION AND EMBANKMENT, EXCEPT AS HEREIN MODIFIED.

ALL SURFACE MATERIAL, EXCAVATED UNSUITABLE MATERIAL AND ALL EXCESS EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY AND EXPENSE OUTSIDE OF THE RIGHT-OF-WAY AT A SITE APPROVED BY THE CITY. THIS ITEM SHALL ALSO INCLUDE THE PLACING OF A MINIMUM OF 6" OF TOPSOIL ON ALL DISTURBED AREAS DURING FINAL GRADING.

PAYMENT FOR ITEM 203 EXCAVATION, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT CUBIC YARD BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 204 EXCAVATION OF SUBGRADE AND STRUCTURAL EMBANKMENT, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 204 SUBGRADE COMPACTION AND PROOF ROLLING, EXCEPT AS HEREIN MODIFIED.

THIS WORK SHALL INCLUDE THE EXCAVATING OF UNSUITABLE SUBGRADE AND REPLACING WITH ODOT ITEM 304 AGGREGATE BASE, ODOT #1, ODOT #2, OR ANY COMBINATION AS DIRECTED BY THE CITY. LOCATION AND AMOUNT OF THIS ITEM SHALL BE LOCATED BY THE CITY AT THE TIME OF CONSTRUCTION ONLY AFTER THE CONTRACTOR HAS SATISFACTORILY ATTEMPTED TO DRY OUT AND WORK THE SUBGRADE. THIS ITEM INCLUDES EXCAVATION AND DISPOSAL OF UNSUITABLE MATERIAL, SUBGRADE COMPACTION, AND THE SUPPLY AND PLACEMENT OF THE ABOVE MENTIONED GRANULAR MATERIAL. THE ESTIMATED QUANTITY IS 4700 CUBIC YARDS, BUT THIS ITEM COULD BE NON-PERFORMED IF DEEMED UNNECESSARY OR EXCEEDS THE ESTIMATED QUANTITY, IF NECESSARY.

PAYMENT FOR ITEM 204 EXCAVATION OF SUBGRADE AND GRANULAR EMBANKMENT, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT CUBIC YARD BID PRICE (REGARDLESS OF QUANTITY) AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 204 SUBGRADE COMPACTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 204 SUBGRADE COMPACTION AND PROOF ROLLING, EXCEPT AS HEREIN MODIFIED.

THIS WORK SHALL INCLUDE "PROOF ROLLING" WITH A LOADED TANDEM DUMP TRUCK AS DIRECTED BY THE CITY UNTIL NO DEFLECTION OR TIRE INDENTATION IN SUBGRADE IS PRESENT.

PAYMENT FOR ITEM 204 SUBGRADE COMPACTION, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT SQUARE YARD BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 204 GEOGRID, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 204 SUBGRADE COMPACTION AND PROOF ROLLING, EXCEPT AS HEREIN MODIFIED.

THE WORK SHALL INCLUDE SUBGRADE STABILIZATION BY UTILIZING TENSAR TX5 GEOGRID, NAUE SECUGRID 60/20, COLBOND ENKAGRID PRO 40, OR AN APPROVED EQUIVALENT. LOCATION AND AMOUNT SHALL BE LOCATED BY THE CITY AT THE TIME OF CONSTRUCTION. TENSAR TX5 GEOGRID SHALL BE INSTALLED AS PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS. THIS WORK SHALL INCLUDE THE ADDITIONAL LABOR AND EQUIPMENT TO PROPERLY PLACE 304 AGGREGATE BASE OVER TENSAR AS RECOMMENDED BY THE MANUFACTURER.

THE ESTIMATED QUANTITY IS 9300 SQUARE YARDS, BUT THIS ITEM COULD BE NON-PERFORMED IF DEEMED UNNECESSARY OR EXCEED THE ESTIMATED QUANTITY, IF NECESSARY. THE METHOD OF MEASUREMENT SHALL BE THE SURFACE AREA COVERED BY STABILIZATION MEMBRANE, NOT INCLUDING OVERLAP OR WASTE.

PAYMENT FOR ITEM 204 GEOGRID, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT SQUARE YARD BID PRICE (REGARDLESS OF QUANTITY) AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 304 AGGREGATE BASE, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 304 AGGREGATE BASE, EXCEPT AS HEREIN MODIFIED.

THIS ITEM SHALL ALSO INCLUDE SATURATING THE AGGREGATE BASE WITH WATER DURING PLACEMENT OF EACH LIFT PRIOR TO COMPACTING.

THIS WORK SHALL INCLUDE "PROOF ROLLING" WITH LOADED TANDEM DUMP TRUCK AS DIRECTED BY THE CITY UNTIL NO DEFLECTION OR TIRE INDENTATION IN THE AGGREGATE BASE IS PRESENT.

PAYMENT FOR ITEM 304 AGGREGATE BASE, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT CUBIC YARD BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 452 NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 452 NON-REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT, EXCEPT AS HEREIN MODIFIED.

CONCRETE DRIVE APPROACHES SHALL HAVE A MINIMUM THICKNESS OF 6" FOR RESIDENTIAL AND 8" FOR COMMERCIAL DRIVES. 6" AGGREGATE BASE SATURATED W/ WATER PRIOR TO COMPACTION, THIS SHALL BE PAID UNDER THE RESPECTIVE ITEM. CONCRETE SHALL BE ODOT QC MISC. (CEMENT ONLY - NO POZZOLAN MATERIAL) AND REINFORCED WITH CONCRETE FIBERS. PERFORM A BROOM FINISH AND TOOLED EDGES ON ALL SURFACES AND APPLY CURING COMPOUND IMMEDIATELY. PROVIDE 1/2" PREMOLED EXPANSION JOINT TO ISOLATE APPROACHES AND WALKS.

DRIVES SHALL BE INSTALLED TO MATCH INTO EXISTING.

PAYMENT FOR ITEM 452 NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT SQUARE YARD BID PRICE AND SHALL INCLUDE ALL MATERIAL, LABOR, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 608 CURB RAMPS, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 608 WALKS, CURB RAMPS, AND STEPS, EXCEPT AS HEREIN MODIFIED.

TRUNCATED DOME SPECIFICATIONS:

INSTALL DETECTABLE WARNINGS (TRUNCATED DOMES) FOR A DISTANCE OF 24" FROM THE BACK OF CURB FOR THE ENTIRE WIDTH OF THE RAMP OPENING WHERE IT IS FLUSH WITH THE PAVEMENT.

4" OF 304 AGGREGATE BASE SATURATED W/ WATER PRIOR TO COMPACTION SHALL BE PLACED UNDER ALL PROPOSED CURB RAMPS.

ALL CONCRETE SHALL BE ODOT QC MISC. (CEMENT ONLY – NO POZZOLAN MATERIAL) AND REINFORCE WITH CONCRETE FIBERS. EXPANSION JOINT MATERIAL, AND CURING AND SEALING COMPOUND SHALL ALSO BE INCLUDED IN THIS ITEM.

THE PANELS SHALL BE CAST IRON PRODUCT PANELS MOLDED IN THE SQUARE PATTERN. COLOR OF THE PANEL SHALL BE APPROVED BY THE CITY PRIOR TO ORDERING.

PAYMENT FOR ITEM 608 CURB RAMPS, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE, SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK PER RAMP.

ITEM 611 PIPING/CONDUITS AND RELATED WORK, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 611, PIPE CULVERTS, SEWERS, DRAINS, AND DRAINAGE STRUCTURES, EXCEPT AS HEREIN MODIFIED.

ALL PIPE CULVERTS, CONDUITS, SEWERS, DRAINS, AND DRAINAGE STRUCTURES SHALL MEET THE MATERIAL REQUIREMENTS OF THIS ITEM. THE FOLLOWING ITEMS WILL NOT BE REQUIRED UNLESS OTHERWISE NOTED: 1) INSTALLATION PLAN, 2) CONSTRUCTION INSPECTION FORMS, 3) PERFORMANCE INSPECTIONS AND REPORTS, 4) CONDUIT AND DRAINAGE STRUCTURE EVALUATIONS.

THE INSTALLATION OF ALL PIPE CULVERTS, CONDUITS, SEWERS, DRAINS, AND DRAINAGE STRUCTURES SHALL BE PER MANUFACTURER'S RECOMMENDATIONS OR AS NOTED ON THE PLANS. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN A HIGH STANDARD OF WORK. CONTRACTOR IS RESPONSIBLE TO ENSURE ALL WORK IS PERFORMED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS OR AS NOTED ON THE PLANS. CONTRACTOR SHALL ALSO ENSURE THAT ALL ITEMS ARE FULLY AND PROPERLY FUNCTIONAL, AND TO A QUALITY ACCEPTABLE BY THE OWNER.

ITEM 611 STORM SEWER, RCP, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 611, PIPE CULVERTS, SEWERS, DRAINS, AND DRAINAGE STRUCTURES EXCEPT AS HEREIN MODIFIED.

THE PIPE SHALL BE REINFORCED CONCRETE PIPE WITH THE CLASS BEING SPECIFIED ON THE PLANS. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE TO FIELD LOCATE ALL EXISTING STORM SEWER, STRUCTURES AND OTHER UTILITIES, PRIOR TO INSTALLING THE PROPOSED STORM SEWER SYSTEM. THE EXISTING STORM SEWER, AND LATERALS SHOWN ON THE PLANS ARE IN THE APPROXIMATE LOCATION AND IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD LOCATE PROPOSED TIE-INS TO THE EXISTING STORM PRIOR TO ANY STORM SEWER CONSTRUCTION. ALL TIE-INS SHALL BE THROUGH PRECAST TEES OR HOLES INSTALLED USING A CORING MACHINE.

PAYMENT FOR ITEM 611 STORM SEWER, RCP, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE, SHALL BE AT THE CONTRACT FOOT BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 623 CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 623 CONSTRUCTION LAYOUT STAKES AND SURVEYING, EXCEPT AS HEREIN MODIFIED.

THE VERTICAL CONTROL IS LISTED ON THE PLANS WITH THE BENCHMARKS. TRAVERSE POINTS ARE SHOWN BELOW FOR HORIZONTAL CONTROL. THE BEARINGS ARE LISTED FOR THE CENTERLINE OF CONSTRUCTION.

PAYMENT FOR ITEM 623 CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT, REQUIRED TO COMPLETE THIS ITEM OF WORK.

TRAVERSE POINT #	STATION	OFFSET	NORTH	EAST	TYPE
115	202+80.51	33.19' LT	712751.7280	1492129.4230	MAG NAIL
114	207+37.98	19.89' LT	713209.0070	1492150.1890	MAG NAIL
113	212+17.65	16.88' RT	713688.0150	1492194.8350	IRON PIN
112	217+53.75	16.15' LT	714224.5810	1492170.8280	IRON PIN
111	222+80.92	16.60' LT	714751.6900	1492178.8380	IRON PIN
C/L STATIONING	230+00	0'	715470.3895	1492207.028	MAG NAIL
C/L STATIONING	235+00	0'	715970.3945	1492208.2820	MAG NAIL
C/L STATIONING	240+00	0'	716470.3895	1492211.4911	MAG NAIL
C/L STATIONING	245+00	0'	716970.3848	1492213.7201	MAG NAIL
C/L STATIONING	250+00	0'	717470.3798	1492215.9492	MAG NAIL
C/L STATIONING	255+00	0'	717970.3747	1492218.1782	MAG NAIL
C/L STATIONING	260+00	0'	718470.3698	1492216.1827	MAG NAIL
C/L STATIONING	265+00	0'	718970.3390	1492211.6218	MAG NAIL

ITEM 638 WATER MAIN, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 638 WATER MAINS AND SERVICE BRANCHES, EXCEPT AS HEREIN MODIFIED.

ALL MATERIALS, PROCEDURES, AND TESTING USED SHALL CONFORM WITH THE CITY OF TIPP CITY ENGINEERING STANDARDS AND SPECIFICATIONS.

PAVEMENT RESTORATION, CURB AND GUTTER AND PLAIN CONCRETE PAVEMENT AND SIDEWALK REPLACEMENT SHALL BE PAID UNDER THEIR RESPECTIVE ITEM NUMBER. HOWEVER STRUCTURAL BEDDING AND ANY REQUIRED STRUCTURAL BACKFILL SHALL BE INCLUDED WITH THIS ITEM.

HYDROSTATIC PRESSURE TEST SHALL INCLUDE ALL WATER MAIN, FH BRANCHES, AND COPPER WATER SERVICES TO THE CURB BOX

CONTRACTOR TO USE ONLY DOMESTIC PIPE AND FITTINGS.

CONTRACTOR TO USE ALL STAINLESS STEEL BANDS FOR ALL TAPPING SLEEVE AND VALVES.

PAYMENT FOR ITEM 638 WATER MAIN, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED (INCLUDING TESTING AND PURITIES) ABOVE SHALL BE AT THE CONTRACT FOOT BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 659 SEEDING AND MULCHING, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 659 SEEDING AND MULCHING, EXCEPT AS HEREIN MODIFIED.

ALL AREAS DESIGNATED FOR SEEDING SHALL HAVE A MINIMUM OF 6" OF TOPSOIL OVER THE ENTIRE AREA. THE AREA SHALL BE HAND RAKED AND DRESSED READY FOR SEEDING. NO STONE OVER 1" IN SIZE PERMITTED.

COMMERCIAL FERTILIZER 12-12-12 SHALL BE INCLUDED IN ITEM 659 SEEDING AND MULCHING, AS PER PLAN AND SHALL BE APPLIED AT THE RATE OF 20 POUNDS PER 1000 SQUARE FEET.

SEEDING APPLICATION SHALL BE AT THE RATE OF 6 POUNDS PER 1000 SQUARE FEET. THE FOLLOWING SEED MIXTURE OR EQUIVALENT SHALL BE USED IN LIEU OF THE MIXTURES LISTED IN 659.09:

- 30% KENTUCKY BLUE(VNS)
- 23% PALMER II PERENNIAL RYE
- 22% YORKTOWN III PERENNIAL RYE
- 15% CREEPING RED FESCUE(VNS)
- 10% BARON KENTUCKY BLUEGRASS

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE CONSTRUCTION LIMITS EXCLUDING THE CURB LAWN. QUANTITY CALCULATIONS FOR ITEM 659 SEEDING AND MULCHING, AS PER PLAN ARE BASED ON THESE LIMITS.

ALL FINAL SEEDING AND MULCHING SHALL BE COMPLETED BY THE SUBSTANTIAL COMPLETION DATE. LIQUIDATED DAMAGES SHALL BE ASSESSED IN ACCORDANCE WITH 108.07 FOR EACH CALENDAR DAY THAT THE SEEDING AND MULCHING IS NOT COMPLETE.

IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO ENSURE THE GRASS IS ESTABLISHED TO THE SATISFACTION OF THE CITY, WHICH MAY REQUIRE WATERING AND RESEEDING.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REGRADING AND RESEEDING ANY AREAS THAT HAVE ERODED OR AREAS THAT NEW GRASS DID NOT GERMINATE UNTIL THE ENTIRE SEEDED AREAS ARE STABILIZED, SHAPED AND DRAINED, AS PER PLAN.

CONTRACTOR IS RESPONSIBLE TO RESTORE ANY LAWN AREA, OUTSIDE OF THE CONSTRUCTION LIMITS, CAUSED BY HIS WORK, WHETHER ACCEPTABLE TO PROPERTY OWNER OR NOT, TO THE SATISFACTION OF THE PROPERTY OWNER AND THE CITY, AT THE CONTRACTOR'S SOLE EXPENSE.

THE TECHNICAL SPECIFICATIONS MOST RESTRICTIVE SHALL APPLY AS DETERMINED BY THE CITY.

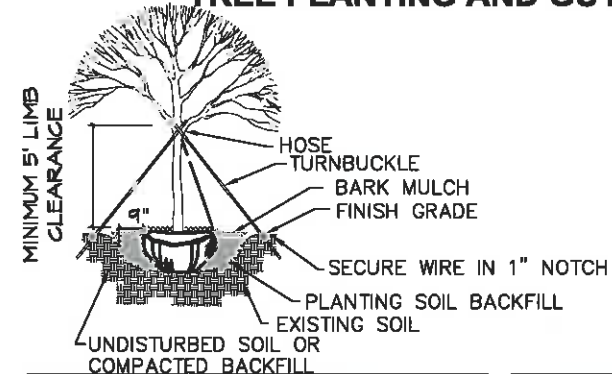
PAYMENT FOR ITEM 659 SEEDING AND MULCHING, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE, SHALL BE AT THE CONTRACT SQUARE YARD BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 661 DECIDUOUS TREE, AS PER PLAN

CONTRACTOR TO INSTALL TREES PER THE TREE PLANTING AND GUYING DETAIL. THE CONTRACTOR SHALL INSTALL THE TREES AS SHOWN ON THE LANDSCAPE PLAN, OR AS ADJUSTED BY THE CITY. TREES ARE TO BE PLANTED 5' OFF THE BACK OF CURB.

PAYMENT FOR ITEM 661 DECIDUOUS TREE, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE, SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

TREE PLANTING AND GUYING



NOTES:

1. PROVIDE GUYING OF DECIDUOUS TREES OVER 2" CALIPER AND EVERGREEN TREES OVER 8'-0".
2. PROVIDE & INSTALL 3 STAKES & GUY WIRES—EQUALLY SPACED.
3. TOP OF ROOT BALL TO BE 2"-3" ABOVE ADJACENT FINISH GRADE.
4. REMOVE ROPE AND BURLAP FROM TOP 1/3 OF ROOT BALL. REMOVE WIRE BASKET FROM ROOT BALL. REMOVE ALL LABELS, TAGS OR OTHER FOREIGN MATERIALS FROM LIMBS.

LOCATION (STATION, OFFSET)	SPECIES
204+11 35.50' LT	ZELKOVA, MUSASHINO
204+61 35.50' RT	ZELKOVA, MUSASHINO
205+11 35.50' LT	ZELKOVA, MUSASHINO
205+61 35.50' RT	ZELKOVA, MUSASHINO
206+61 35.50' RT	ZELKOVA, MUSASHINO
206+91 35.50' LT	ZELKOVA, MUSASHINO
207+61 35.50' RT	MAPLE, SCARLET SENTINEL
208+11 35.50' LT	MAPLE, SCARLET SENTINEL
208+61 35.50' RT	MAPLE, SCARLET SENTINEL
209+61 35.50' RT	MAPLE, SCARLET SENTINEL
210+61 35.50' RT	OAK, CRIMSON SPIRE
211+11 35.50' LT	MAPLE, SCARLET SENTINEL
211+61 35.50' RT	OAK, CRIMSON SPIRE
212+61 35.50' RT	OAK, CRIMSON SPIRE
213+11 35.50' LT	OAK, CRIMSON SPIRE
213+61 35.50' RT	HORNBEAM, FRANS FONTAINE
214+11 35.50' LT	OAK, CRIMSON SPIRE
214+61 35.50' RT	HORNBEAM, FRANS FONTAINE
215+11 35.50' LT	OAK, CRIMSON SPIRE
215+61 35.50' RT	HORNBEAM, FRANS FONTAINE
216+11 35.50' LT	HORNBEAM, FRANS FONTAINE
216+61 35.50' RT	FILBERT, TURKISH
217+11 35.50' LT	HORNBEAM, FRANS FONTAINE
217+61 35.50' RT	FILBERT, TURKISH
218+11 35.50' LT	HORNBEAM, FRANS FONTAINE
218+61 35.50' RT	FILBERT, TURKISH
219+11 35.50' LT	FILBERT, TURKISH
219+61 35.50' RT	ZELKOVA, MUSASHINO
220+11 35.50' LT	FILBERT, TURKISH
220+61 35.50' RT	ZELKOVA, MUSASHINO
221+11 35.50' LT	FILBERT, TURKISH

LOCATION (STATION, OFFSET)	SPECIES
221+61 35.50' RT	ZELKOVA, MUSASHINO
222+11 35.50' LT	ZELKOVA, MUSASHINO
222+61 35.50' RT	MAPLE, SCARLET SENTINEL
223+11 35.50' LT	ZELKOVA, MUSASHINO
223+61 35.50' RT	MAPLE, SCARLET SENTINEL
224+11 35.50' LT	ZELKOVA, MUSASHINO
224+61 35.50' RT	MAPLE, SCARLET SENTINEL
225+11 35.50' LT	MAPLE, SCARLET SENTINEL
225+61 35.50' RT	OAK, CRIMSON SPIRE
226+11 35.50' LT	MAPLE, SCARLET SENTINEL
226+61 35.50' RT	OAK, CRIMSON SPIRE
227+11 35.50' LT	MAPLE, SCARLET SENTINEL
227+61 35.50' RT	OAK, CRIMSON SPIRE
228+11 35.50' LT	ZELKOVA, MUSASHINO
228+61 35.50' RT	ZELKOVA, MUSASHINO

ITEM SPECIAL ROCK EXCAVATION

THIS ITEM OF WORK SHALL CONSIST OF THE EXCAVATION OF BEDROCK. IT SHALL INCLUDE ANY ROCK NOT EXCAVATABLE BY CONVENTIONAL EXCAVATION METHODS. EXCAVATION THROUGH ROCK SHALL BE ATTEMPTED WITH CONVENTIONAL HEAVY TRACKED BACKHOE EQUIPMENT (I.E. KOMATSU 200). ANY MATERIAL EXCAVATABLE WITH CONVENTIONAL EQUIPMENT SHALL BE CONSIDERED INCIDENTAL TO RELATED ITEMS.

ANY MATERIAL UNEXCAVATABLE WITH CONVENTIONAL EQUIPMENT SHALL BE REMOVED BY JACKHAMMERS, ROCK RIPPERS, ETC. NO BLASTING ALLOWED. THIS MATERIAL SHALL BE APPROVED, VERIFIED, AND MEASURED BY CITY PERSONNEL AND PAID FOR UNDER THIS ITEM. NO PAYMENT FOR OVER EXCAVATION WILL BE ALLOWED, REFER TO TRENCH DETAIL.

THE ESTIMATED QUANTITY IS 400 CUBIC YARDS, BUT THIS ITEM COULD BE NON-PERFORMED IF DEEMED UNNECESSARY OR EXCEEDS THE ESTIMATED QUANTITY.

PAYMENT FOR ITEM SPECIAL ROCK EXCAVATION, FOR ALL OPERATIONS DESCRIBED ABOVE, SHALL BE AT THE CONTRACT CUBIC YARD BID PRICE AND SHALL INCLUDE ALL MATERIAL, LABOR, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.



SIDNEY, OHIO 937.487.0200
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SOUTH CR 25-A RECONSTRUCTION, PH. 2
 CITY OF TIPP CITY
 GENERAL NOTES

REVISIONS:

FILE NAME	MIATCH1406GNC
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	7 OF 52

ITEM SPECIAL, MAILBOX SUPPORT SYSTEM

THIS WORK SHALL CONSIST OF FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED MOUNTING HARDWARE IN ACCORDANCE WITH PLAN DETAILS, AND ATTACHING AN OWNER-SUPPLIED MAILBOX AT LOCATIONS SPECIFIED IN THE PLAN, OR OTHERWISE ESTABLISHED BY THE CITY.

WOOD POSTS SHALL BE NOMINAL 4 INCHES BY 4 INCHES SQUARE OR 4.5 INCHES DIAMETER ROUND, AND CONFORM TO 710.14.

STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2 INCHES I.D., AND CONFORM TO AASHTO M 181.

ALL HARDWARE INCLUDING BUT NOT LIMITED TO PLATES, SCREWS, BOLTS, AND ETC. SHALL BE COMMERCIAL-GRADE GALVANIZED STEEL.

POSTS SHALL BE SET PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO BOXES MAY BE MOUNTED ON A SINGLE POST.

THE MAILBOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS, AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION.

IN THE ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER, THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE CITY.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POST MASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO A NEW LOCATION.

PAYMENT UNDER THIS ITEM SHALL BE LIMITED TO FINAL PERMANENT INSTALLATIONS. TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 107.10. HOWEVER, THE SAME MATERIAL AND SIZE LIMITATIONS AS FOR PERMANENT INSTALLATIONS SHALL APPLY.

MAILBOX SUPPORTS, COMPLETE IN PLACE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH, FOR ITEM SPECIAL MAILBOX SUPPORT SYSTEM, (SINGLE) (DOUBLE).

HYDROSTATIC TEST

A. AFTER THE PIPE HAS BEEN LAID AND BACKFILLED, ALL NEWLY LAID PIPE OR VALVED SECTION, INCLUDING COPPER WATER SERVICES, SHALL BE SUBJECTED TO HYDROSTATIC PRESSURE AND LEAKAGE TEST. ALL WATER MAINS MUST BE HYDROSTATICALLY TESTED (AWWA C-600). THE TESTS MUST BE PERFORMED IN THE PRESENCE OF A REPRESENTATIVE OF THE CITY OF TIPP CITY. THE LEAKAGE TEST PRESSURE SHALL BE NOT LESS THAN 150 PSI. THE DURATION OF THE LEAKAGE TEST SHALL NOT BE LESS THAN 2 HOURS. HYDROSTATIC PRESSURE SHALL BE APPLIED BY MEANS OF A PUMP TAKING WATER FROM AN AUXILIARY SUPPLY. ALL PIPING MUST BE PROPERLY FILLED AND FLUSHED TO DISPEL ALL AIR BEFORE THE TEST IS MADE USING POTABLE WATER.

B. LEAKAGE IS DEFINED AS THE QUANTITY OF WATER TO BE SUPPLIED INTO THE NEWLY LAID PIPE, OR ANY VALVED SECTION THEREOF, NECESSARY TO MAINTAIN THE SPECIFIED LEAKAGE TEST PRESSURE AFTER THE PIPE HAS BEEN FILLED WITH WATER AND THE AIR EXPELLED.

C. NO PIPE INSTALLATION WILL BE ACCEPTED IF THE LEAKAGE EXCEEDS THE LEAKAGE DETERMINED BY THE FOLLOWING FORMULA:

Where: S = length of pipe tested, in ft. $L = \frac{S \cdot D \cdot \sqrt{P}}{148,000}$
 D = pipe diameter, in inches
 P = average test pressure
 L = allowable leakage per hour

During the hydrostatic test, a thorough examination of all piping, fittings, valves, hydrants, etc. shall be performed. Leaking joints shall be tightened and cracked or otherwise defective material shall be removed and replaced and the test shall be repeated until satisfactory results are obtained.

Below is a table which represents the allowable leakage in gallons per hour.

AVG. TEST PRESSURE (PSI) BAR	ALLOWABLE LEAKAGE PER 1000 FT. (305M) OF PIPELINE (GPH+)											
	NOMINAL PIPE DIAMETER— INCHES											
	3	4	6	8	10	12	14	16	18	20	24	30
250(17)	0.32	0.43	0.64	0.85	1.07	1.28	1.50	1.71	1.92	2.14	2.56	3.21
225(16)	0.30	0.41	0.61	0.81	1.01	1.22	1.42	1.62	1.82	2.03	2.43	3.04
200(14)	0.29	0.38	0.57	0.76	0.96	1.15	1.34	1.53	1.72	1.91	2.29	2.87
175(12)	0.27	0.36	0.54	0.72	0.89	1.07	1.25	1.43	1.61	1.79	2.15	2.68
150(10)	0.25	0.33	0.50	0.66	0.83	0.99	1.16	1.32	1.49	1.66	1.99	2.48
125(9)	0.23	0.30	0.45	0.60	0.76	0.91	1.06	1.21	1.36	1.51	1.81	2.27

DISINFECTION

A. AFTER SATISFACTORY HYDROSTATIC TESTING, THE COMPLETED WATER WORK SHALL BE DISINFECTED IN ACCORDANCE WITH AWWA C-651.

B. MAINTAIN PIPES FREE OF DIRT AND FOREIGN MATTER DURING CONSTRUCTION BY DEWATERING TRENCH AND SEALING OPEN PIPE BARRELS. SWAB EACH LENGTH OF PIPE AS IT IS INSTALLED. UPON COMPLETION OF MAIN, ISOLATE MAIN SEGMENTS AND FLUSH PIPE AT 2 FPS VELOCITY.

C. STERILIZE MAIN IN ACCORDANCE WITH AWWA C-651. INJECT 3% TO 5% HYPO-CHLORITE SOLUTION TO PROVIDE 50 TO 60 MG PER LITER CONCENTRATION IN MAIN. CHLORINE MAY BE PLACED IN EACH SECTION OF PIPE AT THE TIME OF INSTALLATION. SAMPLE WATER AT EACH HYDRANT OR IF NO HYDRANT IS AVAILABLE, AT A TAP IN THE PROPOSED LINE. ANALYZE SAMPLE USING ORTHOTOLIDINE REAGENT TO VERIFY FREE CHLORINE CONCENTRATION. MAINTAIN CONCENTRATION IN MAIN FOR 24 HOURS. SAMPLE HYDRANTS AT COMPLETION OF STERILIZATION VERIFYING MINIMUM CHLORINE RESIDUAL (SEE CITY WATER DEPARTMENT FOR MINIMUM REQUIREMENTS).

D. FLUSH CHLORINE SOLUTION TO WASTE INTO SANITARY SEWER AT A CONTROLLED RATE, NOT TO EXCEED 25 GPM. IF CHLORINE RESIDUAL DROPS IN 10 MG PER LITER, FLUSH MAIN AT 2 FPS AND REPEAT STERILIZATION PROCEDURE.

E. WATER SAMPLES - PERFORM BACTERIOLOGICAL TEST PER AWWA C-651. SAMPLE MAIN AT HYDRANT OR IF HYDRANT IS NOT AVAILABLE, AT A TAP IN THE PROPOSED LINE. DELIVER SAMPLE TO STATE-CERTIFIED LABORATORY. DELIVER COPIES OF LABORATORY REPORT TO THE CITY IN THE EVENT OF DETECTION OF COLIFORM ORGANISM, REPEAT FLUSHINGS, STERILIZATION, AND SAMPLING OF MAINS UNTIL ACCEPTABLE TEST RESULTS ARE ACHIEVED. THIS IS TO BE PERFORMED PRIOR TO TRANSFER OF SERVICE AT CONTRACTOR'S EXPENSE. CITY PERSONNEL WILL PERFORM BACTERIA SAMPLES.

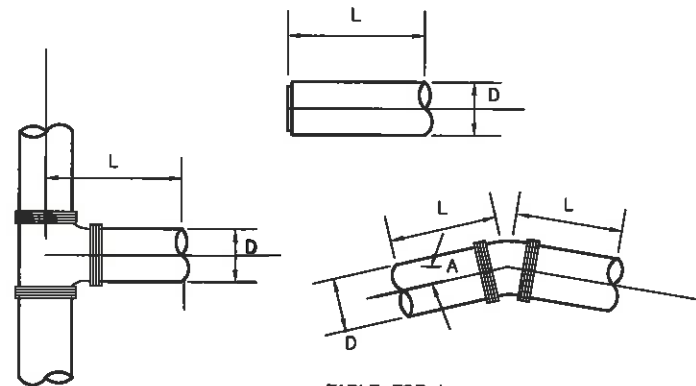


TABLE FOR L
 REQUIRED LENGTH OF RESTRAINED JOINTS
REQUIRED LENGTH OF RESTRAINED JOINTS FOR WATER MAINS

A ~ DEGREE OF DEFLECTION	D-DIAMETER OF PIPE							
	4"	6"	8"	10"	12"	16"	20"	24"
11 1/4'	*	*	*	*	*	5	5	6
22 1/2'	*	2	3	5	6	8	10	12
45°	4	8	12	14	20	30	36	45
90°	12	26	38	48	66	98	125	145
TEE	12	26	38	48	66	98	125	145
END	12	26	38	48	66	98	125	145

*REQUIRED RESTRAINED JOINT AT FITTING ONLY
 USE MEGALUG MECHANICAL JOINT RESTRAINT OR EQUAL.
 FIRE HYDRANT BRANCHES MUST BE ANCHOR PIPE

FIRE HYDRANT NOTES

A. ALL WATER MAINS SHALL BE CONSTRUCTED USING DUCTILE IRON CEMENT MORTAR LINED PIPE.

B. PIPE SHALL CONFORM TO SPECIFICATIONS ANSI A21.51, AWWA C 151 CLASS 53, PUSH ON TYPE.

C. CEMENT MORTAR LINING SHALL CONFORM TO AWWA C 104, ANSI 21.4 SPECIFICATIONS.

D. ALL FITTINGS SHALL CONFORM TO THE LATEST AWWA OR ASA SPECIFICATIONS AS MAY BE APPLICABLE. ALL PIPE & FITTINGS TO BE AMERICAN MADE.

E. ALL JOINTS SHALL BE CLOW "BELL-TITE" JOINTS OR EQUAL.

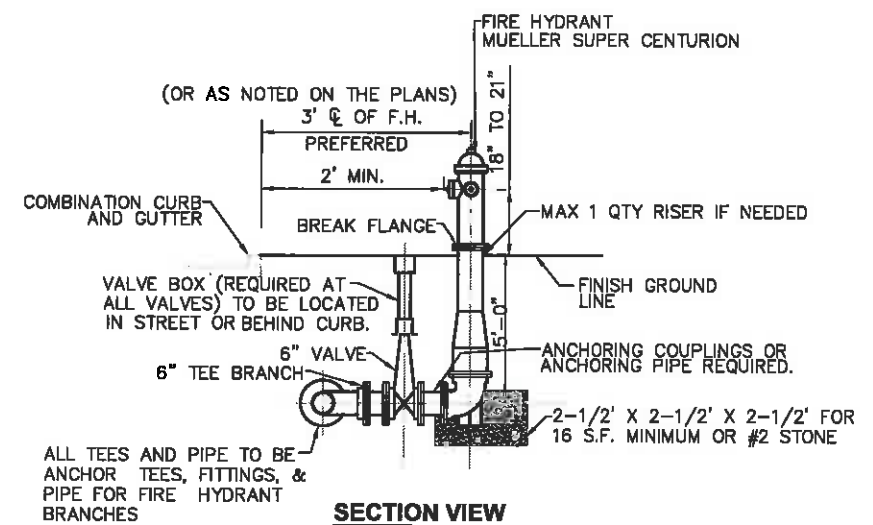
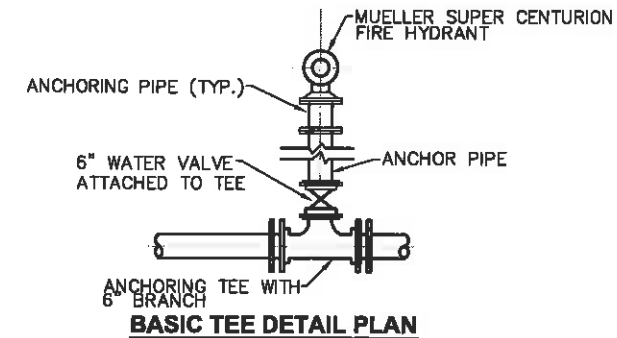
F. MINIMUM DEPTH OF COVER OVER THE TYPE OF PIPE SHALL BE 4'-6" AS MEASURED TO THE FINISH GRADE OF THE STREET OVER THE PIPE.

G. ALL FIRE HYDRANTS SHALL BE MANUFACTURED BY THE MUELLER COMPANY (MUELLER SUPER CENTURION 250) AND SHALL BE A "BREAK FLANGE" MODEL HYDRANT. HYDRANTS SHALL BE FURNISHED WITH ONE (1) FOUR AND HALF INCH (OPEN LEFT) STEAMER NOZZLE AND TWO 2-1/2 INCH HOSE NOZZLES WITH MALE NATIONAL STANDARD THREADS TO FIT COUPLINGS USED BY MUNICIPAL FIRE DEPARTMENT.

H. ALL VALVES SHALL BE MANUFACTURED BY THE MUELLER CO. (OR EQUAL) VALVES SHALL BE IRON BODY, RESILIENT WEDGE, BRONZE MOUNTED AND SHALL CONFORM TO LATEST AWWA SPECIFICATIONS. ALL VALVES SHALL BE DESIGNED FOR WORKING PRESSURE OF 150 P.S.I. AND SHALL OPEN BY TURNING COUNTER-CLOCKWISE. VALVES SHALL BE PROVIDED WITH EITHER BELL OR MECHANICAL JOINT. ALL VALVES AT DEAD ENDS OF MAINS ARE TO BE SECURELY STRAPPED TO THE MAIN.
 ONLY CITY OF TIPP CITY EMPLOYEES MAY OPERATE WATER VALVES, COORDINATE WITH CITY.

I. ALL VALVE BOXES SHALL BE HEAVY DUTY BUFFALO TYPE TWO PIECE 5-1/4" SHAFT, WITH COVER MARKED "WATER." THE BOX SHALL BE OF SUCH LENGTH SO AS TO EXTEND UPWARD TO THE FINISH GRADE OF THE GROUND OR STREET.

J. ALL FIRE HYDRANTS SHALL BE PAINTED A COLOR CONSISTENT WITH TIPP CITY'S STANDARDS.



ALL TEES AND PIPE TO BE ANCHOR TEES, FITTINGS, & PIPE FOR FIRE HYDRANT BRANCHES

SECTION VIEW

REVISIONS:

FILE NAME
 M1ATC1406GND
 DRAWN BY
 BHB
 CHECKED BY
 JSP
 PROJECT No.
 M1ATC1406
 DATE
 12-19-2016
 SHEET NUMBER

ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN

IT IS THE INTENTION TO PERFORM THE REQUIRED WORK WITHIN THESE PLANS WITH THE LEAST INCONVENIENCE TO, AND THE MAXIMUM SAFETY OF, THE CONTRACTOR, LOCAL MERCHANTS, PEDESTRIAN TRAFFIC AND THE TRAVELING PUBLIC.

REQUIREMENTS FOR MAINTAINING TRAFFIC AS SPECIFIED IN THE "OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (CURRENT EDITION, LATEST REVISION), PERTINENT PROVISIONS OF THE "OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS" (INCLUDING SUPPLEMENTAL SPECIFICATIONS) AND APPLICABLE STANDARD CONSTRUCTION DRAWINGS SHALL APPLY TO THIS PROJECT IN ADDITION TO THE FOLLOWING NOTES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SAFE AND EFFECTIVE VEHICULAR TRAFFIC CONTROL 24 HOURS A DAY FOR THE DURATION OF THIS PROJECT. THIS WILL INCLUDE PROVIDING, PLACING, MAINTAINING AND SUBSEQUENTLY REMOVING ALL NECESSARY TRAFFIC CONTROL DEVICES FOR ALL PROPOSED CONSTRUCTION OPERATIONS.

BEFORE THE WORK BEGINS, THE CONTRACTOR SHALL SUBMIT TO THE CITY THE NAME(S) AND TELEPHONE NUMBER(S) OF A PERSON OR PERSONS WHO CAN BE CONTACTED 24 HOURS A DAY BY THE CITY, OR ANY OTHER INTERESTED POLICE AGENCY.

THIS PERSON OR PERSONS SHALL BE RESPONSIBLE FOR REPAIRING AND/OR REPLACING ALL TRAFFIC CONTROL DEVICES NEEDED TO MAINTAIN THE SAFETY OF THE TRAVELED PAVEMENT FOR THE DURATION OF THIS PROJECT. THIS PERSON SHALL HAVE AVAILABLE ALL MATERIALS, EQUIPMENT AND INCIDENTALS NECESSARY TO PERFORM THE REQUIRED REPAIRS WITHIN A REASONABLE PERIOD OF TIME AS PER C.M.S. 614.14.

THE CONTRACTOR SHALL ALSO SUBMIT A CONSTRUCTION SEQUENCING SCHEDULE PRIOR TO WORK BEGINNING FOR APPROVAL BY THE CITY. THE CONSTRUCTION SEQUENCING SCHEDULE SHALL TAKE INTO CONSIDERATION ALL ASPECTS OF THE PROJECT INCLUDING HOW LOCAL TRAFFIC TO THE BUSINESSES WILL BE MAINTAINED. THE CONSTRUCTION SEQUENCE WILL NEED TO BE APPROVED BY THE CITY PRIOR TO ANY COMMENCEMENT OF WORK.

ACCESS TO AND FROM ALL LOCAL RESIDENTIAL AND BUSINESS DRIVES WITHIN THE LIMITS OF THIS PROJECT SHALL BE MAINTAINED AT ALL TIMES (24 HOURS A DAY) BY USING THE EXISTING PAVEMENT, TEMPORARY PAVEMENT, AND THE PROPOSED PAVEMENT UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO SEQUENCE HIS WORK TO HELP MINIMIZE THE NEED FOR TEMPORARY AGGREGATE PAVEMENT. TEMPORARY AGGREGATE PAVEMENT CAN BE ASPHALT GRINDINGS OR OTHER AGGREGATE APPROVED BY THE CITY. THE COST OF INSTALLATION, MATERIAL, AND REMOVAL OF THE TEMPORARY AGGREGATE PAVEMENT IS TO BE PART OF THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

WHERE MORE THAN ONE ACCESS TO A BUSINESS OR RESIDENCE EXISTS, ONLY ONE ACCESS NEEDS TO BE MAINTAINED AT A TIME. WHERE ONLY ONE DRIVE EXISTS, ACCESS SHALL BE MAINTAINED AT ALL TIMES BY CONSTRUCTION OF ONE-HALF OF THE DRIVEWAY AT ONE TIME SUBJECT TO THE APPROVAL OF THE CITY.

TEMPORARY ACCESS SHALL BE PROVIDED TO ALL DRIVEWAYS AND ALLEYS WITH A CHANGE IN ELEVATION FROM DRIVEWAY ACCESS TO TEMPORARY DRIVE RAMP NOT TO EXCEED 1-1/2".

THE CONTRACTOR SHALL NOTIFY THE CITY 14 DAYS PRIOR TO THE START OF ANY DETOUR OR LANE RESTRICTIONS. THE CITY IS REQUIRED TO PROVIDE A 14 DAY NOTIFICATION PRIOR TO THE START OF DETOUR/LANE RESTRICTIONS TO ODOT. THIS DETOUR/LANE RESTRICTION COMMUNICATION MUST STATE DATE OF CLOSURE AND LENGTH OF CLOSURE. ALL LOCAL SCHOOLS AND EMERGENCY SERVICES WHICH ARE LIKELY TO UTILIZE THIS SECTION OF CR 25-A AND EVANSTON ROAD ARE TO BE NOTIFIED OF THE DETOUR NOT LESS THAN 14 DAYS PRIOR TO ROAD CLOSURE.

THE CONTRACTOR SHALL GIVE THE CITY AND ODOT DISTRICT 7 ROADWAY SERVICE MANAGER A MINIMUM OF 14 CALENDAR DAYS NOTICE PRIOR TO CLOSING ANY ROAD OR MOVEMENT TO TRAFFIC.

CITY OF TIPP CITY
ENGINEERING
260 S. GARBER DRIVE
TIPP CITY, OHIO 45371
(937) 667-6305

ODOT ROADWAY SERVICES MANAGER
1001 ST. MARYS AVE.
P.O. BOX 969
SIDNEY, OHIO 45365
(937) 497-6891

ANY DAMAGE TO MAINTENANCE OF TRAFFIC EQUIPMENT SUCH AS SIGNS, DRUMS, ETC. SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

THE CONTRACTOR SHALL ADJUST THE LOCATION AND/OR SPACING OF ALL TRAFFIC CONTROL CHANNELING DEVICES AS DICTATED BY THE PROGRESS OF THE REQUIRED WORK TO ALLOW CONSTRUCTION ACCESS TO WORK AREAS WHILE MAINTAINING SAFE AND EFFECTIVE TRAFFIC CONTROL DURING ALL CONSTRUCTION OPERATIONS. THE ORIGINAL LOCATION, PLACEMENT, SPACING AND SUBSEQUENT RELOCATION OR REMOVAL OF ALL TRAFFIC CONTROL DEVICES SHALL BE SUBJECT TO THE CITY'S APPROVAL.

IT IS INTENDED THAT THE LOCAL TRAFFIC NOT BE SUBJECTED TO ANY LANE CLOSURES UNLESS ACTIVE WORK IS BEING PERFORMED IN OR IMMEDIATELY ADJACENT TO THE CLOSED LANE. THE ROADWAY SHALL NOT BE RESTRICTED TO ANY LANE CLOSURE DURING PERIODS OF INTERMITTENT OR IRREGULAR WORK, NOR CLOSED SOLELY FOR THE CONVENIENCE OF THE CONTRACTOR. THE CITY SHALL MAKE THE FINAL DETERMINATION AS TO WHAT CONSTITUTES ACTIVE WORK AND WHETHER OR NOT THE LANE CLOSURE IS JUSTIFIED.

IF, IN THE OPINION OF THE CITY, THE LANE CLOSURE IS NOT JUSTIFIED, THEY MAY ORDER ALL OR PART OF THE LANE CLOSURE REOPENED TO LOCAL TRAFFIC (UNTIL SUCH TIME THIS CONDITION IS CORRECTED.)

THE CONTRACTOR SHALL NOTIFY THE CITY OF ANY INTENDED CHANGES TO ANY EXISTING OR TEMPORARY TRAFFIC CONTROL DEVICES AND SHALL OBTAIN THE CITY'S APPROVAL PRIOR TO MAKING THE CHANGES. THE CONTRACTOR SHALL ALSO NOTIFY THE CITY AND LOCAL NEWSPAPER 48 HOURS IN ADVANCE OF ANY INTENDED LANE CLOSURES.

NOTICES OF THE DETOURS WILL BE POSTED IN THE LOCAL MEDIA PRIOR TO THE ROAD CLOSURES AND EMERGENCY SERVICES WILL ALSO BE NOTIFIED. ACCESS FOR LOCAL TRAFFIC WILL BE PROVIDED AT ALL TIMES DURING THE ROAD CLOSURES. ROAD CLOSURE PERIOD SHALL NOT EXCEED THE PHASE DAYS LISTED BELOW. NORMAL C&MS DISINCENTIVE WILL APPLY.

THE CONTRACTOR SHALL INSTALL ADVANCE WARNING SIGNS PER STANDARD DRAWING MT-101.60.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

CONSTRUCTION SEQUENCE

THE CONTRACTOR SHALL FURNISH AND INSTALL ADVANCE WARNING "ROAD WORK AHEAD" (W20-1) SIGNS AND "END ROAD WORK" (G20-2) SIGNS AS WELL AS OTHER NECESSARY MAINTENANCE OF TRAFFIC SIGNS AT THE BEGINNING AND END OF THE PROJECT AND ALSO ON ALL CROSSROADS.

THE WORK SHALL CONSIST OF:

PHASE 1: THIS PHASE IS TO CONSIST OF THE INSTALLATION OF ALL PROPOSED PAVEMENT, CURBING, DRIVEWAYS, CURB RAMP, WATERLINE AND STORM SEWER UP TO AND INCLUDING ITEM 301, ASPHALT CONCRETE BASE FROM MICHAELS ROAD TO EVANSTON ROAD (STA. 326+28.1). EVANSTON ROAD MUST REMAIN OPEN TO TWO-WAY TRAFFIC DURING THIS PHASE. CR 25-A WILL BE CLOSED TO THRU TRAFFIC. ALSO, ALL PROPOSED TRAFFIC SIGNAL AND FIBER INTERCONNECT WORK SHALL BE COMPLETED DURING THIS PHASE. THIS PHASE SHALL BE COMPLETED BEFORE NOVEMBER 15TH. CR 25-A SHALL REMAIN OPEN TO THRU-TRAFFIC FROM NOVEMBER 15TH THRU MARCH 15TH, UNLESS OTHERWISE APPROVED BY THE CITY. SEE SHEET 12 FOR DETOUR PLAN AND SHEET 13 FOR MOT PLAN.

THIS PHASE IS TO BE COMPLETED WITHIN A 90 CALENDAR DAY PERIOD.

PHASE 2: THIS PHASE IS TO CONSIST OF THE INSTALLATION OF ALL PROPOSED PAVEMENT, CURBING, AND DRIVEWAYS UP TO AND INCLUDING ITEM 442, INTERMEDIATE COURSE ON EVANSTON ROAD. CR 25-A SHALL BE OPEN TO FULL TRAFFIC. EVANSTON ROAD WILL BE CLOSED TO THRU TRAFFIC. SEE SHEET 14 FOR DETOUR PLAN AND SHEET 15-16 FOR MOT PLAN.

THIS PHASE IS TO BE COMPLETED WITHIN A 30 CALENDAR DAY PERIOD.

PHASE 3: THIS PHASE IS TO CONSIST OF PAVEMENT PLANING, PLACEMENT OF ITEM 442 INTERMEDIATE COURSE AND 442 SURFACE COURSE, AND PAVEMENT MARKINGS. ALL NON-APPLICABLE TRAFFIC CONTROL DEVICES SHALL BE REMOVED. CONTRACTOR SHALL FOLLOW ODOT STANDARD DRAWING MT-97.12

PHASE 4: THIS PHASE IS TO CONSIST OF THE INSTALLATION OF SIDEWALK, MINOR GRADING, AND SEEDING & MULCHING FROM ALONG CR 25-A FROM MICHAELS ROAD TO MAIN STREET (SR 571). CR 25-A SHALL BE OPEN TO TRAFFIC AT ALL TIMES. CONTRACTOR SHALL FOLLOW ODOT STANDARD DRAWING MT-95.31 FOR MAINTAINING TRAFFIC.

THIS PHASE IS TO BE COMPLETED WITHIN A 45 CALENDAR DAY PERIOD (THIS PHASE CAN ONLY START WHEN THE FINISH DATE WILL BE DURING OPTIMAL SEEDING TIME.)

THIS PHASE CAN BE INSTALLED IN CONJUNCTION WITH ANY PHASE OF CONSTRUCTION, HOWEVER WILL NOT ADD ANY ADDITIONAL DAYS OF CONSTRUCTION TO THEIR RESPECTIVE PHASE OF THE PROJECT.

THE PROPOSED STREET LIGHTING CAN BE INSTALLED DURING ANY OF THE ABOVE MENTIONED PHASES. HOWEVER, THIS WORK SHALL NOT ADD ANY ADDITIONAL DAYS OF CONSTRUCTION TO THE RESPECTED PHASE'S TIMELINE FOR WORK TO BE COMPLETED.

THE SEQUENCE OF CONSTRUCTION NEEDS TO PROVIDE A WORK AREA FOR THE CONTRACTOR WHILE ALSO MAINTAINING TRAFFIC IN A MANNER WHICH IS SAFE FOR THE TRAVELING AND PEDESTRIAN PUBLIC.

FOR MAINTENANCE OF LOCAL, BUSINESS, AND EMERGENCY VEHICLE TRAFFIC PURPOSES LOCAL TRAFFIC MUST BE MAINTAINED AT ALL TIMES. MINIMUM LANE WIDTHS OF 11 FEET SHALL BE PROVIDED AT ALL TIMES, ALONG WITH ADEQUATE RADII AT INTERSECTIONS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIAL NEEDED FOR MAINTAINING TRAFFIC THROUGH ALL STAGES OF CONSTRUCTION IN ACCORDANCE WITH ITEM 614, INCLUDING STIPULATIONS STATED ELSEWHERE ON THESE PLANS, SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

MAINTENANCE OF TRAFFIC SIGNAL INSTALLATION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC SIGNAL INSTALLATIONS WITHIN THE PROJECT UNDER THE FOLLOWING CONDITIONS:

- EXISTING SIGNAL/FLASHER INSTALLATIONS WHICH THE PLANS REQUIRE THE CONTRACTOR TO ADJUST, MODIFY, ADD ONTO OR REMOVE, OR WHICH THE CONTRACTOR ACTUALLY ADJUSTS, MODIFIES OR OTHERWISE DISTURBS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ENTIRE INSTALLATION (AT AN INTERSECTION) FROM THE TIME HIS OPERATIONS FIRST DISTURB THE INSTALLATION UNTIL THE INSTALLATION HAS BEEN SUBSEQUENTLY REMOVED OR MODIFIED AND THE WORK IS ACCEPTED.
- NEW OR REUSED SIGNAL INSTALLATIONS OR DEVICES, INSTALLED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THESE FROM THE TIME OF INSTALLATION UNTIL THE WORK IS ACCEPTED.

THE CONTRACTOR SHALL CORRECT AS QUICKLY AS POSSIBLE ALL OUTAGES OR MALFUNCTIONS. HE SHALL PROVIDE THE MAINTAINING AGENCY AND THE ENGINEER SUCH ADDRESSES AND PHONE NUMBERS WHERE HIS MAINTENANCE FORCES CAN BE CONTACTED. THE CONTRACTOR SHALL PROVIDE ONE OR MORE PERSONS TO RECEIVE ALL CALLS AND DISPATCH THE NECESSARY MAINTENANCE FORCES TO CORRECT OUTAGES. SUCH A PERSON OR PERSONS MAY BE USED TO PERFORM OTHER DUTIES AS LONG AS PROMPT ATTENTION IS GIVEN TO THESE CALLS AND A PERSON IS READILY AVAILABLE CONTINUOUSLY 24 HOURS A DAY, 7 DAYS A WEEK. ALL LAMP OUTAGES, CABLE OUTAGES, ELECTRICAL FAILURES, EQUIPMENT MALFUNCTIONS AND MISALIGNED SIGNAL HEADS SHALL BE CORRECTED TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK TO SERVICE WITHIN TWO HOURS AFTER THE CONTRACTOR HAS BEEN NOTIFIED OF THE OUTAGE.

IN THE EVENT NEW SIGNALS ARE DAMAGED PRIOR TO ACCEPTANCE, ALL DAMAGED EQUIPMENT EXCEPT POLES AND CONTROL EQUIPMENT SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK IN SERVICE WITHIN 8 HOURS AFTER THE CONTRACTOR'S NOTIFICATION OF THE OUTAGE. THE CONTRACTOR SHALL ARRANGE FOR FULL TRAFFIC CONTROL UNTIL THE SIGNAL IS BACK IN OPERATION.

IF POLES AND/OR CONTROL EQUIPMENT ARE DAMAGED AND MUST BE REPLACED, THE CONTRACTOR SHALL MAKE TEMPORARY REPAIRS AS NECESSARY TO BRING THE SIGNAL BACK INTO FULL OPERATION WITHIN THE ALLOWED 8-HOUR PERIOD, AND SHALL MAKE PERMANENT REPAIRS OR REPLACEMENT AS SOON THEREAFTER AS POSSIBLE.

NONE OF THE ABOVE SHALL BE CONSTRUED AS COLLECTIVE OR CONSECUTIVE OUTAGE TIME PERIODS AT ANY ONE LOCATION. THAT IS, WHERE MORE THAN ONE OUTAGE OCCURS AT ANY ONE LOCATION THEN THE ALLOTTED TIME LIMIT SHALL BE FOR THE WORST SINGLE OUTAGE.

WHERE OUTAGES ARE THE DIRECT RESULT OF A VEHICLE ACCIDENT THE RESPONSE OF THE CONTRACTOR SHALL BE AS OUTLINED ABOVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTION OF ANY COMPENSATION FOR THIS WORK FROM THOSE PARTIES RESPONSIBLE FOR THE DAMAGE.

WHERE THE CONTRACTOR HAS FAILED TO, OR CANNOT RESPOND TO, AN OUTAGE OR SIGNAL EQUIPMENT MALFUNCTION, AT THESE LOCATIONS WITHIN HIS RESPONSIBILITY, WITHIN PERIODS AS SPECIFIED ABOVE, THE ENGINEER MAY INVOKE THE PROVISIONS OF SECTION 105.15 AND ANY SUBSEQUENT BILLINGS TO THE STATE OR THE CITY OF TIPP CITY FOR POLICE SERVICES AND MAINTENANCE SERVICES BY CITY FORCES SHALL BE DEDUCTED FROM MONIES DUE OR TO BECOME DUE THE CONTRACTOR IN ACCORDANCE WITH PROVISIONS OF SECTION 105.15.

THE CONTRACTOR SHALL PROVIDE THE MAINTENANCE SERVICE ENTIRELY WITH HIS FORCES OR HE MAY CHOOSE TO ENTER INTO A COOPERATIVE UNDERSTANDING WITH THE LOCAL MAINTAINING AGENCY TO PROVIDE THE MAINTENANCE. THE CONTRACTOR SHALL INFORM THE ENGINEER, IN WRITING, OF THE MAINTENANCE METHOD SELECTED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY TRAFFIC SIGNAL COMPONENTS REQUIRED TO BE HANDLED DURING THE RELOCATION OF POLES AND REVISIONS TO THE SIGNAL SYSTEM. WHEN A TRAFFIC SIGNAL MUST BE TAKEN OUT OF SERVICE BY THE CONTRACTOR, DUE TO CONSTRUCTION PROCEDURES, THIS OUTAGE SHALL NOT EXCEED 4 HOURS AND SHALL NOT INCLUDE THE HOURS OF 7 TO 9 AM AND 4 TO 6 PM. ANY SIGNALIZED INTERSECTION, WHERE THE SIGNAL IS OUT OF SERVICE DUE TO CONSTRUCTION PROCEDURES, OR DUE TO AN OUTAGE OR MALFUNCTION OF EQUIPMENT AS DESCRIBED ABOVE, SHALL BE PROTECTED, BY THE CONTRACTOR, BY THE INSTALLATION OF TEMPORARY "STOP" SIGNS.

ANY VEHICULAR TRAFFIC SIGNAL HEAD, EITHER NEW OR EXISTING WHICH WILL BE OUT OF OPERATION SHALL BE COVERED IN THE MANNER DESCRIBED IN 632.25.

THE CONTRACTOR SHALL MAINTAIN COMPLETE RECORDS OF MALFUNCTIONS INCLUDING:

- TIME OF NOTIFICATION OF MALFUNCTION;
- TIME OF WORK CREWS ARRIVAL TO CORRECT THE MALFUNCTION;
- ACTIONS TAKEN TO CORRECT THE MALFUNCTION, INCLUDING A LIST OF PARTS REPAIRED OR REPLACED;
- A DIAGNOSIS OF REASON FOR THE MALFUNCTION AND PROBABILITY OF REOCCURRENCE;
- TIME OF COMPLETION OF THE REPAIR AND SYSTEM RESTORED TO FULL SERVICE.

A COPY OF THESE RECORDS SHALL BE PROVIDED TO THE ENGINEER WITHIN THREE (3) WORKING DAYS FOLLOWING COMPLETION OF EACH REPAIR.

ALL COSTS RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.



SIDNEY, OHIO 937-497-0200
LOVELAND, OHIO 912-239-8334
PORTLAND, INDIANA 260-766-2300
WWW.CHOICEONEENGINEERING.COM

SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
MAINTENANCE OF TRAFFIC GENERAL NOTES

REVISIONS:

FILE NAME

MIATC1406MNA

DRAWN BY

BHB

CHECKED BY

JSP

PROJECT No.

MIATC1406

DATE

12-19-2016

SHEET NUMBER

10 OF 52

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOs) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOs SHOULD NOT BE USED WHERE THE ODOT INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE ODOT, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE ODOT, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOs SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOs SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOs WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOs WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOs. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOs' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOs SHALL BE USED DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE OF PHASE 1. THE LEOs SHOULD BE LOCATED AT THE INTERSECTIONS OF S. CR 25-A & MICHAELS ROAD AND S. CR 25-A & EVANSTON ROAD. LEOs SHALL BE USED DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE OF PHASE 2. LEOs SHOULD BE LOCATED AT THE INTERSECTION OF S. CR 25-A & EVANSTON ROAD.

LEOs (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE ENGINEER'S ESTIMATE.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.....10 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE ENGINEER'S ESTIMATE TO BE USED AS DIRECTED BY THE ENGINEER FOR MAINTENANCE OF TRAFFIC:

ITEM 614 WORK ZONE EDGE LINE, CLASS 1.....2 MILE

ITEM 614 WORK ZONE CENTER LINE, CLASS 1.....0.5 MILE

ITEM 614 WORK ZONE STOP LINE, CLASS 1.....100 FEET

ITEM 616 WATER.....40 M GALLONS

ACCESS TO BUILDINGS, COMMERCIAL FACILITIES, AND EVENT INFORMATION

ACCESS TO BUSINESSES IN OPERATION SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT AT ALL TIMES. ACCESS SHALL BE MAINTAINED BY MINIMIZING THE AMOUNT OF TIME OF CONSTRUCTION ACTIVITY DIRECTLY IN FRONT OF ENTRY WAYS INTO BUILDINGS AND REMOVING EQUIPMENT NOT IN USE.

NOTIFICATION

THE CONTRACTOR SHALL NOTIFY THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 7 CONSTRUCTION ADMINISTRATOR AND THE MIAMI COUNTY ENGINEER 14 DAYS PRIOR TO BEGINNING WORK:

ODOT: 937-497-6722

MIAMI COUNTY: 937-440-5656

ALTERNATE METHODS

THE CONTRACTOR MAY SUBMIT ALTERNATE METHODS FOR THE MAINTENANCE OF TRAFFIC, AND/OR CONSTRUCTION SEQUENCE PROVIDED THE INTENT OF THE ABOVE PLAN PROVISIONS IS FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC AND MERCHANTS RESULTS THEREFROM. NO ALTERNATE PLAN SHALL BE PLACED INTO EFFECT UNTIL APPROVAL HAS BEEN GRANTED, IN WRITING, BY THE CITY.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 3 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

OVERNIGHT AND WEEKEND ACCESS TO DRIVES DURING BASE PREPARATION AND OTHER CONSTRUCTION ACTIVITIES

ACCESS TO DRIVEWAYS DURING OVERNIGHT AND WEEKEND HOURS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT. TEMPORARY ACCESS SHALL BE PROVIDED TO ALL DRIVEWAYS AND ALLEYS WITH A CHANGE IN ELEVATION FROM DRIVEWAY ACCESS TO TEMPORARY DRIVE RAMP NOT TO EXCEED 1-1/2".

ACCESS TO ALL INTERSECTIONS SHALL BE MAINTAINED 24 HOURS A DAY. DURING CONSTRUCTION OF UTILITY CROSSINGS, ONE-WAY TRAFFIC SHALL BE MAINTAINED WITH FLAGGER PER THE REQUIREMENTS OF THE OHIO UNIFORM MANUAL OF TRAFFIC CONTROL DEVICES WITH THE APPROVAL OF THE ENGINEER WITH A MINIMUM OF A 11 FOOT WIDE LANE WITH ADEQUATE (30 FOOT) RADII FOR TURNING.

HAUL ROADS

WHEN PICKING A DUMP SITE, CONTRACTOR IS TO TAKE INTO CONSIDERATION THE HAUL ROAD ROUTE AND ANY NECESSARY ROADWAY REPAIR CAUSED BY HAULING TO THE DUMP SITE.

PRIOR TO HAULING EQUIPMENT OR MATERIALS THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO THE CITY OF THE SPECIFIC ROADS OR STREETS ON THE HAUL ROUTE. IF THE HAUL ROUTE INCLUDES ROADS AND STREETS THAT ARE NOT UNDER THE JURISDICTION AND CONTROL OF THE CITY OR OF THE STATE, THE CONTRACTOR MUST USE LOCAL ROADS AND STREETS THAT ARE NOT RESTRICTED BY LOCAL AUTHORITIES. IF IT IS DETERMINED BY THE CITY THAT THE HAUL ROADS USED TO HAUL EQUIPMENT AND MATERIALS TO THE DUMP SITE WERE DAMAGED FROM THIS OPERATION, THE CITY WILL ORDER THE CONTRACTOR TO PERFORM IMMEDIATE AND PRACTICAL REPAIRS TO ENSURE REASONABLY NORMAL TRAVELING CONDITIONS AND BRING PAVEMENT CONDITIONS BACK TO WHAT THEY WERE PRIOR TO CONTRACTOR'S OPERATION AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL TAKE ALL THIS INTO CONSIDERATION WHEN PICKING A DUMP SITE.

THE CONTRACTOR SHALL NOT FILE A CLAIM FOR DELAYS OR OTHER IMPACTS TO THE WORK CAUSED BY DISPUTE WITH THE LOCAL AUTHORITIES REGARDING THE USE OF LOCAL ROADS OR STREETS AS HAUL ROADS. THE CONTRACTOR SHALL HOLD THE CITY AND THE STATE HARMLESS FOR ANY CLOSURES OR HAULING RESTRICTION OUTSIDE THE PROJECT LIMITS BEYOND THE CONTROL OF THE CITY OR ODOT.



SIDNEY, OHIO 937-497-0200
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SOUTH CR 25-A RECONSTRUCTION, PH. 2

CITY OF TIPP CITY

MAINTENANCE OF TRAFFIC GENERAL NOTES

REVISIONS:

FILE NAME

OT - MIATC1406MIN

DRAWN BY

BHB

CHECKED BY

JSP

PROJECT No.

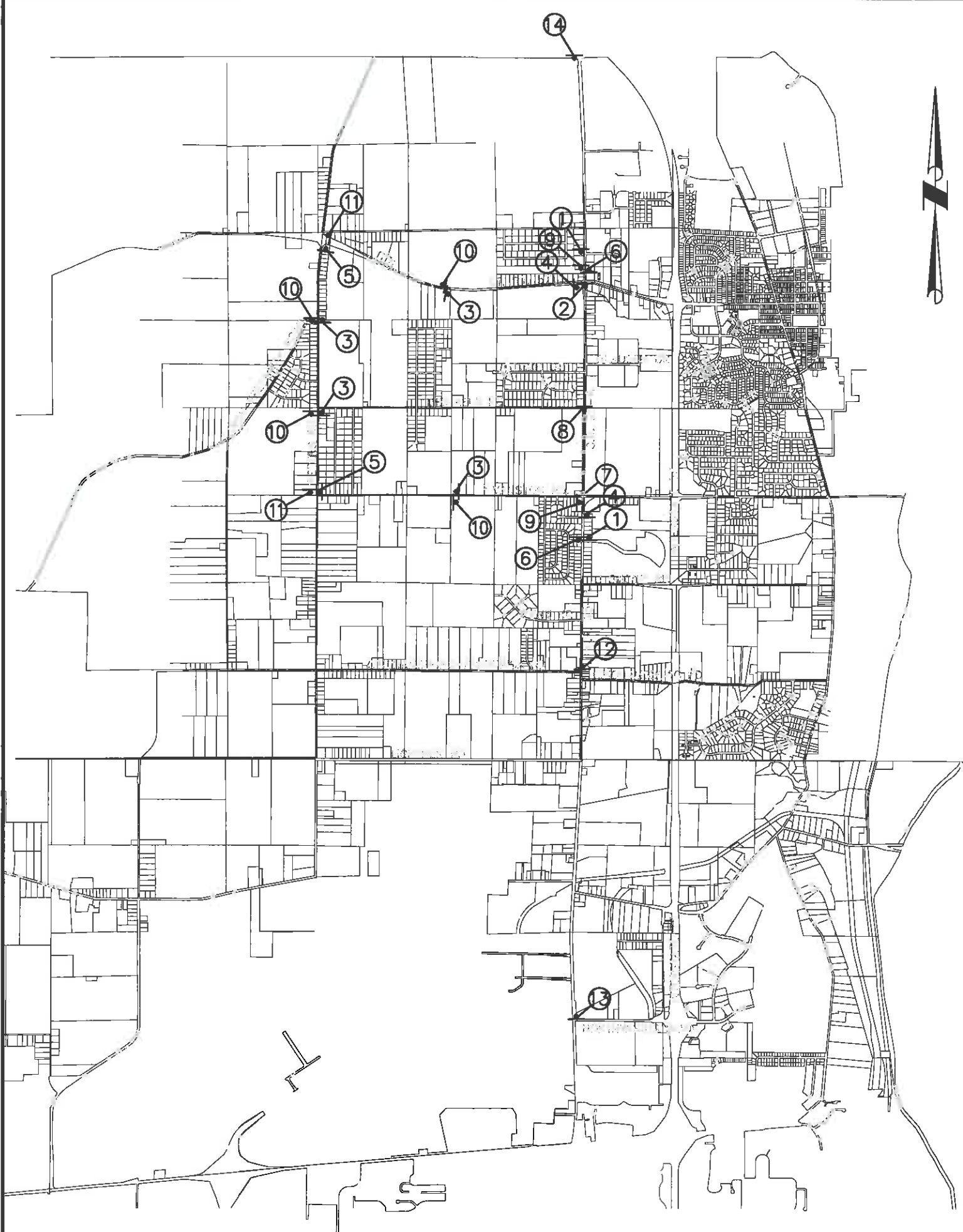
MIATC1406

DATE

12-19-2016

SHEET NUMBER

11 OF 52



NOTE:
THIS DETOUR PLAN IS FOR PHASE 1 OF THE
CONSTRUCTION SEQUENCE ONLY. DETOUR SIGNS
SHALL BE ERECTED FOR THIS PHASE AND SHALL
NOT BE ERECTED FOR ANY OTHER PHASE.

ROAD CLOSED
1.0 MILES AHEAD ⑫
R11-3A-60

ROAD CLOSED
3.0 MILES AHEAD ⑬
R11-3A-60

ROAD CLOSED
2.0 MILES AHEAD ⑭
R11-3A-60

DETOUR AHEAD ①
W20-2-36

R11-3A-60
ROAD CLOSED
1.0 MILES AHEAD
LOCAL TRAFFIC ONLY

DETOUR ②
M4-10R-48

D3-48
S C.R. 25-A

DETOUR ③
M4-9-30

D3-48
S C.R. 25-A

DETOUR ④
M4-9L-30

D3-48
S C.R. 25-A

DETOUR ⑤
M4-9R-30

M4-8a
END
DETOUR ⑥

R11-2
ROAD CLOSED

DETOUR ⑦
M4-10L-48

R11-4-60
ROAD CLOSED
TO
THRU TRAFFIC ⑧

D3-48
S C.R. 25-A

DETOUR ⑨
M4-9R-30

D3-48
S C.R. 25-A

DETOUR ⑩
M4-9-30

D3-48
S C.R. 25-A

DETOUR ⑪
M4-9L-30

PAYMENT FOR ALL OF THE WORK REQUIRED BY THE CONTRACTOR FOR TRAFFIC CONTROL NOTED ON THIS SHEET INCLUDING PROVIDING, ERECTING, MAINTAINING, AND REMOVING ALL FLASHERS, SIGNS, BARRICADES, SUPPORTS, AND ALL OTHER TRAFFIC CONTROL DEVICES SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 DETOUR SIGNING. (THE CONTRACTOR SHALL INSTALL ADVANCE WARNING SIGNS PER STANDARD DRAWING MT-101.60.)

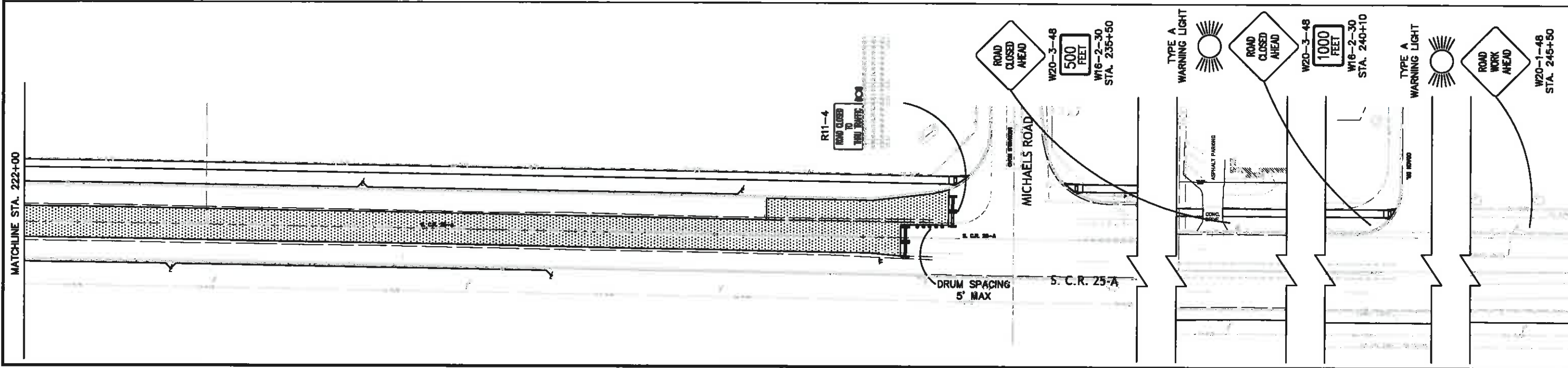
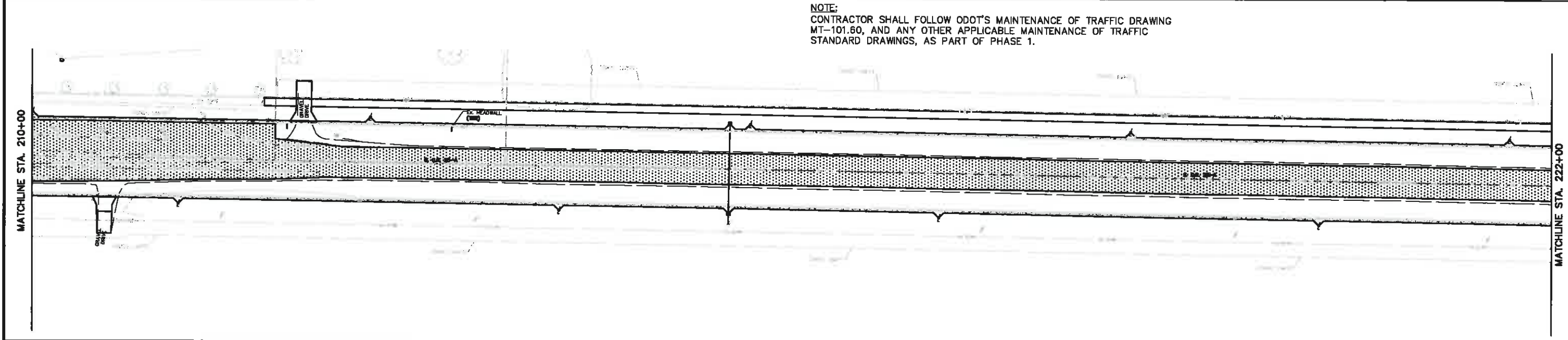
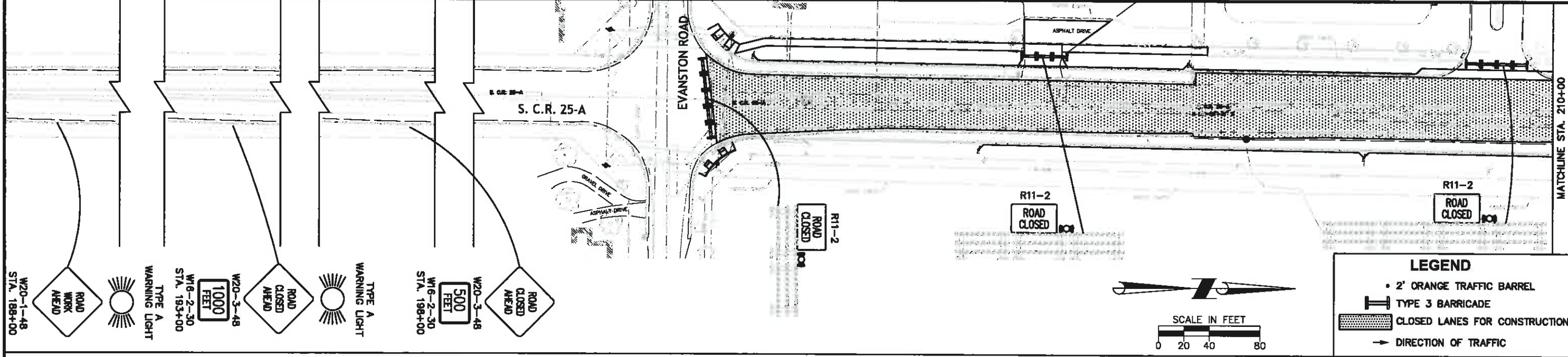
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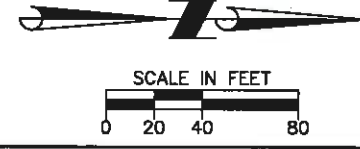
SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
DETOUR PLAN FOR PHASE 1

REVISIONS:

FILE NAME	MIATCH406DETA
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	12 OF 52



NOTE:
 CONTRACTOR SHALL FOLLOW ODOT'S MAINTENANCE OF TRAFFIC DRAWING MT-101.60, AND ANY OTHER APPLICABLE MAINTENANCE OF TRAFFIC STANDARD DRAWINGS, AS PART OF PHASE 1.



LEGEND

- 2' ORANGE TRAFFIC BARREL
- ▬ TYPE 3 BARRICADE
- ▨ CLOSED LANES FOR CONSTRUCTION
- DIRECTION OF TRAFFIC

W20-1-48
 STA. 189+00

MATCHLINE STA. 210+00

MATCHLINE STA. 222+00

MATCHLINE STA. 210+00

MATCHLINE STA. 222+00

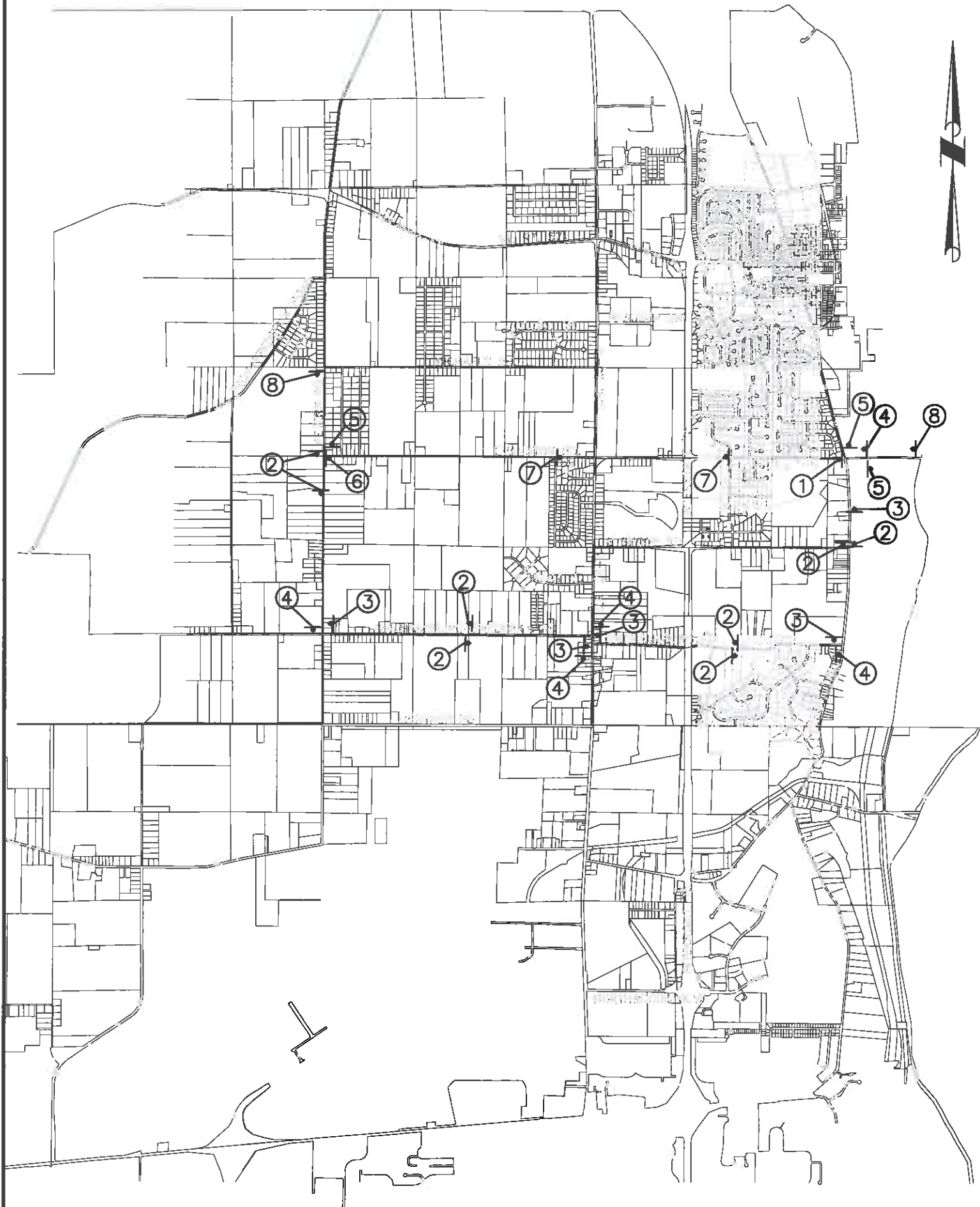
W20-1-48
 STA. 246+50

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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
MAINTENANCE OF TRAFFIC PLAN - PHASE 1

REVISIONS:
FILE NAME MIATC1406MTA
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATC1406
DATE 12-19-2016
SHEET NUMBER 13 OF 52



NOTE:
THIS DETOUR PLAN IS FOR PHASE 2 OF THE
CONSTRUCTION SEQUENCE ONLY. DETOUR SIGNS
SHALL BE ERECTED FOR THIS PHASE AND SHALL
NOT BE ERECTED FOR ANY OTHER PHASE.

R11-3A-60
ROAD CLOSED
0.6 MILES AHEAD
LOCAL TRAFFIC ONLY

← DETOUR ①

M4-10L-48

D3-48
EVANSTON ROAD

↑ DETOUR ②

M4-9-30

D3-48
EVANSTON ROAD

→ DETOUR ③

M4-9R-30

D3-48
EVANSTON ROAD

← DETOUR ④

M4-9L-30

M4-8a
END
DETOUR ⑤

R11-3A
ROAD CLOSED
1.2 MILES AHEAD
LOCAL TRAFFIC ONLY

→ DETOUR ⑥

M4-10R-48

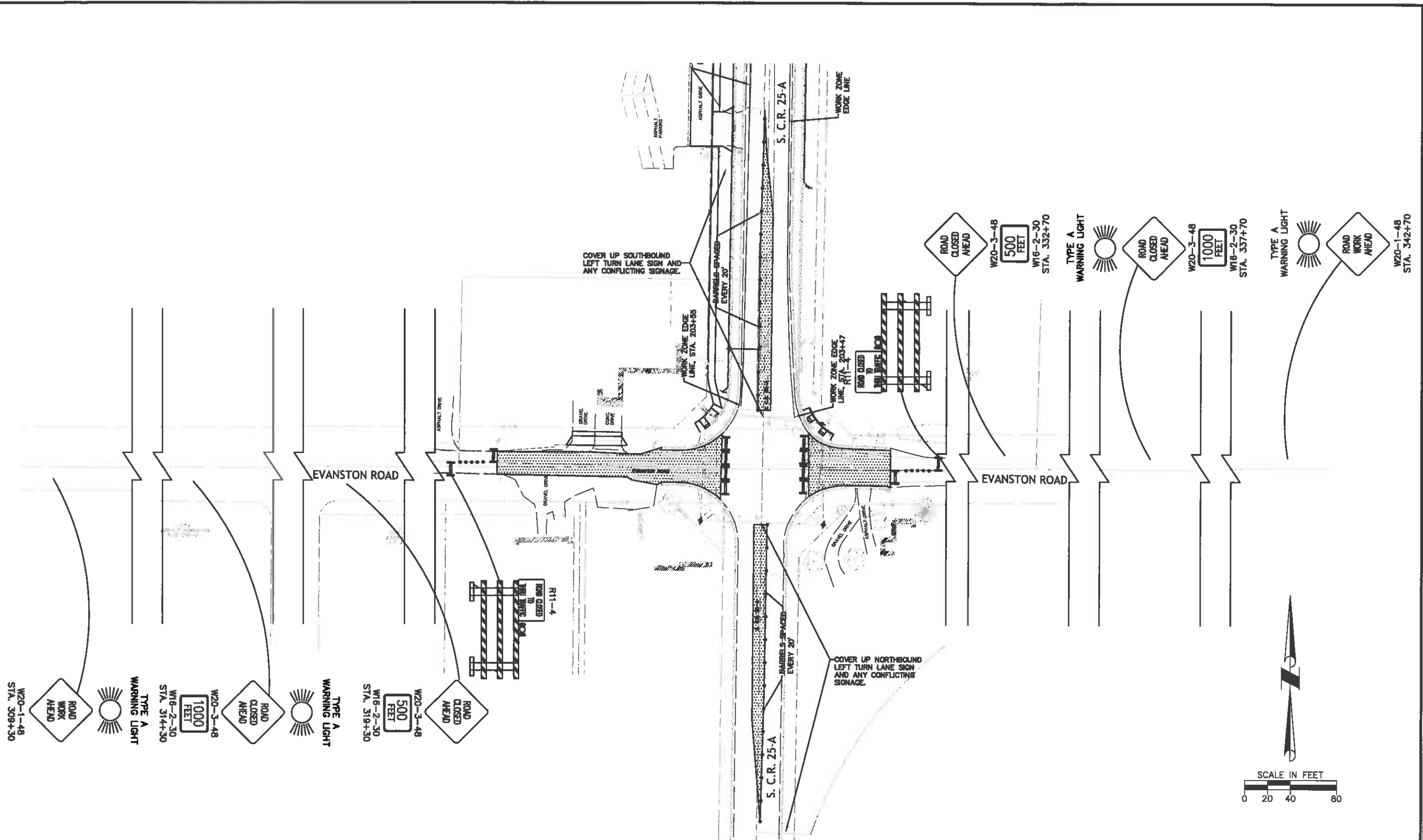
R11-3A-60
ROAD CLOSED
LOCAL TRAFFIC ONLY ⑦

◇ DETOUR
AHEAD ⑧

W20-2-36

PAYMENT FOR ALL OF THE WORK REQUIRED BY THE CONTRACTOR FOR TRAFFIC CONTROL NOTED ON THIS SHEET INCLUDING PROVIDING, ERECTING, MAINTAINING, AND REMOVING ALL FLASHERS, SIGNS, BARRICADES, SUPPORTS, AND ALL OTHER TRAFFIC CONTROL DEVICES SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 DETOUR SIGNING. (THE CONTRACTOR SHALL INSTALL ADVANCE WARNING SIGNS PER STANDARD DRAWING MT-101.60.)

REVISIONS:
FILE NAME MIATC1406DET1B
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATC1406
DATE 12-19-2016
SHEET NUMBER 14 OF 52



W20-1-48
STA. 309+30

ROAD WORK AHEAD

TYPE A
WARNING LIGHT

W20-3-48
STA. 314+30

ROAD CLOSED AHEAD

TYPE A
WARNING LIGHT

W20-3-48
STA. 319+30

ROAD CLOSED AHEAD

W20-3-48
STA. 324+30

ROAD CLOSED AHEAD

W20-3-48
STA. 332+70

ROAD CLOSED AHEAD

TYPE A
WARNING LIGHT

W20-3-48
STA. 337+70

ROAD CLOSED AHEAD

TYPE A
WARNING LIGHT

W20-1-48
STA. 342+70

ROAD WORK AHEAD

NOTE:
CONTRACTOR SHALL FOLLOW ODOT'S MAINTENANCE OF TRAFFIC DRAWINGS MT-101.60 AND ANY OTHER APPLICABLE MAINTENANCE OF TRAFFIC STANDARD DRAWINGS, AS PART OF PHASE 2.

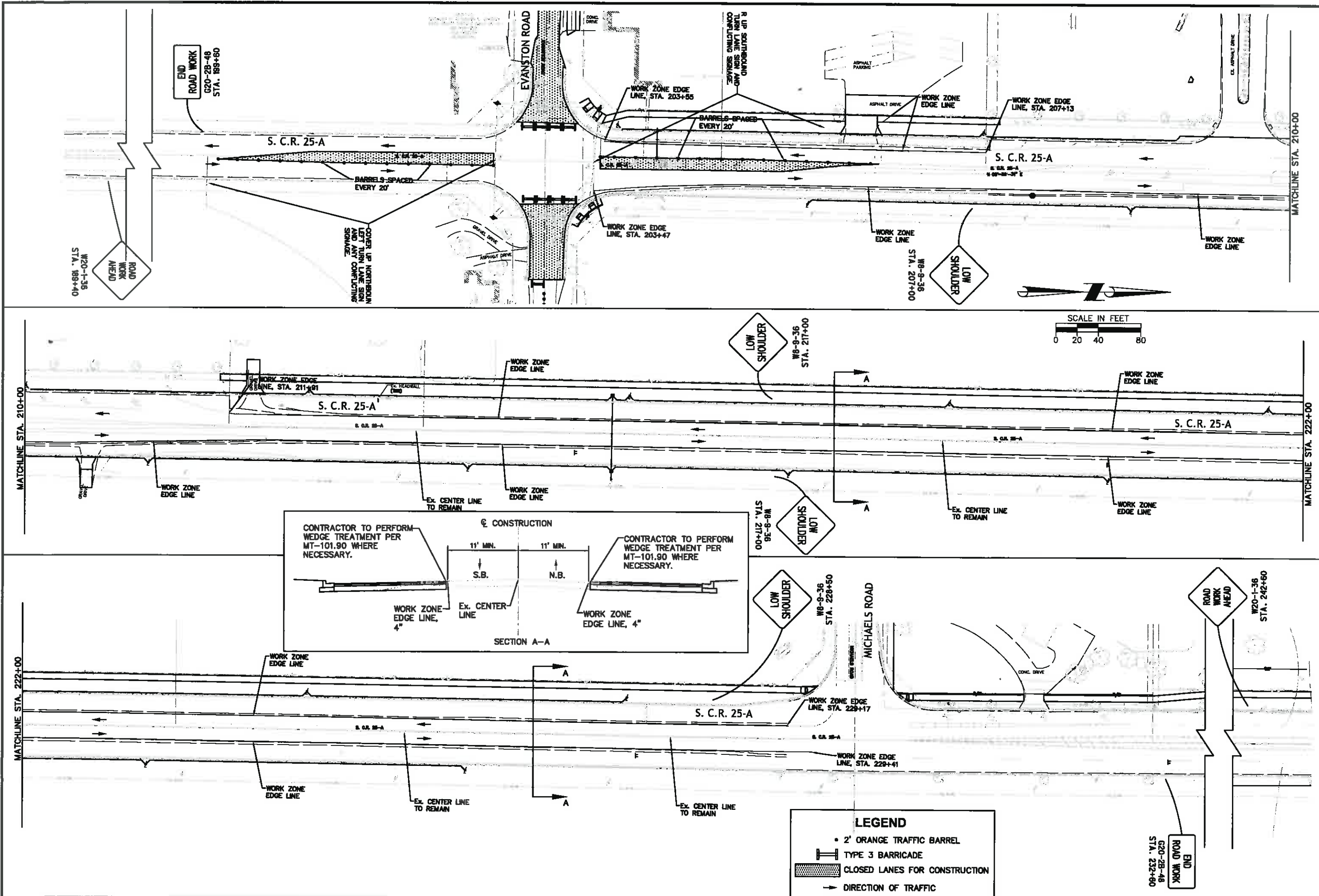
LEGEND

- 2' ORANGE TRAFFIC BARREL
- ▬ TYPE 3 BARRICADE
- ▨ CLOSED LANES FOR CONSTRUCTION
- DIRECTION OF TRAFFIC

REVISIONS:

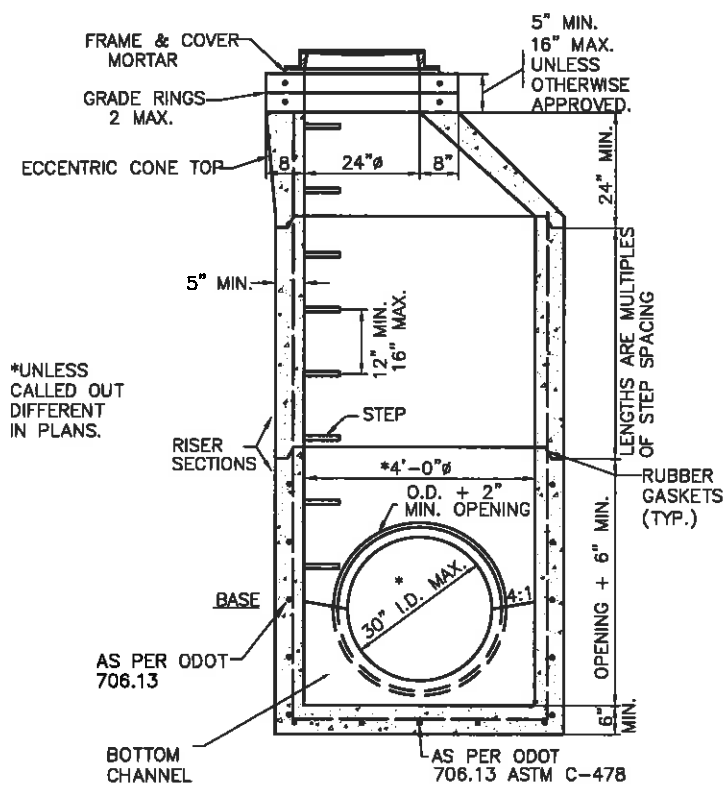
FILE NAME	MIATC1406MTC
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATC1406
DATE	12-19-2016
SHEET NUMBER	15 OF 52

REVISIONS:
FILE NAME MIATCH1406MTB
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATCH1406
DATE 12-19-2016
SHEET NUMBER 16 OF 52



LEGEND

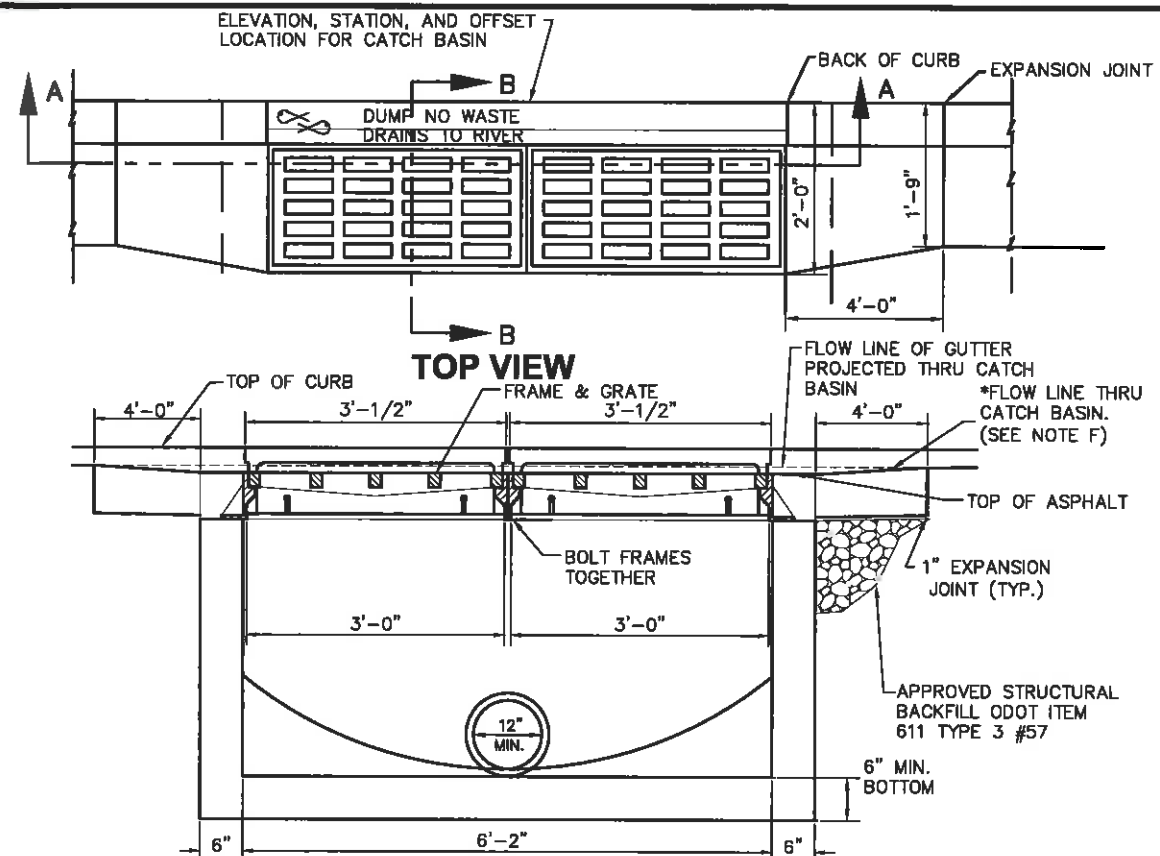
- 2' ORANGE TRAFFIC BARREL
- ▬ TYPE 3 BARRICADE
- ▨ CLOSED LANES FOR CONSTRUCTION
- DIRECTION OF TRAFFIC



TYPE 3 STORM MANHOLE

STORM MANHOLE NOTES

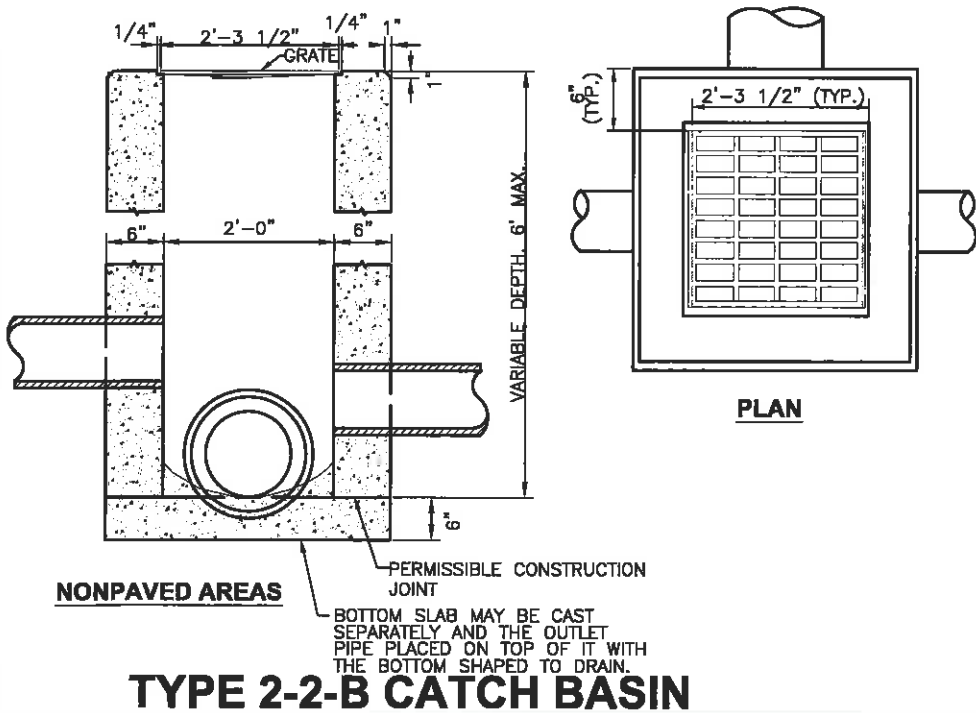
- A. STORM MANHOLE FRAME AND LID SHALL BE EQUAL OF NEENAH NO. R-1772 OR EAST JORDAN IRON WORKS NO. 1022.
- B. SECTIONS OF THE PRECAST MANHOLE SHALL BE CAST AND ASSEMBLED WITH EITHER ALL TONGUE OR ALL GROOVE ENDS UP. LIFT HOLES MAY BE PROVIDED IN EACH SECTION FOR HANDLING.
- C. TOP AND TRANSITION (OR REDUCER) SECTIONS MAY BE EITHER ECCENTRIC CONE OR FLAT SLAB.
- D. OPENINGS IN RISER SECTIONS FOR 18" AND SMALLER INLET PIPES MAY BE PREFABRICATED OR CUT IN THE FIELD PROVIDED THE SIDES OF THE PIPE AT THE SPRING LINE DO NOT PROJECT INTO THE MANHOLE.
- E. MATERIALS FOR BASES AND OTHER PRECAST SECTIONS, INCLUDING REINFORCEMENT NOT SPECIFIED HEREIN, SHALL COMPLY WITH ODOT REQUIREMENT OF 706.13 (ASTM C-478).
- F. LOCATE THE CENTERLINE OF MANHOLE CONES OVER THE CENTERLINE OF THE MAIN SEWER WHENEVER POSSIBLE.
- G. NO LATERALS MAY PROTRUDE INTO THE INTERNAL MANHOLE.
- H. WHEN CONNECTING TO AN EXISTING STORM MANHOLE CARE SHALL BE TAKEN TO KEEP OPENING AS MINIMAL AS POSSIBLE. IF POSSIBLE, SAW CUT OR USE ROTARY HAMMER FOR OPENING TO MINIMIZE DAMAGE TO STORM MANHOLE AND PIPE MUST BE CUT PARALLEL TO STORM MANHOLE. USE NONSHRINK GROUT AROUND PIPE TO SEAL BETWEEN PIPE AND STORM MANHOLE.
- I. JOINTS BETWEEN SECTIONS TO BE RESILIENT AND FLEXIBLE GASKET JOINTS (ODOT 706.11)



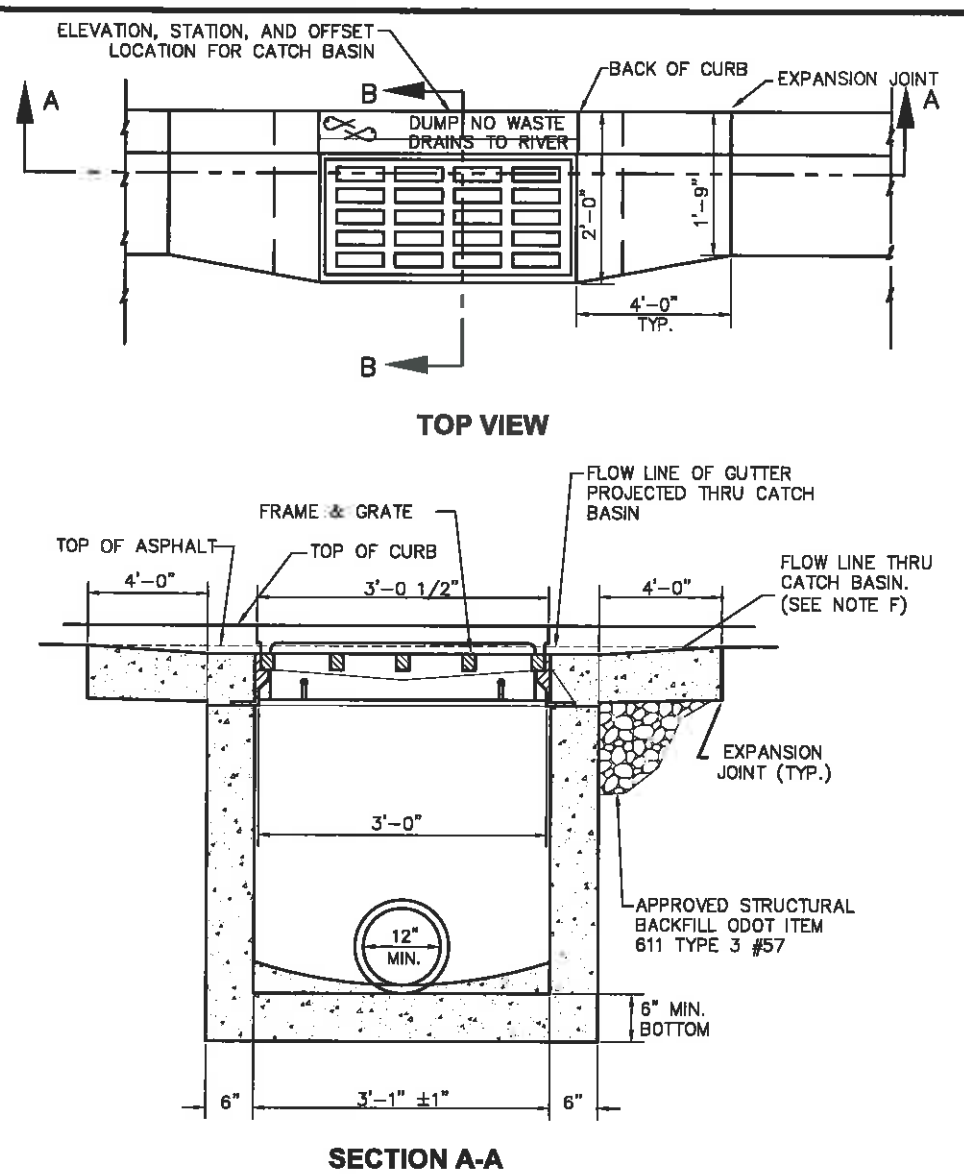
**SECTION A-A
TYPE 1A CATCH BASIN**

TYPE 2-2B CB NOTES

- A. LOCATION AND ELEVATIONS WHEN GIVEN ON THE PLANS IS TOP CENTER OF THE GRATE. WHEN SIDE OPENINGS ARE PROVIDED, ELEVATION SHALL BE THE FLOW LINE OF THE SIDE INLET.
- B. GRATE FOR NONPAVED AREAS SHALL BE EAST JORDAN IRON WORKS 5110 TYPE M3 OR NEENAH CATALOG NO. R-4859-C OR EQUIVALENT.
- C. GRATE ELEVATION TO BE PLACED 4" TO 6" BELOW NORMAL DITCH RETURNING TO NORMAL 10' EACH SIDE OF BASIN.
- D. PRECAST CONSTRUCTION IS REQUIRED, UNLESS OTHERWISE APPROVED, AND CONCRETE SHALL MEET THE REQUIREMENTS OF 706.13. KNOCKOUTS SHALL BE PROVIDED IN PRECAST CONSTRUCTION. PRECAST WALLS SHALL HAVE A SUFFICIENT AMOUNT OF REINFORCEMENT TO PERMIT SHIPPING AND PLACEMENT WITHOUT DAMAGE.
- E. CATCH BASINS NOT PERMITTED IN PAVEMENT AREAS UNLESS USING A FRAME AND GRATE EQUIVALENT OF NEENAH CATALOG NO. R-3405 OR EAST JORDAN IRON WORKS NO. 5250.
- F. FOR PIPES OVER 18" REFER TO ODOT CATCH BASIN 2-3 AND 2-4. FOR SIDE INLETS REFER TO ODOT CATCH BASIN 2-2A.
- G. CARE SHALL BE TAKEN WHEN CONNECTING TO AN EXISTING CATCH BASIN TO KEEP OPENING AS MINIMAL AS POSSIBLE. IF POSSIBLE, SAWCUT OR USE ROTARY HAMMER FOR OPENING TO MINIMIZE DAMAGE TO CATCH BASIN. PIPE TO INTRUDE INTO CATCH BASIN 1" ONLY AND PIPE MUST BE CUT PARALLEL TO CATCH BASIN. USE NONSHRINK GROUT AROUND PIPE TO SEAL BETWEEN PIPE AND CATCH BASIN.

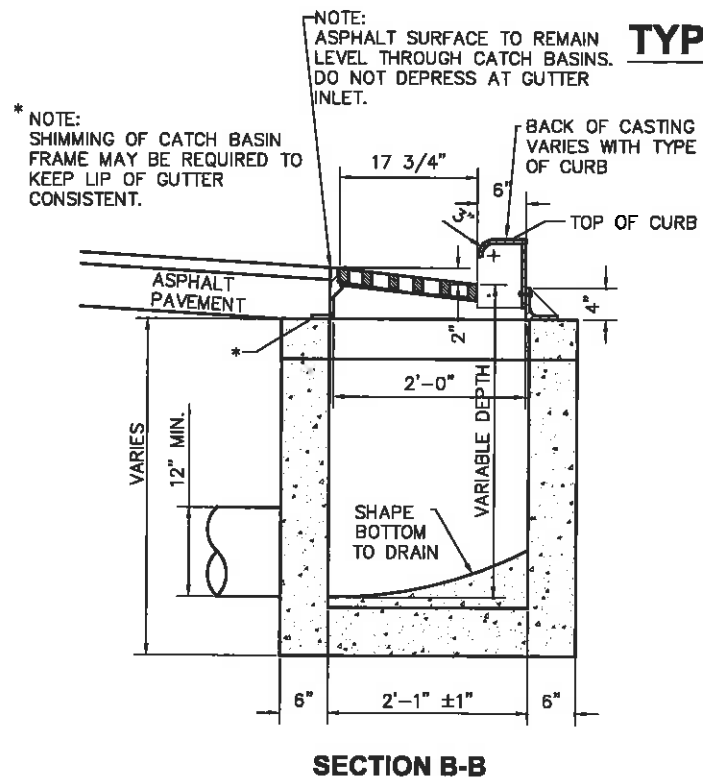


TYPE 2-2-B CATCH BASIN



**TYPE 1 CATCH BASIN
CATCH BASIN NOTES**

- A. CASTING SHALL BE EAST JORDAN 7030 OR NEENAH R-3246 OR EQUIVALENT (EAST JORDAN 7031 FOR TYPE 1A).
- B. CATCH BASIN IN DRIVE APPROACHES (TO BE AVOIDED, IF POSSIBLE) THE BACKS SHALL BE EAST JORDAN TYPE T3 OR NEENAH (R-3246-A) WITH CURB PLATE.
- C. STANDARD GRATE SHALL BE EAST JORDAN TYPE M2, NEENAH TYPE C, OR EQUIVALENT. ALL BAR EDGES TO BE ROUNDED 1/8" RADIUS.
- D. CONCRETE, CAST-IN-PLACE, TO BE ODOT QC MISC. (NO POZZOLAN MATERIAL) KNOCKOUTS MAY BE PROVIDED IN PRECAST CONSTRUCTION. PRECAST WALLS SHALL HAVE A SUFFICIENT AMOUNT OF REINFORCEMENT TO PERMIT SHIPPING AND PLACEMENT WITHOUT DAMAGE.
- E. CARE SHALL BE TAKEN WHEN CONNECTING TO AN EXISTING CATCH BASIN TO KEEP OPENING AS MINIMAL AS POSSIBLE. IF POSSIBLE, SAWCUT OR USE ROTARY HAMMER FOR OPENING TO MINIMIZE DAMAGE TO CATCH BASIN. PIPE TO INTRUDE INTO CATCH BASIN 1" ONLY AND PIPE MUST BE CUT PARALLEL TO CATCH BASIN. USE NONSHRINK GROUT AROUND PIPE TO SEAL BETWEEN PIPE AND CATCH BASIN.
- F. DROP FLOW LINE 1/2" WITHIN BLOCK OUT OF COMBINED CURB AND GUTTER WHILE KEEPING LIP OF GUTTER CONSISTENT WITH TOP OF CURB.

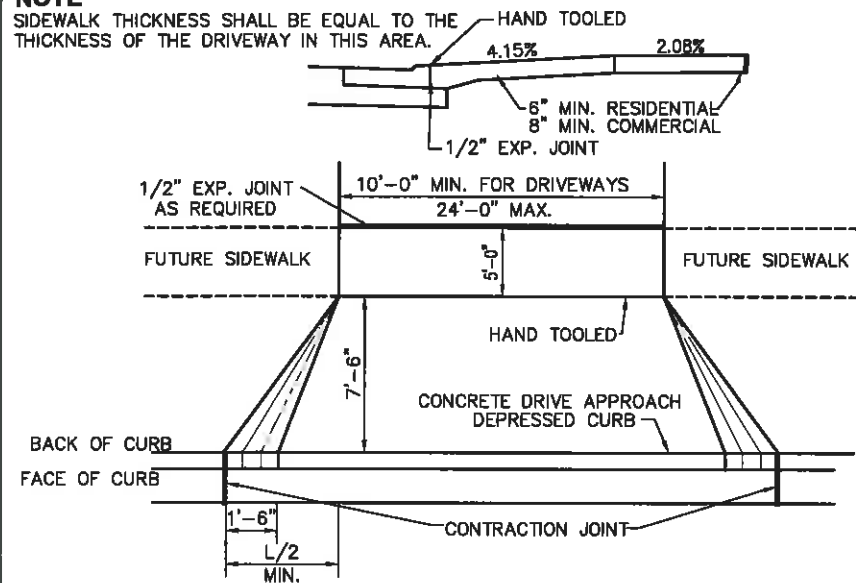


SECTION B-B

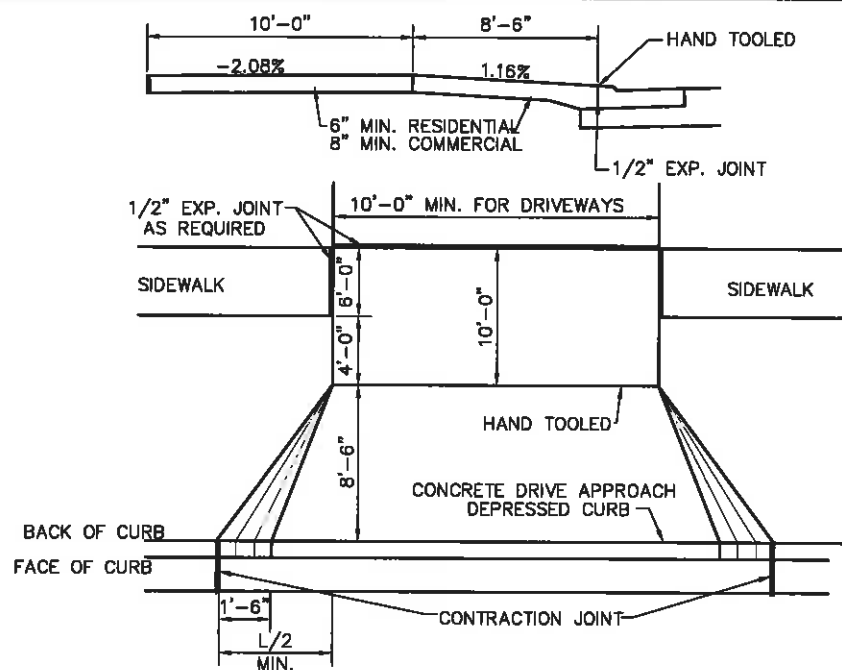
REVISIONS:
FILE NAME MIATCH1406GMA
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATCH1406
DATE 12-19-2016
SHEET NUMBER 17 OF 52

NOTE

SIDEWALK THICKNESS SHALL BE EQUAL TO THE THICKNESS OF THE DRIVEWAY IN THIS AREA.



FOR ALL RIGHT SIDE DRIVES



FOR ALL LEFT SIDE DRIVES

PAVEMENT SYMBOL LEGEND

CURB RAMP:
ITEM 608, CURB RAMPS, AS PER PLAN
ITEM 304, 4" AGGREGATE BASE, AS PER PLAN
ITEM 204, SUBGRADE COMPACTION, AS PER PLAN

RESIDENTIAL CONCRETE DRIVE:
ITEM 452, 6" NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN ON
ITEM 304, 6" AGGREGATE BASE, AS PER PLAN
ITEM 204 SUBGRADE COMPACTION, AS PER PLAN

RESIDENTIAL GRAVEL DRIVE:
ITEM 411, 4" STABILIZED CRUSHED AGGREGATE (LIMESTONE OR CRUSHED GRAVEL ONLY) ON
ITEM 304, 4" AGGREGATE BASE, AS PER PLAN
ITEM 204 SUBGRADE COMPACTION, AS PER PLAN

CONCRETE SIDEWALK:
ITEM 608, 4" CONCRETE SIDEWALK ON
ITEM 304, 4" AGGREGATE BASE, AS PER PLAN.

MANHOLE, CATCH BASIN, & VALVE ADJUSTED OR RECONSTRUCTED TO GRADE
(TBA) TO BE ABANDONED
(TBR) TO BE REMOVED
(ATG) ADJUSTED TO GRADE

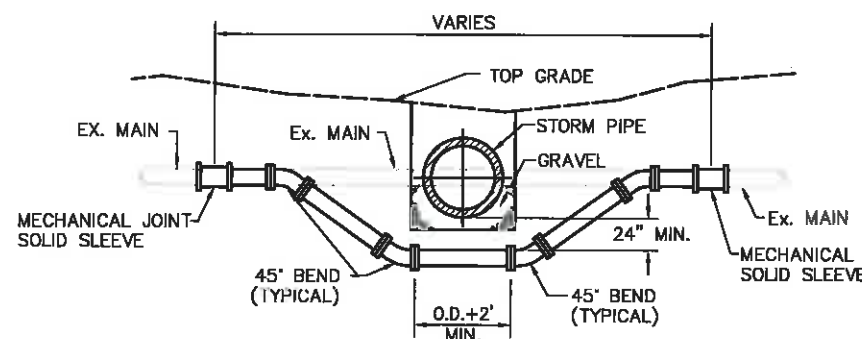
FULL DEPTH PAVEMENT REPLACEMENT:
SEE TYPICAL SECTION

GRIND AND OVERLAY AREAS:
ITEM 442, 1-1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (448) ON
ITEM 407, TACK COAT 0.06 GAL./S.Y.
ITEM 442, 1-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (448), ON
ITEM 407, TACK COAT 0.08 GAL./S.Y.
ITEM 254, 3"± PAVEMENT PLANING, ASPHALT CONCRETE

GRIND AND OVERLAY AREAS:
ITEM 442, 1-1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (448) ON
ITEM 407, TACK COAT 0.08 GAL./S.Y.
ITEM 254, 1-1/2"± PAVEMENT PLANING, ASPHALT CONCRETE

COMMERCIAL ASPHALT DRIVE:
ITEM 441, 3" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448) PG84-22 (2 EQUAL LIFTS) ON
ITEM 301, 4" ASPHALT CONCRETE BASE, PG84-22
ITEM 304, 6" AGGREGATE BASE, AS PER PLAN
ITEM 204 SUBGRADE COMPACTION, AS PER PLAN

COMMERCIAL CONCRETE DRIVE:
ITEM 452, 8" NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN ON
ITEM 304, 6" AGGREGATE BASE, AS PER PLAN
ITEM 204 SUBGRADE COMPACTION, AS PER PLAN
(COST INCLUDED IN ITEM 452 NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN)

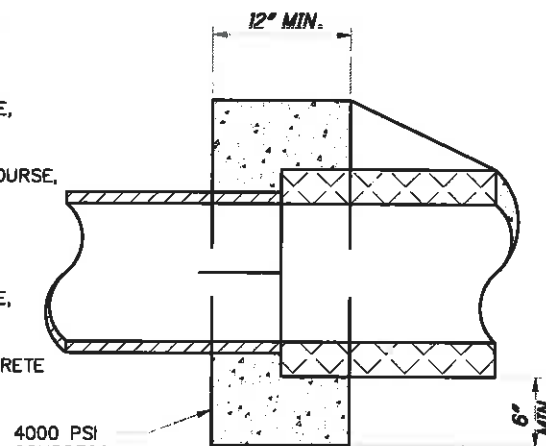


**WATER MAIN RELOCATED
GENERAL DETAIL**

NOTES

- A. DRIVE APPROACHES SHALL MEET THE REQUIREMENTS OF ODOT ITEM 452, 499, 608, AND 609 CAST IN-PLACE CONCRETE.
- B. DRIVE APPROACHES MAY BE PLACED MONOLITHICALLY WITH CURB
- C. MAXIMUM JOINT SPACING SHALL BE 10' LONGITUDINALLY, TRANSVERSELY AND AT TAPERS.
- D. EXPANSION MATERIAL SHALL BE 1/2" PREMOLDED, ODOT APPROVED.
- E. 6" OF COMPACTED ODOT ITEM 304 AGGREGATE BASE SHALL BE PLACED UNDER DRIVE APPROACHES. COST INCLUDED WITH ITEM 452, NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN
- F. PROVIDE BROOM FINISH AND EDGING TO ALL EXPOSED SURFACES.

- G. WHERE ASPHALTIC CONCRETE PAVEMENT IS DISTURBED, THE ASPHALT SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE CITY.
- H. JOINTS SHALL BE CLEANED AND EDGED BY A 1/4" RADIUS EDGER. LONGITUDINAL JOINTS SHALL BE AS DIRECTED BY THE CITY. EXPANSION JOINTS SHALL BE OF SUCH DIMENSIONS AS SHOWN ON STANDARD DRAWINGS FOR CONSTRUCTION JOINTS.
- I. CONCRETE SHALL BE ODOT QC MISC. (CEMENT ONLY - NO POZZOLAN MATERIAL) AND REINFORCED WITH CONCRETE FIBERS.
- J. IF CURB IS REMOVED AND REPLACED DURING DRIVEWAY CONSTRUCTION, JOINTS BETWEEN EXISTING AND NEW CURB ARE TO BE DOWELLED WITH #4 REBAR 6" INTO CURB AND GUTTER 18" O.C.



MASONRY COLLAR

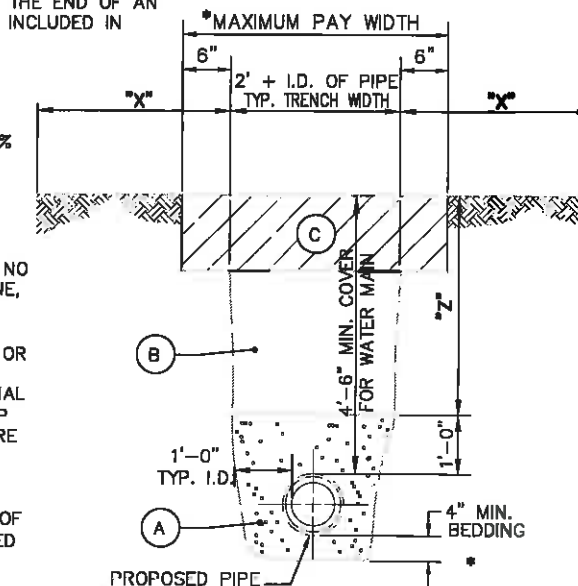
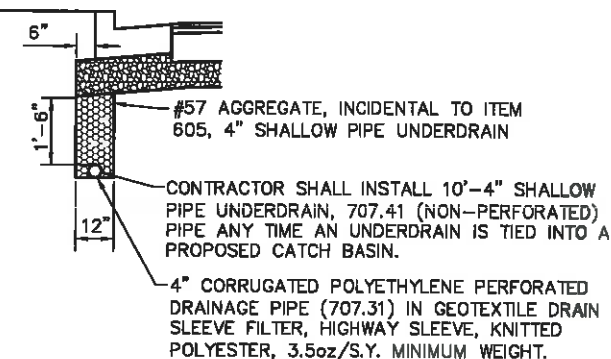
MASONRY COLLARS: PROVIDE A MASONRY COLLAR WHERE PLANS REQUIRE THAT A PIPE EXTENSION BE JOINED TO THE END OF AN EXISTING PIPE WITH A BUTT JOINT. THE COST IS INCLUDED IN THE UNIT PRICE BID FOR THE NEW CONDUIT.

TRENCH DETAIL NOTES

- A. STORM BEDDING SHALL BE SAND OR 310, SANITARY #8'S (60% CRUSHED MIN.), WATER: SAND, 310 MATERIAL, OR #8'S.
- B. ALL TRENCHES WHERE "X" IS GREATER THAN "Z" FOR PROPOSED OR EXISTING PAVEMENT, CURB, DRIVEWAYS, ALLEYS, STONE AREA OR WALKS CAN BE COMPACTED EXISTING NATIVE MATERIAL IN 12" MAXIMUM LIFTS OR AS APPROVED BY THE CITY. NO MATERIAL SHALL BE USED FOR BACKFILLING THAT CONTAINS STONE, ROCKS, ETC., GREATER THAN 4" DIAMETER.
- C. OFF-PAVEMENT AREAS SHALL BE PROVIDED WITH A MINIMUM OF 6" OF TOPSOIL OVER THE COMPACTED MATERIAL AND THEN SEEDED AND MULCHED PER ODOT ITEM 659.
- D. THE OPEN ENDS OF ALL PIPES SHALL BE PLUGGED TO THE APPROVAL OF THE CITY BEFORE LEAVING THE WORK FOR THE NIGHT.

IN-PAVEMENT AREAS SHALL FOLLOW TYPICAL PAVEMENT DETAILS SHOWN ON THE TYPICAL SECTION.

4" BASE PIPE UNDERDRAIN DETAIL

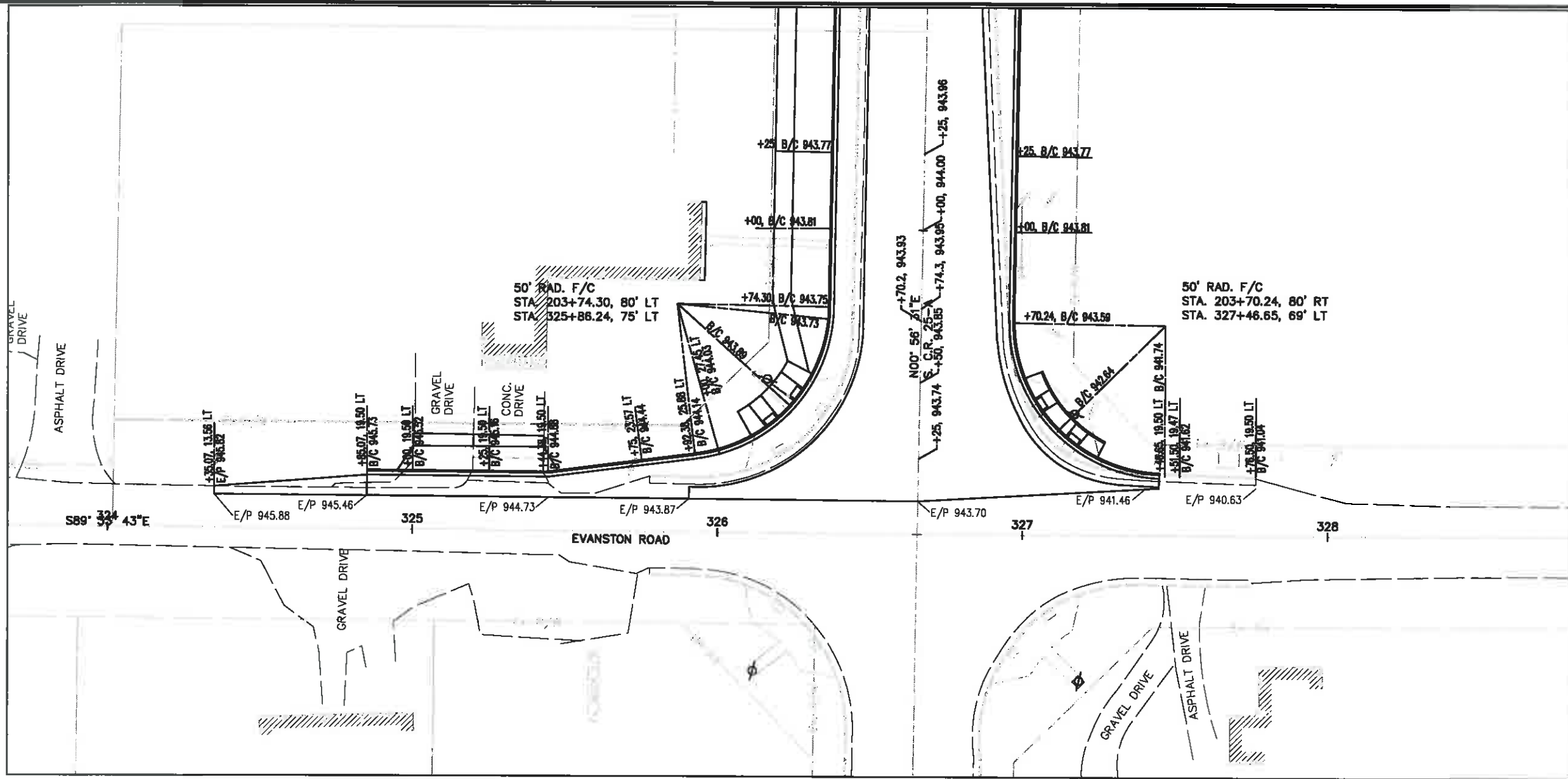


TRENCH DETAIL

NOTE: MAXIMUM PAY LIMITS, ANY PAVEMENT REPAIR BEYOND THE MAXIMUM PAY LIMITS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

REVISIONS:

FILE NAME	MIATCH1406GMB
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	18 OF 52

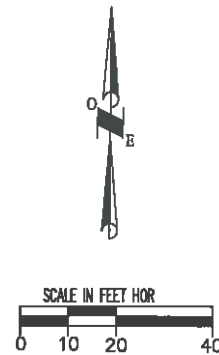


INTERSECTION OF C.R. 25-A AND EVANSTON ROAD

LEGEND

ALL ELEVATIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED

- Ex. B/C - Ex. BACK OF CURB
- Ex. F/L - Ex. FLOWLINE OF GUTTER
- F/C - FACE OF CURB
- E/P - EDGE OF PAVEMENT
- B/C - PROP. BACK OF CURB
- ℄ - Ex. CENTERLINE OF ROAD



REVISIONS:

FILE NAME
 MATCH1406INTA

DRAWN BY
 BHB

CHECKED BY
 JSP

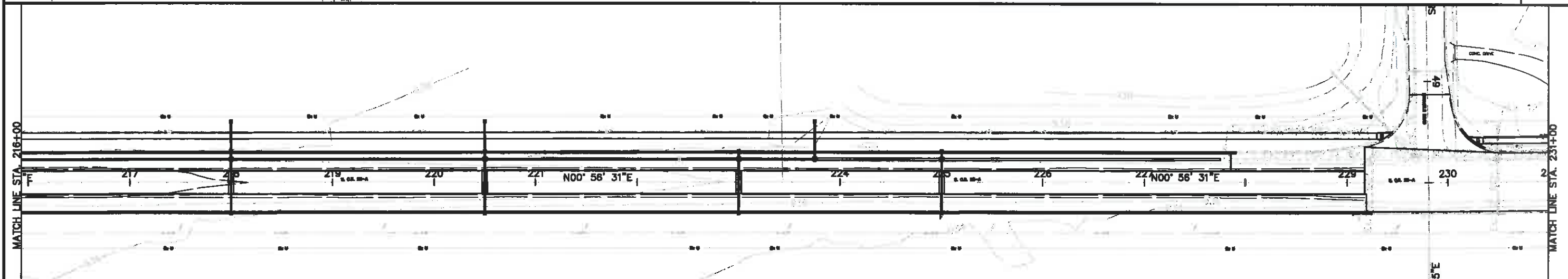
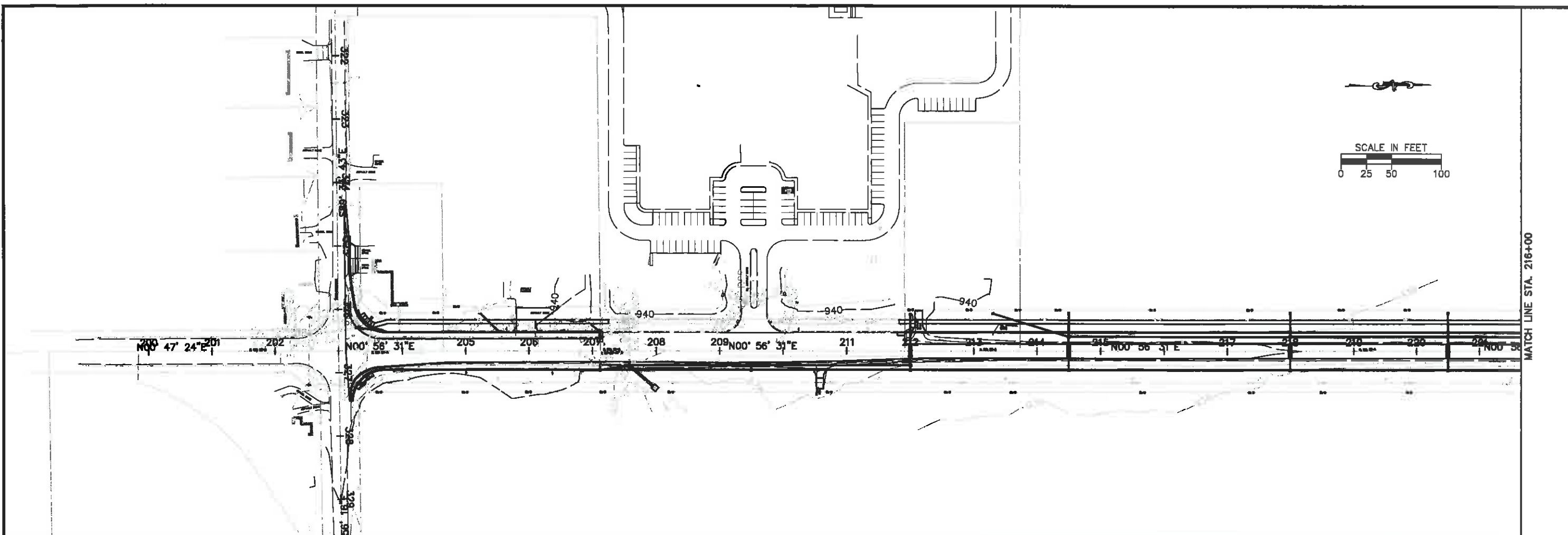
PROJECT No.
 MATCH1406

DATE
 12-19-2016

SHEET NUMBER

REVISIONS:

FILE NAME	SITE PLAN
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	20 OF 52



PROJECT DATA	
TOTAL AREA (RIGHT-OF-WAY):	6.11 ACRES
PROJECT EARTH DISTURBED AREA:	5.39 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.50 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	5.89 ACRES
IMPERVIOUS (PAVED) AREA FOR PRE-CONSTRUCTION SITE:	2.31 ACRES
IMPERVIOUS (PAVED) AREA FOR POST CONSTRUCTION SITE:	4.16 ACRES
RUNOFF COEFFICIENT FOR PRE-CONSTRUCTION SITE:	0.53
RUNOFF COEFFICIENT FOR POST CONSTRUCTION SITE:	0.69
IMMEDIATE RECEIVING WATERS:	CITY STORM SEWER
SUBSEQUENT RECEIVING WATER:	GREAT MIAMI RIVER

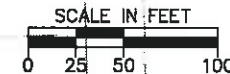
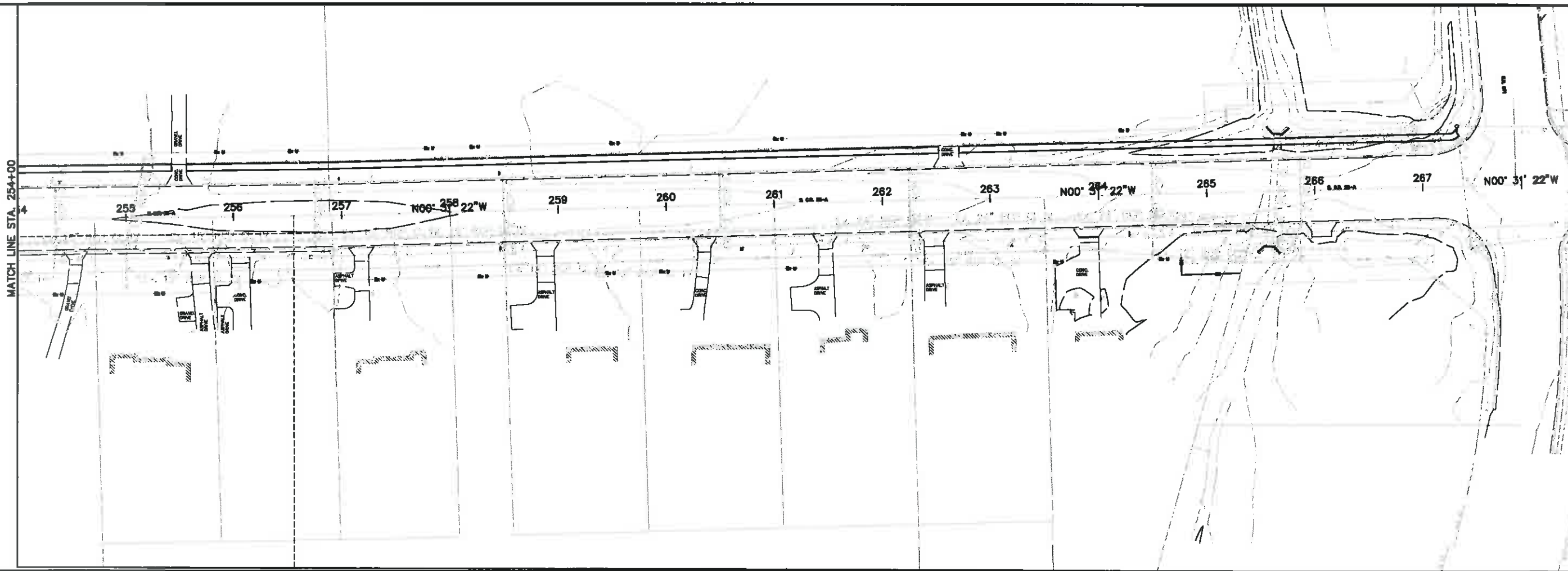
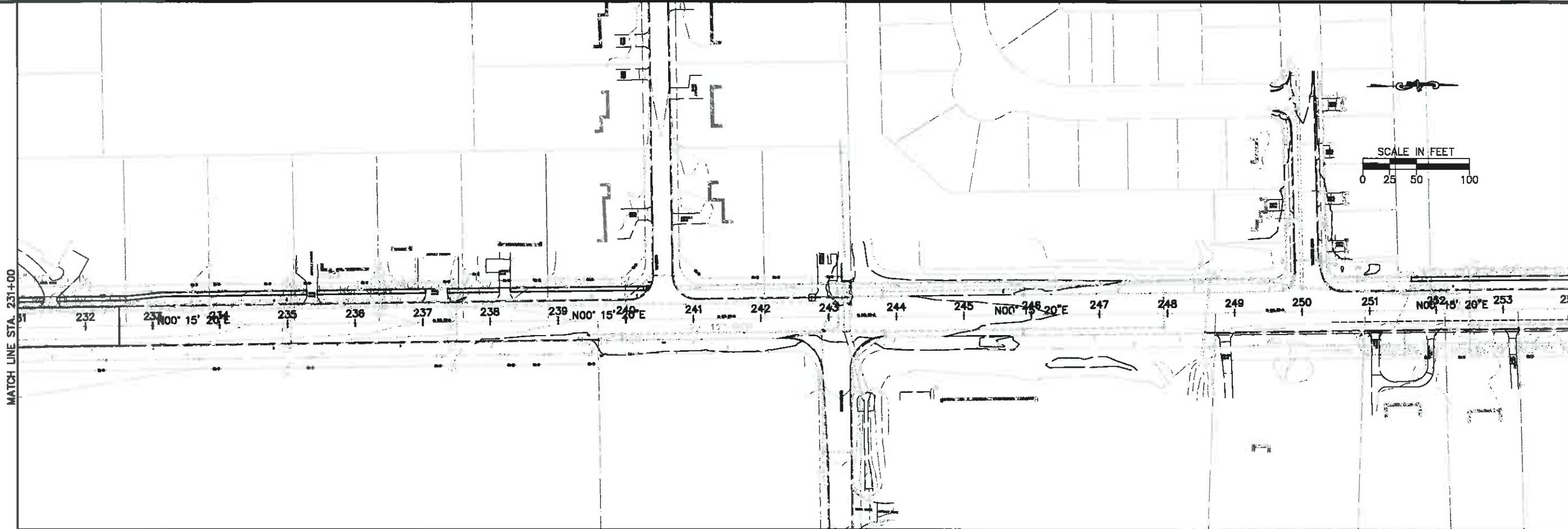
LEGEND

- CATCH BASIN
- MANHOLE

PROJECT DESCRIPTION

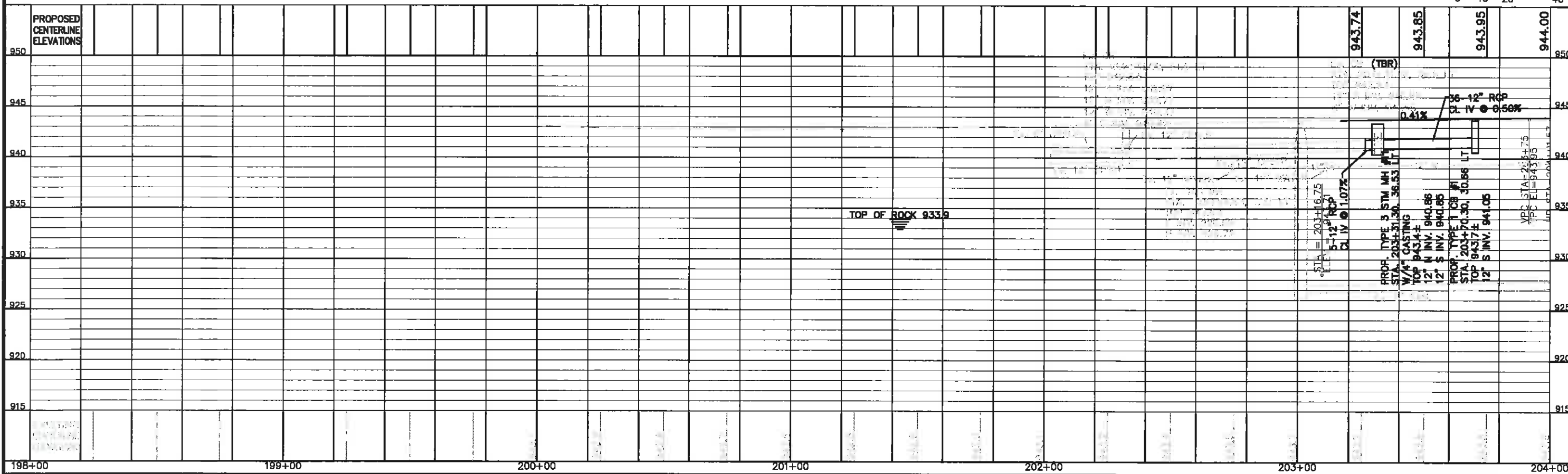
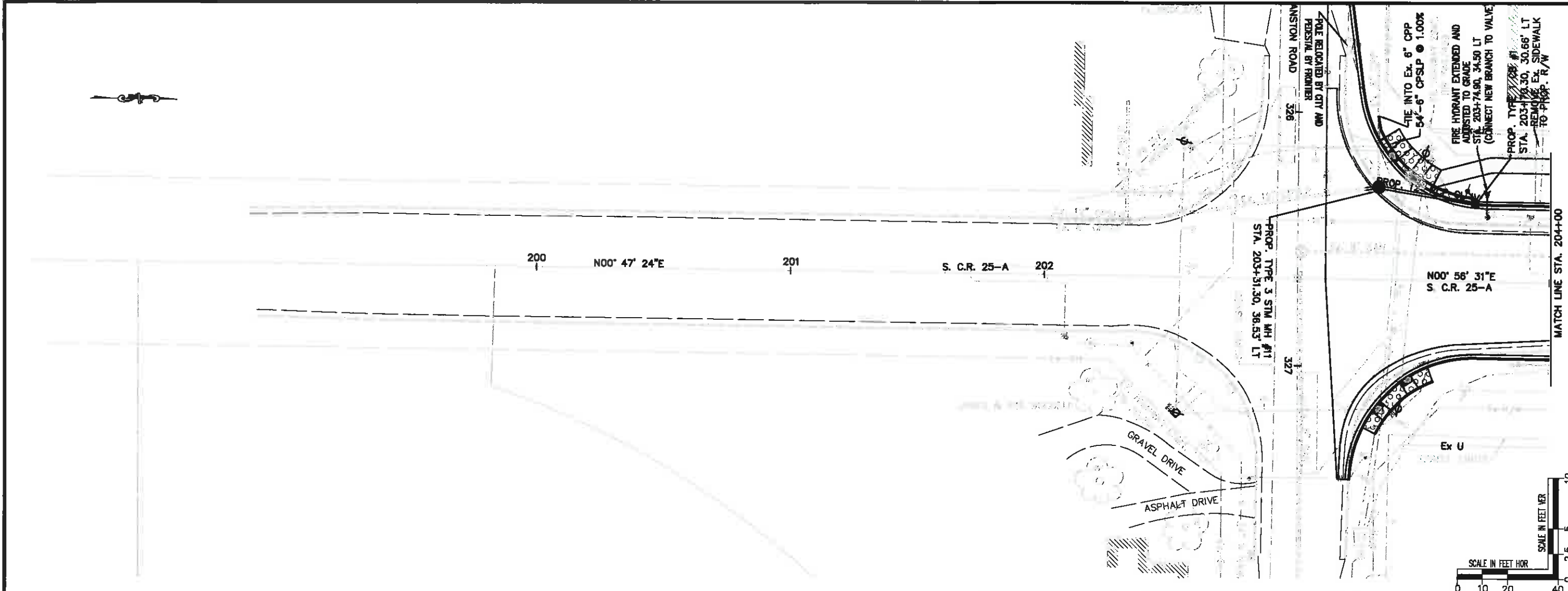
THIS PROJECT CONSISTS OF THE RECONSTRUCTION OF C.R. 25-A FROM MICHAELS RD. TO EVANSTON RD. CONSTRUCTION CONSISTS OF PAVEMENT, CURBING, THE INSTALLATION OF A NEW STORM SEWER SYSTEM, UTILITY RELOCATIONS, STREET LIGHTS AND RESTORATION OF TRAFFIC SIGNAGE. ALSO SIDEWALK WILL BE INSTALLED ALONG THE WEST ONLY FROM MICHAELS RD. TO MAIN ST. (SR 571).

USGS TIPP CITY QUADRANGLE
 LATITUDE: N 39°-56'-54"
 LONGITUDE: W 84°-11'-56"



SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
SITE PLAN

REVISIONS:
FILE NAME MIATC1406S1TB
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATC1406
DATE 12-19-2016
SHEET NUMBER 21 OF 52



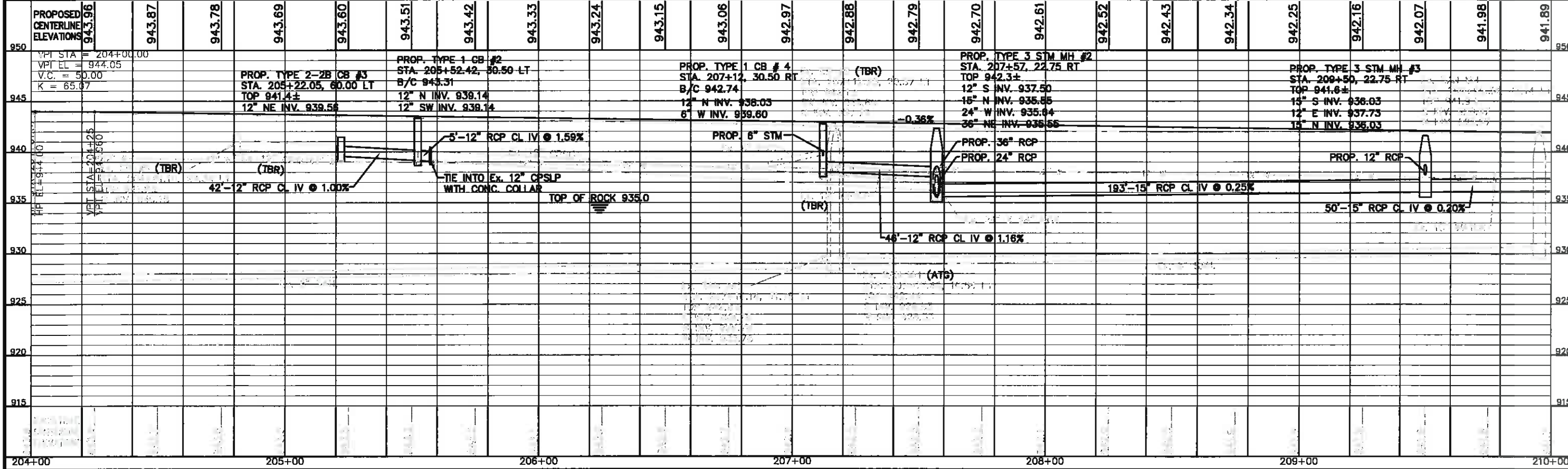
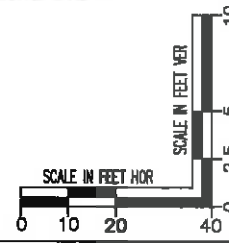
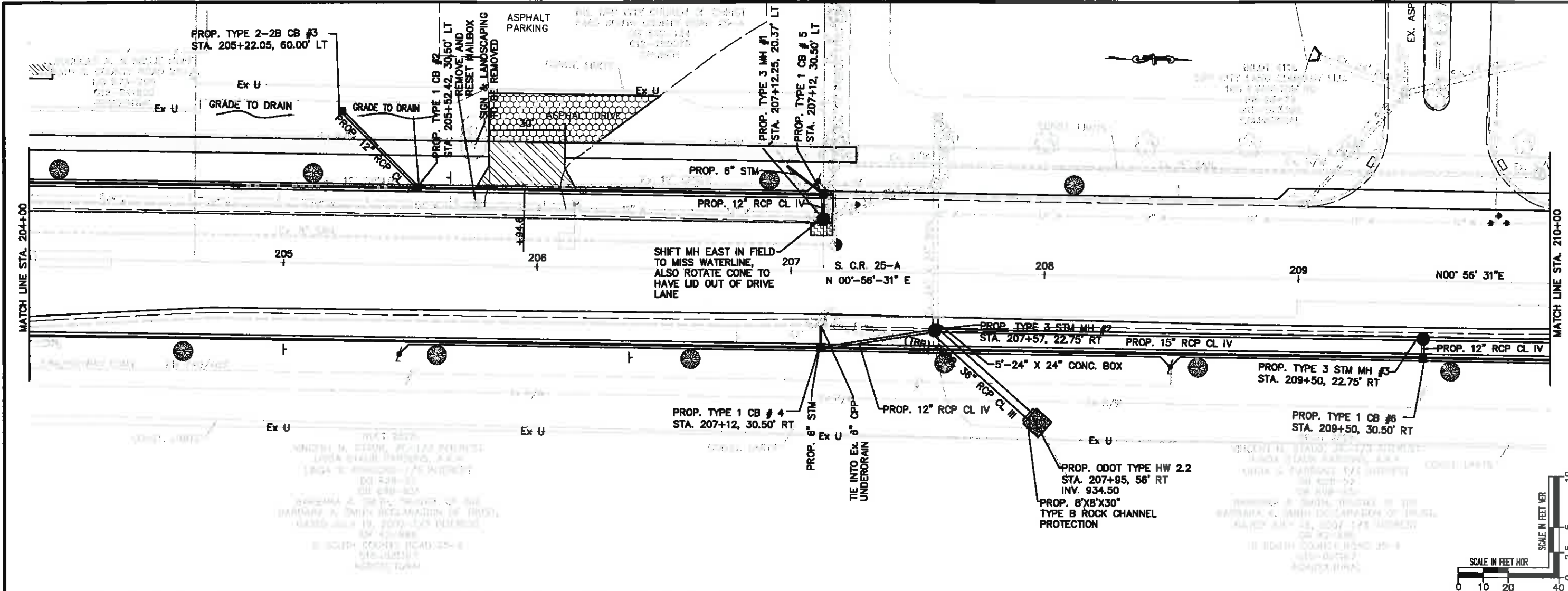
SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A P/P STA. 200+00 TO STA. 204+00

REVISIONS:

FILE NAME	25A (1)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH406
DATE	12-19-2016
SHEET NUMBER	22 OF 52

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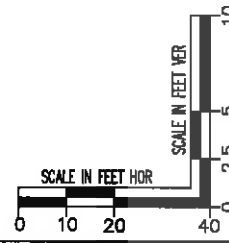
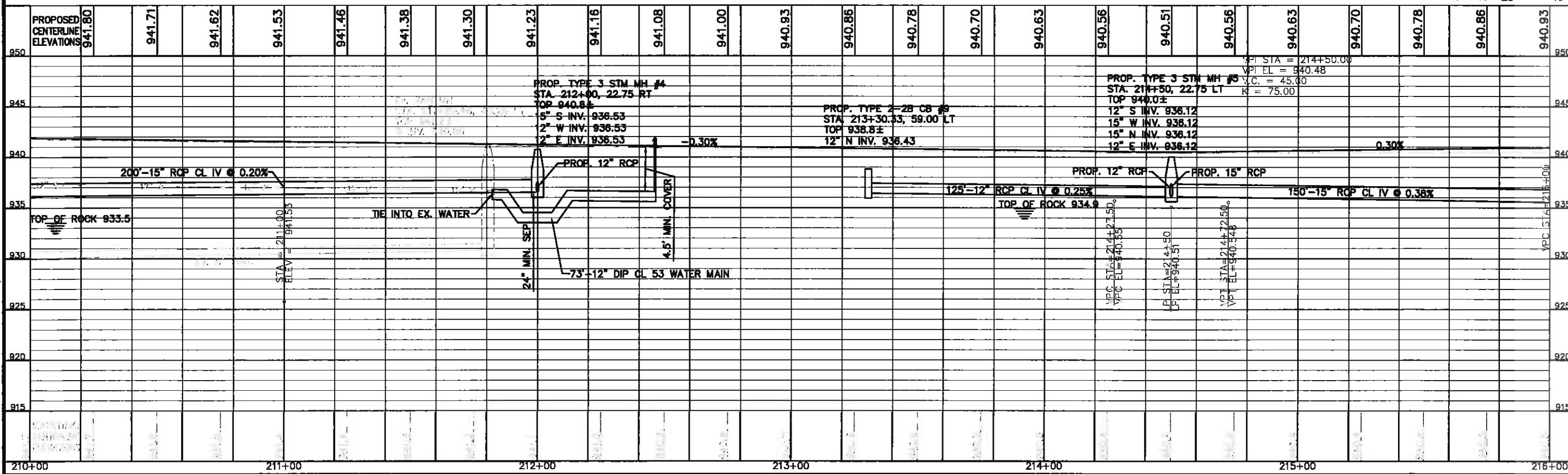
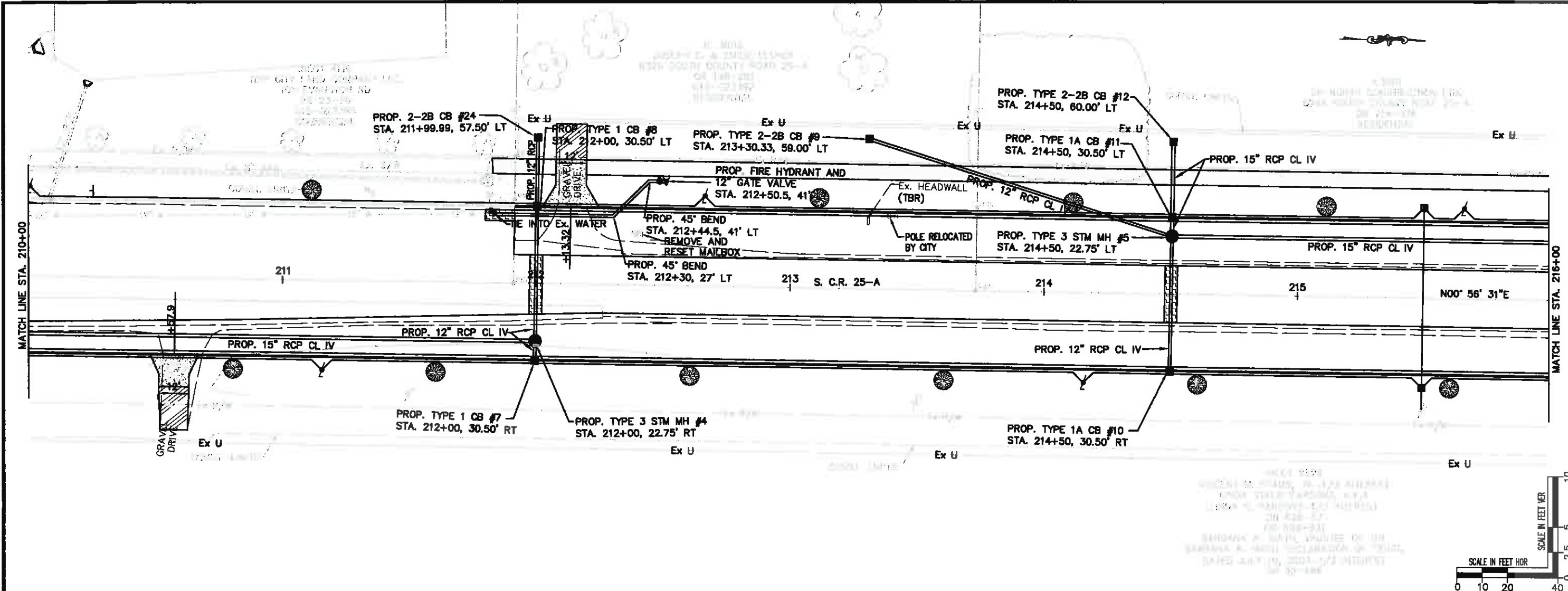
SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A P/P STA. 204+00 TO STA. 210+00



REVISIONS:

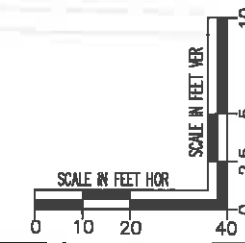
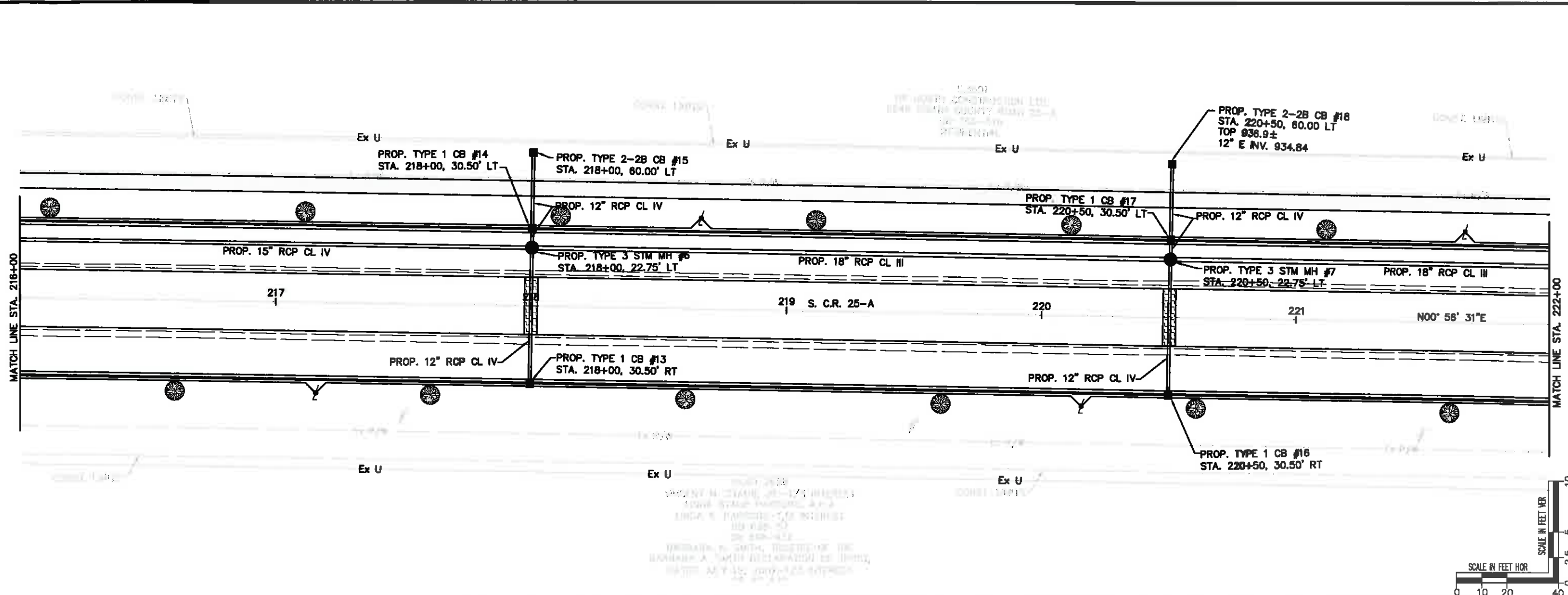
FILE NAME	25A (2)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	23 OF 52

SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A PIP STA 210+00 TO STA. 216+00



REVISIONS:

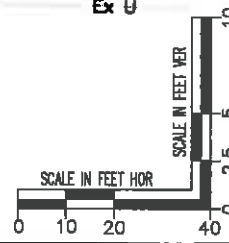
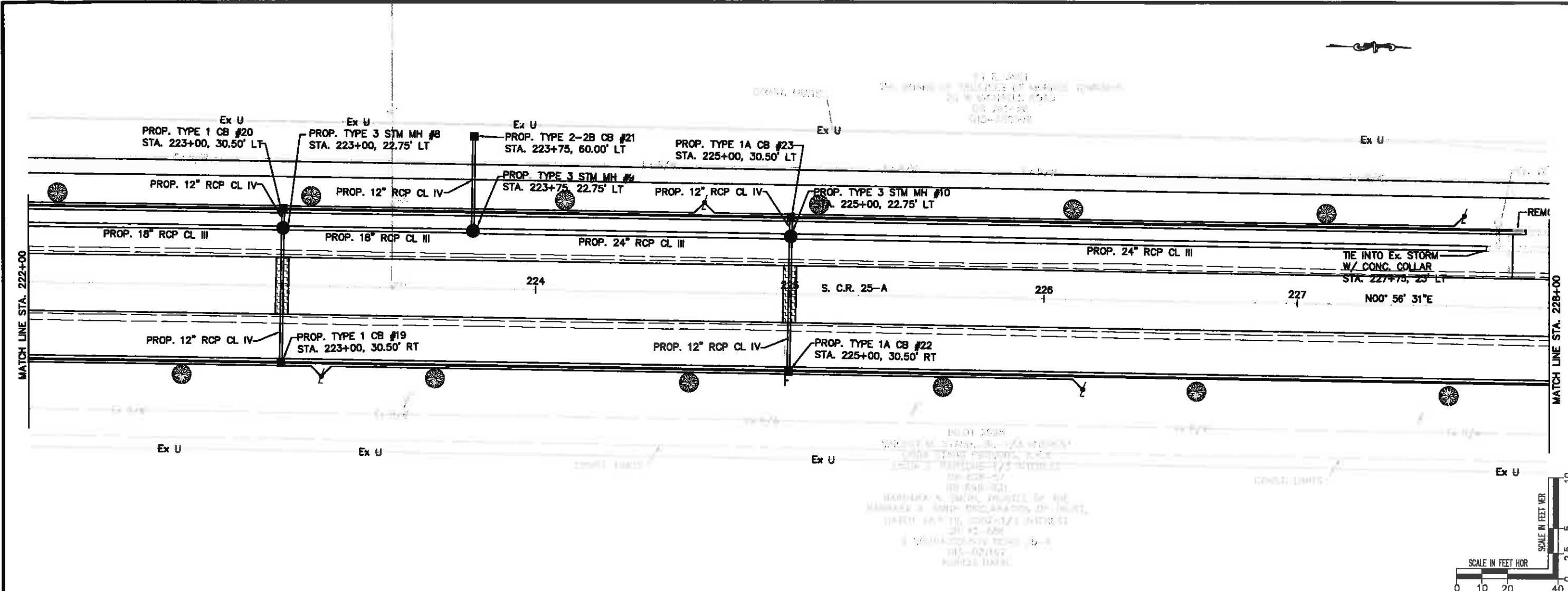
FILE NAME	25A (3)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	24 OF 52



PROPOSED CENTERLINE ELEVATIONS	940.96	940.89	940.78	940.67	940.55	940.44	940.33	940.22	940.10	939.99	939.88	939.76	939.65	939.54	939.43	939.31	939.20	939.09	938.97	938.86	938.75	938.64	938.52	938.41	
950																									
945	YPI STA = 216+25.00 YPI EL = 941.00 Y.C. = 50.00 K = 66.55																								
940																									
935																									
930																									
925																									
920																									
915																									
216+00	217+00	218+00	219+00	220+00	221+00	222+00																			

REVISIONS:

FILE NAME	25A (4)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH406
DATE	12-19-2016
SHEET NUMBER	25 OF 52



PROPOSED CENTERLINE ELEVATIONS	938.33	938.24	938.16	938.08	938.00	937.92	937.83	937.75	937.67	937.58	937.50	937.46	937.50	937.58	937.65	937.73	937.81	937.88	937.96	938.01	937.95	937.86	937.77	937.68	
ELEV.	938.41																								938.00
PROP. TYPE 3 STM MH #8 STA. 223+00, 22.75' LT TOP 937.6± 18" S INV. 933.09 12" W INV. 933.41 12" E INV. 933.41 18" N INV. 933.09																									
PROP. TYPE 3 STM MH #9 STA. 223+75, 22.75' LT TOP 937.4± 18" S INV. 932.75 24" N INV. 932.75 12" W INV. 932.75																									
PROP. TYPE 3 STM MH #10 STA. 225+00, 22.75' LT TOP 937.0± 24" S INV. 932.50 12" W INV. 932.78 12" E INV. 932.78 24" N INV. 932.50																									
PROP. 12" RCP																									
100'-18" RCP CL III @ 0.35%																									
75'-18" RCP CL III @ 0.45%																									
125'-24" RCP CL III @ 0.26%																									
275'-24" RCP CL III @ 0.30%																									
TIE INTO Ex. STORM STA. 227+75, 23' LT																									
TOP OF ROCK 930.7																									
TOP OF ROCK 928.5																									
VC STA = 225+00.00 VPI EL = 937.42 V.C. = 45.00 K = 70.31																									
VC STA = 227+00.00 VPI EL = 938.04 V.C. = 40.00 K = -59.70																									
VC STA = 224+77.50 VPI EL = 937.46																									
VC STA = 225+22.00 VPI EL = 937.49																									
VC STA = 226+80 VPI EL = 937.98																									
VC STA = 225+98.51 VPI EL = 938.51																									
VC STA = 227+120 VPI EL = 937.965																									

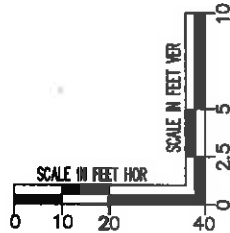
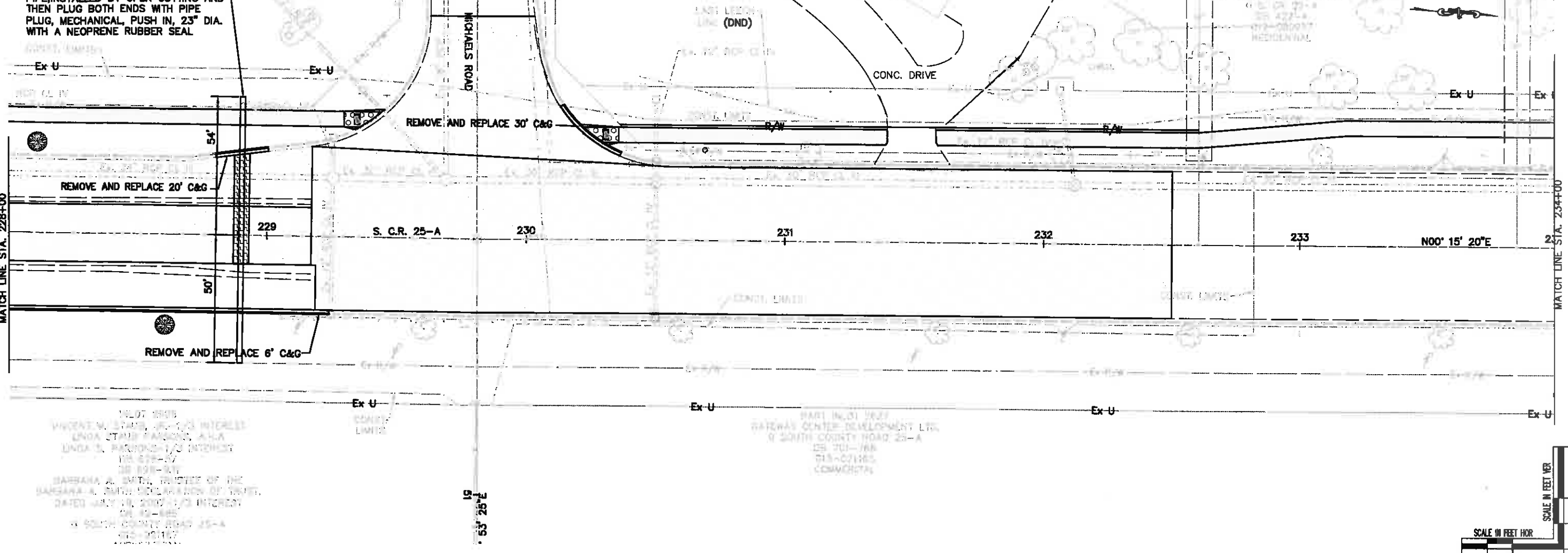
REVISIONS:

FILE NAME	25A (5)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MATCH406
DATE	12-19-2016
SHEET NUMBER	26 OF 52

+90, 104'-24" STEEL CARRIER PIPE, INSTALLED BY OPEN CUTTING AND THEN PLUG BOTH ENDS WITH PIPE PLUG, MECHANICAL, PUSH IN, 23" DIA. WITH A NEOPRENE RUBBER SEAL

MATCH LINE STA. 228+00

MATCH LINE STA. 234+00



ELEVATION	PROPOSED CENTERLINE ELEVATIONS															ELEVATION						
	937.58	937.49	937.39	937.29																		
950																	950					
945																	945					
940																	940					
935																	935					
930																	930					
925																	925					
920																	920					
915																	915					
EXISTING CENTERLINE ELEVATIONS	937.1	937.1	937.0	937.0	937.0	937.0	937.0	937.1	937.1	937.1	937.1	937.1	937.1	937.1	937.0	936.9	936.8	936.8	936.5	936.4	936.3	
	228+00			229+00				230+00				231+00				232+00					233+00	234+00

+90, 95'-24" STEEL CARRIER PIPE, INSTALLED BY OPEN CUTTING AND THEN PLUG BOTH ENDS FOR FUTURE WATERLINE CROSSING

TOP OF ROCK 929.00'

TOP OF ROCK 926.80'

15' MIN. SEPARATION

STA. = 229+19

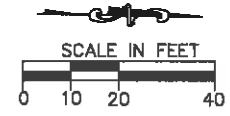
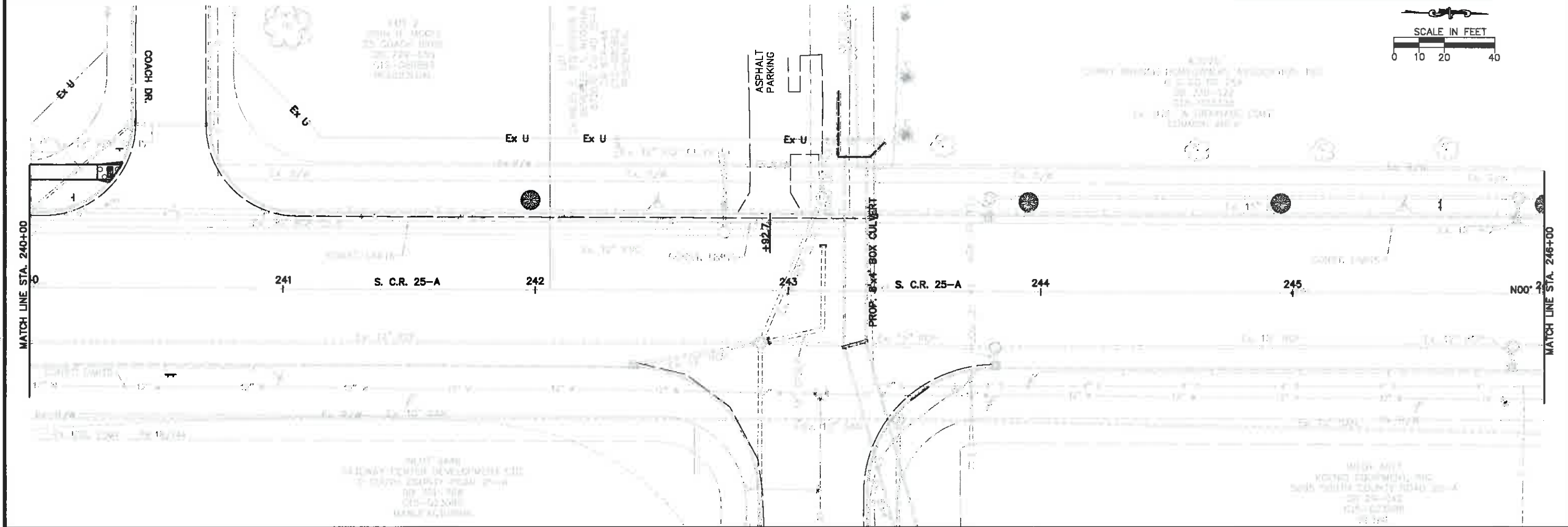
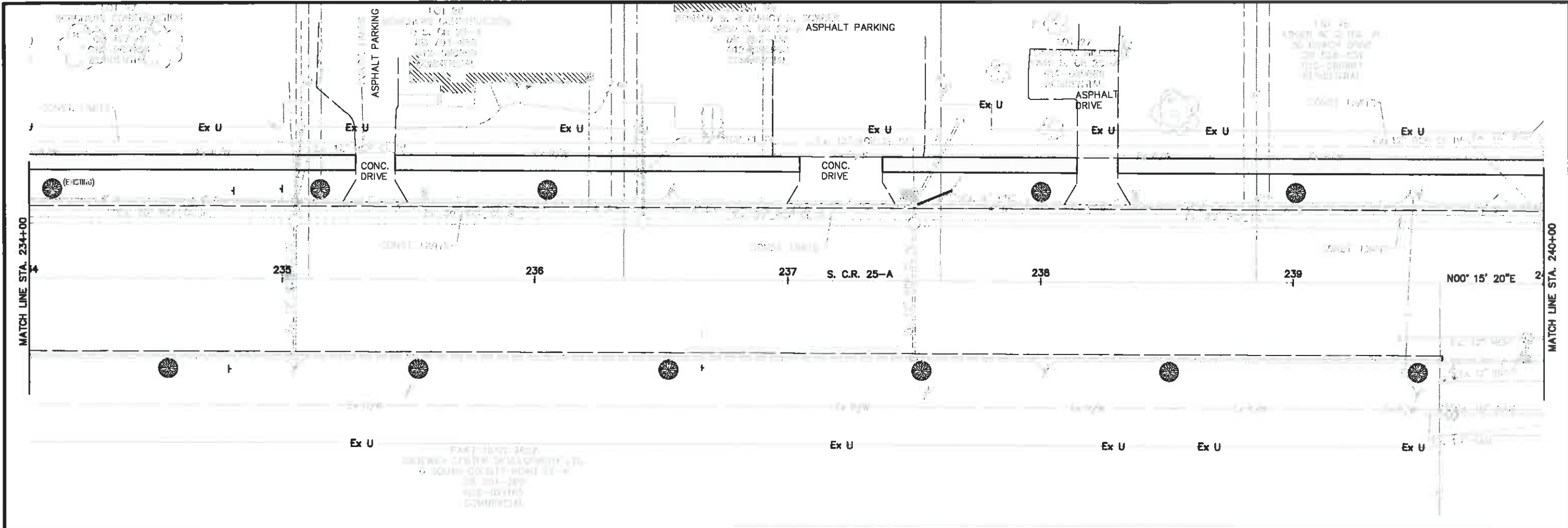
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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A PIP STA. 228+00 TO STA 234+00

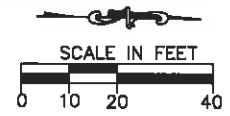
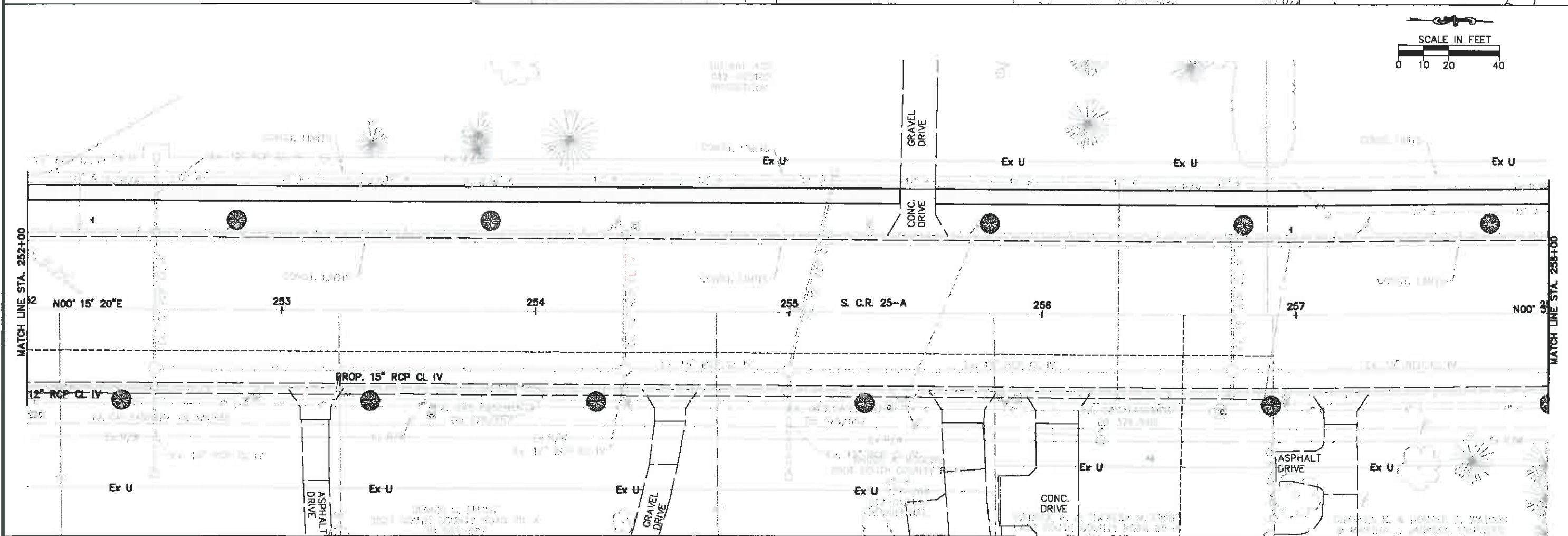
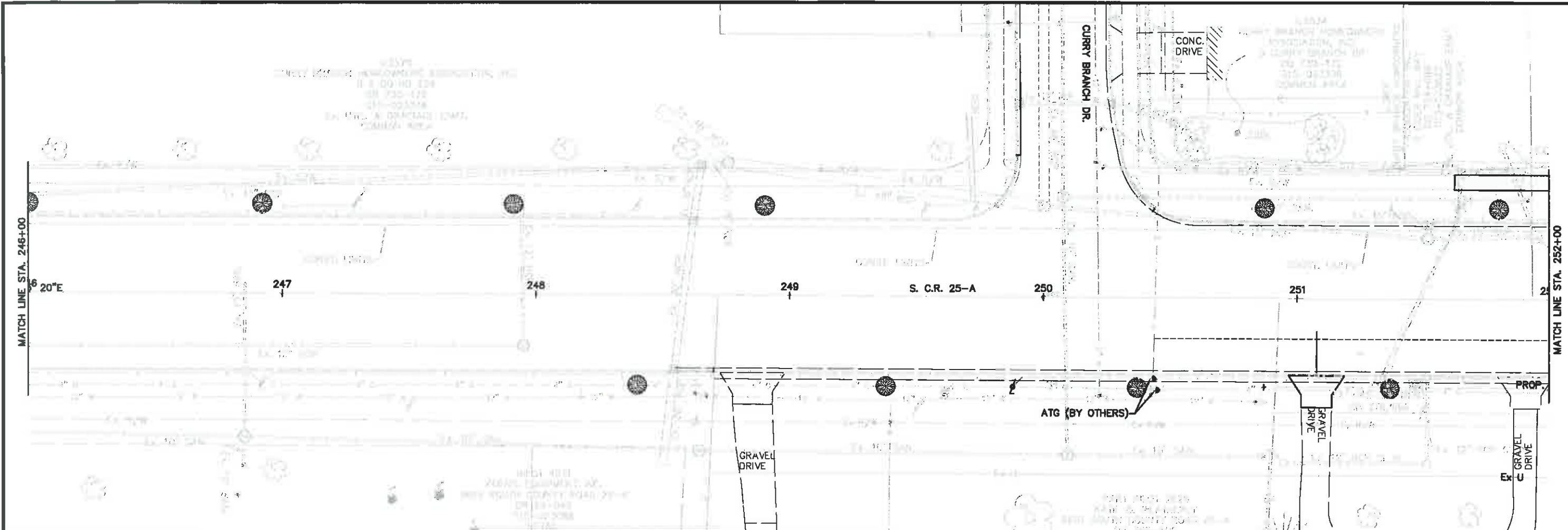
REVISIONS:
08-02-17 CARRIER PIPE

FILE NAME: 25A (6)
DRAWN BY: BHB
CHECKED BY: JSP
PROJECT NO.: MATCH1406
DATE: 12-19-2016
SHEET NUMBER: 27 OF 52



SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A PLAN STA. 234+00 TO STA 246+00

REVISIONS:
FILE NAME 25A (7)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MATCH1406
DATE 12-19-2016
SHEET NUMBER 28 OF 52



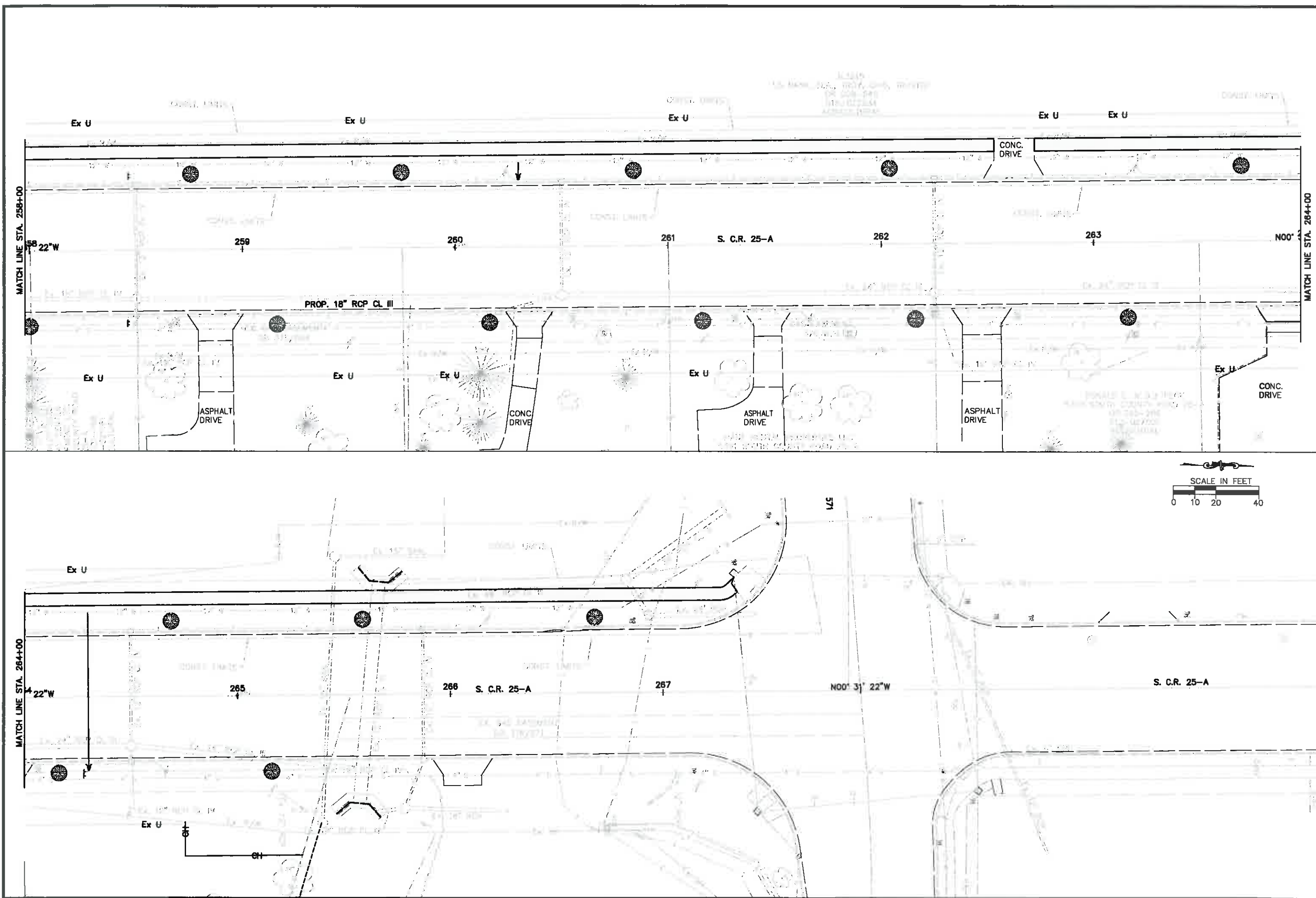
SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A PLAN STA. 246+00 TO STA 258+00

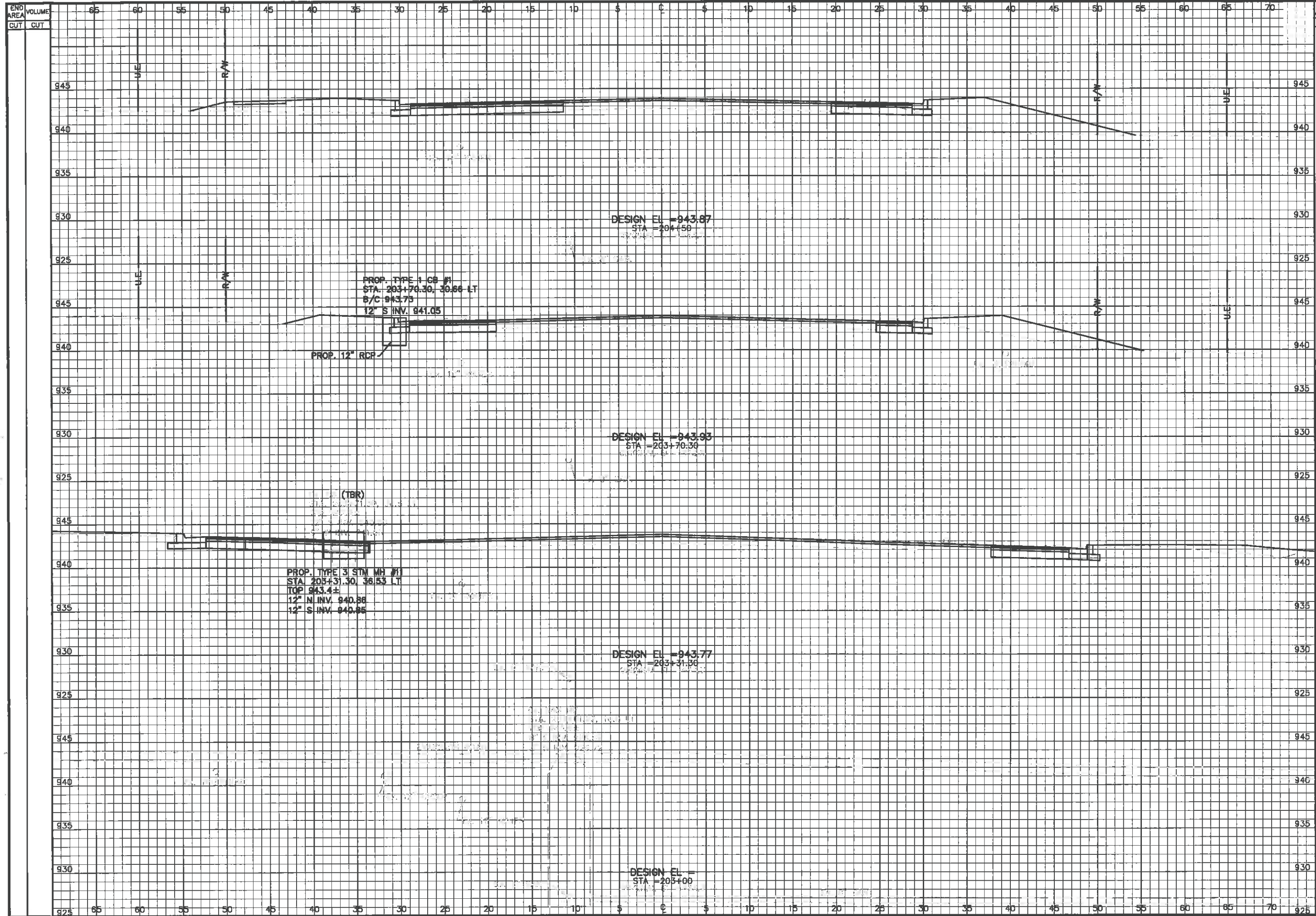
REVISIONS:
FILE NAME 25A (8)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MATCH1406
DATE 12-19-2016
SHEET NUMBER 29 OF 52

SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A PLAN STA. 258+00 TO STA 270+00

REVISIONS:

FILE NAME	25A (9)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MATCH1406
DATE	12-19-2016
SHEET NUMBER	

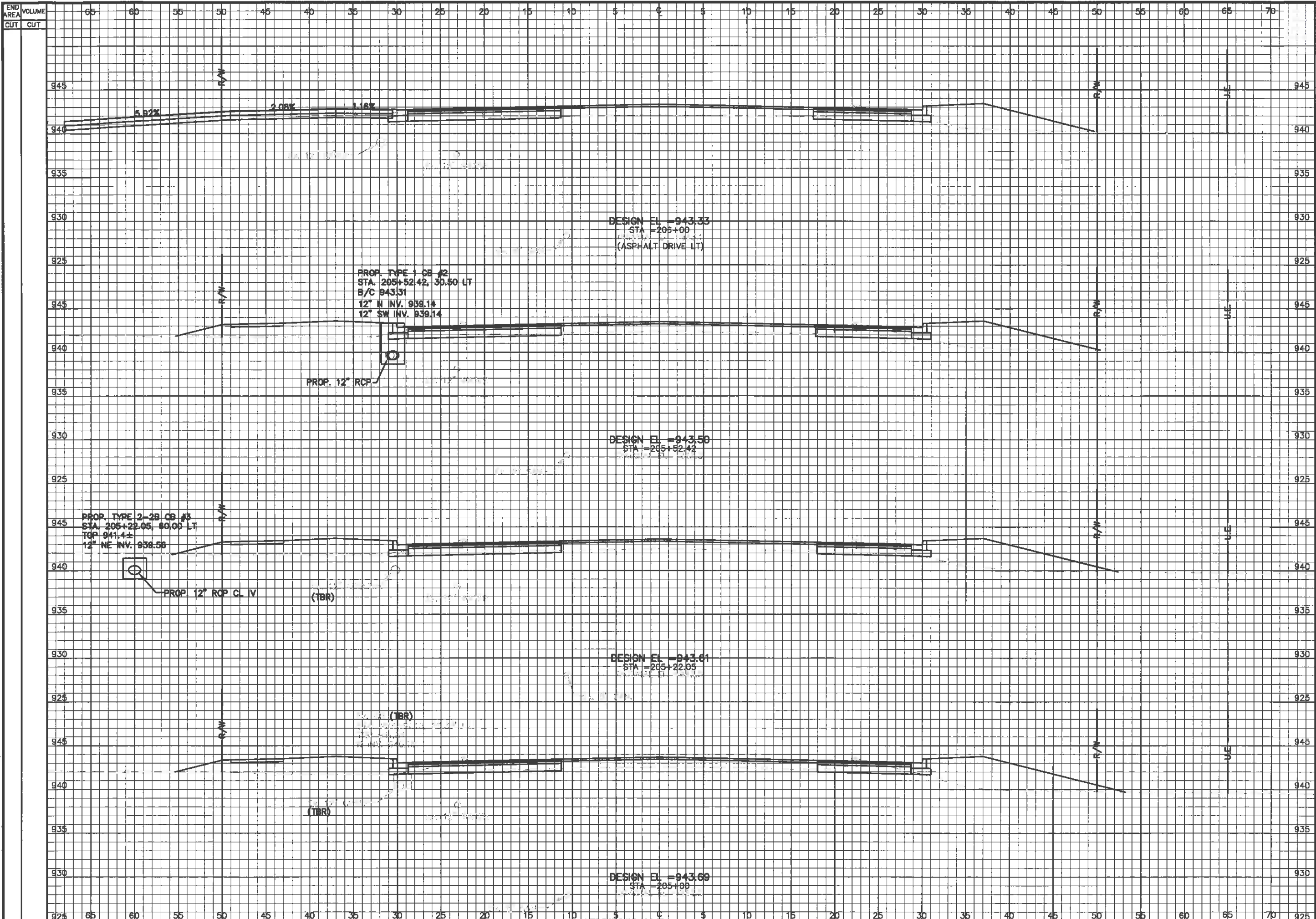




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 PORTLAND, INDIANA 260-766-2500
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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 203+00 TO STA. 204+50

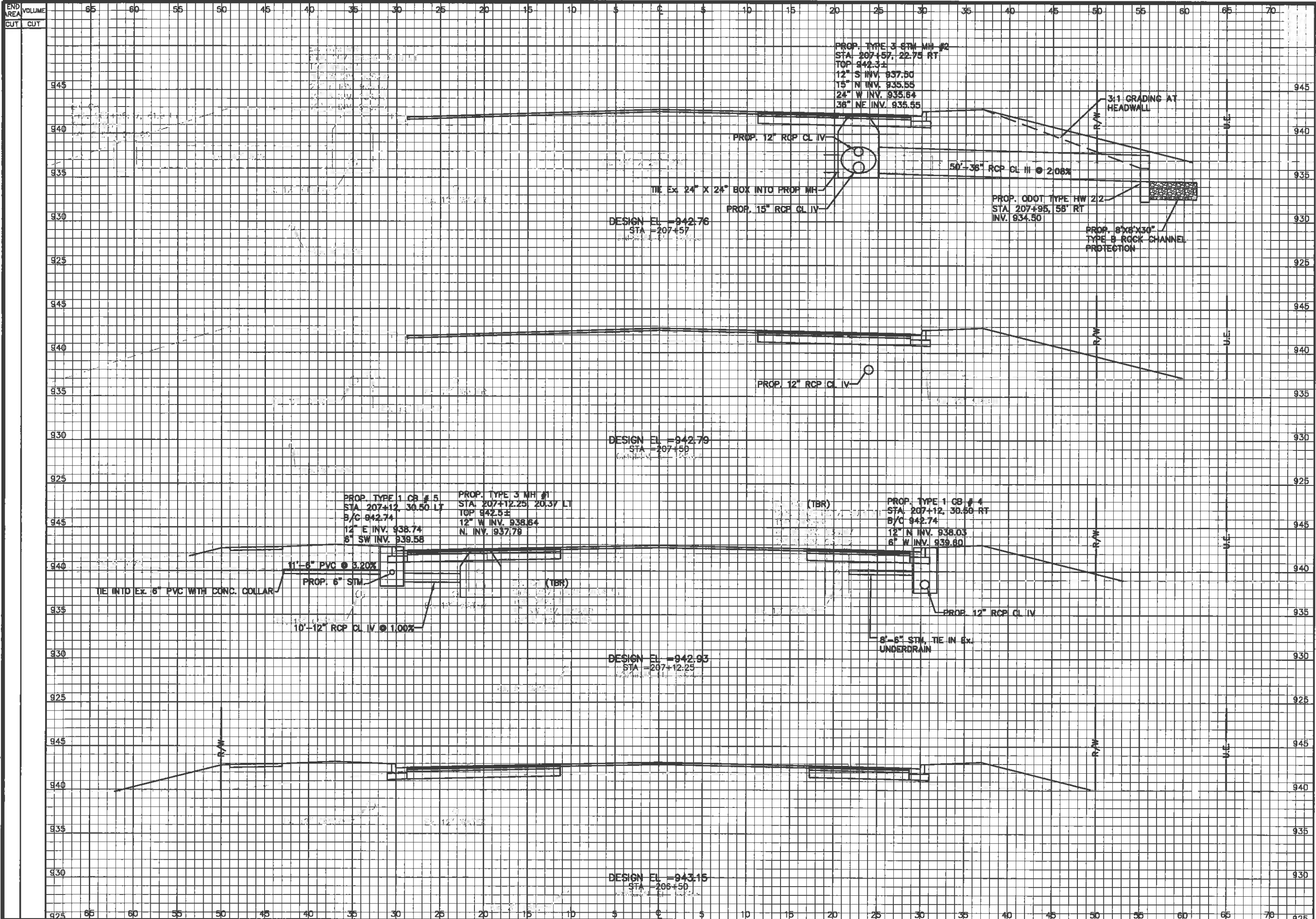
REVISIONS:
FILE NAME CR 25-A - (1)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATC1406
DATE 12-19-2016
SHEET NUMBER 31 OF 52



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 PORTLAND, INDIANA 760.766.7800
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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 205+00 TO STA. 206+00

REVISIONS:
FILE NAME CR 25-A - (2)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATCH1406
DATE 12-19-2016
SHEET NUMBER 32 OF 52



PROP. TYPE 3 STM MH #2
 STA. 207+57, 22.75 RT
 TOP 942.34
 12" S INV. 937.50
 15" N INV. 935.55
 24" W INV. 935.64
 36" NE INV. 935.55

DESIGN EL - 942.76
 STA - 207+57

PROP. ODOT TYPE HW 212
 STA. 207+95, 55' RT
 INV. 934.50

PROP. 8'X8'X30"
 TYPE B ROCK CHANNEL
 PROTECTION

DESIGN EL - 942.79
 STA - 207+59

PROP. TYPE 1 CB # 5
 STA. 207+12, 30.50 LT
 B/C 942.74
 12" E INV. 938.74
 6" SW INV. 939.58

PROP. TYPE 3 MH #1
 STA. 207+12.25, 20.37 LT
 TOP 942.53
 12" W INV. 938.64
 N. INV. 937.79

PROP. TYPE 1 GB # 4
 STA. 207+12, 30.58 RT
 B/C 942.74
 12" N INV. 938.03
 6" W INV. 939.80

DESIGN EL - 942.93
 STA - 207+12.25

DESIGN EL - 943.15
 STA - 206+50

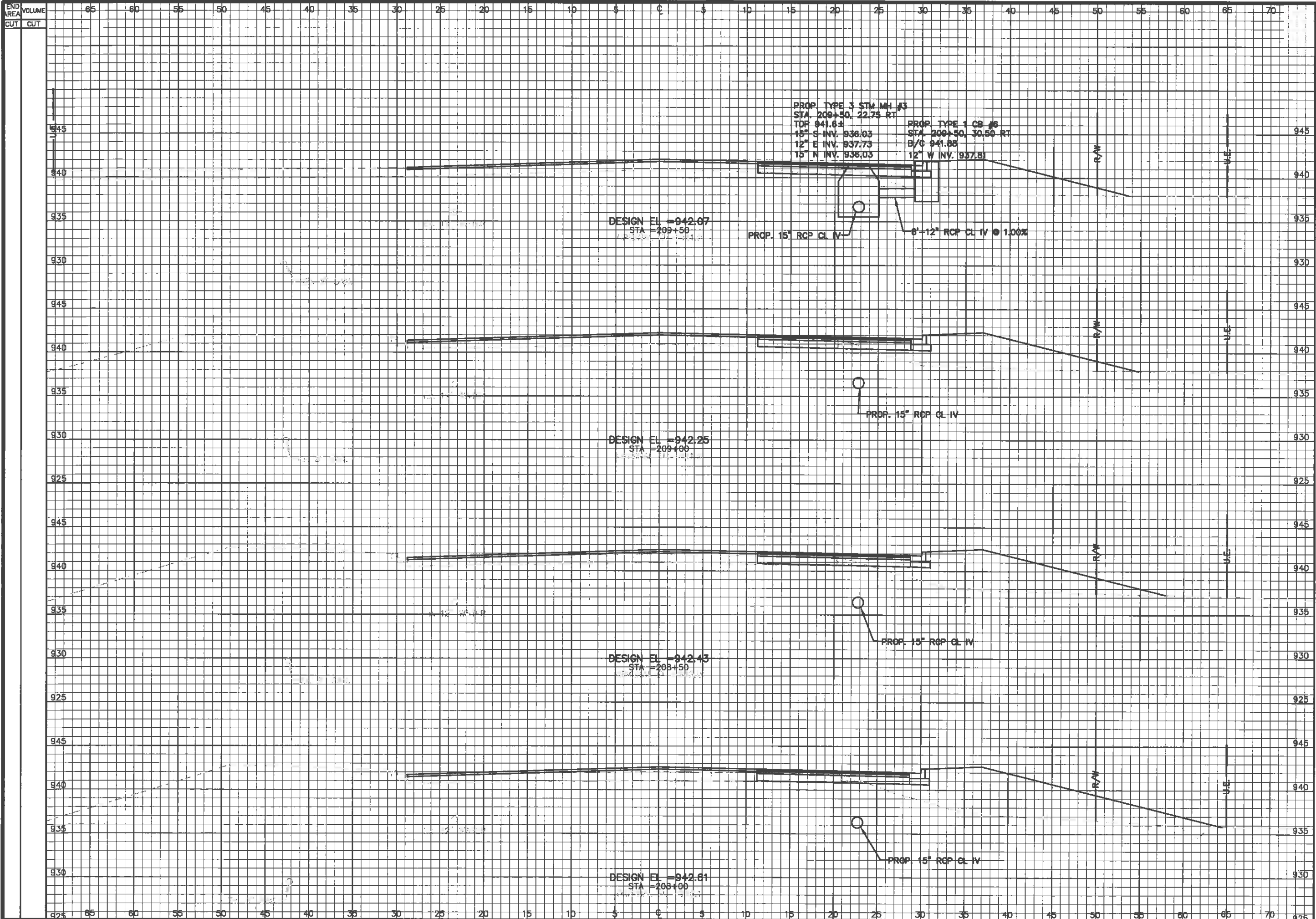


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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 206+50 TO STA. 207+57

REVISIONS:

FILE NAME
 CR 25-A - (3)
 DRAWN BY
 BHB
 CHECKED BY
 JSP
 PROJECT No.
 MIATC1406
 DATE
 12-19-2016
 SHEET NUMBER

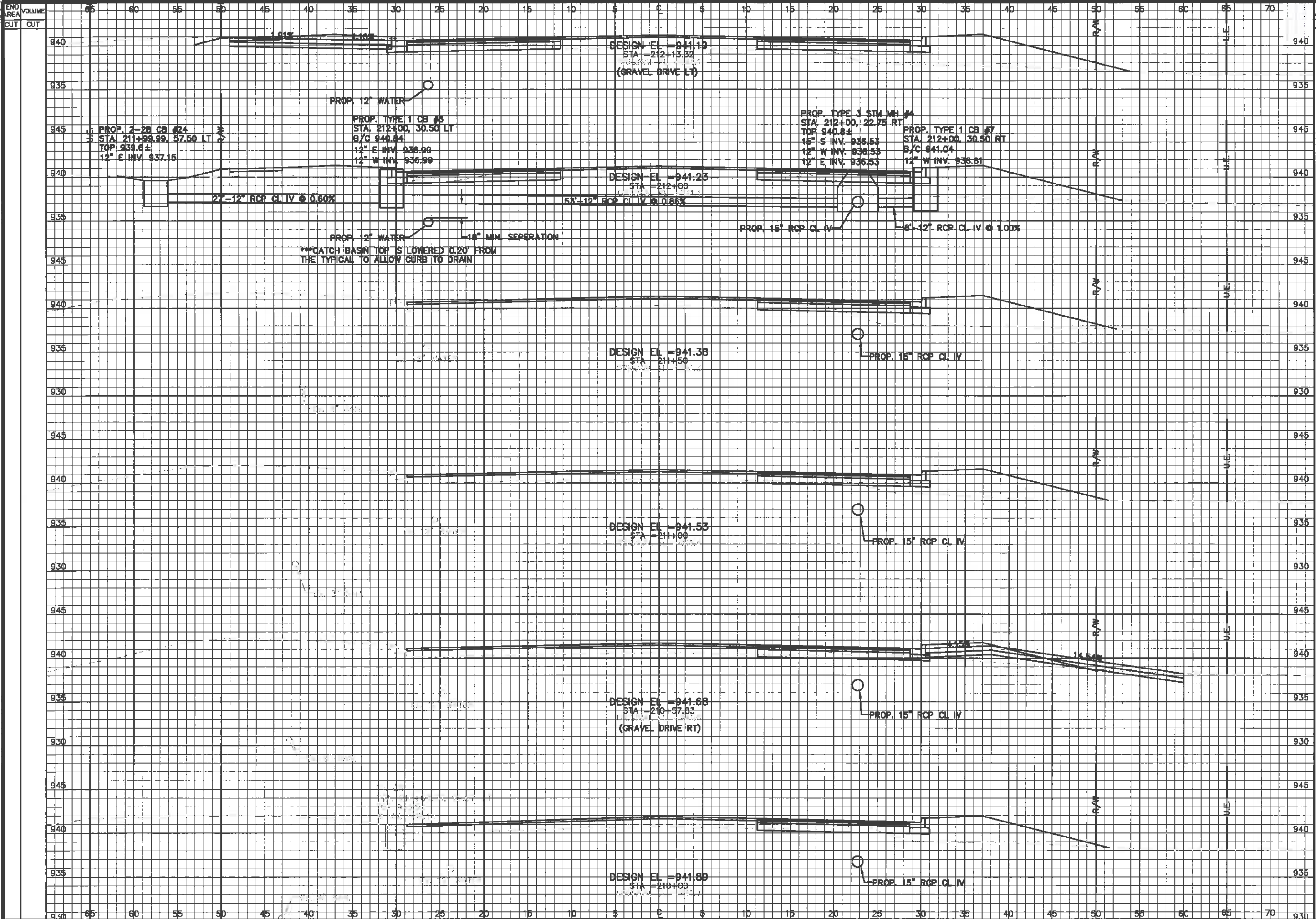


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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 208+00 TO STA. 209+50

REVISIONS:

FILE NAME	CR 25-A - (4)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MATCH 1406
DATE	12-19-2016
SHEET NUMBER	

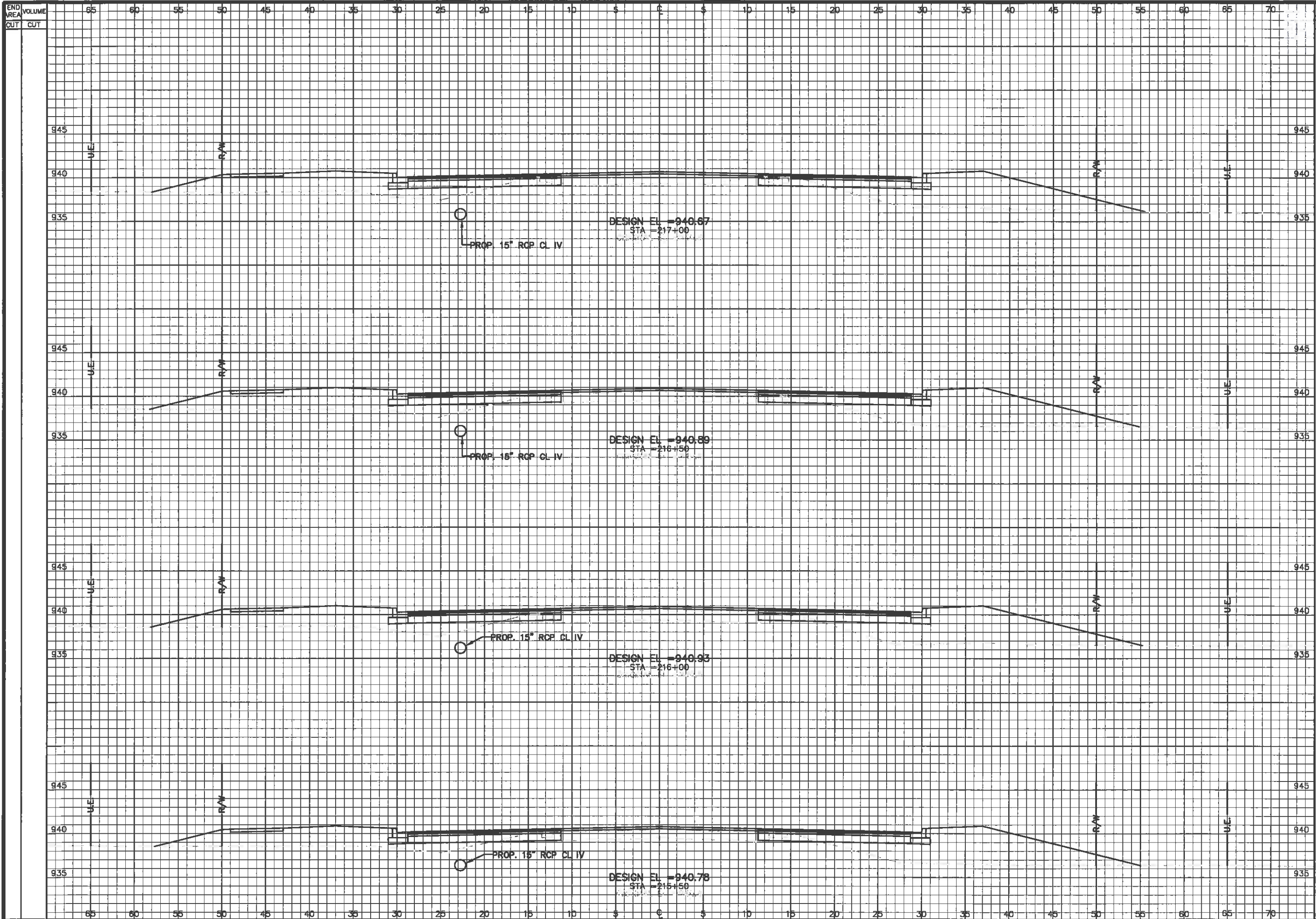


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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 210+00 TO STA. 212+13.32

REVISIONS:

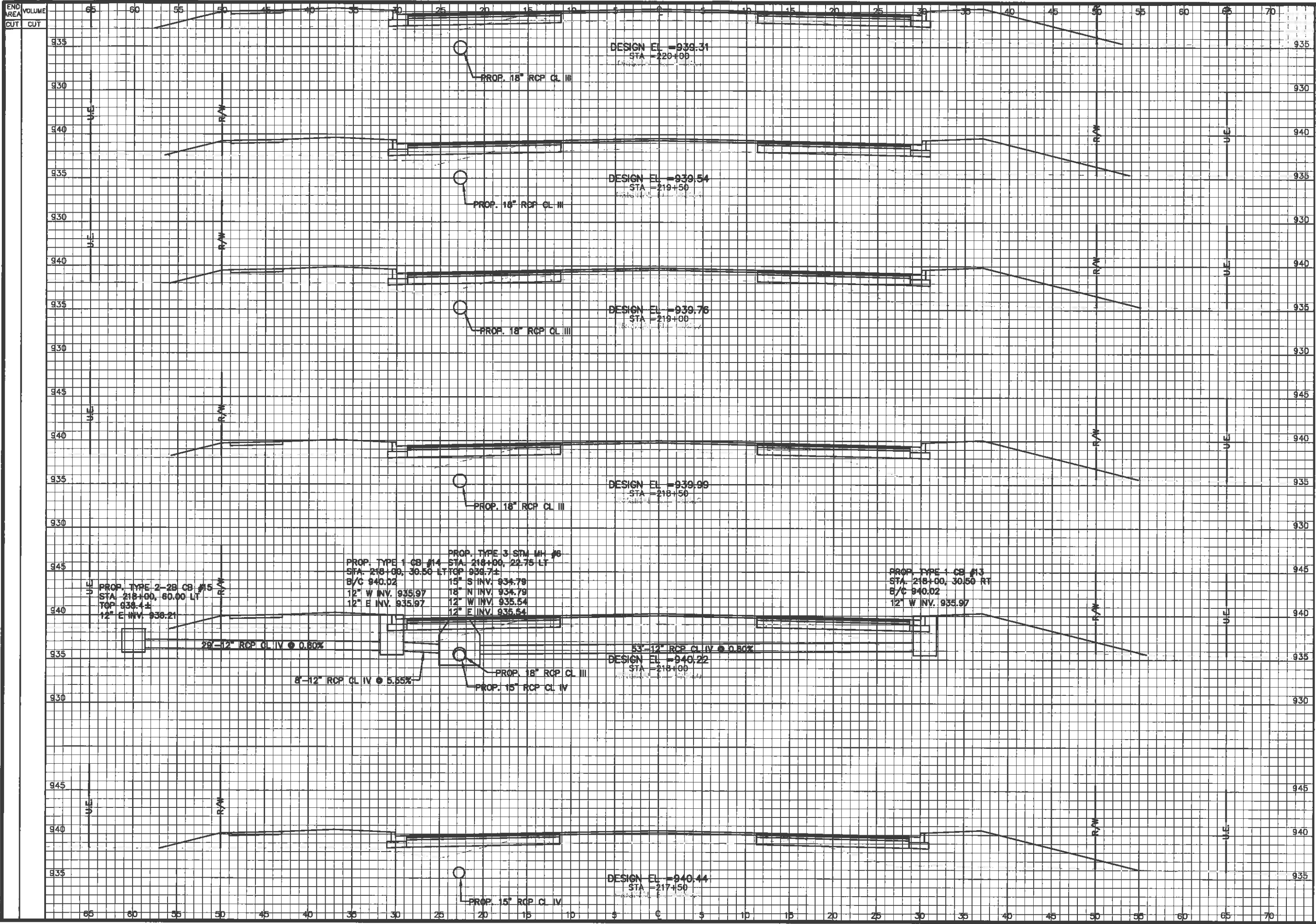
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DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MATCH1406
DATE	12-19-2016
SHEET NUMBER	



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 215+50 TO STA. 217+00

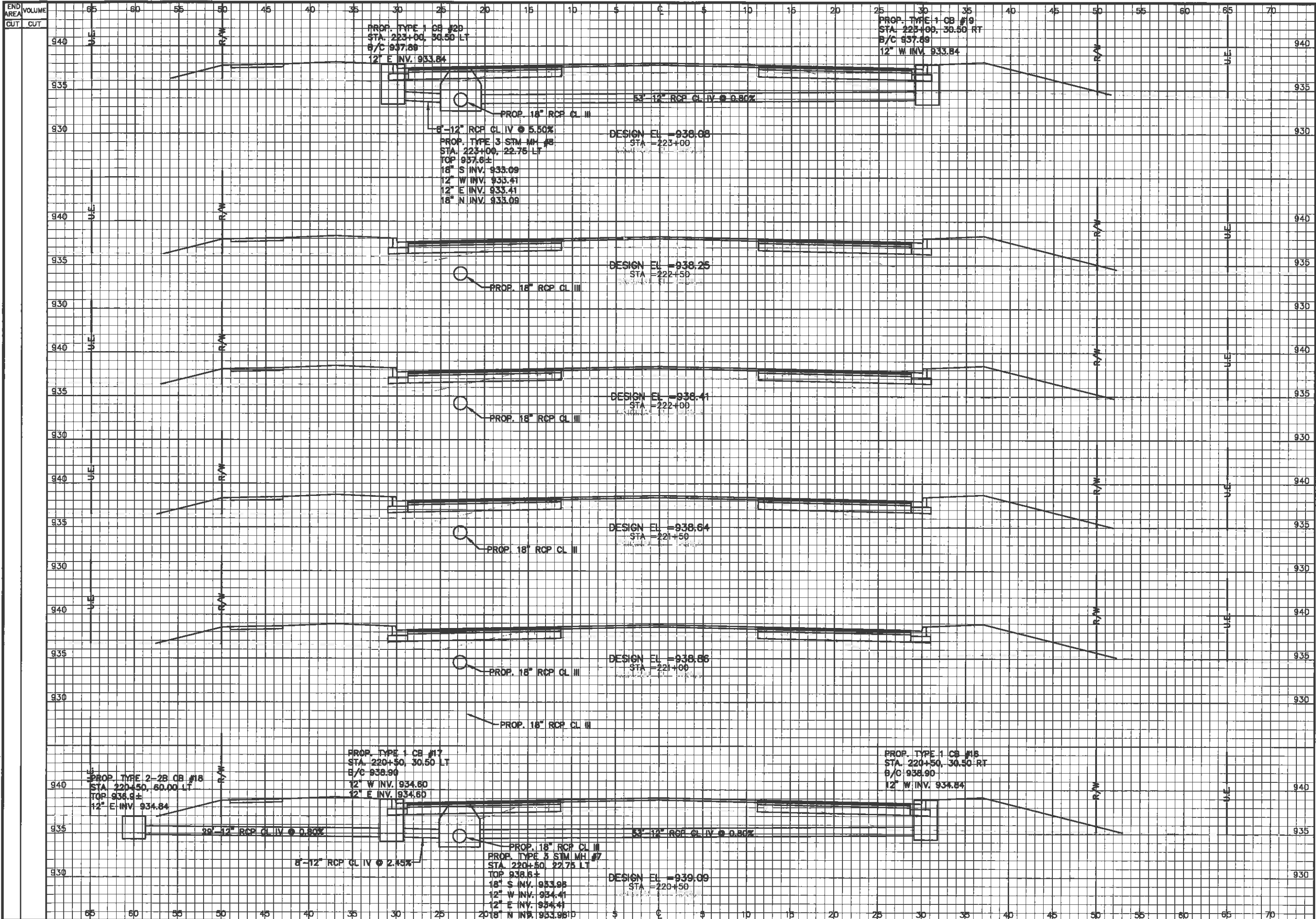
REVISIONS:
FILE NAME CR 25-A - (7)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATCH1406
DATE 12-19-2016
SHEET NUMBER 37 OF 52



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 217+50 TO STA. 220+00

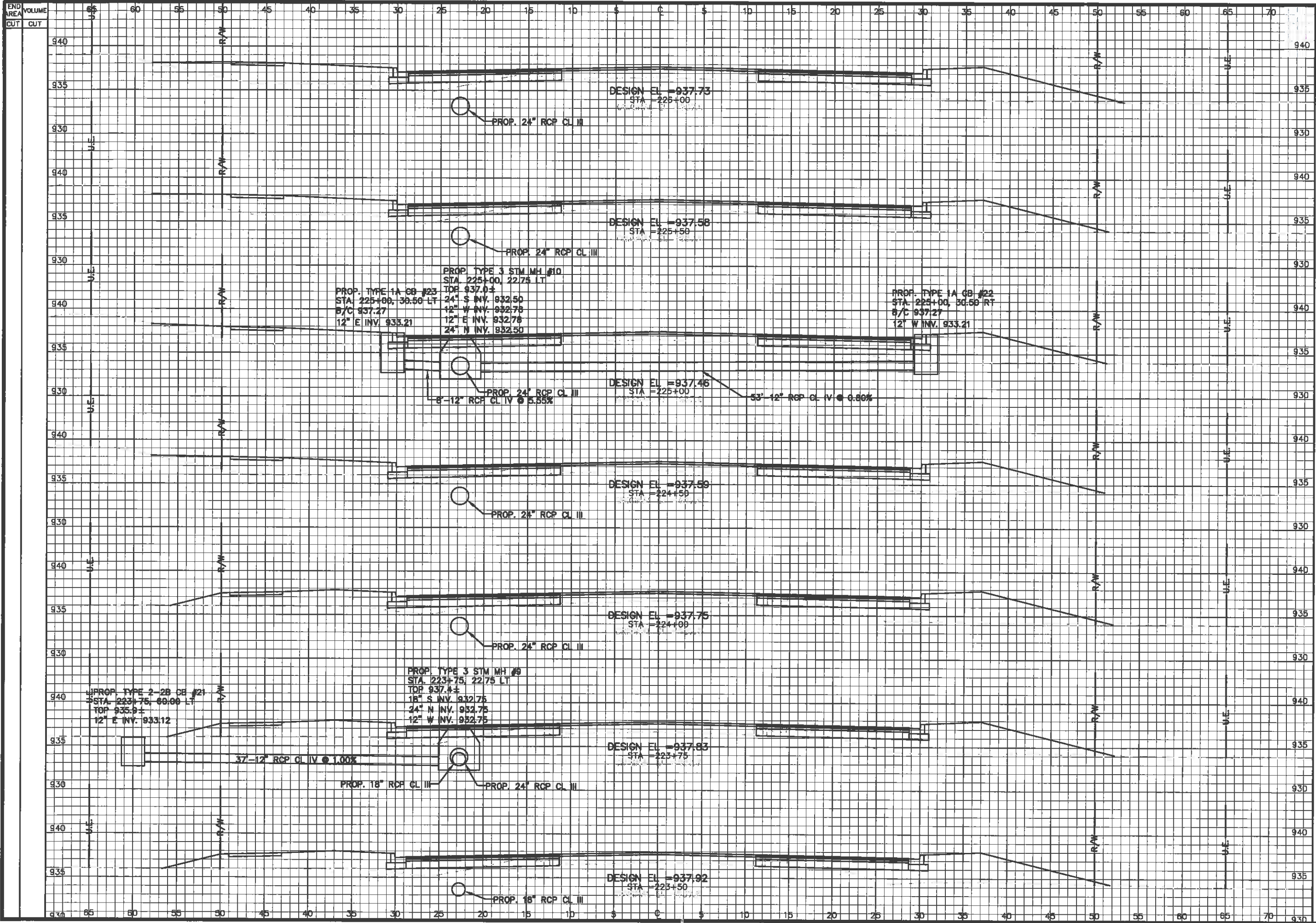
REVISIONS:
FILE NAME CR 25-A - (8)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATC1406
DATE 12-19-2016
SHEET NUMBER 38 OF 52



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 220+50 TO STA. 223+00

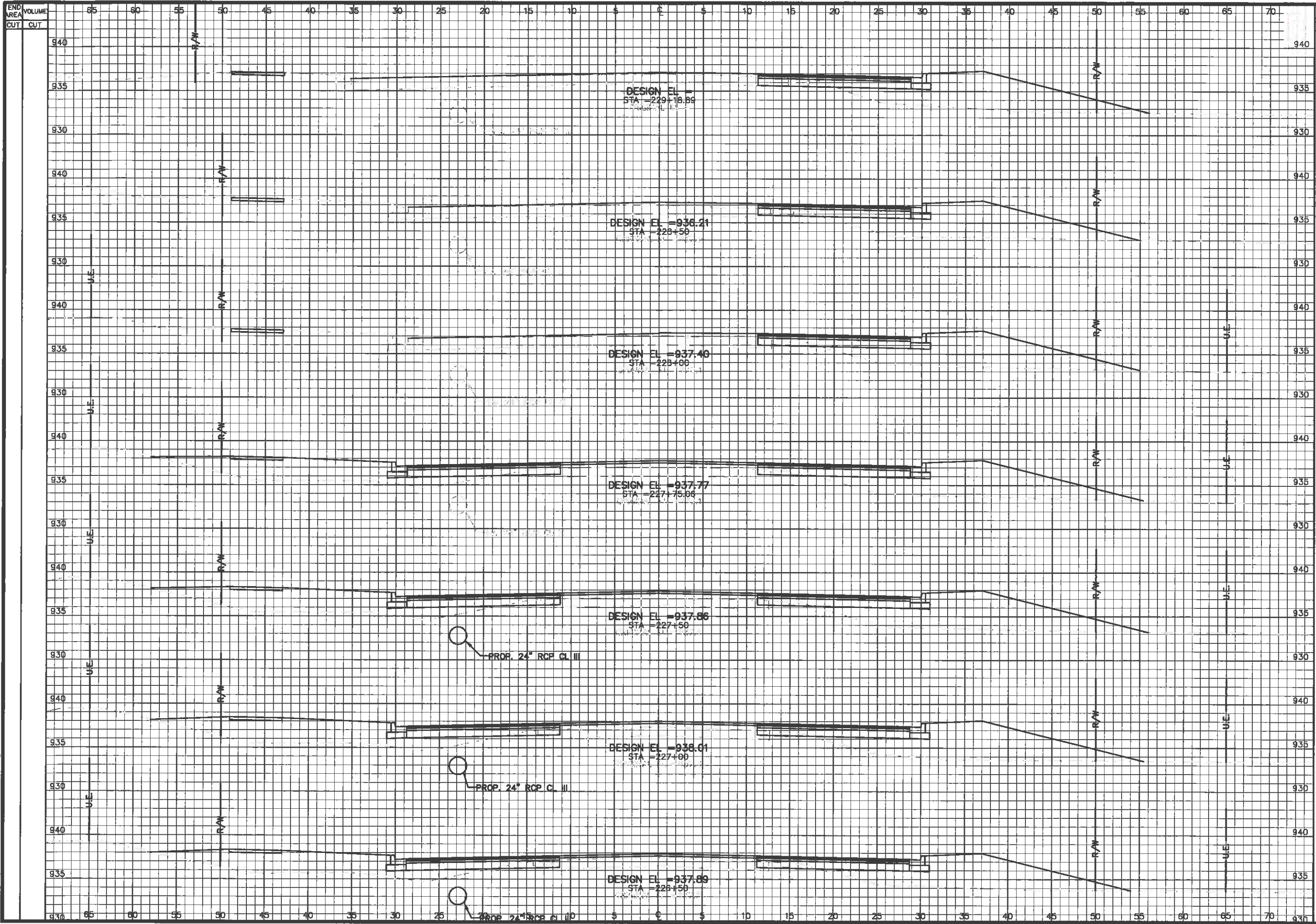
REVISIONS:
FILE NAME CR 25-A - (9)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATC11406
DATE 12-19-2016
SHEET NUMBER 39 OF 52



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 223+50 TO STA. 226+00

REVISIONS:	
FILE NAME	CR 25-A - (10)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATC1406
DATE	12-19-2016
SHEET NUMBER	40 OF 52



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
CR 25-A C.S. STA. 226+50 TO STA. 228+00

REVISIONS:

FILE NAME
CR 25-A - (11)

DRAWN BY
BHB

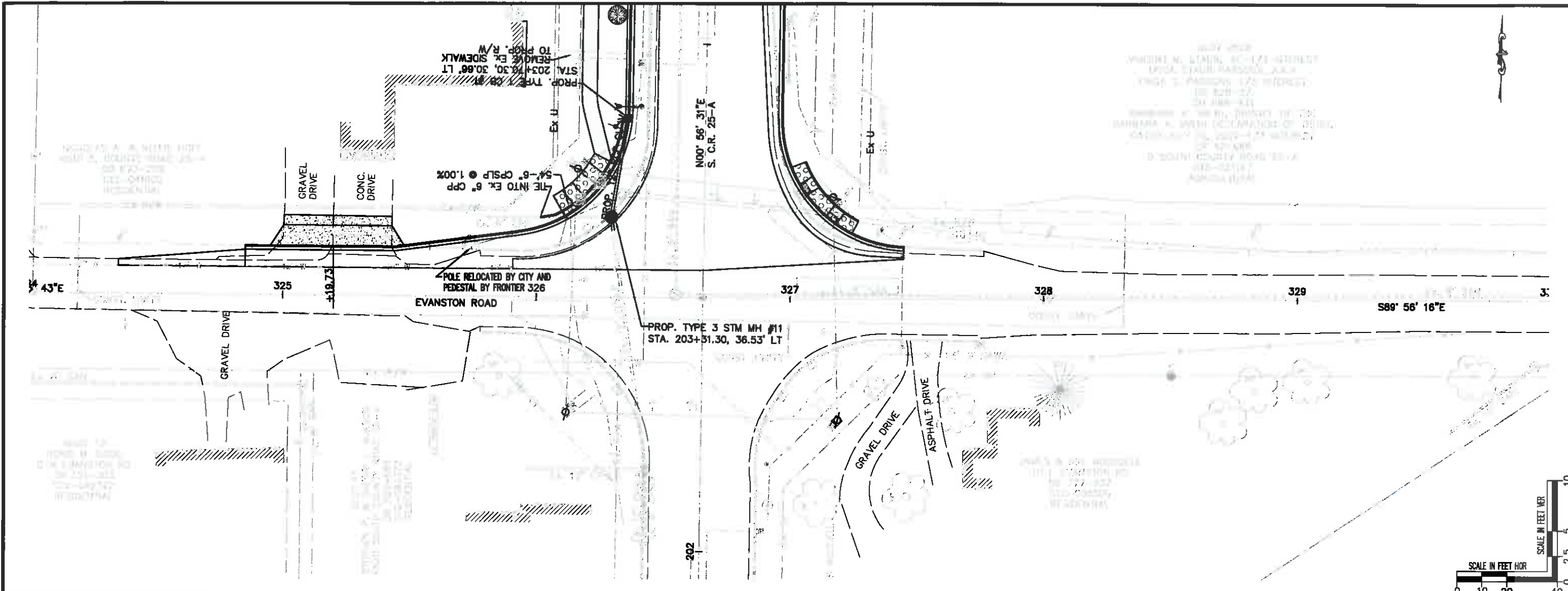
CHECKED BY
JSP

PROJECT No.
MATCH 406

DATE
12-19-2016

SHEET NUMBER

41 OF 52



PROPOSED CENTERLINE ELEVATIONS	324+00	325+00	326+00	327+00	328+00	329+00	330+00
955							
950							
945							
940							
935							
930							
925							
920							

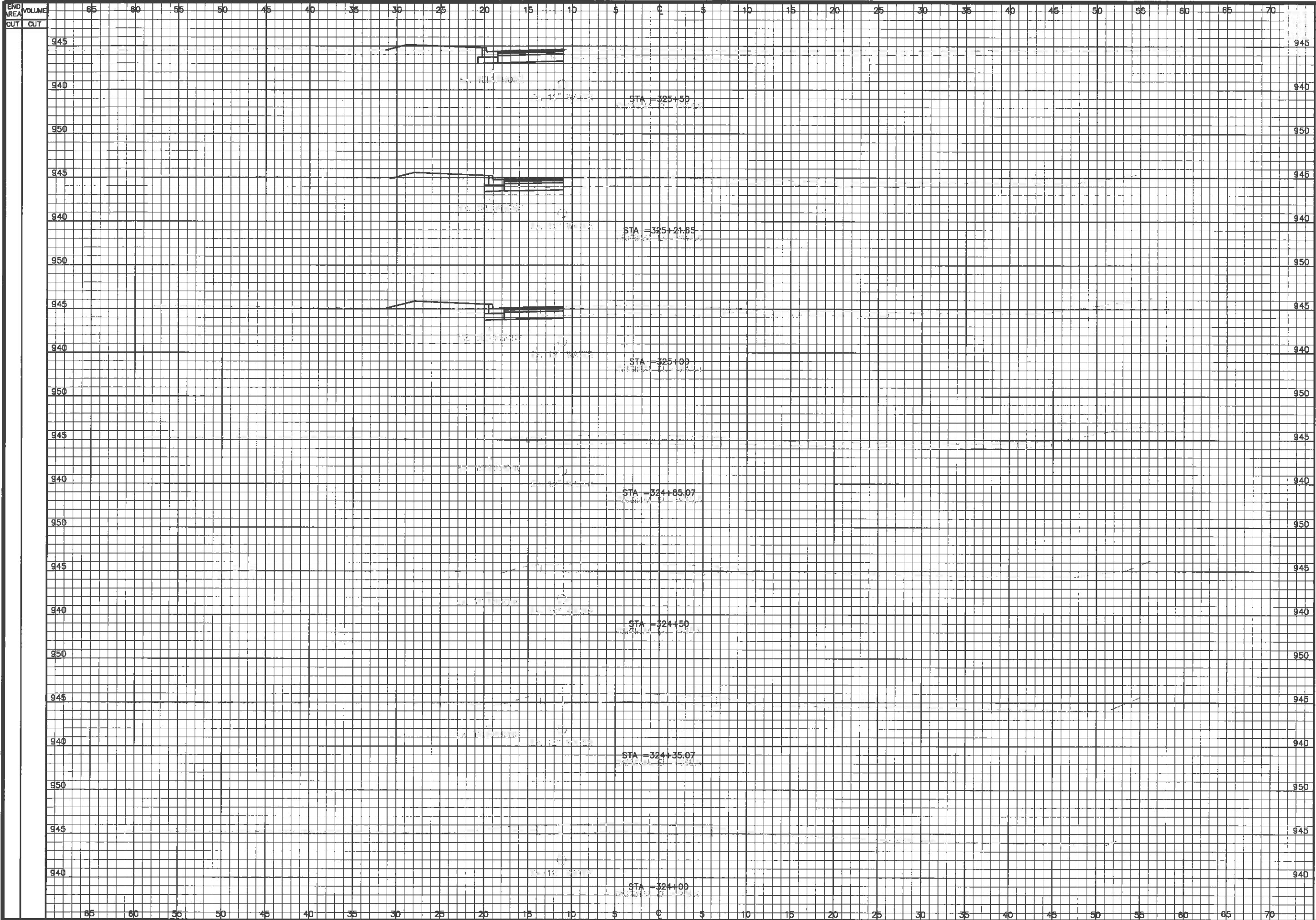
(TBR)
 PROP. TYPE 3 STM MH #11
 STA. 203+31.30, 36.53 LT
 TOP 943.4±
 12" N INV. 940.86
 12" S INV. 940.85
 39'-12" RCP CL IV @ 0.50%

PROP. TYPE 3 STM MH #11
 STA. 203+31.30, 36.53 LT
 TOP 943.4±
 12" N INV. 940.86
 12" S INV. 940.85
 39'-12" RCP CL IV @ 0.50%

SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
EVANSTON P/P STA. 324+00 TO STA. 330+00

REVISIONS:

FILE NAME	EVANSTON (1)
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCI1406
DATE	12-19-2016
SHEET NUMBER	42 OF 52



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
EVANSTON RD. C.S. STA. 324+00 TO STA. 325+50

REVISIONS:

FILE NAME
EVANSTON - (1)

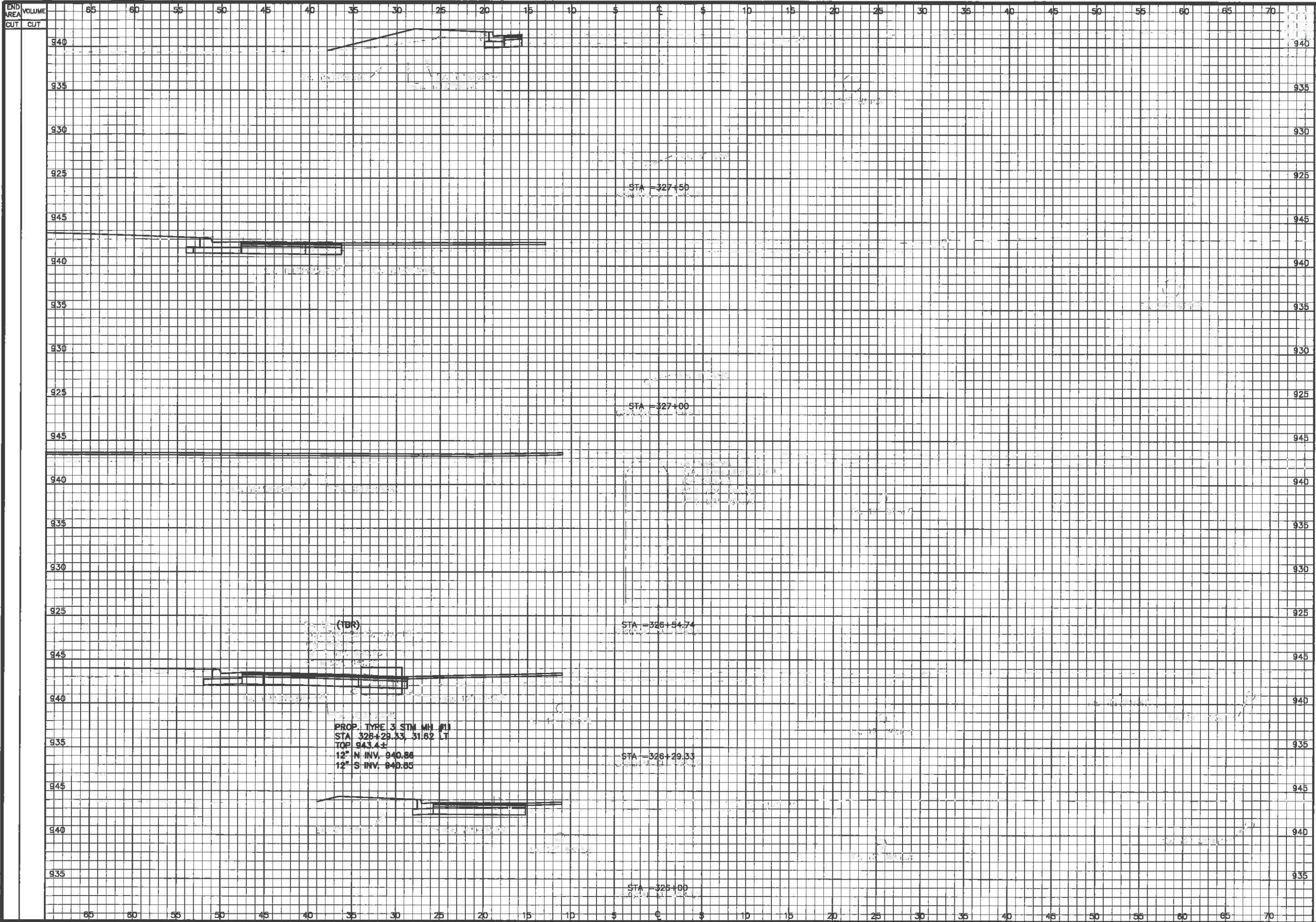
DRAWN BY
BHB

CHECKED BY
JSP

PROJECT No.
MATCH1406

DATE
12-19-2016

SHEET NUMBER



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
 EVANSTON RD. C.S. STA. 326+00 TO STA. 327+50

REVISIONS:

FILE NAME
 EVANSTON - (2)

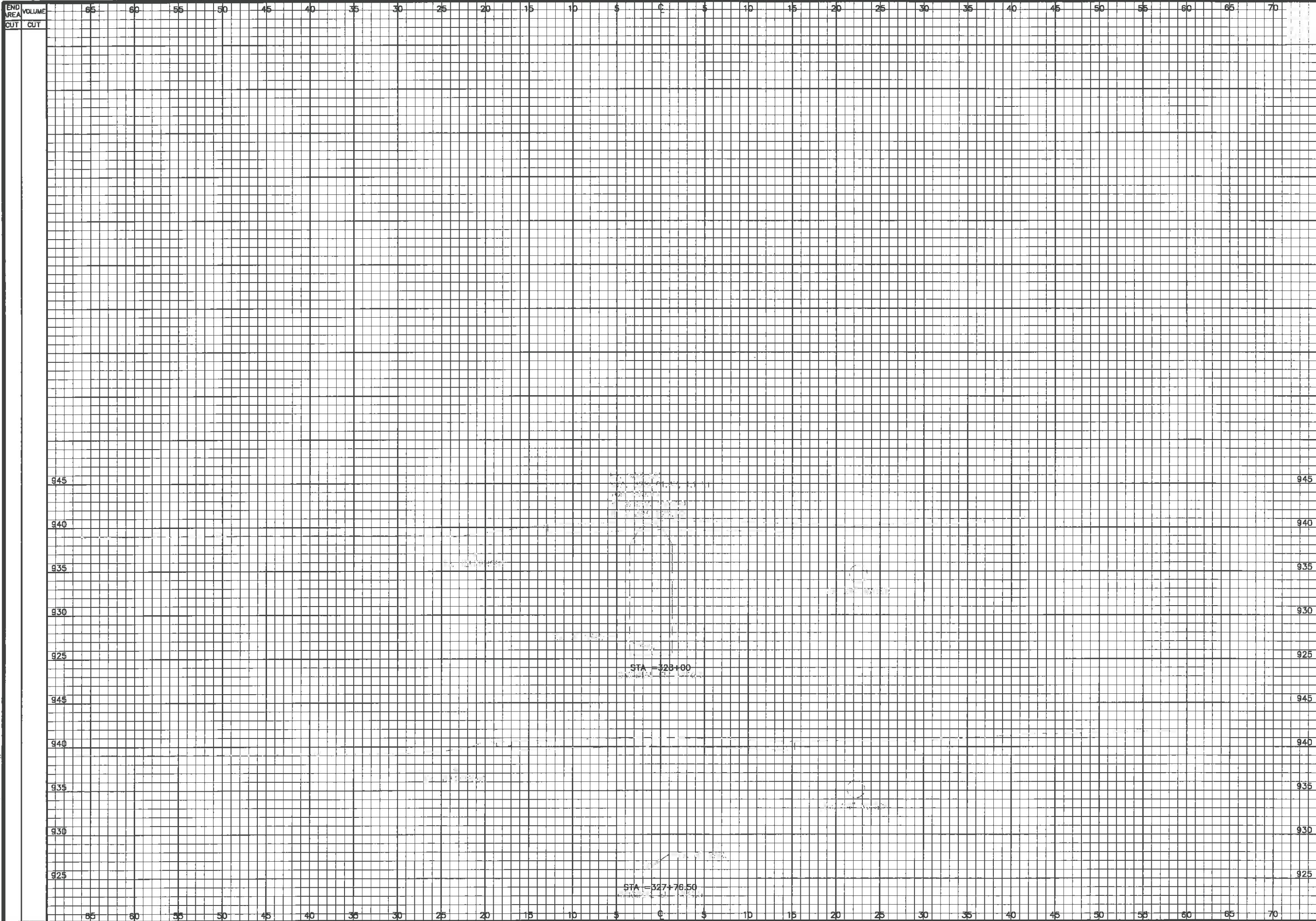
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PROJECT NO.
 MATC1406

DATE
 12-19-2016

SHEET NUMBER



END AREA	VOLUME
CUT	FILL



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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
EVANSTON RD. C.S. STA. 327+76.50 TO STA. 328+00

REVISIONS:
FILE NAME EVANSTON - (3)
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIAT01406
DATE 12-19-2016
SHEET NUMBER 45 OF 52

GENERAL NOTES

- 1) THE CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES AND MISCELLANEOUS CONDUIT AND PIPES PRIOR TO ANY DIGGING. ANY DAMAGE TO ABOVE MENTIONED ITEMS SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR. NOTE: ALL REPAIRS AND MODIFICATIONS TO UTILITIES SHALL BE CLOSELY COORDINATED WITH OWNING UTILITIES OFFICIALS.
- 2) COORDINATE EXACT CONDUIT ROUTES WITH THE CITY OF TIPP CITY TO AVOID CONFLICT WITH SIGNAGE, CURBS, TREES, AND OTHER LANDSCAPING.
- 3) PROVIDE 18" FLUSH GRADE PULL BOX, NEMA 3R, NO BASE AND LOCKING COVER RATED FOR OCCASIONAL HEAVY TRAFFIC. COORDINATE FRAME COVER COLOR WITH THE CITY, COVER LOGO "ELECTRIC". INSTALL PER MANUFACTURER'S RECOMMENDATION. STUB CONDUIT THOROUGH PULL BOX SIDE WALLS. USE A MANUFACTURER'S RECOMMENDED WALL PUNCH AS NECESSARY FOR CONDUIT KNOCKOUTS. SIZE KNOCKOUT ONE TRADE SIZE LARGER THAN CONDUIT TO ALLOW FOR CONDUIT MOVEMENT. COORDINATE WITH THE CITY FOR LOCATIONS. MOUNT BOX SUCH THAT TOP OF BOX WILL BE FLUSH WITH TOP OF FINISH SURFACE.
- 4) CITY TO CONNECT WIRING AND PROVIDE POWER TO THE SYSTEM. CONTRACTOR TO SUPPLY/INSTALL CONDUITS, GROUND ROD, LIGHT POLES, POLE FOUNDATIONS, AND WIRE.
- 5) UNDERDRAINS FOR PULL BOXES SHALL BE USED AS DIRECTED BY THE CITY. CONTRACTOR TO USE ITEM 611, 4" TYPE E CONDUIT, WILL BE NECESSARY FOR EVERY PULL BOX. COST OF UNDERDRAINS TO BE CONSIDERED INCIDENTAL TO RELATED ITEMS.
- 6) THE LOCATION OF UNDERGROUND DUCT CABLE ON NON-METALLIC CONDUIT, WHEN INSTALLED IN LOCATIONS OTHER THAN THE NORMAL OR ALTERNATE TRENCH ALIGNMENT SHOWN ON STANDARD CONSTRUCTION DRAWING HL-20.11, SHALL BE MARKED BY THE USE OF A CONTINUOUS IDENTIFYING TAPE BURIED IN THE TRENCH ABOVE THE LINE. THE IDENTIFYING TAPE SHALL BE AN INERT MATERIAL, APPROXIMATELY 6" WIDE, COMPOSED OF POLYETHYLENE PLASTIC, HIGHLY RESISTANT TO ALKALIS, ACID OR OTHER CHEMICAL COMPONENTS LIKELY TO BE ENCOUNTERED IN SOILS. THE TAPE SHALL BE BRIGHT YELLOW WITH IDENTIFYING PRINTING "ELECTRIC" IN BLACK LETTERS, ONE SIDE ONLY. TAPES SHALL BE SUPPLIED IN CONTINUOUS ROLLS WITH THE IDENTIFYING LETTERING REPEATED THE FULL LENGTH OF THE TAPE. IDENTIFYING TAPES SHALL BE BURIED IN THE ELECTRIC LINE TRENCH WITH ONE STRIP PLACED APPROXIMATELY DOWN THE CENTERLINE AND LOCATED APPROXIMATELY 8" TO 12" BELOW THE FINISHED GRADE. THE TAPE SHALL BE PLACED IN THE TRENCH WITH THE PRINTED SIDE UP AND SHALL BE ESSENTIALLY PARALLEL WITH THE FINISHED SURFACE. THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO ENSURE THAT THE TAPE IS NOT PULLED, DISTORTED OR OTHERWISE MISPLACED IN COMPLETING THE TRENCH BACKFILL. TAPE SHALL BE ALLEN SYSTEM'S, TERRA TAPE, TECTA TAPE OR EQUAL AS APPROVED BY THE CITY. THE COST OF SUPPLYING AND INSTALLING TAPE SHALL BE CONSIDERED INCIDENTAL TO RELATED ITEMS.
- 7) QUESTIONS OR COMMENTS REGARDING CHANGES, ALTERATIONS, RECOMMENDATIONS, AND/OR ADDITIONS TO THE LIGHTING PLAN TO BE DIRECTED TO GARY GARVEY-CITY OF TIPP CITY, 937-667-0519.
- 8) ALL POLE FOUNDATIONS SHALL BE AS SHOWN ON STD. CONST. DWG. HL-20.11. CONTRACTOR TO GET ANCHOR BOLT PATTERN FROM POLE MANUFACTURER.
- 9) PRIOR TO THE CONTRACTOR ORDERING ANY POLES OR ANCHOR PLATE, SHOP DRAWINGS MUST BE APPROVED BY THE ENGINEER.
- 10) THE COST OF ALL TRENCHING ASSOCIATED WITH THE INSTALLATION OF CONDUIT CALLED FOR IN THE PLANS SHALL BE INCIDENTAL TO AND INCLUDED IN THE CONTRACT UNIT PRICE FOR ITEM 625 CONDUIT.

ITEM 625 LIGHT POLE, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 625 HIGHWAY LIGHTING, EXCEPT AS HEREIN MODIFIED.

THIS ITEM WILL INCLUDE ALL MATERIALS AND LABOR NECESSARY TO BE CONSTRUCTED AS SHOWN ON THIS SHEET.

THE POLE WILL BE AS MANUFACTURED BY HAPCO #RTA35F8B4M18, ROUND TAPERED ALUMINUM POLE, SATIN GROUND FINISH, INCLUDING PHILIPS HADCO Rx2 1200, TYPE II ROADWAY DISTRIBUTION LED, WITH 120V PHOTO CELL.

CONTRACTOR TO SUPPLY A POINT BY POINT FOOT CANDLE LIGHT LAYOUT.

PAYMENT FOR ITEM 625, LIGHT POLE, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 625 DISTRIBUTION CABLE, Misc. 8 STRANDED TRIPLEX, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 625 HIGHWAY LIGHTING, EXCEPT AS HEREIN MODIFIED.

CABLE SHALL BE NUMBER 8 AWG, 3 CONDUCTOR, USE-2, RHH, OR RHW-2 600 VOLT COPPER TWISTED CABLE. EACH CONDUCTOR SHALL BE STRANDED, INSULATED WITH MOISTURE AND HEAT RESISTANT, CHEMICALLY CROSSLINKED POLYETHYLENE CABLE SHEATH, AND TWISTED TOGETHER TO FORM A SINGLE CABLE. THE CABLE SHALL BE CAPABLE OF BEING INSTALLED IN RACEWAY, CONDUIT, DIRECT BURIAL, AND AERIAL INSTALLATIONS. THE CABLE SHALL HAVE A SUPERIOR FLAME RETARDANT PROPERTY.

PAYMENT FOR ITEM 625, DISTRIBUTION CABLE, Misc. 8 STRANDED TRIPLEX, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT FOOT BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 625 FUSED CONNECTOR, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 625 HIGHWAY LIGHTING, EXCEPT AS HEREIN MODIFIED.

FUSED "Y" CONNECTIONS SHALL BE INSTALLED IN THE BAS OF EACH POLE. A FUSED CONNECTOR WILL BE REQUIRED FOR EACH HOT LINE CONDUCTOR. EACH CONNECTOR SHALL BE INSTALLED BASED ON MANUFACTURERS RECOMMENDATION.

PAYMENT FOR ITEM 625, FUSED CONNECTOR, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 625 POLE AND BRACKET CABLE, #10 AWG, 600V, AS PER PLAN

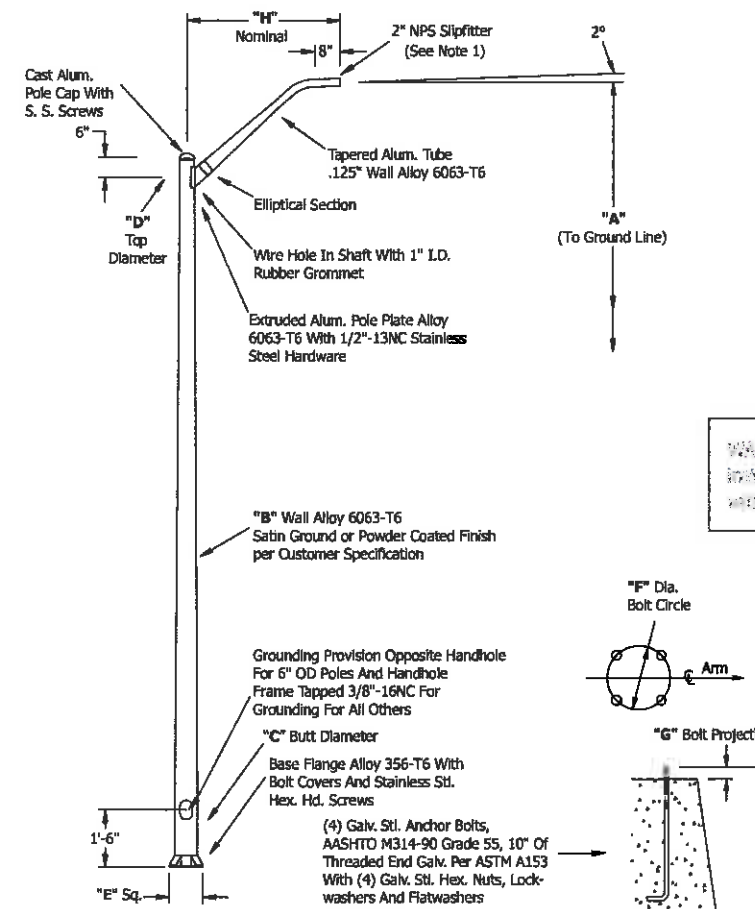
THIS ITEM OF WORK SHALL CONSIST OF THE WORK AS DESCRIBED IN OHIO DEPARTMENT OF TRANSPORTATION ITEM 625 HIGHWAY LIGHTING, EXCEPT AS HEREIN MODIFIED.

POLE AND BRACKET CABLE SHALL BE SUPPLIED FROM THE CONNECTION TO THE FIXTURE. ADEQUATE SLACK SHALL BE PROVIDED AT THE CONNECTION AND LUMINARIES FOR MAKING CONNECTIONS.

PAYMENT FOR ITEM 625, POLE AND BRACKET CABLE, #10 AWG, 600V, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT FOOT BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.



4 Bolt Base RTA35F8B4M18-



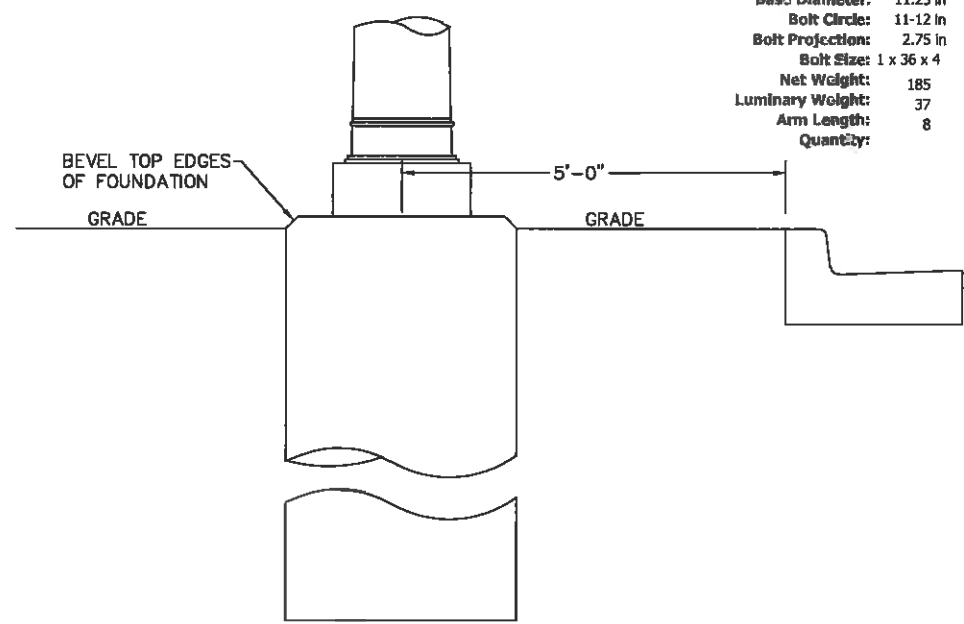
WARNING: Do not install light pole without luminaires.

Mounting Height:	35 ft
Wall Thickness:	.250 in
Butt Diameter:	8 in
Top Diameter:	4.5 in
Base Diameter:	11.25 in
Bolt Circle:	11-12 in
Bolt Projection:	2.75 in
Bolt Size:	1 x 36 x 4
Net Weight:	185
Luminaire Weight:	37
Arm Length:	8
Quantity:	8

Maximum Spacing:	70: 8
	60: 3.5
	90: 2.7
	100: 1.8
	110: 0.8

Notes:
(1) Brackets Supplied With 2" NPS Slipfitter. Specify Modification 140 For 1.25" NPS Slipfitter.

Your Name:
Representative Name:
Architect Name:
Project Name:
Customer P.O. #:
Finish:
Date: 02/27/2016
Notes:



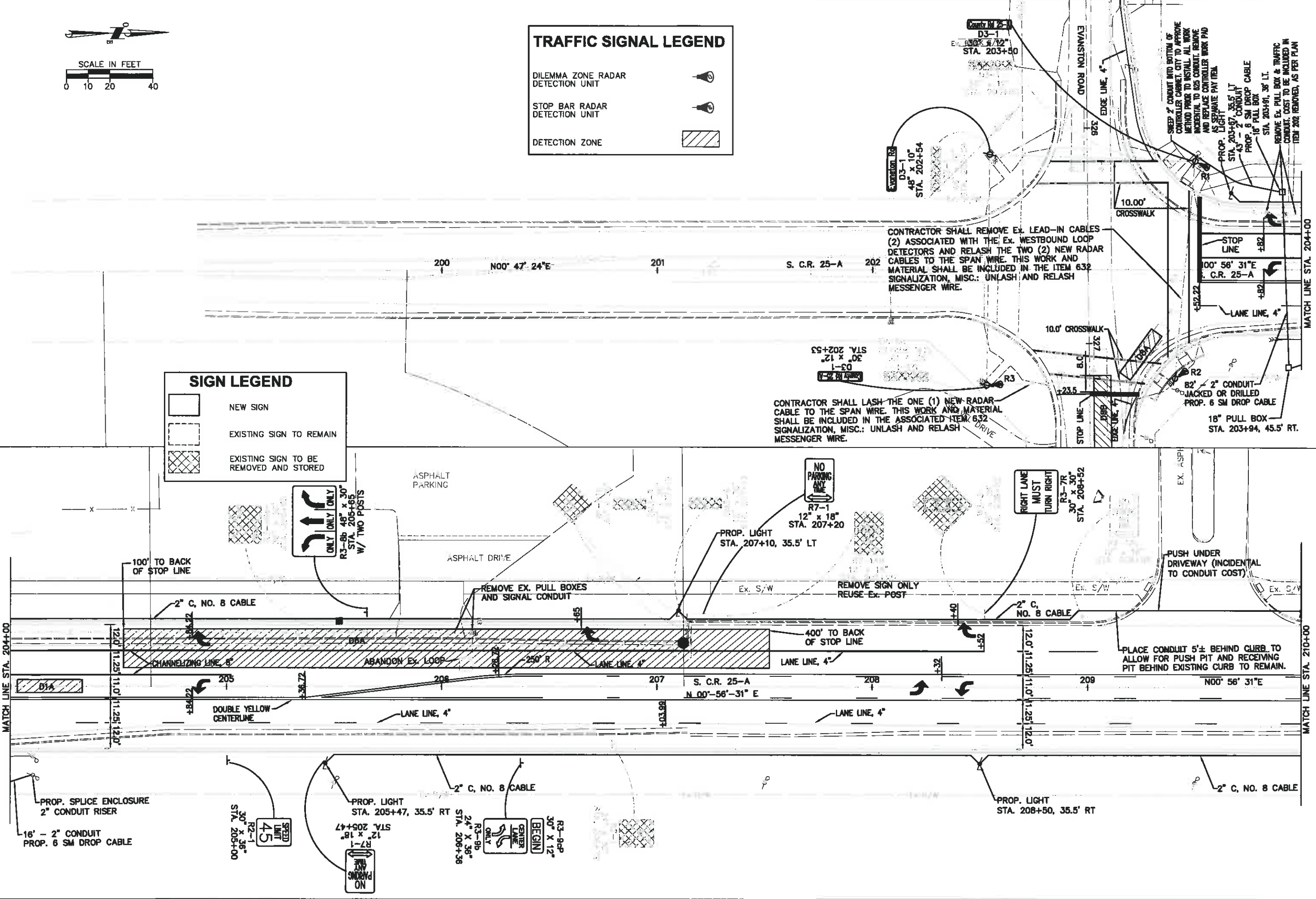
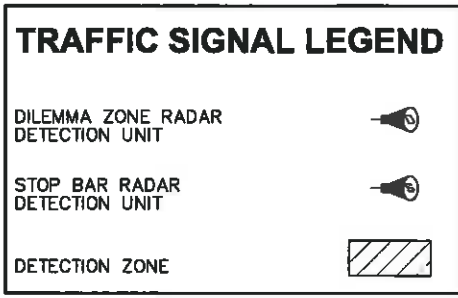
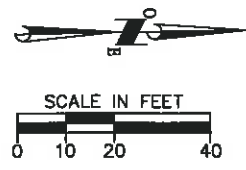
CONCRETE POLE FOUNDATION
NTS

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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
STREET LIGHTING NOTES AND DETAILS

REVISIONS:

FILE NAME	MATCH406ELA
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MATCH1406
DATE	12-19-2016
SHEET NUMBER	46 OF 52



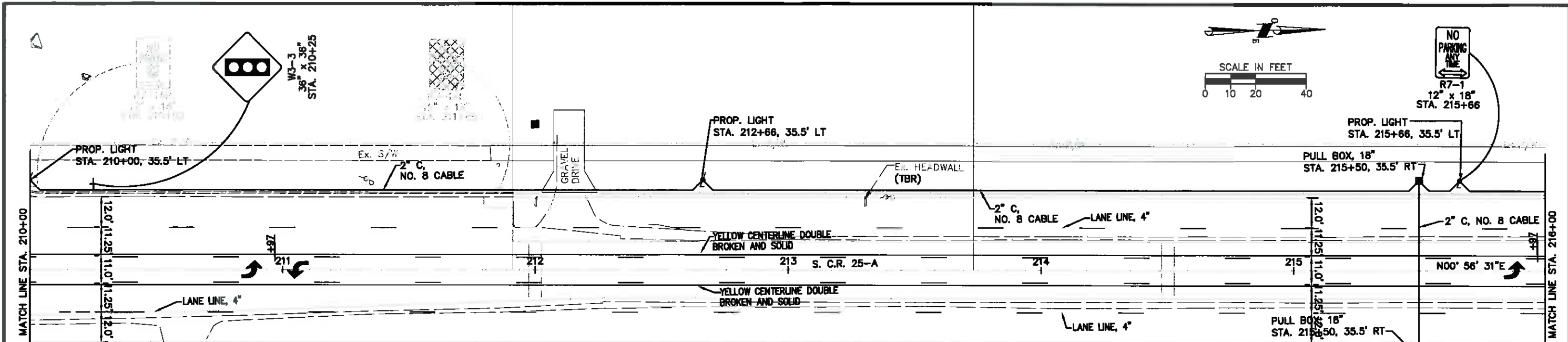
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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY

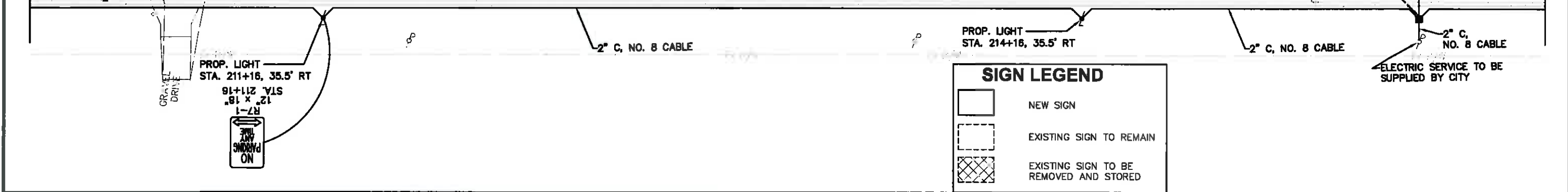
PAVEMENT MARKING, SIGNAGE, AND LIGHTING PLAN STA. 200+00 TO STA. 210+00

REVISIONS:
FILE NAME MATCH406TPA
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MATCH406
DATE 12-19-2016
SHEET NUMBER 47 OF 52



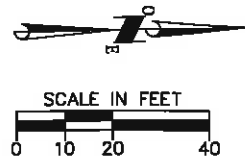
SIGN LEGEND

	NEW SIGN
	EXISTING SIGN TO REMAIN
	EXISTING SIGN TO BE REMOVED AND STORED

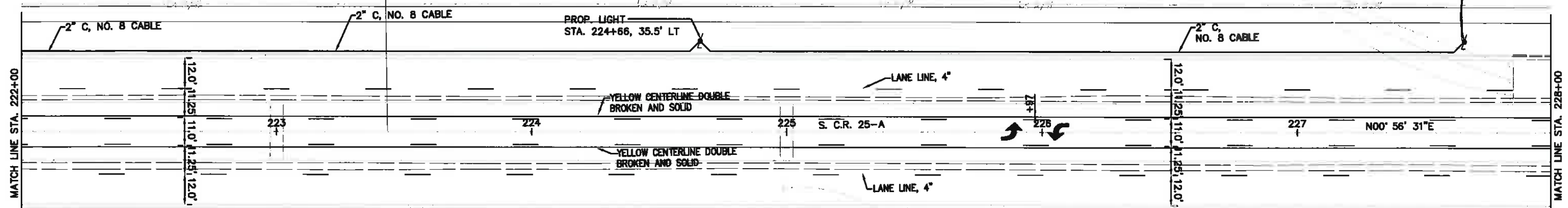


REVISIONS:

FILE NAME	MATCH1406TPB
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MATCH1406
DATE	12-19-2016
SHEET NUMBER	48 OF 52



NO PARKING
R7-1
12' x 18"
STA. 227+66



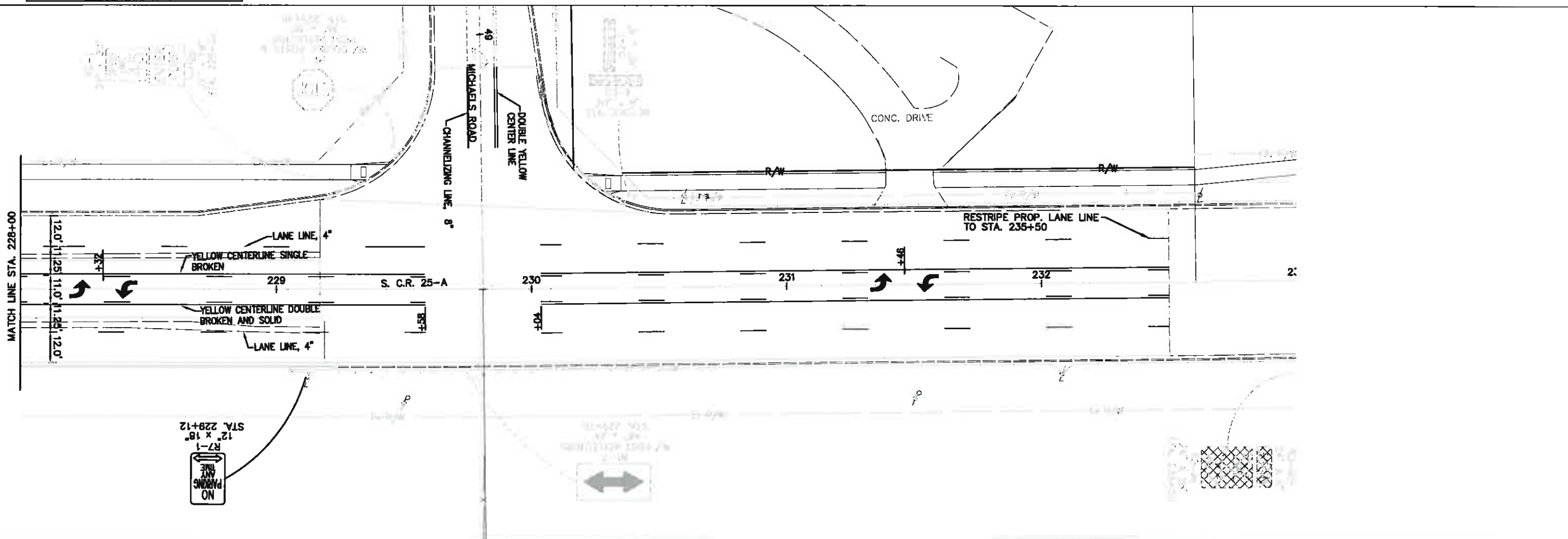
SIGN LEGEND

- NEW SIGN
- EXISTING SIGN TO REMAIN
- EXISTING SIGN TO BE REMOVED AND STORED

PROP. LIGHT
STA. 223+16, 35.5' RT
R7-1
12' x 18"
NO PARKING

W2-2L
36" x 36"
Michigan Rd
W16-6P
5' x 10"
STA. 224+98
W/ TWO POSTS

PROP. LIGHT
STA. 226+16, 35.5' RT



PROP. LIGHT
STA. 229+12, 35.5' RT
R7-1
12' x 18"
NO PARKING

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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
PAVEMENT MARKINGS, SIGNAGE, AND LIGHTING PLAN STA. 222+00 TO STA. 234+00

REVISIONS:

FILE NAME	MIATCH1406TPC
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATCH1406
DATE	12-19-2016
SHEET NUMBER	49 OF 52

ODOT STANDARD CONSTRUCTION DRAWINGS

DRAWINGS IN THESE PLANS SHALL BE CONSIDERED AS REFERENCE TO ITEMS 625, 630, 631, 632, 633, 725, 730, 731, 732, AND 733 RESPECTIVELY.

ALL MATERIALS MUST BE IN COMPLIANCE WITH CONTRACT SPECIFICATIONS UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL WORK AND MATERIALS NOT SPECIFICALLY REFERENCED IN THE CONTRACT SHALL MEET OR EXCEED THE REQUIREMENTS OF:

OHIO DEPARTMENT OF TRANSPORTATION, CONSTRUCTION AND MATERIAL SPECIFICATIONS (2016)

THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (2012)

DETECTOR LOOP DEACTIVATION AND ACTIVATION

THE CONTRACTOR SHALL DEACTIVATE THE SOUTHBOUND LOOP DETECTORS ON CR-25A DURING THE CONSTRUCTION PHASE 1 OF THIS PROJECT. DURING CONSTRUCTION PHASE 2, THE EASTBOUND AND WESTBOUND LOOP DETECTORS SHALL BE DEACTIVATED AND THE NEW RADAR UNIT FOR THE SOUTHBOUND CR-25A TRAFFIC SIGNAL SHALL BE TURNED ON, TESTED AND ACTIVATED. BEFORE EVANSTON ROAD IS OPENED TO TRAFFIC DURING CONSTRUCTION PHASE 2, THE EASTBOUND AND WESTBOUND RADAR UNITS SHALL BE TURNED ON, TESTED AND ACTIVATED. ALL LOOP DETECTOR EQUIPMENT TO BE REMOVED SHALL BE GIVEN UNDAMAGED TO THE CITY. THIS COST SHALL BE INCIDENTAL TO AND INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

REMOVAL OF EX. PULL BOXES AND ELECTRICAL CONDUIT

THE CONTRACTOR SHALL REMOVE ALL PULL BOXES AND ELECTRICAL CONDUIT AT THE S. CR25-A AND EVANSTON ROAD TRAFFIC SIGNAL THAT ARE ABANDONED DUE TO THE REMOVAL OF THE ASSOCIATED LOOP DETECTORS. THIS COST SHALL BE INCLUDED IN ITEM 202, REMOVED, AS PER PLAN BID ITEM.

GUARANTEE

THE CONTRACTOR SHALL GUARANTEE THAT THE TRAFFIC CONTROL SYSTEMS INSTALLED AS PART OF THIS CONTRACT SHALL OPERATE SATISFACTORILY FOR A PERIOD OF 120 DAYS FOLLOWING COMPLETION OF THE 10-DAY PERFORMANCE TEST. IN THE EVENT OF UNSATISFACTORY OPERATION THE CONTRACTOR SHALL CORRECT FAULTY INSTALLATIONS, MAKE REPAIRS AND REPLACE DEFECTIVE PARTS WITH NEW PARTS OF EQUAL OR BETTER QUALITY. EQUIPMENT, MATERIAL AND LABOR COSTS INCURRED IN CORRECTING AN UNSATISFACTORY OPERATION SHALL BE BORNE BY THE CONTRACTOR.

THE GUARANTEE SHALL COVER THE FOLLOWING ITEMS OF THE TRAFFIC CONTROL SYSTEM: RADAR SENSORS AND ASSOCIATED EQUIPMENT.

CUSTOMARY MANUFACTURER'S GUARANTEES FOR THE FOREGOING ITEMS SHALL BE TURNED OVER TO THE MAINTAINING AGENCY FOLLOWING ACCEPTANCE OF THE EQUIPMENT.

THE COST OF GUARANTEEING THE TRAFFIC CONTROL SYSTEM WILL BE INCIDENTAL TO AND INCLUDED IN THE CONTRACT UNIT PRICE OF THE VARIOUS ITEMS MAKING UP THE SYSTEM.

VEHICLE DETECTION LOCATION

THE LOCATION OF THE RADAR UNITS SHOWN IN THE PLANS ARE FOR REPRESENTATIONAL PURPOSES ONLY. THE FINAL LOCATION OF THE VEHICLE DETECTION UNITS WILL BE DETERMINED IN THE FIELD BY THE MANUFACTURER, TRAFFIC ENGINEER, AND CITY.

DETECTION MAINTENANCE

IF VEHICLE DETECTION BECOMES DISABLED THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE CITY.

IF THE LOSS OF VEHICLE DETECTION IS KNOWN PRIOR TO THE START OF CONSTRUCTION, IT SHALL BE DISCUSSED AT THE PRECONSTRUCTION MEETING. AT SUCH TIME, THE CITY SHALL ADVISE THE CONTRACTOR ON THE APPROPRIATE ACTION TO RECTIFY ANY LOSS OF VEHICLE DETECTION. THIS MAY INCLUDE PLACING THE TRAFFIC SIGNAL ON MINIMUM OR MAXIMUM RECALL, MODIFYING THE MINIMUM GREEN TIMES, AND REMOVING THE MALFUNCTIONING DETECTION FROM SERVICE. WHERE NONINTRUSIVE DETECTION (I.E. VIDEO, RADAR) ALREADY EXISTS, THE CONTRACTOR SHALL INSURE THAT DETECTION IS OPERATING AND MAINTAINED BY RECONFIGURING THE DETECTION UNITS ACCORDINGLY DURING ALL CONSTRUCTION PHASES. THIS IS TO AVOID THE SIGNAL FROM MAXING OUT THE EFFECTED SIGNAL PHASE AND CREATING UNNECESSARY DELAYS.

LOCATIONS WHERE NON-INTRUSIVE DETECTION (RADAR) IS PROPOSED AND THE EXISTING VEHICLE DETECTION IS TO BE ABANDONED, THE NON-INTRUSIVE VEHICLE DETECTION SHALL BE INSTALLED, CONFIGURED, INSPECTED BY THE CITY, AND MADE FULLY FUNCTIONAL PRIOR TO THE EXISTING DETECTION BEING DISABLED. THE CONTRACTOR SHALL CONTINUE TO MAINTAIN AND MODIFY THE DETECTION UNTIL FINAL ACCEPTANCE OF THE TRAFFIC SIGNAL. THIS IS TO ENSURE VEHICLE DETECTION REMAINS FULLY FUNCTIONAL THROUGHOUT CONSTRUCTION.

WORK INSPECTION

THE CONTRACTOR SHALL PROVIDE THE CITY WITH 72 HOUR NOTICE OF ANY SIGNAL WORK TO BE PERFORMED AT THE INTERSECTION SITE(S) SO THAT INSPECTION SERVICES CAN BE SUPPLIED.

ITEM 632 SIGNALIZATION, MISC.: UNLASH AND RELASH MESSENGER WIRE

THE CONTRACTOR SHALL REMOVE EXISTING MESSENGER WIRE LASHING RODS AND REINSTALL THEM AS NECESSARY FOR THE INSTALLATION OF ANY NEW CABLES ON THE EXISTING INTERSECTION SIGNAL SPANS. THE CABLES SHALL ENTER THE EXISTING STRAIN POLE THROUGH THE POLE CABLE ENTRANCE FITTING AND USE THE EXISTING CONDUIT SYSTEM TO GET TO THE CONTROLLER CABINET. THE NEW CABLES SHALL BE SUPPORTED BY A NEW CABLE SUPPORT ASSEMBLY AT THE TOP OF THE STRAIN POLE.

THE NEW SIGNAL CABLES SHALL BE BID BY SEPARATE BID ITEMS SPECIFIED IN THESE PLANS.

PAYMENT FOR ITEM 632 SIGNALIZATION MISC.: UNLASH AND RELASH MESSENGER WIRE, SHALL BE MADE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIALS, CABLE SUPPORT ASSEMBLIES AND EQUIPMENT TO INSTALL NEW CABLES ON EXISTING SIGNAL SPAN WIRE INSTALLATIONS.

ITEM 633 CONTROLLER UNIT, TYPE ASC/3-2100, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ODOT CMS 633 AND 733, THE CONTROLLER EQUIPMENT SHALL BE AN **ECONOLITE ASC/3-2100** WITH INTERNAL TRANSCEIVER MODULE. THE CONTROLLER SHALL BE FURNISHED WITH THE MOST RECENT SOFTWARE AND PROVIDE ALL FEATURES OF THE LATEST MODEL AVAILABLE.

THE CONTROLLER UNITS SHALL BE FURNISHED IN ACCORDANCE WITH NEMA TS2 TYPE 2 (1992) STANDARDS, AND SHALL BE CONNECTED TO AND MADE FULLY FUNCTIONAL WITH ALL PROPOSED EQUIPMENT (INCLUDING BUT NOT LIMITED TO RADAR DETECTION UNITS, CONFLICT MONITOR OR MALFUNCTION MANAGEMENT UNIT (MMU), AND ALL OTHER CABINET EQUIPMENT, COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROGRAMMING THE CONTROLLER TO MATCH THE EXISTING CONTROLLER'S SETTINGS.

THE CONTROLLER UNIT SHALL INCLUDE ALL NECESSARY ETHERNET COMMUNICATION AND TELEMETRY MODULES REQUIRED FOR PROPER COMMUNICATION AND OPERATION IN A TRAFFIC RESPONSIVE CLOSED LOOP SYSTEM.

THE CONTRACTOR SHALL FURNISH AND INSTALL A FULLY MANAGED ETHERNET SWITCH THAT PROVIDES DUAL FIBER OPTICAL ETHERNET (10/100/1000 BASE T) PORTS USING INDUSTRY STANDARD FIBER OPTIC CONNECTORS AND FAST ETHERNET (10/100/1000BASE-TX) PORTS. THE TRANSCEIVER SHALL OPERATE ON 120VAC, 10 WATTS AND SHALL MEET OR EXCEED NEMA TS2 ENVIRONMENTAL REQUIREMENTS.

THE CONTROLLER TO BE INSTALLED SHALL BE PRE-TESTED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER FOR INSTALLATION AT THE DESIGNATED INTERSECTION.

REPAIRS/CORRECTIONS, IF REQUIRED, SHALL BE MADE BY THE CONTRACTOR AND RECORDED BEFORE DELIVERY. THE ENGINEER SHALL ALSO BE NOTIFIED OF ANY PROBLEMS. THE CONTROLLER IS TO OPERATE WITHOUT PROBLEMS ON MINIMUM RECALL OF ALL MINOR PHASES FOR 48 HOURS WITH FULL LOAD ON EACH OUTPUT (NOTE THAT TESTING ALSO REQUIRES OPERATION WITH DETECTORS IN A NO CALL PLAN CALL TO MAXIMUM CONFIGURATION).

A WRITTEN REPORT STATING THE CONTROLLER INTERSECTION NAME, DATE AND TIME OF TEST, SIGNED OFF BY THE TECHNICIAN WHO PERFORMED THE TEST, SHALL BE SUBMITTED TO THE ENGINEER UPON SUCCESSFUL COMPLETION OF THE ABOVE TESTS. THE SUCCESSFUL TESTING SHALL BE DEMONSTRATED TO THE ENGINEER PRIOR TO INSTALLATION IF REQUESTED. THE COST FOR THE CONTROLLER TESTING SHALL BE INCLUDED IN THE PRICE OF THE CONTROLLER FURNISHED COMPLETE.

THE CONTROLLER AND ALL RELATED COMPONENTS SHALL BE IN PERFECT WORKING ORDER AND READY FOR INSTALLATION/OPERATION AT THE SPECIFIED INTERSECTION(S) AS A RESULT OF THE WORK DESCRIBED IN THIS ITEM.

WARRANTY
THE CONTROLLER SHALL BE WARRANTED FOR DEFECTS IN MATERIAL AND WORKMANSHIP FOR A MINIMUM OF FIVE (5) YEARS. THE WARRANTY SHALL INCLUDE SOFTWARE UPDATES AND SUPPORT FOR THE ENTIRE WARRANTY PERIOD.

DOCUMENTATION

TWO (2) COMPLETE SETS OF THE FOLLOWING SHALL BE PROVIDED IN THE CABINET:

- USERS MANUALS
- DEVICE PROGRAMMING MANUALS
- WIRING DIAGRAMS AND PARTS LISTS
- INSTALLATION AND DIAGNOSTICS MANUALS
- SOFTWARE AND FIRMWARE UPDATES SHALL BE ACCOMPANIED BY COMPLETE DOCUMENTATION THAT REFERENCES AN UPGRADE VERSION, AND PROVIDES A LIST OF PROBLEMS RESOLVED WITH THE UPGRADE (IF APPLICABLE). ALL FUNCTIONS, FEATURES, AND CAPABILITIES NOT ADDRESSED SHALL OPERATE AS INTENDED BEFORE THE UPGRADE WAS IMPLEMENTED.

PAYMENT FOR ITEM 633 CONTROLLER UNIT, TYPE ASC/3-2100, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK, SO AS TO MAKE THE WHOLE INTERSECTION FUNCTION AND WORK AS INTENDED BY DESIGN.

Old Bid sheet, see next with Addendum No.1 Revisions

ITEM 804 SPLICE ENCLOSURE, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ODOT SPECIFICATION 804, THE FOLLOWING REQUIREMENTS SHALL APPLY:

A RING CUT METHODOLOGY SHALL BE USED. THE CONTRACTOR IS TO CONTACT TIPP CITY I.T. DEPARTMENT FOR VERIFICATION OF FIBER TUBES/COLORS IN USE ON THE EXISTING FIBER OPTIC AERIAL SPAN AT THE POINT OF CONNECTION FOR THE SPLICE ENCLOSURE. EXISTING FIBER TUBES/COLORS ALREADY IN USE ARE NOT TO BE CUT OR DAMAGED DURING THE RING CUT AND SPLICE ENCLOSURE PLACEMENT. THE CONTRACTOR IS TO PAY FOR ANY DAMAGES TO THE EXISTING FIBER THAT OCCURS DURING THE PROPOSED WORK.

THE SPLICE KIT ENCLOSURE SHALL BE TE CONNECTIVITY FOSC 600 OR CITY APPROVED EQUAL.

ADDITIONALLY, THIS BID ITEM SHALL INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT REQUIRED TO REMOVE THE EXISTING FIBER LINE AT THE NEAREST SLACK SPAN AND REPLACE WITH NEW EQUIPMENT OR EXISTING EQUIPMENT, AS APPROVED BY THE CITY (I.E. SNOW-SHOES, ETC.).

THE CITY IS TO RECEIVE COPIES OF ALL FIBER OPTIC TESTING.

PAYMENT FOR ITEM 804 SPLICE ENCLOSURE, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 804 FIBER OPTIC CABLE TESTING, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ODOT SPECIFICATION 804, THE FOLLOWING REQUIREMENTS SHALL APPLY:

THE CONTRACTOR SHALL PROVIDE 2 COMPLETE SETS OF "AS-BUILT" DRAWINGS TO THE CITY WITHIN 30 DAYS OF PROJECT COMPLETION. WHILE INSTALLING AND TESTING THE INSTALLATION, THE CONTRACTOR IS TO MARK PLANS WITH ANY FIELD CHANGES THAT WERE INITIATED DURING CONSTRUCTION. THESE ARE COMMONLY REFERRED TO AS "AS-BUILTS".

AS-BUILT DOCUMENTATION SHALL INCLUDE THE FOLLOWING INFORMATION:

1. PLAN SHEETS WITH THE LOCATION OF CONDUIT IN ROADWAY HAND MARKED.
2. PLAN SHEETS WITH THE SIZES OF CONDUIT HAND MARKED.
3. PLAN SHEETS WITH THE LOCATIONS OF ALL PULL BOXES AND SPLICES HAND MARKED.
4. PLAN SHEETS WITH THE SIZES OF FIBER OPTIC CABLES HAND MARKED.
5. THE PRE-INSTALLATION ON-REEL ATTENUATION LOSS OTDR TRACES OF EACH CABLE FIBER IN db/km SHOWING CABLE/SHEATH LENGTH (NOT OPTICAL FIBER LENGTH), 2-POINT db LOSS, ATTENUATION IN db/km, FIBER TYPE, WAVELENGTH USED, PULSE WIDTH SELECTION, AND FIBER/CABLE IDENTIFICATION. OTDR TRACES SHALL BE HARD COPY AND ELECTRONIC.
6. THE INSTALLED ATTENUATION LOSS OTDR TRACES OF EACH CABLE FIBER IN db/km SHOWING CALBE.SHEATH LENGTH (NOT OPTICAL FIBER LENGTH), 2-POINT db LOSS, ATTENUATION IN db/km, FIBER TYPE, WAVELENGTH USED, PULSE WIDTH SELECTION, AND FIBER/CABLE IDENTIFICATION. OTDR TRACES SHALL BE HARD COPY AND ELECTRONIC.

ANY LINK THAT FAILS THE ABOVE TESTS SHALL BE RESPLICED OR REPLACED UNTIL THESE REQUIREMENTS ARE MET. THE COST OF RESPLICING, REPLACING, AND RETESTING CABLE SHALL BE AT THE CONTRACTOR'S EXPENSE.

PAYMENT FOR ITEM 804 FIBER OPTIC CABLE TESTING, AS PER PLAN FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 804 FIBER OPTIC CABLE, MISC.: FIBER OPTIC SPLICE KITS, AS PER PLAN

A FIBER OPTIC SPLICE KIT SHALL INCLUDE EQUIPMENT AND LABOR TO INSTALL ANY AND ALL NECESSARY ITEMS REQUIRED FOR THE CONNECTIVITY OF A DEVICE TO THE FIBER NETWORK VIA CORE-ALIGNED FUSION SPLICE OF A DROP CABLE TO THE NETWORK TRUNK OR SUB-TRUNK CABLE. THE DROP KIT IS AN AGGREGATION OF ANY AND ALL PARTS AND LABOR NEEDED FOR THIS CONNECTION INCLUDING, BUT NOT LIMITED TO THE FOLLOWING:

- INSTALLING FIBER OPTIC SPLICE ENCLOSURE (AERIAL OR UNDERGROUND) SPLICE TRAY OR TRAY
- GROUND MECHANISM
- DEVICES NECESSARY TO STORE EXCESS CABLE SLACK
- CORE ALIGNED FUSION SPLICING AS SPECIFIED
- AERIAL MOUNTING
- FITTING DROP CABLES WITH APPROPRIATE CONNECTORS
- CONNECTING CONTROL DEVICE TO FIBER OPTIC NETWORK
- STORING CABLE SLACK
- 6 - FIBER TERMINATION PANEL

THE DROP CABLE IS A SEPARATE BID QUANTITY ITEM WHICH IS ALSO NECESSARY FOR COMPLETION OF A DROP KIT INSTALLATION BUT IS LISTED SEPARATELY FOR BETTER ESTIMATING. DROP KITS ARE NOT NECESSARY WHERE NO TRUNK CABLE OPENING IS REQUIRED.

PAYMENT FOR ITEM 804 FIBER OPTIC CABLE, MISC.: FIBER OPTIC SPLICE KITS, AS PER PLAN FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

FIBER OPTIC INSTALLATION NOTE

CONTRACTOR SHALL COORDINATE ALL FIBER INSTALLATION WITH THE CITY PRIOR TO INSTALLATION. ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THE INSTALLATION AND PROVE TO THE CITY THAT THE SIGNAL IS COMMUNICATING WITH THE CITY'S CENTRAL SERVER PROPERLY SHALL BE INCLUDED IN ALL ITEM 804 BID ITEMS LISTED ON SHEET 51.



SIDNEY, OHIO 937-497-0200
LOVELAND, OHIO 513-239-8554
PORTLAND, INDIANA 766-766-2300
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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
S. CR-25A AND EVANSTON ROAD TRAFFIC SIGNAL MODIFICATION PLAN

REVISIONS:
FILE NAME MIATCH1406TNA
DRAWN BY BHB
CHECKED BY JSP
PROJECT No. MIATCH1406
DATE 12-19-2016
SHEET NUMBER 50 OF 50

ODOT STANDARD CONSTRUCTION DRAWINGS

DRAWINGS IN THESE PLANS SHALL BE CONSIDERED AS REFERENCE TO ITEMS 625, 630, 631, 632, 633, 725, 730, 731, 732, AND 733 RESPECTIVELY.

ALL MATERIALS MUST BE IN COMPLIANCE WITH CONTRACT SPECIFICATIONS UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL WORK AND MATERIALS NOT SPECIFICALLY REFERENCED IN THE CONTRACT SHALL MEET OR EXCEED THE REQUIREMENTS OF:

OHIO DEPARTMENT OF TRANSPORTATION, CONSTRUCTION AND MATERIAL SPECIFICATIONS (2016)

THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (2012)

DETECTOR LOOP DEACTIVATION AND ACTIVATION

THE CONTRACTOR SHALL DEACTIVATE THE SOUTHBOUND LOOP DETECTORS ON CR-25A DURING THE CONSTRUCTION PHASE 1 OF THIS PROJECT. DURING CONSTRUCTION PHASE 2, THE EASTBOUND AND WESTBOUND LOOP DETECTORS SHALL BE DEACTIVATED AND THE NEW RADAR UNIT FOR THE SOUTHBOUND CR-25A TRAFFIC SHALL BE TURNED ON, TESTED AND ACTIVATED. BEFORE EVANSTON ROAD IS OPENED TO TRAFFIC DURING CONSTRUCTION PHASE 2, THE EASTBOUND AND WESTBOUND RADAR UNITS SHALL BE TURNED ON, TESTED AND ACTIVATED. ALL LOOP DETECTOR EQUIPMENT TO BE REMOVED SHALL BE GIVEN UNDAMAGED TO THE CITY. THIS COST SHALL BE INCIDENTAL TO AND INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

REMOVAL OF EX. PULL BOXES AND ELECTRICAL CONDUIT

THE CONTRACTOR SHALL REMOVE ALL PULL BOXES AND ELECTRICAL CONDUIT AT THE S. CR25-A AND EVANSTON ROAD TRAFFIC SIGNAL THAT ARE ABANDONED DUE TO THE REMOVAL OF THE ASSOCIATED LOOP DETECTORS. THIS COST SHALL BE INCLUDED IN ITEM 202, REMOVED, AS PER PLAN BID ITEM.

GUARANTEE

THE CONTRACTOR SHALL GUARANTEE THAT THE TRAFFIC CONTROL SYSTEMS INSTALLED AS PART OF THIS CONTRACT SHALL OPERATE SATISFACTORILY FOR A PERIOD OF 120 DAYS FOLLOWING COMPLETION OF THE 10-DAY PERFORMANCE TEST. IN THE EVENT OF UNSATISFACTORY OPERATION THE CONTRACTOR SHALL CORRECT FAULTY INSTALLATIONS, MAKE REPAIRS AND REPLACE DEFECTIVE PARTS WITH NEW PARTS OF EQUAL OR BETTER QUALITY. EQUIPMENT, MATERIAL AND LABOR COSTS INCURRED IN CORRECTING AN UNSATISFACTORY OPERATION SHALL BE BORNE BY THE CONTRACTOR.

THE GUARANTEE SHALL COVER THE FOLLOWING ITEMS OF THE TRAFFIC CONTROL SYSTEM: RADAR SENSORS AND ASSOCIATED EQUIPMENT.

CUSTOMARY MANUFACTURER'S GUARANTEES FOR THE FOREGOING ITEMS SHALL BE TURNED OVER TO THE MAINTAINING AGENCY FOLLOWING ACCEPTANCE OF THE EQUIPMENT.

THE COST OF GUARANTEEING THE TRAFFIC CONTROL SYSTEM WILL BE INCIDENTAL TO AND INCLUDED IN THE CONTRACT UNIT PRICE OF THE VARIOUS ITEMS MAKING UP THE SYSTEM.

VEHICLE DETECTION LOCATION

THE LOCATION OF THE RADAR UNITS SHOWN IN THE PLANS ARE FOR REPRESENTATIONAL PURPOSES ONLY. THE FINAL LOCATION OF THE VEHICLE DETECTION UNITS WILL BE DETERMINED IN THE FIELD BY THE MANUFACTURER, TRAFFIC ENGINEER, AND CITY.

DETECTION MAINTENANCE

IF VEHICLE DETECTION BECOMES DISABLED THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE CITY.

IF THE LOSS OF VEHICLE DETECTION IS KNOWN PRIOR TO THE START OF CONSTRUCTION, IT SHALL BE DISCUSSED AT THE PRECONSTRUCTION MEETING. AT SUCH TIME, THE CITY SHALL ADVISE THE CONTRACTOR ON THE APPROPRIATE ACTION TO RECTIFY ANY LOSS OF VEHICLE DETECTION. THIS MAY INCLUDE PLACING THE TRAFFIC SIGNAL ON MINIMUM OR MAXIMUM RECALL, MODIFYING THE MINIMUM GREEN TIMES, AND REMOVING THE MALFUNCTIONING DETECTION FROM SERVICE. WHERE NONINTRUSIVE DETECTION (I.E. VIDEO, RADAR) ALREADY EXISTS, THE CONTRACTOR SHALL INSURE THAT DETECTION IS OPERATING AND MAINTAINED BY RECONFIGURING THE DETECTION UNITS ACCORDINGLY DURING ALL CONSTRUCTION PHASES. THIS IS TO AVOID THE SIGNAL FROM MAXING OUT THE EFFECTED SIGNAL PHASE AND CREATING UNNECESSARY DELAYS.

LOCATIONS WHERE NON-INTRUSIVE DETECTION (RADAR) IS PROPOSED AND THE EXISTING VEHICLE DETECTION IS TO BE ABANDONED, THE NON-INTRUSIVE VEHICLE DETECTION SHALL BE INSTALLED, CONFIGURED, INSPECTED BY THE CITY, AND MADE FULLY FUNCTIONAL PRIOR TO THE EXISTING DETECTION BEING DISABLED. THE CONTRACTOR SHALL CONTINUE TO MAINTAIN AND MODIFY THE DETECTION UNTIL FINAL ACCEPTANCE OF THE TRAFFIC SIGNAL. THIS IS TO ENSURE VEHICLE DETECTION REMAINS FULLY FUNCTIONAL THROUGHOUT CONSTRUCTION.

WORK INSPECTION

THE CONTRACTOR SHALL PROVIDE THE CITY WITH 72 HOUR NOTICE OF ANY SIGNAL WORK TO BE PERFORMED AT THE INTERSECTION SITE(S) SO THAT INSPECTION SERVICES CAN BE SUPPLIED.

ITEM 632 SIGNALIZATION, MISC.: UNLASH AND RELASH MESSENGER WIRE

THE CONTRACTOR SHALL REMOVE EXISTING MESSENGER WIRE LASHING RODS AND REINSTALL THEM AS NECESSARY FOR THE INSTALLATION OF ANY NEW CABLES ON THE EXISTING INTERSECTION SIGNAL SPANS. THE CABLES SHALL ENTER THE EXISTING STRAIN POLE THROUGH THE POLE CABLE ENTRANCE FITTING AND USE THE EXISTING CONDUIT SYSTEM TO GET TO THE CONTROLLER CABINET. THE NEW CABLES SHALL BE SUPPORTED BY A NEW CABLE SUPPORT ASSEMBLY AT THE TOP OF THE STRAIN POLE.

THE NEW SIGNAL CABLES SHALL BE BID BY SEPARATE BID ITEMS SPECIFIED IN THESE PLANS.

PAYMENT FOR ITEM 632 SIGNALIZATION MISC.: UNLASH AND RELASH MESSENGER WIRE, SHALL BE MADE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIALS, CABLE SUPPORT ASSEMBLIES AND EQUIPMENT TO INSTALL NEW CABLES ON EXISTING SIGNAL SPAN WIRE INSTALLATIONS.

ITEM 633 CONTROLLER UNIT, TYPE COBALT, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ODOT CMS 633 AND 733, THE CONTROLLER EQUIPMENT SHALL BE AN **ECONOLITE COBALT** WITH INTERNAL TRANSCEIVER MODULE. THE CONTROLLER SHALL BE FURNISHED WITH THE MOST RECENT SOFTWARE AND PROVIDE ALL FEATURES OF THE LATEST MODEL AVAILABLE.

THE CONTROLLER UNITS SHALL BE FURNISHED IN ACCORDANCE WITH NEMA TS2 TYPE 2 (1992) STANDARDS, AND SHALL BE CONNECTED TO AND MADE FULLY FUNCTIONAL WITH ALL PROPOSED EQUIPMENT (INCLUDING BUT NOT LIMITED TO RADAR DETECTION UNITS, CONFLICT MONITOR OR MALFUNCTION MANAGEMENT UNIT (MMU), AND ALL OTHER CABINET EQUIPMENT, COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROGRAMMING THE CONTROLLER TO MATCH THE EXISTING CONTROLLER'S SETTINGS.

THE CONTROLLER UNIT SHALL INCLUDE ALL NECESSARY ETHERNET COMMUNICATION AND TELEMETRY MODULES REQUIRED FOR PROPER COMMUNICATION AND OPERATION IN A TRAFFIC RESPONSIVE CLOSED LOOP SYSTEM.

THE CONTRACTOR SHALL FURNISH AND INSTALL A FULLY MANAGED ETHERNET SWITCH THAT PROVIDES DUAL FIBER OPTICAL ETHERNET (10/100/1000 BASE T) PORTS USING INDUSTRY STANDARD FIBER OPTIC CONNECTORS AND FAST ETHERNET (10/100/1000BASE-TX) PORTS. THE TRANSCEIVER SHALL OPERATE ON 120VAC, 10 WATTS AND SHALL MEET OR EXCEED NEMA TS2 ENVIRONMENTAL REQUIREMENTS.

THE CONTROLLER TO BE INSTALLED SHALL BE PRE-TESTED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER FOR INSTALLATION AT THE DESIGNATED INTERSECTION.

REPAIRS/CORRECTIONS, IF REQUIRED, SHALL BE MADE BY THE CONTRACTOR AND RECORDED BEFORE DELIVERY. THE ENGINEER SHALL ALSO BE NOTIFIED OF ANY PROBLEMS. THE CONTROLLER IS TO OPERATE WITHOUT PROBLEMS ON MINIMUM RECALL OF ALL MINOR PHASES FOR 48 HOURS WITH FULL LOAD ON EACH OUTPUT (NOTE THAT TESTING ALSO REQUIRES OPERATION WITH DETECTORS IN A NO CALL CAN CALL TO MAXIMUM CONFIGURATION).

A WRITTEN REPORT STATING THE CONTROLLER INTERSECTION NAME, DATE AND TIME OF TEST, SIGNED OFF BY THE TECHNICIAN WHO PERFORMED THE TESTS, SHALL BE SUBMITTED TO THE ENGINEER UPON SUCCESSFUL COMPLETION OF THE ABOVE TESTS. THE SUCCESSFUL TESTING SHALL BE DEMONSTRATED TO THE ENGINEER PRIOR TO INSTALLATION IF REQUESTED. THE COST FOR THE CONTROLLER TESTING SHALL BE INCLUDED IN THE PRICE OF THE CONTROLLER FURNISHED COMPLETE.

THE CONTROLLER AND ALL RELATED COMPONENTS SHALL BE IN PERFECT WORKING ORDER AND READY FOR INSTALLATION/OPERATION AT THE SPECIFIED INTERSECTION(S) AS A RESULT OF THE WORK DESCRIBED IN THIS ITEM.

WARRANTY
THE CONTROLLER SHALL BE WARRANTED FOR DEFECTS IN MATERIAL AND WORKMANSHIP FOR A MINIMUM OF FIVE (5) YEARS. THE WARRANTY SHALL INCLUDE SOFTWARE UPDATES AND SUPPORT FOR THE ENTIRE WARRANTY PERIOD.

DOCUMENTATION

TWO (2) COMPLETE SETS OF THE FOLLOWING SHALL BE PROVIDED IN THE CABINET:

- USERS MANUALS
- DEVICE PROGRAMMING MANUALS
- WIRING DIAGRAMS AND PARTS LISTS
- INSTALLATION AND DIAGNOSTICS MANUALS
- SOFTWARE AND FIRMWARE UPDATES SHALL BE ACCOMPANIED BY COMPLETE DOCUMENTATION THAT REFERENCES AN UPGRADE VERSION, AND PROVIDES A LIST OF PROBLEMS RESOLVED WITH THE UPGRADE (IF APPLICABLE). ALL FUNCTIONS, FEATURES, AND CAPABILITIES NOT ADDRESSED SHALL OPERATE AS INTENDED BEFORE THE UPGRADE WAS IMPLEMENTED.

PAYMENT FOR ITEM 633 CONTROLLER UNIT, TYPE COBALT, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK, SO AS TO MAKE THE WHOLE INTERSECTION FUNCTION AND WORK AS INTENDED BY DESIGN.

ITEM 804 SPLICE ENCLOSURE, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ODOT SPECIFICATION 804, THE FOLLOWING REQUIREMENTS SHALL APPLY:

A RING CUT METHODOLOGY SHALL BE USED. THE CONTRACTOR IS TO CONTACT TIPP CITY I.T. DEPARTMENT FOR VERIFICATION OF FIBER TUBES/COLORS IN USE ON THE EXISTING FIBER OPTIC AERIAL SPAN AT THE POINT OF CONNECTION FOR THE SPLICE ENCLOSURE. EXISTING FIBER TUBES/COLORS ALREADY IN USE ARE NOT TO BE CUT OR DAMAGED DURING THE RING CUT AND SPLICE ENCLOSURE PLACEMENT. THE CONTRACTOR IS TO PAY FOR ANY DAMAGES TO THE EXISTING FIBER THAT OCCURS DURING THE PROPOSED WORK.

THE SPLICE KIT ENCLOSURE SHALL BE TE CONNECTIVITY FOSC 600 OR CITY APPROVED EQUAL.

ADDITIONALLY, THIS BID ITEM SHALL INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT REQUIRED TO REMOVE THE EXISTING FIBER LINE AT THE NEAREST SLACK SPAN AND REPLACE WITH NEW EQUIPMENT OR EXISTING EQUIPMENT, AS APPROVED BY THE CITY (I.E. SNOW-SHOES, ETC.).

THE CITY IS TO RECEIVE COPIES OF ALL FIBER OPTIC TESTING.

PAYMENT FOR ITEM 804 SPLICE ENCLOSURE, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 804 FIBER OPTIC CABLE TESTING, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ODOT SPECIFICATION 804, THE FOLLOWING REQUIREMENTS SHALL APPLY:

THE CONTRACTOR SHALL PROVIDE 2 COMPLETE SETS OF "AS-BUILT" DRAWINGS TO THE CITY WITHIN 30 DAYS OF PROJECT COMPLETION. WHILE INSTALLING AND TESTING THE INSTALLATION, THE CONTRACTOR IS TO MARK PLANS WITH ANY FIELD CHANGES THAT WERE INITIATED DURING CONSTRUCTION. THESE ARE COMMONLY REFERRED TO AS "AS-BUILTS".

AS-BUILT DOCUMENTATION SHALL INCLUDE THE FOLLOWING INFORMATION:

1. PLAN SHEETS WITH THE LOCATION OF CONDUIT IN ROADWAY HAND MARKED.
2. PLAN SHEETS WITH THE SIZES OF CONDUIT HAND MARKED.
3. PLAN SHEETS WITH THE LOCATIONS OF ALL PULL BOXES AND SPLICES HAND MARKED.
4. PLAN SHEETS WITH THE SIZES OF FIBER OPTIC CABLES HAND MARKED.
5. THE PRE-INSTALLATION ON-REEL ATTENUATION LOSS OTDR TRACES OF EACH CABLE FIBER IN db/km SHOWING CABLE/SHEATH LENGTH (NOT OPTICAL FIBER LENGTH), 2-POINT db LOSS, ATTENUATION IN db/km, FIBER TYPE, WAVELENGTH USED, PULSE WIDTH SELECTION, AND FIBER/CABLE IDENTIFICATION. OTDR TRACES SHALL BE HARD COPY AND ELECTRONIC.
6. THE INSTALLED ATTENUATION LOSS OTDR TRACES OF EACH CABLE FIBER IN db.km SHOWING CALBE.SHEATH LENGTH (NOT OPTICAL FIBER LENGTH), 2-POINT db LOSS, ATTENUATION IN db/km, FIBER TYPE, WAVELENGTH USED, PULSE WIDTH SELECTION, AND FIBER/CABLE IDENTIFICATION. OTDR TRACES SHALL BE HARD COPY AND ELECTRONIC.

ANY LINK THAT FAILS THE ABOVE TESTS SHALL BE RESPLICED OR REPLACED UNTIL THESE REQUIREMENTS ARE MET. THE COST OF RESPLICING, REPLACING, AND RETESTING CABLE SHALL BE AT THE CONTRACTOR'S EXPENSE.

PAYMENT FOR ITEM 804 FIBER OPTIC CABLE TESTING, AS PER PLAN FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 804 FIBER OPTIC CABLE, MISC.: FIBER OPTIC SPLICE KITS, AS PER PLAN

A FIBER OPTIC SPLICE KIT SHALL INCLUDE EQUIPMENT AND LABOR TO INSTALL ANY AND ALL NECESSARY ITEMS REQUIRED FOR THE CONNECTIVITY OF A DEVICE TO THE FIBER NETWORK VIA CORE-ALIGNED FUSION SPLICE OF A DROP CABLE TO THE NETWORK TRUNK OF SUB-TRUNK CABLE. THE DROP KIT IS AN AGGREGATION OF ANY AND ALL PARTS AND LABOR NEEDED FOR THIS CONNECTION INCLUDING, BUT NOT LIMITED TO THE FOLLOWING:

- INSTALLING FIBER OPTIC SPLICE ENCLOSURE (AERIAL OR UNDERGROUND) SPLICE TRAY OR TRAYS
- GROUND MECHANISM
- DEVICES NECESSARY TO STORE EXCESS CABLE SLACK
- CORE ALIGNED FUSION SPLICING AS SPECIFIED
- AERIAL MOUNTING
- FITTING DROP CABLES WITH APPROPRIATE CONNECTORS
- CONNECTING CONTROL DEVICE TO FIBER OPTIC NETWORK
- STORING CABLE SLACK
- 6 - FIBER TERMINATION PANEL

THE DROP CABLE IS A SEPARATE BID QUANTITY ITEM WHICH IS ALSO NECESSARY FOR COMPLETION OF A DROP KIT INSTALLATION BUT IS LISTED SEPARATELY FOR BETTER ESTIMATING. DROP KITS ARE NOT NECESSARY WHERE NO TRUNK CABLE OPENING IS REQUIRED.

PAYMENT FOR ITEM 804 FIBER OPTIC CABLE, MISC.: FIBER OPTIC SPLICE KITS, AS PER PLAN FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT LUMP SUM BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

FIBER OPTIC INSTALLATION NOTE

CONTRACTOR SHALL COORDINATE ALL FIBER INSTALLATION WITH THE CITY PRIOR TO INSTALLATION. ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THE INSTALLATION AND PROVE TO THE CITY THAT THE SIGNAL IS COMMUNICATING WITH THE CITY'S CENTRAL SERVER PROPERLY SHALL BE INCLUDED IN ALL ITEM 804 BID ITEMS LISTED ON SHEET 51.



SYDNEY, OHIO 937-497-0200
LOVELAND, OHIO 513-239-8554
PORTLAND, INDIANA 260-766-2500
WWW.CHOICEONEENGINEERING.COM

SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
S. CR-25A AND EVANSTON ROAD TRAFFIC SIGNAL MODIFICATION PLAN

REVISIONS:
12-08-2017
ADDENDUM No. 1

FILE NAME
MIATC1406TNA

DRAWN BY
BHB

CHECKED BY
JSP

PROJECT No.
MIATC1406

DATE
12-19-2016

SHEET NUMBER

50 OF 52

ITEM 809 STOP-BAR RADAR DETECTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING A WAVETRONIX SMARTSENSOR MATRIX DETECTION UNIT. THE DETECTION UNIT SHALL INCLUDE THE FOLLOWING:

- POWER SHALL BE PROVIDED FROM THE TRAFFIC CABINET.
- ALL REQUIRED INPUT CARDS SHALL BE INCLUDED IN THE TRAFFIC CABINET AND SHALL BE COMPATIBLE WITH CALTRANS, NEMA TS1 AND NEMA TS2 DETECTOR RACKS. THE CARDS SHALL PROVIDE TRUE PRESENCE DETECTOR CALLS OR CONTACT CLOSURE TO THE TRAFFIC CONTROLLER.
- THE UNIT SHALL BE MOUNTED DIRECTLY TO A POLE, AS RECOMMENDED BY THE MANUFACTURER. CABLE(S) SHALL BE PROVIDED AS REQUIRED AND RECOMMENDED BY THE MANUFACTURER.
- SURGE PROTECTION DEVICES, AS RECOMMENDED BY THE MANUFACTURER SHALL BE INCLUDED BOTH AT THE POLE WHERE THE UNIT IS LOCATED TO PROTECT THE UNIT AND IN THE TRAFFIC CABINET TO PROTECT THE CABINET ELECTRONICS.
- THE MANUFACTURER'S REPRESENTATIVE SHALL BE ON SITE DURING INSTALLATION AND TESTING AND SHALL PROVIDE ONSITE TRAINING ON THE SETUP, OPERATION AND MAINTENANCE OF THE UNIT.
- A SERIAL TO ETHERNET COMMUNICATIONS MODULE AND ETHERNET CABLE (MINIMUM 7 FEET).
- THE POWER SUPPLY AND COMMUNICATION MODULES SHALL BE SECURED TO A SINGLE PANEL THAT CAN BE MOUNTED INTERIOR TO THE TRAFFIC CABINET. THE PANEL SHALL INCLUDE MODULAR-PLUG STYLE CONNECTIONS FOR UP TO FOUR (4) SENSOR CABLES. ADDITIONAL SENSORS MAY BE HARD-WIRED TO THE COMMUNICATION MODULES, AS NECESSARY.
- CONTRACTOR SHALL REMOVE THE EXISTING 4-SLOT DETECTOR RACK AND REPLACE IT WITH A NEW 8 SLOT DETECTOR RACK. COST SHALL BE INCLUDED IN THIS PAY ITEM.
- ALL WORK AND REPROGRAMMING TO BE COMPLETED IN THE CONTROLLER CABINET INCLUDED IN THESE PLANS SHALL BE INCLUDED IN THIS PAY ITEM.

PAYMENT FOR ITEM 809 STOP-BAR RADAR DETECTION, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 809 ADVANCE RADAR DETECTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING A WAVETRONIX SMARTSENSOR ADVANCE DETECTION UNIT (MODEL SS-20DE). THE DETECTION UNIT SHALL INCLUDE THE FOLLOWING:

- DETECTION ZONES SHALL BE ESTABLISHED PER MANUFACTURER'S RECOMMENDATIONS.
- POWER SHALL BE PROVIDED FROM THE TRAFFIC CABINET.
- ALL REQUIRED INPUT CARDS SHALL BE INCLUDED IN THE TRAFFIC CABINET AND SHALL BE COMPATIBLE WITH CALTRANS, NEMA TS1 AND NEMA TS2 DETECTOR RACKS. THE CARDS SHALL PROVIDE TRUE PRESENCE DETECTOR CALLS OR CONTACT CLOSURE TO THE TRAFFIC CONTROLLER.
- THE UNIT SHALL BE MOUNTED DIRECTLY TO A POLE, AS RECOMMENDED BY THE MANUFACTURER. CABLE(S) SHALL BE PROVIDED AS REQUIRED AND RECOMMENDED BY THE MANUFACTURER.
- SURGE PROTECTION DEVICES, AS RECOMMENDED BY THE MANUFACTURER SHALL BE INCLUDED BOTH AT THE POLE WHERE THE UNIT IS LOCATED TO PROTECT THE UNIT AND IN THE TRAFFIC CABINET TO PROTECT THE CABINET ELECTRONICS.
- THE MANUFACTURER'S REPRESENTATIVE SHALL BE ON SITE DURING INSTALLATION AND TESTING AND SHALL PROVIDE ONSITE TRAINING ON THE SETUP, OPERATION AND MAINTENANCE OF THE UNIT.
- A SERIAL TO ETHERNET COMMUNICATIONS MODULE AND ETHERNET CABLE (MIN. 7 FEET).
- THE POWER SUPPLY AND COMMUNICATION MODULES SHALL BE SECURED TO A SINGLE PANEL THAT CAN BE MOUNTED INTERIOR TO THE TRAFFIC CABINET. THE PANEL SHALL INCLUDE MODULAR-PLUG STYLE CONNECTIONS FOR UP TO FOUR (4) SENSOR CABLES. ADDITIONAL SENSORS MAY BE HARD-WIRED TO THE COMMUNICATION MODULES, AS NECESSARY.

PAYMENT FOR ITEM 809 ADVANCE RADAR DETECTION, AS PER PLAN, SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH UNIT, COMPLETE AND IN PLACE INCLUDING ALL REQUIRED CABINET HARDWARE, MOUNTING BRACKETS, CABLES, CONDUIT, CONNECTIONS TESTED AND ACCEPTED, AND ANY OTHER NECESSARY HARDWARE TO ESTABLISH A FULLY FUNCTIONAL DETECTION SYSTEM.

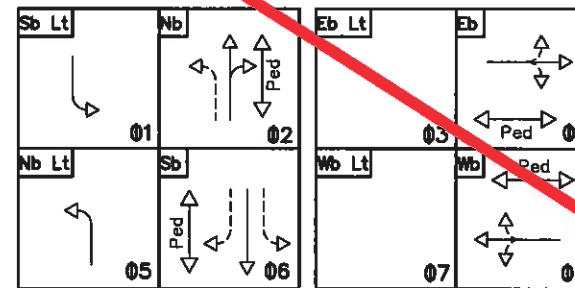
RADAR DETECTION CHART

DETECTION ZONE (ASSOCIATED RADAR UNIT)	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY (SEC)	EXTENSION (SEC)	PURPOSE	DETECTION ZONE LENGTH (FT)
D1A (R2)	SB LT	PRESENCE	1	3	2	THIRD CAR FROM STOP BAR	30
D6A (R1)	SB	PRESENCE	6	-	-	ADVANCE	*
D8A (R3)	WB	PRESENCE	8	12	-	STOP BAR	25
D8B (R3)	WB	PRESENCE	8	3	3	STOP BAR	40

NOTE: ALL DETECTION ZONES SHALL BE CENTERED IN THEIR RESPECTIVE LANES UNLESS OTHERWISE SHOWN.

* SET ADVANCE DETECTION ZONE 400 FT. FROM STOP BAR TO 100' TO STOP BAR.

DETECTION ZONE DISTANCE FOR SOUTHBOUND (D6A)	
FROM STOP LINE TO BEGINNING OF ZONE	400'
FROM STOP LINE TO END OF ZONE	100'
MINIMUM DETECTION VELOCITY	7 MPH



NOTE: FOLLOWS DEFINITIONS IN NEMA TS-1-1994 INCLUDING ALL REVISIONS THERETO.

CONTRACTOR SHALL USE EXISTING CONTROLLER TIMING

SOUTH CR-25A AND EVANSTON RD. TRAFFIC SIGNAL SUMMARY

ITEM	QUAN.	UNIT	DESCRIPTION
825	59	FT.	CONDUIT, 2", 725.051
825	82	FT.	CONDUIT, JACKED OR DRILLED, 725.052, 2"
825	59	FT.	TRENCH
825	2	EACH	PULL BOX, 725.08, 18"
832	1	EACH	CONDUIT RISER, 2" DIAMETER
832	1	LUMP	SIGNALIZATION, MSC.: UNLASH AND RELASH MESSENGER WIRE
833	1	EACH	CONTROLLER UNIT, TYPE ASC/3-2100, AS PER PLAN
833	1	EACH	CONTROLLER WORK PAD
804	1	EACH	FAN-OUT KIT, 6 FIBER
804	192	FT.	DROP CABLE, 6 FIBER
804	2	EACH	FIBER OPTIC PATCH CORD, 1 FIBER
804	1	EACH	FIBER TERMINATION PANEL, 6 FIBER
804	12	EACH	FUSION SPLICE
804	1	EACH	SPLICE ENCLOSURE, AS PER PLAN
804	6	EACH	FIBER OPTIC CONNECTOR
804	1	LS	FIBER OPTIC CABLE TESTING, AS PER PLAN
804	1	LS	FIBER OPTIC CABLE, MISC.: FIBER OPTIC SPLICE KITS, AS PER PLAN
809	1	EACH	ADVANCE RADAR DETECTION, AS PER PLAN
809	2	EACH	STOP-BAR RADAR DETECTION, AS PER PLAN

Old Bid sheet, see next with Addendum No.1 Revisions



SIDNEY, OHIO 45747-0000
LOVELAND, OHIO 513.279.8554
PORTLAND, INDIANA 260.766.2500
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SOUTH CR 25-A RECONSTRUCTION, PH. 2
CITY OF TIPP CITY
S. CR-25A AND EVANSTON ROAD TRAFFIC SIGNAL MODIFICATION PLAN

REVISIONS:

FILE NAME	MIATC1406TMB
DRAWN BY	BHB
CHECKED BY	JSP
PROJECT No.	MIATC1406
DATE	12-19-2016
SHEET NUMBER	51 OF 52

ITEM 809 STOP-BAR RADAR DETECTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING A WAVETRONIX SMARTSENSOR MATRIX DETECTION UNIT. THE DETECTION UNIT SHALL INCLUDE THE FOLLOWING:

- POWER SHALL BE PROVIDED FROM THE TRAFFIC CABINET.
- ALL REQUIRED INPUT CARDS SHALL BE INCLUDED IN THE TRAFFIC CABINET AND SHALL BE COMPATIBLE WITH CALTRANS, NEMA TS1 AND NEMA TS2 DETECTOR RACKS. THE CARDS SHALL PROVIDE TRUE PRESENCE DETECTOR CALLS OR CONTACT CLOSURE TO THE TRAFFIC CONTROLLER.
- THE UNIT SHALL BE MOUNTED DIRECTLY TO A POLE, AS RECOMMENDED BY THE MANUFACTURER. CABLE(S) SHALL BE PROVIDED AS REQUIRED AND RECOMMENDED BY THE MANUFACTURER.
- SURGE PROTECTION DEVICES, AS RECOMMENDED BY THE MANUFACTURER SHALL BE INCLUDED BOTH AT THE POLE WHERE THE UNIT IS LOCATED TO PROTECT THE UNIT AND IN THE TRAFFIC CABINET TO PROTECT THE CABINET ELECTRONICS.
- THE MANUFACTURER'S REPRESENTATIVE SHALL BE ON SITE DURING INSTALLATION AND TESTING AND SHALL PROVIDE ONSITE TRAINING ON THE SETUP, OPERATION AND MAINTENANCE OF THE UNIT.
- A SERIAL TO ETHERNET COMMUNICATIONS MODULE AND ETHERNET CABLE (MINIMUM 7 FEET).
- THE POWER SUPPLY AND COMMUNICATION MODULES SHALL BE SECURED TO A SINGLE PANEL THAT CAN BE MOUNTED INTERIOR TO THE TRAFFIC CABINET. THE PANEL SHALL INCLUDE MODULAR-PLUG STYLE CONNECTIONS FOR UP TO FOUR (4) SENSOR CABLES. ADDITIONAL SENSORS MAY BE HARD-WIRED TO THE COMMUNICATION MODULES, AS NECESSARY.
- CONTRACTOR SHALL REMOVE THE EXISTING 4-SLOT DETECTOR RACK AND REPLACE IT WITH A NEW 8 SLOP DETECTOR RACK. COST SHALL BE INCLUDED IN THIS PAY ITEM.
- ALL WORK AND REPROGRAMMING TO BE COMPLETED IN THE CONTROLLER CABINET INCLUDED IN THESE PLANS SHALL BE INCLUDED IN THIS PAY ITEM.

PAYMENT FOR ITEM 809 STOP-BAR RADAR DETECTION, AS PER PLAN, FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE AT THE CONTRACT EACH BID PRICE AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT REQUIRED TO COMPLETE THIS ITEM OF WORK.

ITEM 809 ADVANCE RADAR DETECTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING A WAVETRONIX SMARTSENSOR ADVANCE DETECTION UNIT (MODEL SS-200E). THE DETECTION UNIT SHALL INCLUDE THE FOLLOWING:

- DETECTION ZONES SHALL BE ESTABLISHED PER MANUFACTURERS RECOMMENDATIONS.
- POWER SHALL BE PROVIDED FROM THE TRAFFIC CABINET.
- ALL REQUIRED INPUT CARDS SHALL BE INCLUDED IN THE TRAFFIC CABINET AND SHALL BE COMPATIBLE WITH CALTRANS, NEMA TS1 AND NEMA TS2 DETECTOR RACKS. THE CARDS SHALL PROVIDE TRUE PRESENCE DETECTOR CALLS OR CONTACT CLOSURE TO THE TRAFFIC CONTROLLER.
- THE UNIT SHALL BE MOUNTED DIRECTLY TO A POLE, AS RECOMMENDED BY THE MANUFACTURER. CABLE(S) SHALL BE PROVIDED AS REQUIRED AND RECOMMENDED BY THE MANUFACTURER.
- SURGE PROTECTION DEVICES, AS RECOMMENDED BY THE MANUFACTURER SHALL BE INCLUDED BOTH AT THE POLE WHERE THE UNIT IS LOCATED TO PROTECT THE UNIT AND IN THE TRAFFIC CABINET TO PROTECT THE CABINET ELECTRONICS.
- THE MANUFACTURER'S REPRESENTATIVE SHALL BE ON SITE DURING INSTALLATION AND TESTING AND SHALL PROVIDE ONSITE TRAINING ON THE SETUP, OPERATION AND MAINTENANCE OF THE UNIT.
- A SERIAL TO ETHERNET COMMUNICATIONS MODULE AND ETHERNET CABLE (MIN. 7 FEET).
- THE POWER SUPPLY AND COMMUNICATION MODULES SHALL BE SECURED TO A SINGLE PANEL THAT CAN BE MOUNTED INTERIOR TO THE TRAFFIC CABINET. THE PANEL SHALL INCLUDE MODULAR-PLUG STYLE CONNECTIONS FOR UP TO FOUR (4) SENSOR CABLES. ADDITIONAL SENSORS MAY BE HARD-WIRED TO THE COMMUNICATION MODULES, AS NECESSARY.

PAYMENT FOR ITEM 809 ADVANCE RADAR DETECTION, AS PER PLAN, SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH UNIT, COMPLETE AND IN PLACE INCLUDING ALL REQUIRED CABINET HARDWARE, MOUNTING BRACKETS, CABLES, CONDUIT, CONNECTIONS TESTED AND ACCEPTED, AND ANY OTHER NECESSARY HARDWARE TO ESTABLISH A FULLY FUNCTIONAL DETECTION SYSTEM.

RADAR DETECTION CHART

DETECTION ZONE (ASSOCIATED RADAR UNIT)	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY (SEC)	EXTENSION (SEC)	PURPOSE	DETECTION ZONE LENGTH (FT)
D1A (R2)	SB LT	PRESENCE	1	3	2	THIRD CAR FROM STOP BAR	30
D6A (R1)	SB	PRESENCE	6	-	-	ADVANCE	*
D8A (R3)	WB	PRESENCE	8	12	-	STOP BAR	25
DBB (R3)	WB	PRESENCE	8	3	3	STOP BAR	40

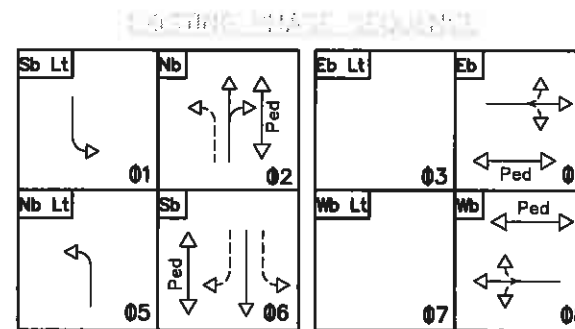
NOTE: ALL DETECTION ZONES SHALL BE CENTERED IN THEIR RESPECTIVE LANES UNLESS OTHERWISE SHOWN.

* SET ADVANCE DETECTION ZONE 400 FT. FROM STOP BAR TO 100' TO STOP BAR.

DETECTION ZONE DISTANCE FOR SOUTHBOUND (D8A)	
FROM STOP LINE TO BEGINNING OF ZONE	400'
FROM STOP LINE TO END OF ZONE	100'
MINIMUM DETECTION VELOCITY	7 MPH

SOUTH CR-25A AND EVANSTON RD. TRAFFIC SIGNAL SUMMARY

ITEM	QUAN.	UNIT	DESCRIPTION
625	59	FT.	CONDUIT, 2", 725.051
625	82	FT.	CONDUIT, JACKED OR DRILLED, 725.052, 2"
625	59	FT.	TRENCH
625	2	EACH	PULL BOX, 725.08, 18"
632	1	EACH	CONDUIT RISER, 2" DIAMETER
632	1	LUMP	SIGNALIZATION, MSC.: UNLASH AND RELASH MESSENGER WIRE
633	1	EACH	CONTROLLER UNIT, TYPE COBALT, AS PER PLAN
633	1	EACH	CONTROLLER WORK PAD
804	1	EACH	FAN-OUT KIT, 6 FIBER
804	192	FT.	DROP CABLE, 6 FIBER
804	2	EACH	FIBER OPTIC PATCH CORD, 1 FIBER
804	1	EACH	FIBER TERMINATION PANEL, 6 FIBER
804	12	EACH	FUSION SPLICE
804	1	EACH	SPLICE ENCLOSURE, AS PER PLAN
804	6	EACH	FIBER OPTIC CONNECTOR
804	1	LS	FIBER OPTIC CABLE TESTING, AS PER PLAN
804	1	LS	FIBER OPTIC CABLE, MISC.: FIBER OPTIC SPLICE KITS, AS PER PLAN
809	1	EACH	ADVANCE RADAR DETECTION, AS PER PLAN
809	2	EACH	STOP-BAR RADAR DETECTION, AS PER PLAN



NOTE: FOLLOWS DEFINITIONS IN NEMA TS-1-1994 INCLUDING ALL REVISIONS THERETO.

TRAFFIC SIGNAL
 CONTRACTOR SHALL USE EXISTING CONTROLLER TIMING



Choice One Engineering
 SIDNEY, OHIO 937-497-0000
 LOVELAND, OHIO 513-239-8554
 PORTLAND, INDIANA 260-766-2500
 WWW.CHOICEONEENGINEERING.COM

SOUTH CR 25-A RECONSTRUCTION, PH. 2
 CITY OF TIPP CITY
 S. CR-25A AND EVANSTON ROAD TRAFFIC SIGNAL MODIFICATION PLAN

REVISIONS:
 02-08-2017
 ADDENDUM No. 1

FILE NAME
 MIATC1406TNS

DRAWN BY
 BHB

CHECKED BY
 JSP

PROJECT No.
 MIATC1406

DATE
 12-19-2016

SHEET NUMBER

51 OF 52

UTILITY OWNERS	
TYPE	NAME AND ADDRESS
ELECTRIC, WATER, STORM & SANITARY	CITY OF TIPP CITY MUNICIPAL BLDG. 280 S. GARBER ST. TIPP CITY, OHIO 45371 937-667-8305
TELEPHONE	FRONTIER COMMUNICATIONS 6484 WESTBROOK RD. CLAYTON, OHIO 45315 937-833-0468
CABLE	TIME WARNER 3891 TURNER RD. DAYTON, OH 45415 937-440-6856 CONTACT: CHRIS GRAVES
GAS	VECTREN 6500 CLYO RD. CENTERVILLE OHIO, 45459 937-312-2533 CONTACT: DON SPECHT
ELECTRIC	DAYTON POWER AND LIGHT CO 1900 DRYDEN ROAD DAYTON, OHIO 45439 JOHN KENTON 937-331-4132 DP&L TRANSMISSION 1900 DRYDEN ROAD DAYTON, OHIO 45439 GREG TOKAR

MIA-TIPP CITY 25A RECONSTRUCTION, PH. 2

CITY OF TIPP CITY
MIAMI COUNTY, OHIO
SEC. 21, T 4 N, R 8 E

PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF THE RECONSTRUCTION OF C.R. 25-A FROM MICHAELS RD. TO EVANSTON RD. CONSTRUCTION CONSISTS OF PAVEMENT, CURBING, THE INSTALLATION OF A NEW STORM SEWER SYSTEM, UTILITY RELOCATIONS, STREET LIGHTS AND RESTORATION OF TRAFFIC SIGNAGE.

BASIS FOR EX. R/W

THE EXISTING RIGHT-OF-WAY WIDTH WERE DETERMINED USING VARIOUS LAND SURVEYS AND SUBDIVISION PLATS AND LISTED AS FOLLOWS.

THE PROPOSED RIGHT-OF-WAY SHALL BE REFERENCED TO THE CENTERLINE OF SURVEY.

BASIS FOR BEARINGS

THE BEARING OF N 00°-15'-20" E ALONG THE CENTERLINE OF COUNTY ROAD 25A AND BEING THE EAST LINE OF THE SOUTHEAST QUARTER OF SECTION 21 WAS BASED ON NAD 83 CORS 96 ADJUSTMENT, GEOID 2003, OHIO SOUTH ZONE, ODOT VRS CORS NETWORK

PERTINENT INFORMATION

REC. P.B. 9, PG. 146
MIA-TIPP CITY 25A RECONSTRUCTION, PH 1

DEEDS AS SHOWN ON R/W DETAIL SHEETS

NOTE:
THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE OBTAINED FROM THE OWNERS OF THE UTILITIES AS REQUIRED BY SECTION 153.64 D.R.C.

I, Wesley D. Goubeaux, P.S. 8254, have conducted a survey of the existing conditions for the City of Tipp City in February, 2012. The results of that survey are contained herein.

The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinate system NAD 83, South Zone using CORS 96 adjustment. The Project Adjustment Factor used for this project is 1.0000545970 and applied to Control Point #109.

I have determined the locations of the existing property lines for property takes contained herein. I have calculated the proposed property lines, Gross Take, Present Roadway Occupied (PRO), Net Take and Net Residues, as well as prepared the legal descriptions necessary to acquire these parcels as shown herein.

As a part of this project, I have set monuments at the proposed Property Corners, Section Corners and other points as shown herein. However, item 604 Monument Assemblies, Item 604 Reference Monuments and Centerline Monuments shall be installed by the construction contractor as specified in the plans. All Centerline Monuments and Right-of-Way Monuments set and/or reset by the contractor's surveyor will include a cap as per Standard Construction Drawing RM-1.1 and bear the surveyor's Ohio registration number and/or name or company name. This work will be done in accordance with OAC 4733-37 as cited below.

All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as Minimum Standards for Boundary Survey in the State of Ohio unless so noted.

The words I and my as used herein are to mean either myself or someone working for me under my direct control or supervision.

By _____
Wesley D. Goubeaux
Surveyor No. 8254 Date _____

LINE CHART		
LINE	BEARING	DIST.
L1	S 00°-56'-31" W	5.20'
L2	N 89°-44'-40" W	5.00'
L3	N 00°-15'-20" E	10.00'
L4	S 89°-44'-40" E	5.00'
L5	N 00°-15'-20" E	48.92'

STATION/OFFSET CHART		
POINT	STATION	OFFSET
A	232+01.42	60.00' LT
B	232+01.42	65.00' LT
C	232+11.42	65.00' LT
D	232+11.42	60.00' LT
E	232+60.34	60.00' LT
F	232+60.36	50.00' LT

NOTE

THIS R/W PLAN IS IN COORDINATION WITH PREVIOUS VERSION OF MIA-TIPP CITY 25A, PHASE 1, RECONSTRUCTION R/W PLANS ORIGINALLY APPROVED BY ODOT AND DATED 6/05/2014.

SUMMARY OF ADDITIONAL RIGHT-OF-WAY REQUIRED

NO. OF STRUCTURES = 0, NO. OF PROPERTY OWNERS = 1, NO. OF TOTAL TAKES = 0, NO. OF PARCELS = 2, P.I.D. NO. 93245

PARCEL NO.	OWNER	OWNERS RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS AND PERSONALTY	AS ACQUIRED	
		BOOK	PAGE								LEFT	RIGHT			BOOK	PAGE
9-WD1	RAY HENDRIX & DANIELLE HENDRIX	20160R-05954		G12-080956	1.213	0.000	0.050	0.000	0.050		1.163		100% LOCAL	ALL 0.050 ACRES OVERLAPS EXISTING UTIL. EASEMENT		

LEGEND

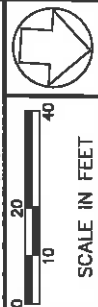
WD = WARRANTY DEED

MONUMENT LEGEND

- o^{IP.F.} IRON PIN FOUND
- o^{R.S.} RAILROAD SPIKE FOUND
- o^{P.K.F.} P.K. NAIL FOUND
- o^{IP.S.} 5/8" IRON PIN WITH CAP SET
- o^{MB} MONUMENT BOX W/IF FOUND

ALL RIGHT-OF-WAY ACQUIRED IN THE NAME OF THE CITY OF TIPP CITY, OHIO UNLESS OTHERWISE SHOWN.

WDG 11/17/16 REVISE PARCEL 9-WD1
REV. DATE DESCRIPTION
DATE OF COMPLETION SEPTEMBER 9, 2016



PID NO. **93245**
R/W DESIGNER BHB
R/W REVIEWER WDG

RIGHT-OF-WAY PLAN

MIA-TIPP CITY 25A RECONSTRUCTION, PH. 2

1 / 1

52
52

