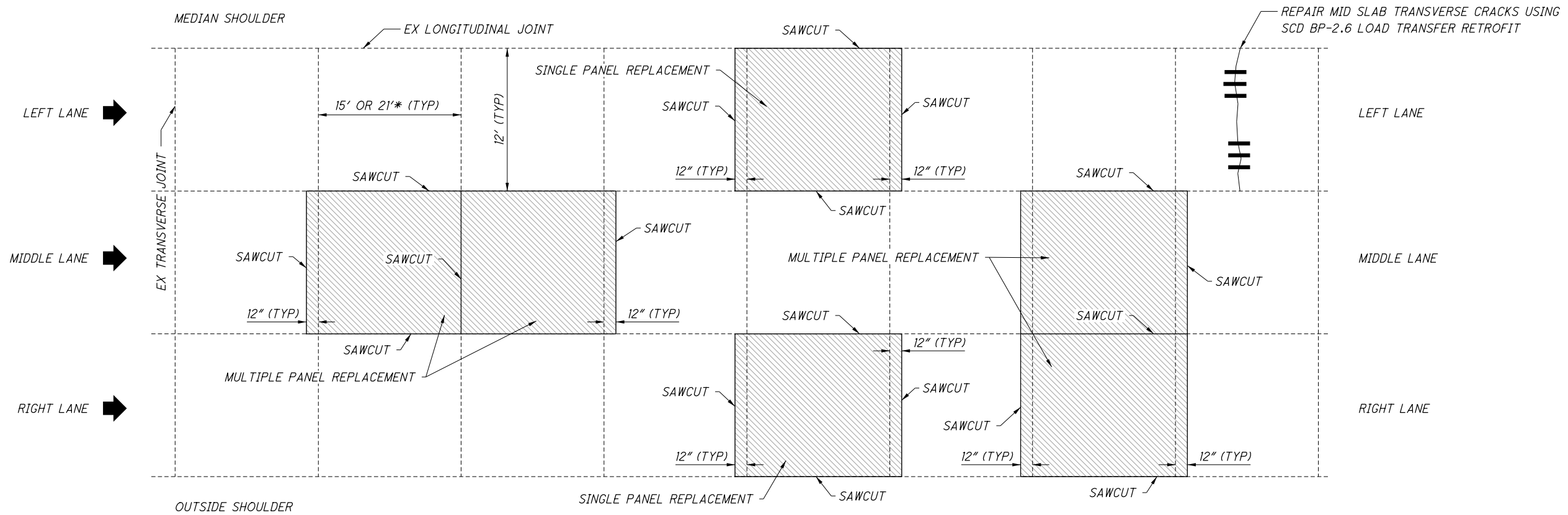
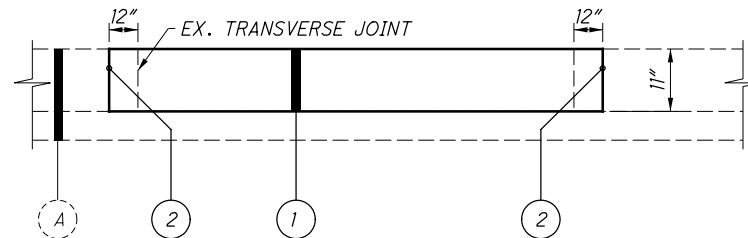


* THE EX. TRANSVERSE JOINTS ON USR 35 ARE SPACED 21' APART WEST OF THE JAMES MCGEE BLVD INTERSECTION AND ARE SPACED 15' APART EAST OF THE JAMES MCGEE BLVD INTERSECTION.



LOAD TRANSFER RETROFIT & PANEL REPLACEMENT (TYPICAL)

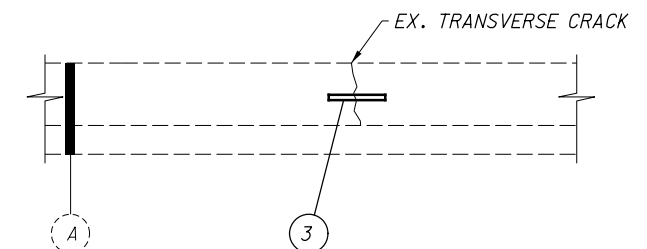
MOT-35-SLM 14.04 TO SLM 15.07



PANEL REPLACEMENT SECTION

REFER TO SCD BP-2.5 (T SHALL MATCH THE EXISTING SURROUNDING SLABS. MIN. 10" THICK)

MULTIPLE PANEL REPLACEMENT - WHERE MULTIPLE FULL CONCRETE PANELS ARE TO BE REPLACED, THE CONTRACTOR SHALL INSTALL JOINTS TO MATCH THE EXISTING ADJACENT PAVEMENT.



RETROFIT DOWEL BAR SECTION

REFER TO SCD BP-2.6

LEGEND

- ① ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC 1 OR CLASS QC MS
- ② ITEM 255 - FULL DEPTH PAVEMENT SAWING
- ③ ITEM 258 - RETROFIT DOWEL BAR

- Ⓐ WEST OF JAMES MCGEE BLVD = EX. 11"± REINFORCED CONCRETE PAVEMENT ON 6" AGGREGATE BASE
- Ⓐ EAST OF JAMES MCGEE BLVD = EX. 10" PLAIN CONCRETE PAVEMENT ON 4" ASPH. CONC. BASE ON 4" AGG. BASE

I:\ProjectData\MOT\109710_US35-1404\Design\Roadway\Sheets\109710_GY001.dgn Sheet 3/25/2022 12:06:59 PM cweiss

ALIGNMENT AND PROFILE

THE WORK PROPOSED BY THIS PROJECT CONSISTS OF REPAIRING THE EXISTING CONCRETE PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

COORDINATION OF WORK:

- MOT-35-14.04 (PID 109710)
- MOT-35-15.07 (PID 108102)
- D07-BP-FY22 (PID 105404)
- MOT-75-(10.44)(10.78) (PID 91606)
- MOT-75-11.25 (PID 101160)

THE CONTRACTOR IS ADVISED THAT ADJACENT CONSTRUCTION PROJECTS WITHIN OR NEAR THE WORK LIMITS OF THIS PLAN MAY IMPACT THE PROJECT SCHEDULE, SEQUENCE OF CONSTRUCTION AND/OR TRAFFIC CONTROL BETWEEN ADJACENT ZONES. THE CONTRACTOR IS REQUIRED TO COORDINATE ALL MAINTENANCE OF TRAFFIC OPERATIONS WITH ADJACENT CONSTRUCTION PROJECTS. COOPERATION WITH THE ENGINEER, INSPECTORS AND ALL OTHER CONTRACTORS ON OR ADJACENT TO THE PROJECT IS REQUIRED PER CMS 105.08.

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC 1, AS PER PLAN

THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.5. ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE BEGINNING WORK. REPAIR AREAS SHALL BE REPLACED TO MATCH THE EXISTING PAVEMENT ELEVATION. NEW CONCRETE SURFACES SHALL BE TEXTURED PER CMS 451.10. THE CONTRACTOR SHALL NOT REMOVE MORE PAVEMENT THAN WHAT CAN BE REPLACED THAT WORK SHIFT.

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN

THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.5. ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE BEGINNING WORK. REPAIR AREAS SHALL BE REPLACED TO MATCH THE EXISTING PAVEMENT ELEVATION. NEW CONCRETE SURFACES SHALL BE TEXTURED PER CMS 451.10. THE CONTRACTOR SHALL NOT REMOVE MORE PAVEMENT THAN WHAT CAN BE REPLACED THAT WORK SHIFT.

ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN

CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DOCUMENT THE LAYOUT OF THE EXISTING PAVEMENT INCLUDING EXISTING ALIGNMENT, PROFILE, PAVEMENT EDGE LOCATIONS AND ELEVATIONS IN A LOG FOR THE IR 75 TO US 35 WESTBOUND RAMP REPAIR AREAS AND SUBMIT TO THE DEPARTMENT FOR ACCEPTANCE. THE DEPARTMENT WILL NOT ALLOW THE CONTRACTOR TO PERFORM ANY PAVEMENT WORK FUNCTIONS UNTIL ACCEPTANCE OF THE SUBMITTED EXISTING PAVEMENT.

THE CONTRACTOR SHALL COORDINATE AND CORRABORATE THE PROPOSED LAYOUT OF ALL PAVEMENT AND APPLICABLE STANDARD CONSTRUCTION DRAWINGS WITH ODOT.

ITEM 258 - RETROFIT DOWEL BAR

THIS PAY ITEM IS TO BE USED TO REPAIR TRANSVERSE CRACKS. THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.6. THE CRACKS TO BE REPAIRED ARE TO BE MARKED BY THE ENGINEER.

PAYMENT FOR THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, MATERIALS, AND INCIDENTALS REQUIRED FOR ITEM 258 RETROFIT DOWEL BAR .

PAVEMENT MARKINGS

THE QUANTITIES PROVIDED IN THE GENERAL SUMMARY ARE INTENDED TO BE USED TO REPLACE THE MARKINGS AND RPMS THAT HAVE BEEN REMOVED OR DAMAGED FROM THE PAVEMENT REPAIRS.

- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6" 642 PAINT = 0.25 MILE
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT = 0.50 MILE
- ITEM 621 - RPM = 20 EACH
- ITEM 621 - RAISED PAVEMENT MARKER REMOVED = 20 EACH
- ITEM 646 - EDGE LINE, 6" = 0.25 MILE
- ITEM 646 - LANE LINE, 6" = 0.50 MILE
- ITEM 646 - CHANNELIZING LINE, 12" = 500 FT
- ITEM 646 - DOTTED LINE, 6" = 200 FT

ITEM 530 SPECIAL - STRUCTURES MISC.: CLEANING OF JOINTS FOR FULL WIDTH OF THE BRIDGE

THE CONTRACTOR SHALL FURNISH ALL MATERIAL, EQUIPMENT, LABOR AND INCIDENTAL ITEMS NECESSARY TO PROPERLY REMOVE AND DISPOSE OF ALL DEBRIS AND OTHER FOREIGN MATERIAL FROM THE EXPANSION JOINTS BY POWER SWEEPING, SCRAPING, ETC. FOLLOWED BY POWER WASHING WITH WATER. THE FOLLOWING PROCEDURE SHALL BE USED:

- 1) REMOVE AND COLLECT LOOSE DEBRIS FROM THE JOINTS PRIOR TO INTRODUCING WATER. CARE SHOULD BE TAKEN WHEN BREAKING UP DEBRIS IN THE JOINTS IN ORDER NOT TO DAMAGE THE ELASTOMERIC SEAL (IF PRESENT).
- 2) SWEEP AND COLLECT SAND, DEBRIS AND SEDIMENT FROM THE JOINTS BY VACUUMING WASH WATER USING A VACUUM TRUCK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE WATER SUPPLY USED DURING THE SWEEPING AND CLEANING OPERATION. THE CONTRACTOR IS RESPONSIBLE FOR THE PROPER DISPOSAL OF ANY DEBRIS COLLECTED DURING OPERATIONS IN ACCORDANCE WITH C&MS 105.16. DISCHARGING TO OR COLLECTING POTABLE WATER FROM STREAMS IS PROHIBITED.

PAYMENT FOR ALL OF THE AFOREMENTIONED WORK WILL BE MADE AT THE CONTRACT BID PRICE PER BRIDGE AS FOLLOWS:

ITEM 530 SPECIAL - STRUCTURE, MISC.: CLEANING OF JOINTS FOR THE FULL WIDTH OF THE BRIDGE 7 EACH

FOR ESTIMATING PURPOSES ONLY, LISTED BELOW ARE THE STRUCTURES AND THE APPROXIMATE LENGTHS OF THE EXPANSION JOINTS THAT ARE TO BE CLEANED:

MOT-35-14.41	=	105 FT
MOT-35-14.73 L	=	134 FT.
MOT-35-14.73 R	=	134 FT.
MOT-35-14.81 L	=	234 FT.
MOT-35-14.81 R	=	230 FT.
MOT-35-14.91 L	=	135 FT.
MOT-35-14.91 R	=	137 FT.
TOTAL	=	1109 FT.

I:\Project+Data\MOT\09710_US35-1404\Design\Roadway\Sheets\09710_GN001.dgn Sheet 3/28/2022 8:23:17 AM cweiss

CALCULATED
TMK
CHECKED
CWW

GENERAL NOTES

MOT - 35 - 14.04

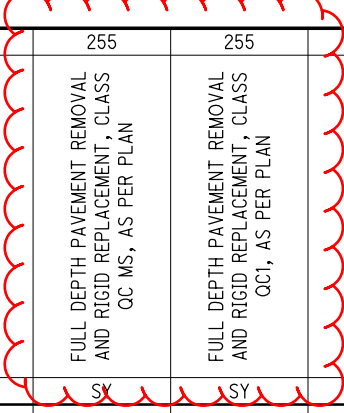
I:\Project+Data\M0T\109710_US35-1404\Design\Roadway\Sheets\109710_GG001.dgn Sheet 3/28/2022 8:23:42 AM cweiss

SHEET NUM.												PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE		
						3	4	5	15	17	23	01/NHS/PV	EXT	TOTAL			NO.			
PAVEMENT																				
													2,400	2,400	255	10011	2,400	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN	3
													2,841	2,841	255	10161	2,841	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN	3
													10,754	10,754	255	20000	10,754	FT	FULL DEPTH PAVEMENT SAWING	
													1,296	1,296	258	10000	1,296	EACH	RETROFIT DOWEL BAR	
													911	911	SPECIAL	45132000	911	FT	PRESSURE RELIEF JOINT, TYPE C	7
TRAFFIC CONTROL																				
						20							20	20	621	00100	20	EACH	RPM	
						20							20	20	621	54000	20	EACH	RAISED PAVEMENT MARKER REMOVED	
						0.25							0.25	0.25	646	10010	0.25	MILE	EDGE LINE, 6"	
						0.5							0.5	0.5	646	10110	0.5	MILE	LANE LINE, 6"	
						500							500	500	646	10310	500	FT	CHANNELIZING LINE, 12"	
						200							200	200	646	20504	200	FT	DOTTED LINE, 6"	
MISCELLANEOUS STRUCTURE																				
						7							7	7	SPECIAL	53000400	7	EACH	STRUCTURES, MISC: CLEANING OF JOINTS FOR THE FULL WIDTH OF THE BRIDGE	3
MAINTENANCE OF TRAFFIC																				
							400						400	400	614	11110	400	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
							LS		1	1			2	2	614	12381	2	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL), AS PER PLAN	4
							LS						LS	LS	614	12420	LS		DETOUR SIGNING	
							4						4	4	614	18601	4	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	5
						0.25							0.25	0.25	614	20110	0.25	MILE	WORK ZONE LANE LINE, CLASS 1, 6", 642 PAINT	
						0.5							0.5	0.5	614	22110	0.5	MILE	WORK ZONE EDGE LINE, CLASS 1, 6", 642 PAINT	
							500		90	230			820	820	622	41100	820	FT	PORTABLE BARRIER, UNANCHORED	
INCIDENTALS																				
													LS	LS	614	11000	LS		MAINTAINING TRAFFIC	
							LS						LS	LS	623	10001	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN	3
													LS	LS	624	10000	LS		MOBILIZATION	

CALCULATED	TMK	CHECKED	CWW
GENERAL SUMMARY			
MOT - 35 - 14.04			
22	23		

I:\ProjectData\MOT\109710_US35-1404\Design\Roadway\Sheets\109710_G5001.dgn Sheet 3/28/2022 8:28:30 AM cweiss

PARTICIPATION	ROUTE	TYPICAL SECTION	LOCATION		LENGTH (L)		NUMBER OF MID SLAB CRACKS REPAIRED WITH RDB	NUMBER OF SLAB REPAIRS	SY	SY	SY	EACH	FT	SPECIAL	FT										
			SLM		MILE	FT															FT	FT	FT	FT	FT
			FROM	TO																					
			FROM BEGIN PROJECT TO JAMES MCGEE BLVD.																						
1	US 35 WB	S1, RD, PJ	14.04	14.41			22	11		338		770		132		57									
1	US 35 EB	S1, RD, PJ	14.04	14.41			25	27		828		1890		150		57									
			FROM JAMES MCGEE BLVD. TO DUNBAR ST.																						
1	US 35 WB	S2, RD, PJ	14.41	14.73			36	25		567		1450		216		66									
1	US 35 EB	S2, RD, PJ	14.41	14.73			34	9		204		522		204		66									
			FROM DUNBAR ST. TO BROADWAY ST.																						
1	US 35 WB	RD, PJ	14.73	14.91			22						132		264										
1	US 35 EB	RD, PJ	14.73	14.91			18						108		264										
			FROM BROADWAY ST. TO END PROJECT																						
1	US 35WB	S4, RD, PJ	14.91	15.07			5	20		400		852		30		66									
1	US 35 EB	S2, RD, PJ	14.91	15.07			39	2		46		116		234		71									
			US 35 X JAMES MCGEE BLVD INTERCHANGE																						
1		WB ENTRANCE RAMP S3, RD					3	8		242		528		18											
1		WB EXIT RAMP RD					5						30												
1		EB EXIT RAMP S3, RD					4	7		216		462		24											
1		EB ENTRANCE RAMP RD					3						18												
			US 35 X IR 75 INTERCHANGE																						
1		WB DUAL ENTRANCE RAMPS S5						120				2400		4164											
LEGEND FOR AUXILLARY AREAS IN 'TYPICAL' COLUMN									PART.	SUBTOTALS															
S1: 23' X 12' SLAB REPLACEMENTS - US 35 WEST OF JAMES MCGEE									1	2841	2400	10754		1296		911									
S2: 17' X 12' SLAB REPLACEMENTS - US 35 EAST OF JAMES MCGEE																									
S3: 17' X 16' SLAB REPLACEMENTS - JAMES MCGEE RAMPS																									
S4: 15' X 12' SLAB REPLACEMENTS - US 35 WB LARGE AREA 300' X 12'																									
S5: 15' X 12' SLAB REPLACEMENTS -IR 75 ENT RAMPS LARGE AREA 900' X 24'																									
RD:RETROFIT DOWEL BAR PJ: PRESSURE RELIEF JOINT REPLACEMENT																									
TOTALS CARRIED TO GENERAL SUMMARY										2841	2400	10754		1296		911									



CALCULATED TMK CHECKED CWV	PAVEMENT SUBSUMMARY	MOT - 35 - 14.04
23 23		