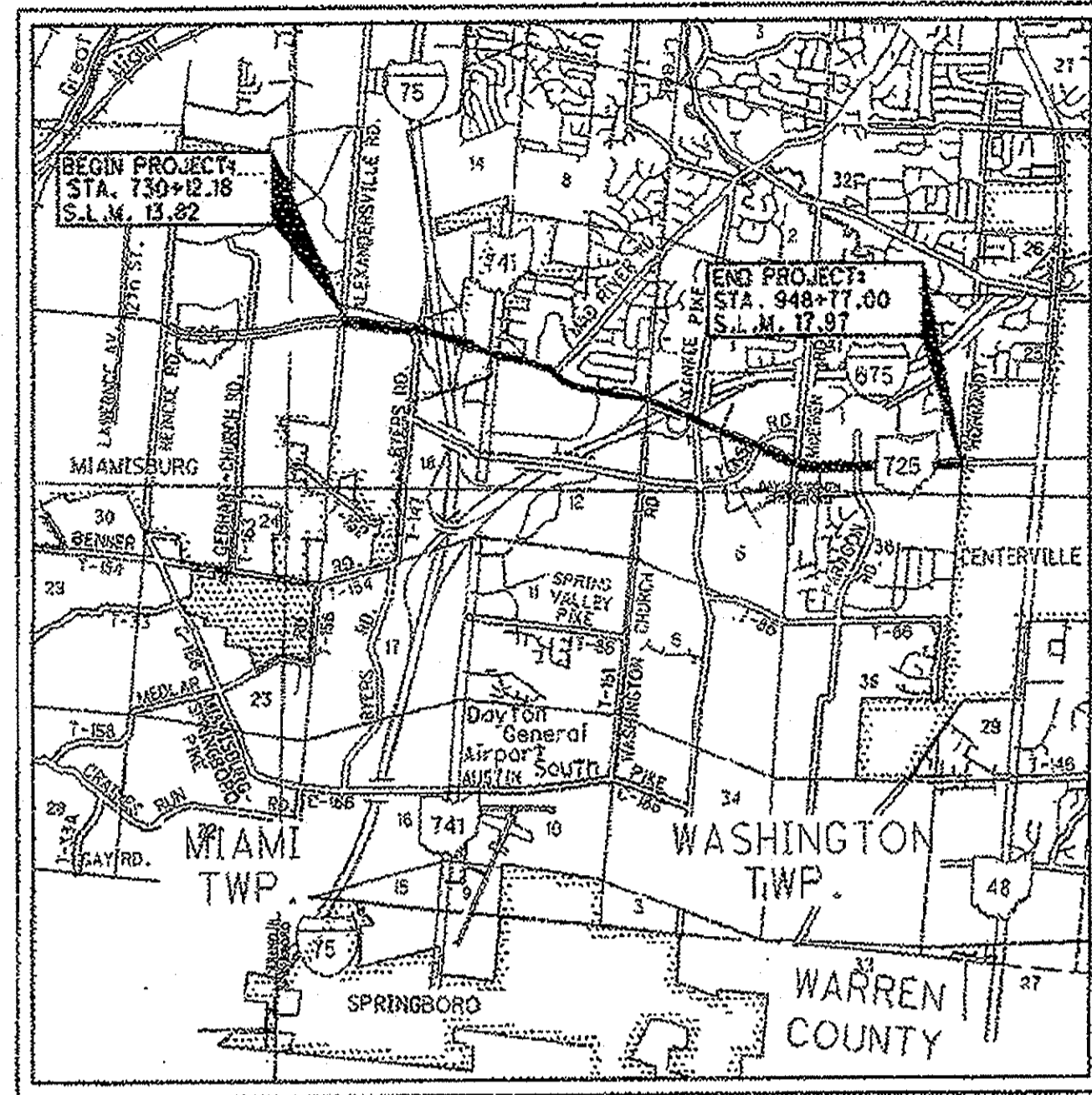


STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

MOT-725-13.82

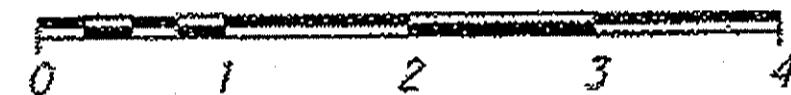
**CITY OF MIAMISBURG
CITY OF CENTERVILLE
MIAMI TWP.
WASHINGTON TWP.
MONTGOMERY COUNTY**



LOCATION MAP

LATITUDE: 39°38'0" LONGITUDE: 84°16'0"

SCALE IN MILES



PORTION TO BE IMPROVED _____
 INTERSTATE & DIVIDED HIGHWAY _____
 UNDIVIDED STATE & FEDERAL ROUTES _____
 OTHER ROADS _____

DESIGN DESIGNATION - SEE SCHEMATIC SHEETS

CURRENT ADT (2010) _____
 DESIGN YEAR ADT (2030) _____
 DESIGN HOURLY VOLUME (2030) _____
 DIRECTIONAL DISTRIBUTION _____
 TRUCKS (24 HOUR B&C) _____
 DESIGN SPEED _____
 LEGAL SPEED _____
 DESIGN FUNCTIONAL CLASSIFICATION:
 OTHER PRINCIPAL ATERIAL - URBAN

DESIGN EXCEPTIONS

NONE

UNDERGROUND UTILITIES

CONTACT BOTH SERVICES
CALL TWO WORKING DAYS
BEFORE YOU DIG

CALL
1-800-362-2764
(TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY
OIL & GAS PRODUCERS PROTECTIVE
SERVICE CALL: 1-800-925-0988

PLAN PREPARED BY:
OHIO DEPARTMENT OF
TRANSPORTATION
DISTRICT 7 PRODUCTION

INDEX OF SHEETS:

TITLE SHEET	1
SCHEMATIC	2-4
TYPICAL SECTIONS	5-14
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TRAFFIC LOOP DETECTORS	71-73
TRAFFIC CONTROL SHEETS	74-102,101A,101B,101C,101D

ENGINEERS SEAL:



SIGNED: *Sherry Sugan*
DATE: 7/28/09

STANDARD CONSTRUCTION DRAWINGS										SUPPLEMENTAL SPECIFICATIONS	
BP-2.1	7-18-08	TC-41.20	1-19-01	TC-82.10	4-19-02	MT-35.10	4-20-01	MT-99.20	1-16-09	800	7-17-09
BP-2.2	7-18-08	TC-41.41	1-19-01	TC-85.20	3-06-09						
BP-3.1	10-19-07	TC-42.20	7-16-04			MT-95.30	9-5-06	MT-102.20	9-5-06	832	4-25-06
BP-4.1	7-16-04					MT-95.31	9-5-06	MT-102.30	9-5-06		
BP-5.1	7-28-00	TC-52.10	1-19-07			MT-95.32	9-5-06				
BP-7.1	1-19-07	TC-52.20	1-19-07					MT-105.10	1-16-09		
						MT-95.50	9-5-06				
RM-1.1	7-18-08	TC-61.10	1-19-01			MT-97.10	9-5-06				
RM-3.1	4-18-03	TC-65.10	1-21-05			MT-97.12	9-5-06				
		TC-65.11	1-21-05								
HL-20.11	1-19-07										
HL-30.11	4-17-09	TC-71.10	1-19-07								
HL-30.21	1-19-07	TC-72.20	1-21-05								
HL-30.22	4-17-09	TC-73.10	1-19-01								

SPECIAL PROVISIONS

PROJECT DESCRIPTION

IMPROVEMENT OF 4.2 MILES OF STATE ROUTE 725 BY RESURFACING THE ROADWAY. OTHER WORK TO INCLUDE MISCELLANEOUS ROADWAY ITEMS SUCH AS, CATCH BASIN REPAIR, AND TRUNCATED DOMES WHERE NEEDED.

PROJECT EARTH DISTURBED AREA: 0 ACRES
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A
 NOTICE OF INTENT EARTH DISTURBED AREA: N/A

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2008 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DELCLARE THE THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED: *Rex Ditchon, P.E., P.E./P.E.A.*
DATE: 7-27-09 DISTRICT DEPUTY DIRECTOR

APPROVED: *Alan M. Mattioni, III*
DATE: 8-19-09 DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. NON-FEDERAL

PID NO. 82505

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT NONE

MOT-725-13.82

MOT-SR-725-13.82
 090478 PID-82505
 Dist 7 10/21/2009
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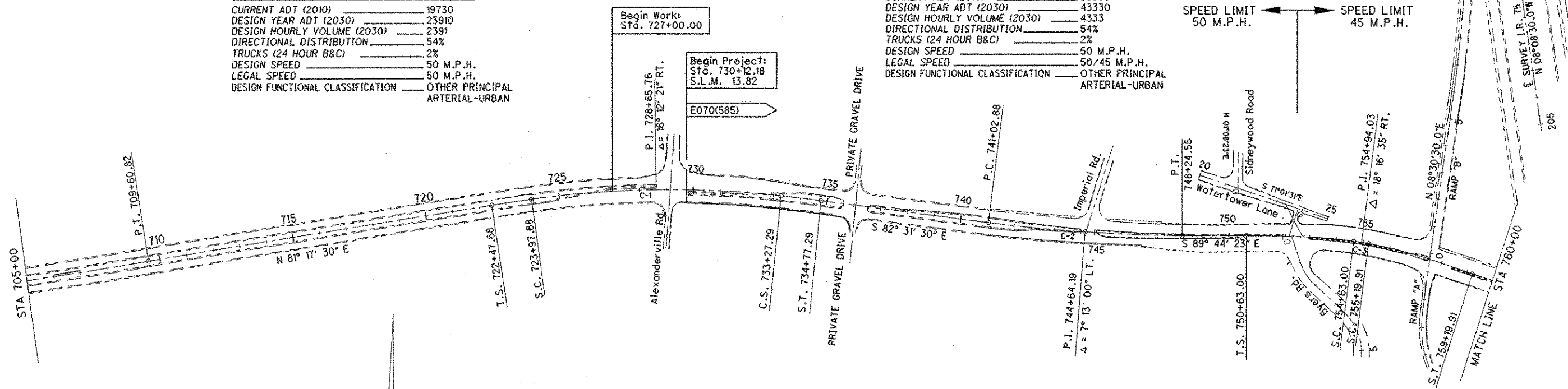
DESIGN DESIGNATION
(WEST OF ALEXANDERSVILLE RD.)

CURRENT ADT (2010)	19730
DESIGN YEAR ADT (2030)	23910
DESIGN HOURLY VOLUME (2030)	2391
DIRECTIONAL DISTRIBUTION	54%
TRUCKS (24 HOUR B&C)	2%
DESIGN SPEED	50 M.P.H.
LEGAL SPEED	50 M.P.H.
DESIGN FUNCTIONAL CLASSIFICATION	OTHER PRINCIPAL ARTERIAL-URBAN

DESIGN DESIGNATION
(BETWEEN ALEXANDERSVILLE AND I.R.-75)

CURRENT ADT (2010)	38850
DESIGN YEAR ADT (2030)	43330
DESIGN HOURLY VOLUME (2030)	4333
DIRECTIONAL DISTRIBUTION	54%
TRUCKS (24 HOUR B&C)	2%
DESIGN SPEED	50 M.P.H.
LEGAL SPEED	50/45 M.P.H.
DESIGN FUNCTIONAL CLASSIFICATION	OTHER PRINCIPAL ARTERIAL-URBAN

SPEED LIMIT 50 M.P.H. SPEED LIMIT 45 M.P.H.



C-1 CURVE DATA

P.I. = Sta. 728+65.76	$\Delta c = 13^{\circ} 57' 21''$
$\Delta = 16^{\circ} 12' 21''$ RT.	Lc = 928.09'
Dc = $1^{\circ} 30' 00''$	Os = $1^{\circ} 07' 30''$
Ls = 150'	p = 0.25'
Ts = 618.86'	K = 75.00'
Es = 38.77'	Xc = 149.99'
R = 3819.72'	Yc = 0.98'

C-2 CURVE DATA

P.I. = Sta. 744+64.19	$\Delta c = 7^{\circ} 13' 00''$ LT.
$\Delta = 7^{\circ} 13' 00''$ LT.	Lc = 1000.00'
Dc = $1^{\circ} 00' 00''$	Os = $8^{\circ} 00' 00''$
Ls = 400'	p = 4.65'
Ts = 361.31'	K = 199.87'
Es = 11.38'	Xc = 399.22'
R = 721.67'	Yc = 18.59'

C-3 CURVE DATA

P.I. = Sta. 754+94.03	$\Delta c = 2^{\circ} 16' 35''$
$\Delta = 18^{\circ} 16' 35''$ RT.	Lc = 56.91'
Dc = $4^{\circ} 00' 00''$	Os = $8^{\circ} 00' 00''$
Ls = 400'	p = 4.65'
Ts = 431.03'	K = 199.87'
Es = 23.12'	Xc = 399.22'
R = 1432.40'	Yc = 18.59'

C-4 CURVE DATA

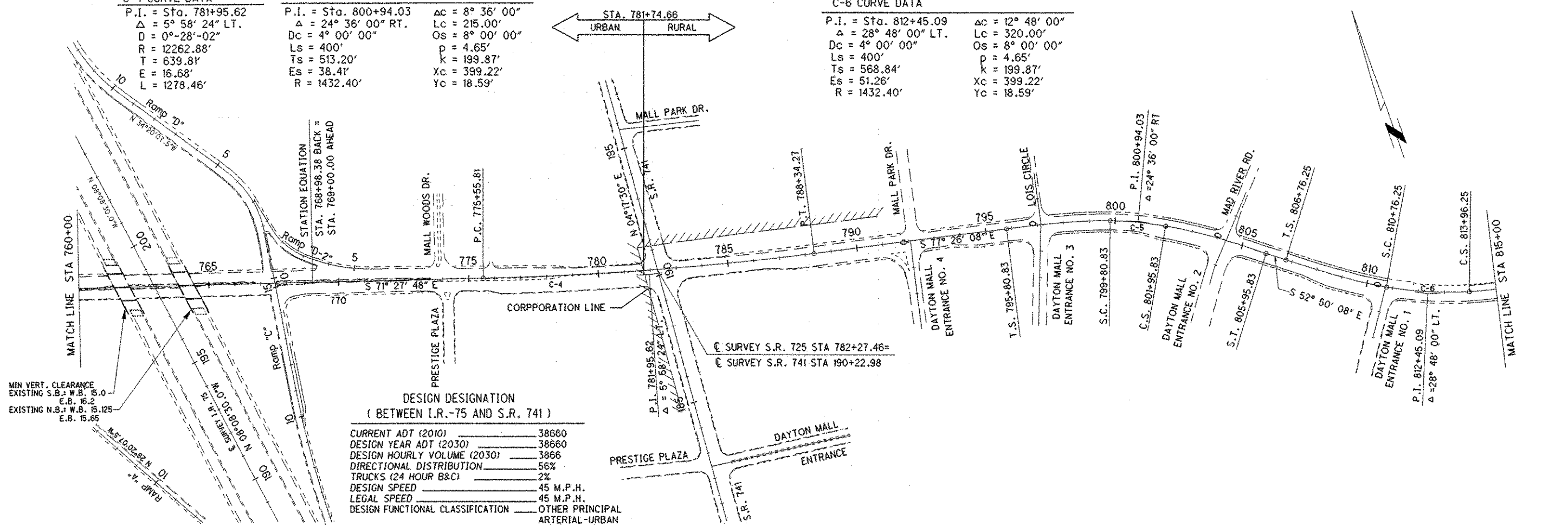
P.I. = Sta. 781+95.62	$\Delta = 5^{\circ} 58' 24''$ LT.
$\Delta = 5^{\circ} 58' 24''$ LT.	Lc = 215.00'
Dc = $0^{\circ} 28' 02''$	Os = $8^{\circ} 00' 00''$
Ls = 12262.88'	p = 4.65'
Ts = 639.81'	K = 199.87'
Es = 16.68'	Xc = 399.22'
R = 1278.46'	Yc = 18.59'

C-5 CURVE DATA

P.I. = Sta. 800+94.03	$\Delta c = 8^{\circ} 36' 00''$
$\Delta = 24^{\circ} 36' 00''$ RT.	Lc = 215.00'
Dc = $4^{\circ} 00' 00''$	Os = $8^{\circ} 00' 00''$
Ls = 400'	p = 4.65'
Ts = 513.20'	K = 199.87'
Es = 38.41'	Xc = 399.22'
R = 1432.40'	Yc = 18.59'

C-6 CURVE DATA

P.I. = Sta. 812+45.09	$\Delta c = 12^{\circ} 48' 00''$
$\Delta = 28^{\circ} 48' 00''$ LT.	Lc = 320.00'
Dc = $4^{\circ} 00' 00''$	Os = $8^{\circ} 00' 00''$
Ls = 400'	p = 4.65'
Ts = 568.84'	K = 199.87'
Es = 51.26'	Xc = 399.22'
R = 1432.40'	Yc = 18.59'



DESIGN DESIGNATION
(BETWEEN I.R.-75 AND S.R. 741)

CURRENT ADT (2010)	38660
DESIGN YEAR ADT (2030)	38660
DESIGN HOURLY VOLUME (2030)	3866
DIRECTIONAL DISTRIBUTION	56%
TRUCKS (24 HOUR B&C)	2%
DESIGN SPEED	45 M.P.H.
LEGAL SPEED	45 M.P.H.
DESIGN FUNCTIONAL CLASSIFICATION	OTHER PRINCIPAL ARTERIAL-URBAN

SCHEMATIC PLAN

MOT-725-13.82

C-7 CURVE DATA

P.I. = Sta. 838+23.45	$\Delta C = 1^{\circ} 16' 30''$
$\Delta = 15^{\circ} 16' 30''$ RT.	Lc = 36.43'
Dc = $3^{\circ} 30' 00''$	$\phi s = 7^{\circ} 00' 00''$
Ls = 400'	p = 4.07'
Ts = 419.96'	K = 199.90'
Es = 18.76'	Xc = 399.40'
R = 1637.02'	Yc = 16.27'

DESIGN DESIGNATION
(BETWEEN S.R. 741 AND I.R.675)

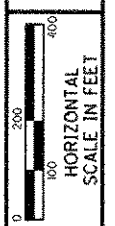
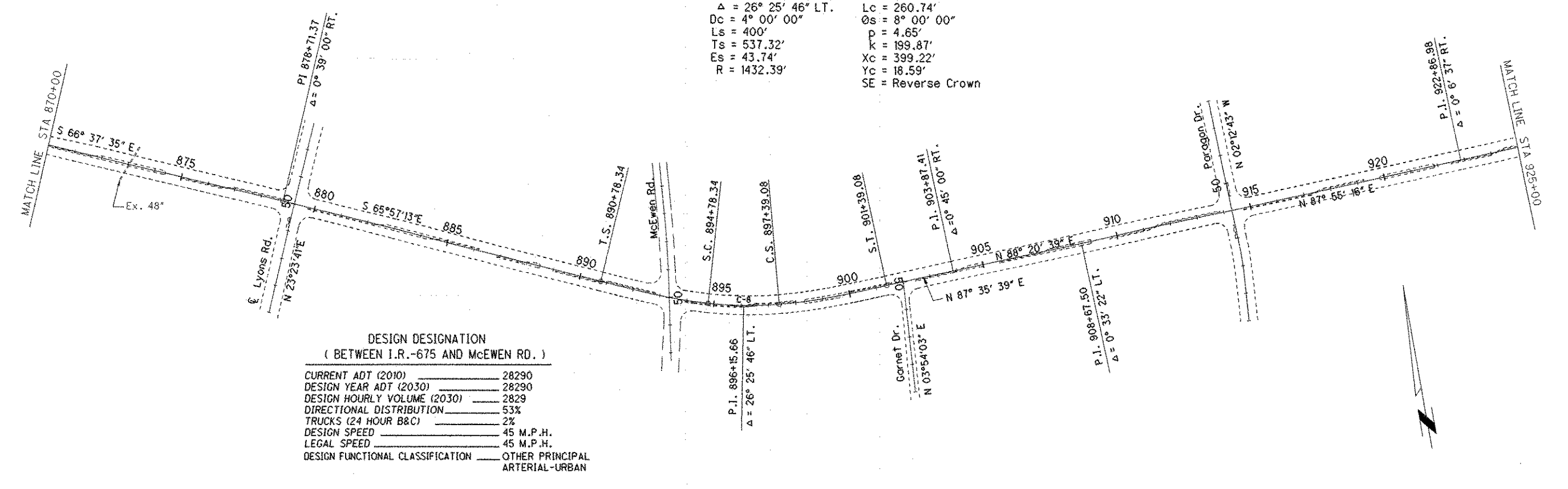
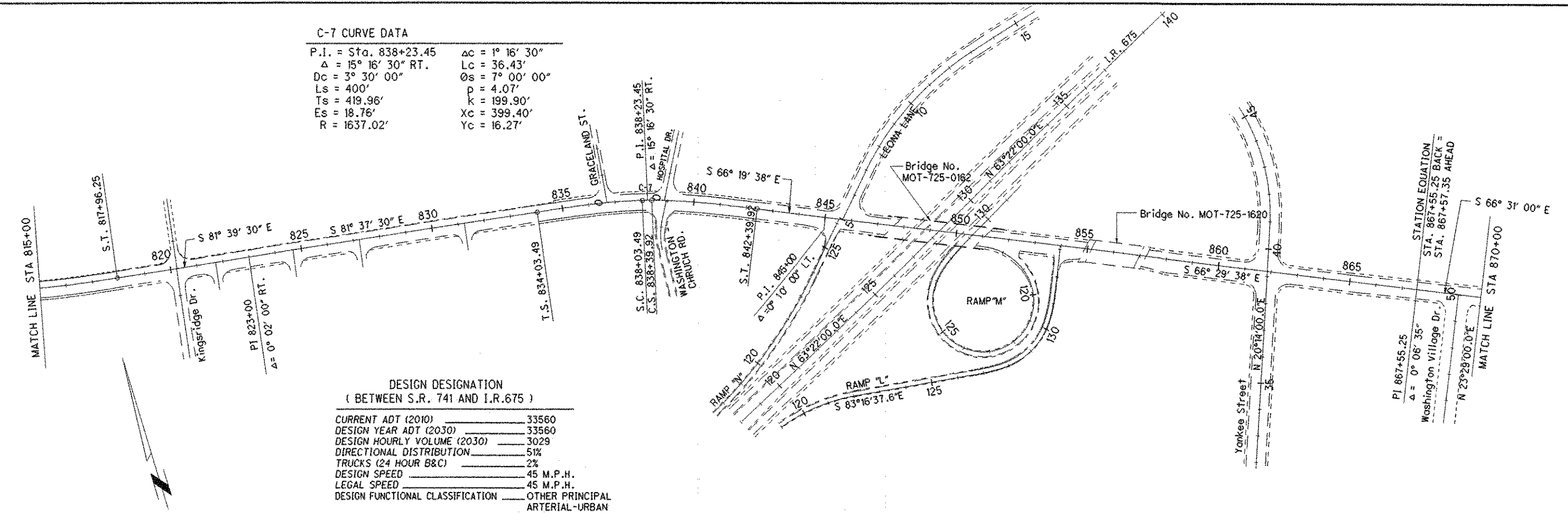
CURRENT ADT (2010)	33560
DESIGN YEAR ADT (2030)	33560
DESIGN HOURLY VOLUME (2030)	3029
DIRECTIONAL DISTRIBUTION	51%
TRUCKS (24 HOUR B&C)	2%
DESIGN SPEED	45 M.P.H.
LEGAL SPEED	45 M.P.H.
DESIGN FUNCTIONAL CLASSIFICATION	OTHER PRINCIPAL ARTERIAL-URBAN

C-8 CURVE DATA

P.I. = Sta. 896+15.66	$\Delta C = 10^{\circ} 25' 46''$
$\Delta = 26^{\circ} 25' 46''$ LT.	Lc = 260.74'
Dc = $4^{\circ} 00' 00''$	$\phi s = 8^{\circ} 00' 00''$
Ls = 400'	p = 4.65'
Ts = 537.32'	K = 199.87'
Es = 43.74'	Xc = 399.22'
R = 1432.39'	Yc = 18.59'
	SE = Reverse Crown

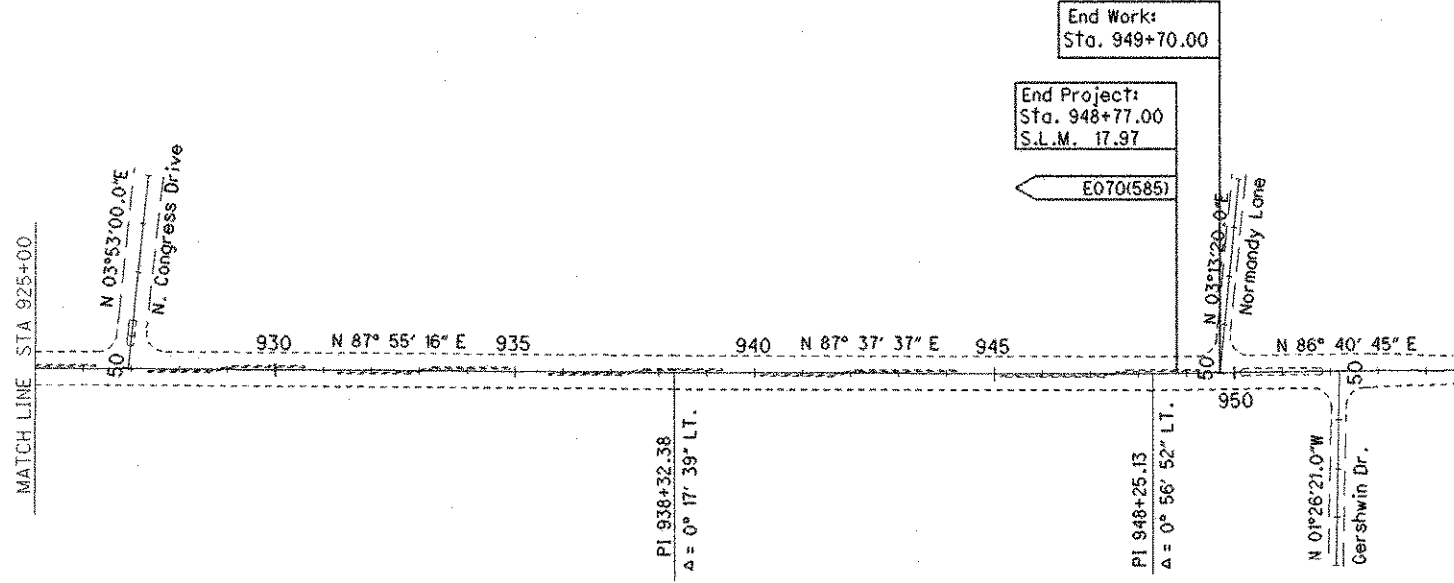
DESIGN DESIGNATION
(BETWEEN I.R.-675 AND McEWEN RD.)

CURRENT ADT (2010)	28290
DESIGN YEAR ADT (2030)	28290
DESIGN HOURLY VOLUME (2030)	2829
DIRECTIONAL DISTRIBUTION	53%
TRUCKS (24 HOUR B&C)	2%
DESIGN SPEED	45 M.P.H.
LEGAL SPEED	45 M.P.H.
DESIGN FUNCTIONAL CLASSIFICATION	OTHER PRINCIPAL ARTERIAL-URBAN



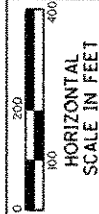
SCHEMATIC PLAN

MOT-725-13.82



DESIGN DESIGNATION
(BETWEEN McEWEN AND NORMANDY LANE)

CURRENT ADT (2010)	18460
DESIGN YEAR ADT (2030)	18460
DESIGN HOURLY VOLUME (2030)	1846
DIRECTIONAL DISTRIBUTION	55%
TRUCKS (24 HOUR B&C)	3%
DESIGN SPEED	45 M.P.H.
LEGAL SPEED	45 M.P.H.
DESIGN FUNCTIONAL CLASSIFICATION	



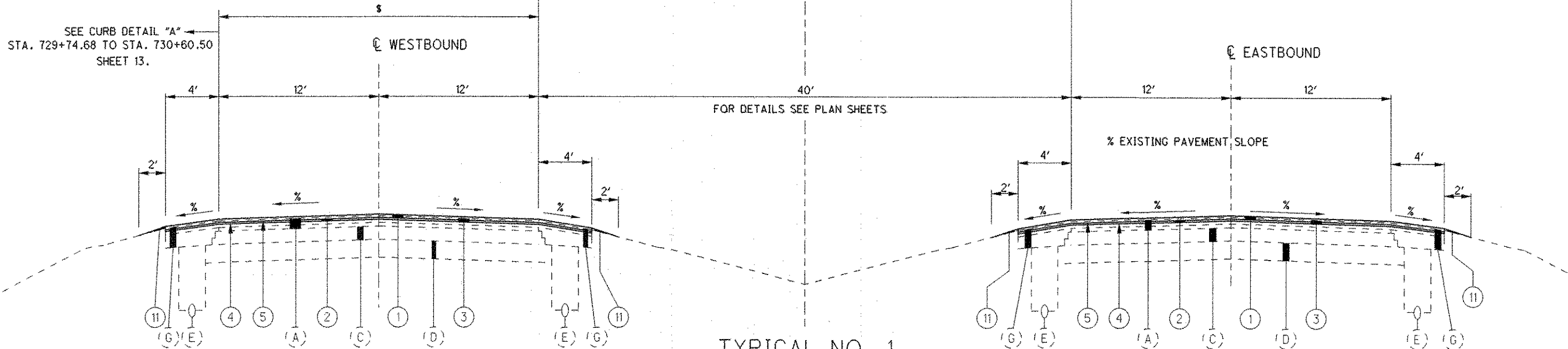
SCHEMATIC PLAN

MOT-725-13.82

- § VARIES 65.2' TO 62' STA. 730+12.19 TO STA. 730+60.50
- § 62' STA. 730+60.50 TO STA. 731+57.10
- § VARIES 62' TO 54.9' STA. 731+57.10 TO STA. 732+50
- § VARIES 54.9' TO 26.4' STA. 732+50 TO STA. 734+00
- § VARIES 26.4' TO 24' STA. 734+00 TO STA. 734+15.90
- § 36' STA. 736+65 TO STA. 737+96
- § VARIES 36' TO 24' STA. 737+96 TO STA. 739+91.80

- ++ VARIES 69' TO 64.2' STA. 729+74.68 TO STA. 729+82.50 (WB)
- ++ 44' STA. 729+74.68 TO STA. 729+82.80 (EB)
- * VARIES 0' TO 20' STA. 735+42.00 TO STA. 735+62.00 (WB)
- * VARIES 20' TO 12' STA. 736+52.00 TO STA. 736+65.00 (WB)
- # VARIES 0' TO 20' STA. 735+42.00 TO STA. 735+62.00 (EB)
- # VARIES 20' TO 0' STA. 736+52.00 TO STA. 736+65.00 (EB)

% EXISTING PAVEMENT SLOPE (TYP)



TYPICAL NO. 1
NORMAL SECTION
LIMITING STATIONS

STA. 729+74.68 TO STA. 730+12.18 = TRANSITION
STA. 730+12.18 TO STA. 740+41.80 = 1029.62 FT.

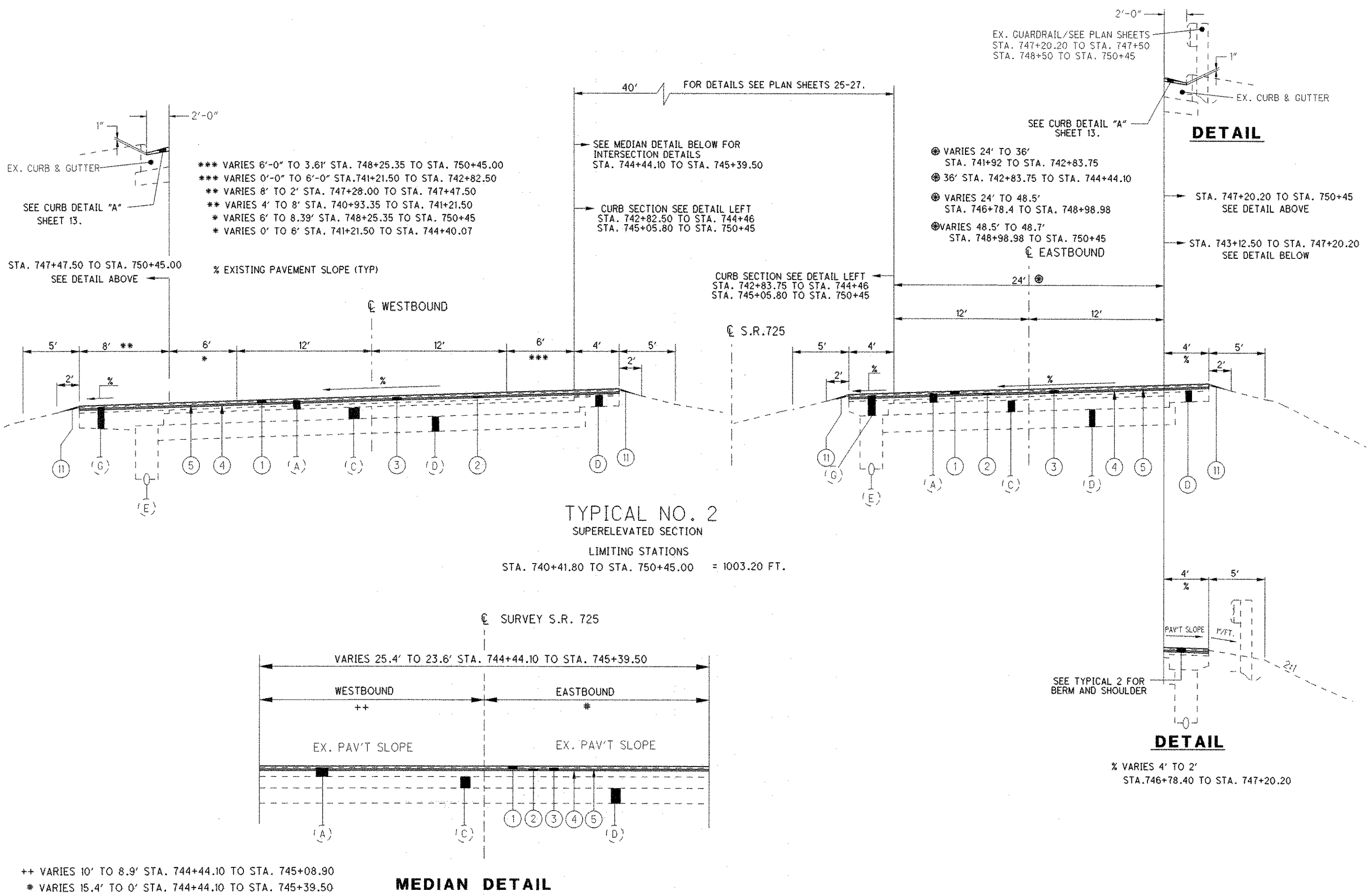
PROPOSED LEGEND

- ① ITEM 446, 1 1/2" ASPHALT CONCRETE, SURFACE COURSE, TYPE 1H, PG76-22M, AS PER PLAN
- ② ITEM 448, 3/4" ASPHALT CONCRETE, INTERMEDIATE COURSE, TYPE 1, PG64-28
- ③ ITEM 254, 1 1/2" PAVEMENT PLANING, ASPHALT CONCRETE
- ④ ITEM 407, TACK COAT, @ 0.075 GAL./SY.
- ⑤ ITEM 407, TACK COAT, FOR INTERMEDIATE COURSE @ 0.040 GAL./SY.
- ⑥ ITEM 609, CONCRETE MEDIAN, AS PER PLAN
- ⑦ ITEM 446, VARIABLE DEPTH ASPHALT CONCRETE, SURFACE COURSE, TYPE 1H, PG76-22M
- ⑧ ITEM 301, ASPHALT CONCRETE BASE, PG64-22, AS PER PLAN
- ⑨ ITEM 407, TACK COAT, 702.13 0.075 GAL./SY.
- ⑩ ITEM 202, CONCRETE MEDIAN REMOVED, AS PER PLAN
- ⑪ ITEM 617, COMPACTED AGGREGATE
- ⑫ ITEM 202, TRAFFIC ISLAND REMOVED, AS PER PLAN
- ⑬ ITEM 408, PRIME COAT
- ⑭ ITEM 202, PAVEMENT REMOVED, ASPHALT
- ⑮ ITEM 254, 2 1/4" PAVEMENT PLANING, ASPHALT CONCRETE

EXISTING LEGEND

- (A) EXISTING ASPHALT CONCRETE (VARIABLE DEPTH)
- (B) EXISTING PAVEMENT (VARIABLE DEPTH)
- (C) EXISTING AGGREGATE BASE (VARIABLE DEPTH)
- (D) EXISTING SUBBASE (VARIABLE DEPTH)
- (E) EXISTING UNDERDRAIN
- (F) EXISTING CURB
- (G) 3" ASPHALT CONCRETE ON AGGREGATE BERM (VARIABLE DEPTH)
- (H) EXISTING CONCRETE PAVEMENT (VARIABLE DEPTH)
- (I) EXISTING CONCRETE MEDIAN
- (J) EXISTING LONGITUDINAL JOINTS
- (K) EXISTING ASPHALT CONCRETE BASE (VARIABLE DEPTH)
- (L) 10 1/2" EXISTING ASPHALT CONCRETE ON VARIABLE DEPTH AGGREGATE BASE

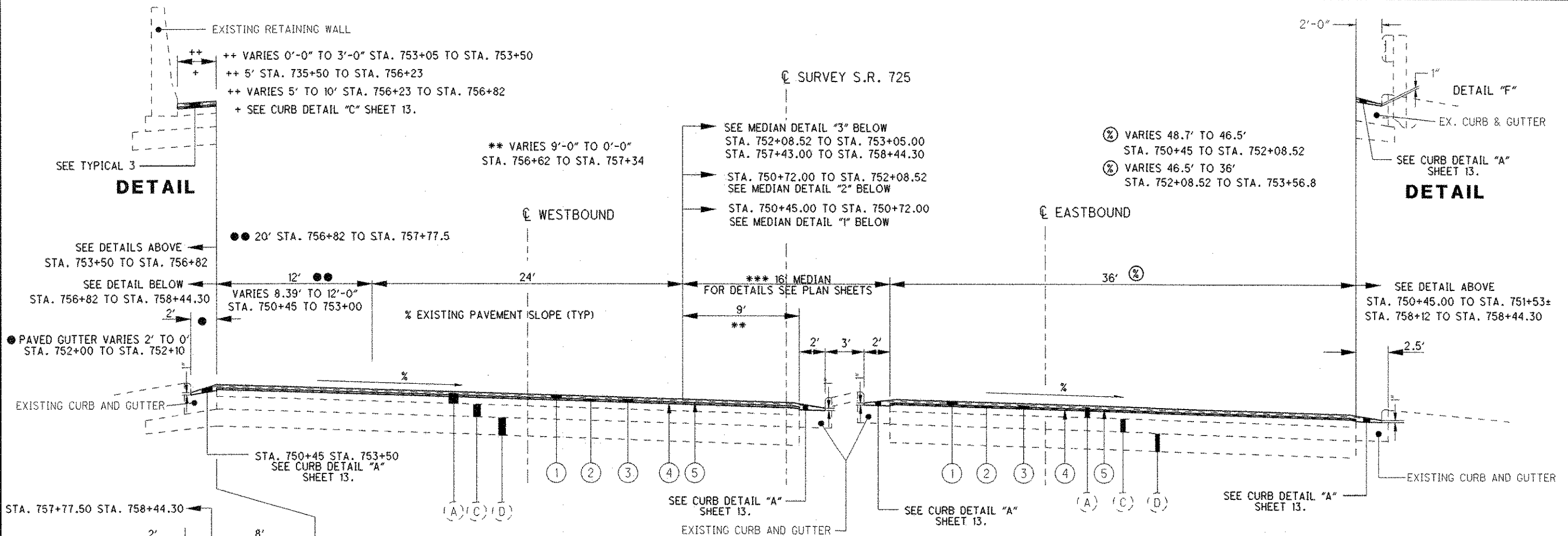
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TYPICAL SECTIONS

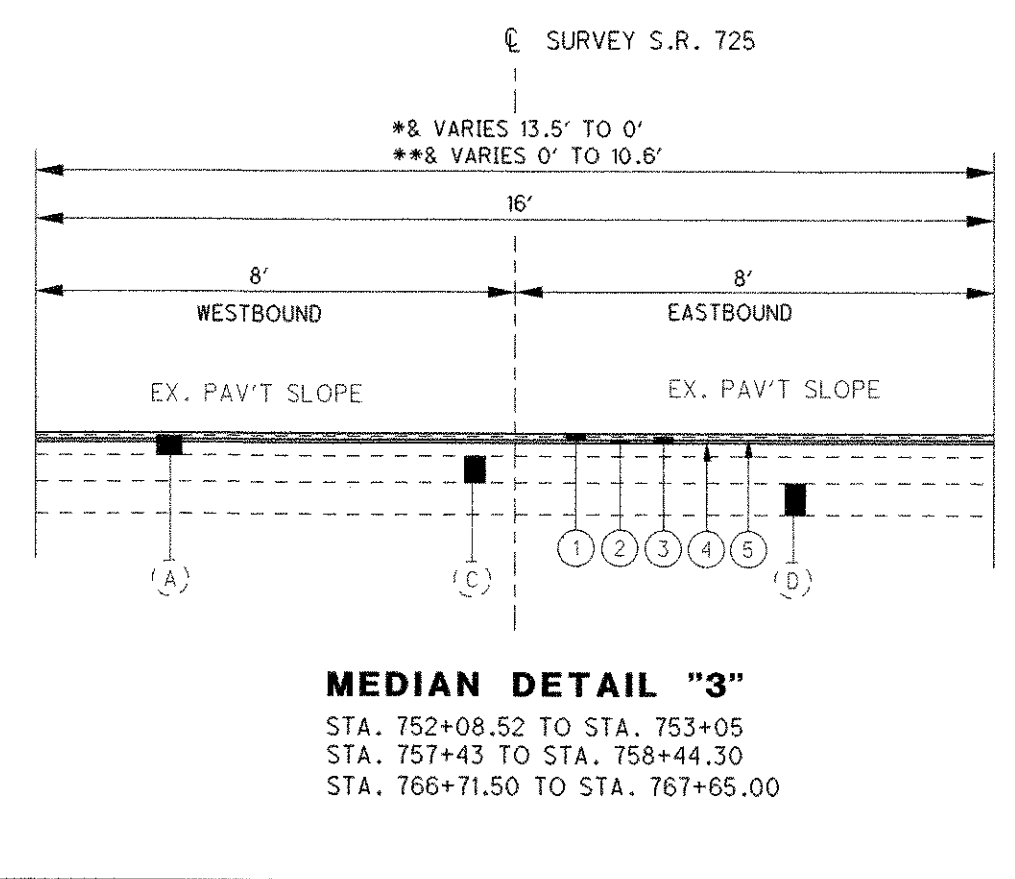
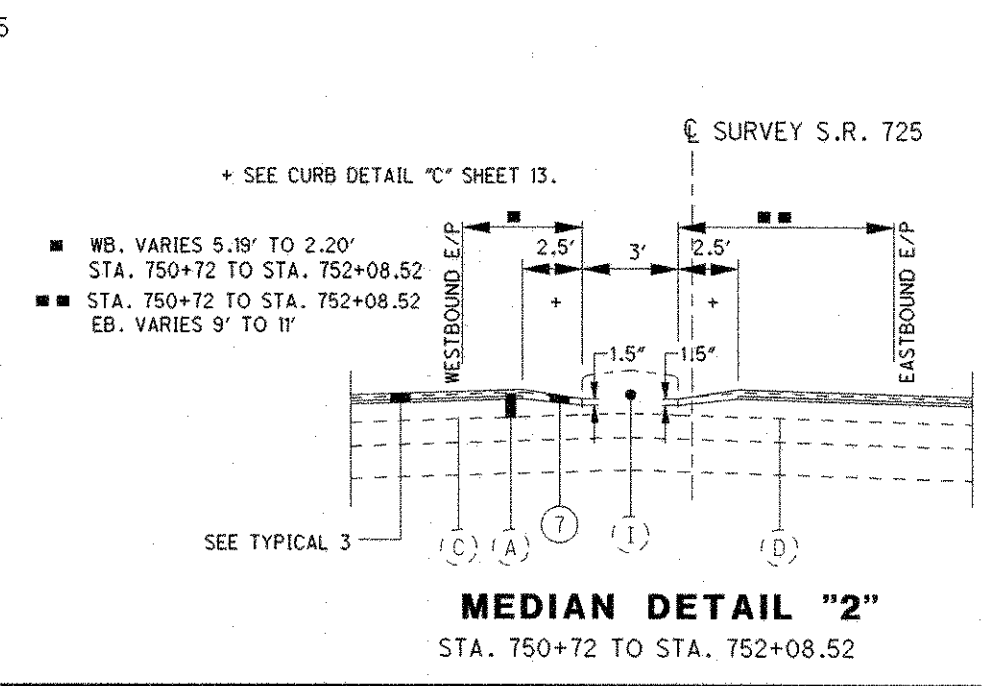
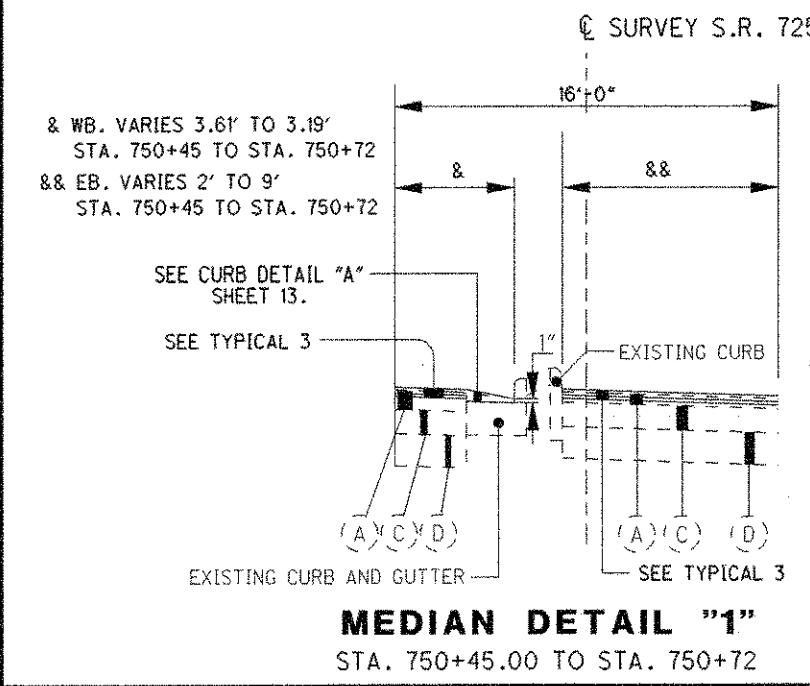
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**TYPICAL NO. 3
SUPERELEVATED SECTION**

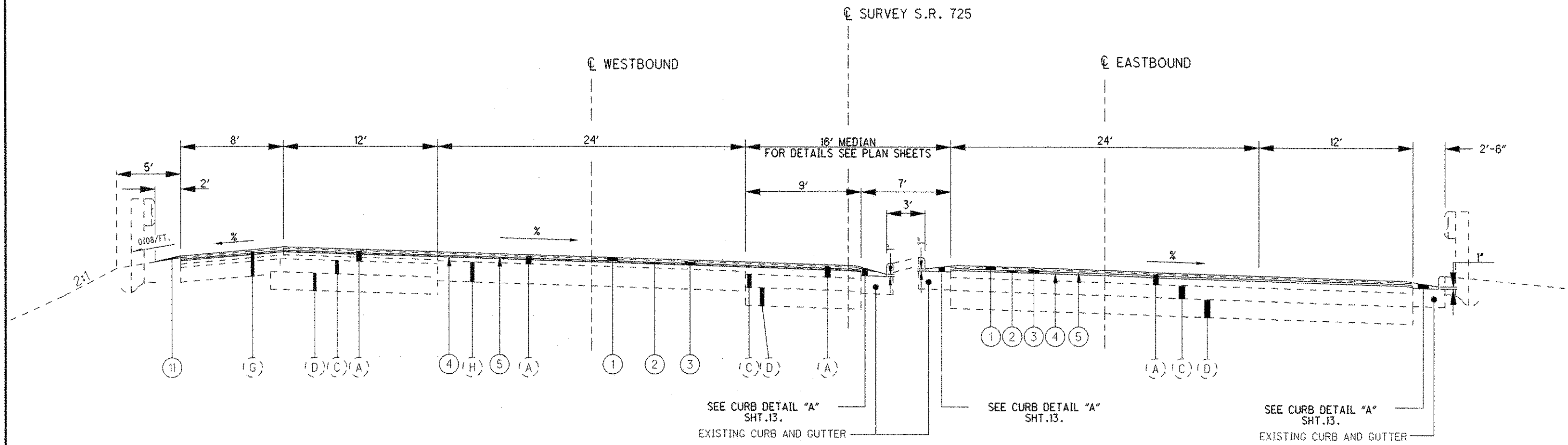
LIMITING STATIONS
 STA. 750+45.00 TO STA. 758+44.30 = 799.30 FT.



TYPICAL SECTIONS

MOT-725-13.82

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TYPICAL NO. 4
 SUPERELEVATED SECTION
 LIMITING STATIONS

STA. 758+44.30 TO STA. 760+63.00 = 2187 FT.

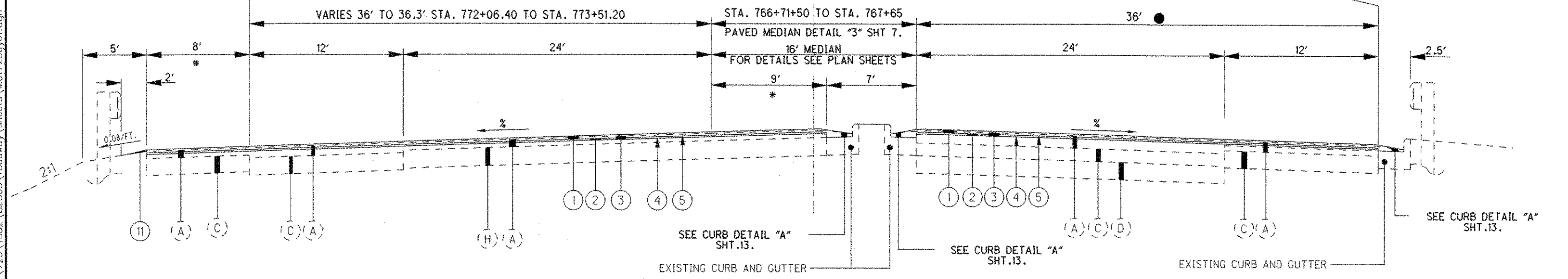
SEE TYPICAL No. 1 FOR BERM DETAIL
 STA. 772+06.40 TO STA. 773+51.20

SEE TYPICAL NO.14 RAMP D-2
 STA. 769+13.00 TO STA. 771+43.84
 SHEET 13.

* BERM VARIES 2.5' TO 8'
 STA. 764+45 TO STA. 765+05

SEE CURB "A" DETAIL SHEET 13.
 STA. 760+92.60 TO STA. 764+45

* BERM VARIES 8' TO 2'
 STA. 760+80.67 TO STA. 760+92.60



TYPICAL NO. 5
 NORMAL SECTION
 LIMITING STATIONS

STA. 760+63.00 TO STA. 773+51.20 = 1288.2 FT.

* VARIES 9' TO 0' STA. 760+63.00 TO STA. 761+35.93

- VARIES 36' TO 45' STA. 764+27.10 TO STA. 765+00
- 45' STA. 765+00 TO STA. 766+71.50
- VARIES 36' TO 43' STA. 768+50 TO STA. 769+00
- 43' STA. 769+00 TO STA. 773+51.20

SEE CURB "A" DETAIL SHEET 13.
 STA. 773+15 TO STA. 773+51.20

SEE TYPICAL No. 1 FOR BERM DETAIL
 STA. 768+75.77 TO STA. 773+15

SEE SHEET 30
 FOR CONCRETE SECTION RAMP "C"
 STA. 767+37.8 TO STA. 768+75.77

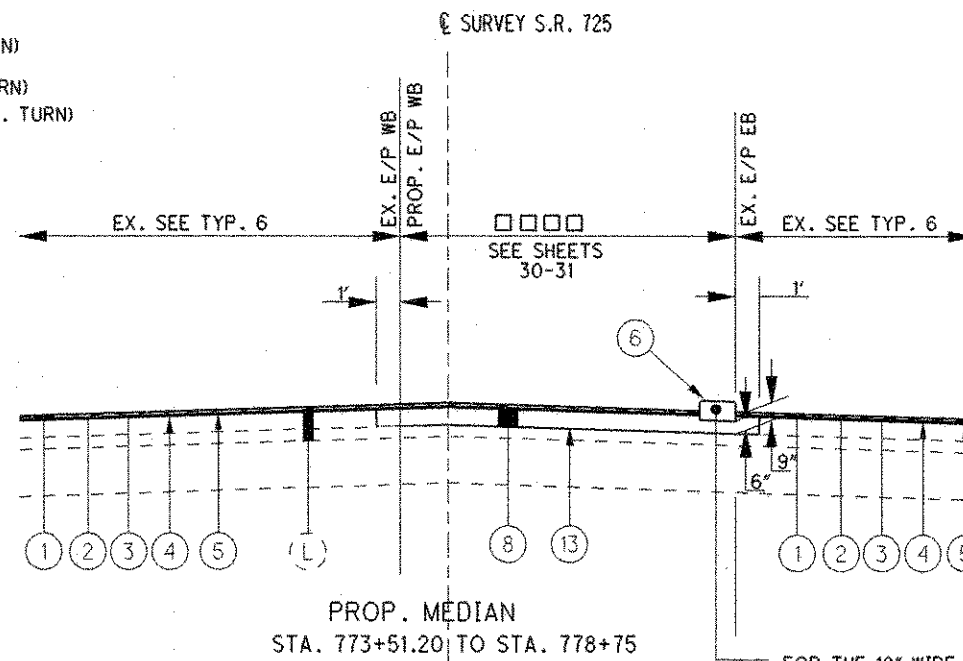
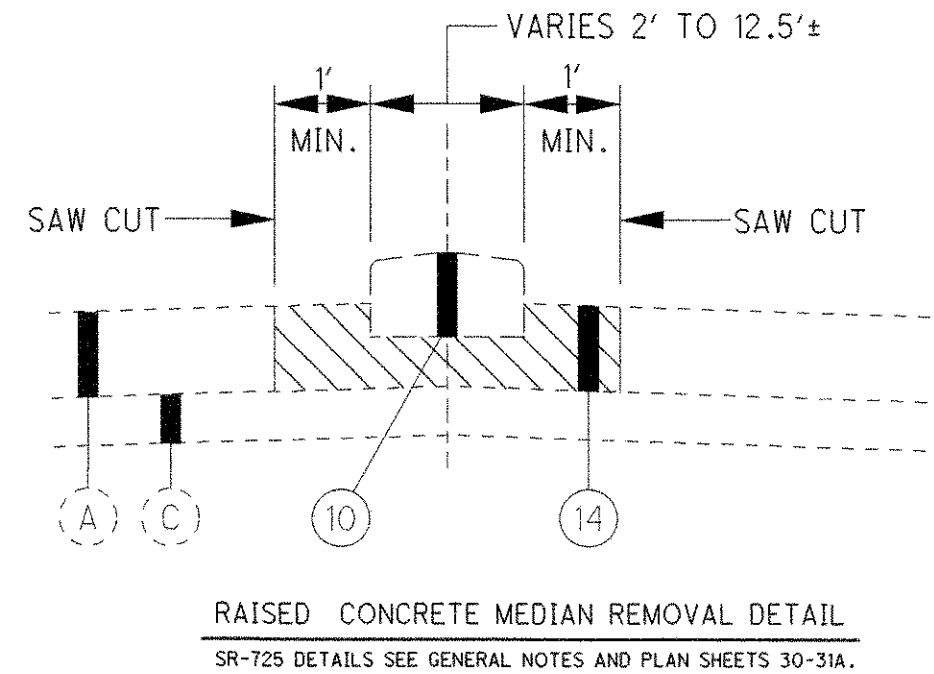
TYPICAL SECTIONS

MOT-725-13.82

18" TRAFFIC DIVIDER STA. 773+74.05 TO STA. 774+55
SEE PLAN SHEETS 30.

24" TRAFFIC DIVIDER STA. 774+79.69 TO STA. 778+75
SEE PLAN SHEETS 30,31.

- 14' STA. 773+21.50 TO STA. 773+75.07 (EB. LT. TURN)
- 12.6' STA. 773+75.07 TO STA. 773+88.8 (EB. LT. TURN)
- VARIES 12.6' TO 0' STA. 773+88.8 TO STA. 774+23.7 (EB. LT. TURN)
- VARIES 0' TO 11.46' STA. 773+88.8 TO STA. 774+23.7 (WB. LT. TURN)
- VARIES 11.46' TO 11.64' STA. 774+23.7 TO STA. 774+54.04 (WB. LT. TURN)
- 14' STA. 774+54.04 TO STA. 774+79.69 (WB. LT. TURN)



+ SEE CURB DETAIL "C" SHEET 13.

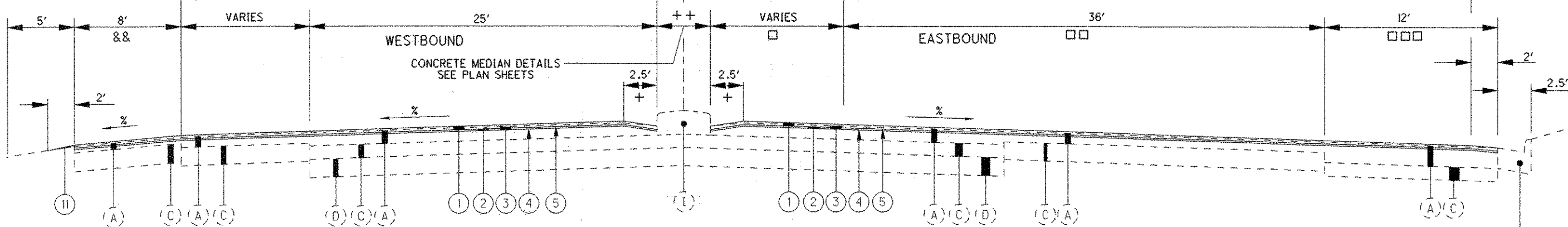
++ REMOVE EXISTING RAISED CONCRETE MEDIAN
STA. 773+74.05 TO STA. 778+75
SEE DETAIL ABOVE RIGHT & SHEET 31A.

STA. 779+00.00 TO STA. 784+91
SEE CURB DETAIL "A" SHT. 13.

- VARIES 0' TO 10' STA. 777+50 TO STA. 778+75
- VARIES 10' TO 11.25 STA. 778+75 TO STA. 781+48.25
- VARIES 36' TO 25' STA. 783+24.6 TO STA. 783+55
- 25' STA. 783+55 TO STA. 784+87.84
- VARIES 0' TO 13' STA. 775+73.25 TO STA. 776+23.25
- VARIES 13' TO 13.3' STA. 776+23.25 TO STA. 778+75
- VARIES 13.3' TO 12' STA. 778+75 TO STA. 781+48.25
- VARIES 12' TO 0' STA. 783+13 TO STA. 783+24.6

→ STA. 774+79.69 TO STA. 775+25
SEE CURB DETAIL "A"
SHT. 13.

→ SEE CURB DETAIL "B" SHT. 13,
STA. 775+25 TO STA. 784+87.84



&& VARIES 8' TO 2.5' STA. 778+50 TO STA. 779+00

- ▲ VARIES 36.3' TO 35.0' STA. 773+51.20 TO STA. 774+79.69
- ▲ VARIES 47' TO 46' STA. 774+79.69 TO STA. 775+98.6
- ▲ VARIES 46' TO 43.6' STA. 775+98.6 TO STA. 777+50
- ▲ VARIES 43.6' TO 26' STA. 777+50 TO STA. 778+75
- ▲ VARIES 39.2' TO 36' STA. 783+55 TO STA. 784+87.84

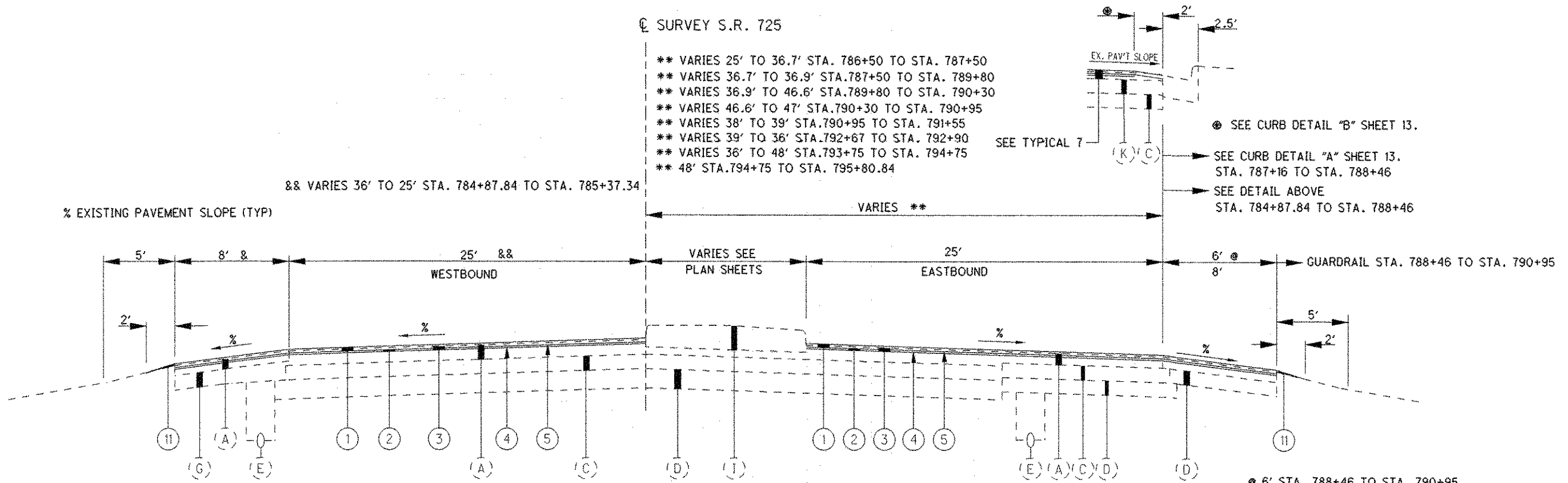
TYPICAL NO. 6
NORMAL SECTION

LIMITING STATIONS
STA. 773+51.20 TO STA. 784+87.84 = 1136.64 FT.

TYPICAL SECTIONS

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& VARIES 2' TO 10'± STA. 784+91 TO STA. 785+06
 & VARIES 10'± TO 8' STA. 785+06 TO STA. 791+55

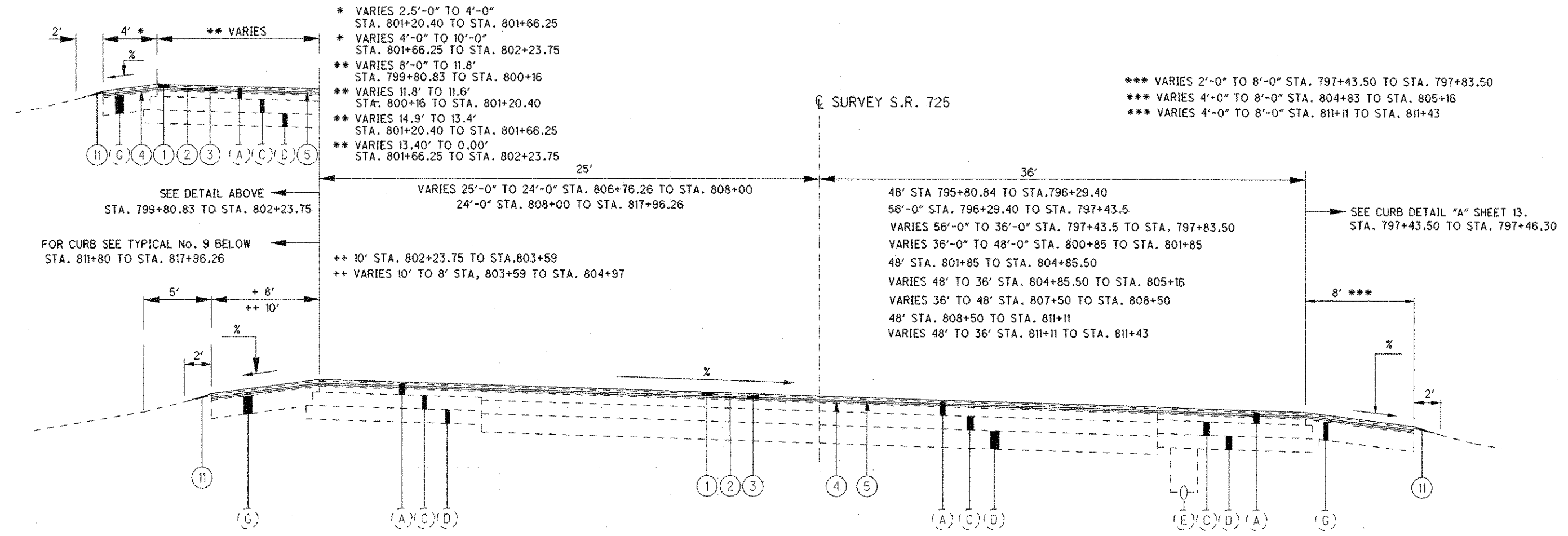
TYPICAL NO. 7
 NORMAL SECTION
 LIMITING STATIONS
 STA. 784+87.84 TO STA. 795+80.84 = 1093 FT.

- ⊙ 6' STA. 788+46 TO STA. 790+95
- ⊙ VARIES 2.5'± TO 6' STA. 792+67 TO STA. 793+00.66
- ⊙ VARIES 6' TO 10' STA. 793+00.66 TO 793+19.60
- ⊙ 10' STA. 793+19.60 TO 793+75
- ⊙ VARIE 10' TO 8' STA. 793+75 TO STA. 794+75

TYPICAL SECTIONS

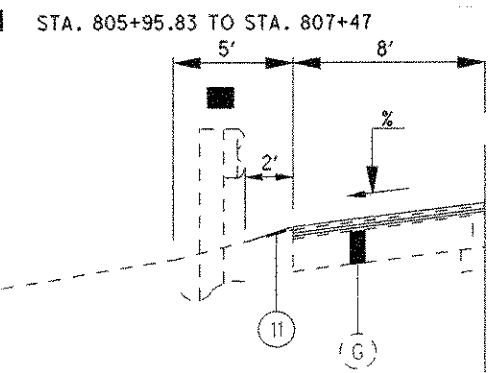
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TYPICAL NO. 8
SUPERELEVATED SECTION

LIMITING STATIONS
 STA. 795+80.84 TO STA. 805+95.83 = 104.99 FT.
 STA. 806+76.26 TO STA. 817+96.26 = 1120 FT. (OPPOSITE HAND)



TYPICAL NO. 9
NORMAL SECTION

LIMITING STATIONS
 STA. 805+95.83 TO STA. 806+76.26 = 80.43 FT.
 STA. 817+96.26 TO STA. 834+03.50 = 1607.24 FT.

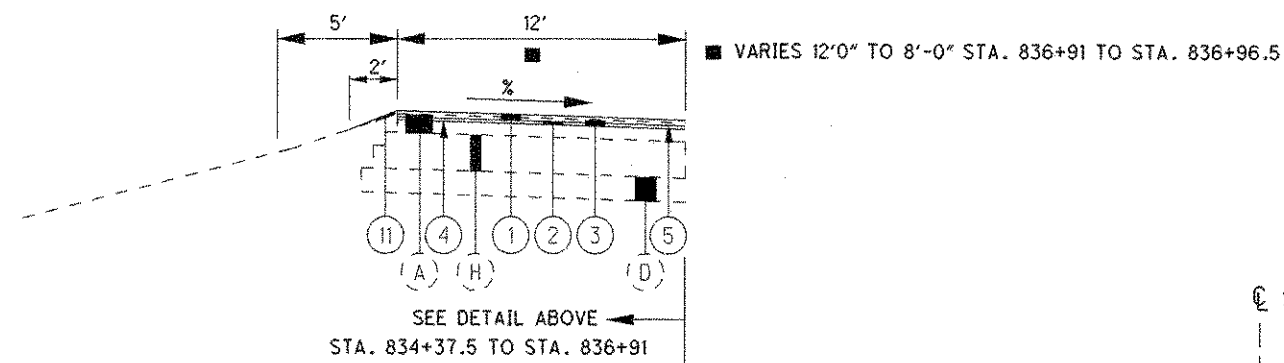
- VARIES 26' TO 51'± STA. 833+25 TO STA. 833+75
- VARIES 51'± TO 36' STA. 833+75 TO STA. 834+29.25

- # VARIES 8' TO 0' STA. 820+60 TO 820+87
- # VARIES 0' TO 8' STA. 821+13 TO STA. 821+40

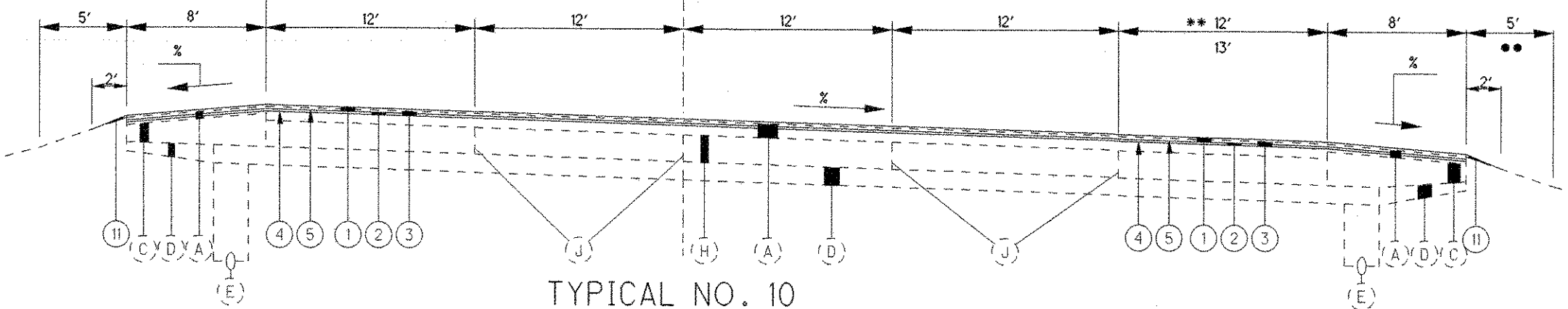
TYPICAL SECTIONS

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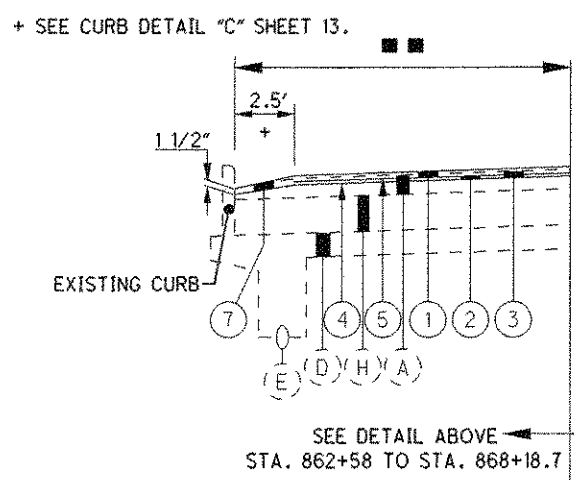


●● FOR GUARDRAIL AREAS SEE PLAN SHEETS 39 & 40.
STA. 841+50± TO STA. 842+39.93



TYPICAL NO. 10
SUPERELEVATED SECTION

LIMITING STATIONS
STA. 834+03.50 TO STA. 842+39.93 = 836.43 FT.



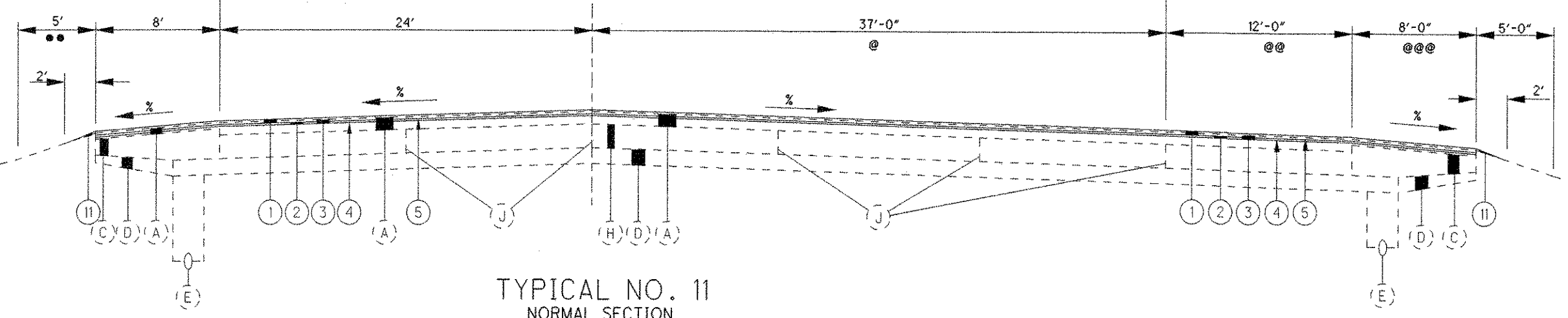
% EXISTING PAVEMENT SLOPE (TYP)

☉ SURVEY S.R. 725

- VARIES 37' TO 36' STA. 862+76.37 TO STA. 866+00
- VARIES 36' TO 32.6' STA. 866+00 TO STA. 868+48.2
- VARIES 32.6' TO 33.8' STA. 866+48.2 TO STA. 869+55

FOR RAMP "M" SEE TYPICAL NO. 15

- VARIES 8' TO 0' STA. 869+52 TO STA. 869+69
- VARIES 0' TO 39' STA. 851+08.76 TO STA. 852+44.48
- VARIES 2'-0" TO 12'-0"± STA. 857+09 TO STA. 858+00.00
- 12'-0" TO 0' STA. 860+05 TO STA. 862+76.37



TYPICAL NO. 11
NORMAL SECTION

LIMITING STATIONS

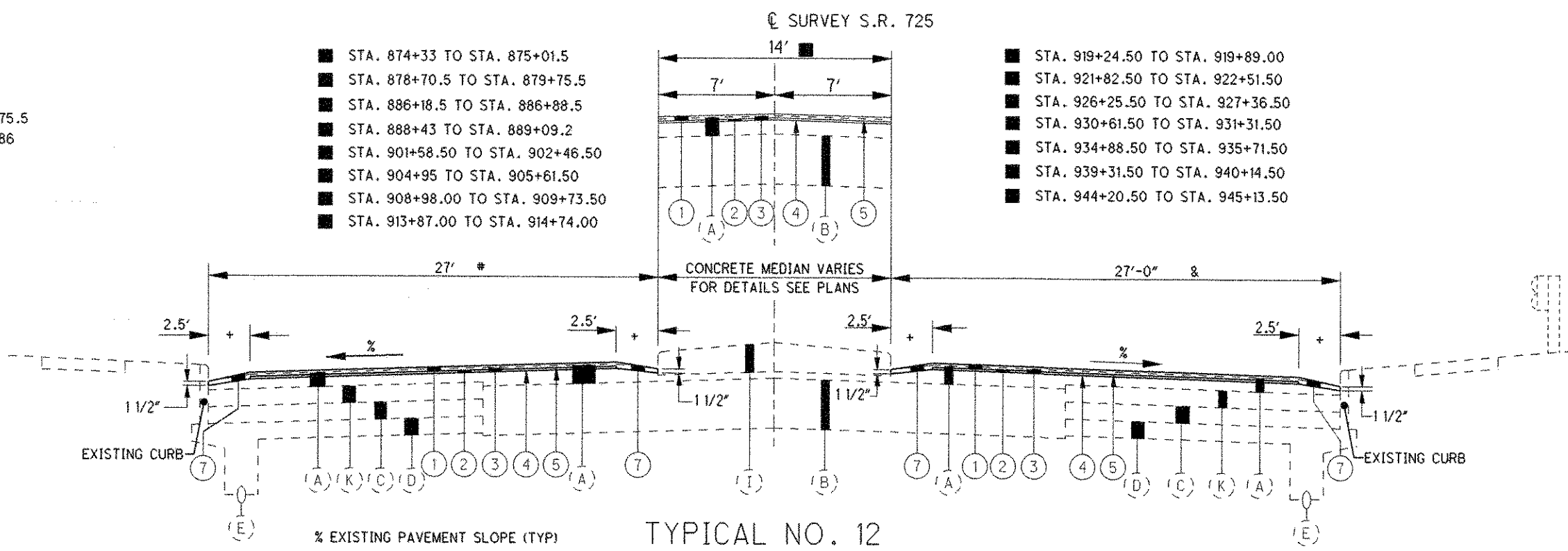
STA. 842+39.93 TO STA. 847+24.70 = 484.77 FT.
 STA. 847+24.70 TO STA. 851+46.26 = (BRIDGE, APPROACH SLABS & TRANSITION)
 STA. 851+46.26 TO STA. 854+52.04 = 343.28 FT.
 STA. 854+52.04 TO STA. 857+61.20 = (BRIDGE, APPROACH SLABS & TRANSITION)
 STA. 857+61.20 TO STA. 867+55.25 = 994.05 FT.
 STA. EQU. STA. 867+55.25 BACK = STA. 867+57.35 AHEAD
 STA. 867+57.35 TO STA. 869+55.00 = 197.65 FT.

- FOR GUARDRAIL AREAS SEE PLAN SHEETS 25-53.
STA. 842+66± TO STA. 858+92
- VARIES 15.5' TO 13.9' STA. 862+58 TO STA. 863+04
VARIES 13.9' TO 19.7' STA. 863+04 TO STA. 869+55

TYPICAL SECTIONS

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- * 47.5' STA. 869+55 TO STA. 871+50
- * VARIES 47.5' TO 37.5' STA. 871+50 TO STA. 872+00
- * 37.5' STA. 872+00 TO STA. 875+01.5
- * 48.5' STA. 875+01.5 TO STA. 876+50
- * VARIES 48.5' TO 37.5' STA. 876+50 TO STA. 877+00
- * 37.5' STA. 877+00 TO STA. 878+49.84
- * VARIES 37.5' TO 34.3' STA. 878+49.84 TO STA. 879+75.5
- * VARIES 45.3' TO 38' STA. 879+75.5 TO STA. 879+97.86
- * 38' STA. 879+97.86 TO STA. 882+25
- * VARIES 38' TO 27' STA. 882+25 TO STA. 882+75
- * 38' STA. 886+88.5 TO STA. 887+75
- * VARIES 38' TO 27' STA. 887+75 TO STA. 888+25
- * 38' STA. 889+09.2 TO STA. 890+58
- * VARIES 38' TO 27' STA. 890+58 TO STA. 891+08
- * 38' STA. 902+46.5 TO STA. 903+95
- * VARIES 38' TO 27' STA. 903+95 TO STA. 904+95
- * 38' STA. 905+61.50 TO STA. 907+10
- * VARIES 38' TO 27' STA. 907+10 TO STA. 907+60
- * 38' STA. 909+73.50 TO STA. 911+22
- * VARIES 38' TO 27' STA. 911+22 TO STA. 911+72
- * 38' STA. 914+74 TO STA. 917+26
- * VARIES 38' TO 27' STA. 917+26 TO STA. 917+76
- * 38' STA. 922+51.50 TO STA. 924+00
- * VARIES 38' TO 27' STA. 924+00 TO STA. 924+50
- * 38' STA. 927+36.50 TO STA. 928+63
- * VARIES 38' TO 27' STA. 928+63 TO STA. 929+13
- * 38' STA. 931+31.50 TO STA. 932+80
- * VARIES 38' TO 27' STA. 932+80 TO STA. 933+30
- * 38' STA. 935+71.50 TO STA. 937+20
- * VARIES 38' TO 27' STA. 937+20 TO STA. 937+70
- * 38' STA. 940+14.50 TO STA. 941+63
- * VARIES 38' TO 27' STA. 941+63 TO STA. 942+13
- * 38' STA. 945+13.50 TO STA. 947+27
- * VARIES 38' TO 27' STA. 947+27 TO STA. 947+77



- STA. 874+33 TO STA. 875+01.5
- STA. 878+70.5 TO STA. 879+75.5
- STA. 886+18.5 TO STA. 886+88.5
- STA. 888+43 TO STA. 889+09.2
- STA. 901+58.50 TO STA. 902+46.50
- STA. 904+95 TO STA. 905+61.50
- STA. 908+98.00 TO STA. 909+73.50
- STA. 913+87.00 TO STA. 914+74.00

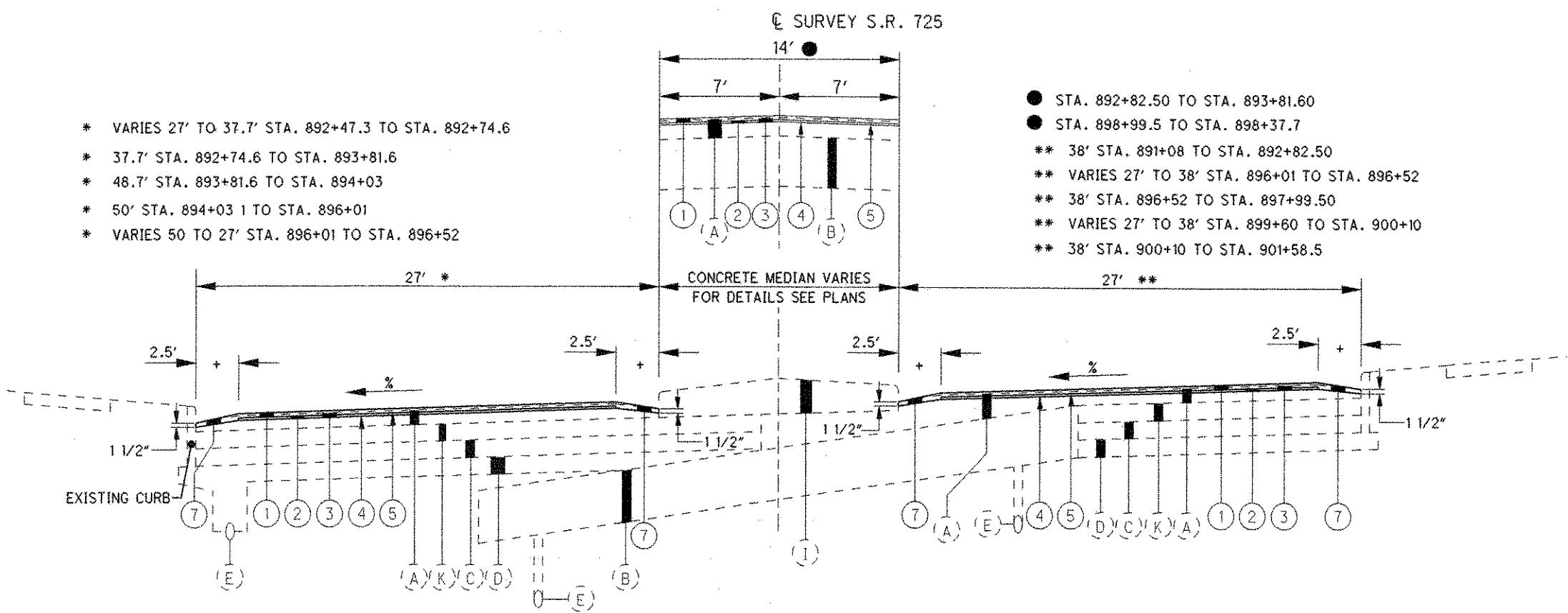
- STA. 919+24.50 TO STA. 919+89.00
- STA. 921+82.50 TO STA. 922+51.50
- STA. 926+25.50 TO STA. 927+36.50
- STA. 930+61.50 TO STA. 931+31.50
- STA. 934+88.50 TO STA. 935+71.50
- STA. 939+31.50 TO STA. 940+14.50
- STA. 944+20.50 TO STA. 945+13.50

TYPICAL NO. 12
NORMAL SECTION

LIMITING STATIONS
 STA. 869+55.00 TO STA. 891+08.00 = 2153 FT.
 STA. 901+58.50 TO STA. 948+89.50 = 4731 FT.
 STA. 948+89.50 TO STA. 949+27.00 = TRANSITION

+ SEE CURB DETAIL "C" SHEET 13.

- * VARIES 27' TO 37.7' STA. 892+47.3 TO STA. 892+74.6
- * 37.7' STA. 892+74.6 TO STA. 893+81.6
- * 48.7' STA. 893+81.6 TO STA. 894+03
- * 50' STA. 894+03.1 TO STA. 896+01
- * VARIES 50 TO 27' STA. 896+01 TO STA. 896+52



- STA. 892+82.50 TO STA. 893+81.60
- STA. 898+99.5 TO STA. 898+37.7
- ** 38' STA. 891+08 TO STA. 892+82.50
- ** VARIES 27' TO 38' STA. 896+01 TO STA. 896+52
- ** 38' STA. 896+52 TO STA. 897+99.50
- ** VARIES 27' TO 38' STA. 899+60 TO STA. 900+10
- ** 38' STA. 900+10 TO STA. 901+58.5

TYPICAL NO. 13
SUPERLEVATED SECTION

LIMITING STATIONS
 STA. 891+08.00 TO STA. 901+58.50 = 1050.5 FT.

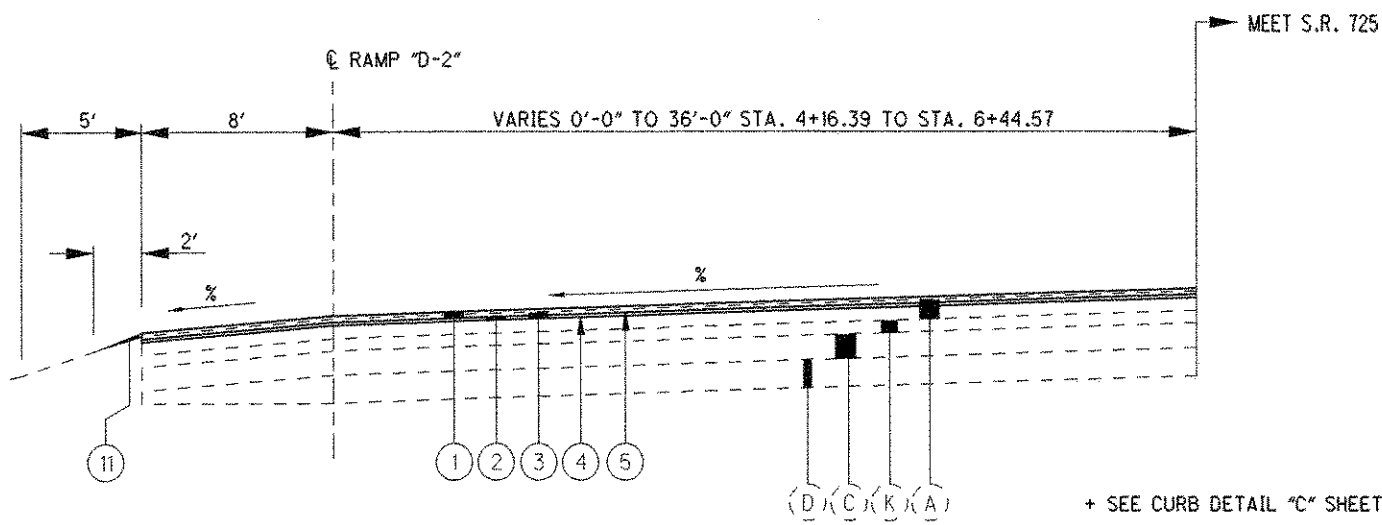
- & VARIES 27' TO 38' STA. 876+50 TO STA. 877+00
- & 38' STA. 877+00 TO STA. 878+70.5
- & VARIES 27' TO 38' STA. 884+20 TO STA. 884+70
- & 38' STA. 884+70 TO STA. 886+18.5
- & VARIES 27' TO 38' STA. 890+58 TO STA. 891+08
- & VARIES 27' TO 38' STA. 911+22 TO STA. 911+72
- & 38' STA. 911+72 TO STA. 913+87
- & VARIES 27' TO 38' STA. 917+26 TO STA. 917+76
- & 38' STA. 917+76 TO STA. 919+24.5
- & VARIES 27' TO 38' STA. 920+50 TO STA. 920+90
- & 38' STA. 920+90 TO STA. 921+82.50
- & VARIES 27' TO 38' STA. 924+00 TO STA. 924+50
- & 38' STA. 924+50 TO STA. 926+25.5
- & VARIES 27' TO 38' STA. 928+63 TO STA. 929+13
- & 38' STA. 929+13 TO STA. 930+61.5
- & VARIES 27' TO 38' STA. 932+80 TO STA. 933+30
- & 38' STA. 933+30 TO STA. 934+88.50
- & VARIES 27' TO 38' STA. 937+20 TO STA. 937+70
- & 38' STA. 937+70 TO STA. 939+31.5
- & VARIES 27' TO 38' STA. 941+63 TO STA. 942+13
- & 38' STA. 942+13 TO STA. 944+20.5
- & VARIES 27' TO 38' STA. 947+27 TO STA. 947+77
- & 38' STA. 947+77 TO STA. 948+89.50

TYPICAL SECTIONS

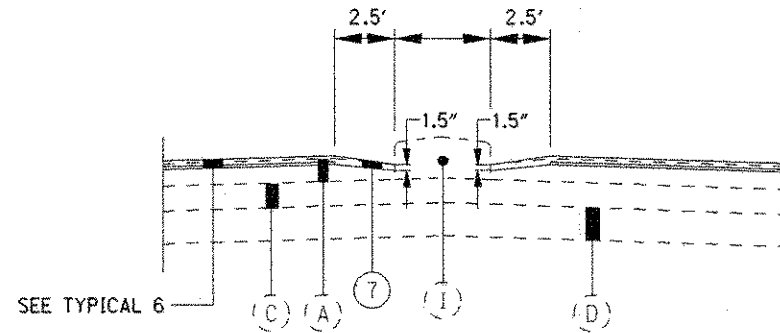
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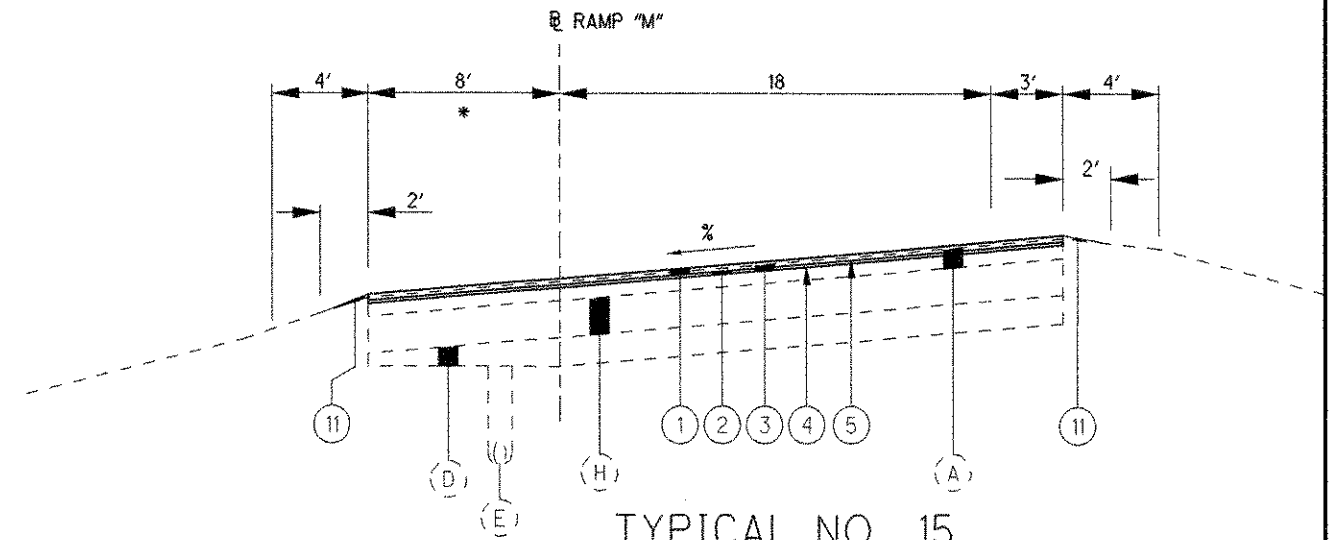


TYPICAL NO. 14
 RAMP "D-2" I.R. 75 AND S.R. 725
 SUPERELEVATED SECTION
 LIMITING STATIONS
 STA. 4+16.39 TO STA. 6+44.57 = 228.18 FT.

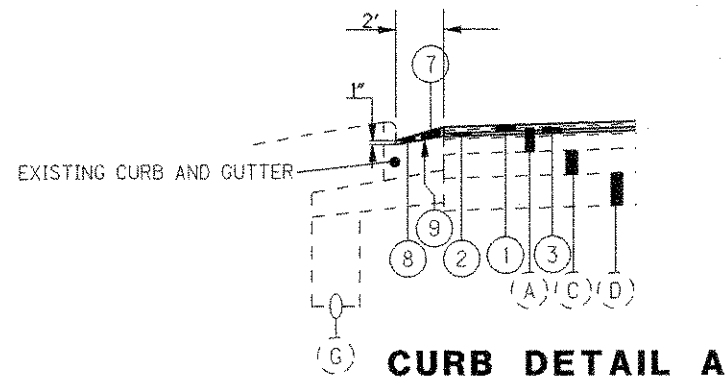


CURB DETAIL "C"

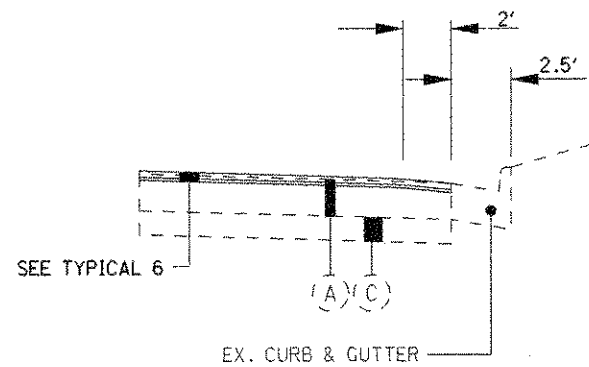
* VARIES 8' TO 6' STA. 118+57.25 TO 118+97.25



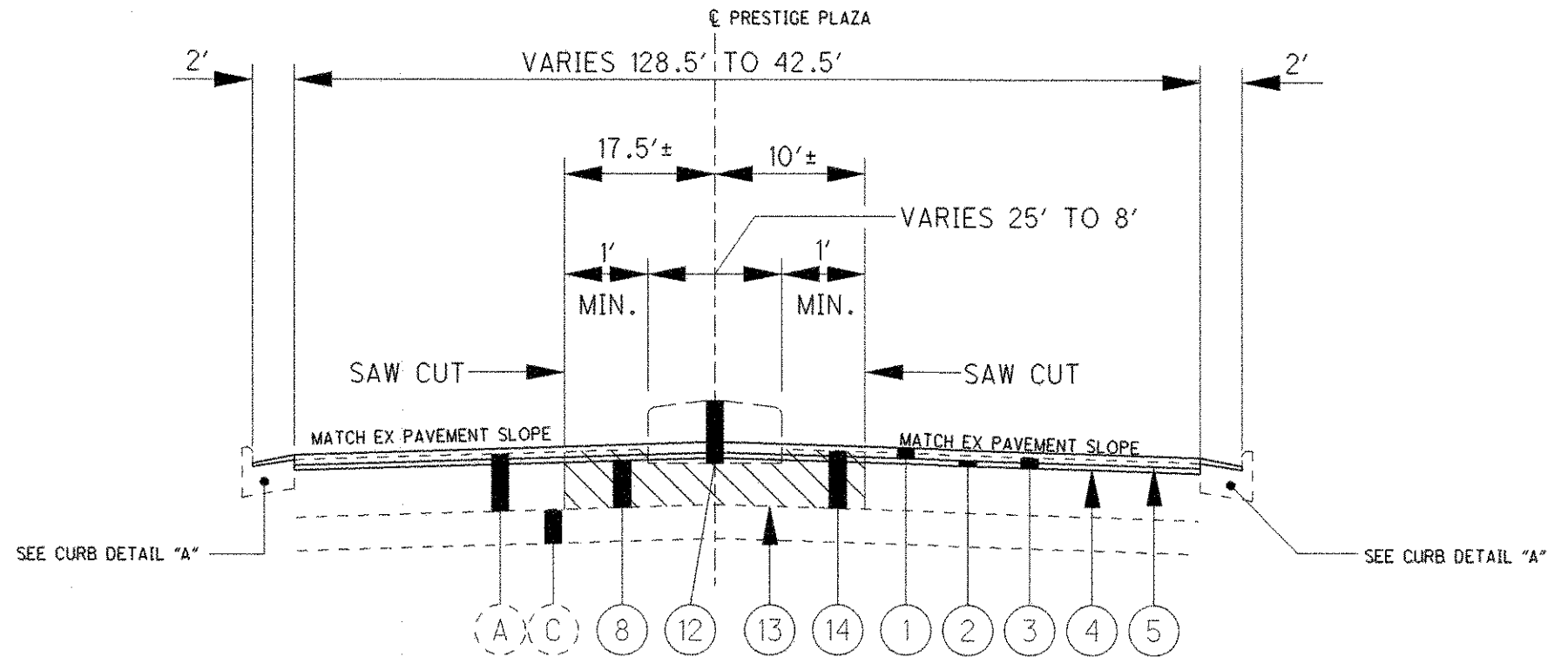
TYPICAL NO. 15
 RAMP "M" I.R. 675 AND S.R. 725
 SUPERELEVATED SECTION
 LIMITING STATIONS
 STA. 118+57.45 TO STA. 127+31.86 = 874.41 FT.
 STA. 127+31.86 TO STA. 127+69.36 = TRANSITION.



CURB DETAIL A



CURB DETAIL B



TYPICAL NO. 16
 NORMAL SECTION
 LIMITING STATIONS
 STA. 0+45 TO STA. 0+82.5 = 37.5 FT.

TYPICAL SECTIONS

MOT-725-13.8.2

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

TELEPHONE		
CINCINNATI BELL 221 EAST FOURTH ST., RM 343 CINCINNATI, OHIO 45202 513-397-9900 Mark Connor	AT&T OHIO 3233 WOODMAN DRIVE, RM 225 DAYTON, OHIO 45420 937-296-3704 Shawn Jenkins	
GAS		
VECTREN ENERGY DELIVERY 4285 JAMES H. MCGEE BLVD. DAYTON, OHIO 45427 937-440-1965 Don Specht	CABLE TV TIME WARNER CABLE 3691 TURNER ROAD DAYTON, OHIO 45415 937-425-8850 Tim Kuss	LEVEL 3 COMMUNICATIONS, LLC 400 PIKE STREET CINCINNATI, OHIO 45202 513-632-1806 Tim Taylor
ELECTRIC		
DAYTON POWER & LIGHT 1900 DRYDEN ROAD DAYTON, OHIO 45439 937-331-4132 John Kenton	WATER/SANITARY MONTGOMERY COUNTY SANITARY 1850 SPAULDING RD. DAYTON, OHIO 45439 937-297-5025 ED SCHLAAK	CITY OF MIAMISBURG 10 NORTH FIRST STREET MIAMISBURG, OHIO 45342 937-847-6456 ROBERT STANLEY
HIGHWAY LIGHTING OHIO DEPARTMENT OF TRANSPORTATION 1001 ST. MARYS AVE. SIDNEY, OHIO 45365 937-497-6834	CITY OF WEST CARROLLTON 300 E. CENTRAL AVE. WEST CARROLLTON, OHIO 45449 937-859-3183 G.TRACY WILLIAMS	

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS OF THE UTILITY AS REQUIRED BY SECTION 153.64 O.R.C.

EXISTING PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 7 OFFICE IN SIDNEY, OHIO.

ELEVATION DATUM

ALL ELEVATIONS ARE ORTHOMETRIC HEIGHTS USING THE NORTH AMERICAN VERTICLE DATUM OF 1988 (NAVD 88) AND THE GEOID03 GEOID. HORIZONTAL POSITIONS ARE BASED ON THE OHIO STATE PLANE SOUTH ZONE, A LAMBERT CONFORMAL CONIC MAP PROJECTION, THE NORTH AMERICAN DATUM OF 1983 ADJUSTED TO THE NATIONAL SPATIAL REFERENCE SYSTEM OF 2007 (NAD83NSRS 2007)), AND THE GRS80 ELLIPSOID.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ITEM 448 - 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-28

THE CONTRACTOR SHALL TAKE CARE DURING THE PLANING OPERATION NOT TO REMOVE MORE MATERIAL THAN HE CAN REPLACE WITH THE ASPHALT CONCRETE INTERMEDIATE COURSE,PER DAILY OPERATION.

ITEM 201- CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

ITEM 608- CURB RAMPS, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION OF THE CURB RAMPS AND TRUNCATED DOMES AT LOCATIONS INDICATED IN THESE PLANS. THE CONTRACTOR SHALL CONFIRM WITH THE PROJECT ENGINEER THE EXACT LOCATIONS AND PLACEMENT OF EACH CURB RAMP AND TRUNCATED DOME. EACH INSTALLATION SHALL BE A NEW COMPLETE INSTALLATON, ALL EXISTING CURB RAMPS AND WALK SHALL BE REMOVED. THE CONTRACTOR SHALL MAKE THESE INSTALLATIONS IN ACCORDANCE WITH THE 2008 CMS AND STANDARD DRAWING BP-7.1 - NEW CURB RAMPS (with Truncated Domes) dated 1/19/07. WITH THE EXCEPTION THAT THE REMOVAL OF EXISTING WALKS,RAMPS, AND CURB BE INCLUDED. ALL WORK, LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER SQUARE FOOT FOR ITEM 608 - CURB RAMPS, AS PER PLAN

ITEM 519- PATCHING CONCRETE STRUCTURE, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PATCHING OF EXISTING CONCRETE INLET COVERS AT LOCATIONS INDICATED IN THESSE PLANS. THE CONTRACTOR SHALL PERFORM THIS WORK IN ACCORDANCE WITH THE 2008 CMS, EXCEPT THAT 35 LB MAX PNEUMATIC HAMMER SHALL BE USED. THE CONTRACTOR SHALL TAKE CARE NOT TO CAUSE FURTHER DAMAGE TO THE INLET COVERS.. THE METHOD HE CHOOSE SHALL FIRST GAIN THE APPROVAL OF THE PROJECT ENGINEER PRIOR TO THE START OF THIS WORK. ALL WORK, LABOR,MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THIS WORK SHALL BE PAID FOR UNDER THE UNIT PRICE BID PER SQUARE FOOT FOR ITEM 519- PATCHING CONCRETE STRUCTURE, AS PER PLAN

ITEM 621. RAISED PAVEMENT MARKERS REMOVED

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE TRAFFIC CONTROL GENERAL SUMMARY FOR THE REMOVAL AND DISPOSAL OF THE RAISED PAVEMENT MARKERS BY THE CONTRACTOR.

ITEM 621, RAISED PAVEMENT MARKER REMOVED	1537 EACH
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ITEM 446 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1H, PG76-22M, AS PER PLAN

ALL REQUIREMENTS OF ITEM 446 - ASPHALT CONCRETE SHALL APPLY WITH THE FOLLOWING ADDITIONS.

441.02 - THE COURSE AGGREGATE FOR TYPE 1H SHALL BE ONE HUNDRED PRECENT (100%) CRUSHED MATERIAL. THE ASPHALT BINDER USED SHE BE PG 76-22M.

IN ADDITION TO THE REQUIREMENTS OF 401.12, USE A MATERIAL TRANSFER DEVICE (MTD) FOR ALL OF THE SURFACE COURSE MAINLINE PAVING. USE A MATERIAL TRANSFER DEVICE CAPABLE OF REMIXING AND TRANSFERING THE ASPHALT MIXTURE FROM THE MATERIAL TRANSFER DEVICE TO THE ASPHALT SPREADING EQUIPMENT. ALL LABOR AND EQUIPMENT ASSOCIATED WITH THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER CUBIC YARD FOR ITEM 446 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1H, PG 76-22M, AS PER PLAN.

ITEM 202- CONCRETE MEDIAN REMOVED, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF THE EXISTING CONCRETE MEDIAN, USING A SAW CUT (I) ONE FOOT OUTSIDE THE EXISTING CONCRETE MEDIAN, UNLESS OTHERWISE SHOWN IN PLANS. THE CONTRACTOR SHALL TAKE CARE NOT TO DISTURB ANY EXISTING PAVEMENT OUTSIDE OF THE SAW CUT LINE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ADDITIONAL MATERIAL WITHIN THE PROPOSED SAW CUT LINES AS NOTED IN THESE PLANS. ALL WORK, LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER SQUARE YARD FOR ITEM 202 - CONCRETE MEDIAN REMOVED, AS PER PLAN.

ITEM 202- TRAFFIC ISLAND REMOVED, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF THE EXISTING CONCRETE ISLAND, USING A SAW CUT (I) ONE FOOT OUTSIDE THE EXISTING CONCRETE ISLAND, UNLESS OTHERWISE SHOWN IN PLANS. THE CONTRACTOR SHALL TAKE CARE NOT TO DISTURB ANY EXISTING PAVEMENT OUTSIDE OF THE SAW CUT LINE DURING EXCAVATION, RESULTING IN ADDITIONAL PAVEMENT REPAIR. ALL WORK, LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER SQUARE YARD FOR ITEM 202 - CONCRETE ISLAND REMOVED, AS PER PLAN.

ITEM 609- CONCRETE MEDIAN, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING THE PROPOSED CONCRETE MEDIAN IN ACCORDANCE WITH THE 2008 CMS, CURRENT STANDARD DRAWING RM-3.1, AND THESE PLANS. IT SHALL ALSO THE CONTRACTORS RESPONSIBILITY TO RECOGNIZE THE PLACEMENT OF BOTH THE 18" MEDIAN AND THE 24" MEDIAN AS NOTED IN THESE PLANS. ANY CHANGES FROM THESE PLANS BY THE CONTRACTOR SHALL BE APPROVED BY THE PROJECT ENGINEER BEFORE ANY WORK BEGINS. ALL WORK, LABOR, EQUIPMENT, INCIDENTALS, AND MATERIALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER CUBIC YARD FOR ITEM 609 - CONCRETE MEDIAN, AS PER PLAN.

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GENERAL NOTES

MOT-725-13.82

GENERAL REQUIREMENTS

IT IS THE INTENTION TO PERFORM THE REQUIRED WORK WITHIN THESE PLANS WITH THE LEAST INCONVENIENCE TO AND THE MAXIMUM SAFETY OF THE CONTRACTOR AND THE TRAVELING PUBLIC. THE REQUIREMENTS FOR MAINTAINING TRAFFIC AS SPECIFIED IN THE "OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREET AND HIGHWAYS" (CURRENT EDITION, LATEST REVISION), PERTINENT PROVISIONS OF THE "OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS" (INCLUDING SUPPLEMENTAL SPECIFICATIONS) AND APPLICABLE STANDARD CONSTRUCTION DRAWINGS SHALL APPLY TO THIS PROJECT IN ADDITION TO THE FOLLOWING NOTES AND DETAILS.

ITEM 614 - MAINTAINING TRAFFIC

THE FOLLOWING WORK SHALL BE PERFORMED:

REMOVE CONCRETE MEDIAN ON S.R.-725 @ MALL WOODS DR & PRESTIGE PLAZA AS INDICATED IN THESE PLANS.

REMOVE CONCRETE TRAFFIC ISLAND AT INTERSECTION OF PRESTIGE PLAZA & S.R.-725 AS INDICATED IN THESE PLANS.

RESURFACE S.R.-725 AND RAMP "M" AT I.R.-675 INTERCHANGE.

ADJUST MANHOLES, CATCH BASINS, VALVES BOXES, AND MONUMENT BOXES TO GRADE

REPLACE LOOP DETECTORS WHERE INDICATED IN PLANS.

CONSTRUCT NEW CURB RAMPS WITH TRUNCATED DOMES.

PATCHING OF EXISTING CONCRETE INLET COVERS.

RECONSTRUCT CATCH BASINS TO GRADE WHERE INDICATED IN PLANS.

UPGRADE SIGNALS AT INTERSECTION OF S.R.-725 @ MALL WOODS DR & PRESTIGE PLAZA

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SAFE AND EFFECTIVE TRAFFIC CONTROL 24 HOURS A DAY FOR THE DURATION OF THIS PROJECT. THIS INCLUDES PROVIDING, PLACING, MAINTAINING, AND SUBSEQUENTLY REMOVING ALL NECESSARY TRAFFIC CONTROL MEASURES FOR ALL PROPOSED CONSTRUCTION OPERATIONS.

THE CONTRACTOR SHALL CONTACT THE ODOT DISTRICT 7 ROADWAY SERVICE OFFICE (937) 497-6890 A MINIMUM OF 14 CALENDAR DAYS PRIOR TO INTENDED LANE RESTRICTIONS/ROAD CLOSURES TO ALLOW SUFFICIENT TIME FOR THE NECESSARY COORDINATION.

BEFORE WORK BEGINS, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THE NAME(S) AND TELEPHONE NUMBER(S) OF PERSON(S) WHO CAN BE CONTACTED 24 HOURS A DAY BY THE OHIO DEPARTMENT OF TRANSPORTATION, THE HIGHWAY PATROL, AND ALL OTHER INTERESTED POLICE AGENCIES.

THIS PERSON(S) SHALL BE RESPONSIBLE FOR REPAIRING AND OR REPLACING ALL TRAFFIC CONTROL DEVICES NEEDED TO MAINTAIN SAFETY FOR THE DURATION OF THIS PROJECT. THIS PERSON(S) SHALL HAVE AVAILABLE ALL MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO PERFORM THE REQUIRED REPAIRS WITHIN A REASONABLE PERIOD OF TIME.

THE CONTRACTOR SHALL PROVIDE, ERECT, AND MAINTAIN SIGNS (IN PROPER POSITION, CLEAN AND LEGIBLE, AND IN GOOD WORKING CONDITION) AND REMOVE ALL LIGHTS, SIGNS, CONES, DRUMS, AND ANY OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC ACCORDING TO THESE PLAN NOTES AND DETAILS.

THE CONTRACTOR SHALL FURNISH AND INSTALL ADVANCE WARNING "ROAD WORK AHEAD" (W20-1) SIGNS AND "END ROAD WORK" (G20-2) SIGNS ON SR-725. THE SIGNS SHALL BE DUAL INSTALLATIONS AND THE ACTUAL LOCATION SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

VEHICLES AND OTHER EQUIPMENT SHALL NOT BE PERMITTED TO STOP OR TO BE PARKED ALONG THE ROADWAY EXCEPT WITHIN DESIGNATED WORK AREAS AND SHALL NOT ENTER OR LEAVE WORK AREAS IN A MANNER WHICH WILL BE HAZARDOUS TO, OR INTERFERE WITH THE NORMAL FLOW OF TRAFFIC. PERSONAL VEHICLES WILL NOT BE PERMITTED TO PARK WITHIN THE RIGHT-OF-WAY EXCEPT WITHIN SPECIFIC AREAS DESIGNATED BY THE ENGINEER.

ACCESS TO AND FROM ALL CROSS ROADS WITHIN THE LIMITS OF THIS PROJECT SHALL BE MAINTAINED AT ALL TIMES ON EITHER THE EXISTING OR PROPOSED PAVEMENTS, UNLESS OTHERWISE SHOWN IN THESE PLANS OR OTHERWISE DIRECTED BY THE ENGINEER.

TRAFFIC SHALL BE MAINTAINED IN A UNIFORM PATTERN THROUGHOUT THE ENTIRE LENGTH OF THE PROJECT AND SHALL NOT BE SUBJECTED TO CONSTANT LANE SHIFTS.

RIGHT AND LEFT TURN LANES SHALL BE MAINTAINED IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWINGS OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY INTENDED CHANGES TO ANY EXISTING OR TEMPORARY TRAFFIC CONTROL DEVICES AND SHALL OBTAIN THE ENGINEER'S APPROVAL PRIOR TO MAKING THE CHANGES. THE CONTRACTOR SHALL ALSO NOTIFY THE ENGINEER FORTY-EIGHT (48) HOURS IN ADVANCE OF ANY INTENDED LANE CLOSURES, OR LANE SHIFTS.

THE USE OF EXISTING SHOULDER AREAS TO MAINTAIN TRAFFIC IS PROHIBITED, UNLESS OTHERWISE APPROVED BY THE ENGINEER. SHOULD ANY EXISTING OR NEW SHOULDER AREA USED TO MAINTAIN TRAFFIC BECOME DAMAGED OR DESTROYED DUE TO THE NEGLIGENCE OR FAILURE TO PROVIDE ADEQUATE SIGNS OR OTHER APPROPRIATE TRAFFIC CONTROL DEVICES, THE RESTORATION OR REPLACEMENT OF THE SHOULDER AREA WILL BE AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL BE PERMITTED TO CLOSE ONE LANE OF SR-725 DIRECTIONAL TRAFFIC AT A TIME IN ORDER TO PERFORM THE REQUIRED WORK. HOWEVER A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF EITHER THE EXISTING OR COMPLETED PAVEMENTS.

UNLESS OTHERWISE NOTED IN THESE PLANS, THE STANDARD CHANNELIZING DEVICE FOR CLOSING ANY LANE TO TRAFFIC SHALL BE PROPERLY WEIGHTED AND REFLECTORIZED PLASTIC DRUMS AND OR GRABBER CONES LOCATED AND SPACED ACCORDING WITH APPLICABLE STANDARD DRAWINGS OR PLAN NOTES AND DETAILS.

NO AREA OF PAVEMENT PLANING SHALL BE OPENED TO THE TRAVELING PUBLIC. IT IS THE INTENT OF THE OHIO DEPARTMENT OF TRANSPORTATION THAT THE PAVEMENT PLANING AND THE PLACEMENT OF ITEM 448 - ASPHALT INTERMEDIATE COURSE BE IN CONJUNCTION WITH EACH OTHER ON A DAILY BASIS PRIOR TO OPENING THE ROAD TO THE TRAVELING PUBLIC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ASSURING THAT THIS IS A COMPLETE PROCESS EACH DAY.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE TRAFFIC SIGNALS LOCATED WITHIN THE LIMITS OF THIS PROJECT FOR THE DURATION OF THE PROJECT.

ALL SIGNALS WITHIN THE LIMITS OF THIS PROJECT ARE THE JURISDICTION OF THE OHIO DEPARTMENT OF TRANSPORTATION, EXCEPT FOR THOSE AT THE FOLLOWING LOCATIONS: ALEXANDERVILLE RD., IMPERIAL RD., AND BYERS RD., ARE UNDER THE JURISDICTION OF THE CITY OF MIAMISBURG.

THE CONTRACTOR SHALL PROVIDE THE DISTRICT TRAFFIC ENGINEER AND THE CITY OF MIAMISBURG WITH WRITTEN PRELIMINARY NOTIFICATION OF AT LEAST FOURTEEN (14) WORKING DAYS PRIOR TO THE INTENDED START OF ANY CONSTRUCTION ACTIVITIES AFFECTING THE OPERATIONS OF EXISTING TRAFFIC SIGNALS.

FINAL NOTIFICATION OF THE INTENDED START OF ANY CONSTRUCTION ACTIVITIES AFFECTING THE OPERATION OF THE EXISTING TRAFFIC SIGNALS SHALL BE PROVIDED FIVE (5) WORKING DAYS PRIOR TO THE START OF ANY SUCH OPERATIONS.

CONTACTS:
CRAIG ELEY (937) 497-6832 ODOT DISTRICT 7 TRAFFIC
ROBERT STANLEY (937) 847-6531 CITY OF MIAMISBURG

THE CONTRACTOR AND THE PROJECT ENGINEER SHALL COOPERATE TO KEEP THE DISTRICT TRAFFIC ENGINEER AND THE CITY OF MIAMISBURG INFORMED OF THE CURRENT STATUS OF ALL LANE CLOSURES AND/OR LANE SHIFTS IN EFFECT AT ANY GIVEN TIME DURING THE CONSTRUCTION OF THIS PROJECT.

ALL CONCRETE MEDIAN REMOVAL, CONCRETE TRAFFIC ISLAND REMOVAL, SIGNAL WORK, GRINDING AND PAVING OPERATIONS ALONG SR 725 THROUGHOUT THE LIMITS OF THIS PROJECT, TO INCLUDE RAMP "L" AT IR-675 SHALL BE DONE AS A NIGHT TIME OPERATION. THE CONTRACTOR SHALL BE PERMITTED TO PERFORM THESE OPERATIONS BETWEEN THE HOURS OF 8 P.M. AND 7 A.M. SUNDAY NIGHT THRU FRIDAY MORNING. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE THE ROADWAY READY AND OPEN TO TRAFFIC WITH IN THE PROPOSED TIME LIMIT.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	NEW YEARS
MEMORIALDAY	FOURTH OF JULY
LABOR DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

WEEK DAY	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA WIDE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614. REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614. REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 30 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

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ITEM 614. LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEO'S) BY CONTRACTORS OTHER THAN THE USE SPECIFIED IN THIS NOTE WILL NOT GENERALLY BE PERMITTED AT PROJECT COST UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE ENGINEER. LEO'S SHOULD NOT BE USED WHERE THE OHIO MANUAL OF UNIFORMED TRAFFIC CONTROL DEVICES (OMUTCD) INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

- * FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED. IN GENERAL, LEOs SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.
- * DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
- * DURING A TRAFFIC SIGNAL INSTALLATION OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (e.g., DIRECTING MOTORIST THROUGH A RED LIGHT).

LEOs SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORIST FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOs WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOs AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOs. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOs DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A LIST OF THE APPROPRIATE LAW ENFORCEMENT AGENCY(S), INCLUDING ADDRESS AND TELEPHONE NUMBER.

THE LEO SHOULD REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT TO RECIEVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING THE SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF THE SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHOULD NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF THE SHIFT.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614- LAW ENFORCEMENT OFFICER (WITH PATROL CAR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR 150 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COST (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR.

EXISTING PAVEMENT MARKINGS

EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE MAINTENANCE OF TRAFFIC SCHEMES SHALL BE REMOVED BY THE CONTRACTOR. PAYMENT FOR THE REMOVAL OF EXISTING CONFLICTING PAVEMENT MARKINGS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614. WORKZONE PAVEMENT MARKINGS

THE FOLLOWING ITEMS AND QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USE BY THE CONTRACTOR FOR WORK ZONE PAVEMENT MARKINGS. THE CONTRACTOR SHALL APPLY THESE MARKING IN ACCORDANCE WITH 2008 C.M.S., UNLESS OTHERWISE DIRECTED BY THE ENGINEER:

- WORK ZONE LANE LINE, CLASS III, 642 PAINT.....16 MILES
- WORK ZONE CENTER LINE, CLASS III, 642 PAINT.....4 MILES
- WORK ZONE EDGE LINE, CLASS III, 642 PAINT.....27 MILES
- WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT...33920 FT
- WORK ZONE STOP LINE, CLASS III, 642 PAINT.....5510 FT
- WORK ZONE CROSSWALK LINE, CLASS III, 642 PAINT....5540 FT
- WORK ZONE ARROW, CLASS III, 642 PAINT.....120 EACH

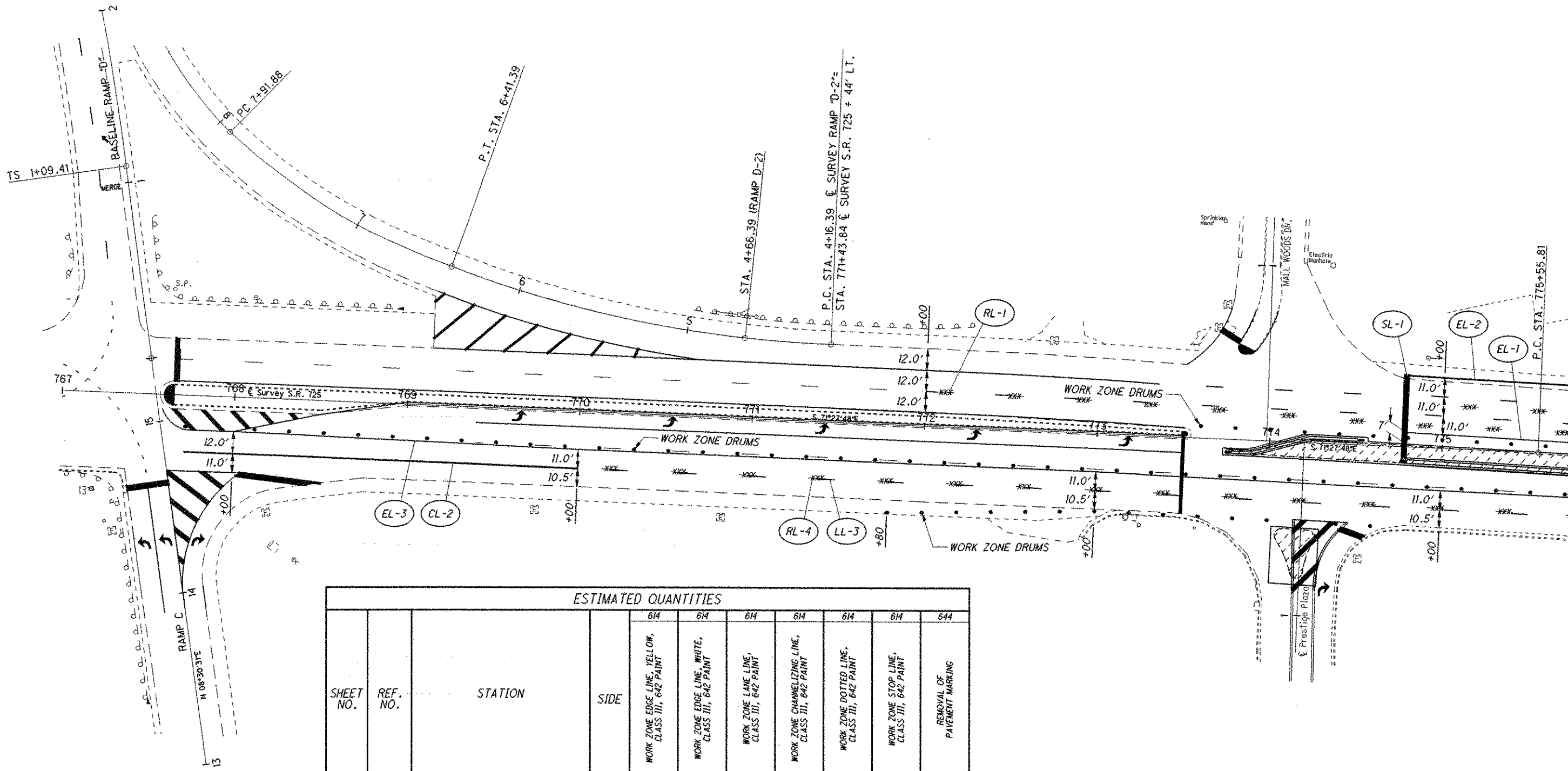
ALL WORK, LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR EACH ITEM AS SET FORTH IN THE 2008 C.M.S.

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		ESTIMATED QUANTITIES										
SHEET NO.	REF. NO.	STATION		SIDE	614		614		614		644	
		FROM	TO		WORK ZONE EDGE LINE, YELLOW, CLASS III, 642 PAINT	WORK ZONE EDGE LINE, WHITE, CLASS III, 642 PAINT	WORK ZONE LANE LINE, CLASS III, 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT	WORK ZONE DOTTED LINE, CLASS III, 642 PAINT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	REMOVAL OF PAVEMENT MARKING	
					MILE	MILE	MILE	FT	FT	FT	FT	
17A	EL-1	774+80	781+00	LT	0.12							
17A	EL-2	774+80	779+00	LT		0.08						
17B	LL-1	773+50	777+00	LT			0.07					
17B	LL-2	772+00	778+00	LT			0.12					
17B	DL-1	777+00	779+00	LT					200			
17B	CL-1	778+00	781+00	LT				300				
17A	SL-1	774+77	-	LT						2		
17A	RL-1	772+00	781+00	LT							900	
17B	RL-2	774+77	778+00	LT							323	
17B	RL-3	774+77	779+00	LT							423	
17A	EL-3	768+00	778+75	RT	0.21							
17A	CL-2	767+72	770+00	RT				228				
17A	LL-3	770+00	778+75	RT			0.17					
17A	RL-4	767+72	778+75	RT							1103	
FOR INFORMATION PURPOSES ONLY					0.33	0.08						
TOTALS CARRIED TO GENERAL SUMMARY					0.41	0.36	528	200	2	2749		

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HORIZONTAL SCALE IN FEET

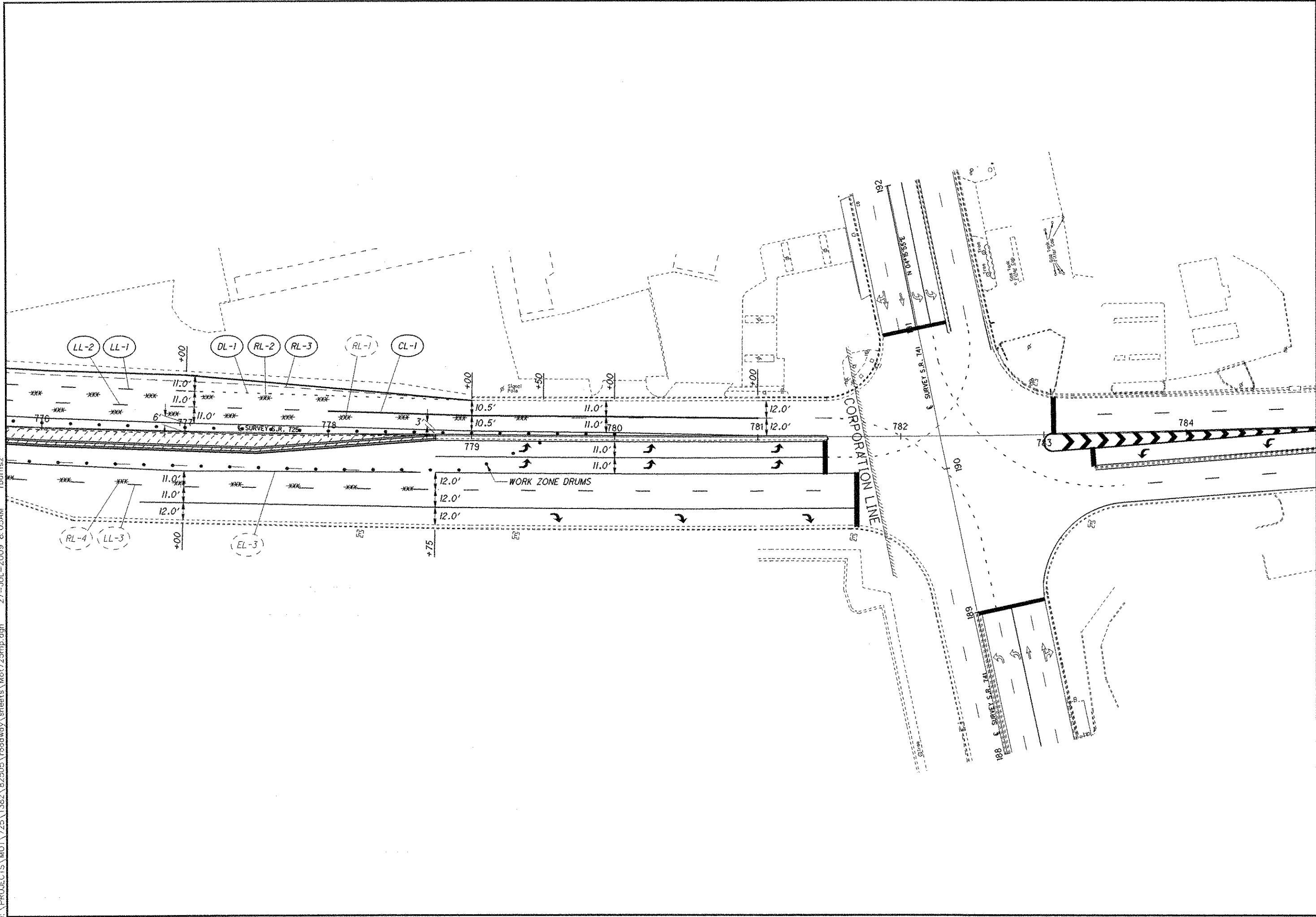
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MAINTENANCE OF TRAFFIC
STA. 767+00 TO 775+75

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**MAINTENANCE OF TRAFFIC
STA. 775+75 TO 784+90**

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SHEET NUMBER											PARTICIPATION		ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
15	16	17	17A	19	20	21	22	23	24	OFFICE CALCULATIONS	URBAN	RURAL						
																	ROADWAY	
LUMP											LUMP	LUMP	201	11000	LUMP		CLEARING AND GRUBBING	
										710	710		202	23010	710	SQ. YD.	PAVEMENT REMOVED, ASPHALT	
										13752	2504	11248	202	23500	13752	SQ. YD.	WEARING COURSE REMOVED	
										500	500		202	30601	500	SQ. YD.	CONCRETE MEDIAN REMOVED, AS PER PLAN	15
										45	45		202	30801	45	SQ. YD.	TRAFFIC ISLAND REMOVED AS PER PLAN	15
				12	18	6	3	4	5				10	38	604	EACH	MONUMENT BOX ADJUSTED TO GRADE	
						1000	750	1000	375					3125	608	SQ. FT.	CURB RAMPS, AS PER PLAN	15
																	DRAINAGE	
				10	5	8	14	9	7				10	43	604	EACH	MANHOLE ADJUSTED TO GRADE	
						32.8	51.6	40	40.2					165	519	SQ. FT.	PATCHING CONCRETE STRUCTURE, AS PER PLAN	15
				4		1							4	1	604	EACH	CATCH BASIN ADJUSTED TO GRADE	
					2	1								3	604	EACH	CATCH BASIN RECONSTRUCTED TO GRADE	
																	PAVEMENT	
										154868	50875	103993	254	01000	154868	SY. YD.	PAVEMENT PLANNING, ASPHALT CONCRETE	
										146	146		301	46000	146	CU. YD.	ASPHALT CONCRETE BASE, PG64-22	
										12168	3743	8425	407	10000	12168	GALLON	TACK COAT	
										487.8	262	226	407	13900	488	GALLON	TACK COAT, 702.13	
										5939	1900	4039	407	14000	5939	GALLON	TACK COAT, FOR INTERMEDIATE COURSE	
										284	284		408	01000	284	GALLON	PRIME COAT	
										7012	704	6308	446	50015	7012	CU. YD.	ASPHALT CONCRETE, SURFACE COURSE, TYPE 1H, PG76-22M, AS PER PLAN	15
										3110	1007	2103	448	46010	3110	CU. YD.	ASPHALT CONCRETE, INTERMEDIATE COURSE, TYPE 1, PG64-28	
										26	26		609	72101	26	CU. YD.	CONCRETE MEDIAN, AS PER PLAN	
										94	36	58	617	10100	94	CU. YD.	COMPACTED AGGREGATE	
																	WATER WORK	
				11	13	8	5	13					12	38	638	EACH	VALVE BOX ADJUSTED TO GRADE	
																	MAINTENANCE OF TRAFFIC	
			150										45	105	614	HOURS	LAW ENFORCEMENT OFFICER WITH PATROL CAR	
10													3	7	614	EACH	REPLACEMENT SIGNS	
30													9	21	614	EACH	REPLACEMENT DRUMS	
		16	.36										3.8	12.6	614	MILE	WORK ZONE LANE LINE, CLASS III, 642 PAINT	
		4											0.1	3.9	614	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT	
		27	.41										8.3	19.1	614	MILE	WORK ZONE EDGE LINE, CLASS III, 642 PAINT	
		34082	528										13324	20758	614	FOOT	WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT	
			200										200		614	FOOT	WORK ZONE DOTTED LINE, CLASS III, 642 PAINT	
		5956	2										1656	4302	614	FOOT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
		5540											808	4732	614	FOOT	WORK ZONE CROSSWALK LINE, CLASS III, 642 PAINT	
		382											118	264	614	EACH	WORK ZONE ARROW, CLASS III, 642 PAINT	
				2749									2749		644	FOOT	REMOVAL OF PAVEMENT MARKING	
																	FOR TRAFFIC CONTROL SUMMARY, SEE SHEET 54. FOR SIGNAL GENERAL SUMMARY, SEE SHEET 54A.	
													LUMP	LUMP	614		MAINTAINING TRAFFIC	
													0.9	2.1	619	MONTH	FIELD OFFICE, TYPE B	
													LUMP	LUMP	623		CONSTRUCTION LAYOUT STAKES	
													LUMP	LUMP	624		MOBILIZATION	

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REF NO.	SHEET NO.	STATION		SIDE	519	604	604	604	604	608	638	CALCULATED				
		FROM	TO		PATCHING CONCRETE STRUCTURE, AS PER PLAN	CATCH BASIN ADJUSTED TO GRADE	CATCH BASIN RECONSTRUCTED TO GRADE	MANHOLE ADJUSTED TO GRADE	MONUMENT BOX ADJUSTED TO GRADE	CURB RAMPS, AS PER PLAN	VALVE BOX ADJUSTED TO GRADE	REB	CREVED			
		SQ. FT.	EACH	EACH	EACH	EACH	SQ. FT.	EACH								
CB-1	27	744+32		RT		1										
CB-2	27	746+51		LT		1										
CB-3	27	746+51		RT		1										
CB-4	27	749+42		LT		1										
MH-1	27	749+42		LT				1								
MH-2	27	749+48		RT				1								
MH-3	28	751+76		RT				1								
MH-4	28	752+02		RT				1								
RM-1	28	751+97.85		RT					1							
MH-5	28	752+04		RT				1								
WV-1	28	752+20		RT							1					
WV-2	28	752+21		RT							1					
MH-6	28	753+05		RT				1								
WV-3	28	753+26		RT							1					
RM-2	28	754+64.50		RT					1							
RM-3	28	754+64.58		LT					1							
RM-4	28	755+20.75		LT						1						
RM-5	28	755+20.75		RT						1						
WV-4	28	755+98		RT								1				
WV-5	28	756+07		RT								1				
RM-6	28	757+87.38		LT						1						
MH-7	29	760+20		RT				1								
MH-8	29	765+00		RT				1								
MH-9	29	765+10		LT				1								
WV-6	30	769+00		RT								1				
WV-7	30	769+53		RT								1				
MH-10	30	773+76		LT				1								
WV-8	30	773+97		RT								1				
WV-9	30	774+00		RT								1				
WV-10	30	774+20		RT								1				
RM-7	31	775+55.81		LT						1						
RM-8	31	775+55.81		RT						1						
RM-9	31	780+00		LT						1						
RM-10	31	780+00		RT						1						
WV-11	31	780+20		RT								1				
RM-11	32	785+80.01		RT						1						
RM-12	32	785+80.01		LT						1						
TOTALS CARRIED TO GENERAL SUMMARY						4			10	12		11				

ESTIMATED QUANTITIES

MOT-725-13.8.2

I:\PROJECTS\MOT\725\1382\82505\roadway\sheets\Mot725qc01.dgn 27-JUL-2009 8:13AM rburns2

REF NO.	SHEET NO.	STATION		SIDE	519	604	604	604	604	608	638								
		FROM	TO		PATCHING CONCRETE STRUCTURE, AS PER PLAN SQ. FT.	CATCH BASIN ADJUSTED TO GRADE EACH	CATCH BASIN RECONSTRUCTED TO GRADE EACH	MANHOLE ADJUSTED TO GRADE EACH	MONUMENT BOX ADJUSTED TO GRADE EACH	CURB RAMPS, AS PER PLAN SQ. FT.	VALVE BOX ADJUSTED TO GRADE EACH								
WV-12	32	781+75		RT															
WV-13	32	782+21		RT															
WV-14	32	782+78		RT															
MH-11	32	782+72		RT					1										
MH-12	32	782+92		RT					1										
RM-13	32	788+34.27		LT						1									
RM-14	32	788+34.27		C						1									
RM-15	32	788+34.27		LT						1									
WV-15	32	788+35		RT															
WV-16	32	788+35		RT															
WV-17	32	788+37		RT															
RM-16	33	793+00		LT						1									
RM-17	33	793+00		C						1									
RM-18	33	793+00		RT															
RM-19	34	797+14.41		RT						1									
WV-18	34	798+00		RT															
RM-20	34	799+80.83		C						1									
RM-21	34	799+80.83		RT						1									
RM-22	34	801+95.83		LT						1									
RM-23	34	801+95.83		C						1									
RM-24	34	801+95.83		RT						1									
WV-19	35	803+76		RT															
WV-20	35	803+79		RT															
WV-21	35	804+40		RT															
WV-22	35	804+52		LT															
RM-25	35	804+64.44		LT						1									
RM-26	35	804+64.44		RT						1									
RM-27	35	804+64.44		RT						1									
RM-28	35	808+09.44		RT						1									
CB-5	36	811+11		LT					1										
RM-29	36	812+00		RT						1									
RM-30	36	812+00		LT						1									
RM-31	36	816+62.89		LT						1									
RM-32	36	816+62.89		RT						1									
MH-13	36	812+65		RT					1										
WV-23	36	815+00		LT															
MH-14	36	815+21		RT					1										
WV-24	36	815+92		LT															
CB-6	36	817+40±		LT					1										
MH-15	36	817+75		RT					1										
TOTALS CARRIED TO GENERAL SUMMARY								2	5	18					13				

I:\PROJECTS\MOT\725\1382\82505\roadway\sheets\Mot725gc01.dgn 27-JUL-2009 8:13AM rburns2

CALCULATED
REB
CHECKED

ESTIMATED QUANTITIES

MOT-725-13.82

REF NO.	SHEET NO.	STATION		SIDE	519	604	604	604	604	608	638										
		FROM	TO		PATCHING CONCRETE STRUCTURE, AS PER PLAN SQ. FT.	CATCH BASIN ADJUSTED TO GRADE EACH	CATCH BASIN RECONSTRUCTED TO GRADE EACH	MANHOLE ADJUSTED TO GRADE EACH	MONUMENT BOX ADJUSTED TO GRADE EACH	CURB RAMPS, AS PER PLAN SQ. FT.	VALVE BOX ADJUSTED TO GRADE EACH										
WV-25	37	819+60		LT							1										
WV-26	37	820+38		LT							1										
WV-27	37	820+43		LT							1										
WV-28	37	820+48		LT							1										
MH-16	37	821+20		RT					1												
CB-7	37	822+00		LT		1															
RM-33	37	823+00		⊕						1											
MH-17	37	824+84		LT					1												
RM-34	38	830+50.03		⊕						1											
MH-18	38	830+98		LT					1												
CB-8	38	831+62		LT				1													
WV-29	38	833+10		LT							1										
RM-35	42	861+14.90		⊕						1											
WV-30	42	862+03		RT							1										
WV-31	42	862+05		RT							1										
WV-32	42	862+15		RT							1										
CR-1	42	862+42±		LT							125										
CR-2	43	868+57±		LT							125										
CR-3	43	869+64±		LT							125										
CR-4	43	869+57±		RT							125										
RM-36	43	870+00		⊕						1											
CR-5	44	878+71±		LT							125										
CR-6	44	878+71±		RT							125										
RM-37	44	878+71.37		⊕						1											
MH-19	44	878+95		LT					1												
MH-20	44	879+22		RT					1												
MH-21	44	879+30		RT					1												
CR-7	44	879+75		LT							125										
CR-8	44	879+75		RT							125										
MH-22	44	MH-22		LT					1												
CB-9	44	CB-9		RT	12.8																
MH-23	45	882+96		LT					1												
CB-10	45	883+00		RT	9.6																
RM-38	45	887+00		⊕						1											
CB-11	45	888+11		RT	4.8																
CB-12	45	888+40		LT	5.6																
TOTALS CARRIED TO GENERAL SUMMARY					32.8	1	1	8	6	1000	8										

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ESTIMATED QUANTITIES
 MOT-725-13.82
 21
 102

I:\PROJECTS\MOT\725\1382\82505\roadway\sheets\Mot725c01.dgn 27-JUL-2009 8:14AM rburns2

REF NO.	SHEET NO.	STATION		SIDE	519	604	604	604	604	608	638									
		FROM	TO		PATCHING CONCRETE STRUCTURE AS PER PLAN SQ. FT.	CATCH BASIN ADJUSTED TO GRADE EACH	CATCH BASIN RECONSTRUCTED TO GRADE EACH	MANHOLE ADJUSTED TO GRADE EACH	MONUMENT BOX ADJUSTED TO GRADE EACH	CURB RAMPS, AS PER PLAN SQ. FT.	VALVE BOX ADJUSTED TO GRADE EACH									
CB-13	46	890+23		RT	8															
CB-14	46	890+74		LT	8.8															
CB-15	46	892+45		LT	6.3															
WV-33	46	892+75		LT																
WV-34	46	893+27		RT															1	
MH-24	46	893+34		RT					1										1	
WV-35	46	893+34		RT															1	
MH-25	46	893+52		RT					1											
CR-9	46	892+69±		LT								125								
CR-10	46	893+05±		RT								125								
CR-11	46	893+83±		LT								125								
CR-12	46	894+00±		RT								125								
RM-39	46	894+78.34		C							1									
MH-26	46	895+46		LT					1											
CB-16	46	895+50		RT	7.8															
MH-27	46	895+98		LT					1											
CB-17	46	896+55		RT	3.5															
MH-28	46	896+57		LT					1											
RM-40	46	897+39.08		C							1									
MH-29	47	898+37		LT					1											
MH-30	47	899+76		RT					1											
CB-18	47	899+76		RT	4.1															
MH-31	47	899+88		LT					1											
CB-19	47	900+10		RT	4.1															
MH-32	47	900+31		LT					1											
WV-36	47	901+55		RT															1	
CR-13	47	901+57±		RT								125								
MH-33	47	901+68		RT					1											
WV-37	47	901+60		RT															1	
MH-34	47	902+04		RT					1											
MH-35	47	902+25		LT					1											
CR-14	47	902+29±		RT								125								
RM-41	47	903+87.41		C							1									
CB-20	47	904+21		RT	3															
CB-21	47	904+75		RT	3															
MH-36	47	905+22		LT					1											
MH-37	47	905+36		RT					1											
CB-22	47,48	906+00		RT	3															
TOTALS CARRIED TO GENERAL SUMMARY					51.6				14	3		750		5						

CALCULATED	REB
	CHECKED
ESTIMATED QUANTITIES	
MOT-725-13.82	
22	
102	

REF NO.	SHEET NO.	STATION		SIDE	519	604	604	604	604	608	638										
					PATCHING CONCRETE STRUCTURE, AS PER PLAN	CATCH BASIN ADJUSTED TO GRADE	CATCH BASIN RECONSTRUCTED TO GRADE	MANHOLE ADJUSTED TO GRADE	MONUMENT BOX ADJUSTED TO GRADE	CURB RAMPS, AS PER PLAN	VALVE BOX ADJUSTED TO GRADE										
					SQ. FT.	EACH	EACH	EACH	EACH	SQ. FT.	EACH										
		FROM	TO																		
CB-23	48	906+29		LT	9.6																
MH-38	48	906+29		RT					1												
MH-39	48	907+20		LT					1												
WV-38	48	908+58		RT									1								
MH-40	48	909+45		LT					1												
CB-24	48	910+00		RT	6.4																
CB-25	48	910+07		RT	4.8																
WV-39	48	910+15		RT																	
MH-41	48	910+23		LT					1												
WV-40	48	910+35		RT																	
WV-41	48	910+50		RT																	
WV-42	48	910+83		RT																	
WV-43	48	911+43		RT																	
CB-26	48	911+74		RT	7.2																
RM-42	48	913+00		⊕						1											
CR-15	49	913+77±		LT								125									
CR-16	49	913+81±		RT								125									
WV-44	49	913+82		RT																	
MH-42	49	913+95		LT					1												
WV-45	49	914+60		RT																	
MH-43	49	914+65		RT					1												
WV-46	49	914+65		RT																	
WV-47	49	914+70		RT																	
CR-17	49	914+83±		LT								125									
CR-18	49	914+83±		RT								125									
RM-43	49	918+00		⊕						1											
WV-48	49	918+59		RT																	
MH-44	49	919+97		RT					1												
CB-27	49	920+09		RT	8.4																
WV-49	50	922+66		RT																	
RM-44	50	922+86.98		⊕						1											
MH-45	50	922+94		RT					1												
MH-46	50	925+94		RT					1												
CR-19	50	926+25±		RT								125									
CR-20	50	926+55±		LT								125									
WV-50	50	926+55		RT																	
RM-45	50	926+92.85		⊕						1											
CR-21	50	927+09±		RT								125									
CB-28	50	927+50		RT	3.6																
CR-22	50	927+56±		LT								125									
TOTALS CARRIED TO GENERAL SUMMARY					40				9	4		1000		13							

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ESTIMATED QUANTITIES

MOT - 725 - 13.82

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REF NO.	SHEET NO.	STATION		SIDE	519	604	604	604	604	608	638										
		FROM	TO		PATCHING CONCRETE STRUCTURE AS PER PLAN SQ. FT.	CATCH BASIN ADJUSTED TO GRADE EACH	CATCH BASIN RECONSTRUCTED TO GRADE EACH	MANHOLE ADJUSTED TO GRADE EACH	MONUMENT BOX ADJUSTED TO GRADE EACH	CURB RAMPS, AS PER PLAN SQ. FT.	VALVE BOX ADJUSTED TO GRADE EACH										
MH-47	51	929+07		RT					1												
MH-48	51	929+97		RT					1												
RM-46	51	931+00		C						1											
MH-49	51	931+88		RT					1												
MH-50	51	932+96		RT					1												
CB-29	51	933+18±		LT	8.4																
MH-51	51	934+90		RT					1												
RM-47	51	935+00		C						1											
MH-52	52	937+20		RT					1												
MH-53	52	937+50		RT					1												
RM-48	52	938+32.39		C						1											
CB-30	52	940+87		LT	7.6																
CB-31	52	943+00		LT	9																
RM-49	52	943+29.89		C						1											
CR-23	52	944+00±		RT								125									
CR-24	52	944+95±		RT								125									
CB-32	53	946+76		LT	7.6																
CB-33	53	947+40		RT	7.6																
RM-50	53	948+25.13		C						1											
CR-24	53	949+27.00		LT								125									
TOTALS CARRIED TO GENERAL SUMMARY					40.2				7	5		375									

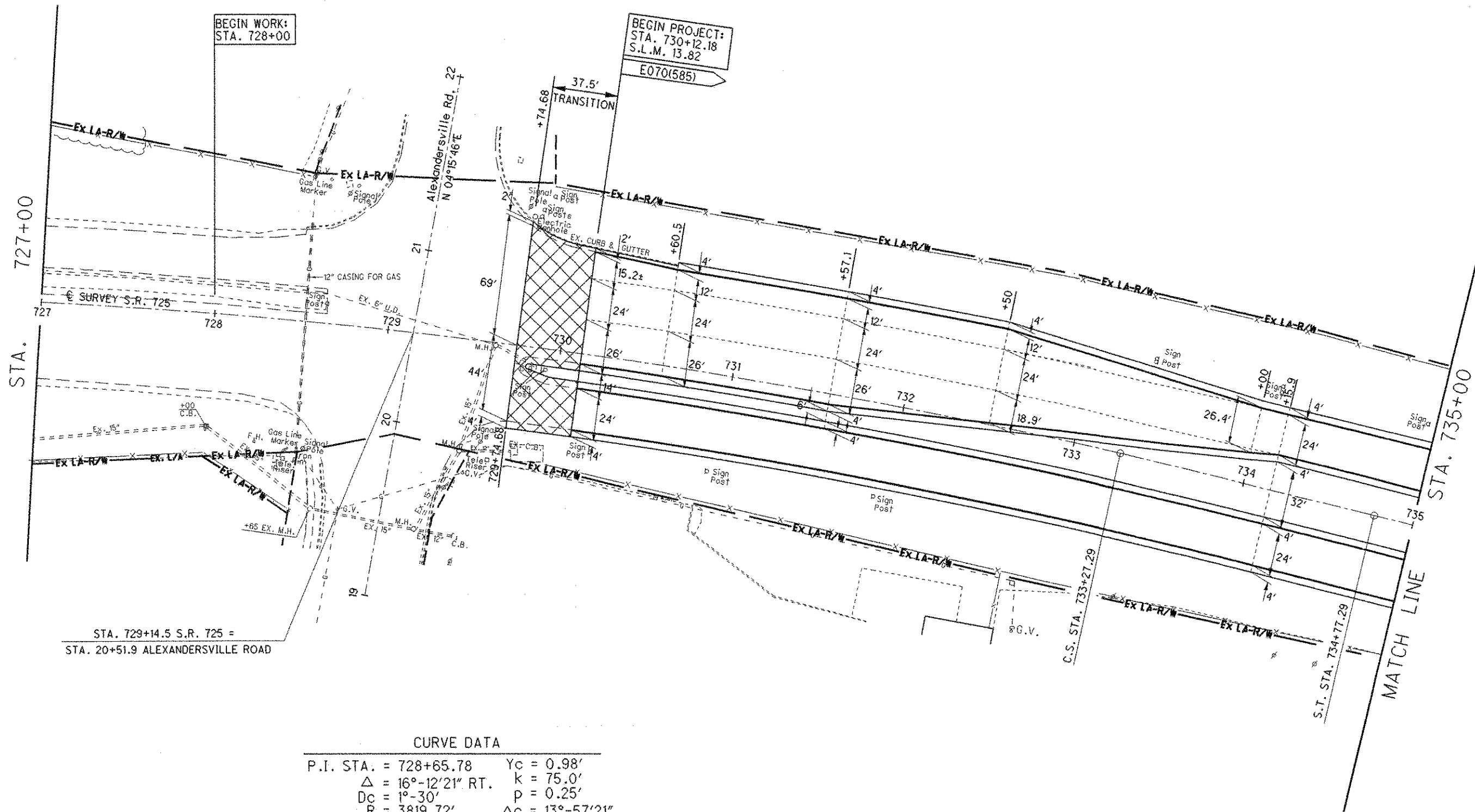
I:\PROJECTS\MOT\725\1382\B2505\roadway\sheet\Mot725qc01.dgn 27-JUL-2009 8:15AM rourms2

CALCULATED
REB
CHECKED

ESTIMATED QUANTITIES

MOT-725-13.82

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BEGIN WORK:
STA. 728+00

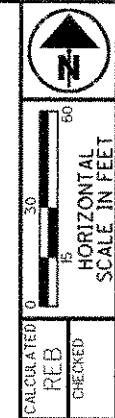
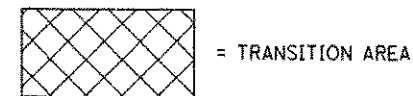
BEGIN PROJECT:
STA. 730+12.18
S.L.M. 13.82
E070(585)

STA. 729+14.5 S.R. 725 =
STA. 20+51.9 ALEXANDERSVILLE ROAD

CURVE DATA

P.I. STA. = 728+65.78	Yc = 0.98'
$\Delta = 16^\circ-12'21''$ RT.	k = 75.0'
Dc = 1'-30'	p = 0.25'
R = 3819.72'	$\Delta c = 13^\circ-57'21''$
Ls = 150.00'	Lc = 928.09'
$\theta = 1^\circ07'30''$	Ts = 618.86'
Xc = 149.99'	Es = 38.77'

PAVEMENT LEGEND

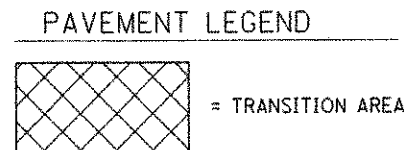
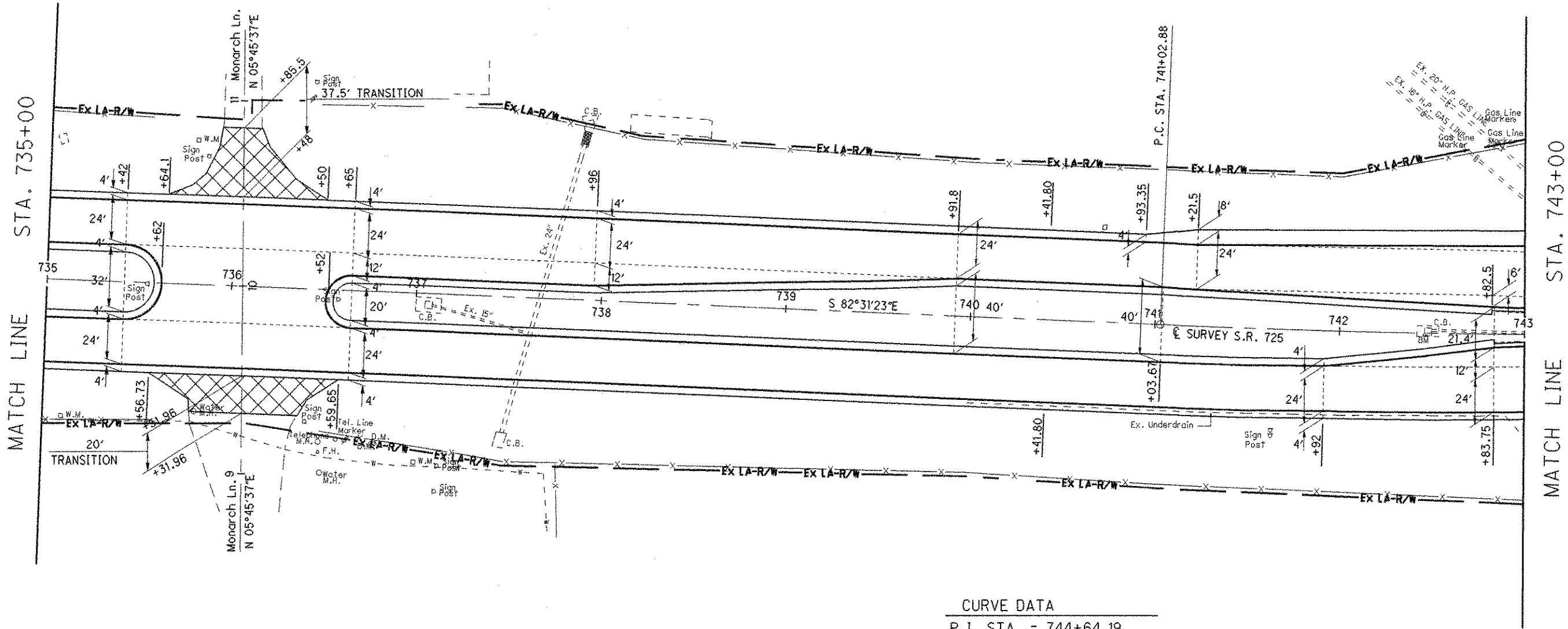


PLAN SHEET
STA. 727+00 TO STA. 735+00

MOT-725-13.82

25
102

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CURVE DATA

P.I. STA. =	744+64.19
Δ =	7°-13'-00" LT.
Dc =	1°-00'
R =	5729.58'
T =	361.31'
L =	721.67'
E =	11.38'

Sta. 742+45 Elev. 990.01
 Square cut in the SW corner of a CB.
 in the median.

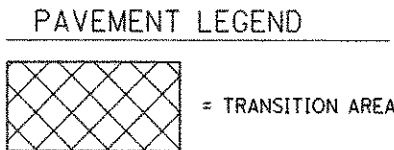
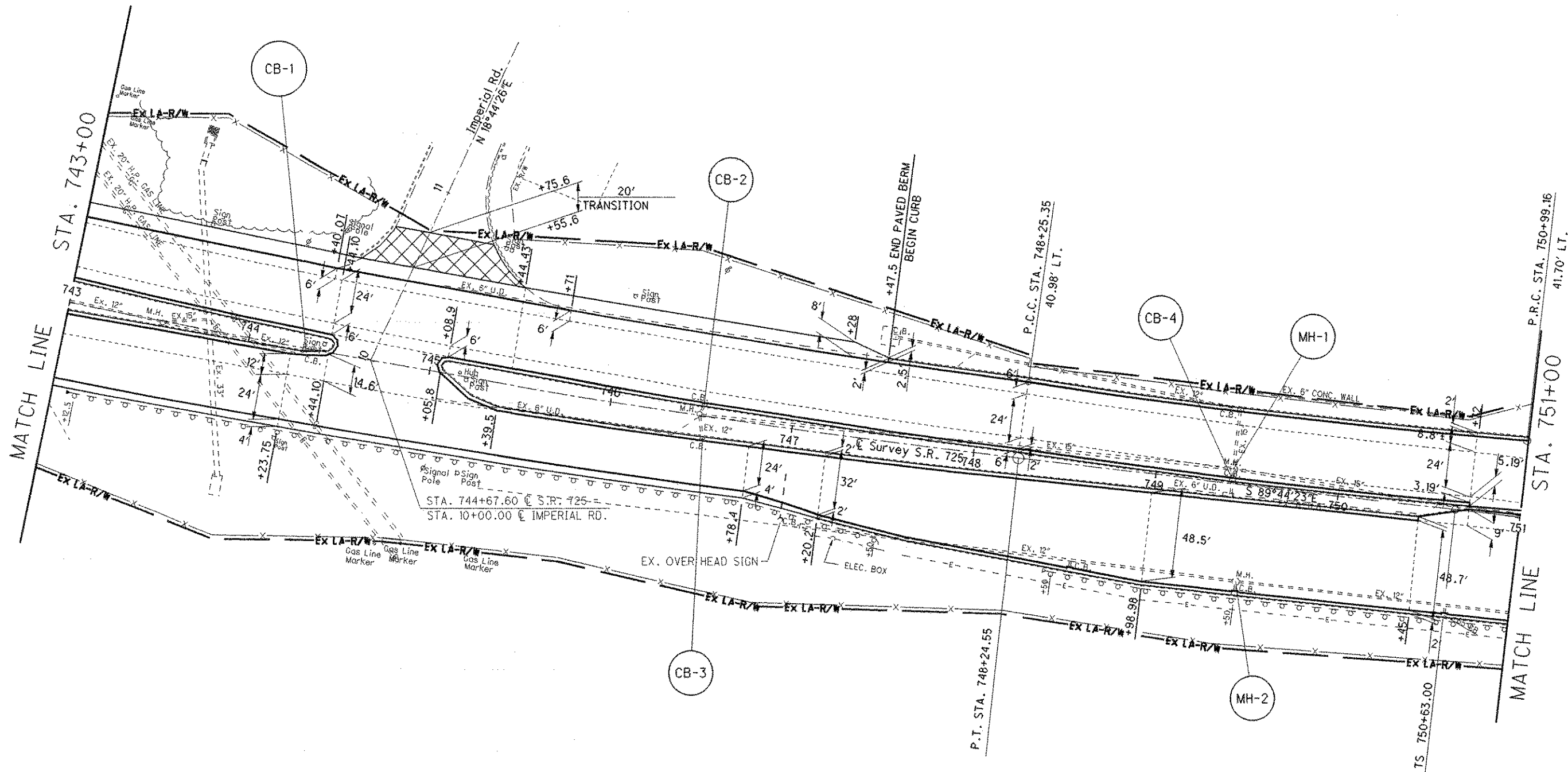
CALCULATED
 REB
 CHECKED

HORIZONTAL SCALE IN FEET

PLAN SHEET
 STA. 735+00 TO STA. 743+00

MOT-725-13.82

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FOR BALLOON QUANTITIES SEE SHEET 19-24.

CALCULATED
REB
CHECKED

HORIZONTAL SCALE IN FEET

PLAN SHEET - S.R. 725
STA. 743+00 TO STA. 751+00

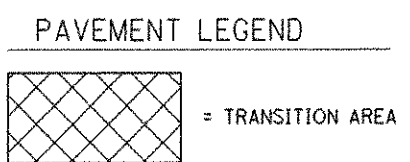
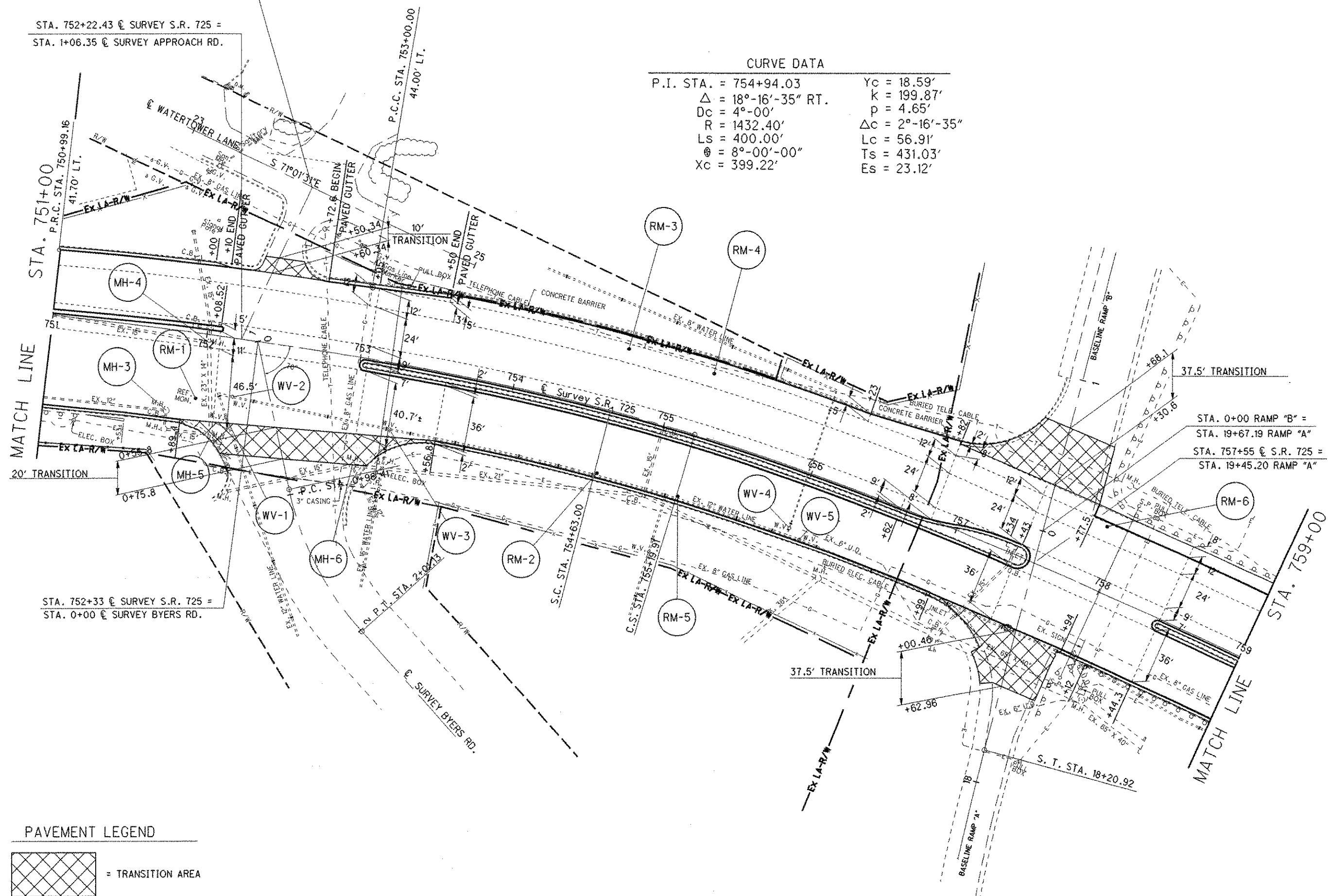
MOT-725-13.82

STA. 23+84.15 @ WATERTOWER LN. =
 STA. 0+00 APPROACH RD.

STA. 752+22.43 @ SURVEY S.R. 725 =
 STA. 1+06.35 @ SURVEY APPROACH RD.

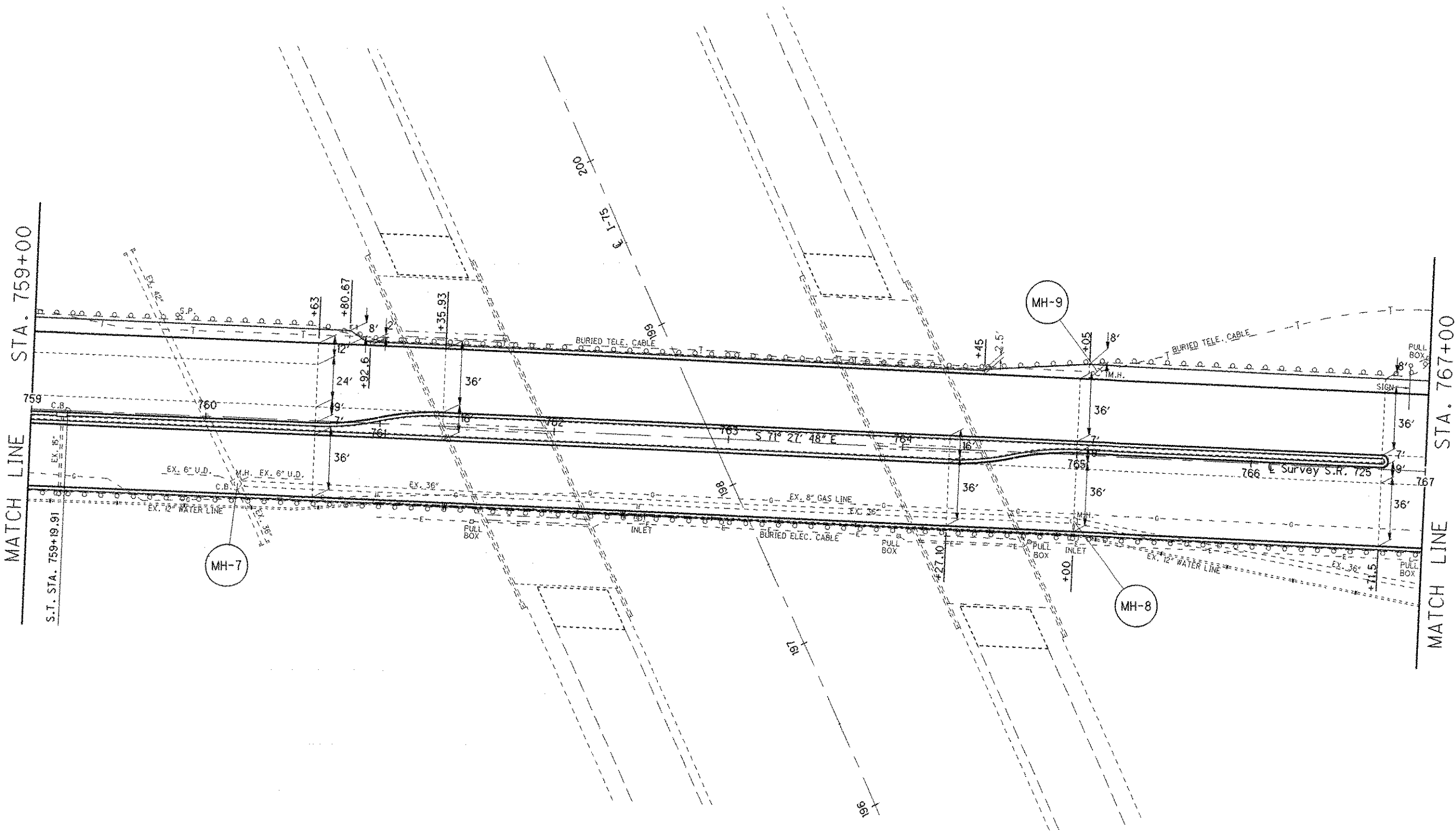
CURVE DATA

P.I. STA. = 754+94.03	Yc = 18.59'
$\Delta = 18^{\circ}-16'-35''$ RT.	K = 199.87'
Dc = 4'-00'	p = 4.65'
R = 1432.40'	$\Delta c = 2^{\circ}-16'-35''$
Ls = 400.00'	Lc = 56.91'
$\theta = 8^{\circ}-00'-00''$	Ts = 431.03'
Xc = 399.22'	Es = 23.12'



FOR BALLOON QUANTITIES SEE SHEET 19-24.

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CALCULATED	REB	CHECKED

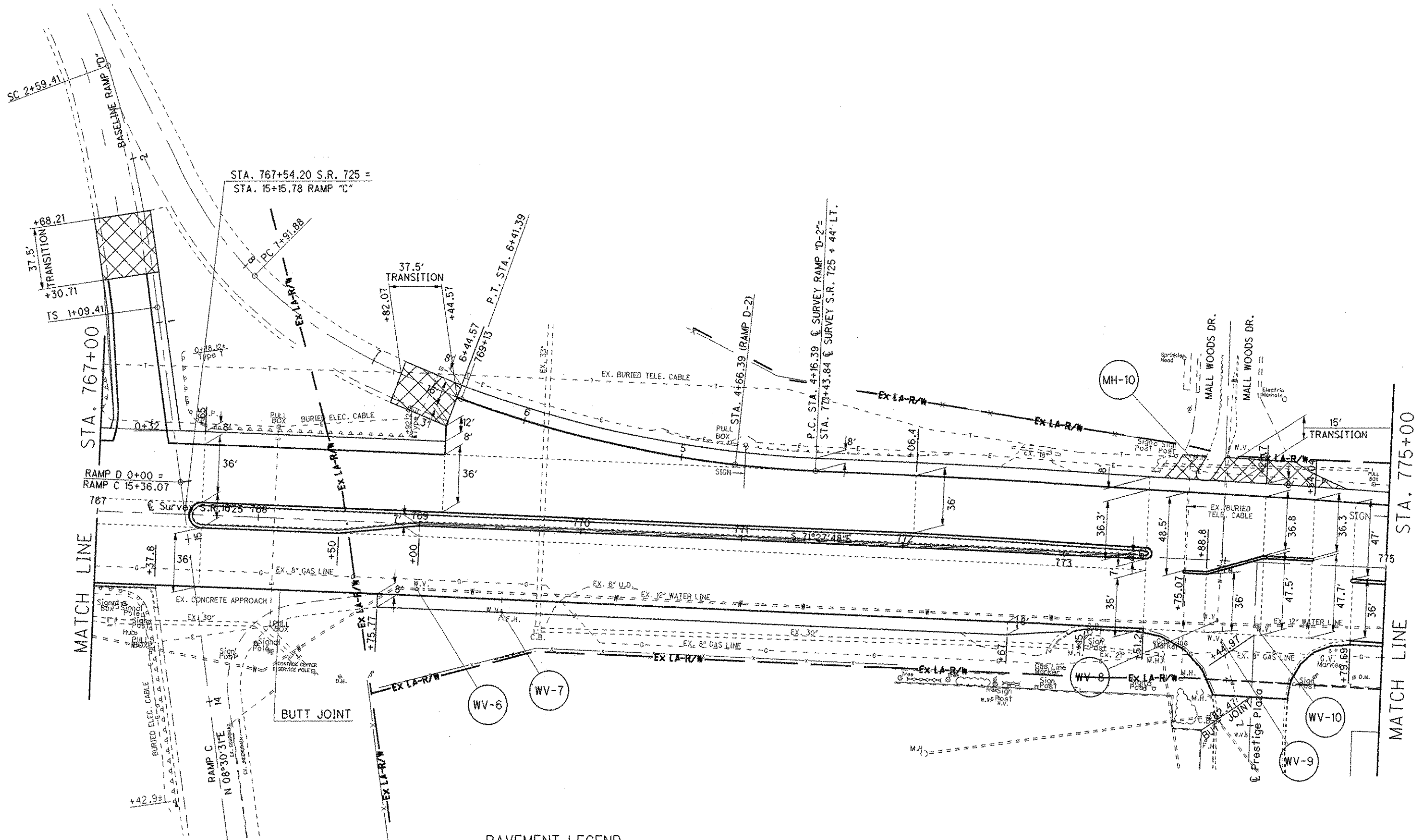
0	30	60
HORIZONTAL SCALE IN FEET		

PLAN SHEET - S.R. 725
STA. 759+00 TO STA. 767+00

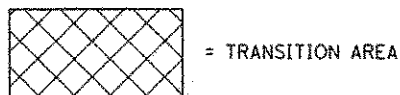
MOT-725-13.82

FOR BALLOON QUANTITIES SEE SHEET 19-24.

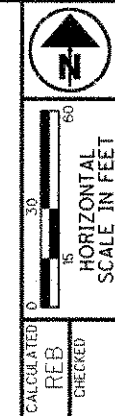
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PAVEMENT LEGEND

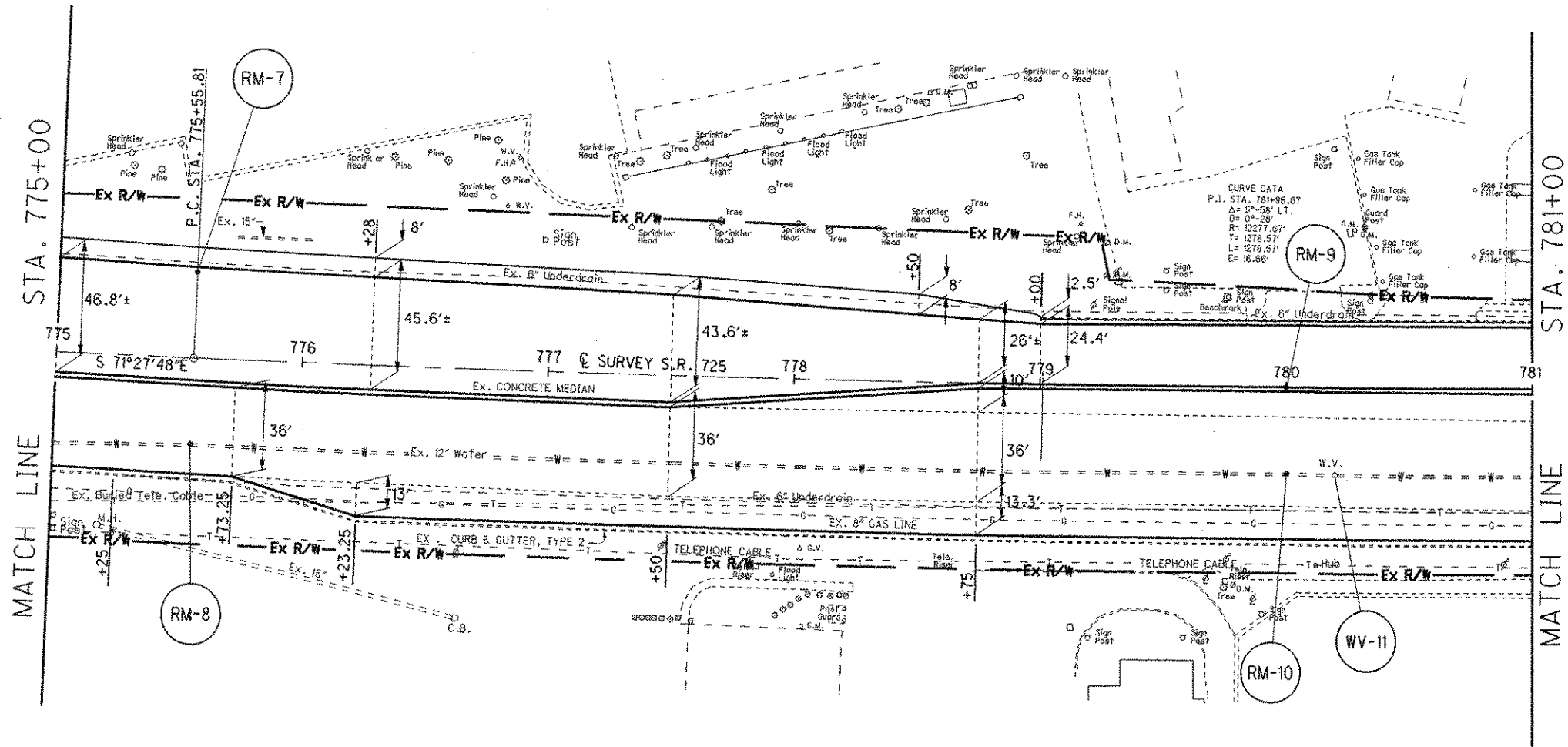


FOR BALLOON QUANTITIES SEE SHEET 19-24.
 FOR MEDIAN REMOVAL ON SR-725 SEE SHEET 31A.
 FOR ISLAND REMOVAL ON PRESTIGE PLAZA SEE SHEET 31A.



PLAN SHEET - S.R. 725
 STA. 767+00 TO STA. 775+00

MOT-725-13.82



CURVE DATA
 P.I. STA. 781+95.67
 $\Delta = 5^{\circ}58'17.00''$ LT.
 $D = 0^{\circ}28'$
 $R = 12277.67'$
 $T = 1278.57'$
 $L = 1278.57'$
 $E = 18.86'$

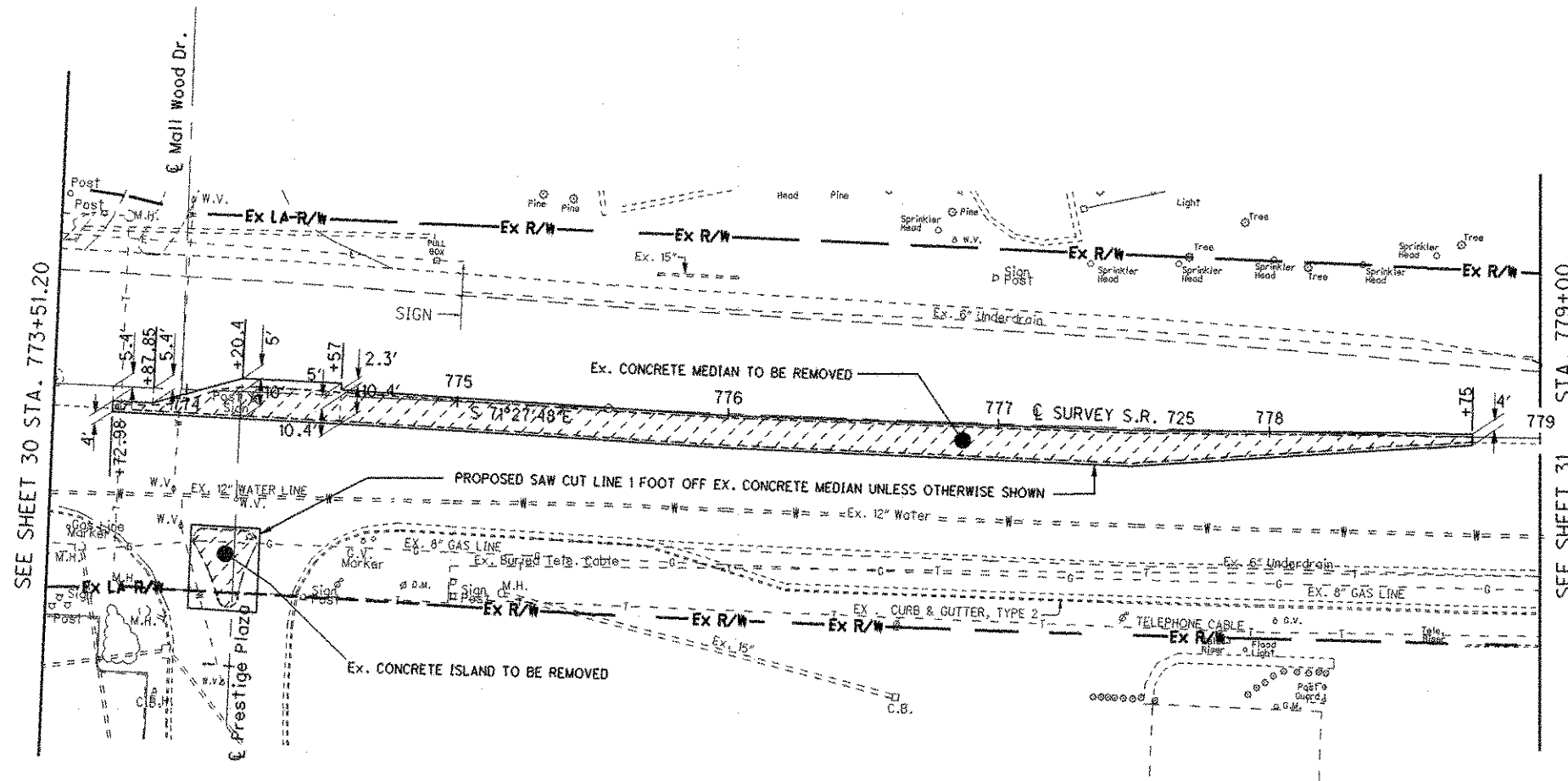
BM#156 Sta. 779+74 Elev. 995.55
 Square cut in the SW corner of a
 overhead sign base.

FOR MEDIAN REMOVAL ON SR-725 SEE SHEET 31A.
 FOR ISLAND REMOVAL ON PRESTIGE PLAZA SEE SHEET 31A.
 FOR BALLOON QUANTITIES SEE SHEET 19-24.

CALCULATED: 0
 REB: 60
 CHECKED: 60
 HORIZONTAL SCALE IN FEET

PLAN SHEET - S.R. 725
 STA. 775+00 TO STA. 781+00

MOT-725-13.82



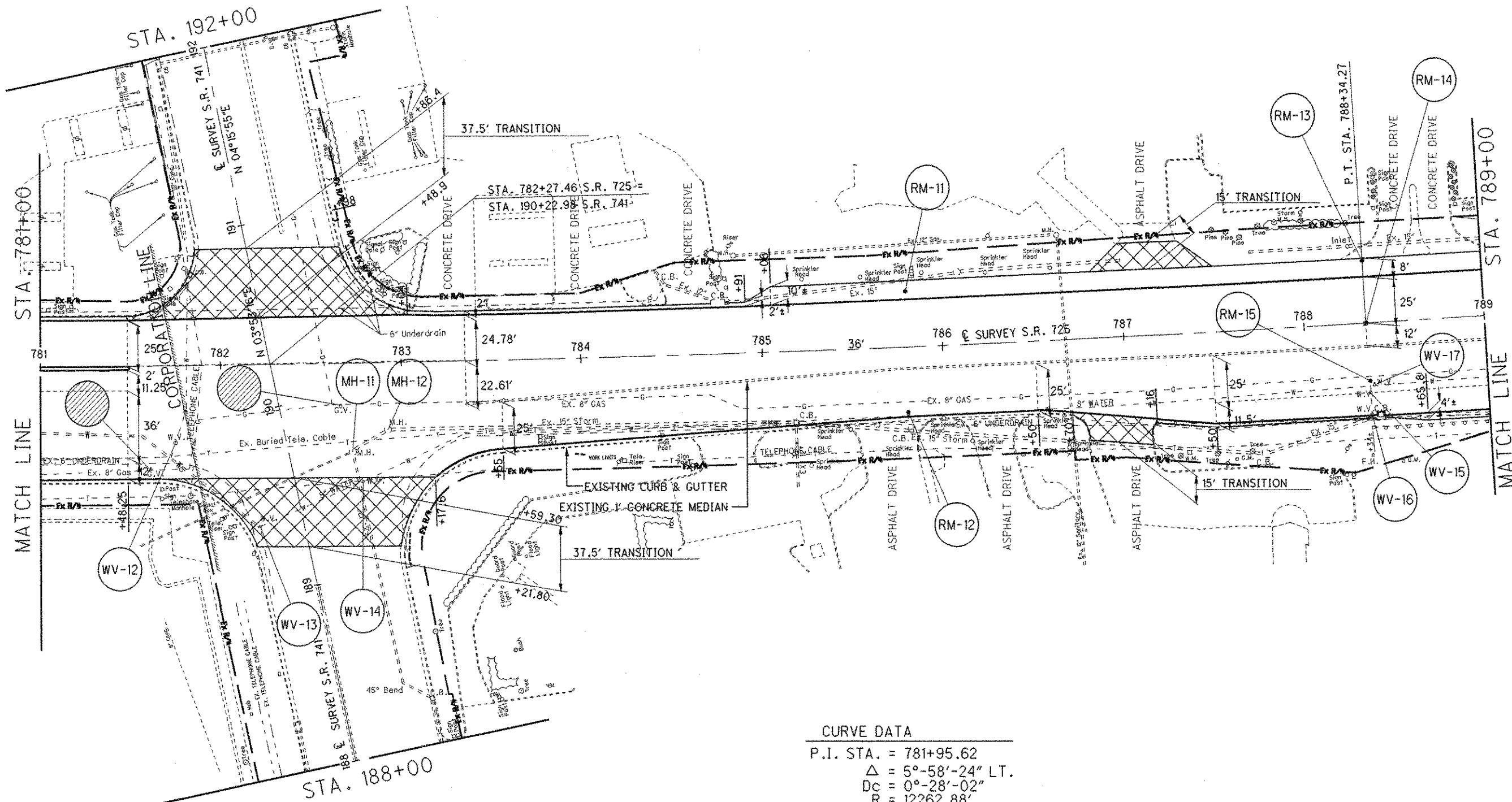
FOR MORE DETAILS ON THE REMOVAL OF THE CONCRETE MEDIAN / ISLAND SEE PLAN SHEETS AND TYPICALS.

CALCULATED
REB
CHECKED

PLAN SHEET - S.R. 725 / PRESTIGE PLAZA
CONCRETE MEDIAN & ISLAND REMOVE DETAIL

MOT-725-13.82

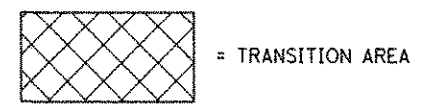




CURVE DATA

P.I. STA.	= 781+95.62
Δ	= 5°-58'-24" LT.
Dc	= 0°-28'-02"
R	= 12262.88'
T	= 639.81'
L	= 1278.46'
E	= 16.68'

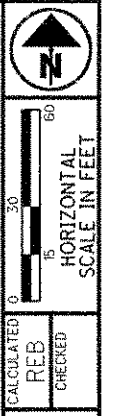
PAVEMENT LEGEND



FOR BALLOON QUANTITIES SEE SHEET 19-24.

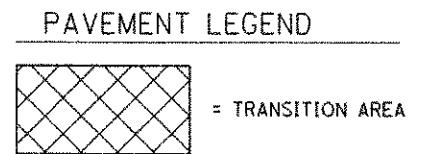
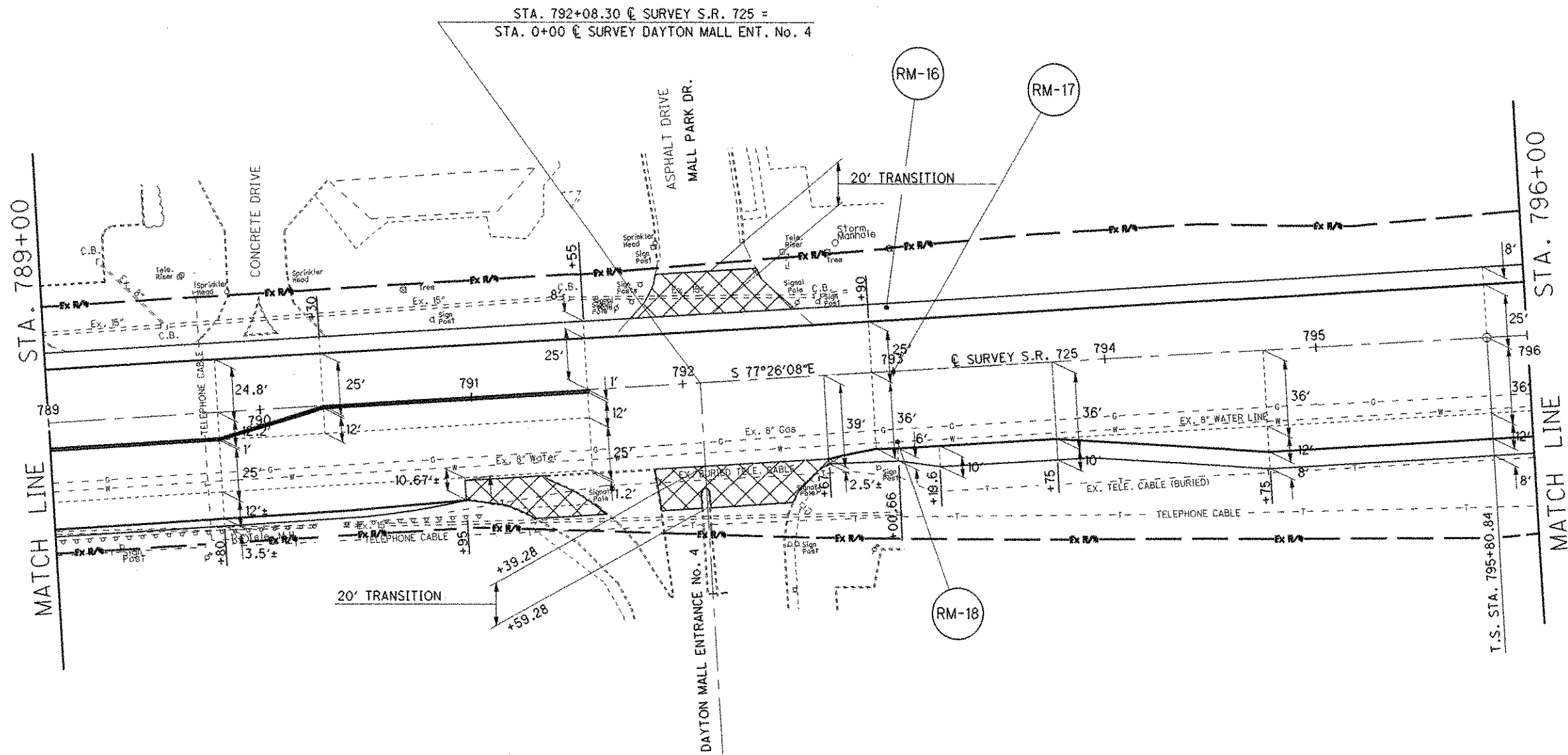
BM #157 Sta. 785+15 Rt. Elev. 991.34
RR Spike in power pole (DP&L No. 35482?)

= GAS VALVE TO BE ADJUSTED BY OTHERS



**PLAN SHEET - S.R. 725
STA. 781+00 TO STA. 789+00**

MOT-725-13.82



FOR BALLOON QUANTITIES SEE SHEET 19-24.

CALCULATED
REB
CHECKED

HORIZONTAL SCALE IN FEET

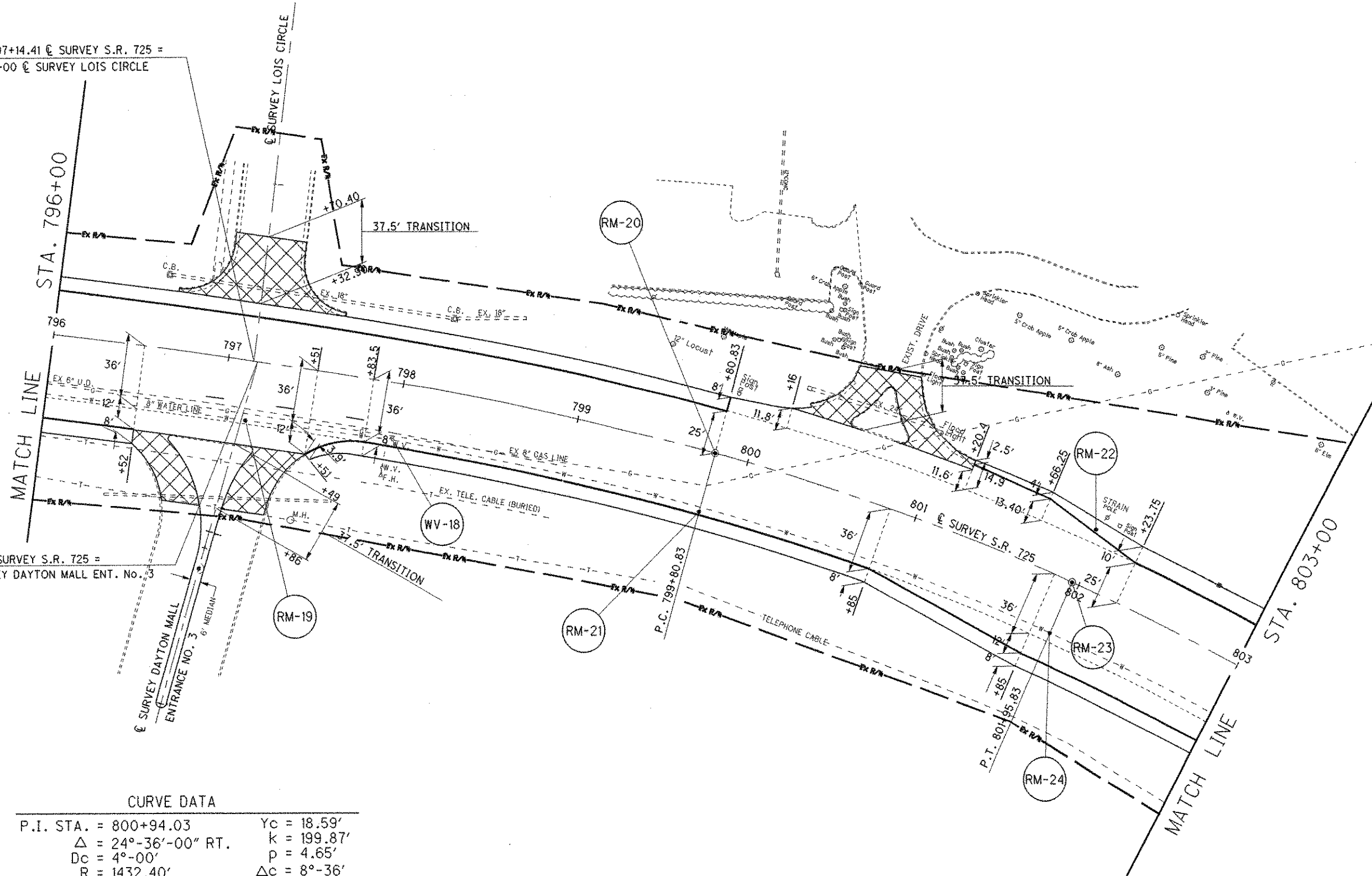
PLAN SHEET - S.R. 725
STA. 789+00 TO STA. 796+00

MOT-725-13.82

I:\PROJECTS\MOT\725\1382\82505\roadway\sheets\Mot725gp01.dgn 27-JUL-2009 8:25AM rburns2

STA. 797+14.41 @ SURVEY S.R. 725 =
STA. 0+00 @ SURVEY LOIS CIRCLE

STA. 797+16.49 @ SURVEY S.R. 725 =
STA. 0+00 @ SURVEY DAYTON MALL ENT. NO. 3



CURVE DATA

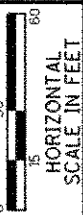
P.I. STA. = 800+94.03	Yc = 18.59'
$\Delta = 24^{\circ}-36'-00''$ RT.	k = 199.87'
Dc = 4'-00"	p = 4.65'
R = 1432.40'	$\Delta c = 8^{\circ}-36'$
Ls = 400.00'	Lc = 215'
$\theta = 8^{\circ}-00'-00''$	Ts = 513.20'
Xc = 399.22'	Es = 38.41'

PAVEMENT LEGEND



= TRANSITION AREA

FOR BALLOON QUANTITIES SEE SHEET 19-24.

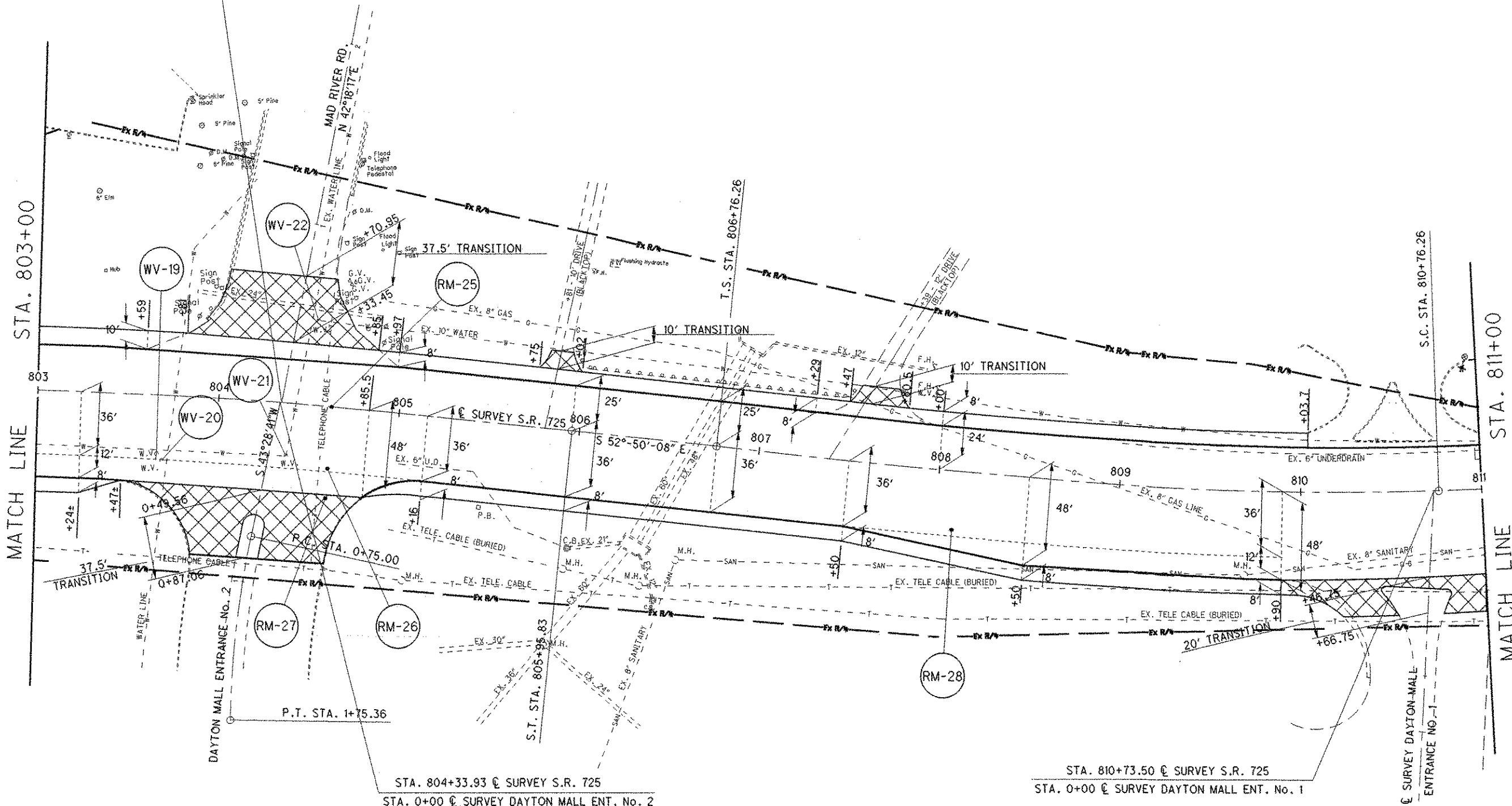


CALCULATED
CHECKED

**PLAN SHEET - S.R. 725
STA. 796+00 TO STA. 803+00**

MOT-725-13.82

STA. 804+34.83 @ SURVEY S.R. 725
 STA. 0+00 @ SURVEY MAD RIVER RD.



CURVE DATA

P.I. STA. = 800+94.03	k = 199.87'
$\Delta = 24^\circ-36'-00''$ RT.	p = 4.65'
Dc = 4'-00'-00"	$\Delta c = 8'-36'$
R = 1432.40'	Lc = 215.00'
Ls = 400.00'	Ts = 513.20'
$\theta = 8^\circ-00'-00''$	Es = 38.41'
Xc = 399.22'	L.T. = 266.94'
Yc = 18.59'	S.T. = 133.58'



FOR BALLOON QUANTITIES SEE SHEET 19-24.



PLAN SHEET - S.R. 725
 STA. 803+00 TO STA. 811+00

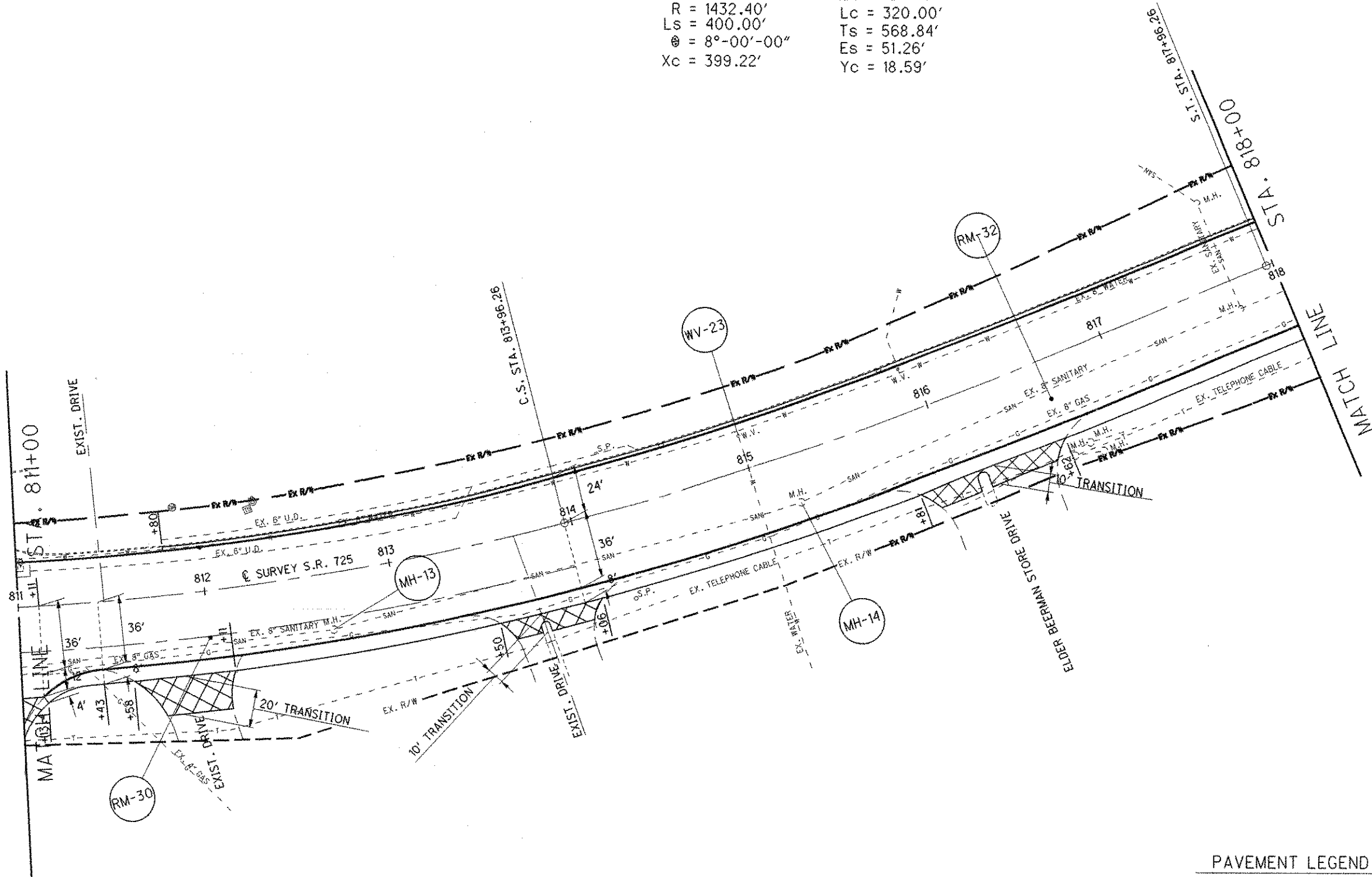
MOT-725-13.82

I:\PROJECTS\MOT\725\1382\82505\roadway\sheet1\Mot725gp01.dgn 27-JUL-2009 8:22AM rburns2

PROJECTS\MOT\725\1382\82505\roadway\sheet1\Mot725sp01.dgn 27-JUL-2009 8:23AM rburns2

CURVE DATA

P.I. STA. = 812+45.09	K = 199.87'
$\Delta = 28^{\circ}-48'-00''$ LT.	p = 4.65'
Dc = $4^{\circ}-00'-00''$	$\Delta c = 12^{\circ}-48'$
R = 1432.40'	Lc = 320.00'
Ls = 400.00'	Ts = 568.84'
$\theta = 8^{\circ}-00'-00''$	Es = 51.26'
Xc = 399.22'	Yc = 18.59'



PAVEMENT LEGEND



= TRANSITION AREA

FOR BALLOON QUANTITIES SEE SHEET 19-24.



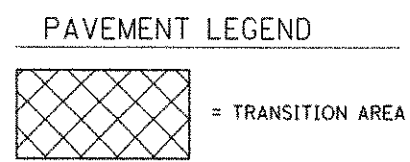
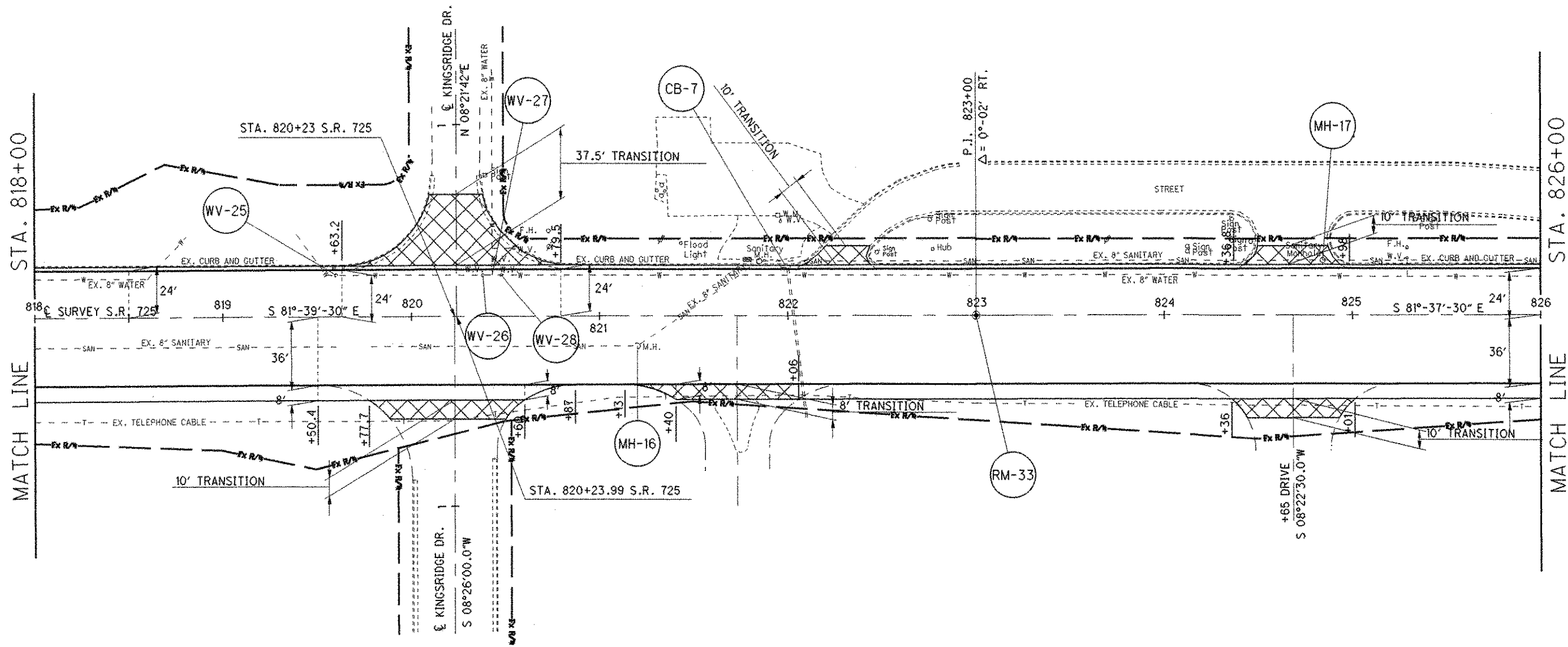
0 15 30 45 60
 HORIZONTAL
 SCALE IN FEET

CALCULATED
 REB
 CHECKED

PLAN SHEET - S.R. 725
 STA. 811+00 TO STA. 818+00

MOT-725-13.82

36
 102



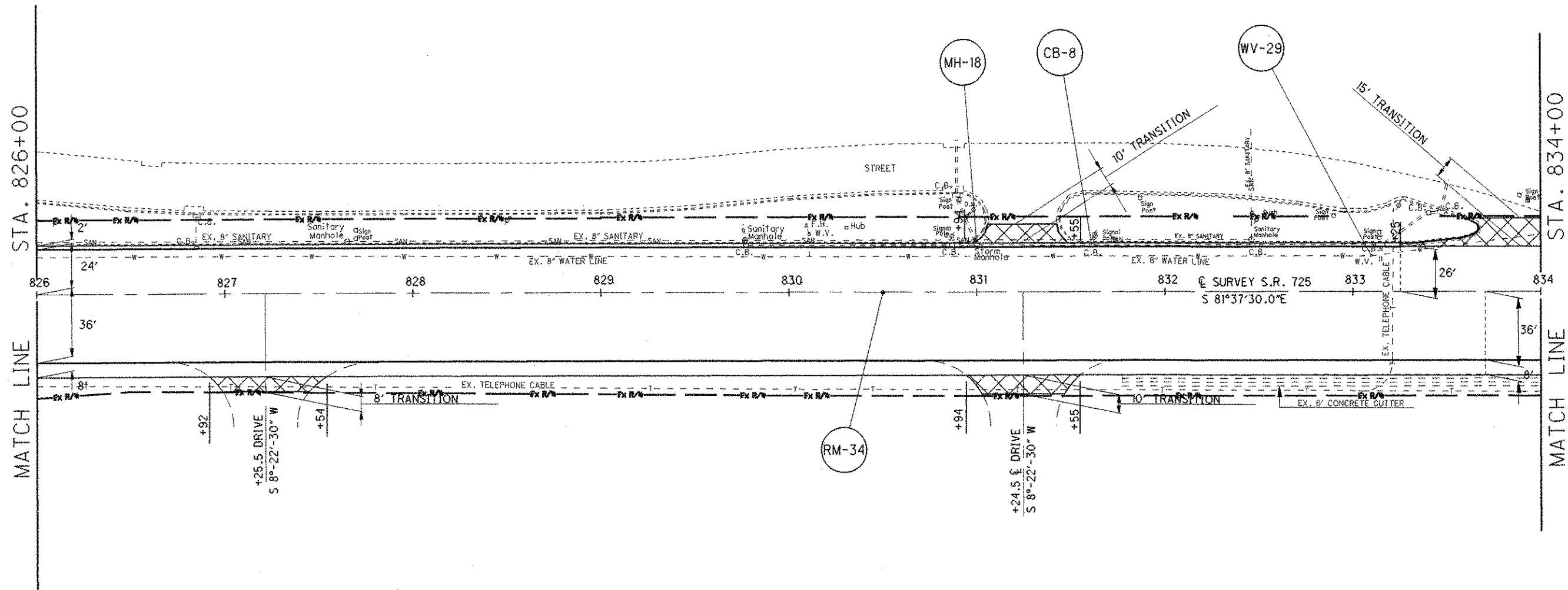
FOR BALLOON QUANTITIES SEE SHEET 19-24.

CALCULATED
REB
CHECKED

HORIZONTAL SCALE IN FEET

PLAN SHEET - S.R. 725
STA. 818+00 TO STA. 826+00

MOT-725-13.82



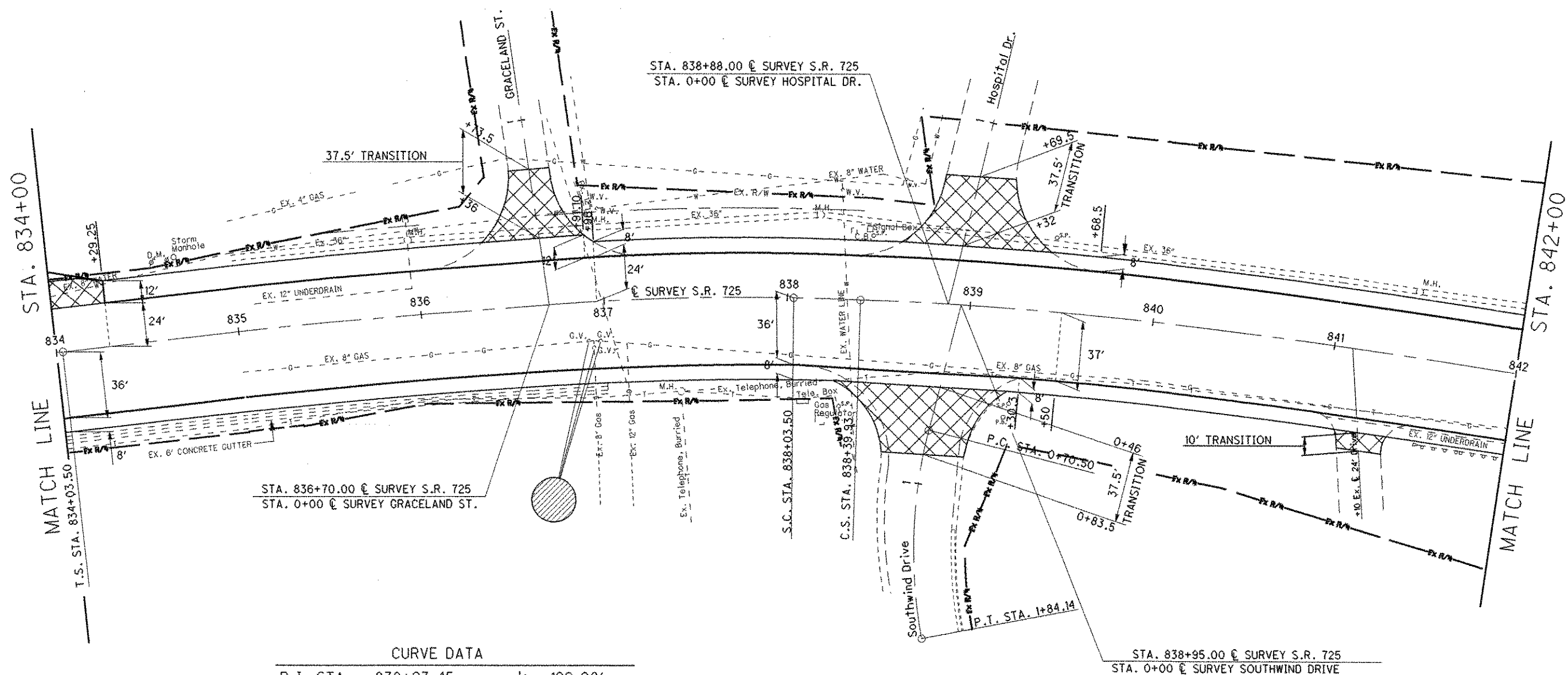
FOR BALLOON QUANTITIES SEE SHEET 19-24.

CALCULATED
REB
CHECKED

HORIZONTAL SCALE IN FEET


PLAN SHEET - S.R. 725
STA. 826+00 TO STA. 834+00

MOT-725-13.82




CURVE DATA

P.I. STA. = 838+23.45	k = 199.90'
$\Delta = 15^\circ-16'-30''$ RT.	p = 4.07'
Dc = 3°-30'-00"	$\Delta c = 1^\circ-16'-30''$
R = 1637.02'	Lc = 36.43'
Ls = 400.00'	Ts = 419.96'
$\theta = 7^\circ-00'-00''$	Es = 18.76'
Xc = 399.40'	
Yc = 16.27'	

 = GAS VALVES TO BE ADJUSTED BY OTHERS

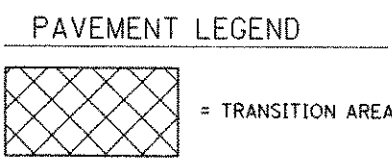
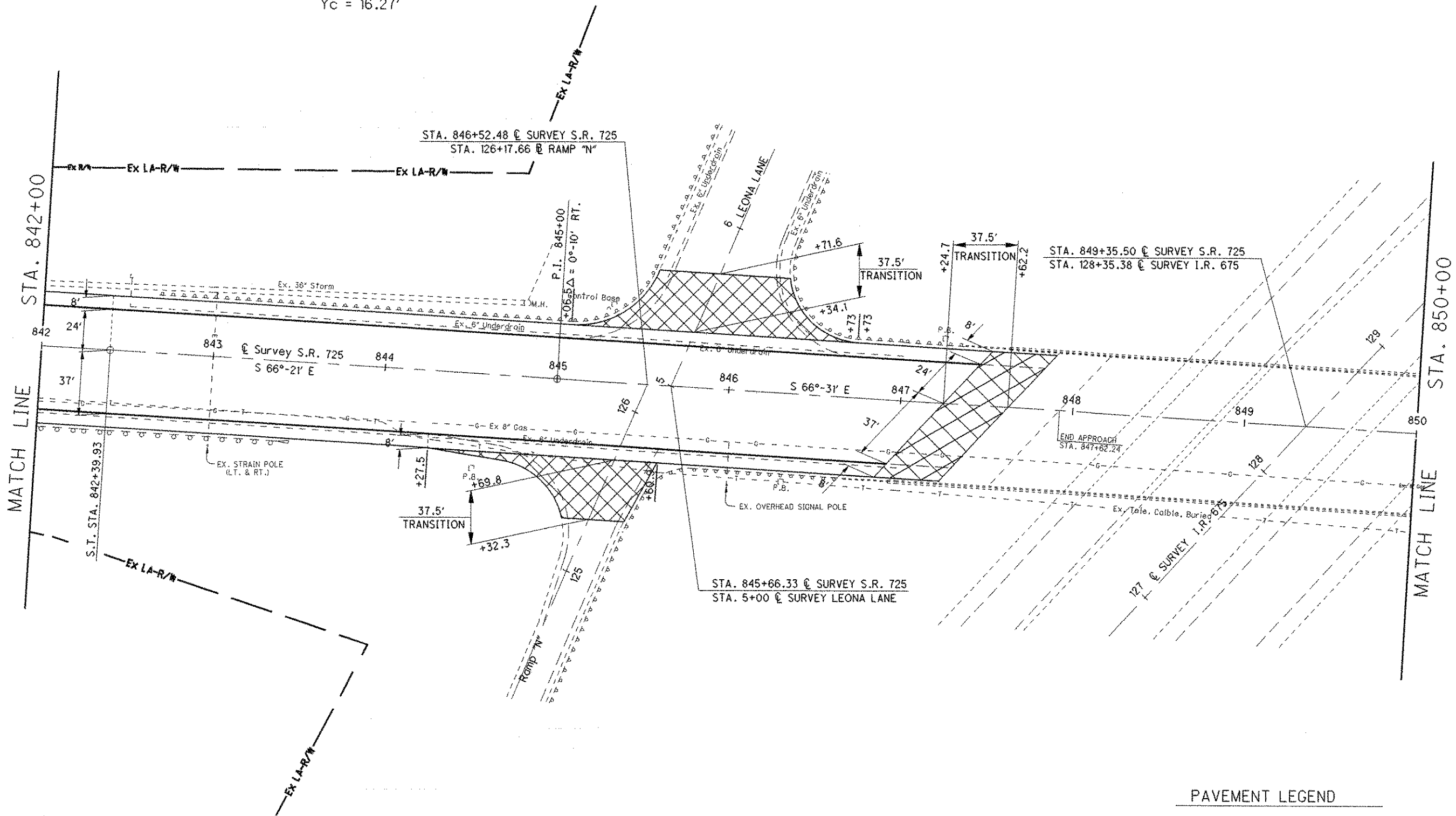
PAVEMENT LEGEND

 = TRANSITION AREA

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CURVE DATA

P.I. STA. = 838+23.45 k = 199.90'
 Δ = 15°-16'-30" RT. p = 4.07'
 Dc = 3°-30'-00" Δc = 1°-16'-30"
 R = 1637.02' Lc = 36.43'
 Ls = 400.00' Ts = 419.96'
 θ = 7°-00'-00" Es = 18.76'
 Xc = 399.40'
 Yc = 16.27'

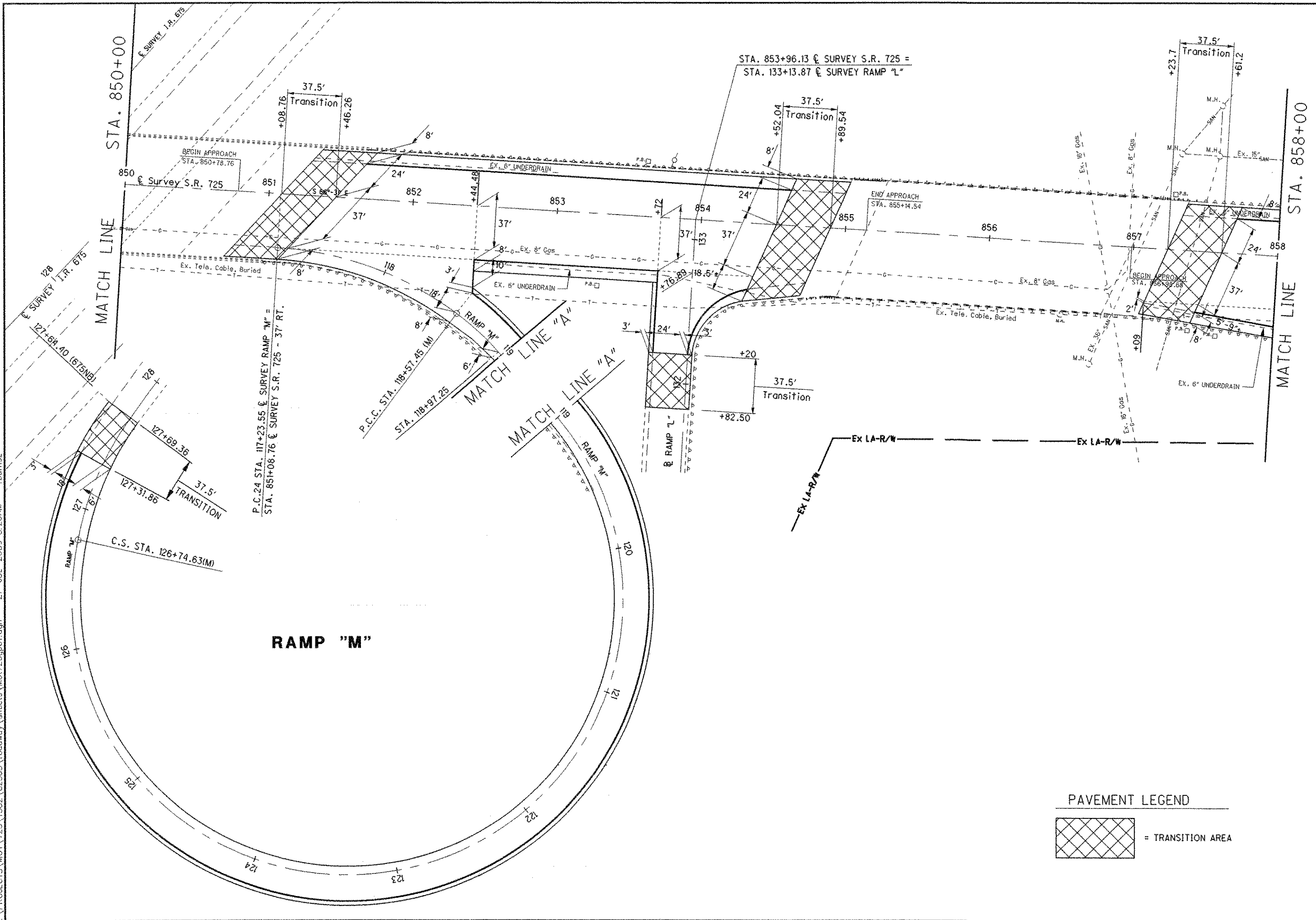


CALCULATED 0
 REB
 CHECKED

PLAN SHEET - S.R. 725
 STA. 842+00 TO STA. 850+00

MOT-725-13.8.2

E:\PROJECTS\MOT\725\1382\82505\roadway\sheet\Mo1725gp01.dgn 27-JUL-2009 8:26AM rburns2

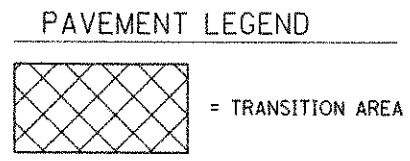
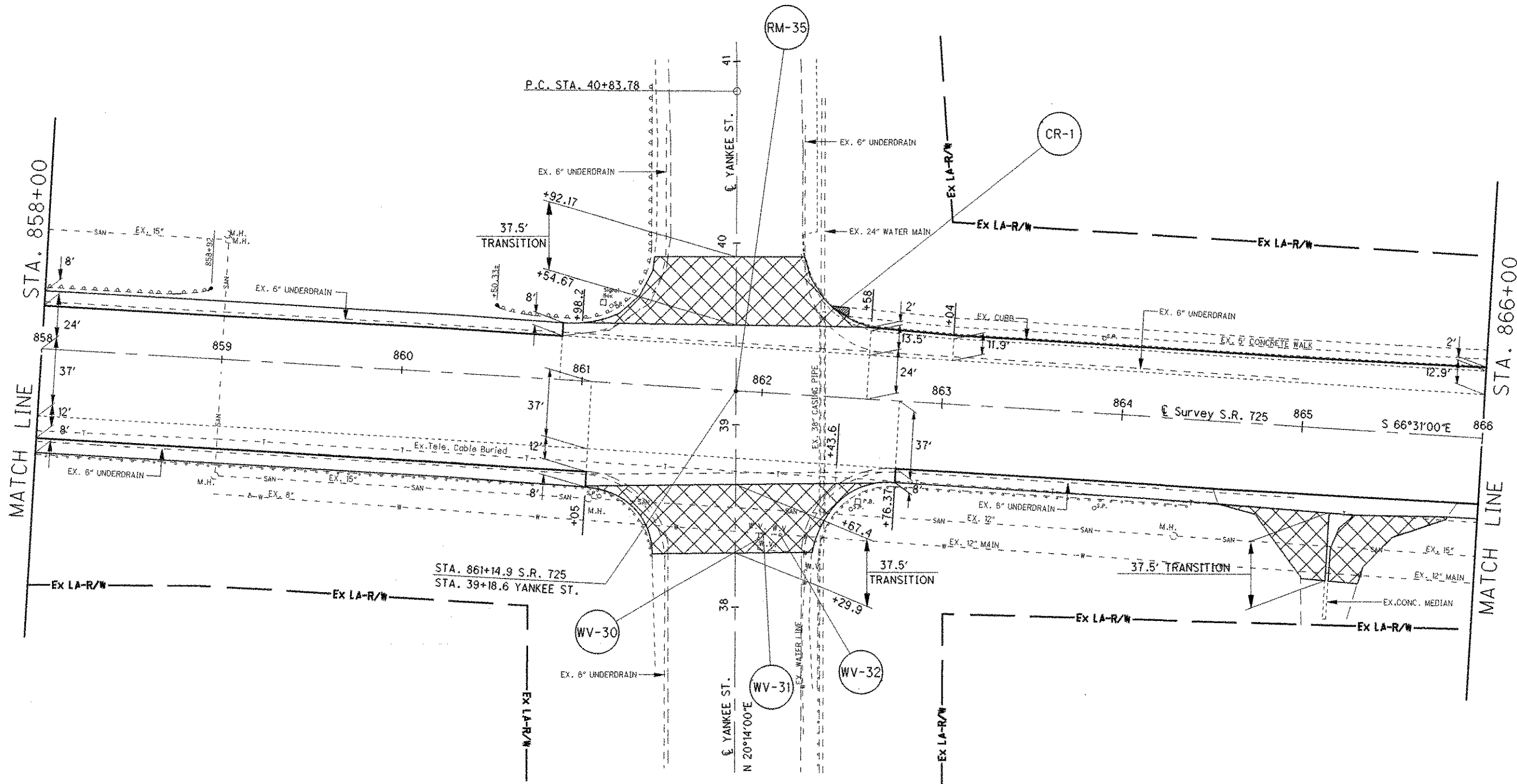


CALCULATED
REB
CHECKED
HORIZONTAL
SCALE IN FEET
0 15 30 45 60

PLAN SHEET - S.R. 725
STA. 850+00 TO STA. 858+00

MOT-725-13.82

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FOR BALLOON QUANTITIES SEE SHEET 19-24.

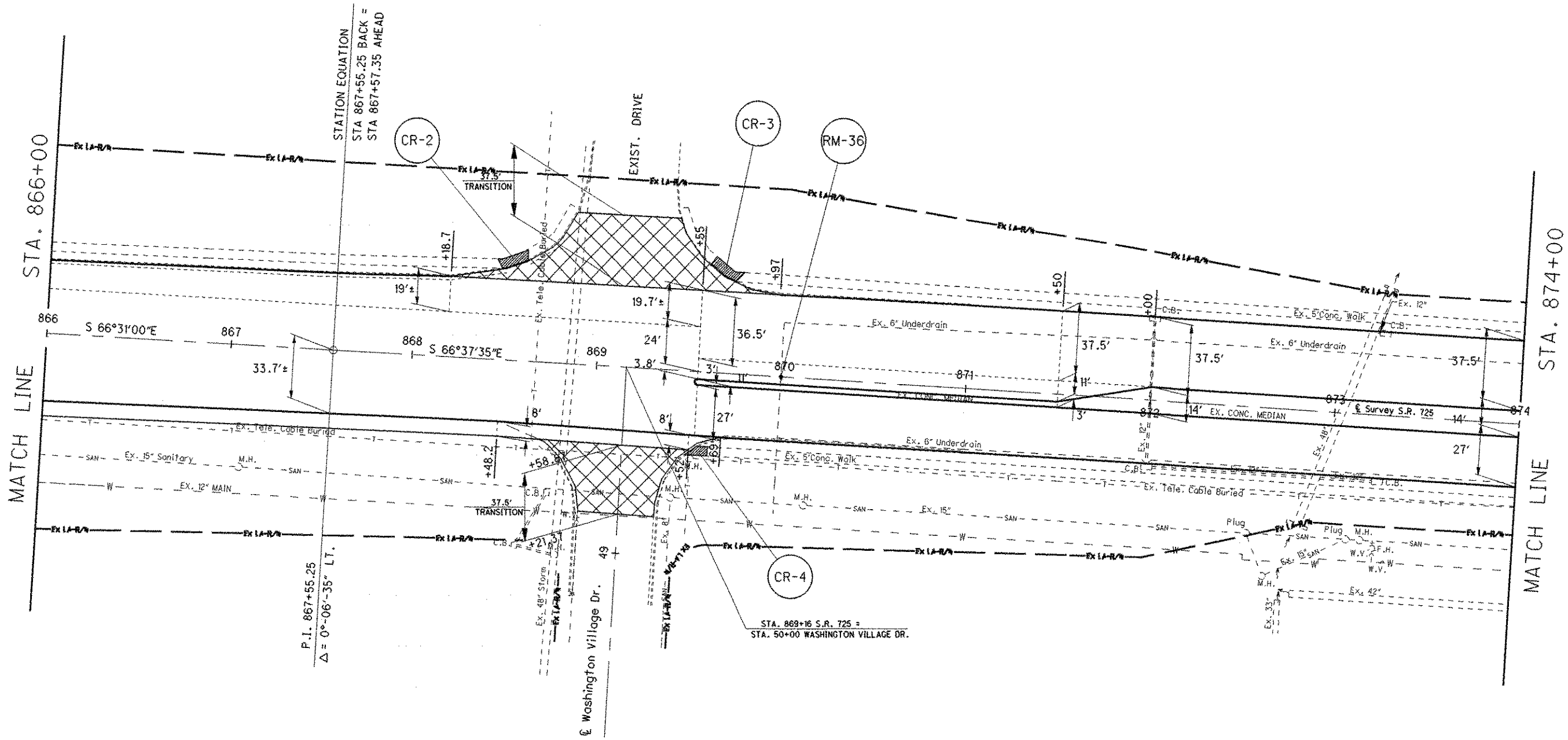
CALCULATED
REB
CHECKED

0 30 60
HORIZONTAL
SCALE IN FEET

PLAN SHEET - S.R. 725
STA. 858+00 TO STA. 866+00

MOT-725-13.8.2

42
102



STATION EQUATION
 STA 867+55.25 BACK =
 STA 867+57.35 AHEAD

P.I. 867+55.25
 $\Delta = 0^{\circ}-06'-35''$ LT.

STA. 869+16 S.R. 725 =
 STA. 50+00 WASHINGTON VILLAGE DR.

PAVEMENT LEGEND



= TRANSITION AREA

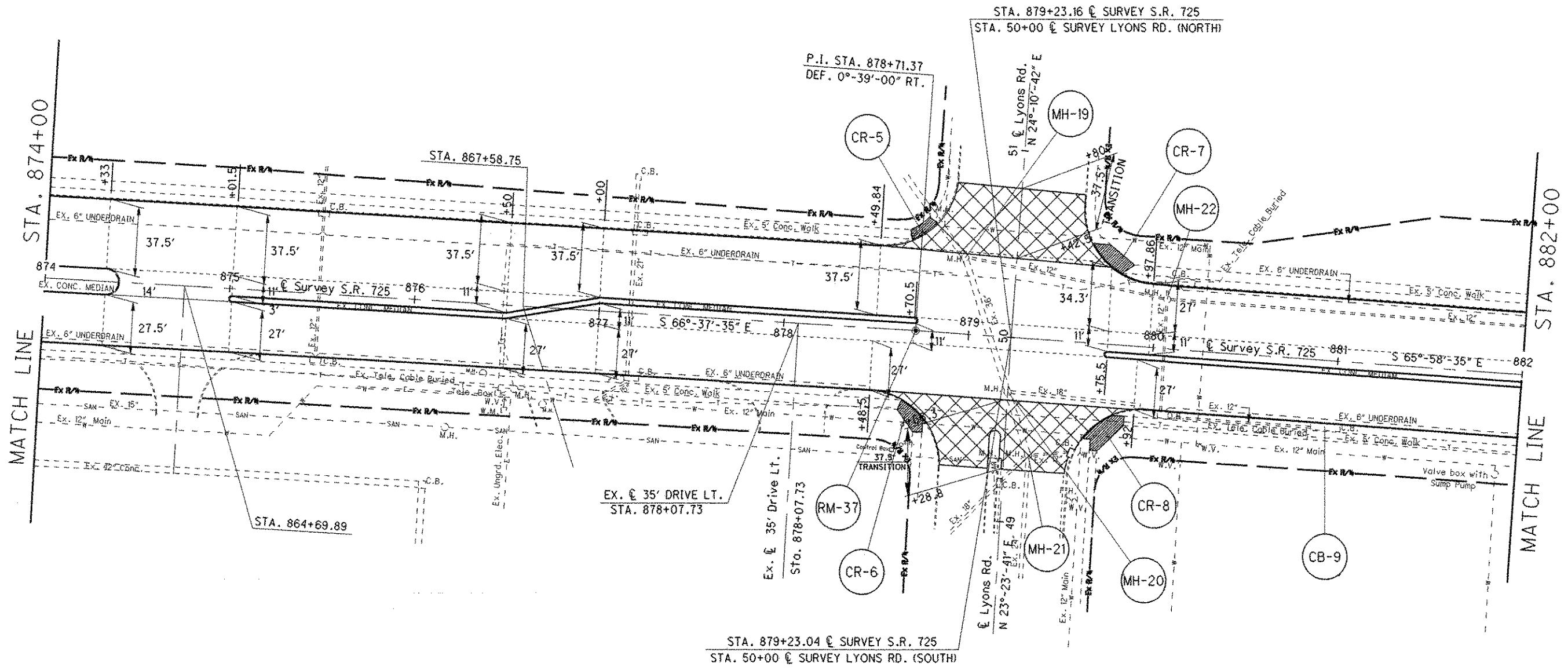
FOR BALLOON QUANTITIES SEE SHEET 19-24.

CALCULATED
 REB
 CHECKED

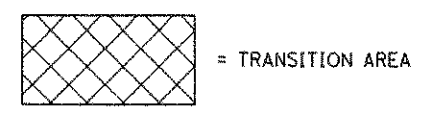
0 30 60
 HORIZONTAL
 SCALE IN FEET

PLAN SHEET - S.R. 725
 STA. 866+00 TO STA. 874+00

MOT-725-13.82



PAVEMENT LEGEND

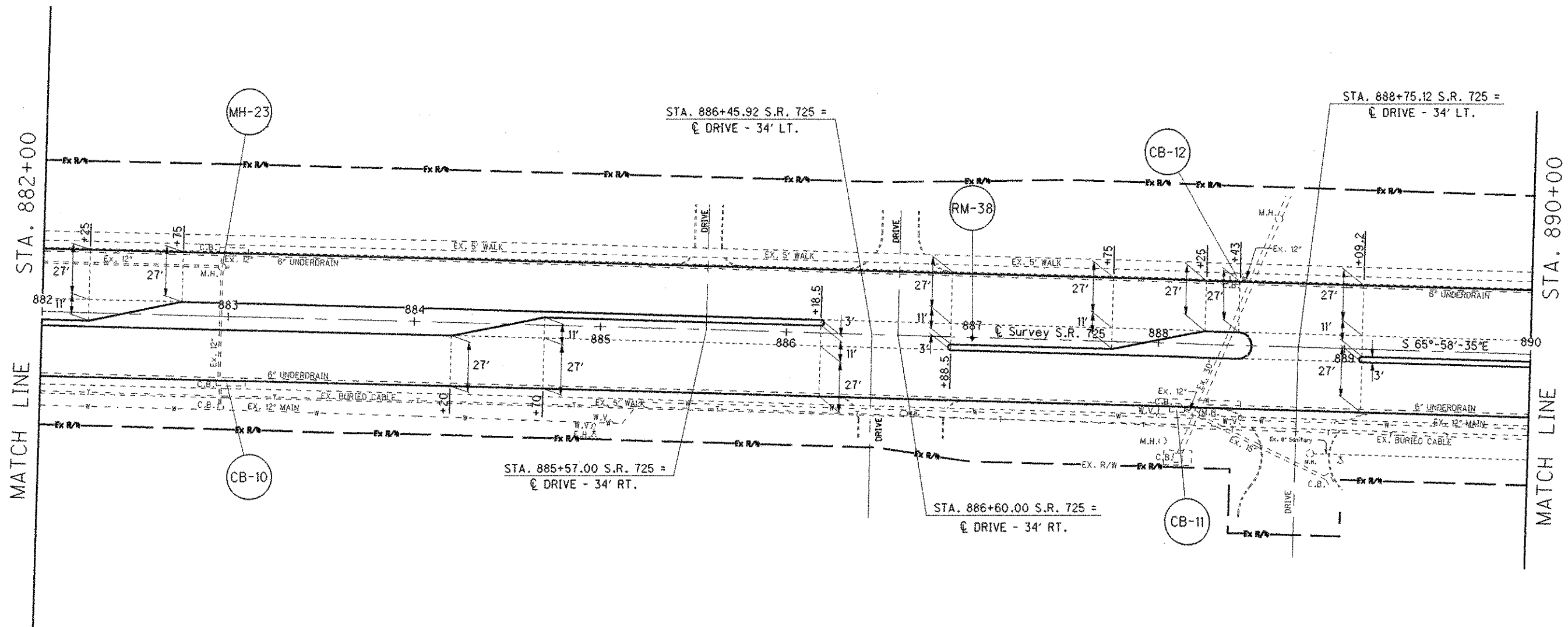


FOR BALLOON QUANTITIES SEE SHEET 19-24.



CALCULATED
 REB
 CHECKED

PLAN SHEET - S.R. 725
 STA. 874+00 TO STA. 882+00



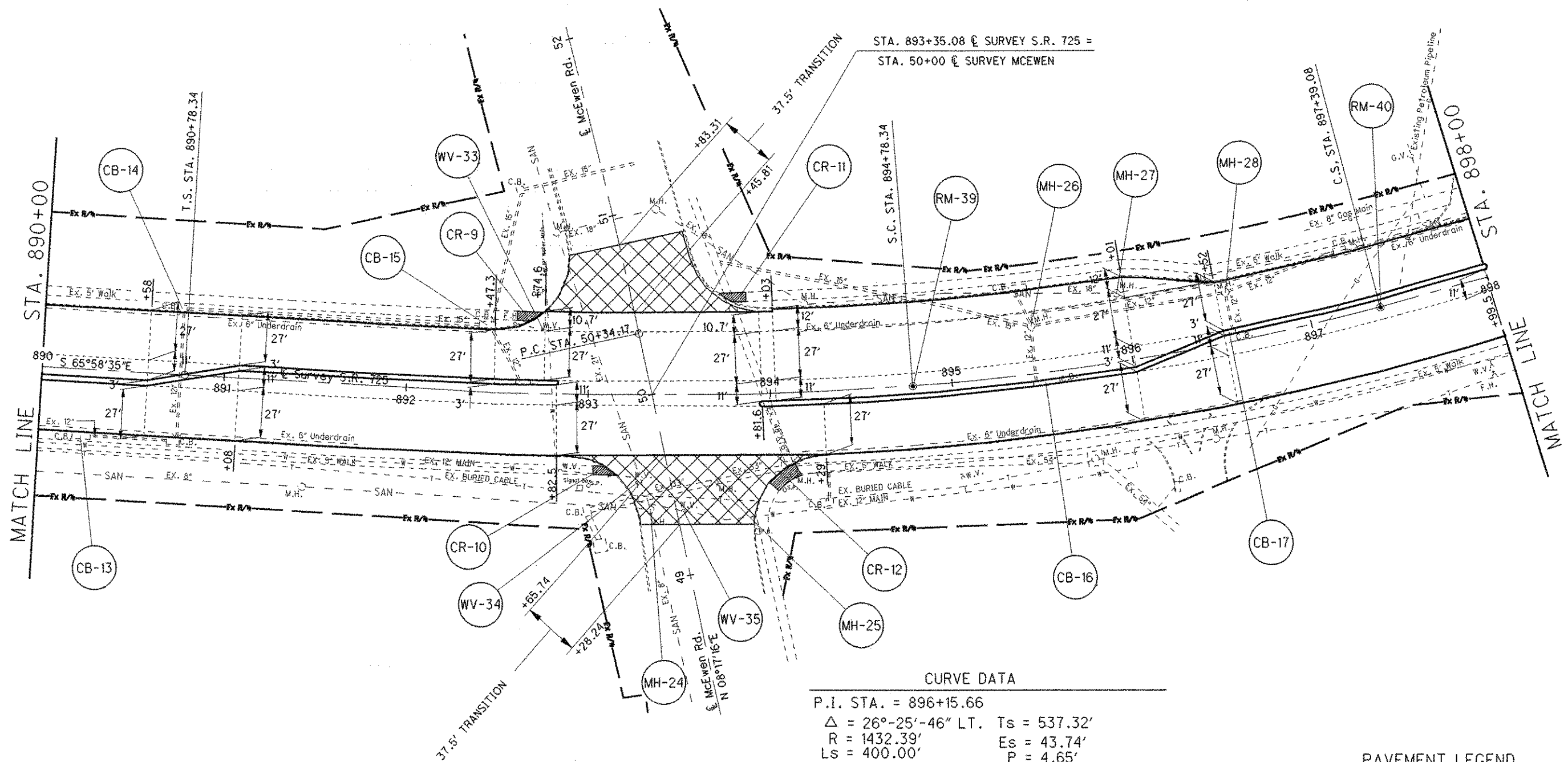
CALCULATED
REB
CHECKED

0 30 60
HORIZONTAL
SCALE IN FEET

PLAN SHEET - S.R. 725
STA. 882+00 TO STA. 890+00

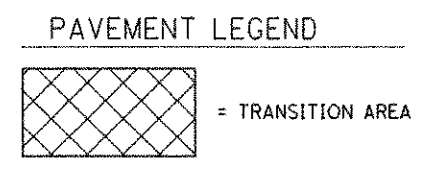
MOT-725-13.82

FOR BALLOON QUANTITIES SEE SHEET 19-24.



CURVE DATA

P.I. STA. = 896+15.66	
$\Delta = 26^{\circ}-25'-46''$ LT.	Ts = 537.32'
R = 1432.39'	Es = 43.74'
Ls = 400.00'	P = 4.65'
Lc = 260.74'	K = 199.87'
Xc = 399.22'	$\theta = 8^{\circ}-00'-00''$
Yc = 18.59'	$\Delta c = 10^{\circ}-25'-46''$
Dc = $4^{\circ}-00'-00''$	SE = REVERSE CROWN



CALCULATED
REB
CHECKED

HORIZONTAL
SCALE IN FEET

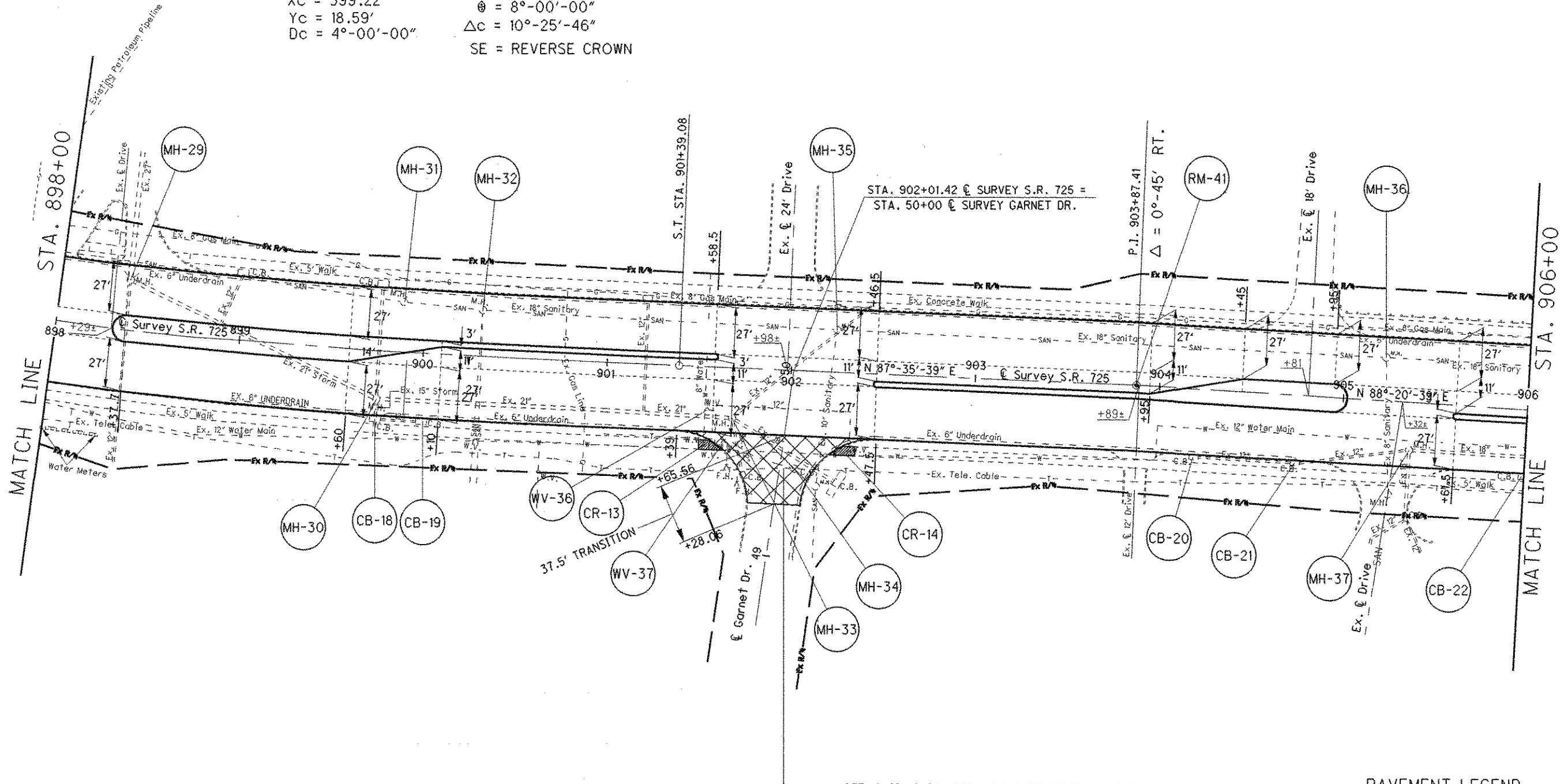
**PLAN SHEET - S.R. 725
STA. 890+00 TO STA. 898+00**

MOT-725-13.82

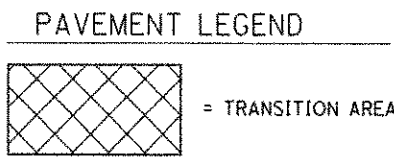
FOR BALLOON QUANTITIES SEE SHEET 19-24.

CURVE DATA

P.I. STA. = 896+15.66
 $\Delta = 26^{\circ}-25'-46''$ LT. $T_s = 537.32'$
 $R = 1432.39'$ $E_s = 43.74'$
 $L_s = 400.00'$ $P = 4.65'$
 $L_c = 260.74'$ $K = 199.87'$
 $X_c = 399.22'$ $\theta = 8^{\circ}-00'-00''$
 $Y_c = 18.59'$ $\Delta c = 10^{\circ}-25'-46''$
 $D_c = 4^{\circ}-00'-00''$ $SE = \text{REVERSE CROWN}$



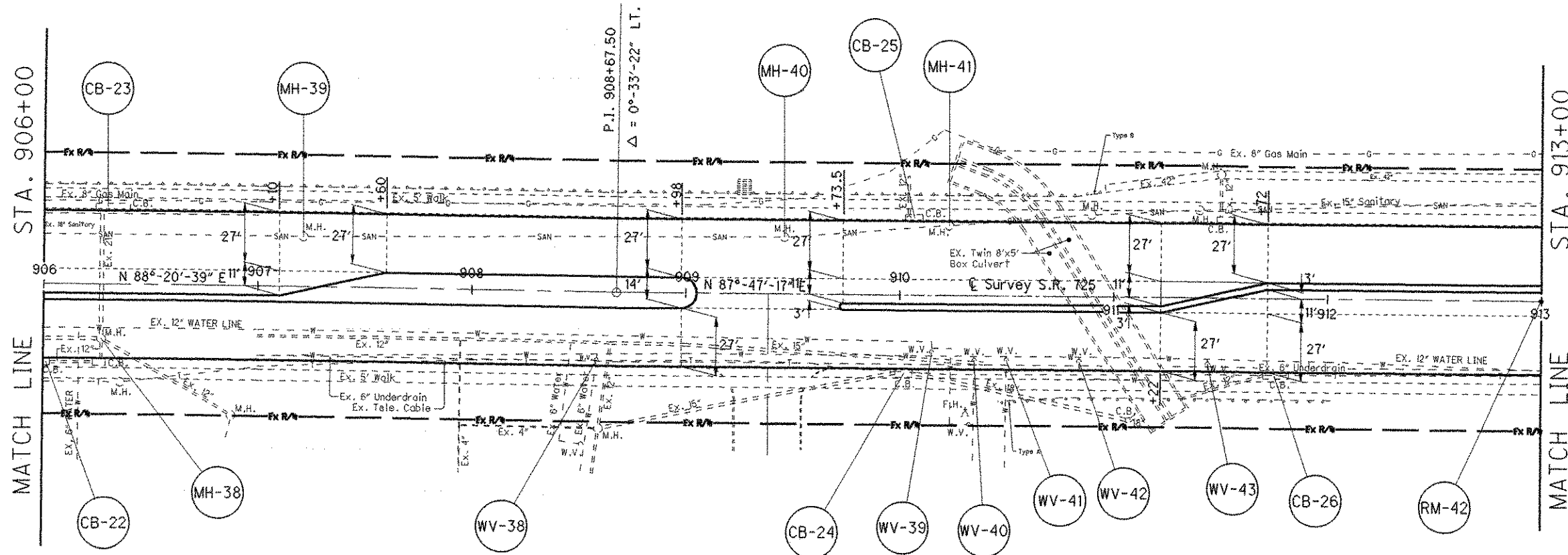
NOTE: QUICK CURB HERE AT GARNET DRIVE NOT SHOWN, CARE SHALL BE TAKEN NOT TO REMOVE IT OR DISTURB IT.



PLAN SHEET - S.R. 725
 STA. 898+00 TO STA. 906+00

MOT-725-13.8.2

FOR BALLOON QUANTITIES SEE SHEET 19-24.



CALCULATED
REB
CHECKED

0 30 60
HORIZONTAL
SCALE IN FEET

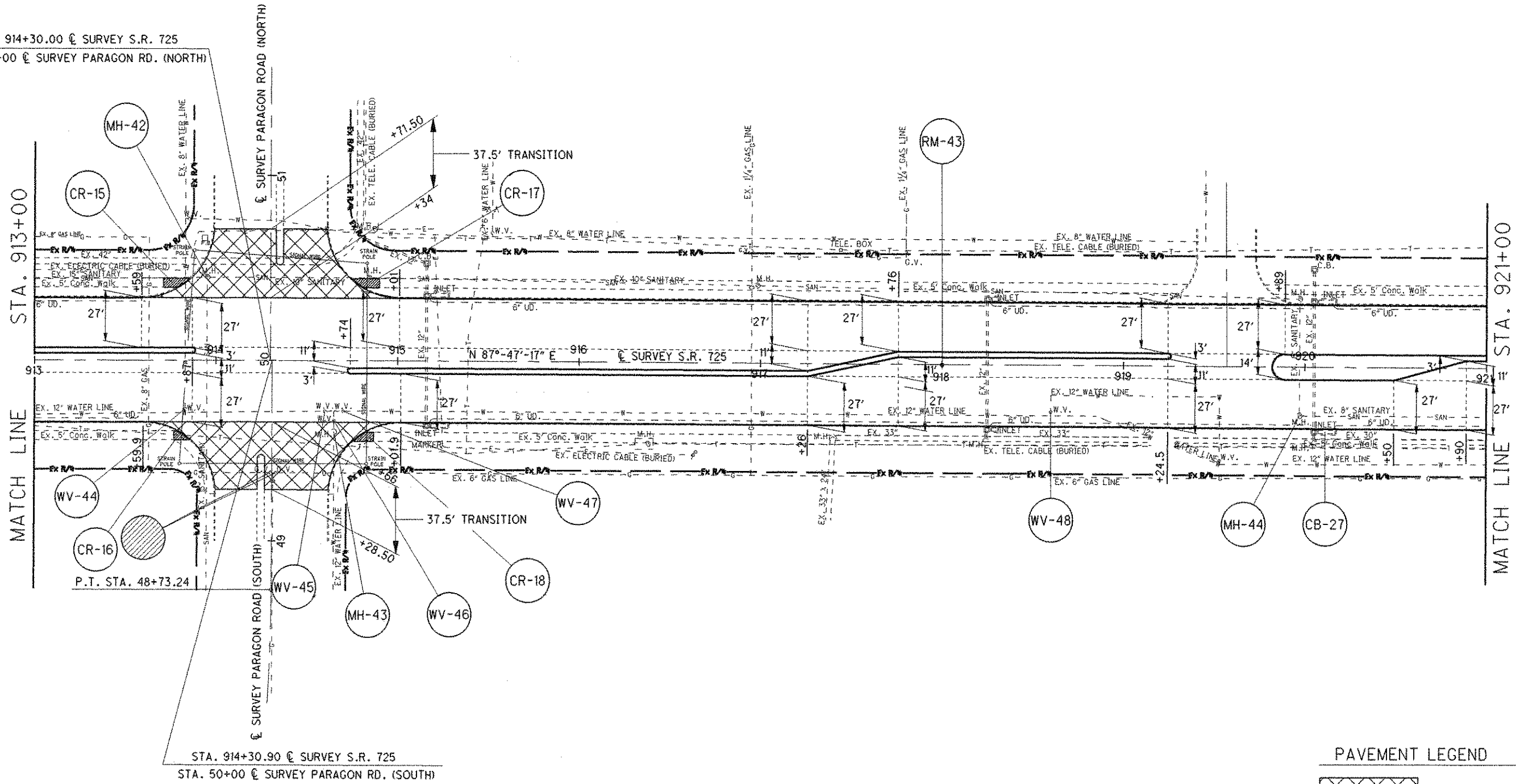
PLAN SHEET - S.R. 725
STA. 906+00 TO STA. 913+00

MOT-725-13.82

FOR BALLOON QUANTITIES SEE SHEET 19-24.

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STA. 914+30.00 @ SURVEY S.R. 725
 STA. 50+00 @ SURVEY PARAGON RD. (NORTH)



STA. 914+30.90 @ SURVEY S.R. 725
 STA. 50+00 @ SURVEY PARAGON RD. (SOUTH)

= GAS VALVES TO BE ADJUSTED BY OTHERS

PAVEMENT LEGEND

= TRANSITION AREA

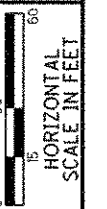
FOR BALLOON QUANTITIES SEE SHEET 19-24.

PLAN SHEET - S.R. 725
 STA. 913+00 TO STA. 921+00

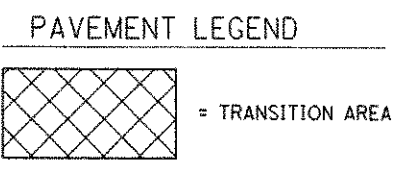
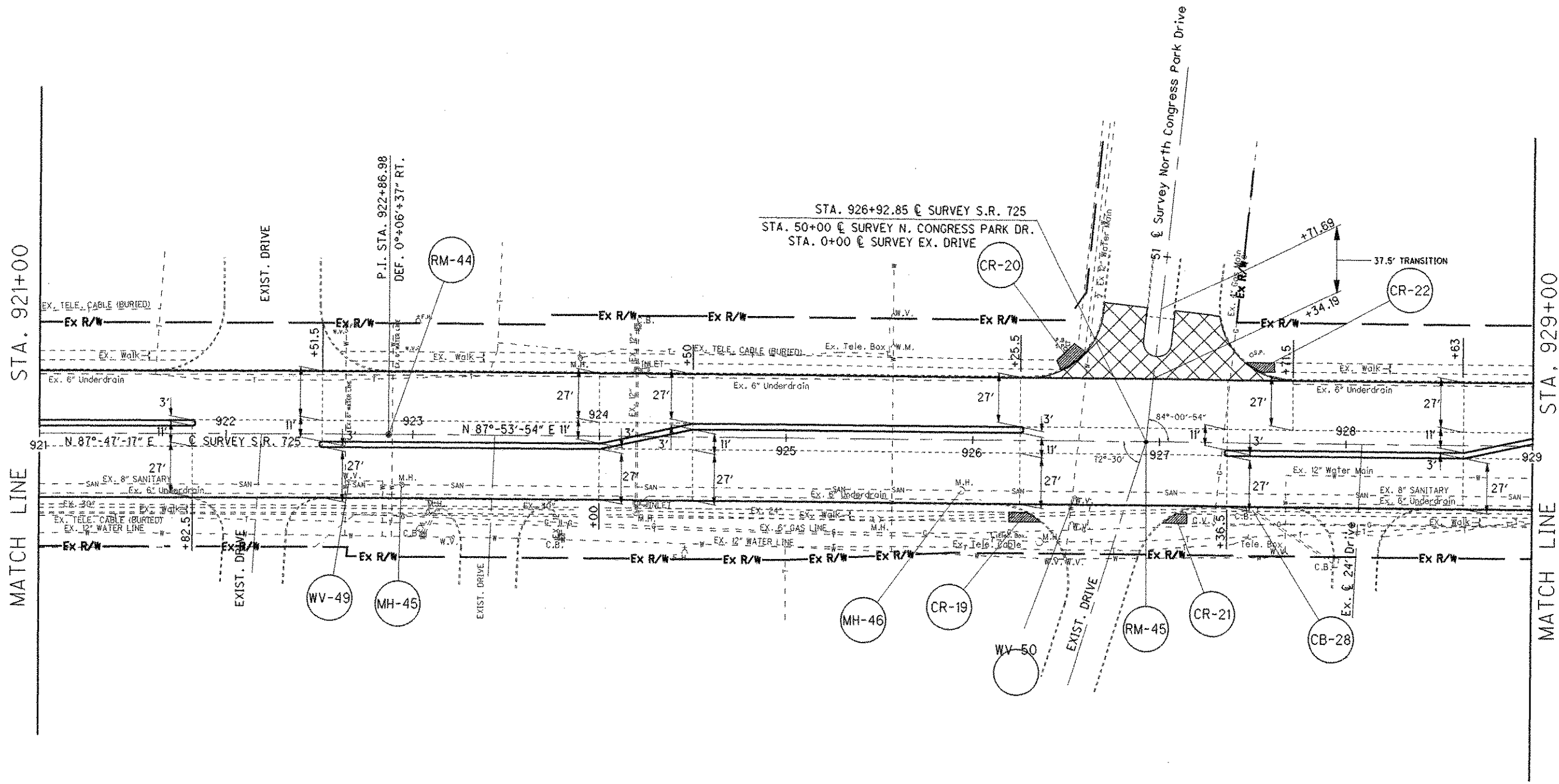
MOT-725-13.82

49
 102

CALCULATED
 REB
 CHECKED



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FOR BALLOON QUANTITIES SEE SHEET 19-24.

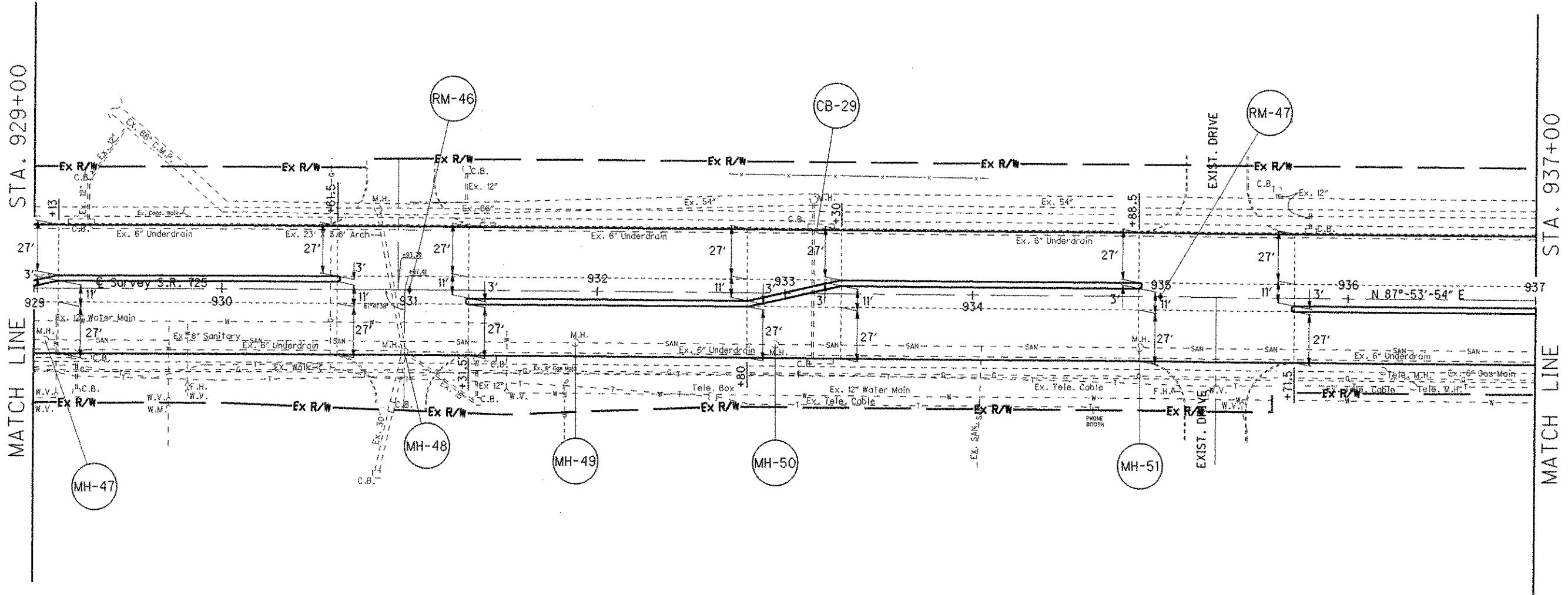
CALCULATED 0
 REB
 CHECKED

HORIZONTAL SCALE IN FEET

PLAN SHEET - S.R. 725
 STA. 921+00 TO STA. 929+00

MOT-725-13.82

50
102



CALCULATED
REB
CHECKED

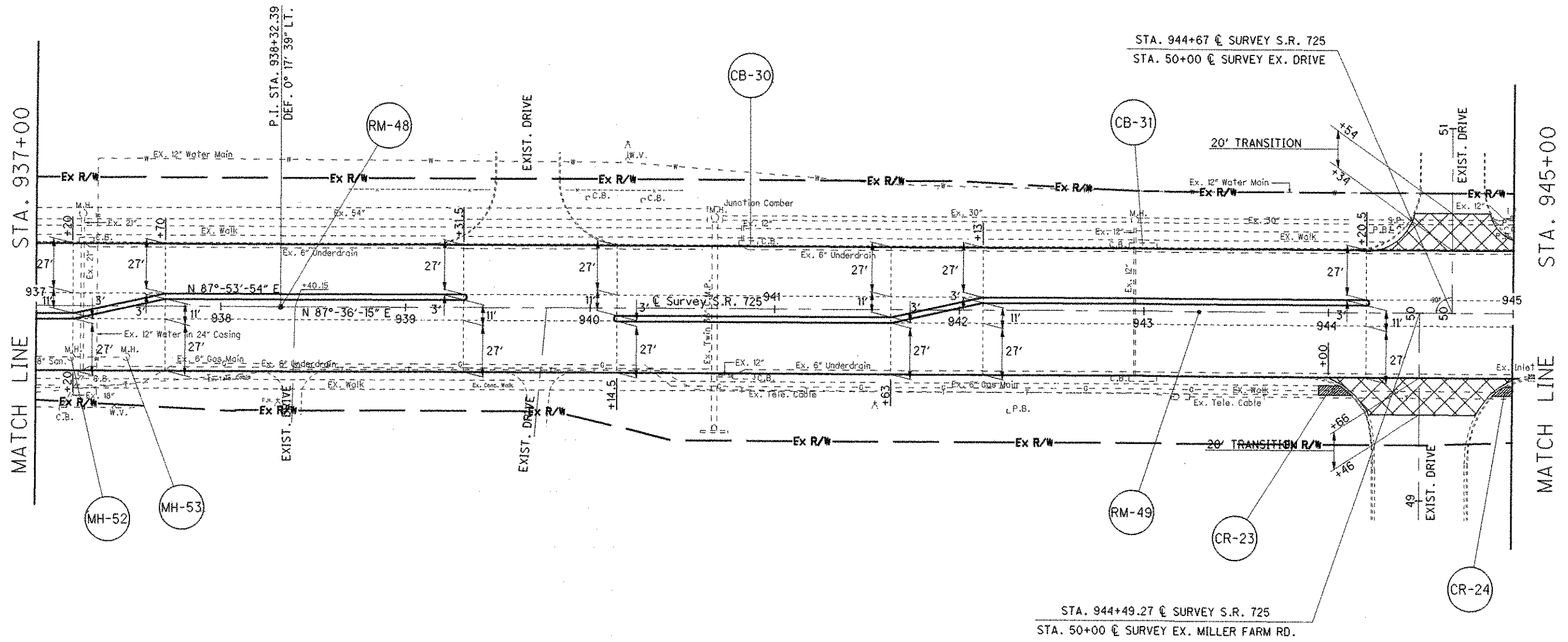
0 30 60
HORIZONTAL
SCALE IN FEET

PLAN SHEET - S.R. 725
STA. 929+00 TO STA. 937+00

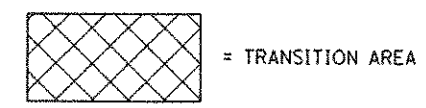
MOT-725-13.8.2

FOR BALLOON QUANTITIES SEE SHEET 19-24.

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PAVEMENT LEGEND



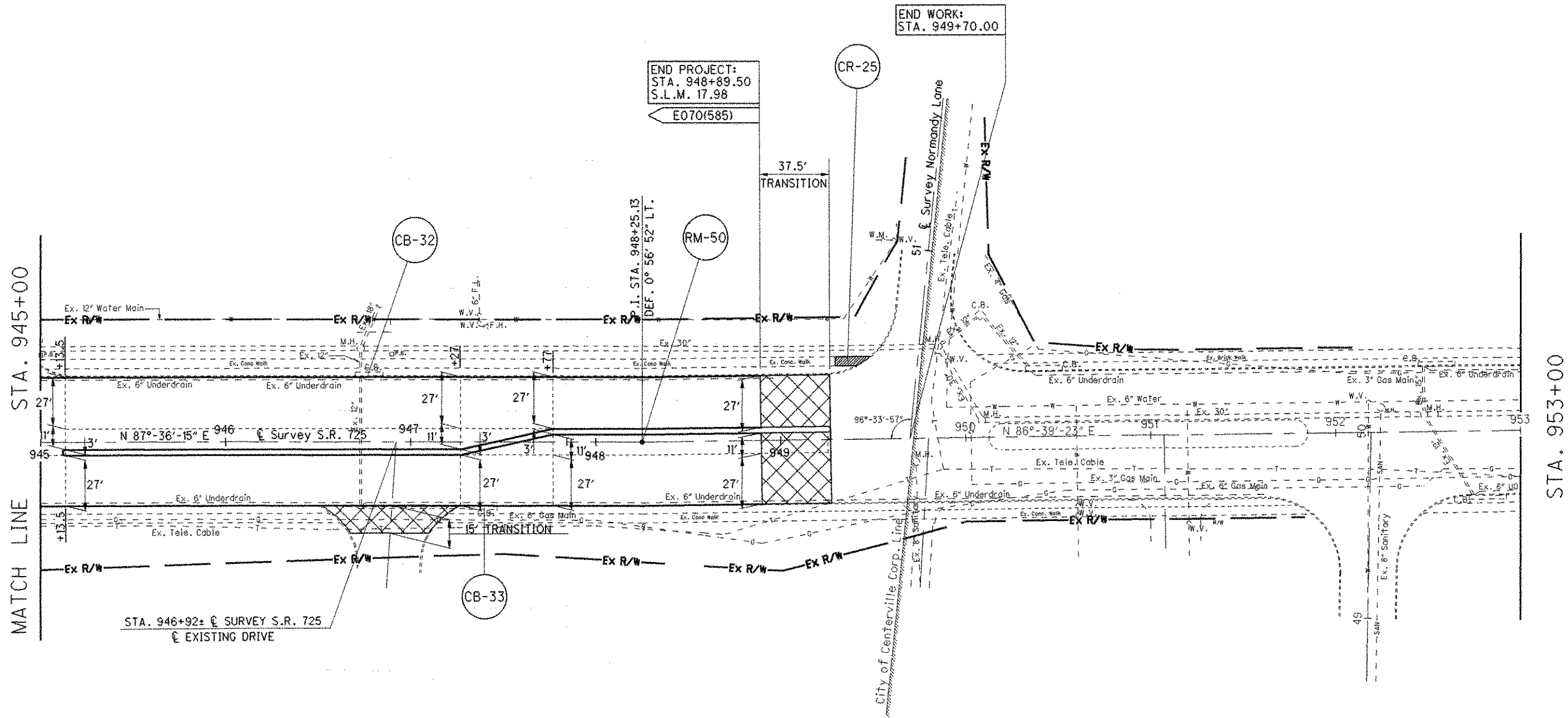
FOR BALLOON QUANTITIES SEE SHEET 19-24.

CALCULATED
REB
CHECKED

HORIZONTAL SCALE IN FEET

PLAN SHEET - S.R. 725
STA. 937+00 TO STA. 945+00

MOT-725-13.82

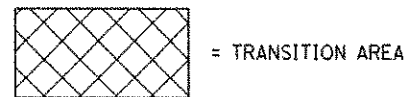


END PROJECT:
 STA. 948+89.50
 S.L.M. 17.98
 E070(585)

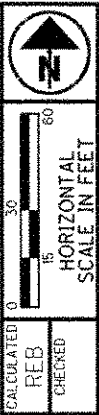
END WORK:
 STA. 949+70.00

STA. 953+00

PAVEMENT LEGEND



FOR BALLOON QUANTITIES SEE SHEET 19-24.



CALCULATED
 REB
 CHECKED

PLAN SHEET - S.R. 725
 STA. 945+00 TO STA. 953+00

MOT-725-13.82

15	SHEET NUMBER																		PARTICIPATION		ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	CALCULATED	REB	CHECKED														
	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	102	URBAN	RURAL																								
													PAVEMENT MARKING																														
		120	114	118	100	86	115	113	164	112	63	106	64	110	73	78	8	452	1092	621	00100	1544	EACH	RAISED PAVEMENT MARKER																			
1537																		445	1092	621	54000	1537	EACH	RAISED PAVEMENT MARKER REMOVED																			
		1.10	1.08	0.96	0.89	0.58	0.47	0.55	0.78	0.60	0.93	0.88	0.98	1.38	1.10	1.17	0.17	4.1	9.6	644	00100	13.7	MILE	EDGE LINE																			
		0.47	0.57	0.56	0.60	0.37	0.37	0.56	0.79	0.48	0.40	0.71	0.50	0.74	0.65	0.53	0.08	2.2	6.2	644	00200	8.4	MILE	LANE LINE																			
		0.02	0.001			0.09	0.28	0.47	0.40	0.49	0.13	0.07	0.01			0.01	0.01	0.01	1.99	644	00300	2.0	MILE	CENTERLINE																			
		1855	1099	1607	1474	1113	1839	698	788	841	919	932	774	1205	811	1196	163	6935	10379	644	00400	17314	FT	CHANNELIZING LINE																			
		243	203	185	240	248	138	157	247	257	261	222	159	104	209	110	36	826	2194	644	00500	3020	FT	STOP LINE																			
		404										701	708	118	681		158	404	2366	644	00600	2770	FT	CROSSWALK LINE																			
		105		163	130	191		315		385	48							398	939	644	00700	1337	FT	TRANSVERSE/DIAGONAL LINE																			
		24	74	64	58			123		198		124	121	310	314	44		220	1234	644	00900	1454	SO. FT.	ISLAND MARKING																			
		16	13	16	17	13	23	12	14	5	8	11	11	16	11	19	2	62	132	644	01300	194	EACH	LANE ARROW																			
		122		345	425					238								467	663	644	01500	1130	FT	DOTTED LINE, 4"																			
																			1	1	647	50100	1	EACH	PAVEMENT MARKING MISC.: "MERGE", TYPE A																		
																			1	1	647	50100	1	EACH	PAVEMENT MARKING MISC.: "MERGE ARROW", TYPE A																		

TRAFFIC CONTROL GENERAL SUMMARY

MOT-725-13.82

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SHEET NUMBER										PARTICIPATION		ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
71	72	73	101D					URBAN	RURAL								
TRAFFIC SIGNALS																	
										30		625	25300	30	FT	CONDUIT, 1 1/2", 725.04	
										13		630	02100	13	FT	GROUND MOUNTED SUPPORT, NO. 2 POST	
										12.5		630	80100	12.5	SQ. FT.	SIGN, FLAT SHEET	
										2		630	84900	2	EACH	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	
										2		630	86002	2	EACH	REMOVAL OF MOUNTED POST SUPPORT AND DISPOSAL	
										2		630	87400	2	EACH	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	
										3		632	05001	3	EACH	VEHICULAR SIGNAL HEAD, (LED), 3 SECTION, 12" LENS, 1-WAY, AS PER PLAN	102
										1		632	05081	1	EACH	VEHICULAR SIGNAL HEAD, (LED), 5 SECTION, 12" LENS, 1-WAY, AS PER PLAN	102
										3		632	25000	3	EACH	COVERING OF VEHICULAR SIGNAL HEADS	
	91	94		18				64	139			632	26501	203	EACH	DETECTOR LOOP, AS PER PLAN	102
								2				632	27005	2	EACH	LOOP DECTECTOR UNIT, AS PER PLAN	102
	91	94		18				64	139			632	27200	203	EACH	LOOP DETECTOR TIE IN	
								275				632	40700	275	FT	SIGNAL CABLE, 7 CONDUCTOR, NO. 14	
								640				632	65200	640	FT	LOOP DETECTOR LEAD IN CABLE	
								LUMP		LUMP		633	99300	LUMP		CONTROLLER ITEM, MISC.: EXISTING CONTROLLER AND CABINET MODIFICATION	102

TRAFFIC CONTROL GENERAL SUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621		
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)			
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH	
FROM	TO																						
74		S.R. 725	729+87		WB											69							
74,75		S.R. 725	729+87	735+42	↑	555												15					
74,75		S.R. 725	729+87	732+50	↑								263								7		
74		S.R. 725	729+87	733+15.6	↑								329/329		90						16		
74-76		S.R. 725	729+87	744+44	↑			1457													18		
74,75		S.R. 725	729+87	735+42	↑		561																
74		S.R. 725	730+16		↑																	2	
74		S.R. 725	731+04		↑																	2	
74		S.R. 725	731+92		↑																	2	
75,76		S.R. 725	736+65	744+10	↓	745																	
75,76		S.R. 725	736+65	744+44	↓		776																
75		S.R. 725	736+65	737+96	↓								131								3		
75		S.R. 725	736+95		↓																	1	
75		S.R. 725	737+75		WB																	1	
76		S.R. 725 @ IMPERIAL RD	744+10	11+03	INTERSEC	104																	
76		S.R. 725 @ IMPERIAL RD	10+60	11+00	↑				40				40								3	2	
76		S.R. 725 @ IMPERIAL RD	10+60		↑											36							
76		S.R. 725 @ IMPERIAL RD	10+90		↑												2						
76		S.R. 725 @ IMPERIAL RD	11+03	745+71	↑	80																	
76		S.R. 725 @ IMPERIAL RD	744+32	744+44	↑																	28	
76		S.R. 725 @ IMPERIAL RD	10+60	745+40	↓								122										
76		S.R. 725 @ IMPERIAL RD	745+09	745+40	INTERSEC		50																
76		S.R. 725	745+40	745+47	WB											44							
76		S.R. 725	745+40	746+92	↑								149								3		
76		S.R. 725	745+77		↑																	1	
76		S.R. 725	746+57		↑																	1	
76,77		S.R. 725	745+71	752+12.5	↑	642															15		
76,77		S.R. 725	745+09	752+12.5	↑																	704	
76,77		S.R. 725	745+40	752+12.5	↑																	673	
76,77		S.R. 725	749+00	752+12.5	↑																	313	
77		S.R. 725	752+08.5		↓																		
77		WATERTOWER LN CONNECTION	1+40	1+50	WB											15						24	
77		S.R. 725 @ BYERS RD	752+13		INTERSEC																	20	
77		S.R. 725 @ BYERS RD	752+19		↑																		
77		S.R. 725 @ BYERS RD	0+69		↑																		
77		S.R. 725 @ BYERS RD	0+75		↑																		
77		S.R. 725 @ BYERS RD	0+80		↑																		
77		S.R. 725 @ BYERS RD	0+80	1+32	INTERSEC																		
77		S.R. 725	752+86		WB																		
77		S.R. 725	752+86	753+04	↑																		
77		S.R. 725	752+86	754+65	↑																	18	
77		S.R. 725	752+86	756+15	↑																		
77		S.R. 725	752+86	756+95	↑																		
77		S.R. 725	754+65	756+95	↑																		
77		S.R. 725	752+86	756+83	↑	397																	
77		S.R. 725	753+04	757+43	↑																		
77		S.R. 725	753+17		↑																		
77		S.R. 725	754+04		WB																		
TOTALS						2,523.0	3,217.0	2,461.0	93.0		122.0	1,855.0	404.0	90.0	15	243.0	16.0	24.0	45.0		71.0	4.0	
SUB-TOTALS						5,740.0	2,461.0		93.0		122.0	1,855.0	404.0		105.0	243.0	16.0	24.0			120.0		
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						1.10 MI	0.47 MI	0.02 MI	122	1,855	404	105	243	16	24	45.0	120						

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PAVEMENT MARKING SUBSUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621		
			4"	4"		4"	4"	4"	4"	8"	12"	24"	24"	24"	24"	24"	24"	24"	24"	24"	24"	24"	24"
			WHITE EDGE LINE	YELLOW EDGE LINE		WHITE LANE LINE	CENTER LINE SOLID DOUBLE YELLOW	CENTER LINE YELLOW SOLID/DASHED	WHITE DOTTED LINE	WHITE CHANNELIZING LINE	CROSSWALK LINE	WHITE TRANSVERSE/DIAGONAL LINE	YELLOW TRANSVERSE/DIAGONAL LINE	STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)			
			FROM	TO		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH	
77		S.R. 725	754+92		WB												1						
74		S.R. 725 @ ALEXANDERVILLE	21+76.6	729+87	INTERSEC	71																	
74		S.R. 725 @ ALEXANDERVILLE	19+79.67	729+94.5	INTERSEC	68																	
74		S.R. 725 @ ALEXANDERVILLE	729+87 WB	729+94.5 EB	INTERSEC		33																
74,75		S.R. 725	729+94.5	735+42	EB	548	548																
74-76		S.R. 725	729+87	744+32	EB			1445													19		
75		S.R. 725 @ MONARCH LN	735+42	735+42	INTERSEC		63																
75		S.R. 725 @ MONARCH LN	735+42	11+00	↑	88																	
75		S.R. 725 @ MONARCH LN	11+00	736+65		85																	
75		S.R. 725 @ MONARCH LN	735+42 EB	9+30		50																	
75		S.R. 725 @ MONARCH LN	9+30	736+65	↓	40			12														
75		S.R. 725 @ MONARCH LN	9+30	9+48						12							53						
75		S.R. 725 @ MONARCH LN	736+65 WB	736+65 EB	INTERSEC		44																
75,76		S.R. 725	736+65	744+32	EB	767	770											15					
75,76		S.R. 725	742+84	744+32	↑					148												4	
76		S.R. 725	743+14														1						
76		S.R. 725	744+02														1						
76		S.R. 725	744+32																				
76,77		S.R. 725	744+32	751+96		768																15	
76,77		S.R. 725	745+09	752+15			667																
76,77		S.R. 725	745+40	752+00				660														9	
76,77		S.R. 725	749+00	752+00				300														4	
76,77		S.R. 725	747+70	751+96						426												11	
76,77		S.R. 725	750+72	752+00						128												3	
76		S.R. 725	748+13														1						
76		S.R. 725	749+01														1						
76		S.R. 725	749+89														1						
76		S.R. 725	750+77														1						
77		S.R. 725	750+81														1						
77		S.R. 725	751+65														1						
77		S.R. 725	751+69														1						
77		S.R. 725	753+04																				
77		S.R. 725	753+04	757+43			439	439														6	
77		S.R. 725	753+04	754+94				190														3	
77		S.R. 725	753+14	753+57		43																	
77		S.R. 725	753+57	757+00		343												15					
77		S.R. 725	754+94	757+43						249												6	
77		S.R. 725	755+28														1						
77		S.R. 725	756+16														1						
77		S.R. 725	757+04														1						
77		S.R. 725	757+43		EB											48		57					
77		S.R. 725 @ RAMP A (75)	757+00	18+50	INTERSEC	82																	
77		S.R. 725 @ RAMP A (75)	18+50	757+99	INTERSEC	65																	
77		S.R. 725 @ RAMP B (75)	756+83	1+00	INTERSEC	92																	
77		S.R. 725 @ RAMP B (75)	0+32		INTERSEC																		
77		S.R. 725 @ RAMP B (75)	0+32	1+00	INTERSEC																	4	
TOTALS						3,120.0	2,564.0	3,034.0	12.0							203.0	13.0	74.0	45.0		69.0		
SUB-TOTALS						5684.0	3,034.0		12.0							203.0	13.0	74.0			114.0		
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						1.08 MI	0.57 MI	0.001 MI			1099				203	13	74			114			

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PAVEMENT MARKING SUBSUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621	
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)		
			FROM	TO		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH		
77		S.R. 725 @ RAMP B (75)	0+57		INTERSEC																	
77		S.R. 725 @ RAMP B (75)	0+67		INTERSEC																	
77		S.R. 725 @ RAMP B (75)	0+18.5	1+00	INTERSEC	82																
77		S.R. 725 @ RAMP B (75)	0+32	758+44	INTERSEC					121												
77,78		S.R. 725	757+76	766+92	WB	916												15				
77		S.R. 725	758+09		↑											35						
77		S.R. 725	758+30		↑										10							
77,78		S.R. 725	758+09	764+00	↑			591												8		
77,78		S.R. 725	758+09	766+60	↑			851												11		
77,78		S.R. 725	758+09	760+36	↑						227				2					6		
77,78		S.R. 725	758+30	766+75	↓																	
78		S.R. 725	766+75		WB												8					
77-79		S.R. 725	757+99	767+38	EB	939																
77,78		S.R. 725	758+44	766+75	↑			831	831												11	
77,78		S.R. 725	758+44	761+35	↑																	
78		S.R. 725	761+50	766+75	↑						525							15			13	
78		S.R. 725	762+05		↑																	
78		S.R. 725	762+93		↑																	
78		S.R. 725	763+81		↑																	
78		S.R. 725	764+69		↑																	
78		S.R. 725	765+57		↑																	
78		S.R. 725	766+45		↓																	
78		S.R. 725	766+75		EB										45							
78,79		S.R. 725 @ RAMP C (75)	766+60	14+65	INTERSEC					129												
79		S.R. 725 @ RAMP C (75)	13+90	767+38	↑	85																
79		S.R. 725 @ RAMP C (75)	14+65		↑																	
79		S.R. 725 @ RAMP C (75)	13+90	14+65	↑						75										3	
79		S.R. 725 @ RAMP C (75)	13+90	14+71	↑						82		83								3	
79		S.R. 725 @ RAMP C (75)	14+35		↑																	
79		S.R. 725 @ RAMP C (75)	13+90	14+64	↑						78				2						3	
79		S.R. 725 @ RAMP C (75)	14+32		↑																	
79		S.R. 725 @ RAMP C (75)	14+71	14+64	↑						42										3	
79		S.R. 725 @ RAMP C (75)	14+64		↑																	
78,79		S.R. 725 @ RAMP D (75)	766+75	0+36						95												
79		S.R. 725 @ RAMP D (75)	0+36	1+30																	3	
78,79		S.R. 725 @ RAMP D (75)	766+92	2+00		182																
79		S.R. 725 @ RAMP D (75)	2+00	767+65		204																
79		S.R. 725 @ RAMP D2 (75)	770+68	6+44									157								4	
79		S.R. 725 @ RAMP D2 (75)	6+44	6+54								10										
79		S.R. 725 @ RAMP D2 (75)	4+16.39	6+54	INTERSEC	238																
79		S.R. 725	767+65		WB																	
79		S.R. 725	767+65	769+14	WB	149																
79		S.R. 725	769+14	773+35	WB								421								10	
79		S.R. 725	767+65	773+35	WB																10	
79		S.R. 725	767+65	773+51	WB																	
79		S.R. 725	767+65		WB																	
TOTALS						2,795.0	2,258.0	2,937.0			345.0	1,607.0		83.0	80.0	185.0	16.0	64.0	30.0		88.0	
SUB-TOTALS							5,053.0	2,937.0			345.0	1,607.0		163.0		185.0	16.0	64.0			118.0	
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY							0.96 MI	0.56 MI			345	1607		163	185	16	64			118		

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PAVEMENT MARKING SUBSUMMARY
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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621	
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)		
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH		
FROM	TO																					
79		S.R. 725	773+51		WB												8					
79		S.R. 725	771+43.84	773+50	WB	206																
79		S.R. 725 @ MALL WOOD DR	773+50	0+75	INTERSEC	43																
79		S.R. 725 @ MALL WOOD DR	0+75	774+75	INTERSEC	59																
79		S.R. 725 @ MALL WOOD DR	0+51	0+75	INTERSEC		27/22															
79		S.R. 725 @ MALL WOOD DR	0+51		INTERSEC										32		38					
79		S.R. 725	773+75	774+54	WB		81										6/6					
79-81		S.R. 725	774+77	781+48	↑		671															
79		S.R. 725	774+77		↑										46							
79,80		S.R. 725	774+77	776+28	↑			151												3		
79-81		S.R. 725	774+77	781+48	↓	671		671									15			9		
79,80		S.R. 725	774+77	777+50	WB						273					3				7		
79		S.R. 725	767+57	773+51	EB		600															
79		S.R. 725	767+57	768+50	↑									64								
79		S.R. 725	767+72	773+51	↑			579												7		
79		S.R. 725	769+40	773+51	↑			411			411									10		
79		S.R. 725	768+50	773+51	↑	501																
79		S.R. 725	769+69		↑																	
79		S.R. 725	770+57		↑												1					
79		S.R. 725	771+45		↑												1					
79		S.R. 725	772+33		↑												1					
79		S.R. 725	773+21		↓												1					
79		S.R. 725	773+51		EB											45						
79		S.R. 725 @ PRESTIGE PLAZA	773+51	0+85	INTERSEC	68																
79		S.R. 725 @ PRESTIGE PLAZA	0+45	0+85	INTERSEC				40/45						66							
79		S.R. 725 @ PRESTIGE PLAZA	0+50.2		INTERSEC																	
79		S.R. 725 @ PRESTIGE PLAZA	0+85	774+77	INTERSEC	60										14						
79		S.R. 725	773+75	774+54	EB		81															
79		S.R. 725	774+14.7	774+45	↑	34																
79-81		S.R. 725	774+77	781+70	↑	696																
79-81		S.R. 725	774+77	781+48	↑		671		693										15		9	
79,80		S.R. 725	774+77	778+75	↑			398													5	
80,81		S.R. 725	778+75	781+48	↑			273													3	
80,81		S.R. 725	776+75	781+70	↑						495										12	
80,81		S.R. 725	778+75	781+70	↑						295										7	
80		S.R. 725	779+42		↑																2	
80		S.R. 725	779+64		↑																1	
80		S.R. 725	780+30		↑																2	
80		S.R. 725	780+52		↑																1	
81		S.R. 725	781+18		↑																2	
81		S.R. 725	781+40		↑																1	
81		S.R. 725	781+48		↓																22	
81		S.R. 725	781+70		EB											36						
81		S.R. 725 @ S.R. 741	781+48	190+98	INTERSEC	68					133											
81		S.R. 725 @ S.R. 741	190+98		INTERSEC											45						
81		S.R. 725 @ S.R. 741	190+92	190+98	INTERSEC		6/6															
81		S.R. 725 @ S.R. 741	781+48	189+10	INTERSEC					150												
81		S.R. 725 @ S.R. 741	190+98	783+55	INTERSEC					142												
81		S.R. 725 @ S.R. 741	190+98	783+06	INTERSEC	81																
TOTALS						2,487.0	2,165.0	3,176.0			425.0	1,474.0			130.0	240.0	17.0	58.0	30.0		70.0	
SUB-TOTALS						4,652.0		3,176.0			425.0	1,474.0			130.0	240.0	17.0	58.0			100.0	
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.89 MI	0.60 MI				425	1474			130	240	17	58			100	

PAVEMENT MARKING SUBSUMMARY

MOT-725-13.82

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621		
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)			
			FROM	TO		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH	
81		S.R. 725 @ S.R. 741	781+70	189+10	INTERSEC	90																	
81		S.R. 725 @ S.R. 741	189+10	783+55	INTERSEC	93																	
81		S.R. 725	783+06		WB												24						
81		S.R. 725	783+55		↑											12							
81		S.R. 725	783+85														1						
81		S.R. 725	784+73														1						
81		S.R. 725	785+61														1						
81		S.R. 725	786+49														1						
81		S.R. 725	787+37														1						
81		S.R. 725	788+25														1						
82		S.R. 725	789+13														1						
81		S.R. 725	783+04	784+93							189/189			15							9		
81,82		S.R. 725	784+93	789+60							467										12		
81,82		S.R. 725	783+55	791+55	↓																		
81,82		S.R. 725	783+06	791+55	WB	849		849										15			11		
82		S.R. 725 @ MALL PARK DR	791+55	0+65	INTERSEC	60																	
82		S.R. 725 @ MALL PARK DR	0+32.5	0+65	↑				33			33										3	
82		S.R. 725 @ MALL PARK DR	0+32.5													40							
82		S.R. 725 @ MALL PARK DR	0+65	792+63		52																	
82		S.R. 725 @ MALL ENT. No 4	791+02	0+92		80																	
82		S.R. 725 @ MALL ENT. No 4	791+32.5	0+92			72																
82		S.R. 725 @ MALL ENT. No 4	791+86	0+92		56																	
82		S.R. 725 @ MALL ENT. No 4	791+55													34							
82		S.R. 725 @ MALL ENT. No 4	0+52	0+92			40/40					40/40											
82		S.R. 725 @ MALL ENT. No 4	0+52													42							
82		S.R. 725 @ MALL ENT. No 4	0+82													3							
82		S.R. 725 @ MALL ENT. No 4	0+82	792+90	INTERSEC	68																	
82		S.R. 725	792+63		WB											36							
82		S.R. 725	792+63	793+75	↑							112										3	
82		S.R. 725	792+93													1							
82		S.R. 725	793+67													1							
82,83		S.R. 725	792+63	796+65		402												15					
82-84		S.R. 725	792+63	803+75	↓			1112														14	
82,83		S.R. 725	792+63	796+74	WB				411													6	
83		S.R. 725 @ LOIS CIRCLE	796+57	0+58	INTERSEC	43																	
83		S.R. 725 @ LOIS CIRCLE	0+34	0+58	↑				24				24										
83		S.R. 725 @ LOIS CIRCLE	0+34													40							
83		S.R. 725 @ LOIS CIRCLE	0+58	797+71		56																	
83		S.R. 725 @ MALL ENT. No 3	797+33	0+58									19										
83		S.R. 725 @ MALL ENT. No 3	796+30	797+33																			
83		S.R. 725 @ MALL ENT. No 3	796+70	1+00																			
83		S.R. 725 @ MALL ENT. No 3	0+58	1+00																			
83		S.R. 725 @ MALL ENT. No 3	796+24	1+00		77																	
83		S.R. 725 @ MALL ENT. No 3	0+68													1							
83		S.R. 725 @ MALL ENT. No 3	0+58	797+84	↓	96																	
83		S.R. 725 @ MALL ENT. No 3	0+58		INTERSEC											20							
TOTALS						2,022.0	1,049.0	1,961.0	486.0				1,113.0		191.0	248.0	13.0		30.0		50.0	6.0	
SUB-TOTALS						3,071.0		1,961.0	486.0				1,113.0		191.0	248.0	13.0				86.0		
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.58 Mi	0.37 Mi	0.09 Mi				1113		191	248	13			86				

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PAVEMENT MARKING SUBSUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621	
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)		
			FROM	TO		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH
83,84		S.R. 725	797+59	803+75	WB				616												8	
83		S.R. 725	797+59	799+75	WB	216																
83		S.R. 725	799+75	800+41	WB	76																
83,84		S.R. 725	800+87	803+59	WB	272																
81,82		S.R. 725	783+55	791+02	EB	747																
81,82		S.R. 725	783+55	791+55	▲			800	800									15				
81		S.R. 725	787+50	788+50				100													10	
81,82		S.R. 725	788+50	791+86									336								3	
82		S.R. 725	790+50	791+55									105								8	
82		S.R. 725	791+18	791+32.5									16								3	
82		S.R. 725	788+96													1					3	
82		S.R. 725	789+84													1					3	
82		S.R. 725	790+58													1					3	
82		S.R. 725	790+72													1					3	
82		S.R. 725	791+25													1					3	
82,83		S.R. 725	792+90	796+24		334																
82-84		S.R. 725	792+90	803+66				1076													14	
82		S.R. 725	794+75	797+33									258								6	
82,83		S.R. 725	795+25	796+74									149								3	
82		S.R. 725	795+06													1						
82		S.R. 725	795+56													1						
82		S.R. 725	795+94													1						
83		S.R. 725	796+44													1						
83		S.R. 725	796+30	796+70									40								3	
83,84		S.R. 725	797+84	803+66		582															15	
83,84		S.R. 725	797+59	803+75									616								15	
83,84		S.R. 725	801+85	803+66									181								5	
83		S.R. 725	798+17													1						
83		S.R. 725	799+05													1						
83		S.R. 725	799+93													1						
83		S.R. 725	800+81													1						
83		S.R. 725	801+69													1						
83		S.R. 725	802+57													1						
84		S.R. 725	803+45													1						
83		S.R. 725	802+48													1						
84		S.R. 725	803+36													1						
84		S.R. 725	803+66		EB																	
84		S.R. 725 @ MAD RIVER RD	803+59	0+81	INTERSEC	84										57						
84		S.R. 725 @ MAD RIVER RD	0+40		▲											43						
84		S.R. 725 @ MAD RIVER RD	0+40	0+81				41					41/41									
84		S.R. 725 @ MAD RIVER RD	0+81	804+88		64																
84		S.R. 725 @ MALL ENT. No 2	803+66	0+90		38																
84		S.R. 725 @ MALL ENT. No 2	0+69													38						
84		S.R. 725 @ MALL ENT. No 2	0+81													5						
84		S.R. 725 @ MALL ENT. No 2	0+69	0+90									14/21/21									
84		S.R. 725 @ MALL ENT. No 2	0+64	805+17	INTERSEC	62																
TOTALS						2,475.0		1,976.0	1,457.0				1,839.0			138.0	23.0		30.0		67.0	18.0
SUB-TOTALS						2,475.0		1,976.0	1,457.0				1,839.0			138.0	23.0				115.0	
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.47 MI		0.37 MI	0.28 MI				1839			138	23				115	

PAVEMENT MARKING SUBSUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621				
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)					
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH					
		FROM	TO																						
84		S.R. 725	805+17	810+00	EB	483																			
84,85		S.R. 725	804+88	819+60	EB			1472													18				
TII		S.R. 725	808+50	810+95	EB								245								6				
84		S.R. 725 @ MALL ENT. No 1	810+00	0+56.5	INTERSEC								48												
84		S.R. 725 @ MALL ENT. No 1	810+00	0+71	↑	42																			
84		S.R. 725 @ MALL ENT. No 1	0+56.5	0+71	↑		15																		
84		S.R. 725 @ MALL ENT. No 1	810+95	0+56	↑								26												
84		S.R. 725 @ MALL ENT. No 1	0+56		↑																				
84		S.R. 725 @ MALL ENT. No 1	0+56	0+71	↑		14																		
84		S.R. 725 @ MALL ENT. No 1	810+00	810+95	↓																				
84,85		S.R. 725 @ MALL ENT. No 1	0+62	815+49	INTERSEC	437																			
85,86		S.R. 725	816+83	819+60	EB	277																			
85,86		S.R. 725	817+75	819+60	EB								185								15				
86		S.R. 725	818+42		EB																4				
86		S.R. 725	819+30		EB																1				
84		S.R. 725	804+88		WB																				
84		S.R. 725	804+88	806+25	↑								137								42				
84		S.R. 725	804+88	806+50	↑				162												3				
84		S.R. 725	806+50	809+50	↑																2				
84		S.R. 725	807+00	809+50	↑								300								4				
84		S.R. 725	805+18																		250				
84		S.R. 725	806+06																		1				
84		S.R. 725	807+46																		1				
84		S.R. 725	807+54																		1				
84		S.R. 725	808+96																		1				
84		S.R. 725	809+04																		1				
84		S.R. 725	804+88	819+50		1462		1462													18				
84		S.R. 725	809+50																		66				
84,85		S.R. 725	809+50	812+38					288/288												234				
85		S.R. 725	812+38																		57				
85		S.R. 725	812+38	816+60																					
85		S.R. 725	813+34																		422/422				
85		S.R. 725	813+42																		1				
85		S.R. 725	815+46																		1				
85		S.R. 725	815+54																		1				
85		S.R. 725	816+60	819+60																	300				
85		S.R. 725	816+17	46.9' RT	WB																42				
86		S.R. 725 @ KINGSRIDGE DR	819+50	250+98	INTERSEC	58																			
86		S.R. 725 @ KINGSRIDGE DR	250+98		↑																16				
86		S.R. 725 @ KINGSRIDGE DR	251+09		↑																12				
86		S.R. 725 @ KINGSRIDGE DR	250+98	251+20	↑																				
86		S.R. 725 @ KINGSRIDGE DR	251+09	251+20	↑								23												
86		S.R. 725 @ KINGSRIDGE DR	251+20	820+75	↑	70																			
86		S.R. 725 @ KINGSRIDGE DR	819+60	249+64	↓	59																			
86		S.R. 725 @ KINGSRIDGE DR	249+64	249+81	↓																				
86		S.R. 725 @ KINGSRIDGE DR	249+64	249+81	INTERSEC								17/17												
TOTALS						2,888.0	29.0	2,934.0	1068.0	1,394.0			698.0		81.0	234.0	157.0	12.0	123.0	30.0	49.0	34.0			
SUB-TOTALS						2,917.0		2,934.0	2,462.0				698.0		315.0		157.0	12.0	123.0		113.0				
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.55 MI		0.56 MI		0.47 MI			698		315	157	12	123		113					

PAVEMENT MARKING SUBSUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621		
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)			
			FROM	TO		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH	
86		S.R. 725 @ KINGSRIDGE DR	249+81		INTERSEC																		
86		S.R. 725 @ KINGSRIDGE DR	249+81	830+69	INTERSEC	1028																	
86		S.R. 725	820+75		WB																		
86		S.R. 725	820+75	822+05	↑	130											2			15			
86		S.R. 725	822+46	824+39	↑	193																	
86,87		S.R. 725	824+96	830+94	↑	598																	
87		S.R. 725	830+94	831+01	↑	10			10														
87		S.R. 725	831+01	28' LT	↑																		
87		S.R. 725	831+42	831+69	↑	35																	
87		S.R. 725	831+69	833+42	↑	245														15			
86,87		S.R. 725	820+75	830+59	↑			984														12	
86		S.R. 725	820+75	822+30	↑																	4	
86		S.R. 725	820+75	822+50	↑				175													3	
86,87		S.R. 725	822+50	828+80	↑					630												8	
86,87		S.R. 725	823+50	829+40	↑					590												8	
86		S.R. 725	823+84		↑																	1	
86		S.R. 725	823+92		↑																	1	
87		S.R. 725	828+46		↑																	1	
87		S.R. 725	828+54		↓																	1	
87		S.R. 725	829+40	830+59	WB				119													3	
86,87		S.R. 725	820+75	830+69	EB	994		994												15		12	
87		S.R. 725	829+60	830+69	EB					109												3	
87		S.R. 725	829+68		EB																	1	
87		S.R. 725	830+29		EB																	1	
87		S.R. 725	830+59		EB																	12	
87		S.R. 725	830+69		EB																	24	
87		S.R. 725 @ SOUTHTOWNE	830+69	0+64	INTERSEC	50																	
87		S.R. 725 @ SOUTHTOWNE	0+46	0+64	INTERSEC				18				18										
87		S.R. 725 @ SOUTHTOWNE	0+46		INTERSEC																	34	
87		S.R. 725 @ SOUTHTOWNE	0+64	831+77	INTERSEC	48																	
87,88		S.R. 725	831+77	838+05	EB	550		550	587											15		7	
88		S.R. 725	835+99		EB																	1	
88		S.R. 725	836+87		EB																	1	
88		S.R. 725	837+75		EB																	1	
88		S.R. 725	835+70	838+05	EB																	6	
88		S.R. 725	838+05		EB																	12	
88		S.R. 725	838+23		EB																	27	
87,88		S.R. 725	833+45	836+04	WB	261																	
87		S.R. 725	831+69		WB																	36	
87,88		S.R. 725	831+69	834+30	WB								261									7	
87,88		S.R. 725	831+69	838+05	WB			1639														20	
87		S.R. 725	831+99		WB																	1	
87		S.R. 725	832+87		WB																	1	
87		S.R. 725	833+75		WB																	1	
TOTALS						4,142.0		4,167.0	909.0	1,220.0							2475.0	14.0		60.0		74.0	30.0
SUB-TOTALS						4,142.0			2,129.0				788.0					247.0	14.0				164.0
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.78 Mi		0.79 Mi	0.40 Mi				788				247	14				164	

PAVEMENT MARKING SUBSUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	MARKING																			
			FROM	TO		644	644	644	644	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH	
88		S.R. 725 @ GRACELAND ST	836+04	0+80	INTERSEC	82																			
88		S.R. 725 @ GRACELAND ST	0+38		INTERSEC																				
88		S.R. 725 @ GRACELAND ST	0+38	0+80	INTERSEC																				
88		S.R. 725 @ GRACELAND ST	0+80	837+16	INTERSEC	78																			
88		S.R. 725	837+16	838+40	WB	124																			
88		S.R. 725 @ HOSPITAL DR	838+40	0+36	INTERSEC	36																			
88		S.R. 725 @ HOSPITAL DR	0+36		INTERSEC																				
88		S.R. 725 @ HOSPITAL DR	0+40	0+80	INTERSEC																				
88		S.R. 725 @ HOSPITAL DR	0+80	839+64	INTERSEC	79																			
88		S.R. 725	839+64		WB																				
88,89		S.R. 725	839+64	845+00	WB	536																			
88,89		S.R. 725	839+64	841+50	WB																				
88		S.R. 725	839+94		WB																				
88		S.R. 725	840+82		WB																				
89		S.R. 725 @ LEONA Ln	845+00	6+00	INTERSEC	115																			
89		S.R. 725 @ LEONA Ln	5+24		INTERSEC																				
89		S.R. 725 @ LEONA Ln	5+53		INTERSEC																				
89		S.R. 725 @ LEONA Ln	5+24	6+00	INTERSEC																				
89		S.R. 725 @ LEONA Ln	5+53	6+00	INTERSEC																				
89		S.R. 725 @ LEONA Ln	6+00	846+73	INTERSEC	90																			
89		S.R. 725	846+73		WB																				
89		S.R. 725	846+73	849+62																					
89		S.R. 725	847+03																						
89		S.R. 725	847+91																						
89		S.R. 725	848+79																						
89		S.R. 725	846+73	852+00																					
89,90		S.R. 725	846+73	860+99																					
89-91		S.R. 725	846+73	859+87.7		1315																			
89-91		S.R. 725	849+62	852+00																					
90		S.R. 725	852+00																						
90		S.R. 725	852+00	853+32																					
90		S.R. 725	853+32																						
90		S.R. 725	854+00																						
90,91		S.R. 725	854+00	860+99																					
90		S.R. 725	854+00	857+72	WB																				
88		S.R. 725 @ SOUTHWIND DR	838+05	1+00	INTERSEC	85																			
88		S.R. 725 @ SOUTHWIND DR	0+50		INTERSEC																				
88		S.R. 725 @ SOUTHWIND DR	0+50	1+00	INTERSEC																				
88		S.R. 725 @ SOUTHWIND DR	1+00	839+30	INTERSEC	76																			
88,89		S.R. 725	839+30	844+67	EB	537																			
88,89		S.R. 725	839+64	845+00	EB																				
89		S.R. 725	842+50	845+00	EB																				
89		S.R. 725	845+00		EB																				
					TOTALS	3,153.0																			
					SUB-TOTALS	3,153.0																			
					TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY	0.60 Mi																			

CALCULATED	REB	CHECKED
PAVEMENT MARKING SUBSUMMARY		
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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621		
			4"	4"		4"	4"	4"	4"	8"	12"	24"	24"	24"	24"	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)			
			WHITE EDGE LINE	YELLOW EDGE LINE		WHITE LANE LINE	CENTER LINE SOLID DOUBLE YELLOW	CENTER LINE YELLOW SOLID/DASHED	WHITE DOTTED LINE	WHITE CHANNELIZING LINE	CROSSWALK LINE	WHITE TRANSVERSE/DIAGONAL LINE	YELLOW TRANSVERSE/DIAGONAL LINE	STOP LINE	LANE ARROW	SQ. FT.	EACH	EACH	EACH	EACH			
FROM	TO	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH		
89		S.R. 725 @ RAMP "N"	844+67	125+20	INTERSEC	72										24							
89		S.R. 725 @ RAMP "N"	125+20	845+61	INTERSEC		67																
89,90		S.R. 725	845+61	851+08.76	EB	548																	
89,90		S.R. 725	846+73	861+99	EB			1526												19			
90		S.R. 725 @ RAMP "M"	117+23.55 M	128+00 M	INTERSEC	1077																	
90		S.R. 725	851+99	852+45	INTERSEC						46/51												
90		S.R. 725 @ RAMP "M"	118+57.45 M	128+00 M	INTERSEC		943																
90		S.R. 725	852+45	853+72	INTERSEC	127																	
90		S.R. 725 @ RAMP "M" & "L"	853+72	132+10 L	INTERSEC		67																
90		S.R. 725 @ RAMP "M" & "L"	132+70 L		INTERSEC										73								
90		S.R. 725 @ RAMP "M" & "L"	132+70 L	132+10 L	INTERSEC						60												
90		S.R. 725 @ RAMP "M" & "L"	132+42 L	132+70 L	INTERSEC								17										
90		S.R. 725 @ RAMP "M" & "L"	132+10 L	854+74.5	INTERSEC	117																	
90		S.R. 725 @ RAMP "M" & "L"	132+40 L		INTERSEC										2								
90,91		S.R. 725	854+74.5	861+05	EB	631												15					
91		S.R. 725	858+50	860+99	EB						249									6			
91		S.R. 725	858+50	861+50	EB						300									8			
91		S.R. 725 @ YANKEE ST	860+87.7	40+03	INTERSEC	104																	
91		S.R. 725 @ YANKEE ST	40+03		↑										48								
91		S.R. 725 @ YANKEE ST	40+03	40+15		12			12		12/12/12												
91		S.R. 725 @ YANKEE ST	40+15	862+63		84																	
91		S.R. 725 @ YANKEE ST	860+99	38+61							55												
91		S.R. 725 @ YANKEE ST	38+61	38+85							24												
91		S.R. 725	860+99	861+50									31										
91		S.R. 725 @ YANKEE ST	861+05	38+20		80																	
91		S.R. 725 @ YANKEE ST	38+20	38+34		14		12	14		14/14/14												
91		S.R. 725 @ YANKEE ST	38+34		↓										48								
91		S.R. 725 @ YANKEE ST	38+34	862+75	INTERSEC	68																	
91,92		S.R. 725	862+75	868+61	EB	586																	
91,92		S.R. 725	862+63	868+48	↑			585												7			
91		S.R. 725	862+63	864+87					224												3		
91,92		S.R. 725	864+87	868+48						361											5		
91		S.R. 725	865+83																		1		
91		S.R. 725	865+91																		1		
92		S.R. 725	867+44																		1		
92		S.R. 725	867+52		EB																1		
92		S.R. 725 @ WASHINGTON VGE	868+61	49+10		70																	
92		S.R. 725 @ WASHINGTON VGE	49+10	49+49		39			39		39												
92		S.R. 725 @ WASHINGTON VGE	49+49												33								
92		S.R. 725 @ WASHINGTON VGE	49+49	869+72		31																	
92		S.R. 725 @ WASHINGTON VGE	49+39												2								
92		S.R. 725 @ WASHINGTON VGE	868+18.7	50+73		72																	
92		S.R. 725 @ WASHINGTON VGE	50+73	50+90		17				17													
92		S.R. 725 @ WASHINGTON VGE	50+73												35								
92		S.R. 725 @ WASHINGTON VGE	50+90	869+97		82																	
TOTALS						3,831.0	1,077.0	2,123.0	289.0	378.0		919.0		48.0		261.0	8.0		15.0		40.0	8.0	
SUB-TOTALS						4,908.0		2,123.0		667.0				48.0		261.0	8.0				63.0		
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.93 Mi	0.40 Mi	0.13 Mi				919		48		261	8				63		

CALCULATED
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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621		
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)		
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH
91		S.R. 725	862+63		WB										52							
91		S.R. 725	862+63	864+77	↑								214						5			
91		S.R. 725	862+63	864+30									167						4			
91		S.R. 725	862+93																			
91		S.R. 725	863+81												2							
91		S.R. 725	864+69												2							
91,92		S.R. 725	864+87	868+48											1							
91,92		S.R. 725	862+63	868+48				585											7	5		
91,92		S.R. 725	864+77	868+48				371											5			
91,92		S.R. 725	862+63	868+18.7		556											15					
92,93		S.R. 725	869+97	878+49		852																
92,93		S.R. 725	869+58	878+68															23			
92		S.R. 725	869+58	871+30									172						4			
92		S.R. 725	869+88																			
92		S.R. 725	870+76																			
92,93		S.R. 725	869+58	874+33				476														
93		S.R. 725	874+33																			
93		S.R. 725	875+01														77					
93,94		S.R. 725	875+01	876+30																3		
93		S.R. 725	875+31																			
93		S.R. 725	876+19																			
93		S.R. 725	875+01	878+68	↓																	
93		S.R. 725	878+68		WB			638														
92,93		S.R. 725	869+72	878+68	EB				896											12		
92,93		S.R. 725	869+72	878+49	↑	877											15					
92,93		S.R. 725	869+72	874+33				461														
93		S.R. 725	875+01	878+68				367														
93		S.R. 725	877+20	878+68									148							4		
93		S.R. 725	877+50		↓																	
93		S.R. 725	878+38																			
93		S.R. 725	878+68		EB										42							
93		S.R. 725 @ LYONS RD	878+72		INTERSEC								91									
93		S.R. 725 @ LYONS RD	878+78		↑								87									
93		S.R. 725 @ LYONS RD	879+68										85									
93		S.R. 725 @ LYONS RD	879+74										80									
93		S.R. 725 @ LYONS RD	49+50										83									
93		S.R. 725 @ LYONS RD	49+56										90									
93		S.R. 725 @ LYONS RD	50+42										90									
93		S.R. 725 @ LYONS RD	50+48										95									
93		S.R. 725 @ LYONS RD	878+49	50+90		64																
93		S.R. 725 @ LYONS RD	50+53												48							
93		S.R. 725 @ LYONS RD	50+53	50+90				37/37					37/37									
93		S.R. 725 @ LYONS RD	50+90	879+78		61																
93		S.R. 725 @ LYONS RD	879+78												40							
93		S.R. 725 @ LYONS RD	878+49	49+18		71																
93		S.R. 725 @ LYONS RD	49+18	49+46		28		28/28					28									
93		S.R. 725 @ LYONS RD	49+46												40							
93		S.R. 725 @ LYONS RD	49+46	879+98	INTERSEC	47																
TOTALS						2,556.0	2,072.0	3,765.0		361.0			932.0	701.0		222.0	11.0	124.0	30.0		71.0	5.0
SUB-TOTALS						4,628.0		3,765.0		361.0			932.0	701.0		222.0	11.0	124.0			106.0	
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.88 MI	0.71 MI	0.07 MI				932	701		222	11	124			106		

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621				
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)					
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH			
		FROM	TO																						
93-95		S.R. 725	879+78	892+47	WB	1269		1269								24			15						
93		S.R. 725	879+78	882+00	↑								222								5				
93		S.R. 725	880+08																						
93		S.R. 725	880+96																						
93		S.R. 725	881+84																						
93,94		S.R. 725	879+78	886+15			642																		
94		S.R. 725	886+18.5															11							
94		S.R. 725	886+88.5	887+55								67									3				
94		S.R. 725	887+18.5													1									
94		S.R. 725	886+88.5	888+43			157																		
94		S.R. 725	886+88.5															11							
94		S.R. 725	888+43														77								
94,95		S.R. 725	889+09.5	890+38								129									3				
94,95		S.R. 725	889+09.5	892+82.5			374																		
94		S.R. 725	889+39.5		↓																				
95		S.R. 725	890+27.5		WB																				
93-95		S.R. 725	879+98	892+82.5	EB	1285		1285											15		16				
93,94		S.R. 725	879+78	886+18.5	↑		641																		
94		S.R. 725	884+90									129									3				
94		S.R. 725	885+00.5													1									
94		S.R. 725	885+88.5													1									
94		S.R. 725	886+88.5	888+43			155																		
94,95		S.R. 725	889+09.5	892+82.5			373																		
95		S.R. 725	891+28	892+82.5								155									4				
95		S.R. 725	891+64.5																						
95		S.R. 725	892+52.5		↓																				
95		S.R. 725	892+82.5		EB											36		11/11							
95		S.R. 725 @ McEWEN RD	892+91		INTERSEC									84											
95		S.R. 725 @ McEWEN RD	892+97		↑									86											
95		S.R. 725 @ McEWEN RD	893+86											92											
95		S.R. 725 @ McEWEN RD	893+92											95											
95		S.R. 725 @ McEWEN RD	49+55											83											
95		S.R. 725 @ McEWEN RD	49+61											84											
95		S.R. 725 @ McEWEN RD	50+45											93											
95		S.R. 725 @ McEWEN RD	50+51											91											
95		S.R. 725 @ McEWEN RD	892+82.5	49+18		69																			
95		S.R. 725 @ McEWEN RD	49+18	49+40				19	19			19/19													
95		S.R. 725 @ McEWEN RD	49+40													47									
95		S.R. 725 @ McEWEN RD	49+18	894+30		58																			
95		S.R. 725 @ McEWEN RD	892+47	50+93		67																			
95		S.R. 725 @ McEWEN RD	50+59													52									
95		S.R. 725 @ McEWEN RD	50+59	50+93				34/34	34			34													
95		S.R. 725 @ McEWEN RD	50+85.5		↓																				
95		S.R. 725 @ McEWEN RD	50+93	893+97	INTERSEC	70										1									
TOTALS						2,818.0	2,342.0	2,641.0	53.0			774.0	708.0			159.0	11.0	121.0	30.0		34.0				
SUB-TOTALS						5,160.0	2,641.0		53.0			774.0	708.0			159.0	11.0	121.0			64.0				
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.98 Mi	0.50 Mi	0.01 Mi				774	708			159	11	121			64				

CALCULATED
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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)	
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH	
95,96		S.R. 725	893+97	913+60	WB	1963		1963									75		25		
95		S.R. 725	893+97		↑									48							
95		S.R. 725	893+97	895+80							183/183								10		
95		S.R. 725	894+27												2						
95		S.R. 725	895+15												2						
95		S.R. 725	893+97	898+00			403									11/11					
96		S.R. 725	898+38	901+58.5			321										79/11				
96		S.R. 725	902+46.5	904+95			249										11/77				
96		S.R. 725	902+76.5											1							
96		S.R. 725	903+64.5											1							
96		S.R. 725	902+46.5	903+75							129								3		
96,97		S.R. 725	905+61.5	906+90							129								3		
96,97		S.R. 725	905+61.5	908+98			337										11/77				
96		S.R. 725	905+91.5											1							
97		S.R. 725	906+79.5											1							
97		S.R. 725	909+73.5	911+02							129								3		
97		S.R. 725	910+03.5											1							
97		S.R. 725	910+91.5											1							
97,98		S.R. 725	909+73.5	913+86	WB		413										11/11				
95,96		S.R. 725	894+30	901+39	EB	709													25		
95,96		S.R. 725	894+30	913+86	↑			1956													
95		S.R. 725	896+82												1						
95		S.R. 725	897+70												1						
95		S.R. 725	896+72	898+00							129								3		
95		S.R. 725	893+97	898+00			403														
96		S.R. 725	898+38	901+58.5			321														
96		S.R. 725	900+30	901+58.5							129								3		
96		S.R. 725	900+40.5											1							
96		S.R. 725	901+28.5		EB									1							
96		S.R. 725 @ GARNET RD	901+39	901+58.5	INTERSEC	22					51										
96		S.R. 725 @ GARNET RD	49+56		INTERSEC						67										
96		S.R. 725 @ GARNET RD	49+62		INTERSEC										14						
96		S.R. 725 @ GARNET RD	49+52		INTERSEC																
96		S.R. 725 @ GARNET RD	49+52	902+48	INTERSEC	27															
96-98		S.R. 725	902+48	913+60	EB	1112												15			
96		S.R. 725	902+46.5	904+95	EB		249														
96,97		S.R. 725	905+61.5	908+98	EB		337														
97		S.R. 725	909+73.5	913+86	EB		413														
97,98		S.R. 725	911+92	913+86	EB						194								5		
97		S.R. 725	912+69		EB									1							
98		S.R. 725	913+57		EB									1							
98		S.R. 725	913+86		EB									42							
TOTALS						3,833.0	3,446.0	3,919.0			1,205.0	118.0			104.0	16.0	310.0	30.0		80.0	
SUB-TOTALS						7,279.0		3,919.0			1,205.0	118.0			104.0	16.0	310.0			110.0	
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						1.38 Mi	0.74 Mi				1205	118			104	16	310			110	

PAVEMENT MARKING SUBSUMMARY

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621	
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)		
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	EACH	EACH	EACH			
FROM	TO																					
98		S.R. 725 @ PARAGON RD	49+18	49+46	INTERSEC		28/28	28/28				28										
98		S.R. 725 @ PARAGON RD	49+46		↑										38		1					
98		S.R. 725 @ PARAGON RD	49+551		↑								83									
98		S.R. 725 @ PARAGON RD	49+61		↑								97									
98		S.R. 725 @ PARAGON RD	49+46	915+02	↑	36																
98		S.R. 725 @ PARAGON RD	913+60	50+54	↑	35																
98		S.R. 725 @ PARAGON RD	50+39.5		↑																	
98		S.R. 725 @ PARAGON RD	50+45.5		↑																	
98		S.R. 725 @ PARAGON RD	50+54		↑																	
98		S.R. 725 @ PARAGON RD	50+54	50+82	↓		28/28	28/28							38		1					
98		S.R. 725 @ PARAGON RD	50+82	915+01	INTERSEC	64						28										
98		S.R. 725	914+74		WB										42		1					
98,99		S.R. 725	915+01	926+30	↑	1129											15					
98,99		S.R. 725	915+01	926+25.5	↑			1125												14		
98		S.R. 725	915+01	917+06	↑							205								5		
98		S.R. 725	915+04		↑										1							
98		S.R. 725	915+92		↑										1							
98		S.R. 725	916+80		↑										1							
98		S.R. 725	914+74	919+24.5	↑			451									1					
98,99		S.R. 725	919+89	921+82.5	↑			194									2					
99		S.R. 725	922+51.5	924+00	↑							149								3		
99		S.R. 725	922+81.5		↑										1							
99		S.R. 725	923+69.5		↓										1							
99		S.R. 725	922+51.5		WB			374									2					
99		S.R. 725 @ CONGRESS PARK	926+38		INTERSEC								70									
99		S.R. 725 @ CONGRESS PARK	926+44		↑								73									
99		S.R. 725 @ CONGRESS PARK	926+30	50+80	↑	58																
99		S.R. 725 @ CONGRESS PARK	50+39		↑																	
99		S.R. 725 @ CONGRESS PARK	50+45		↑																	
99		S.R. 725 @ CONGRESS PARK	50+54		↓																	
99		S.R. 725 @ CONGRESS PARK	50+54	50+80	↓		26/26	26				26			50		1					
99		S.R. 725 @ CONGRESS PARK	50+80	927+72	INTERSEC	58																
99		S.R. 725	927+36		WB										41		1					
99-102		S.R. 725	927+72	949+27	↑	2155		2155												27		
99		S.R. 725	927+36.5	928+53	↑							117								3		
99		S.R. 725	927+66.5		↑												1					
99		S.R. 725	928+45		↑												1					
99,100		S.R. 725	927+36.5	930+61.5	↑			325									2					
100		S.R. 725	931+31.5	932+60	↑							129								3		
100		S.R. 725	931+61.5		↑												1					
100		S.R. 725	932+49.5		↑												1					
100		S.R. 725	931+31.5	934+88.5	↑			357									2					
100		S.R. 725	935+71.5	937+00	↑							129								3		
100		S.R. 725	936+01.5		↑												1					
100		S.R. 725	936+89.5		↓												1					
100,101		S.R. 725	935+71.5	939+31.5	WB			360									2					
TOTALS						3,535.0	2,225.0	3,418.0				811.0	681.0		209.0	11.0	314.0	15.0		58.0		
SUB-TOTALS						5,760.0		3,418.0				811.0	681.0		209.0	11.0	314.0			73.0		
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						1.10 Mi	0.65 Mi					811	681		209	11	314			73		

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621		
			4" WHITE EDGE LINE	4" YELLOW EDGE LINE		4" WHITE LANE LINE	4" CENTER LINE SOLID DOUBLE YELLOW	4" CENTER LINE YELLOW SOLID/DASHED	4" WHITE DOTTED LINE	8" WHITE CHANNELIZING LINE	12" CROSSWALK LINE	24" WHITE TRANSVERSE/DIAGONAL LINE	24" YELLOW TRANSVERSE/DIAGONAL LINE	24" STOP LINE	LANE ARROW	ISLAND MARKING	RPM ONE WAY (WHITE)	RPM TWO WAY (YELLOW/RED)	RPM TWO WAY (WHITE/RED)	RPM TWO WAY (YELLOW/YELLOW)			
			FOOT	FOOT		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. YD.	EACH	EACH	EACH	EACH
101		S.R. 725	940+14.5	941+43	WB							129									3		
101		S.R. 725	940+44.5		WB																		
101		S.R. 725	941+32.5		WB																		
101		S.R. 725	940+14.5	944+20.5	WB		406										11/11						
101		S.R. 725 @ PRIVATE DR	0+46		INTERSEC											33							
101		S.R. 725 @ PRIVATE DR	0+51		INTERSEC																		
101		S.R. 725 @ PRIVATE DR	0+46	0+81.5	INTERSEC				36			36											
101A		S.R. 725	945+13.5	947+02	WB							194										5	
101A		S.R. 725	945+43.5		WB																		
101A		S.R. 725	945+13.5	949+27	WB		414										11/11						
98-101		S.R. 725	915+02	944+08	EB	2906												15					
98-99		S.R. 725	914+90	926+25.5	↑			1136														14	
98		S.R. 725	914+74	919+24.5			451																
98		S.R. 725	917+69	919+24.5								156										4	
98		S.R. 725	918+06.5																				
98		S.R. 725	918+94.5																				
98,97		S.R. 725	919+889	921+82.5			194																
98,97		S.R. 725	921+00	921+82.5								83										3	
99		S.R. 725	921+52.5																				
99		S.R. 725	922+51.5	926+25.5																			
99		S.R. 725	925+07.5																				
99		S.R. 725	925+95.5																				
99		S.R. 725	926+25.5													36							
99,100		S.R. 725	927+36.5	930+61.5			325																
100		S.R. 725	929+33	930+61.5								129										3	
100		S.R. 725	929+43.5																				
100		S.R. 725	930+31.5																				
99-101		S.R. 725	927+36.5	944+20.5				1684														21	
100		S.R. 725	931+31.5	934+88.5			357																
100		S.R. 725	933+70.5																				
100		S.R. 725	934+58.5																				
100		S.R. 725	933+50	934+88.5								139										3	
100,101		S.R. 725	935+71.5	939+31.5			360																
101		S.R. 725	938+13.5																				
101		S.R. 725	939+01.5																				
101		S.R. 725	937+90	939+31.5																			
101		S.R. 725	940+14.5	944+20.5			406					142										3	
101		S.R. 725	943+02.5																				
101		S.R. 725	943+90.5																				
101		S.R. 725	942+33	944+20.5								188										5	
101		S.R. 725	944+20.5		EB											41							
TOTALS						2,906.0	3,287.0	2,820.0	36.0			1,196.0				110.0	19.0	44.0	15.0		63.0		
SUB-TOTALS						6,193.0	2,820.0		36.0			1,196.0				110.0	19.0	44.0			78.0		
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						1.17 Mi	0.53 Mi	0.01 Mi				1196			110	19	44			78			

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644	644	644	644	644	644	644	644	644	644	644	644	621	621	621	621	
			4"	4"		4"	4"	4"	4"	8"	12"	24"	24"	24"	LANE	ISLAND	RPM	RPM	RPM	RPM		
			WHITE EDGE LINE	YELLOW EDGE LINE		WHITE LANE LINE	CENTER LINE SOLID DOUBLE YELLOW	CENTER LINE YELLOW SOLID/DASHED	WHITE DOTTED LINE	WHITE CHANNELIZING LINE	CROSSWALK LINE	WHITE TRANSVERSE/DIAGONAL LINE	YELLOW TRANSVERSE/DIAGONAL LINE	STOP LINE	ARROW	MARKING	ONE WAY (WHITE)	TWO WAY (YELLOW/RED)	TWO WAY (WHITE/RED)	TWO WAY (YELLOW/YELLOW)		
FROM	TO		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	SQ. FT.	EACH	EACH	EACH	EACH		
101		S.R. 725 @ MILLER FARM RD	943+92	49+19	INTERSEC	58																
101		S.R. 725 @ MILLER FARM RD	49+19	49+52	INTERSEC				33			33										
101		S.R. 725 @ MILLER FARM RD	49+52		INTERSEC										36							
101		S.R. 725 @ MILLER FARM RD	49+56		INTERSEC							72										
101		S.R. 725 @ MILLER FARM RD	49+62		INTERSEC							86										
101,101A		S.R. 725	944+94	949+27	EB	433																
101,101A		S.R. 725	945+13.5	949+27	EB		414	414													5	
101A		S.R. 725	947+97	949+27	EB						130										3	
101A		S.R. 725	948+09		EB											1						
101A		S.R. 725	948+97		EB											1						
TOTALS						491.0	414.0	414.0	33.0			163.0	158.0		36.0	2.0					8.0	
SUB-TOTALS						905.0		414.0	33.0			163.0	158.0		36.0	2.0					8.0	
TOTALS CARRIED TO TRAFFIC CONTROL GENERAL SUMMARY						0.17 Mi	0.08 Mi	0.01 Mi				163	158		36	2				8		

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SHEET NO.	REFERENCE NO.	STATION	LOOP SIZE * ANGULAR DESIGN	632	632
				LOOP DETECTOR TIE-IN	DETECTOR LOOP, AS PER PLAN
				FT.	EACH
ALEXANDERVILLE RD. INTERSECTION					
74	L-1	730+32	* 18 X 9	1	1
74	L-2	730+77	6 X 6	1	1
74	L-3	730+79	6 X 6	1	1
74	L-4	731+96	6 X 6	1	1
74	L-5	731+98	6 X 6	1	1
74	L-6	733+04	6 X 6	1	1
74	L-7	733+06	6 X 6	1	1
IMPERIAL RD. INTERSECTION					
76	L-8	10+58 IMP. RD.	* 18 X 9	1	1
76	L-9	10+69 IMP. RD.	* 18 X 9	1	1
76	L-10	750+63	6 X 6	1	1
76	L-11	750+65	6 X 6	1	1
BYERS RD. INTERSECTION					
77	L-12	0+88 BYERS	* 18 X 9	1	1
77	L-13	0+80 BYERS	* 18 X 9	1	1
77	L-14	0+81 BYERS	* 18 X 9	1	1
77	L-15	753+32	26 X 6	1	1
RAMPS A&B INTERSECTION					
77	L-16	754+22	6 X 6	1	1
77	L-17	754+24	6 X 6	1	1
77	L-18	755+30	6 X 6	1	1
77	L-19	755+32	6 X 6	1	1
77	L-20	0+26 RAMP B	* 18 X 9	1	1
77	L-21	0+29 RAMP B	* 18 X 9	1	1
77	L-22	0+37 RAMP B	* 18 X 9	1	1
77	L-23	0+43 RAMP B	* 18 X 9	1	1
77	L-24	758+76	* 18 X 9	1	1
78	L-25	760+18	6 X 6	1	1
78	L-26	760+20	6 X 6	1	1
78	L-27	760+22	6 X 6	1	1
78	L-28	761+26	6 X 6	1	1
78	L-29	761+28	6 X 6	1	1
78	L-30	761+30	6 X 6	1	1
78	L-31	761+37	6 X 6	1	1
78	L-32	761+39	6 X 6	1	1
78	L-33	761+41	6 X 6	1	1
TOTALS CARRIED TO NEXT TABLE				33	33

SHEET NO.	REFERENCE NO.	STATION	LOOP SIZE * ANGULAR DESIGN	632	632
				LOOP DETECTOR TIE-IN	DETECTOR LOOP, AS PER PLAN
				FT.	EACH
RAMPS C&D INTERSECTION					
78	L-34	763+31	6 X 6	1	1
78	L-35	763+33	6 X 6	1	1
78	L-36	763+48	6 X 6	1	1
78	L-37	763+50	6 X 6	1	1
78	L-38	764+52	6 X 6	1	1
78	L-39	764+54	6 X 6	1	1
78	L-40	766+58	* 18 X 9	1	1
79	L-41	766+50	* 18 X 9	1	1
79	L-42	14+57 Ramp C	25 X 6	1	1
79	L-43	14+66 Ramp C	* 18 X 9	1	1
79	L-44	14+68 Ramp C	* 18 X 9	1	1
79	L-45	769+82	6 X 6	1	1
79	L-46	769+84	6 X 6	1	1
79	L-47	770+90	6 X 6	1	1
79	L-48	770+92	6 X 6	1	1
SR 725/SR 741 INTERSECTION					
101B	L-52	774+88	6 X 6	1	1
101B	L-53	774+88	6 X 6	1	1
101B	L-54	774+88	6 X 6	1	1
80	L-55	778+35	6 X 6	1	1
80	L-56	778+37	6 X 6	1	1
80	L-57	779+33	6 X 6	1	1
80	L-58	779+35	20 X 6	1	1
80	L-59	779+37	20 X 6	1	1
80	L-60	779+39	25 X 6	1	1
81	L-61	781+18	* 18 X 9	1	1
81	L-62	781+20	* 18 X 9	1	1
81	L-63	783+02.5	* 18 X 9	1	1
81	L-64	783+04.5	* 18 X 9	1	1
81	L-65	783+56.7	* 18 X 9	1	1
TOTALS PREVIOUS TABLE				33	33
TOTALS THIS TABLE				29	29
TOTALS CARRIED TO NEXT TABLE				62	62

SHEET NO.	REFERENCE NO.	STATION	LOOP SIZE * ANGULAR DESIGN	632	632
				LOOP DETECTOR TIE-IN	DETECTOR LOOP, AS PER PLAN
				FT.	EACH
SR 725/SR 741 INTERSECTION (CONTINUED)					
81	L-66	785+15	6 X 6	1	1
81	L-67	785+17	6 X 6	1	1
81	L-68	786+23	6 X 6	1	1
81	L-69	786+25	6 X 6	1	1
82	L-70	790+33	6 X 6	1	1
82	L-71	790+35	6 X 6	1	1
MALL PARK DR/MALL INTERANCE No.4 INTERSECTION					
82	L-72	0+34 MALL PARK	* 18 X 9	1	1
82	L-73	0+36 MALL PARK	* 18 X 9	1	1
82	L-74	0+50 M.E. No4	* 18 X 9	1	1
82	L-75	0+52 M.E. No4	* 18 X 9	1	1
LOIS CIRCLE/MALL INTERANCE No.3 INTERSECTION					
82	L-76	793+70	6 X 6	1	1
82	L-77	793+72	6 X 6	1	1
MAD RIVER RD/MALL INTERANCE No.2 INTERSECTION					
83	L-78	800+44	6 X 6	1	1
83	L-79	800+46	6 X 6	1	1
83	L-80	801+50	6 X 6	1	1
83	L-81	801+52	6 X 6	1	1
84	L-82	803+27	* 18 X 9	1	1
84	L-83	0+71 M.E. No2	* 18 X 9	1	1
84	L-84	0+71 M.E. No2	* 18 X 9	1	1
84	L-85	0+71 M.E. No2	* 18 X 9	1	1
84	L-86	0+43 M.R. RD.	* 18 X 9	1	1
84	L-87	0+45 M.R. RD.	* 18 X 9	1	1
84	L-88	804+93	* 18 X 9	1	1
84	L-89	804+95	* 18 X 9	1	1
84	L-90	805+48	* 18 X 9	1	1
84	L-91	806+85	6 X 6	1	1
84	L-92	806+87	6 X 6	1	1
84	L-93	807+94	6 X 6	1	1
84	L-94	807+96	6 X 6	1	1
TOTALS PREVIOUS TABLE				62	62
TOTALS THIS TABLE				29	29
TOTALS CARRIED TO TRAFFIC CONTROL SUMMARY				91	91

LOOP DETECTOR QUANTITY TABLE

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SHEET NO.	REFERENCE NO.	STATION	LOOP SIZE * ANGULAR DESIGN	632	632
				LOOP DETECTOR TIE-IN	DETECTOR LOOP, AS PER PLAN
				FT.	EACH
MAD RIVER RD/MALL INTERANCE No.2 INTERSECTION (CONTINUED)					
86	L-95	818+36	6 X 6	1	1
86	L-96	818+38	6 X 6	1	1
86	L-97	818+55	6 X 6	1	1
86	L-98	818+57	6 X 6	1	1
KINGSRIDGE DR. INTERSECTION					
86	L-99	819+37	* 18 X 9	1	1
86	L-100	249+77.5 K-RIDGE	* 18 X 9	1	1
86	L-101	249+78 K-RIDGE	* 18 X 9	1	1
86	L-102	249+78 K-RIDGE	* 18 X 9	1	1
86	L-103	251+00 K-RIDGE	* 18 X 9	1	1
86	L-104	251+12 K-RIDGE	* 18 X 9	1	1
86	L-105	820+79	* 18 X 9	1	1
86	L-106	822+27	6 X 6	1	1
86	L-107	822+29	6 X 6	1	1
86	L-108	822+46	6 X 6	1	1
86	L-109	822+48	6 X 6	1	1
SOUTHTOWNE CENTRE DR.					
87	L-110	830+36	* 18 X 9	1	1
87	L-111	0+52 S-TOWNE	* 18 X 9	1	1
87	L-112	0+52 S-TOWNE	* 18 X 9	1	1
87	L-113	831+09 - 26.5' LT	* 16 X 8	1	1
87	L-114	831+21 - 26' LT	* 16 X 8	1	1
87	L-115	832+13	* 18 X 9	1	1
SOUTHWIND DR./HOSPITAL DR. INTERSECTION					
88	L-116	837+57	6 X 6	1	1
88	L-117	837+59	6 X 6	1	1
88	L-118	837+79	* 18 X 9	1	1
88	L-119	839+75	* 18 X 9	1	1
88	L-120	0+44 HOSPITAL	* 18 X 9	1	1
88	L-120A	0+46 HOSPITAL	* 18 X 9	1	1
88	L-121	0+52.5 S-WIND	* 18 X 9	1	1
88	L-121A	0+53 S-WIND	* 18 X 9	1	1
88	L-122	840+77	6 X 6	1	1
88	L-123	840+79	6 X 6	1	1
TOTALS CARRIED TO NEXT TABLE				31	31

SHEET NO.	REFERENCE NO.	STATION	LOOP SIZE * ANGULAR DESIGN	632	632
				LOOP DETECTOR TIE-IN	DETECTOR LOOP, AS PER PLAN
				FT.	EACH
RAMP N/LEONA Ln INTERSECTION					
89	L-124	844+34.5	* 18 X 9	1	1
89	L-125	5+27 LEONA DR.	* 18 X 9	1	1
89	L-126	5+58 LEONA DR	* 18 X 9	1	1
89	L-127	846+94	* 18 X 9	1	1
RAMP L INTERSECTION					
90	L-128	132+48 RAMP L	* 18 X 9	1	1
90	L-129	132+50 RAMP L	* 18 X 9	1	1
YANKEE ST. INTERSECTION					
90	L-130	857+73	6 X 6	1	1
90	L-131	857+75	6 X 6	1	1
91	L-132	858.83	6 X 6	1	1
91	L-133	858+85	6 X 6	1	1
91	L-134	860+69	* 18 X 9	1	1
91	L-135	862+67	30 X 6	1	1
91	L-136	862+69	* 18 X 9	1	1
91	L-137	864+54	6 X 6	1	1
91	L-138	864+56	6 X 6	1	1
91	L-139	865+76	6 X 6	1	1
91	L-140	865+78	6 X 6	1	1
LYONS RD. INTERSECTION					
93	L-141	875+42	6 X 6	1	1
93	L-142	875+44	6 X 6	1	1
93	L-143	876+50	6 X 6	1	1
93	L-144	876+52	6 X 6	1	1
93	L-145	878+33	* 18 X 9	1	1
93	L-146	878+38	* 18 X 9	1	1
93	L-147	878+40	* 18 X 9	1	1
93	L-148	49+18 LYONS	* 18 X 9	1	1
93	L-149	49+20 LYONS	* 18 X 9	1	1
93	L-150	49+21 LYONS	* 18 X 9	1	1
93	L-151	50+57 LYONS	* 18 X 9	1	1
93	L-151A	50+56 LYONS	* 18 X 9	1	1
93	L-151B	50+56 LYONS	* 18 X 9	1	1
93	L-152	880+23	* 18 X 9	1	1
TOTALS PREVIOUS TABLE				31	31
TOTALS THIS TABLE				31	31
TOTALS CARRIED TO NEXT TABLE				62	62

SHEET NO.	REFERENCE NO.	STATION	LOOP SIZE * ANGULAR DESIGN	632	632
				LOOP DETECTOR TIE-IN	DETECTOR LOOP, AS PER PLAN
				FT.	EACH
LYONS RD. INTERSECTION (CONTINUED)					
93	L-153	881+85	6 X 6	1	1
93	L-154	881+87	6 X 6	1	1
94	L-155	882+83	6 X 6	1	1
94	L-156	882+85	6 X 6	1	1
94	L-157	883+00	6 X 6	1	1
94	L-158	883+02	6 X 6	1	1
MCEWEN RD. INTERSECTION					
94	L-159	889+45	6 X 6	1	1
94	L-160	889+47	6 X 6	1	1
94	L-161	889+62	6 X 6	1	1
94	L-162	889+64	6 X 6	1	1
95	L-163	890+85	6 X 6	1	1
95	L-164	890+87	6 X 6	1	1
95	L-165	892+09	* 18 X 9	1	1
95	L-166	49+10 McEWEN	* 18 X 9	1	1
95	L-166A	49+15 McEWEN	* 18 X 9	1	1
95	L-167	49+20 McEWEN	* 18 X 9	1	1
95	L-168	50+60 McEWEN	* 18 X 9	1	1
95	L-169	50+61 McEWEN	* 18 X 9	1	1
95	L-170	50+62 McEWEN	* 18 X 9	1	1
95	L-171	894+01	* 18 X 9	1	1
95	L-172	896+01	6 X 6	1	1
95	L-173	896+03	6 X 6	1	1
95	L-174	897+09	6 X 6	1	1
95	L-175	897+11	6 X 6	1	1
PARAGON RD. INTERSECTION					
97	L-176	910+61	6 X 6	1	1
97	L-177	910+63	6 X 6	1	1
97	L-178	911+65	6 X 6	1	1
97	L-179	911+67	6 X 6	1	1
98	L-180	913+57	* 18 X 9	1	1
98	L-181	49+18 PARAGON	* 18 X 9	1	1
98	L-182	49+22 PARAGON	* 18 X 9	1	1
98	L-182A	49+26 PARAGON	* 18 X 9	1	1
98	L-183	50+56.5 PARAGON	* 18 X 9	1	1
98	L-183A	50+58 PARAGON	* 18 X 9	1	1
TOTALS PREVIOUS TABLE				60	60
TOTALS THIS TABLE				34	34
TOTALS CARRIED TO TRAFFIC CONTROL SUMMARY				94	94

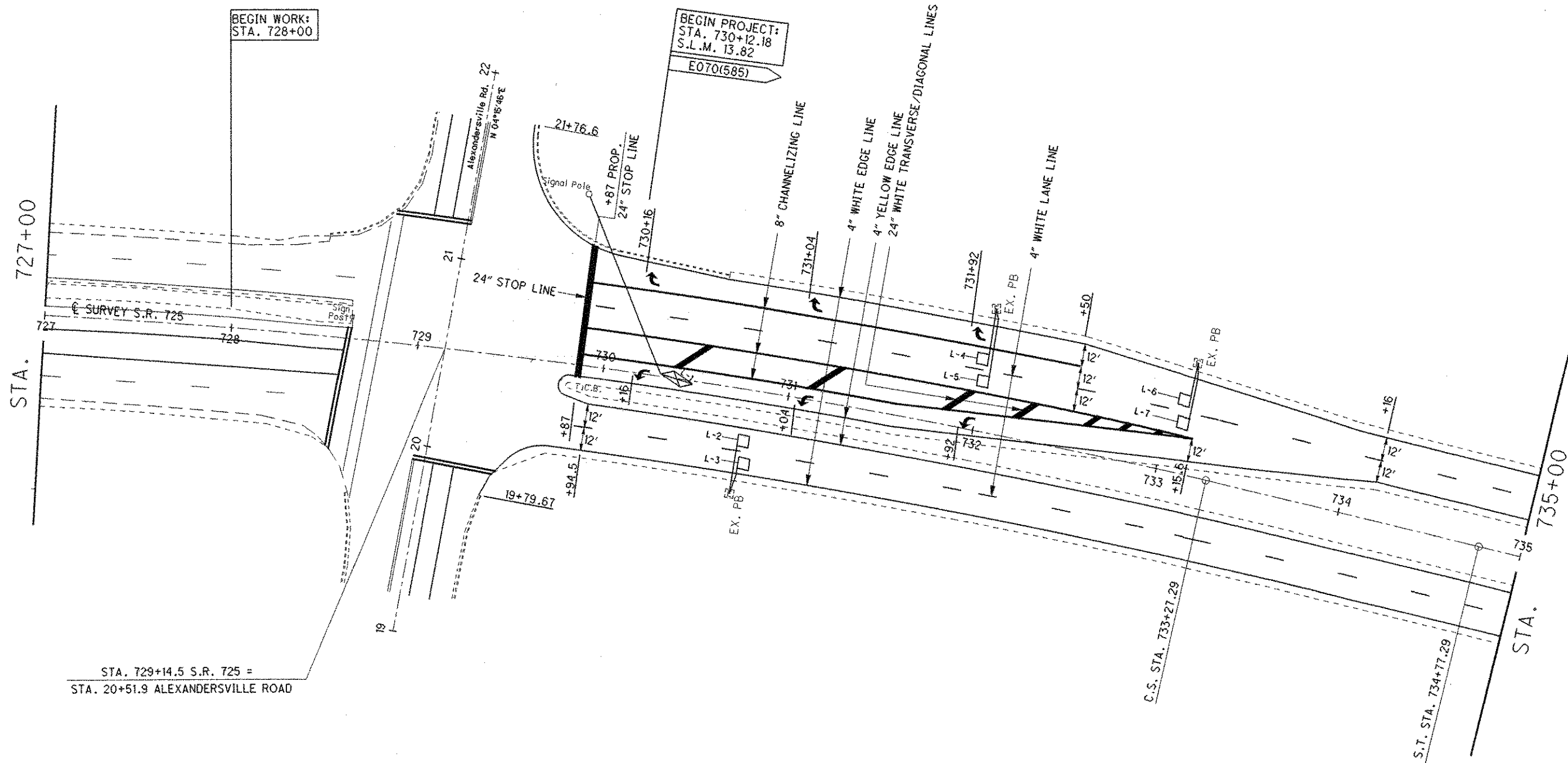
LOOP DETECTOR QUANTITY TABLE

CALCULATED
REB
CHECKED

MOT-725-13.82

E:\PROJECTS\MOT\725\1362\82505\roadway\sheets\Mot725ta.dgn 27-JUL-2009 9:05AM rburns2

SHEET NO.	REFERENCE NO.	STATION	LOOP SIZE * ANGULAR DESIGN	632	632
				LOOP DETECTOR TIE-IN	DETECTOR LOOP, AS PER PLAN
FT.				EACH	EACH
PARAGON RD. INTERSECTION (CONTINUED)					
98	L-184	50+57 PARAGON	* 18 X 9	1	1
98	L-185	915+46	* 18 X 9	1	1
98	L-186	916+82	6 X 6	1	1
98	L-187	916+84	6 X 6	1	1
98	L-188	917+91	6 X 6	1	1
98	L-189	917+93	6 X 6	1	1
CONGRESS PARK DR. INTERSECTION					
99	L-190	925+52	* 18 X 9	1	1
99	L-191	50+56 C-PARK	* 18 X 9	1	1
99	L-192	50+57 C-PARK	* 18 X 9	1	1
99	L-193	927+40	* 18 X 9	1	1
MILLER FARM RD. INTERSECTION					
101	L-194	943+40	* 18 X 9	1	1
101A	L-195	945+57	* 18 X 9	1	1
PRESTIGE PLAZA & MALL WOOD DR. INTERSECTIONS					
101B	L-49	772+88	30 X 6	1	1
101B	L-50	773+48	* 18 X 9	1	1
101B	L-51	0+61 MALL WOODS DR.	* 18 X 9	1	1
101B	L-196	774+80	* 18 X 9	1	1
101B	L-197	775+40	30 X 6	1	1
101B	L-198	0+53 PRESTIGE PLAZA	* 18 X 9	1	1
TOTALS CARRIED TO TRAFFIC CONTROL SUMMARY				18	18



BEGIN WORK:
STA. 728+00

BEGIN PROJECT:
STA. 730+12.18
S.L.M. 13.82
E070(585)

STA. 729+14.5 S.R. 725 =
STA. 20+51.9 ALEXANDERSVILLE ROAD



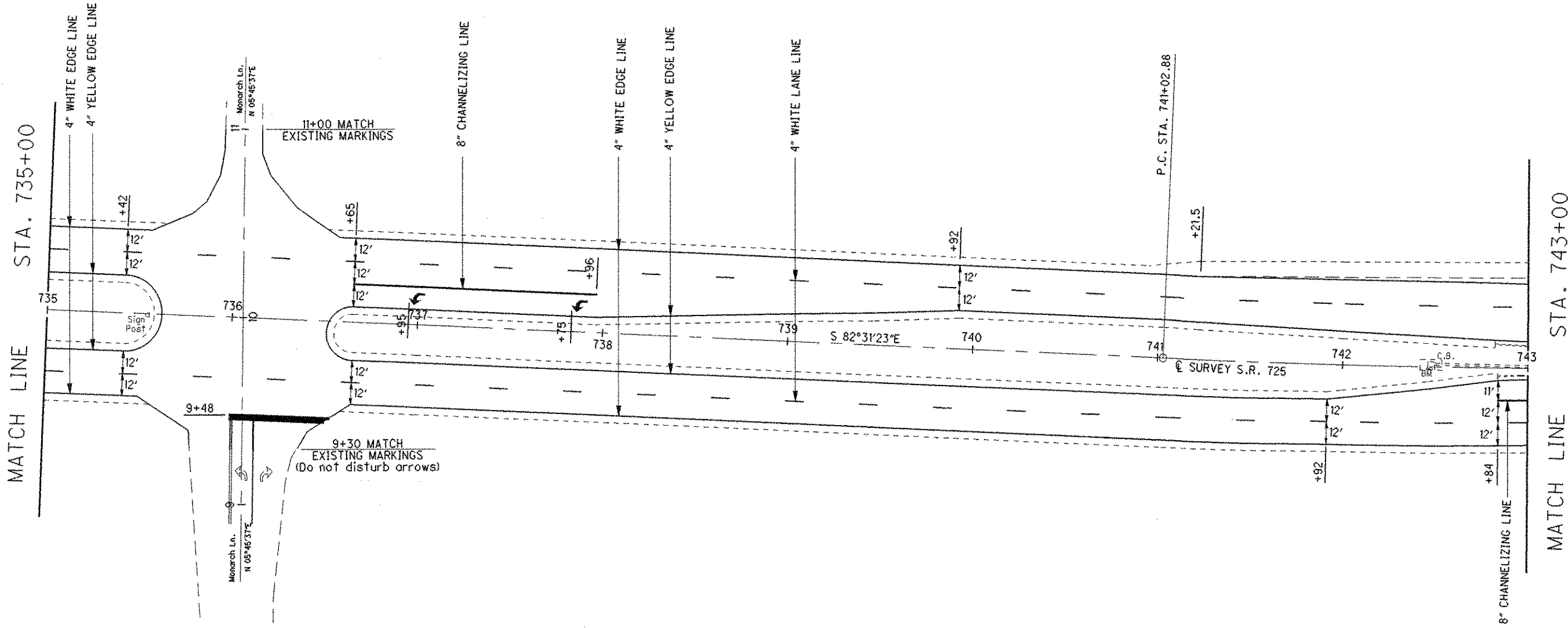
= ANGULAR DESIGN DETECTION LOOP

CALCULATED	REB	CHECKED

0 30 60
HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 727+00 TO STA. 735+00

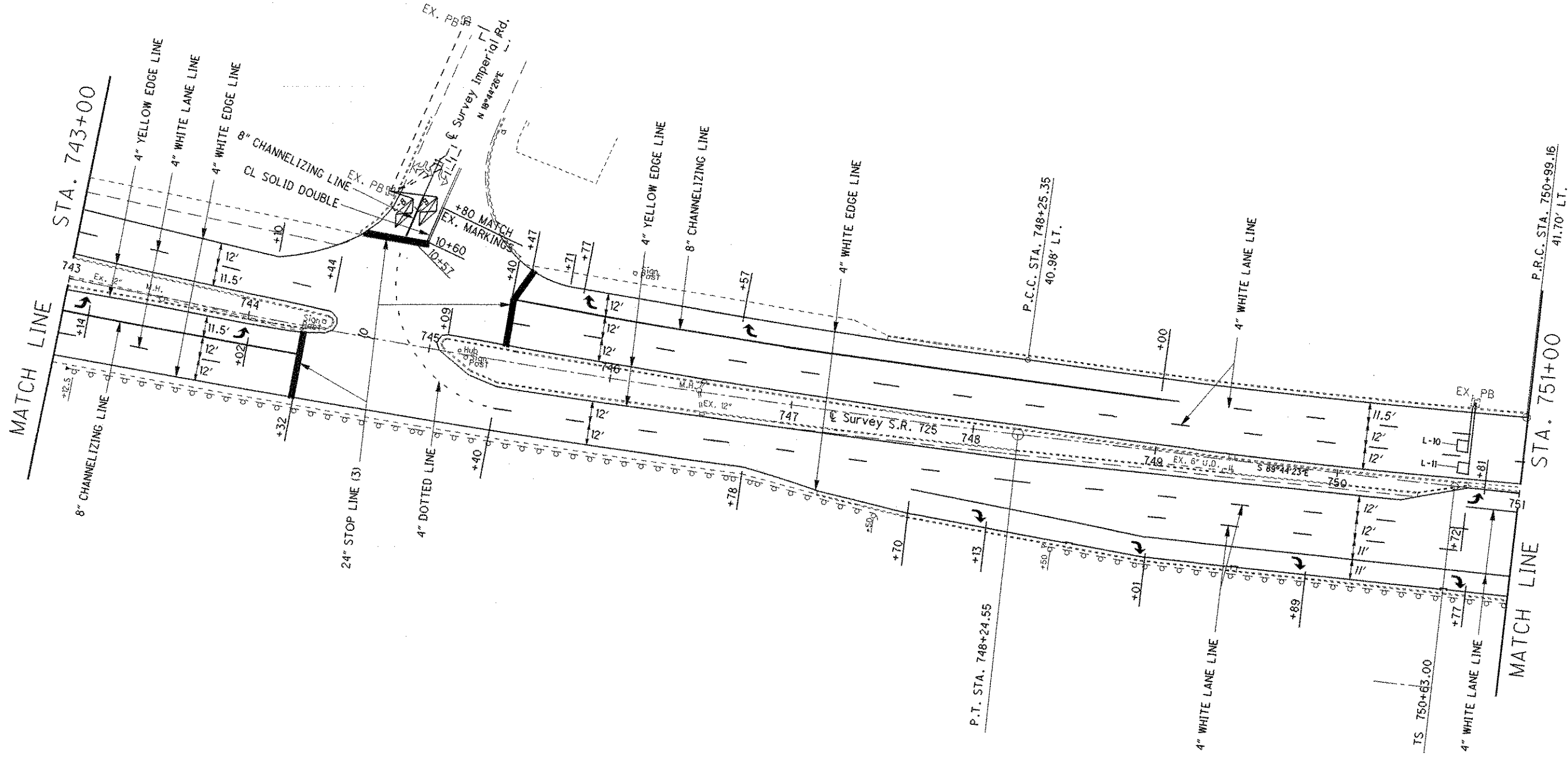
MOT-725-13.82



MATCH LINE STA. 735+00

MATCH LINE STA. 743+00





 = ANGULAR DESIGN DETECTION LOOP

CALCULATED 0
 REB
 CHECKED

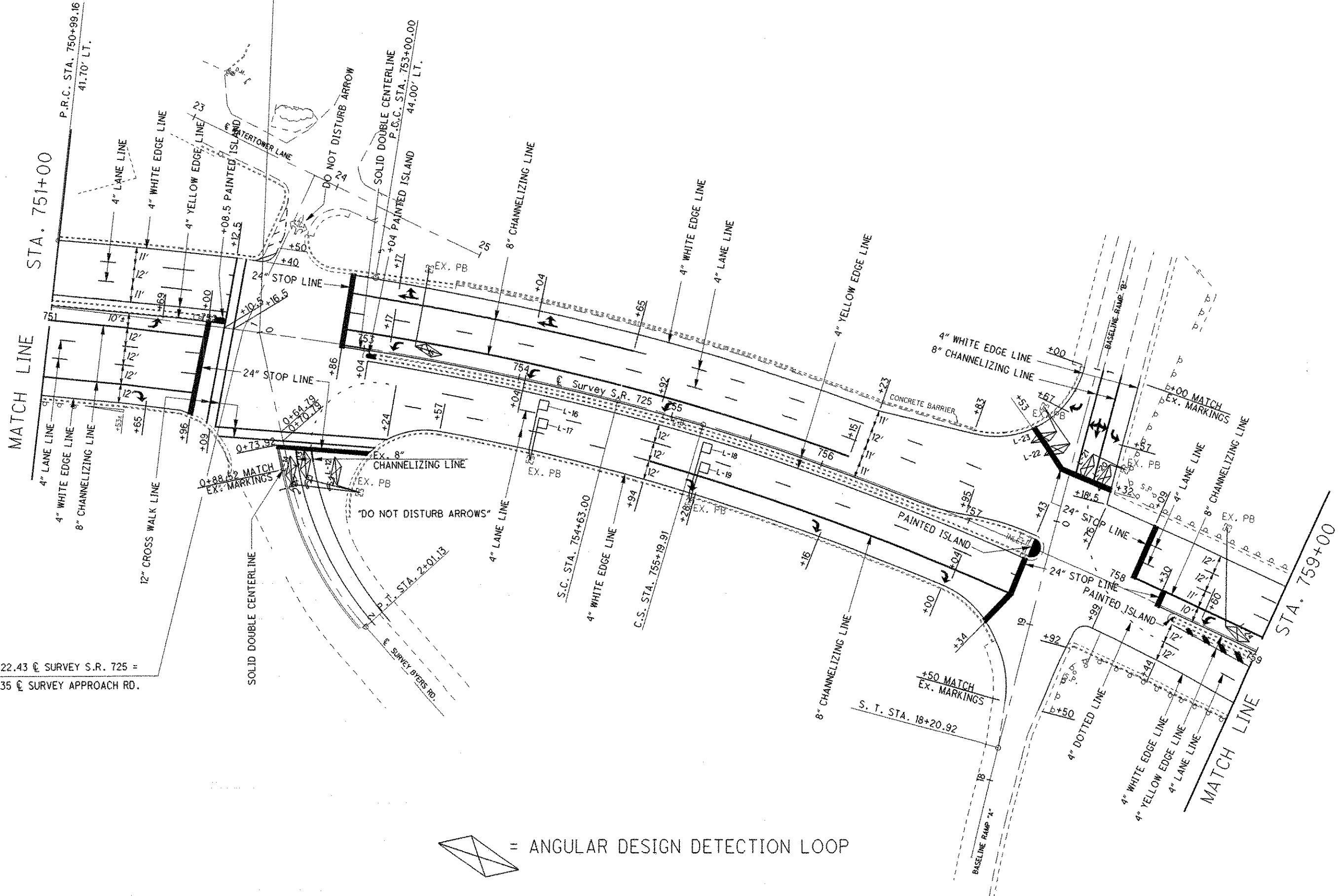
0 30 60
 HORIZONTAL
 SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET - S.R. 725
 STA. 743+00 TO STA. 751+00

MOT-725-13.82

STA. 752+22.43 @ SURVEY S.R. 725 =
STA. 1+06.35 @ SURVEY APPROACH RD.

STA. 752+33 @ SURVEY S.R. 725 =
STA. 0+00 @ SURVEY BYERS RD.



 = ANGULAR DESIGN DETECTION LOOP

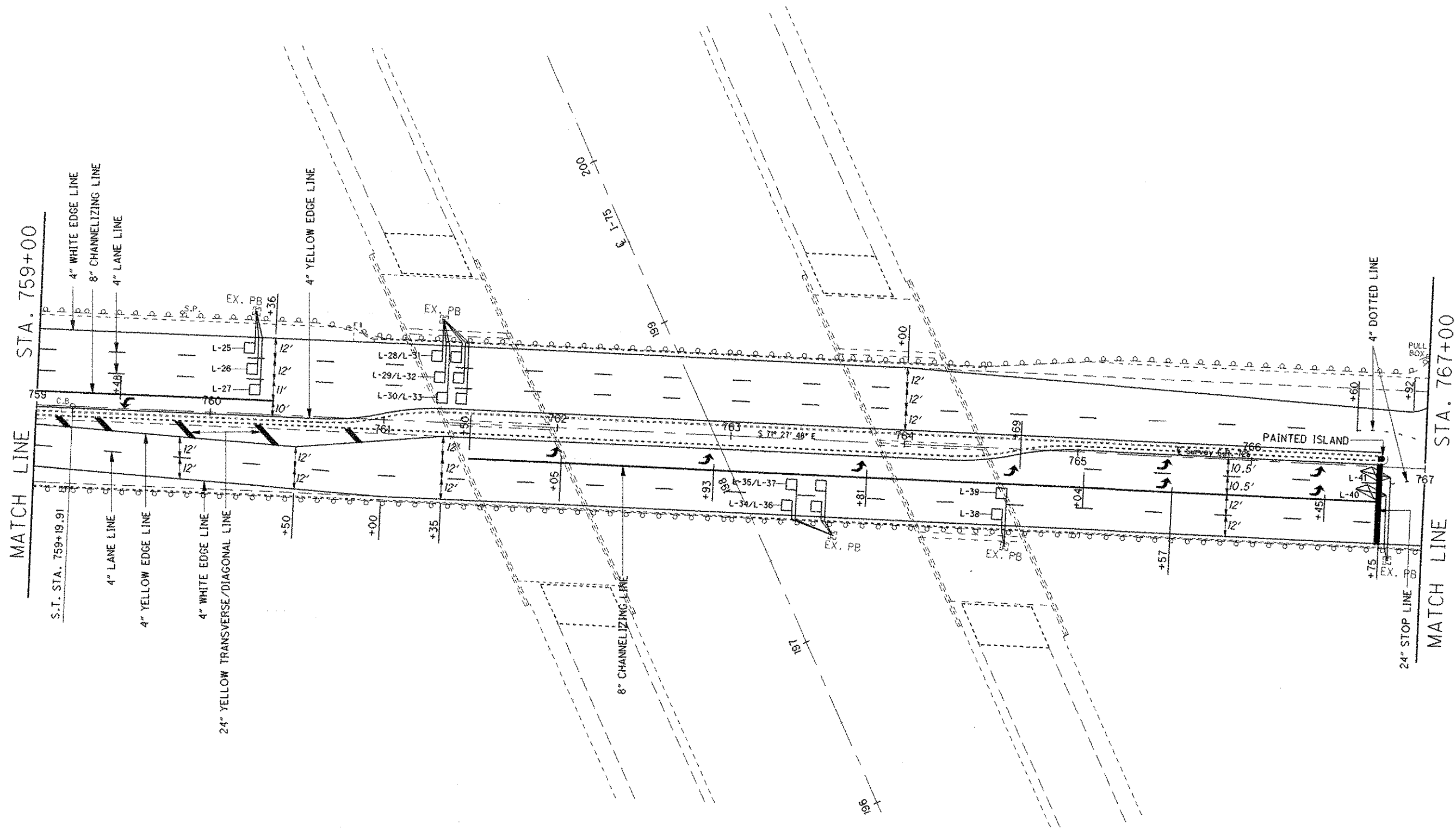
CALCULATED
REB
CHECKED



HORIZONTAL SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 751+00 TO STA. 759+00

MOT-725-13.82



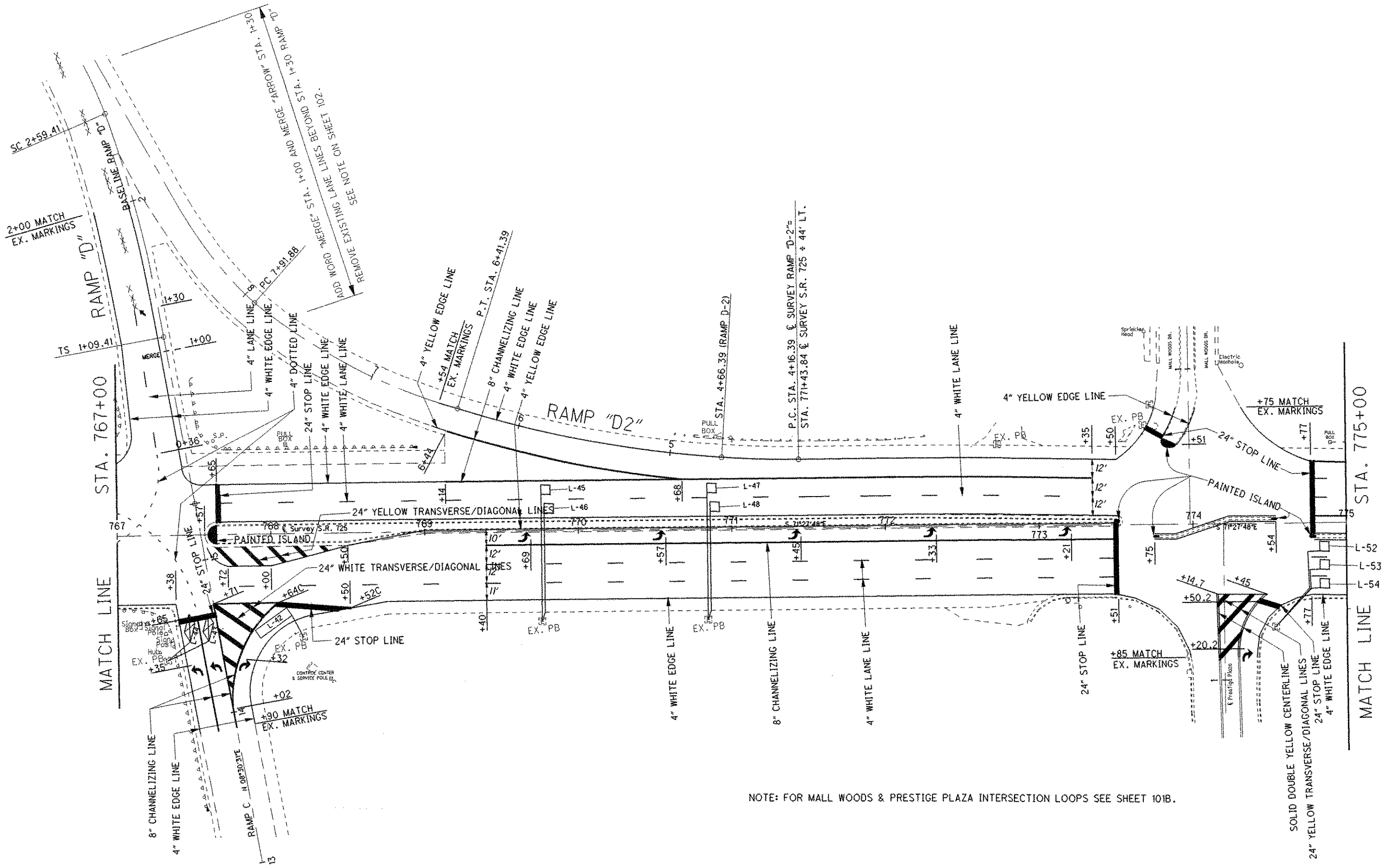
 = ANGULAR DESIGN DETECTION LOOP

CALCULATED
REB
CHECKED

0 30 60
HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 759+00 TO STA. 767+00

MOT-725-13.82



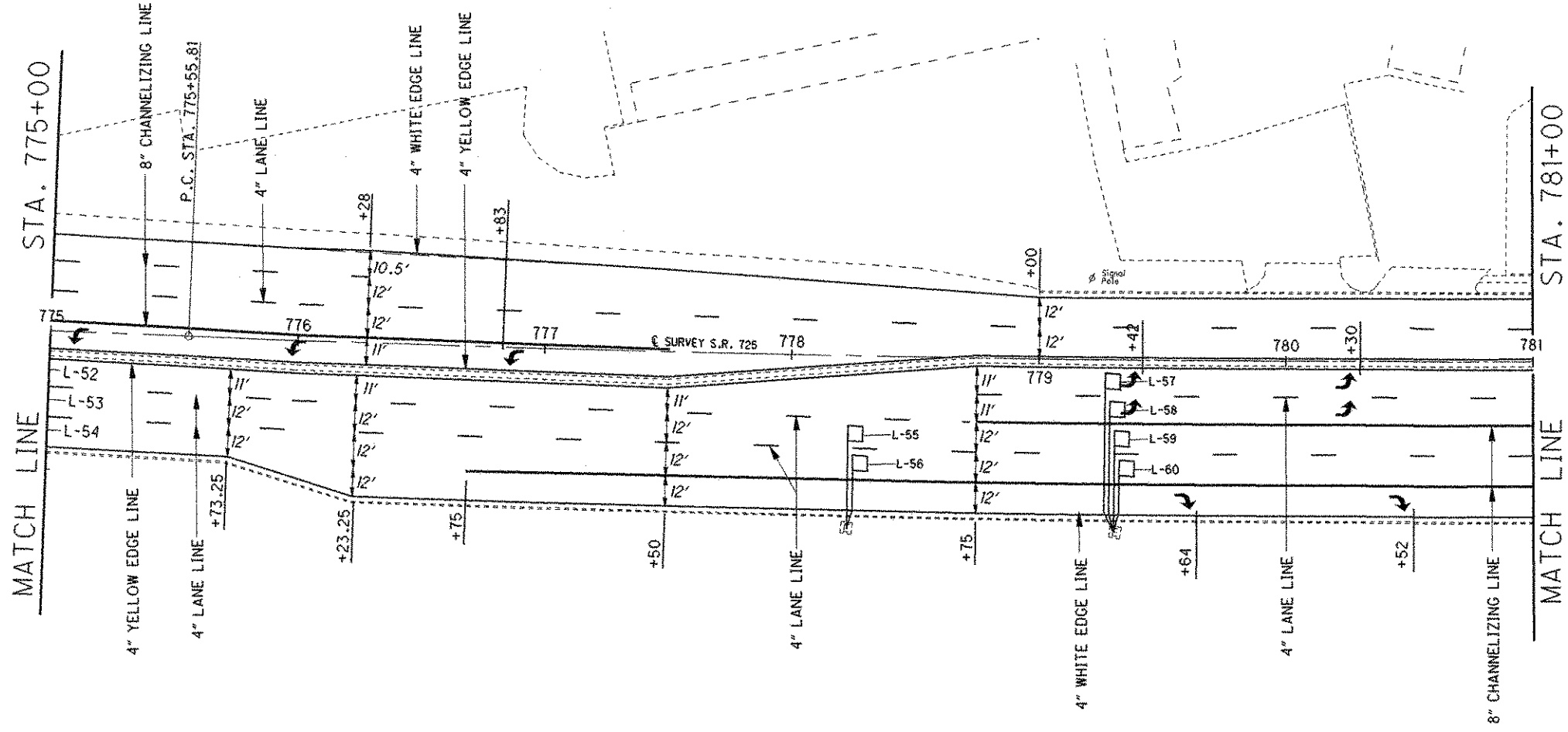
NOTE: FOR MALL WOODS & PRESTIGE PLAZA INTERSECTION LOOPS SEE SHEET 101B.

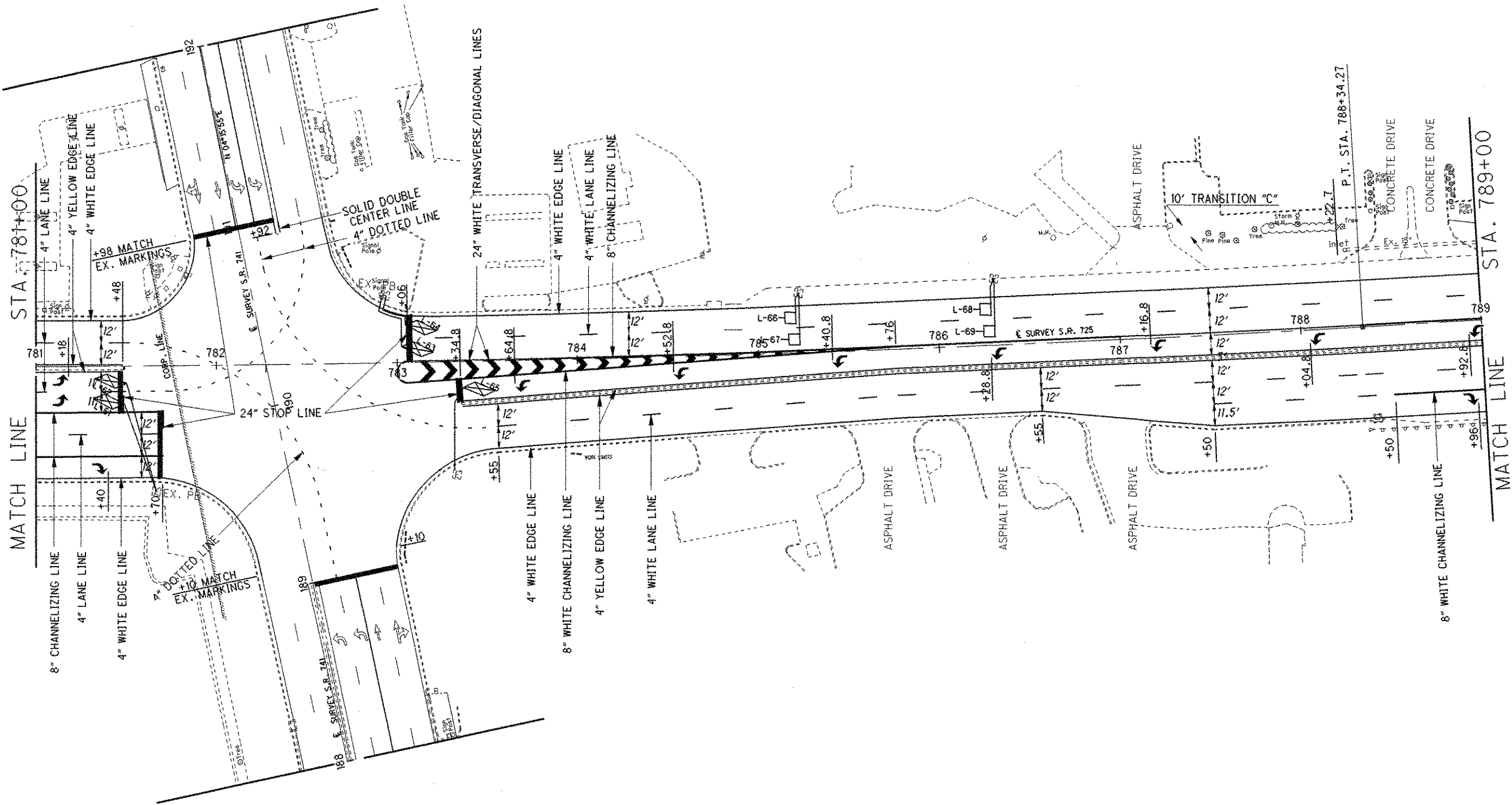
CALCULATED 0
 REB
 CHECKED

30
 60
 HORIZONTAL
 SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 767+00 TO STA. 775+00

MOT-725-13.82





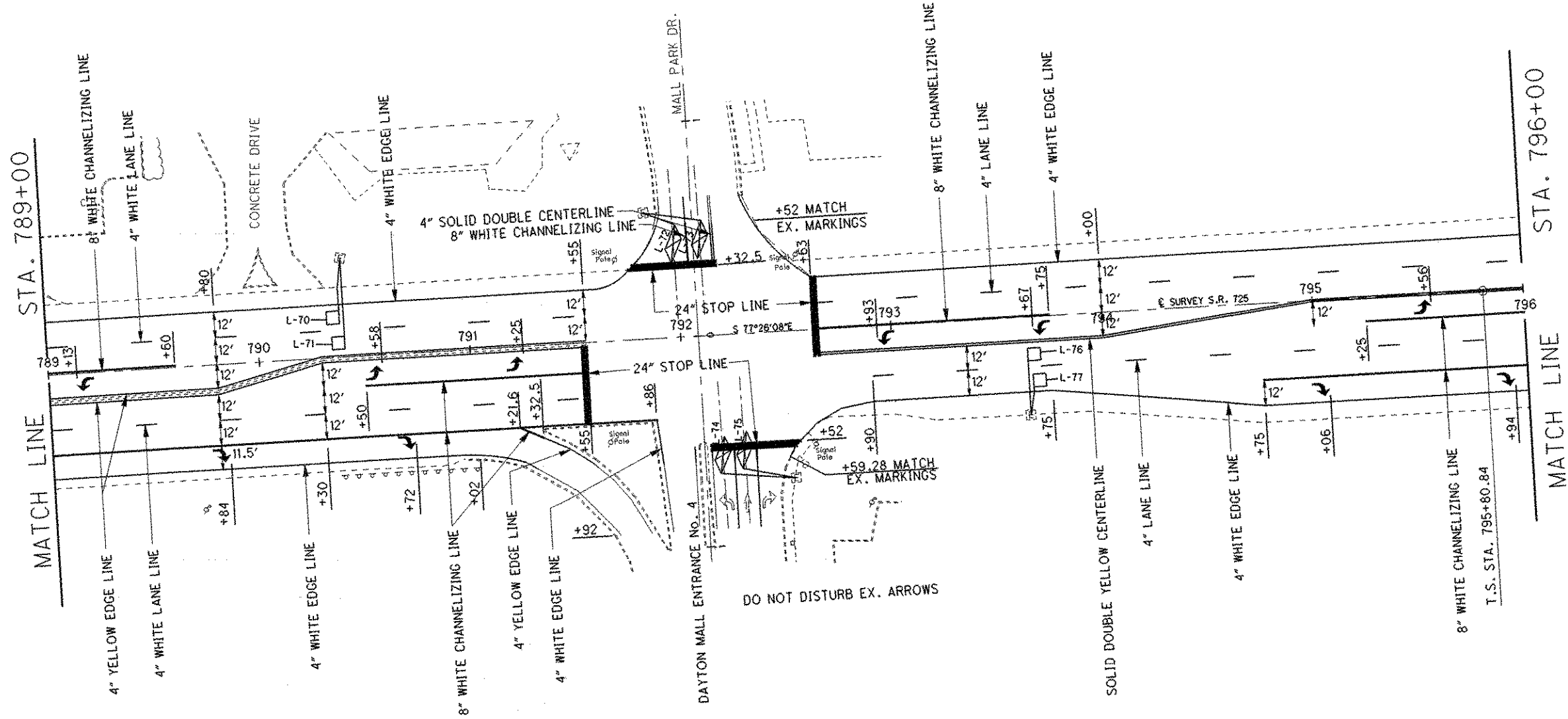
= ANGULAR DESIGN DETECTION LOOP

CALCULATED
REB
CHECKED

HORIZONTAL SCALE IN FEET

**TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 781+00 TO STA. 789+00**

MOT-725-13.82



 = ANGULAR DESIGN DETECTION LOOP

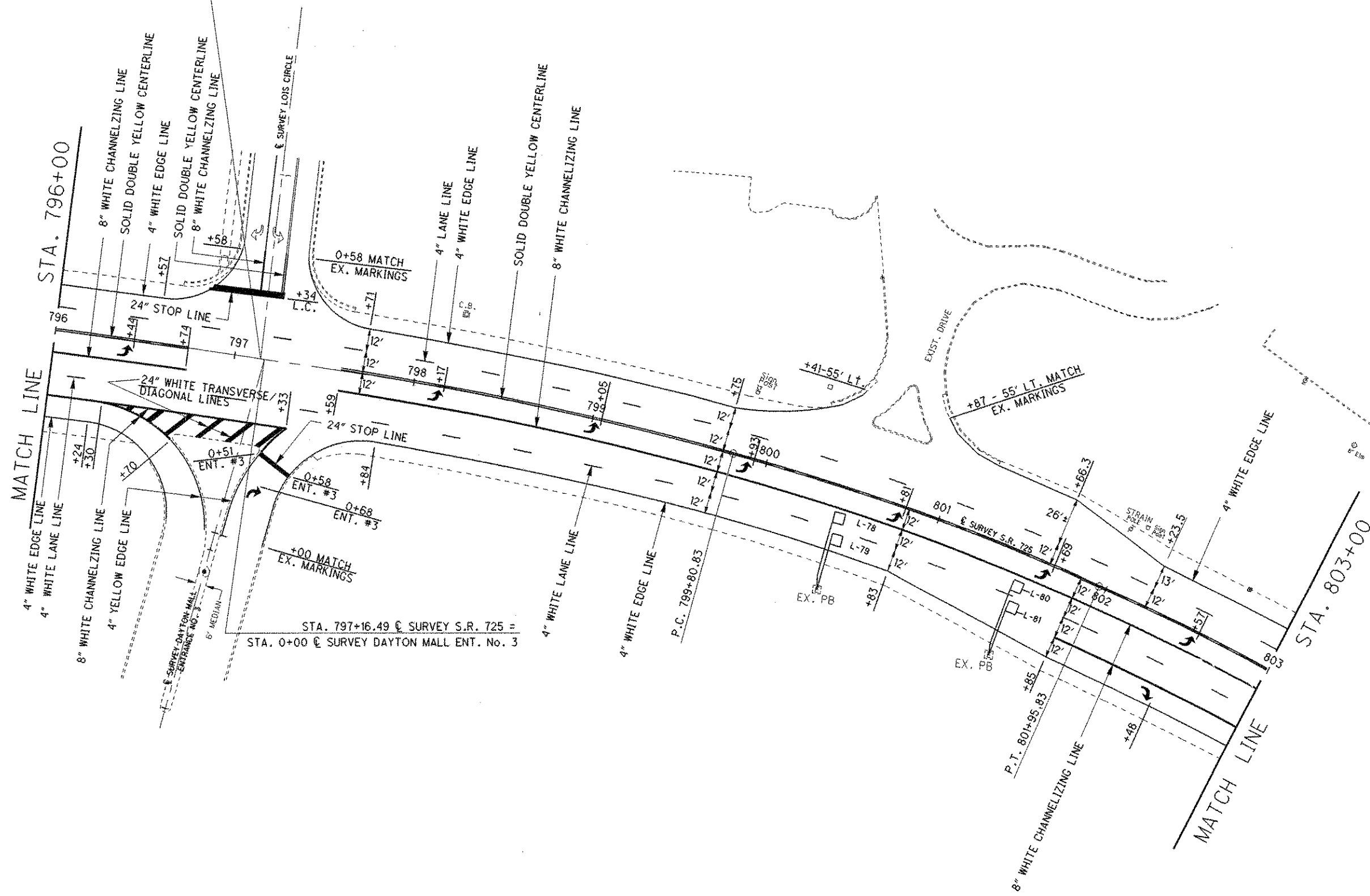
CALCULATED	REB
CHECKED	CHECKED

0 30 60
HORIZONTAL SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 789+00 TO STA. 796+00

MOT-725-13.82

STA. 797+14.41 @ SURVEY S.R. 725 =
STA. 0+00 @ SURVEY LOIS CIRCLE



STA. 797+16.49 @ SURVEY S.R. 725 =
STA. 0+00 @ SURVEY DAYTON MALL ENT. No. 3

CALCULATED
CHECKED

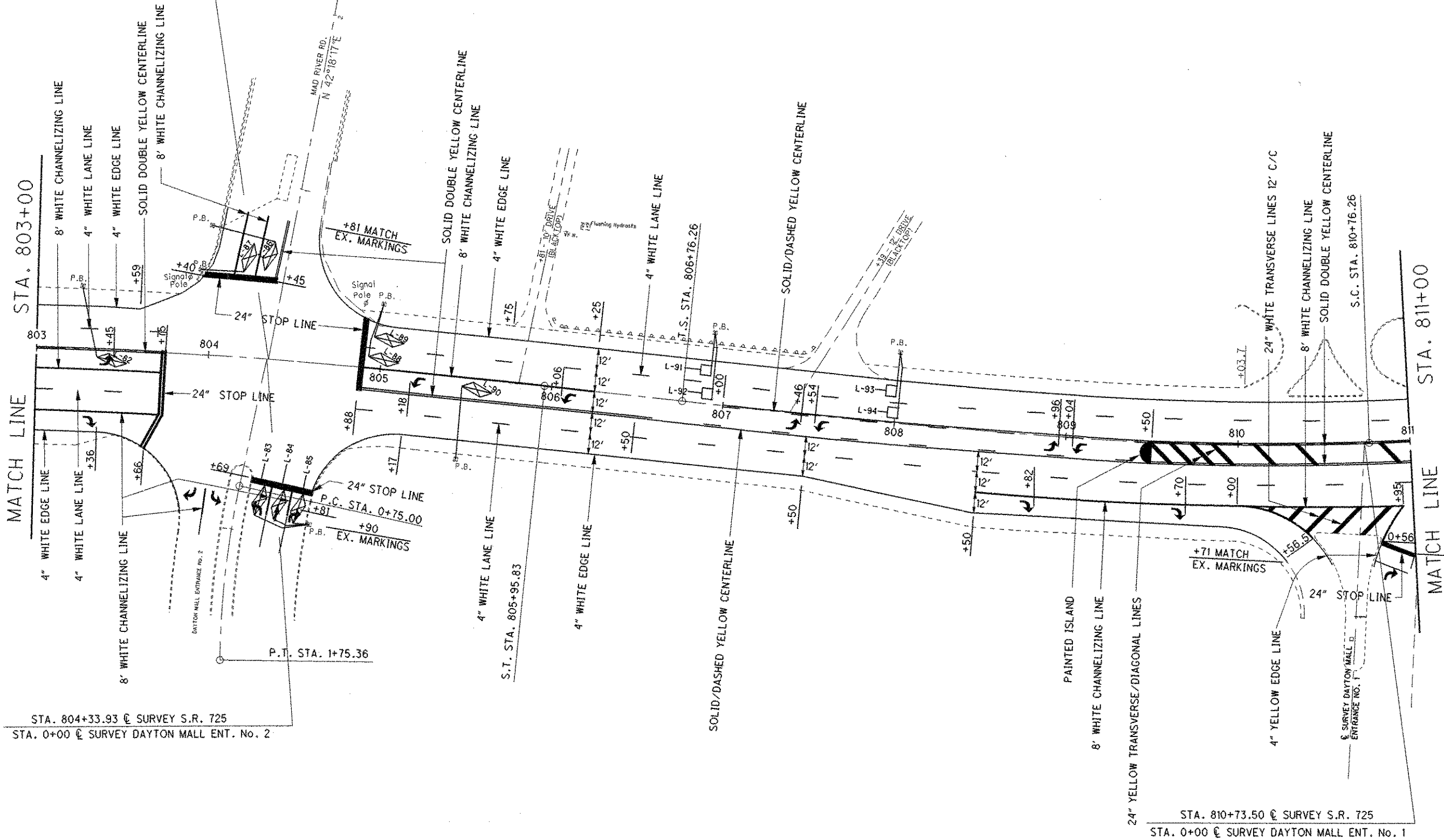
0 15 30 60
HORIZONTAL SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 796+00 TO STA. 803+00

MOT-725-13.82

I:\PROJECTS\MOT\725\1382\roadway\sheets\Mot725tp.dgn 27-JUL-2009 9:10AM rburns2

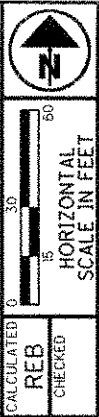
STA. 804+34.83 @ SURVEY S.R. 725
STA. 0+00 @ SURVEY MAD RIVER RD.



STA. 804+33.93 @ SURVEY S.R. 725
STA. 0+00 @ SURVEY DAYTON MALL ENT. No. 2

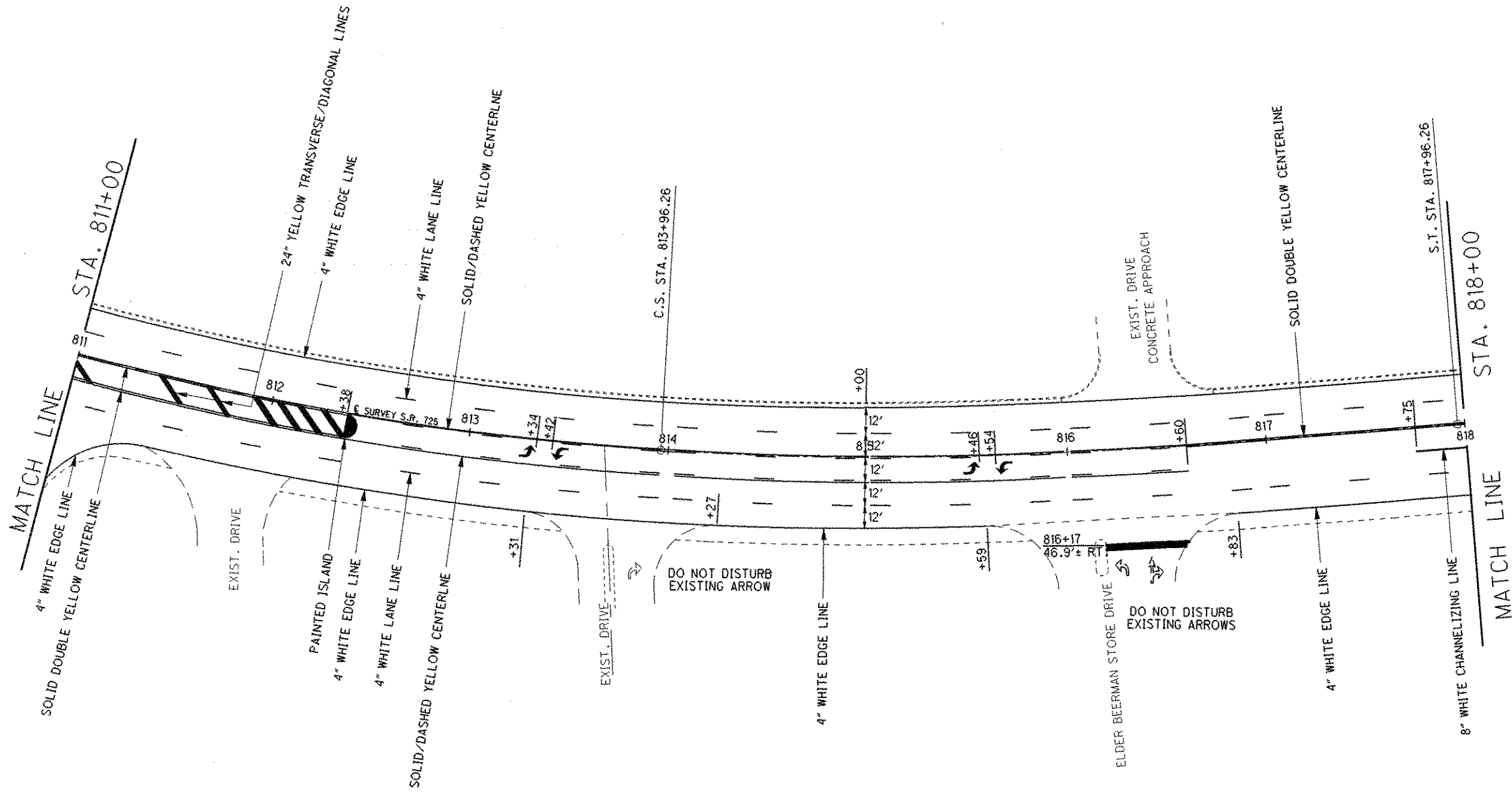
STA. 810+73.50 @ SURVEY S.R. 725
STA. 0+00 @ SURVEY DAYTON MALL ENT. No. 1

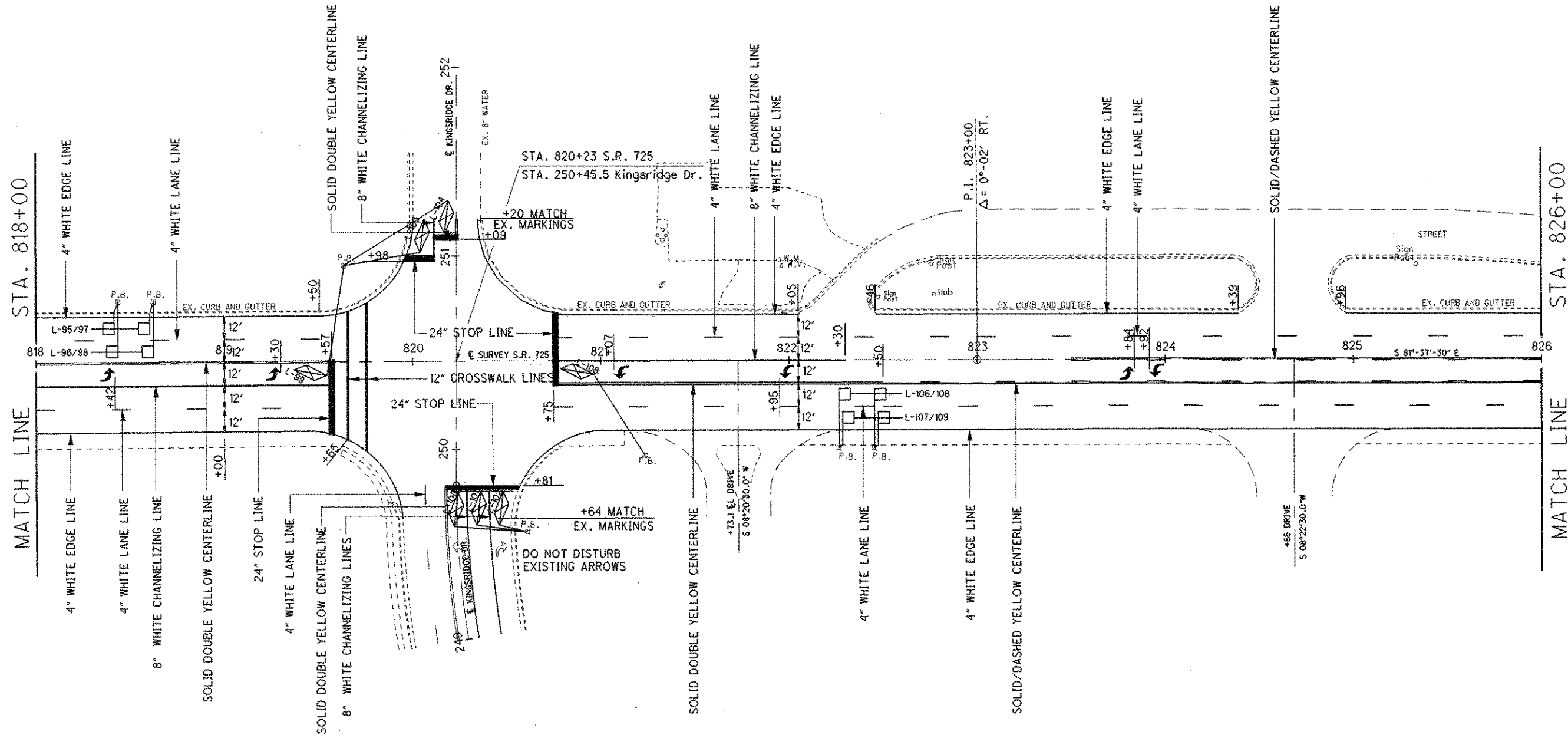
 = ANGULAR DESIGN DETECTION LOOP



TRAFFIC CONTROL PLAN SHEET - S.R. 725
STA. 803+00 TO STA. 811+00

MOT - 725 - 13.82





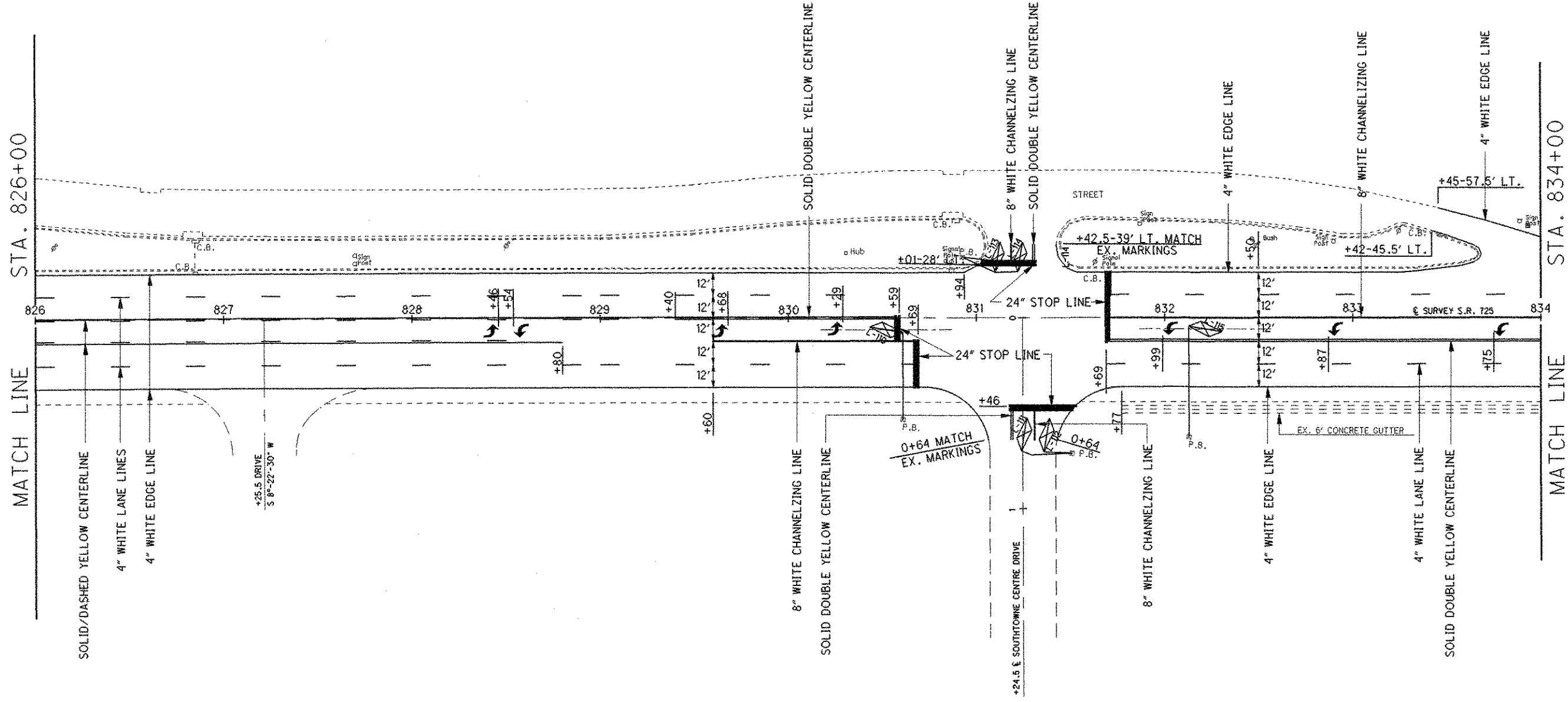
 = ANGULAR DESIGN DETECTION LOOP

CALCULATED
REB
CHECKED

0 15 30 60
HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 818+00 TO STA. 826+00

MOT-725-13.82



 = ANGULAR DESIGN DETECTION LOOP

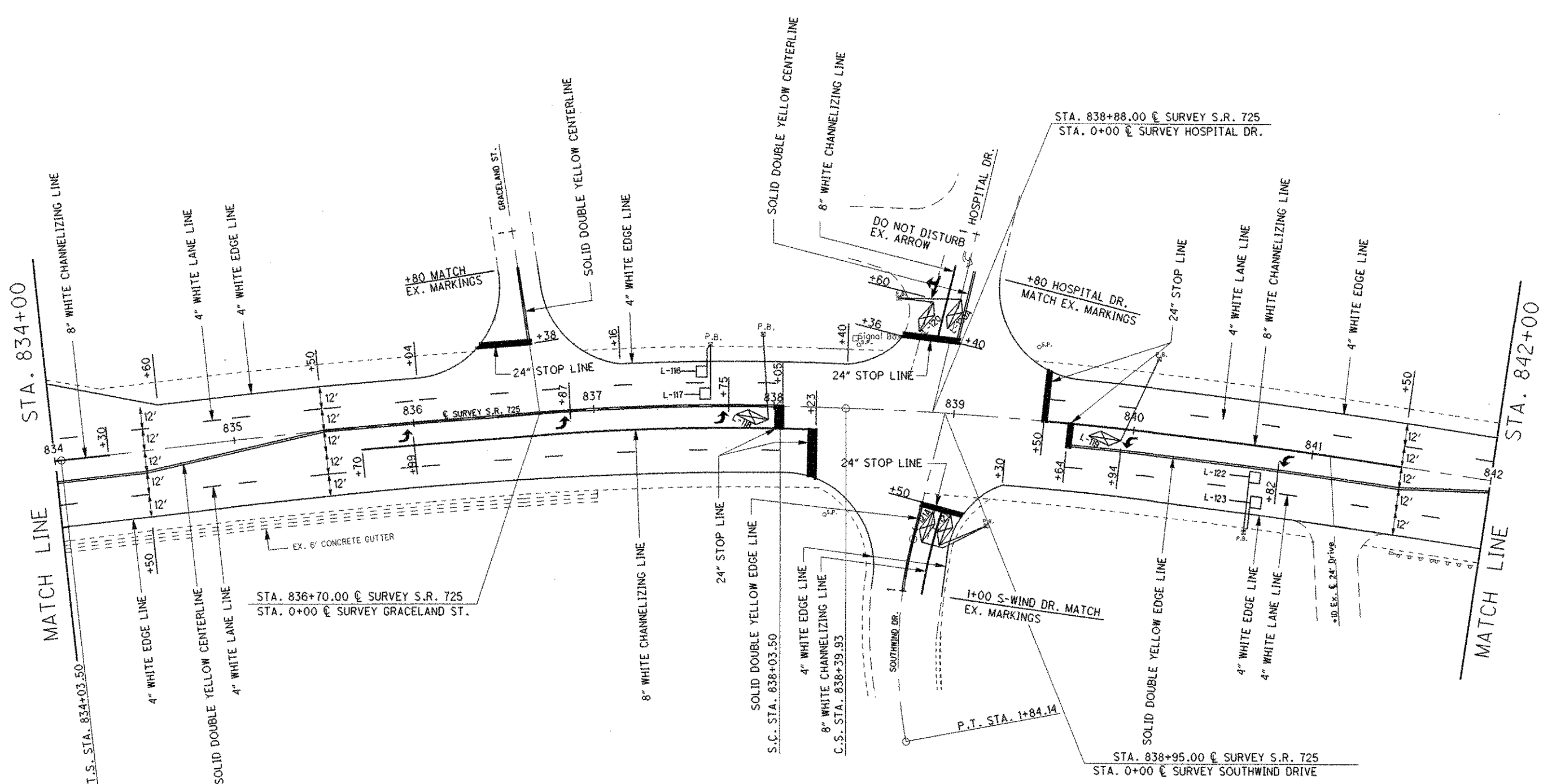
CALCULATED
REB
CHECKED




HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET - S.R. 725
STA. 826+00 TO STA. 834+00

MOT-725-13.8.2



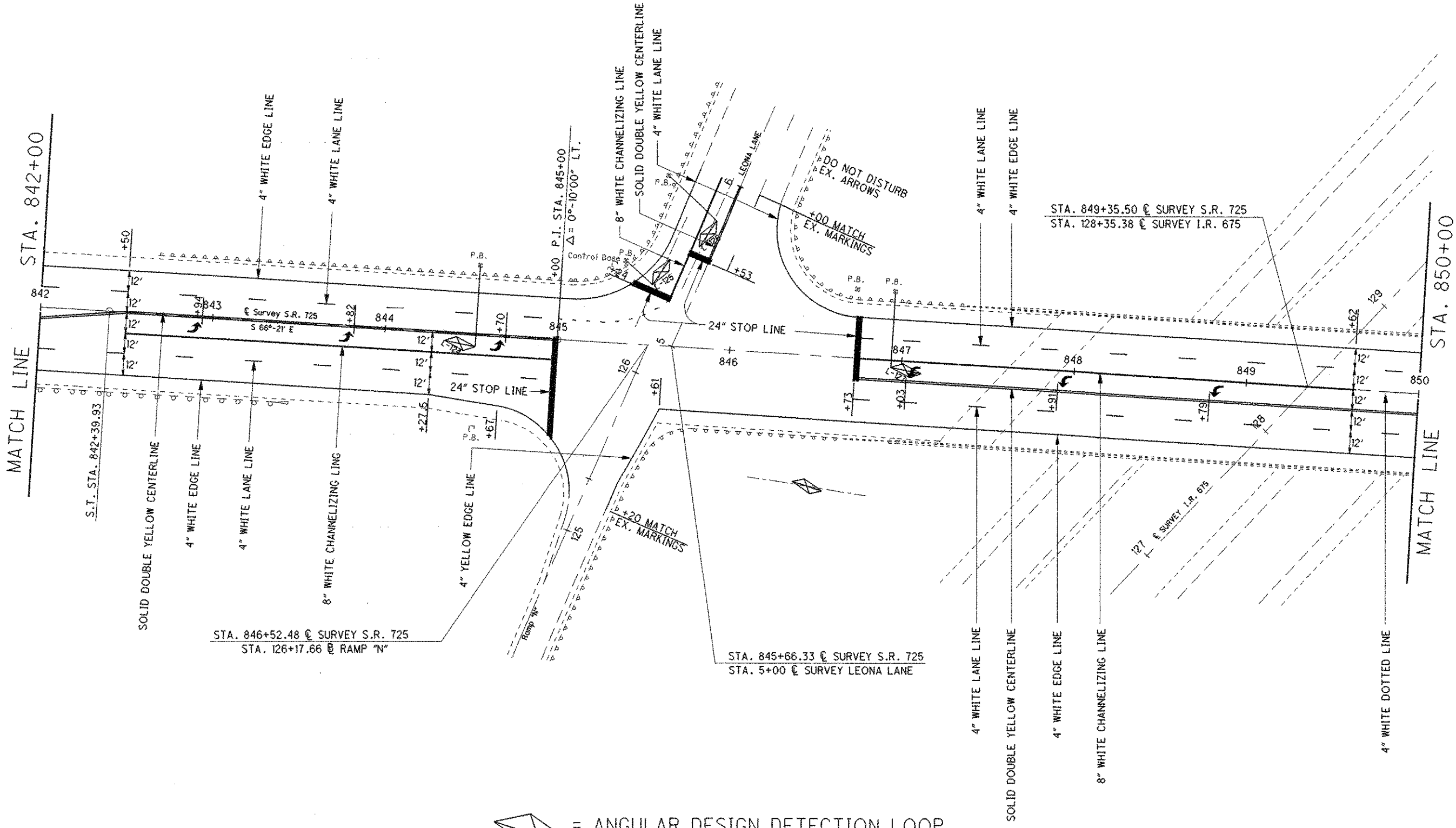
 = ANGULAR DESIGN DETECTION LOOP

CALCULATED
REB
CHECKED

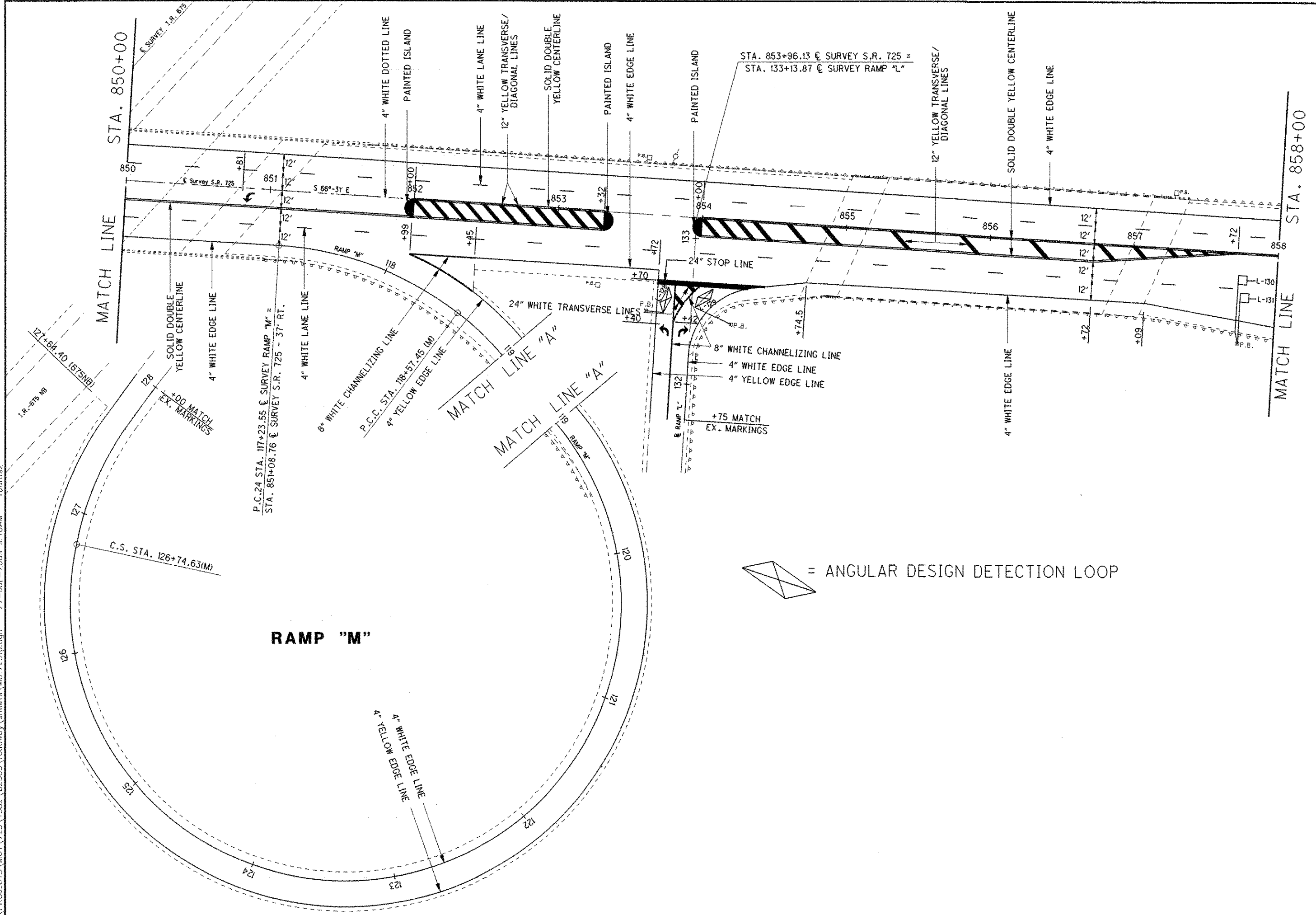
0 15 30 60
HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 834+00 TO STA. 842+00

MOT-725-13.82


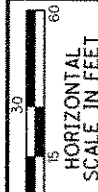


 = ANGULAR DESIGN DETECTION LOOP



 = ANGULAR DESIGN DETECTION LOOP

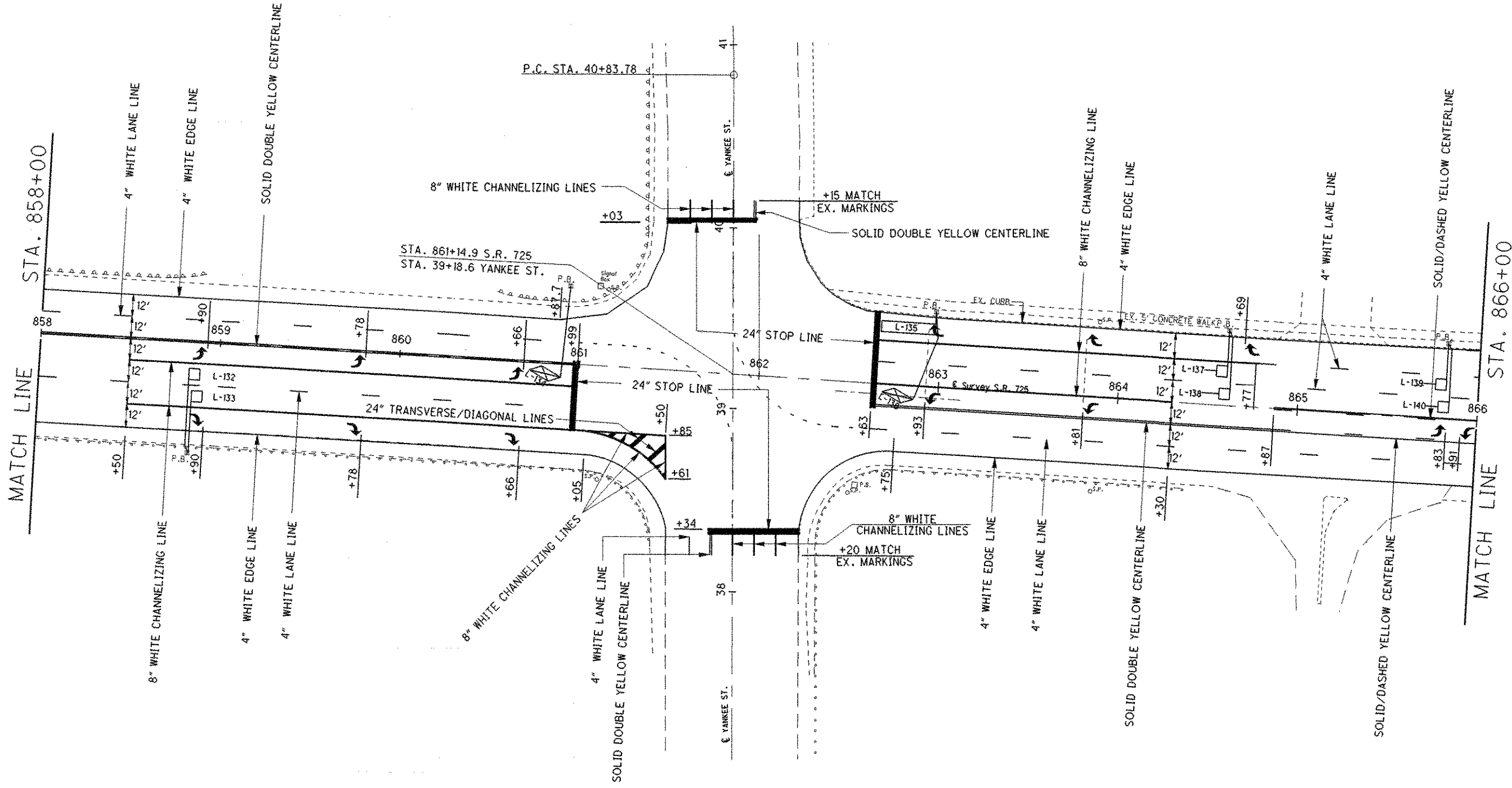
CALCULATED
 REB
 CHECKED

HORIZONTAL
 SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 850+00 TO STA. 858+00

MOT-725-13.82



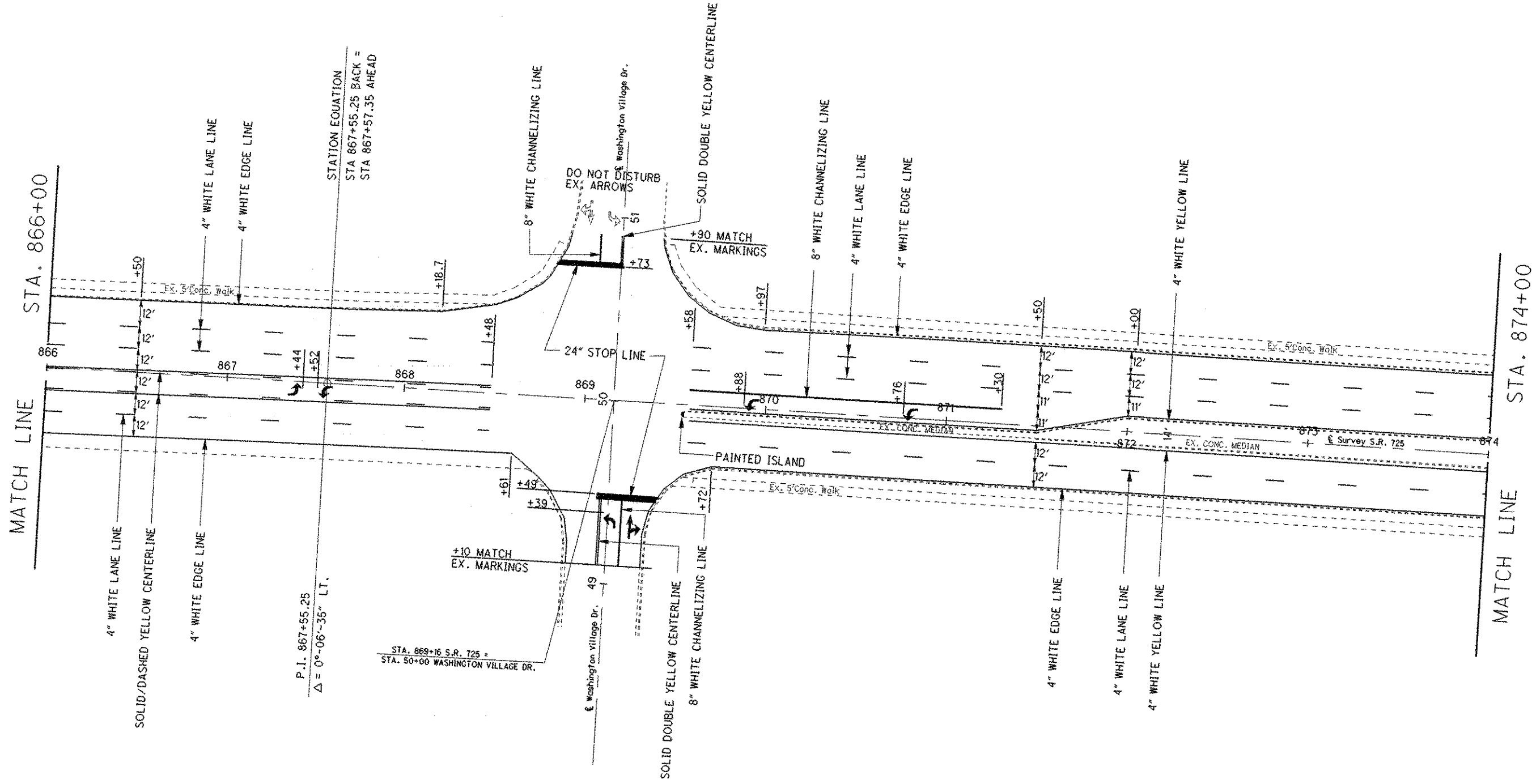
 = ANGULAR DESIGN DETECTION LOOP

CALCULATED
 REB
 CHECKED

0 15 30 60
 HORIZONTAL
 SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET - S.R. 725
STA. 858+00 TO STA. 866+00

MOT-725-13.82

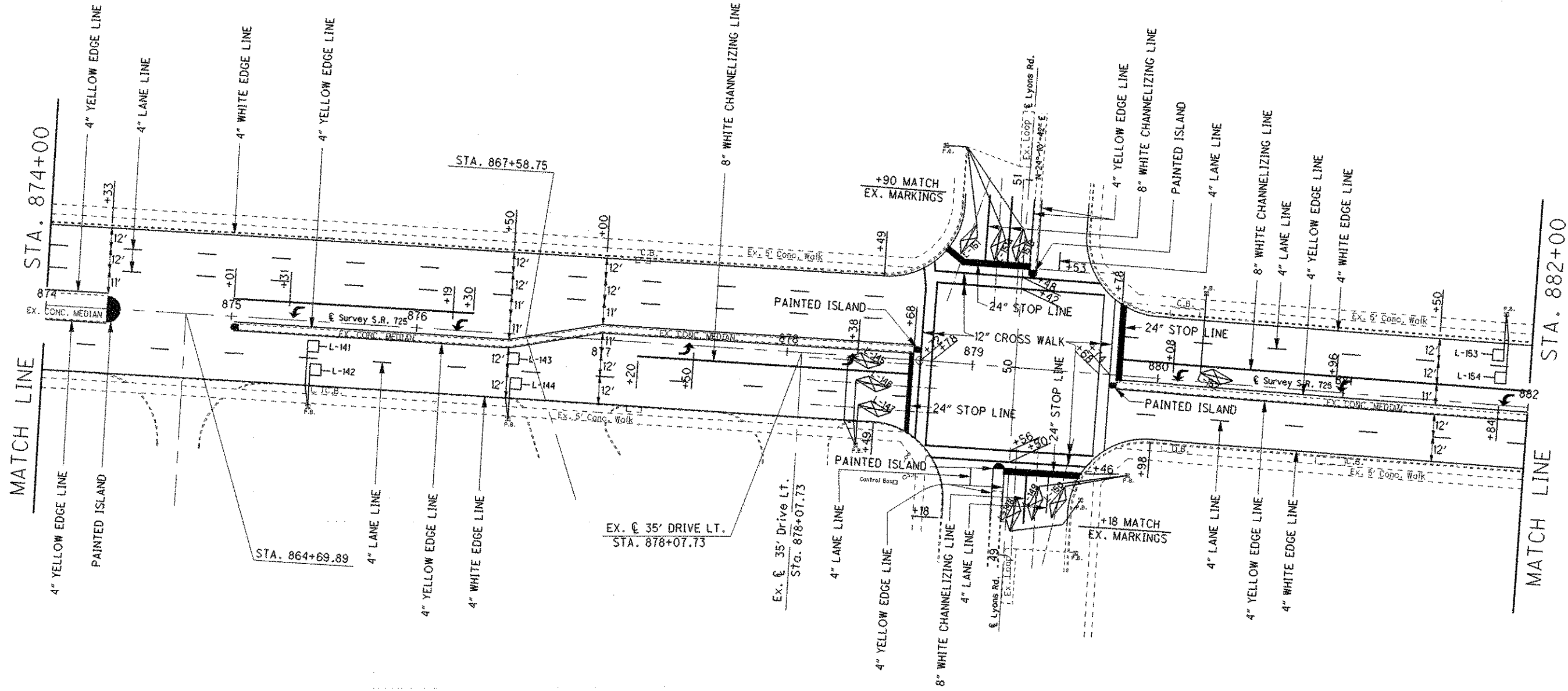


STATION EQUATION
 STA 867+55.25 BACK =
 STA 867+57.35 AHEAD

P.I. 867+55.25
 $\Delta = 0^{\circ}-06'-35''$ LT.

STA. 869+16 S.R. 725 =
 STA. 50+00 WASHINGTON VILLAGE DR.





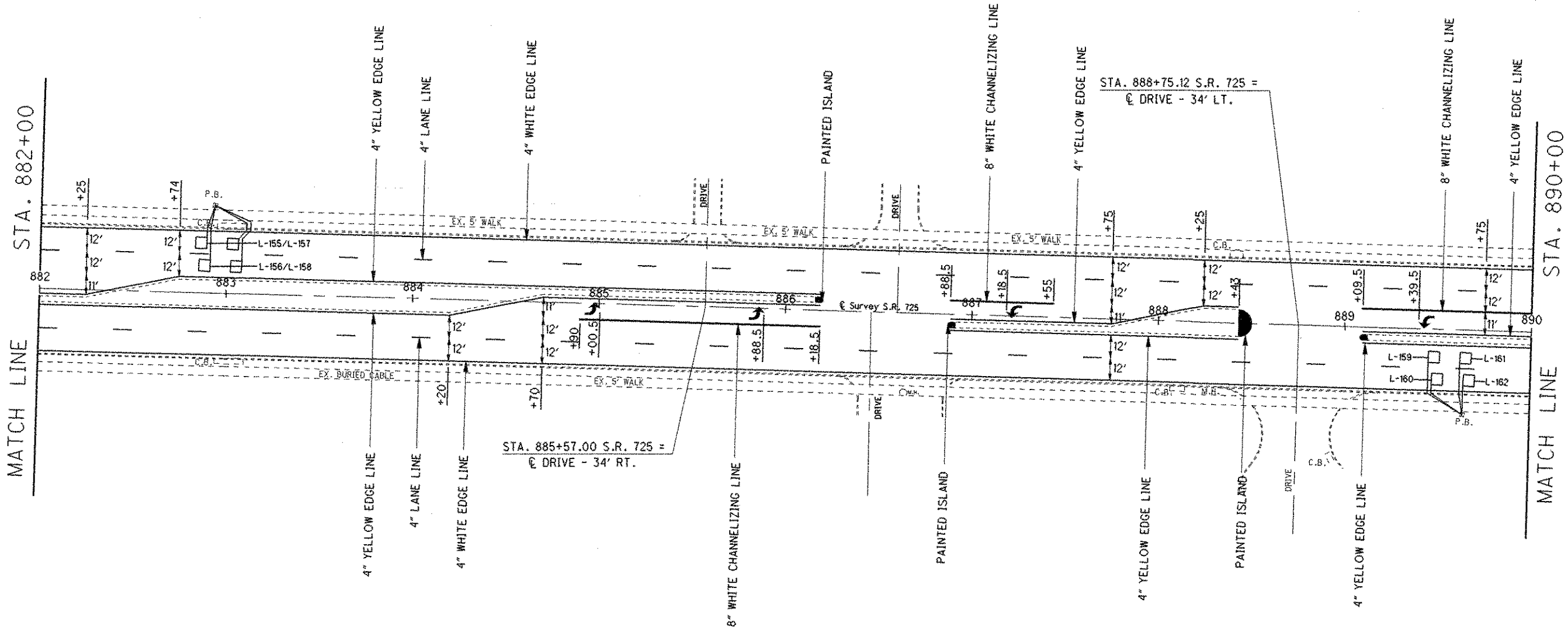
= ANGULAR DESIGN DETECTION LOOP

CALCULATED
REB
CHECKED

0 15 30 60
HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 874+00 TO STA. 882+00

MOT-725-13.82



MATCH LINE STA. 882+00

MATCH LINE STA. 890+00

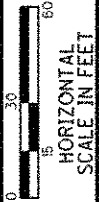
STA. 885+57.00 S.R. 725 =
☉ DRIVE - 34' RT.

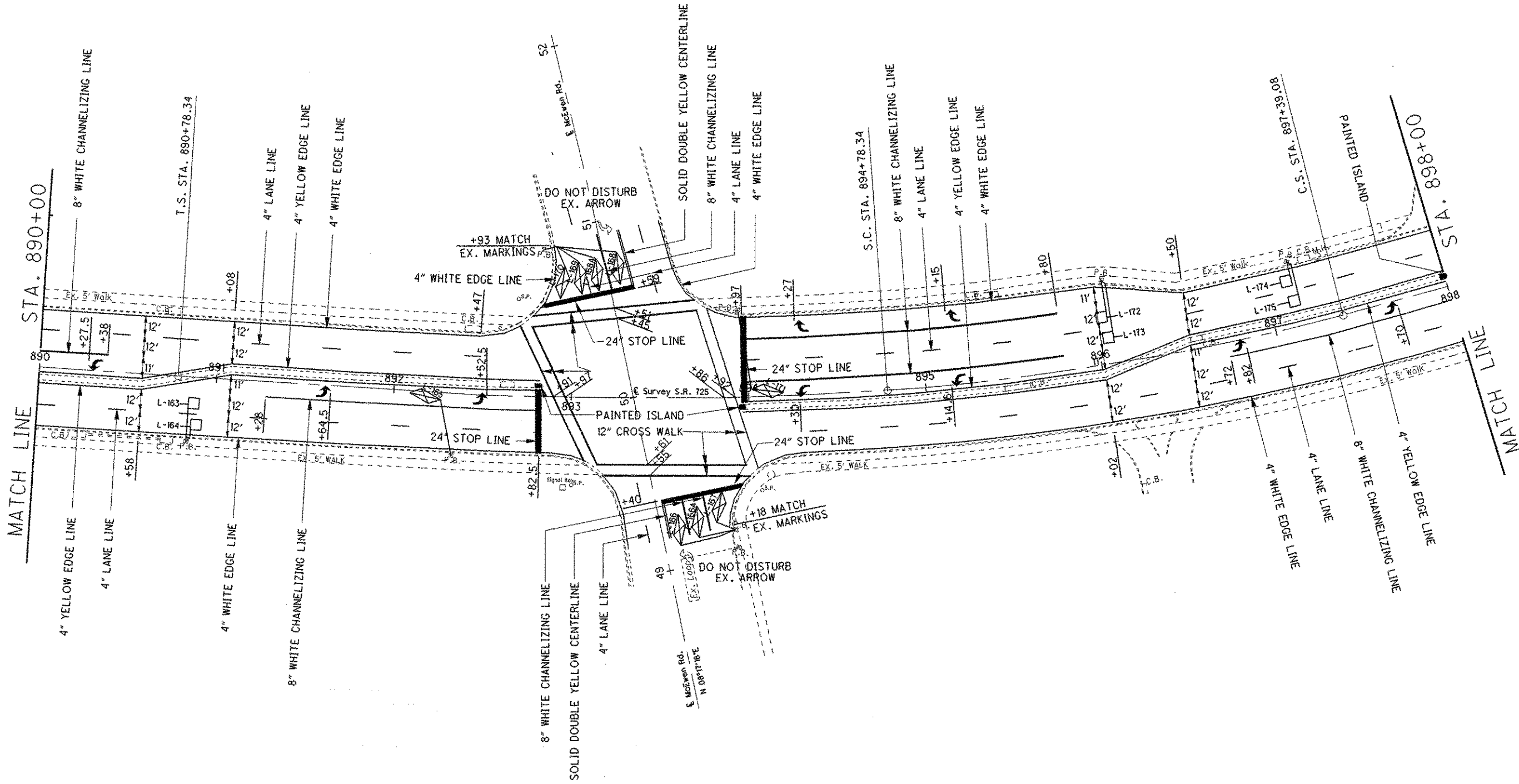
STA. 888+75.12 S.R. 725 =
☉ DRIVE - 34' LT.

TRAFFIC CONTROL PLAN SHEET - S.R. 725
STA. 882+00 TO STA. 890+00

MOT-725-13.82


CALCULATED	REB	CHECKED
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 = ANGULAR DESIGN DETECTION LOOP

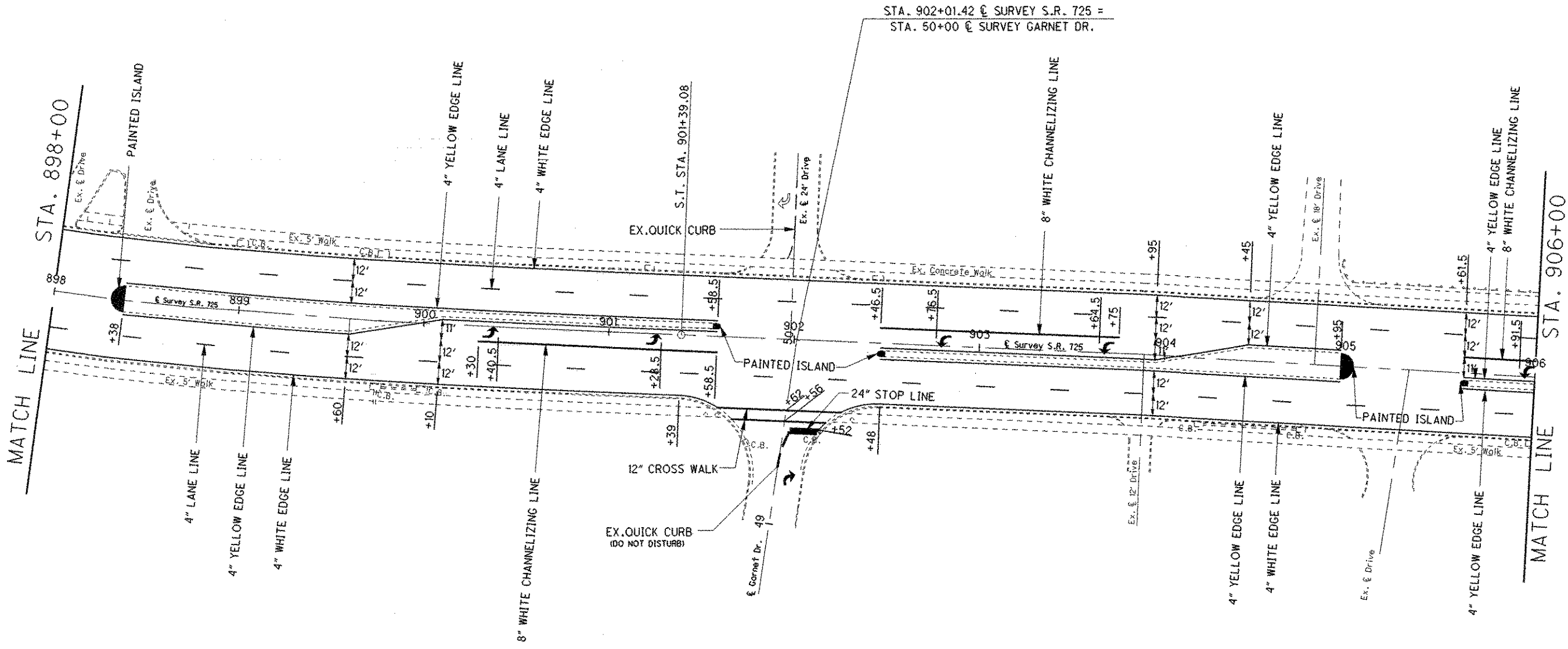
CALCULATED
REB
CHECKED



0 30 60
HORIZONTAL
SCALE IN FEET

**TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 890+00 TO STA. 898+00**

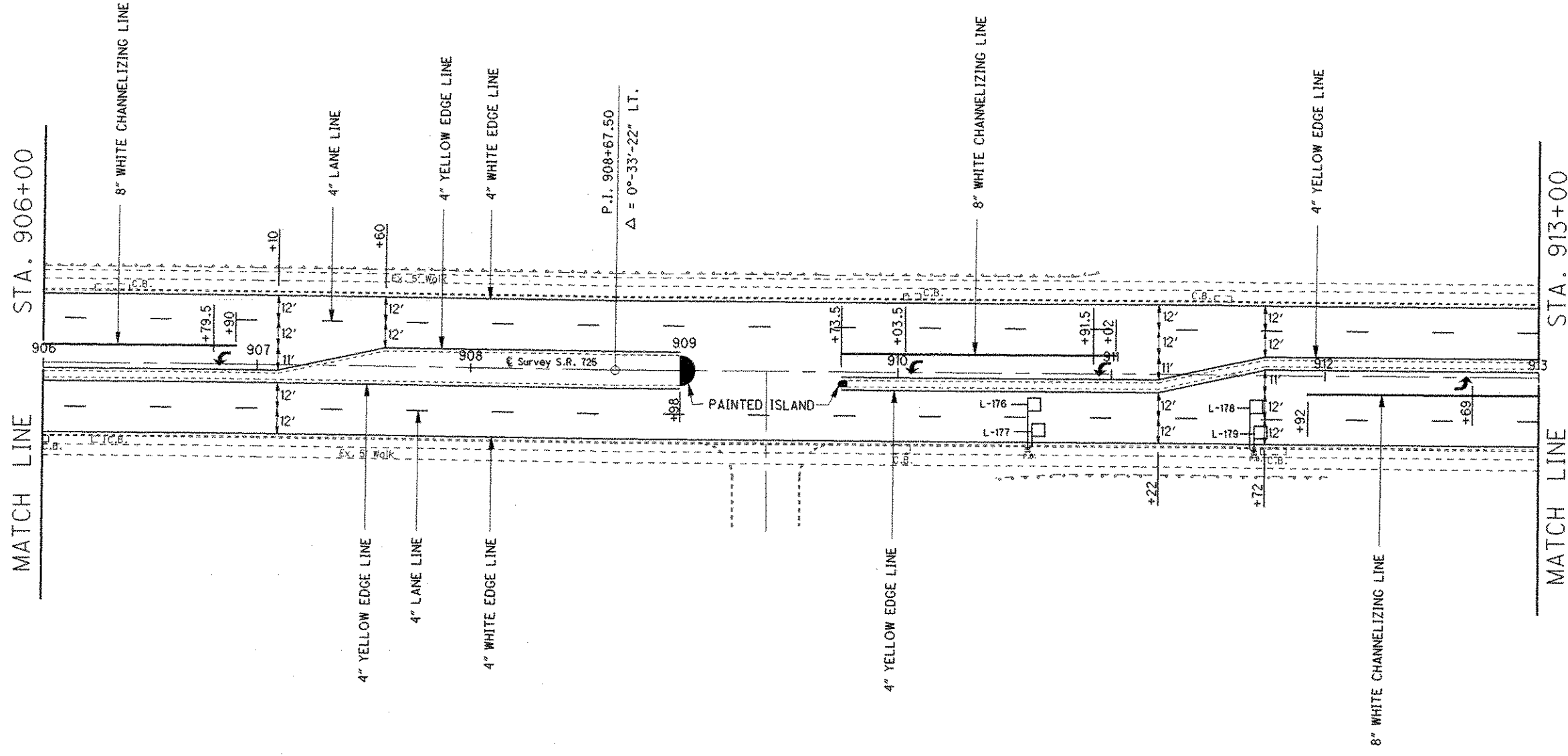
MOT-725-13.82



CALCULATED	REB	CHECKED

0 15 30 60
 HORIZONTAL SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 898+00 TO STA. 906+00

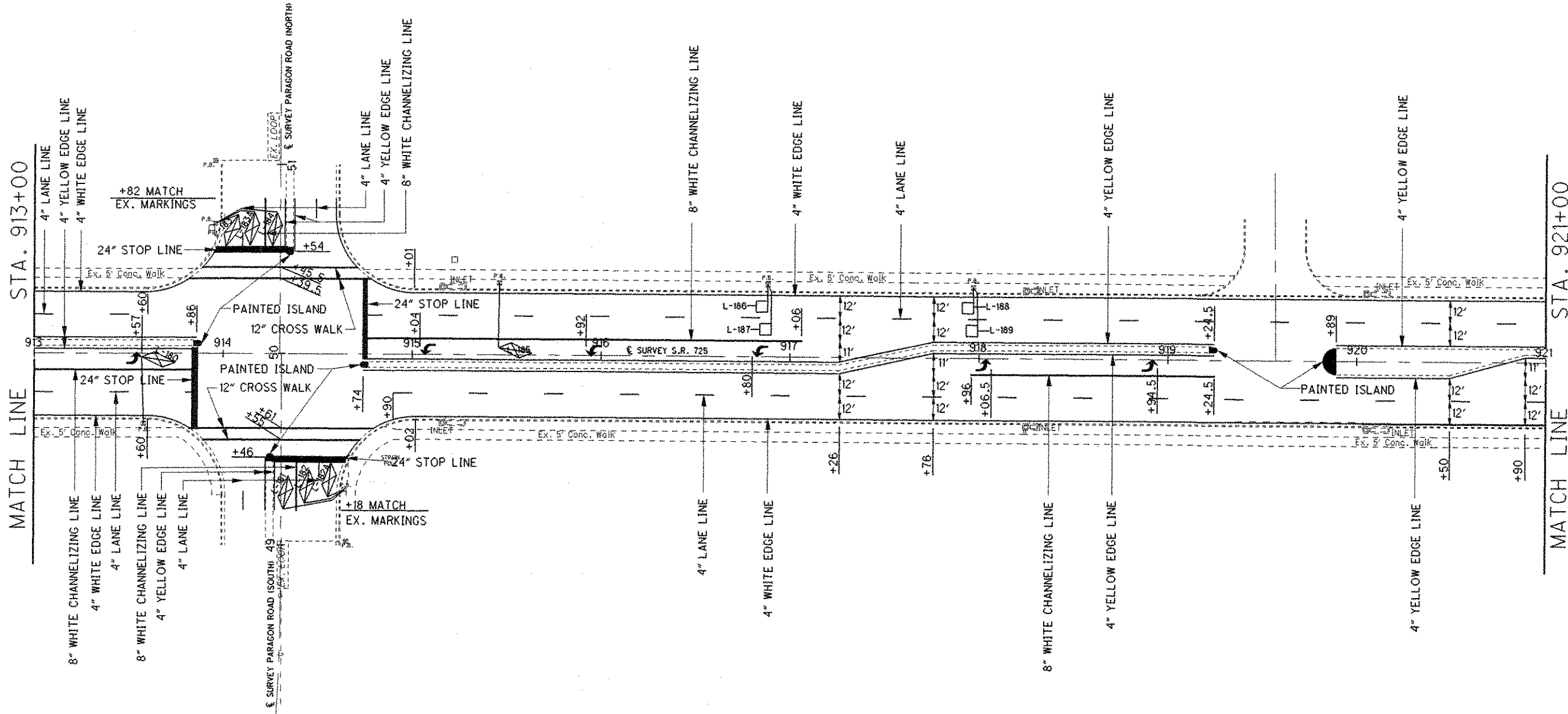


CALCULATED	REB	CHECKED

0 15 30 60
HORIZONTAL SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET - S.R. 725
STA. 906+00 TO STA. 913+00

MOT - 725 - 13.82

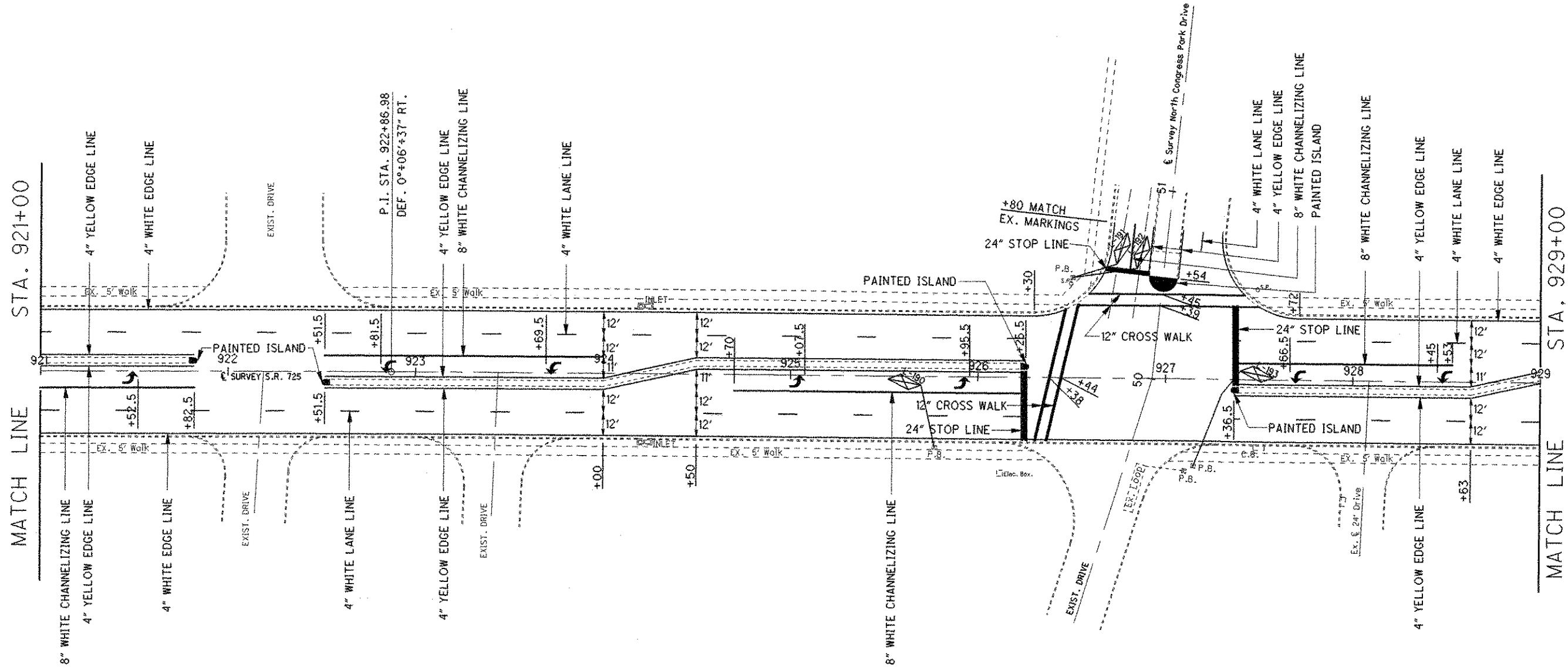


CALCULATED
REB
CHECKED

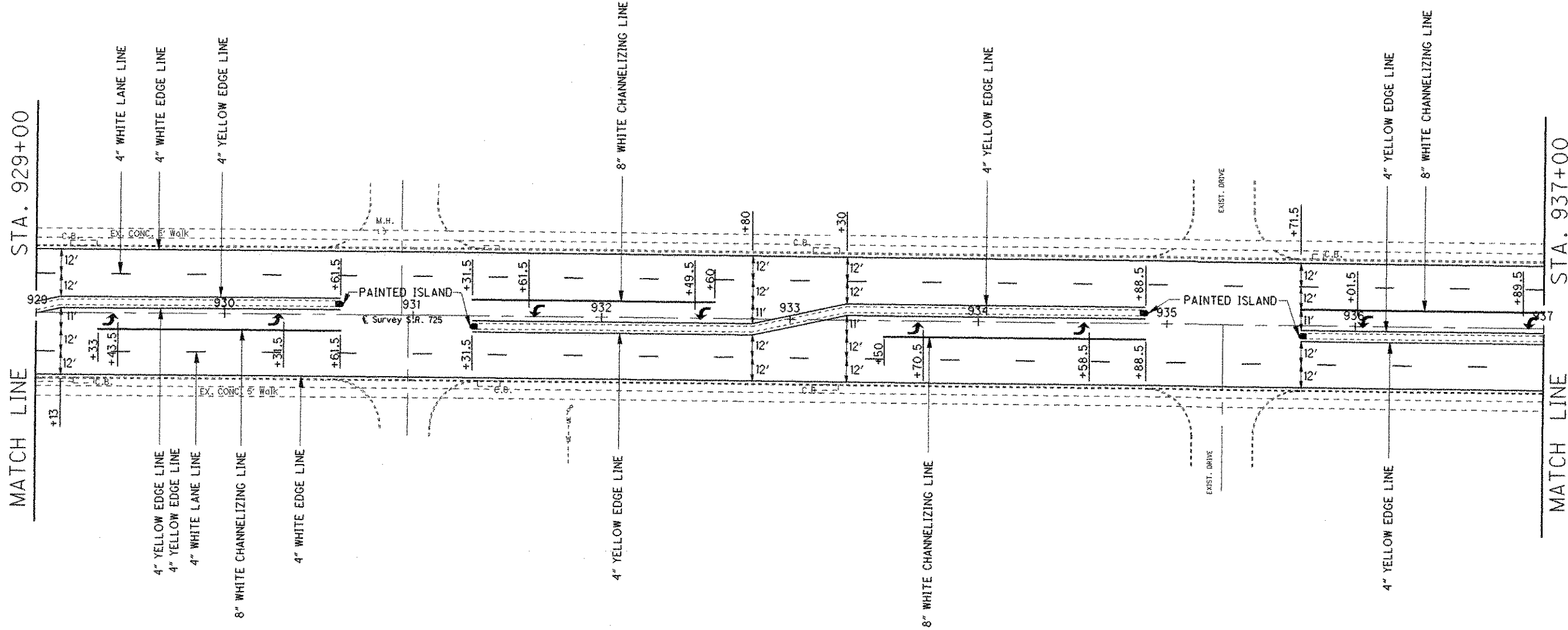
0 15 30 60
HORIZONTAL
SCALE IN FEET

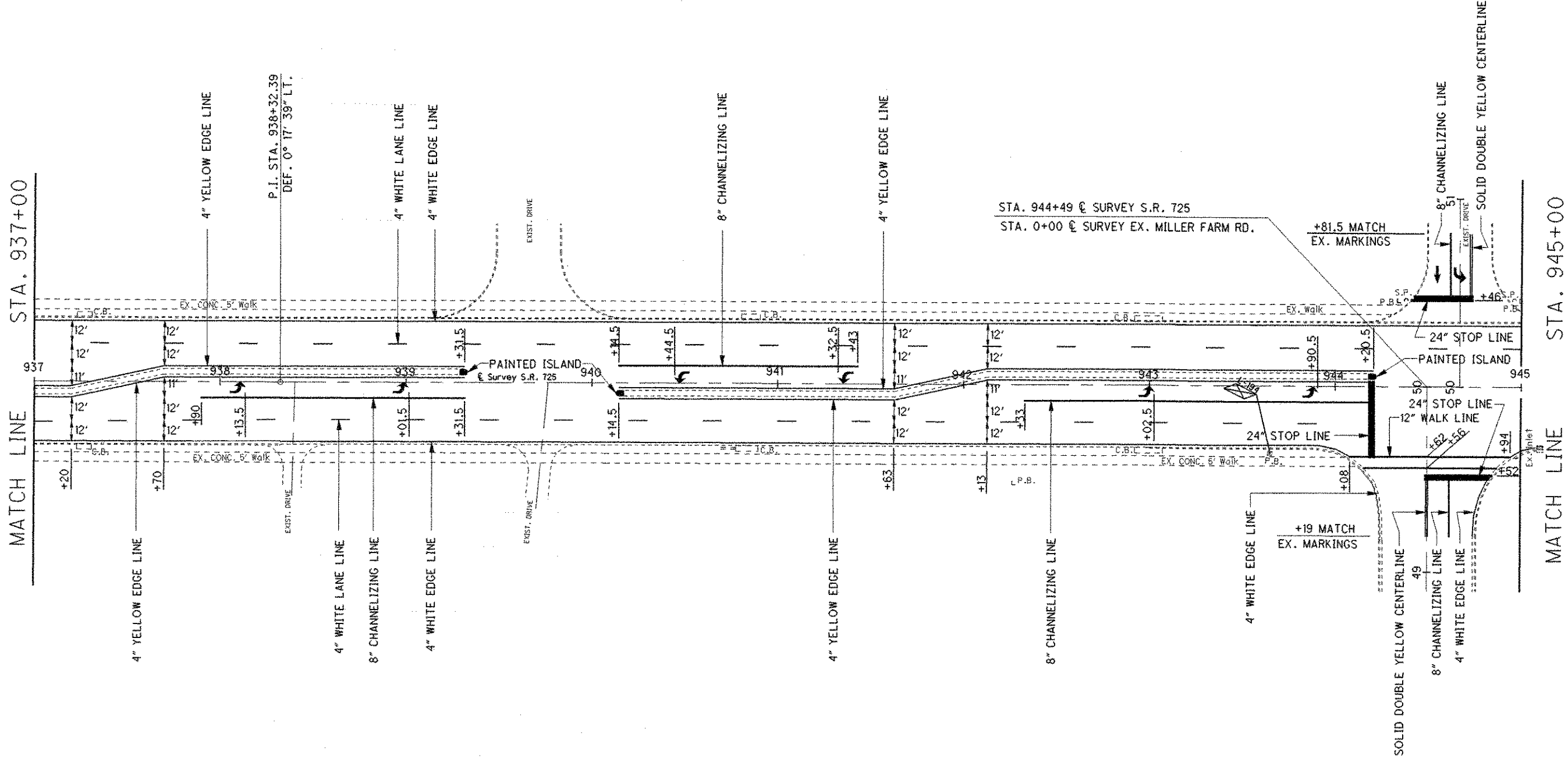
TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 913+00 TO STA. 921+00

MOT-725-13.82



 = ANGULAR DESIGN DETECTION LOOP





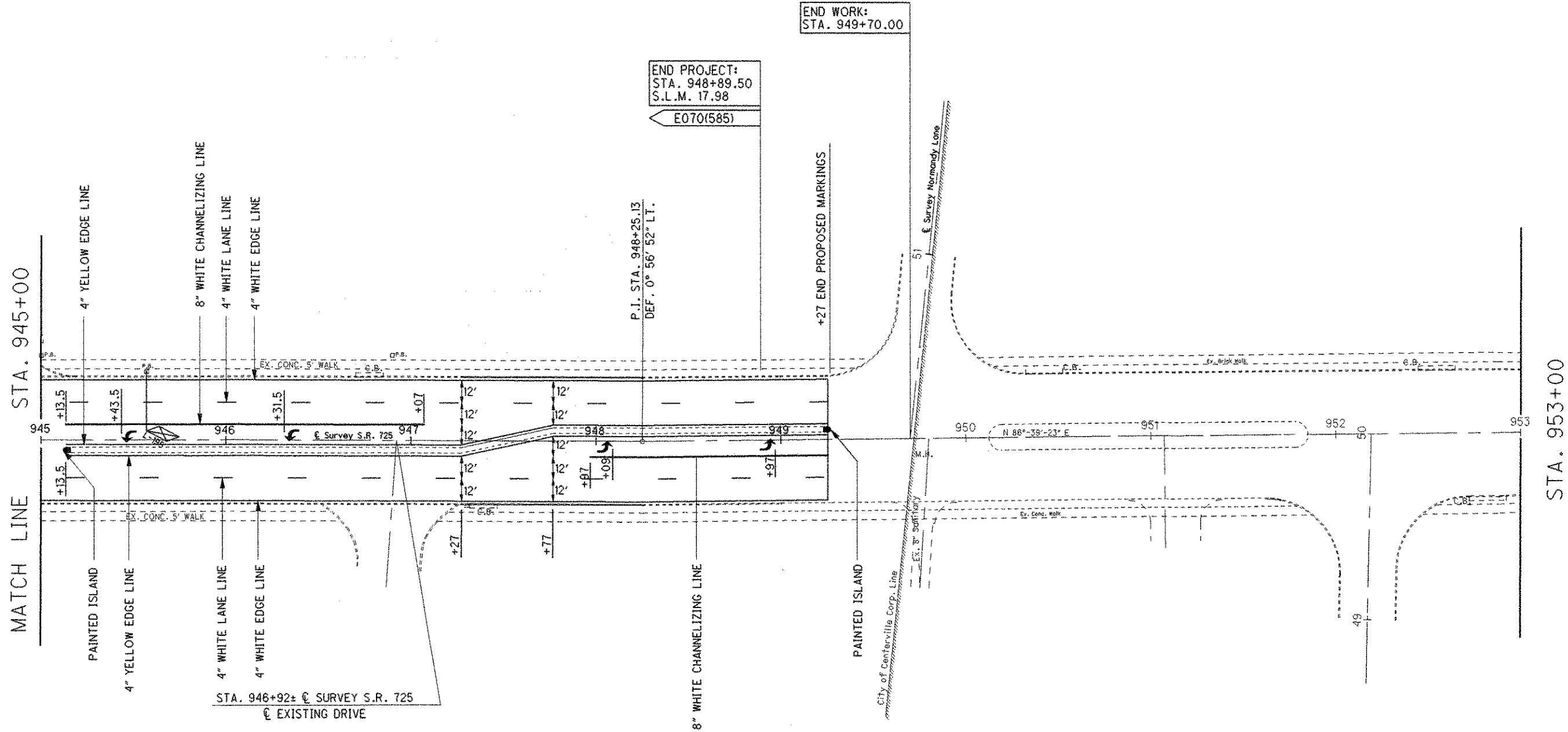
 = ANGULAR DESIGN DETECTION LOOP

CALCULATED
REB
CHECKED

0 30 60
REB
HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET - S.R. 725
STA. 937+00 TO STA. 945+00

MOT - 725 - 13.82



 = ANGULAR DESIGN DETECTION LOOP

END PROJECT:
STA. 948+89.50
S.L.M. 17.98
E070(585)


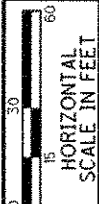
P.I. STA. 948+25.13
DEF. 0° 56' 52" L.T.

END WORK:
STA. 949+70.00

+27 END PROPOSED MARKINGS

STA. 953+00

CALCULATED
REB
CHECKED

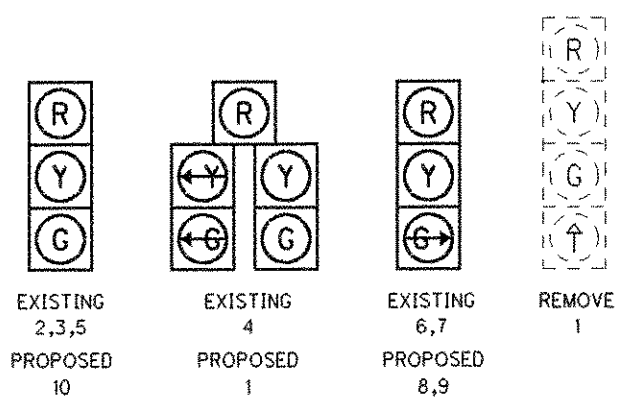
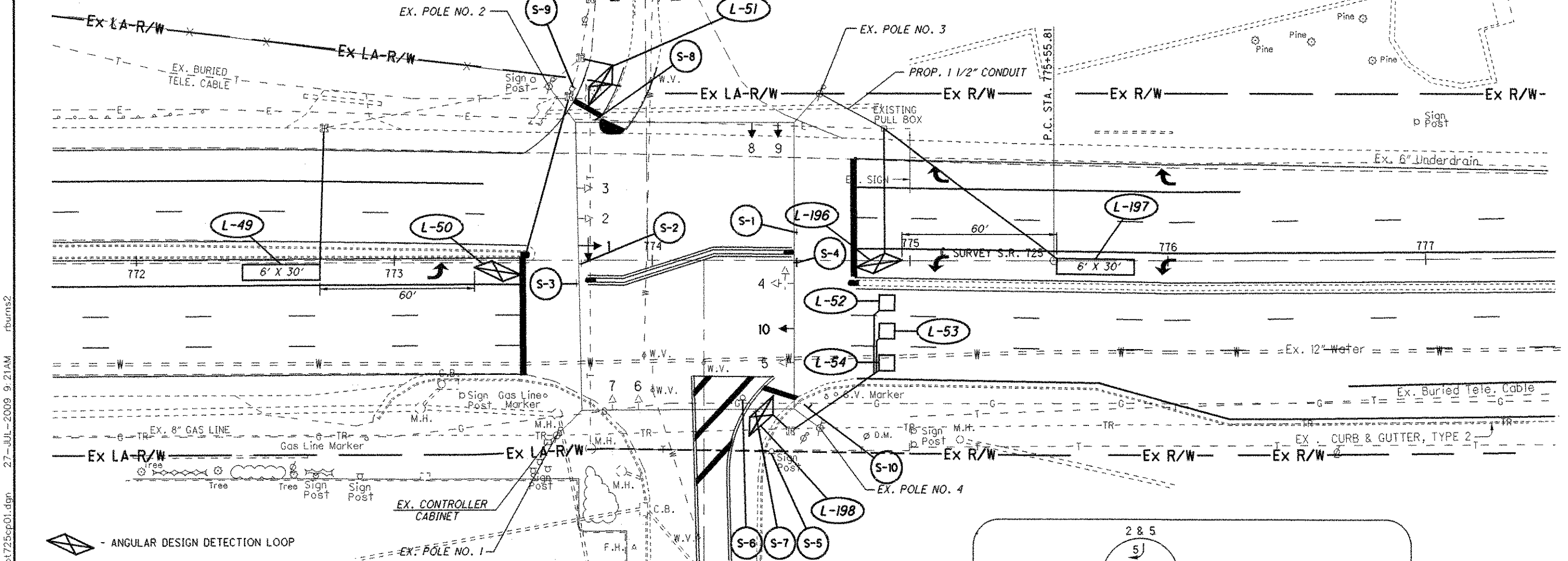
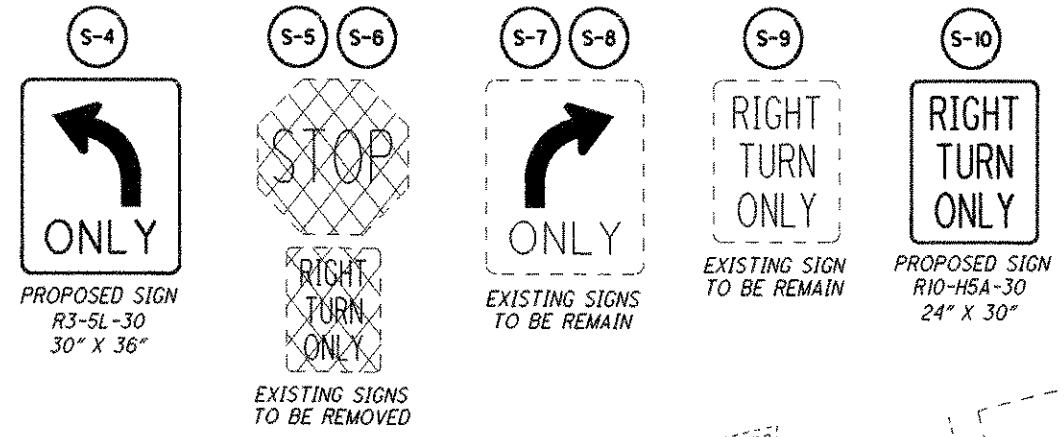
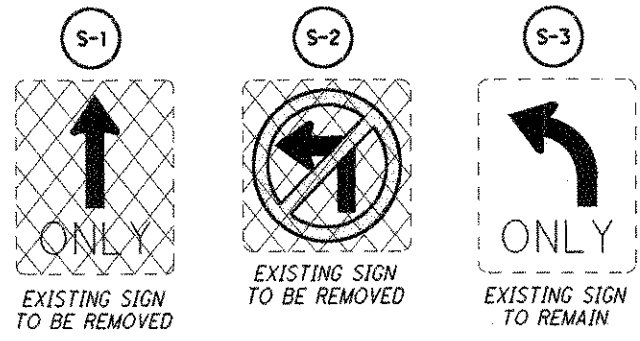



HORIZONTAL
SCALE IN FEET

TRAFFIC CONTROL PLAN SHEET-S.R.725
STA. 945+00 TO STA. 953+00

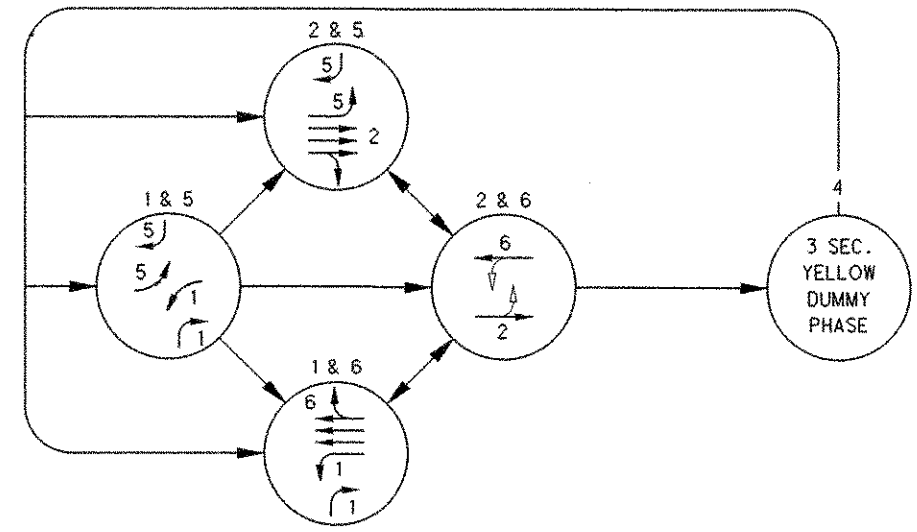
MOT-725-13.82

101A
102



SIGNAL HEADS

FOR LOOP DETECTOR QUANTITIES SEE SHEETS 71-73.
FOR SIGNAL QUANTITIES SEE SHEET 101D.
FOR TRAFFIC SIGNAL DETAILS SEE SHEETS 101C, 101D AND 102



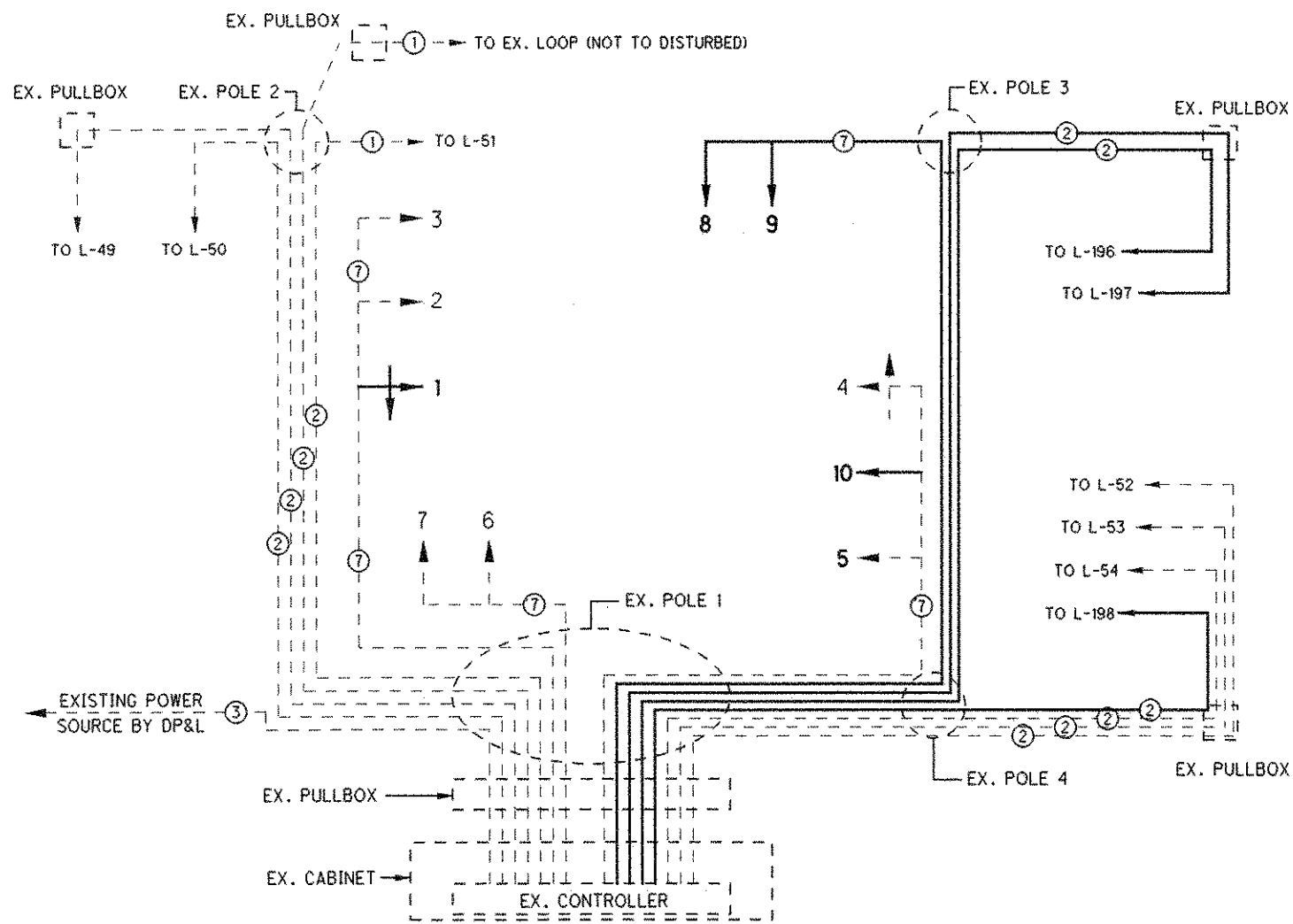
PHASE DIAGRAM

PROJECTS\MOT\725\1362\82505\roadway\sheets\Mot725cp01.dgn 27-JUL-2009 9:21AM rburns2

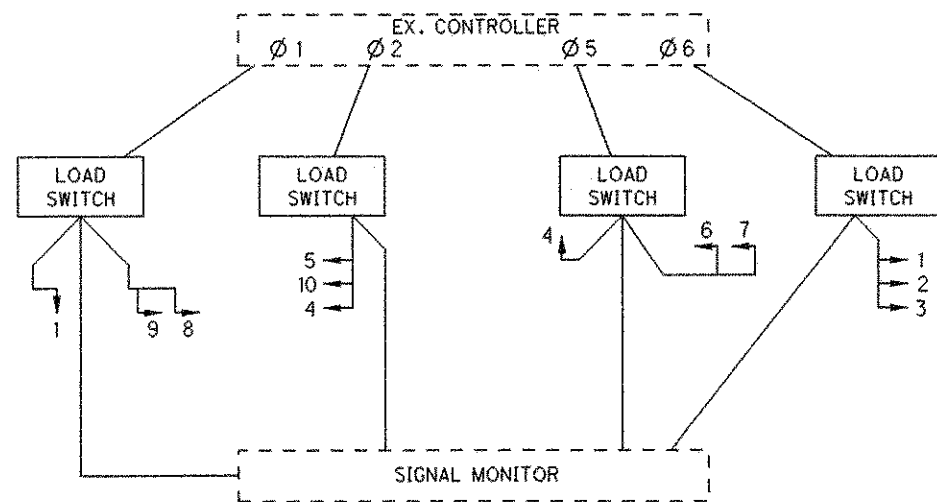
TRAFFIC SIGNAL PLAN
SR-725 / MALL WOODS DRIVE

MOT-725-13.82

101B
102



WIRING DIAGRAM



SIGNAL HOOKUP DIAGRAM

TRAFFIC SIGNAL DETECTORS

DETECTOR NUMBER	SIZE	MODE	DELAY (SEC)	CONNECT TO DETECTOR UNIT #/CHANNEL #	ASSOCIATED CONTROLLER PHASE	DELAY INHIBITED DURING GREEN	EXTENSION (SEC)
L-50	* 18' X 9'	PRESENCE	-	1/1&2	5	-	-
L-49	6' X 30'	PRESENCE	-	2/1&2	5	-	-
L-52	6' X 6'	PULSE	-	3/1	SYSTEM	-	-
L-53	6' X 6'	PULSE	-	3/2	SYSTEM	-	-
L-54	6' X 6'	PULSE	-	4/1	SYSTEM	-	-
EX. LOOP	EXISTING	PRESENCE	-	5/1	5	-	2.5
L-51	* 18' X 9'	PRESENCE	15	5/2	5	Ø 5	-
L-196	* 18' X 9'	PRESENCE	-	6/1	1	-	-
L-197	6' X 30'	PRESENCE	-	6/2	1	-	-
L-198	* 18' X 9'	PRESENCE	15	6/3	1	Ø 1	-

* = ANGULAR DESIGN DETECTION LOOP

**ACTUATED
TRAFFIC SIGNAL CONTROLLER TIMING CHART**

INTERSECTION NAME: SR-725 @ MALL WOODS
MAINTAINING AGENCY: MIAMISBURG

START UP		DUAL ENTRY ●							
START IN: Y/R FLASH ● : ALL RED ○		REST IN RED: RING 1 ○ RING 2 ○							
TIME FOR FLASH OR ALL RED: 10 sec.		OVERLAP							
FIRST PHASE(S) <u>Ø 2 & Ø 6</u>		PHASES							
COLOR DISPLAYED: GREEN ● : YELLOW ○									
INTERVAL OR FEATURE		CONTROLLER MOVEMENT NUMBER							
		1	2	3	4	5	6	7	8
INTERSECTION MOVEMENT									
MINIMUM GREEN (INITIAL) (sec)		8	30		DUMMY PHASE	8	30		
ADDED INITIAL* (sec/ACTUATION)									
PASSAGE TIME (PRESET GAP) (sec)		0.5				0.5			
TIME BEFORE REDUCTION* (sec)									
MINIMUM GAP* (sec)									
TIME TO REDUCE* (sec)									
MAXIMUM GREEN I (sec)		30	60			30	60		
MAXIMUM GREEN II (sec)									
YELLOW CHANGE (sec)		3.0	4.0		3.0	3.0	4.0		
ALL RED CLEARANCE (sec)		2.0	2.0			2.0	2.0		
WALK (sec)									
PEDESTRIAN CLEARANCE (sec)									
RECALL	MAXIMUM (ON/OFF)								
	MINIMUM (ON/OFF)		ON				ON		
	PEDESTRIAN (ON/OFF)								
MEMORY (ON/OFF)									
CALL TO NON-ACTUATED	NO. 1		X				X		
	NO. 2								
FLASH			Y				Y		

* VOLUME DENSITY CONTROLS
FOR CONTROLLER-PROGRAMMED DETECTOR TIMINGS, SEE LOOP DETECTOR CHART.

COORDINATION TIMING NOT INCLUDED IN PROJECT

SIGNAL QUANTITIES				
ITEM	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
625	30	FT	CONDUIT, 1 1/2", 725.04	
630	13	FT	GROUND MOUNTED SUPPORT, NO. 2 POST	
630	12.5	SO. FT.	SIGN, FLAT SHEET	
630	2	EACH	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	
630	2	EACH	REMOVAL OF MOUNTED POST SUPPORT AND DISPOSAL	
630	2	EACH	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	
632	3	EACH	VEHICULAR SIGNAL HEAD, (LED), 3 SECTION, 12" LENS, 1-WAY, AS PER PLAN	102
632	1	EACH	VEHICULAR SIGNAL HEAD, (LED), 5 SECTION, 12" LENS, 1-WAY, AS PER PLAN	102
632	3	EACH	COVERING OF VEHICULAR SIGNAL HEADS	
632	2	EACH	LOOP DETECTOR UNIT, AS PER PLAN	102
632	275	FT	SIGNAL CABLE, 7 CONDUCTOR, NO. 14	
632	640	FT	LOOP DETECTOR LEAD IN CABLE	
633	LUMP		CONTROLLER ITEM, MISC.: EXISTING CONTROLLER AND CABINET MODIFICATION	102

TOTALS CARRIED TO TRAFFIC CONTROL SUMMARY

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**SIGNAL GENERAL SUMMARY AND INTERSECTION TIMINGS
SR-725 / MALL WOODS DRIVE**

MOT-725-13.82

101D
102

ITEM 632- DETECTOR LOOP, AS PER PLAN

DETECTOR LOOPS THROUGHOUT THIS PROJECT SHALL BE INSTALLED INTO THE ITEM 448- ASPHALT CONCRETE INTERMEDIATE COURSE, PRIOR TO THE APPLICATION OF THE ITEM 446 - ASPHALT CONCRETE SURFACE COURSE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTICE AND BE AWARE THAT SOME OF THE DETECTOR LOOPS WITHIN THIS PROJECT SHALL BE THE ANGULAR DESIGN DETECTION LOOP INSTALLATION IN ACCORDANCE WITH STD TC-82.10. LOCATIONS CAN BE FOUND ON SHEETS 71-73 OF THESE PLANS. CONTRACTOR SHALL PERFORM THE DETECTOR LOOP INSTALLATIONS AS A NIGHT TIME OPERATION. ALL WORK, LABOR, EQUIPMENT, INCIDENTALS, AND MATERIALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH FOR ITEM 632 - DETECTOR LOOP, AS PER PLAN.

ITEM 632- LOOP DETECTOR UNIT, AS PER PLAN

THE LOOP AMPLIFIER SHALL BE A CANOGA MODEL C924 OR LATER RACK MOUNT 4 CHANNEL VEHICLE DETECTOR COMPATIBLE WITH A TS2 CABINET.

PAYMENT SHALL BE MADE AT THE CONTRACT PRICE PER EACH

ITEM 632- CONTROLLER MISC.: EXISTING CONTROLLER AND CABINET MODIFICATIONS

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY MODIFICATIONS TO THE CABINET AND EQUIPMENT IN THE CABINET NEEDED TO IMPLEMENT THE PROPOSED CHANGES SHOWN ON THE SIGNAL PLAN. ALL WORK SHALL BE PERFORMED BETWEEN 9:00 AM TO 3:00 PM MONDAY-THURSDAY AND THE SIGNAL MUST STAY IN OPERATION.

PAYMENT SHALL BE MADE AT THE LUMP SUM PRICE BID WHICH SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS AND INCIDENTALS REQUIRED TO COMPLETE THIS ITEM IN A SATISFACTORY AND WORKMANLIKE MANNER.

ITEM 632- VEHICULAR SIGNAL HEAD, (LED), COLOR, BY TYPE, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF CMS 632 AND 732, THE FOLLOWING REQUIREMENTS SHALL ALSO APPLY:

1. ALL UPPER SIGNAL SUPPORT HARDWARE AND PIPING UP TO AND INCLUDING THE WIRE INLET FITTING SHALL BE FERROUS METAL FOR SIGNAL DISPLAYS OF TWO OR MORE SECTIONS.
2. THE ENTRANCE FITTING SHALL BE OF THE TRI-STUD DESIGN WITH SERRATED RINGS IN ORDER TO ACHIEVE POSITIVE LOCKING.

THE DEPARTMENT WILL MEASURE VEHICULAR SIGNAL HEAD, (LED), COLOR, BY TYPE, AS PER PLAN BY THE NUMBER OF COMPLETE UNITS FURNISHED AND INSTALLED, AND WILL INCLUDE ALL SUPPORT AND MOUNTING HARDWARE, DISCONNECT HANGERS, CLOSURE CAPS, DIMMERS, AND LAMPS AS SPECIFIED.

ITEM 647 - PAVEMENT MARKING, MISC.: "MERGE" OR "MERGE ARROW"

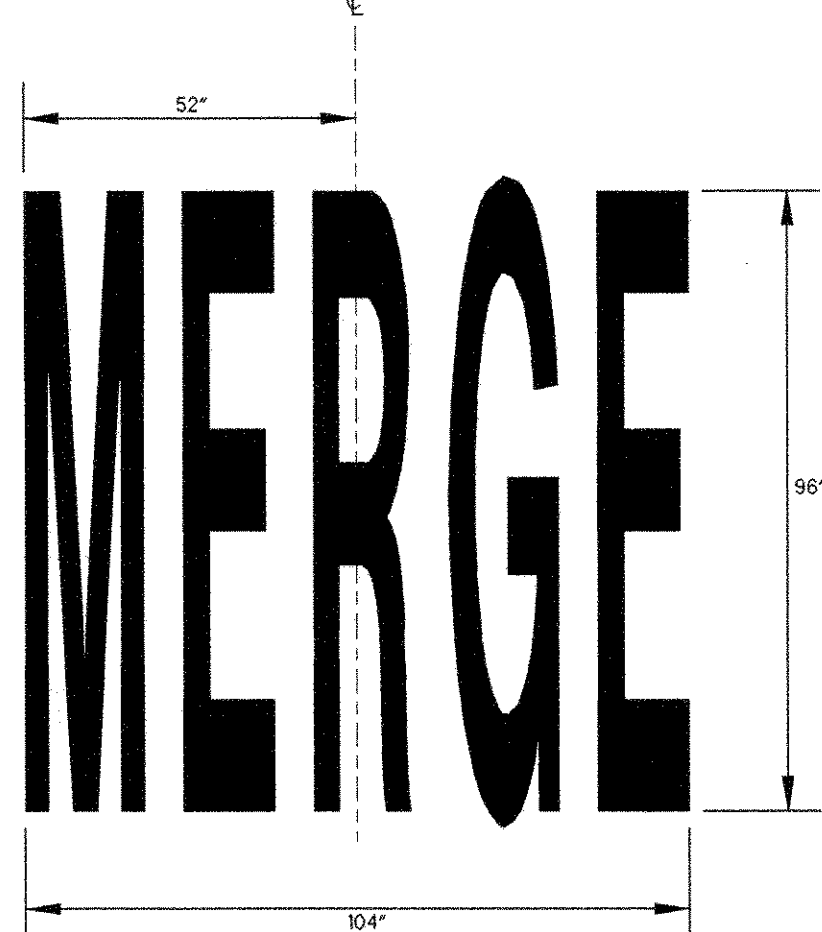
THE WORD ON PAVEMENT "MERGE" AND THE "MERGE ARROW" SHALL BE PLACED PER ITEM 647 - WORD ON PAVEMENT, TYPE A AND TO THE DIMENSIONS SHOWN BELOW. THE WORD ON PAVEMENT "MERGE" SHALL BE CENTERED BETWEEN CONSECUTIVE LANE LINE MARKINGS TO AVOID CONFUSION. THESE TEMPLATES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR WITH THE APPROVAL FROM THE DISTRICT TRAFFIC SAFETY ENGINEER. THE LOCATION (RAMP "D" AT THE IR-75/SR-725 INTERCHANGE) LAYOUT SHALL ALSO BE APPROVED BY THE DISTRICT SAFETY ENGINEER PRIOR TO APPLICATION. THE CONTRACTOR SHALL REMOVE WITH CARE ANY EXISTING MARKINGS THAT MAY CONFLICT WITH THE PERFORMANCE OF THE MERGE AREA, RESULTING IN MINIMAL DAMAGE TO THE EXISTING PAVEMENT WHERE INDICATED IN THESE PLANS. ALL LABOR, TOOLS, MATERIALS AND EQUIPMENT NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR EACH ITEM 647 - PAVEMENT MARKING, MISC.: "MERGE" TYPE A OR ITEM 647 - PAVEMENT MARKING MISC.: MERGE ARROW, TYPE A.

WORD "MERGE" LAYOUT

LETTERS ARE SIXTEEN INCHES (16.0") WIDE WITH A SIX INCH (6.0") SPACE.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE TRAFFIC CONTROL SUMMARY:

ITEM 647, PAVEMENT MARKING, MISC: "MERGE", TYPE A	1 EACH
ITEM 647, PAVEMENT MARKING, MISC: MERGE ARROW, TYPE A	1 EACH



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TRAFFIC CONTROL NOTES

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