

ITEM 614, MAINTAINING TRAFFIC

THIS PROJECT WILL INVOLVE THE CONSTRUCTION OF VARIOUS CURB, CURB RAMPS, WALK, AND ASSOCIATED SIGNAL AND DRAINAGE ITEMS. DURING WORKING HOURS, THERE MAY BE LANE RESTRICTIONS USING DRUMS AND/OR FLAGGERS.

EACH CORNER OF THE INTERSECTION SHALL BE CONSTRUCTED INDIVIDUALLY, MINIMIZING DISTRUPTION TO THE TRAVELING PUBLIC.

NORTHEAST CORNER OF S.R. 78 AND C.R. 56
DURING WORKING HOURS, CLOSE THE WESTBOUND RIGHT TURN LANE AND SOUTH BOUND LEFT TURN LANE USING DRUMS TO ALLOW TRAFFIC TO BE SHIFTED AWAY FROM THE CURB LINE. OPEN LANES TO TRAFFIC AFTER WORKING HOURS. MEET THE OPEN TRENCH REQUIREMENTS SPECIFIED IN THESE PLANS.

NORTHWEST CORNER OF S.R. 78 AND C.R. 56
DURING WORKING HOURS, CLOSE THE SOUTHBOUND LEFT TURN LANE AND SHIFT THE OUTSIDE LANE EAST. CLOSE THE SHOULDER ALONG S.R. 78. CLOSE THE FAIRGROUNDS DRIVEWAY AS APPROVED BY THE ENGINEER. ACCESS TO THE FAIRGROUNDS SHALL BE OPEN TO TRAFFIC DURING THE COUNTY FAIR (AUGUST 26 THROUGH SEPTEMBER 4) AND THE JULY 4TH ACTIVITIES (JUNE 23 THROUGH JULY 5). OPEN LANES TO TRAFFIC AFTER WORKING HOURS. MEET THE OPEN TRENCH REQUIREMENTS SPECIFIED IN THESE PLANS.

SOUTHWEST CORNER OF S.R. 78 AND C.R. 56
DURING WORKING HOURS, CLOSE THE SOUTHERN SHOULDER OF S.R. 78 AND THE NORTHBOUND LEFT TURN LANE ON C.R. 56. SHIFT TRAFIC AWAY FROM THE CURB LINE WITH DRUMS. OPEN LANES TO TRAFFIC AFTER WORKING HOURS. MEET THE OPEN TRENCH REQUIREMENTS SPECIFIED IN THESE PLANS.

SOUTHEAST CORNER OF S.R. 78 AND C.R. 56
DURING WORKING HOURS, CLOSE THE NORTHBOUND THROUGH LANE ALONG C.R. 56, SHIFT TRAFIC AWAY FROM THE CURB LINE WITH DRUMS. OPEN LANES TO TRAFFIC AFTER WORKING HOURS. MEET THE OPEN TRENCH REQUIREMENTS SPECIFIED IN THESE PLANS.

WALK ALONG C.R. 56 AND S.R. 821
WORK ALONG C.R. 56 AND S.R. 821 FOR SIDEWALK CONSTRUCTION SHALL BE DONE OFFROAD, WITH FLAGGERS BEING UTILIZED WHEN LANE RESTRICTIONS ARE REQUIRED. OPEN LANES TO TRAFFIC AFTER WORKING HOURS. MEET THE OPEN TRENCH REQUIREMENTS SPECIFIED IN THESE PLANS.

ITEM 614, MAINTAINING TRAFFIC (LANE CLOSURE/ REDUCTION REQUIRED)

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 3.25 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 0.70 M. GAL.

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

- 614 WORK ZONE CENTERLINE, CLASS I, 740.06, TYPE 1 0.14 MILE
- 614 WORK ZONE EDGE LINE, CLASS I, 4", 740.06, TYPE 1 0.21 MILE
- 614 WORK ZONE STOP LINE, CLASS I, 740.06, TYPE 1 30 FT

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

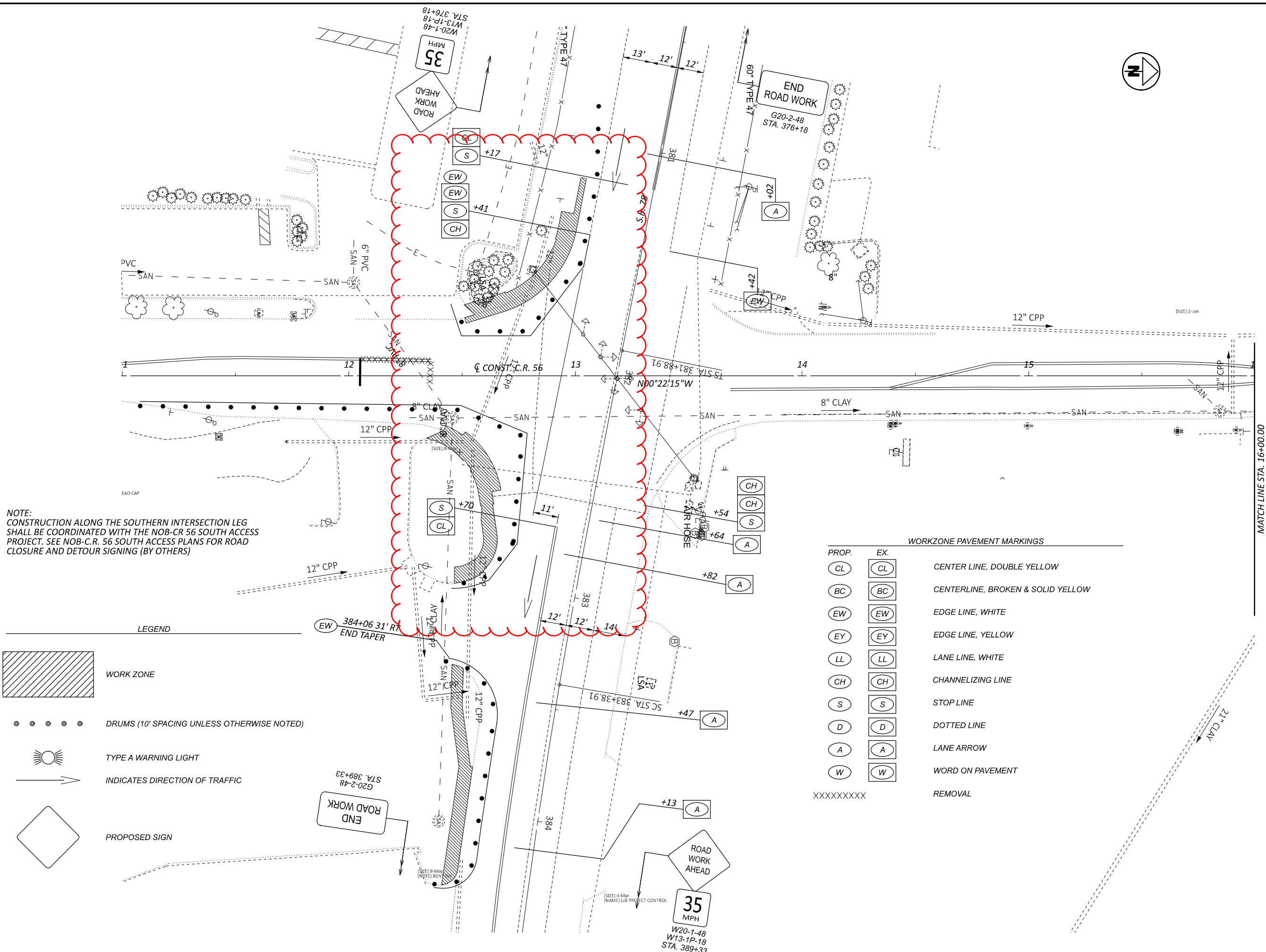
INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.





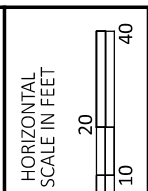
NOTE:
 CONSTRUCTION ALONG THE SOUTHERN INTERSECTION LEG SHALL BE COORDINATED WITH THE NOB-CR 56 SOUTH ACCESS PROJECT. SEE NOB-C.R. 56 SOUTH ACCESS PLANS FOR ROAD CLOSURE AND DETOUR SIGNING (BY OTHERS)

LEGEND

- WORK ZONE
- DRUMS (10' SPACING UNLESS OTHERWISE NOTED)
- TYPE A WARNING LIGHT
- INDICATES DIRECTION OF TRAFFIC
- PROPOSED SIGN

WORKZONE PAVEMENT MARKINGS

PROP.	EX.	
CL	CL	CENTER LINE, DOUBLE YELLOW
BC	BC	CENTERLINE, BROKEN & SOLID YELLOW
EW	EW	EDGE LINE, WHITE
EY	EY	EDGE LINE, YELLOW
LL	LL	LANE LINE, WHITE
CH	CH	CHANNELIZING LINE
S	S	STOP LINE
D	D	DOTTED LINE
A	A	LANE ARROW
W	W	WORD ON PAVEMENT
XXXXXXXXXX		REMOVAL



**MAINTENANCE OF TRAFFIC PLAN
 PHASE 1**

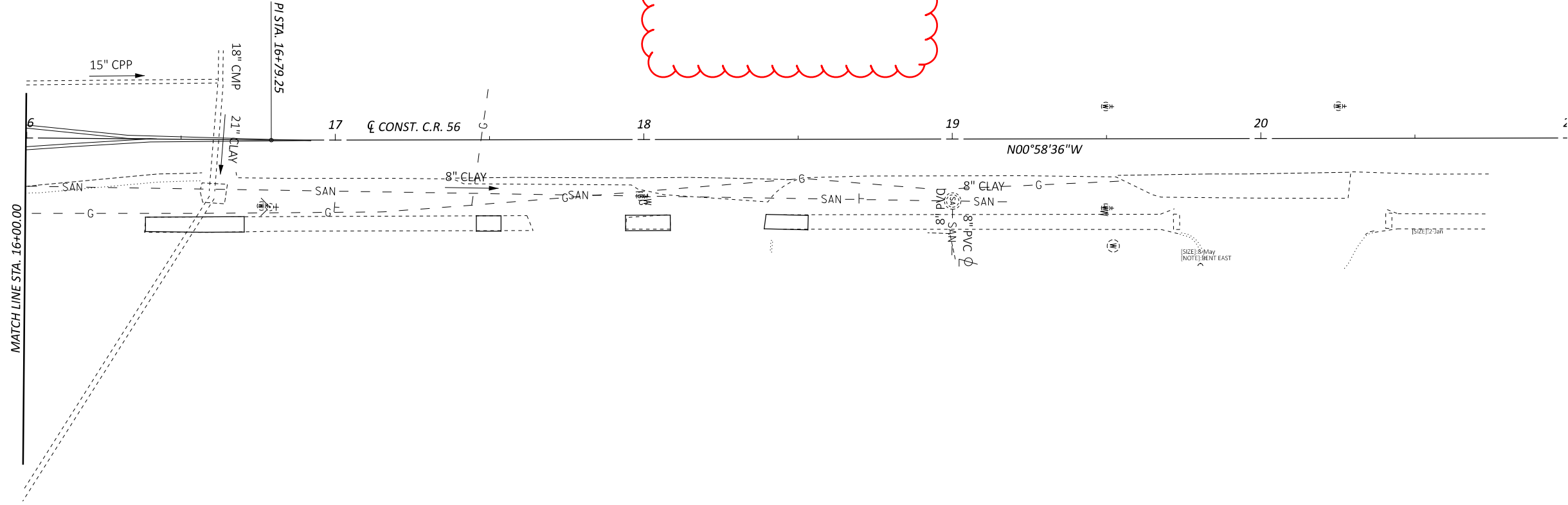
DESIGN AGENCY

DESIGNER
 ARW

REVIEWER
 JRE 06/21/23

PROJECT ID
 117279

SHEET TOTAL
 5 38

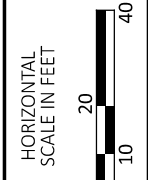


DESIGN AGENCY




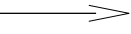
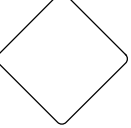


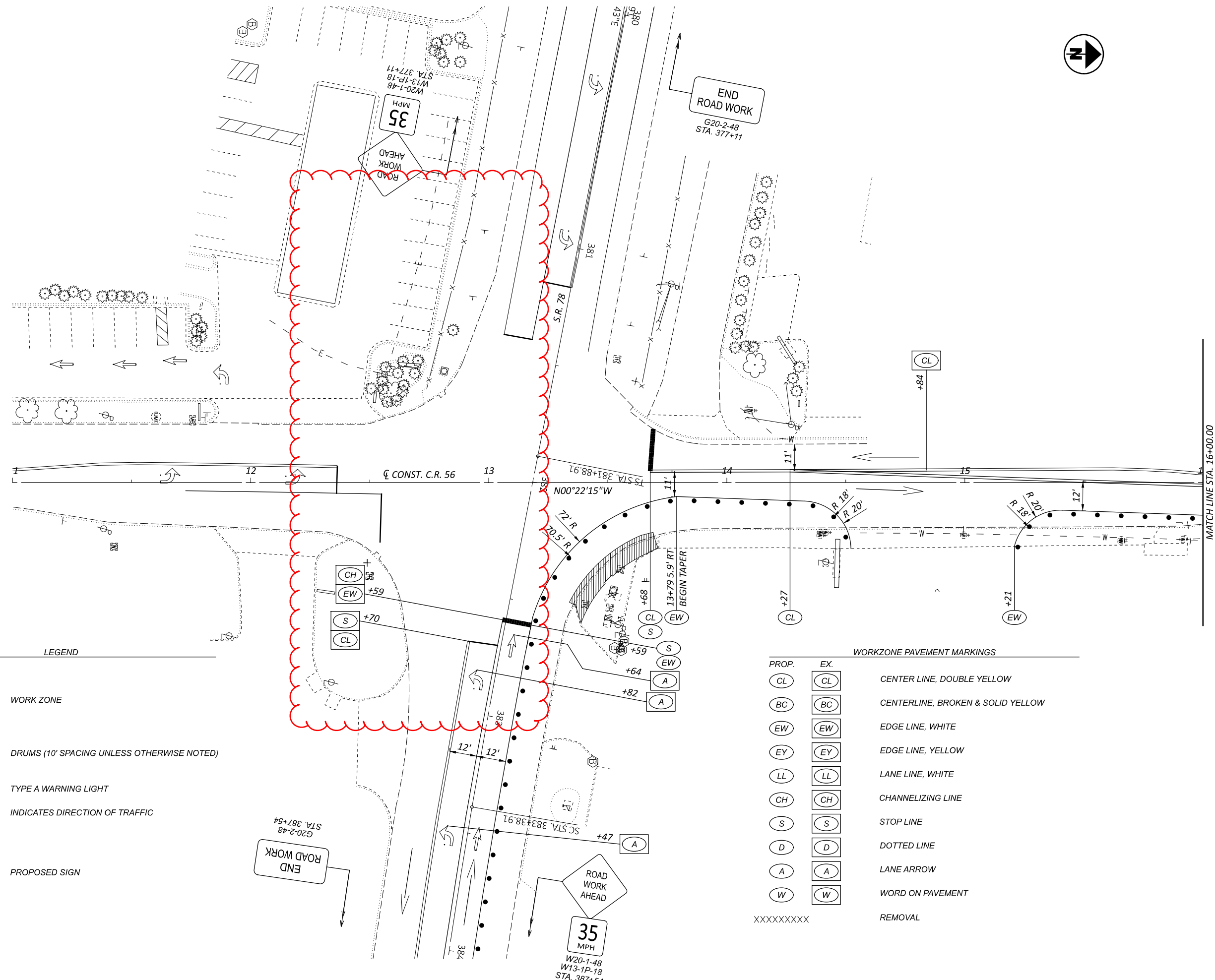
DESIGNER	ARW
REVIEWER	JRE
PROJECT ID	06/21/23
SHEET	117279
TOTAL	38

MAINTENANCE OF TRAFFIC PLAN
 PHASE 1



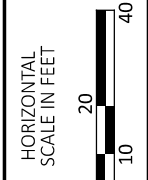
LEGEND

-  WORK ZONE
-  DRUMS (10' SPACING UNLESS OTHERWISE NOTED)
-  TYPE A WARNING LIGHT
-  INDICATES DIRECTION OF TRAFFIC
-  PROPOSED SIGN



WORKZONE PAVEMENT MARKINGS

PROP.	EX.	DESCRIPTION
CL	CL	CENTER LINE, DOUBLE YELLOW
BC	BC	CENTERLINE, BROKEN & SOLID YELLOW
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LL	LL	LANE LINE, WHITE
CH	CH	CHANNELIZING LINE
S	S	STOP LINE
D	D	DOTTED LINE
A	A	LANE ARROW
W	W	WORD ON PAVEMENT
XXXXXXX		REMOVAL

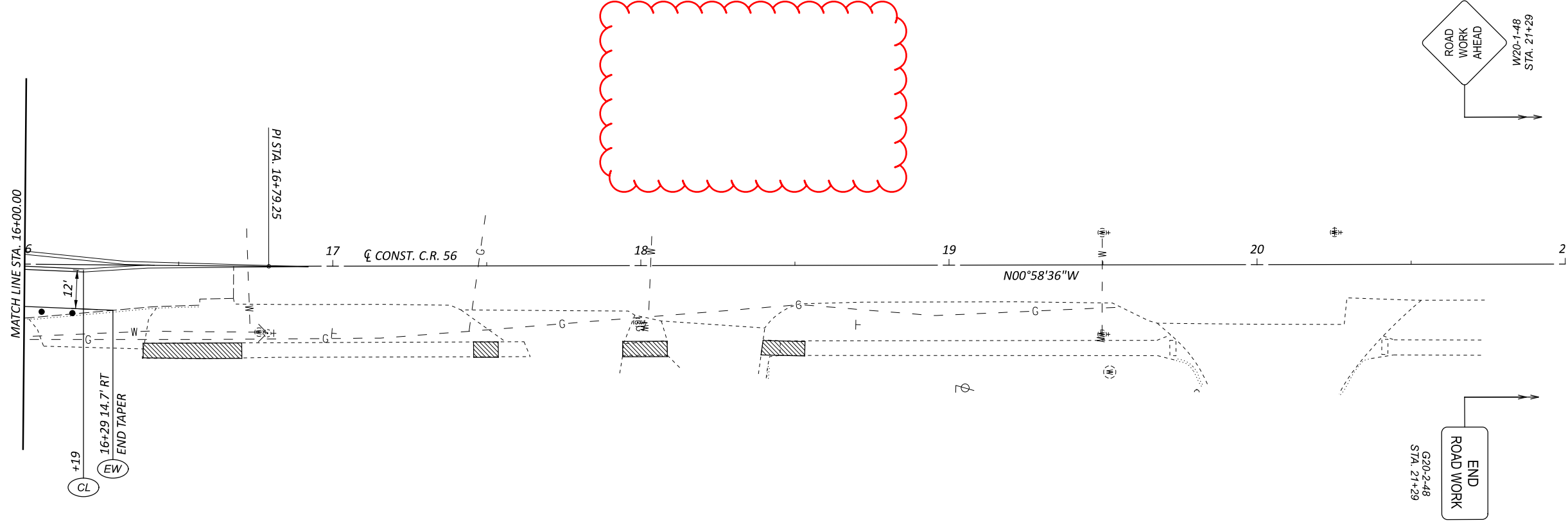


MAINTENANCE OF TRAFFIC PLAN
 PHASE 2

DESIGN AGENCY

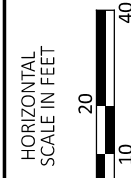


DESIGNER	ARW
REVIEWER	JRE
DATE	06/21/23
PROJECT ID	117279
SHEET	7
TOTAL	38

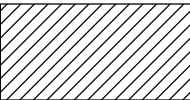



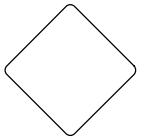


DESIGN AGENCY	
DESIGNER	ARW
REVIEWER	JRE 06/21/23
PROJECT ID	117279
SHEET	TOTAL
8	38





















MAINTENANCE OF TRAFFIC PLAN
PHASE 2

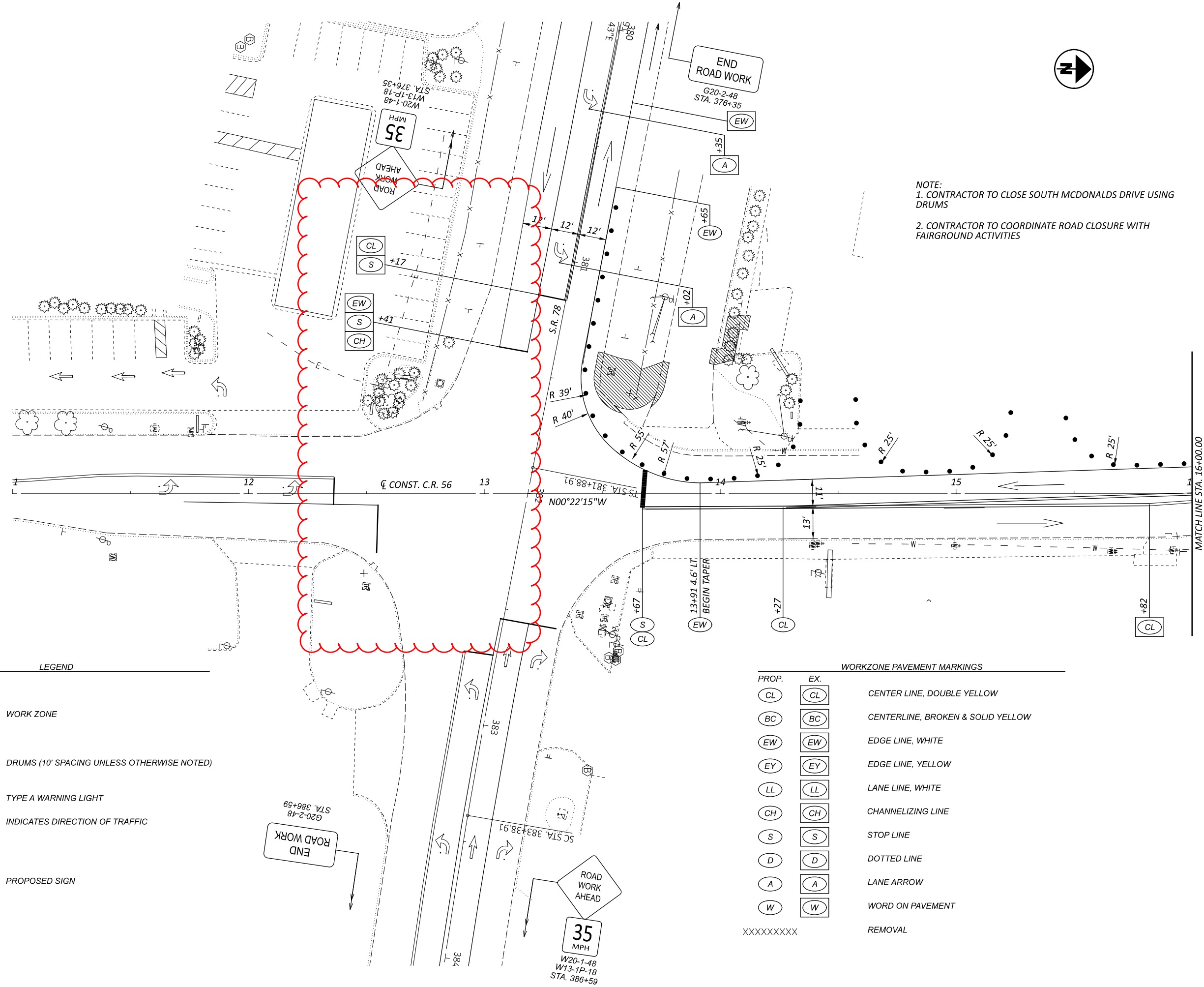


LEGEND

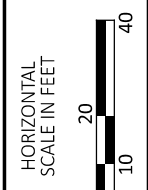
	WORK ZONE
	DRUMS (10' SPACING UNLESS OTHERWISE NOTED)
	TYPE A WARNING LIGHT
	INDICATES DIRECTION OF TRAFFIC
	PROPOSED SIGN

WORKZONE PAVEMENT MARKINGS

PROP.	EX.	
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		LANE LINE, WHITE
		CHANNELIZING LINE
		STOP LINE
		DOTTED LINE
		LANE ARROW
		WORD ON PAVEMENT
XXXXXXXXXX		REMOVAL



NOTE:
 1. CONTRACTOR TO CLOSE SOUTH MCDONALDS DRIVE USING DRUMS
 2. CONTRACTOR TO COORDINATE ROAD CLOSURE WITH FAIRGROUND ACTIVITIES

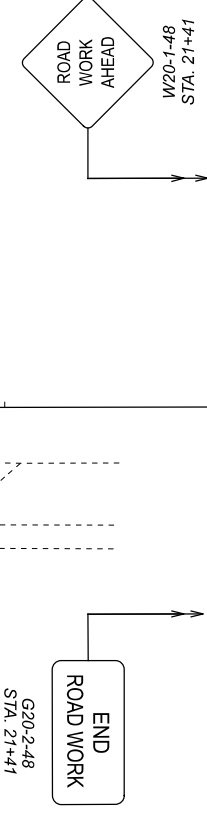
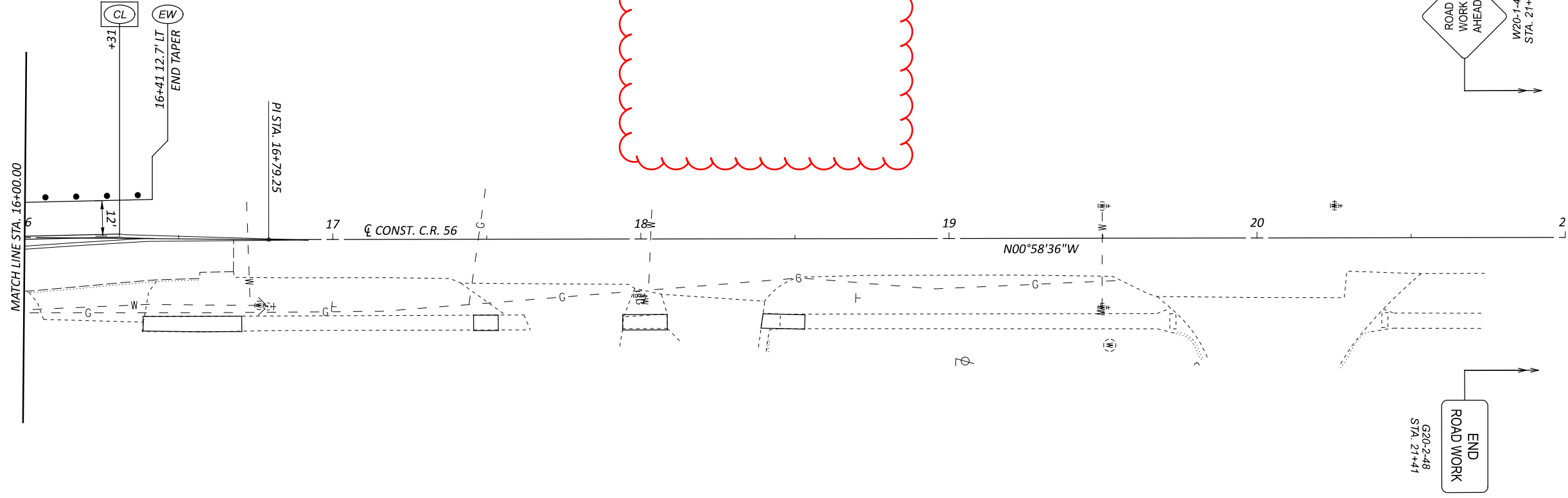


MAINTENANCE OF TRAFFIC PLAN
 PHASE 3

DESIGN AGENCY



DESIGNER	ARW
REVIEWER	JRE
PROJECT ID	117279
SHEET	9
TOTAL	38



MAINTENANCE OF TRAFFIC PLAN
 PHASE 3

DESIGN AGENCY



DESIGNER

ARW

REVIEWER

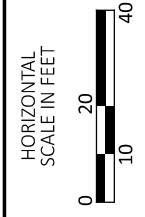
JRE 06/21/23

PROJECT ID

117279


SHEET TOTAL

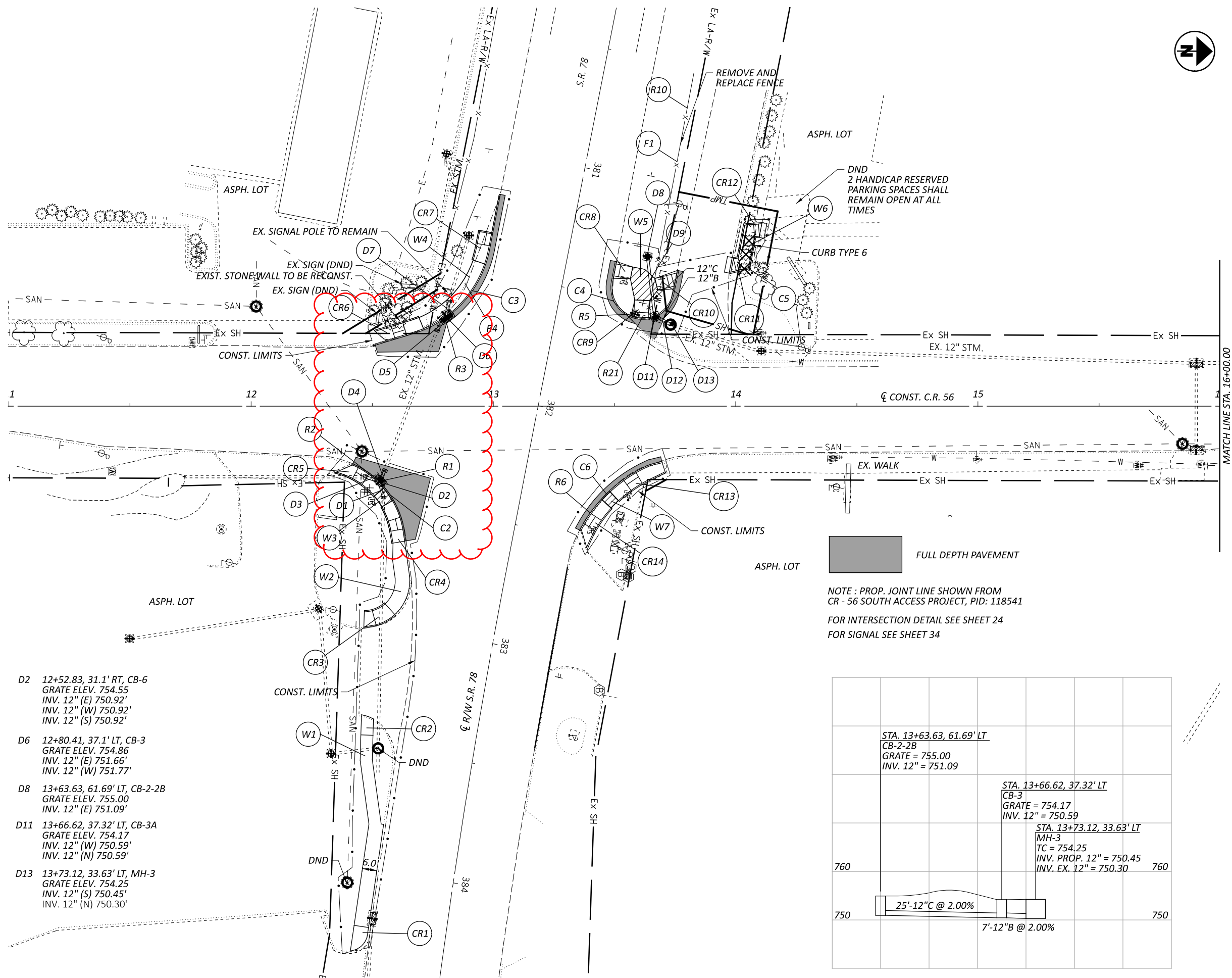
10 38



OFFICE CALCS	SHEET NUM.								PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
	3	4	13	14	15	28	01/SAF/21	02/STR/21	03/SAF/21								
	LS								LS			201	11000	LS		ROADWAY	
				287					84	203	202	23000	287	SY		PAVEMENT REMOVED	
				2,506					333	2,173	202	30000	2,506	SF		WALK REMOVED	
				329					210	119	202	32000	329	FT		CURB REMOVED	
				13					13		202	34900	13	FT		PIPE REMOVED	
				50						50	202	38000	50	FT		GUARDRAIL REMOVED	
				1						1	202	42000	1	EACH		ANCHOR ASSEMBLY REMOVED, TYPE A	
				3					3		202	58100	3	EACH		CATCH BASIN REMOVED	
				290					290		202	75000	290	FT		FENCE REMOVED	
									56		203	10000	56	CY		EXCAVATION	
									125		203	20000	125	CY		EMBANKMENT	
169									134	35	204	10000	169	SY		SUBGRADE COMPACTION	
				25						25	606	15050	25	FT		GUARDRAIL, TYPE MGS	
				1						1	606	26150	1	EACH		ANCHOR ASSEMBLY, MGS TYPE E (MASH 2016)	
									209		607	23000	209	FT		FENCE, TYPE CLT	
				4,109					1,227	2,883	608	10000	4,109	SF		4" CONCRETE WALK	
				179					179		608	15000	179	SF		8" CONCRETE WALK	
				378					733		608	52000	734	SF		CURB RAMP	
									147		608	52001	147	SF		CURB RAMP, AS PER PLAN	3
									20	30	608	53020	50	SF		DETECTABLE WARNING	
									5		623	40500	5	EACH		REFERENCE MONUMENT, TYPE A	
																EROSION CONTROL	
				0.7					0.7		616	10000	0.7	MGAL		WATER	
				50					50		659	00300	50	CY		TOPSOIL	
				444					444		659	10000	444	SY		SEEDING AND MULCHING	
				23					23		659	14000	23	SY		REPAIR SEEDING AND MULCHING	
				23					23		659	15000	23	SY		INTER-SEEDING	
				0.07					0.07		659	20000	0.07	TON		COMMERCIAL FERTILIZER	
				0.1					0.1		659	31000	0.1	ACRE		LIME	
				3					3		659	35000	3	MGAL		WATER	
									5,000		832	30000	5,000	EACH		EROSION CONTROL	
																DRAINAGE	
									0.15		602	20000	0.15	CY		CONCRETE MASONRY	
									19		611	04400	19	FT		12" CONDUIT, TYPE B	
									61	18	611	04600	61	FT		12" CONDUIT, TYPE C	
									1		611	98150	1	EACH		CATCH BASIN, NO. 3	
									2	1	611	98180	2	EACH		CATCH BASIN, NO. 3A	
									1		611	98370	1	EACH		CATCH BASIN, NO. 6	
									1		611	98470	1	EACH		CATCH BASIN, NO. 2-2B	
									2		611	99574	2	EACH		MANHOLE, NO. 3	
																PAVEMENT	
38									30	8	301	56000	38	CY		ASPHALT CONCRETE BASE, PG64-22, (449)	
28									22	6	304	20000	28	CY		AGGREGATE BASE	
20									15	5	407	20000	20	GAL		NON-TRACKING TACK COAT	
16									12	4	441	50000	16	CY		ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	
									269	171	452	12010	269	SY		8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC 1P	
									322	154	609	26000	322	FT		CURB, TYPE 6	

GENERAL SUMMARY

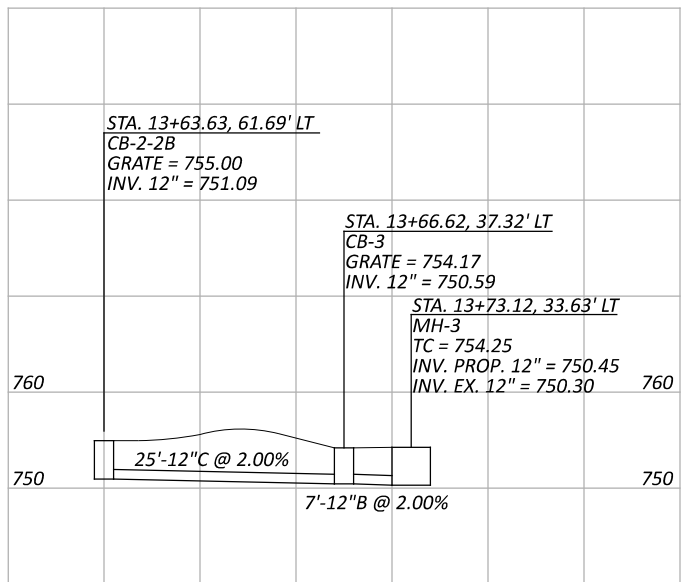
DESIGN AGENCY

 DESIGNER
 ASN
 REVIEWER
 JRE 06/21/23
 PROJECT ID
 117279
 SHEET TOTAL
 11 38



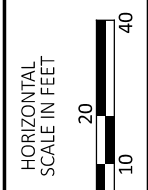
- D2 12+52.83, 31.1' RT, CB-6
 GRATE ELEV. 754.55
 INV. 12" (E) 750.92'
 INV. 12" (W) 750.92'
 INV. 12" (S) 750.92'
- D6 12+80.41, 37.1' LT, CB-3
 GRATE ELEV. 754.86
 INV. 12" (E) 751.66'
 INV. 12" (W) 751.77'
- D8 13+63.63, 61.69' LT, CB-2-2B
 GRATE ELEV. 755.00
 INV. 12" (E) 751.09'
- D11 13+66.62, 37.32' LT, CB-3A
 GRATE ELEV. 754.17
 INV. 12" (W) 750.59'
 INV. 12" (N) 750.59'
- D13 13+73.12, 33.63' LT, MH-3
 GRATE ELEV. 754.25
 INV. 12" (S) 750.45'
 INV. 12" (N) 750.30'

FULL DEPTH PAVEMENT

NOTE : PROP. JOINT LINE SHOWN FROM
 CR - 56 SOUTH ACCESS PROJECT, PID: 118541
 FOR INTERSECTION DETAIL SEE SHEET 24
 FOR SIGNAL SEE SHEET 34



STORM SEWER PROFILE



PLAN C.R. 56
STA. 11+00 TO STA. 16+00

REF NO.	SHEET NO.	STATION TO STATION		625	632	632	632	632	632	632	632	632	632	632	632	632	632	632	632		
		FROM	TO																		
L1	35																				
L2	35																				
L3	35																				
L4	35																				
L5	35																				
PS1	35			1													1		1		
PS2	35			1													1		1		
PS3	35			1													1		1		
PS4	35			1													1		1		
PS5	35			1													1		1		
PS6	35			1													1		1		
	35	2A	2B																		
	35	2B	CONT																		
	35	4A	4B																		
	35	4B	CONT																		
	35	6B/8B	6A																		
	35	6A	CONT																		
	35	6C	CONT																		
	35	6B/8B	8A																		
	35	8A	CONT																		
	35	2A			1				1												
	35	2B			1				1												
	35	4A			1				1												
	35	4B			1				1												
	35	6B/8B				1			1												
	35	6A			1				1												
	35	6C			1				1												
	35	8A			1				1												
	35	PH2A	CONT						1												
	35	PH2B	CONT						1												
	35	PH4A	CONT						1												
	35	PH4B	CONT						1												
	35	PH6A	CONT						1												
	35	PH6B	CONT						1												
	35	PH8A	CONT						1												
	35	PH8B	CONT						1												
	35	PB4A	CONT																		
	35	PB4B	CONT																		
	35	PB8A	CONT																		
	35	PB8B	CONT																		
TOTALS CARRIED TO GENERAL SUMMARY					6	7	1	8	8	8	8	4					1215	635	6	627	6

TRAFFIC SIGNAL
SUBSUMMARY

DESIGN AGENCY

 DESIGNER
 LAS
 REVIEWER
 TVF 06/21/23
 PROJECT ID
 117279
 SHEET TOTAL
 33 38

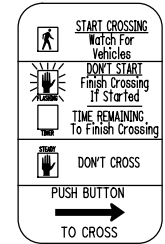
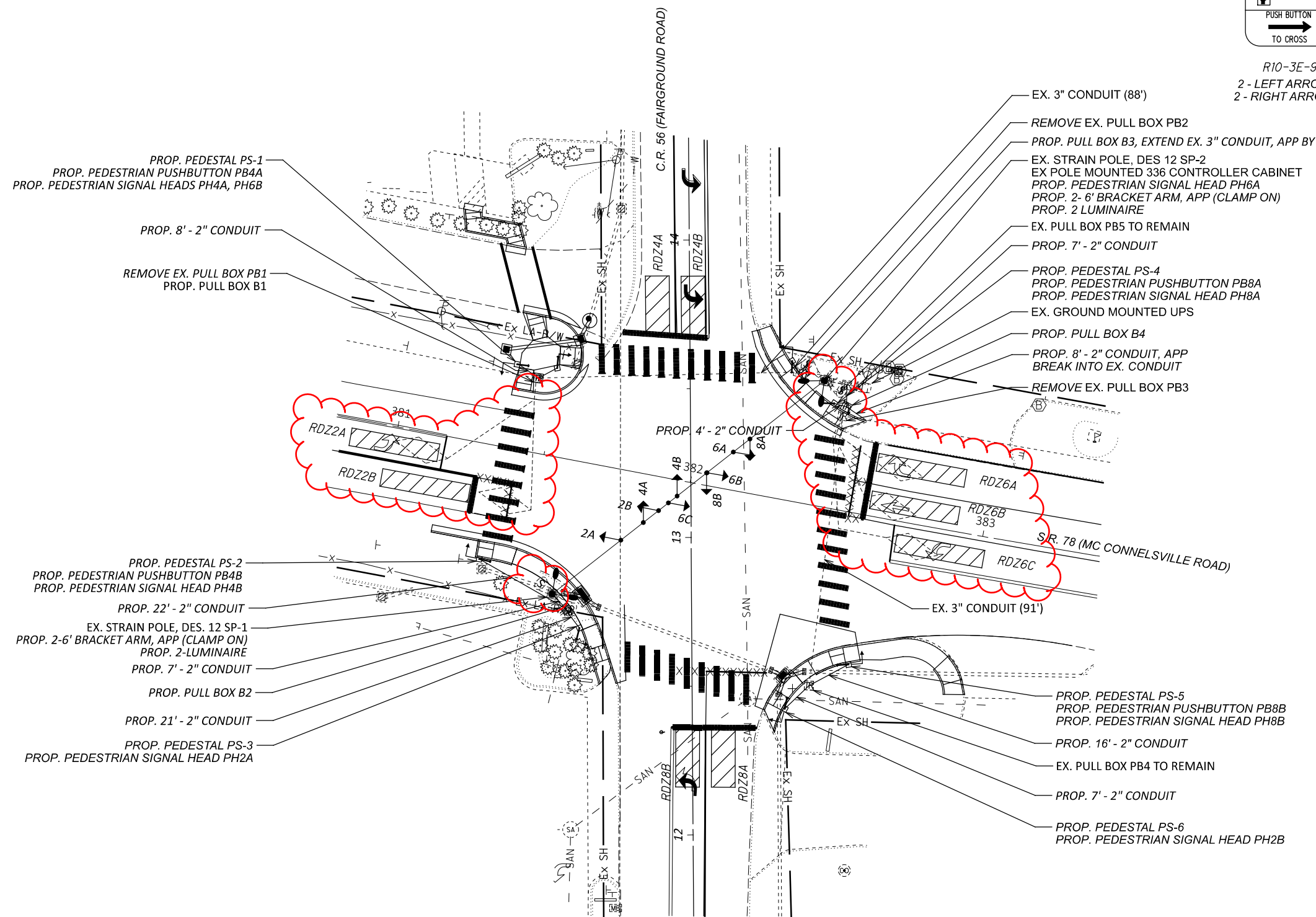
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				EACH	FT	FT	FT	EACH	FT	EACH	EACH	FT	EACH	FT	FT	FT	FT	EACH	FT	EACH	FT	EACH	FT				
			TO																								
NW	35	EX PB1																									
	35	B1																									
	35	PS1	EX PB1		8					8																	
SW	35	B2																									
	35	PS2	B2		22					22																	
	35	PS3	B2		21					21																	
	35	B2	SP1		7					7																	
NE	35	EX PB2																									
	35	B3																									
	35	B4				8				8																	
	35	B4	EX PB5		7					7																	
	35	PS4	B4		4					4																	
	35	EX PB3																									
SE	35	PS5	EX PB4		16					16																	
	35	PS6	EX PB4		7					7																	
	35	SP1	POWER SERVICE																								
	35	SP2	POWER SERVICE												1												
	35	SP1	SP2												117		117										
	35	CONTROLLER																									
	35	SP-2			2					2																	
	35	SP-1			2					2																	
TOTALS CARRIED TO GENERAL SUMMARY					4	92	8			4	100	4	3	100	1	117	117	276	20	1	117	1					

196
80

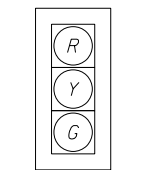
TRAFFIC SIGNAL
SUBSUMMARY

DESIGN AGENCY

 DESIGNER: LAS
 REVIEWER: TVF
 PROJECT ID: 117279
 SHEET: 34 TOTAL: 38



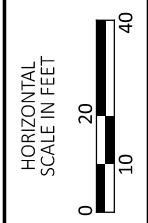
R10-3E-9
 2 - LEFT ARROWS
 2 - RIGHT ARROWS



PROPOSED 12" W/BACKPLATES
 2A, 2B, 4A 4B
 6A, 6B, 6C
 8A, 8B



PROPOSED D2 (COUNTDOWN)
 PH2A PH2B
 PH4A PH4B
 PH6A PH6B
 PH8A PH8B



PULL BOX #	STATION	SIDE	OFFSET	SIZE (IN.)	SIZE (IN.)
EX PB 1	381+45	LT	25	18	REMOVE
EX PB 2	382+22	LT	45	18	REMOVE
EX PB 5	382+43	LT	45	18	REMAIN
EX PB 3	382+42	LT	33	18	REMOVE
EX PB 4	382+50	RT	60	18	REMAIN
B1	381+35.5	LT	25	18	PROP
B2	381+67	RT	49.5	18	PROP
B3	382+31	LT	43.5	18	PROP
B4	382+47	LT	35	18	PROP

PEDESTAL #	STATION	SIDE	OFFSET	HEIGHT
PS-1	381+35.5	LT	28	11
PS-2	381+35	RT	38.5	11
PS-3	381+72.5	RT	55	11
PS-4	382+43	LT	35	11
PS-5	382+64	RT	50	11
PS-6	382+45.5	RT	69	11

LEGEND		PROP	EXIST
TRAFFIC SIGNAL 3 UNIT 12"			
SIGNAL SUPPORT POLE			
PEDESTRIAN HEAD			
PEDESTRIAN PUSH BUTTON			
PEDESTAL SUPPORT			
LUMINAIRE, CONVENTIONAL			
CONTROLLER CABINET POLE MOUNTED (336)			
CONTROLLER CABINET AND WORK PAD (TS-2)			
TRAFFIC PULL BOX			
STOP-LINE RADAR DETECTION UNIT			
DETECTOR LOOP			
DETECTION ZONE			

TRAFFIC SIGNAL PLAN
 SR 78 (MCCONNELLSVILLE RD) & CR 56 (FAIRGROUNDS RD.)

DESIGN AGENCY

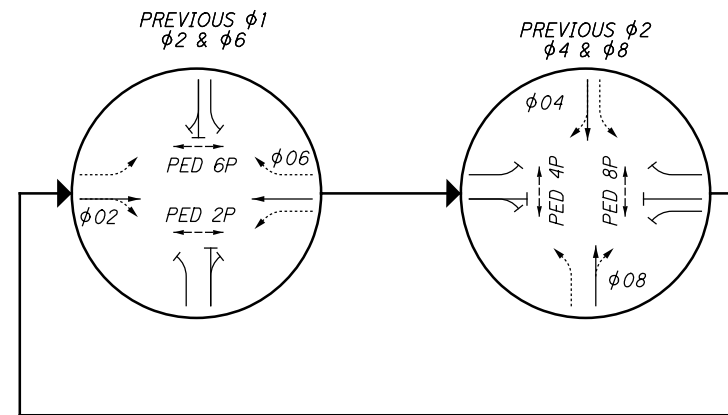
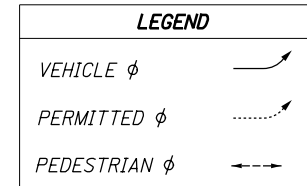
 DESIGNER
 LAS
 REVIEWER
 TVF 06/21/23
 PROJECT ID
 117279
 SHEET TOTAL
 35 38

SIGNAL TIMING CHART

INTERSECTION: SR 78 (MCCONNELSVILLE RD) & CR 56 (FAIRGROUNDS RD)									
MAINTAINING AGENCY: ODOT DISTRICT 10									
START UP	DUAL ENTRY:		YES		PHASES:				
	REST IN RED:		RING 1		RING 2				
START IN:	ALL-RED FLASH				2&6, 4&8				
TIME FOR: FLASH, ALL RED (SEC.):	9, 6		OVERLAP		A	B	C	D	
FIRST PHASE(S):	2 & 6		PHASES		-	-	-	-	
COLOR DISPLAYED:	GREEN								
INTERVAL OR FEATURE	CONTROLLER MOVEMENT NO.								
INTERSECTION MOVEMENT (PHASE)	1	2	3	4	5	6	7	8	
DIRECTION	-	EB	-	SB	-	WB	-	NB	
MINIMUM GREEN (INITIAL) (SEC.)	-	20	-	10	-	20	-	10	
ADDED INITIAL *(SEC./ACTUATION)	-	1.5	-	-	-	1.5	-	-	
MAXIMUM INITIAL *(SEC.)	-	30	-	-	-	30	-	-	
PASSAGE TIME (PRESET GAP) (SEC.)	-	-	-	-	-	-	-	-	
TIME BEFORE REDUCTION *(SEC.)	-	30	-	-	-	30	-	-	
MINIMUM GAP *(SEC.)	-	-	-	-	-	-	-	-	
TIME TO REDUCE *(SEC.)	-	10	-	-	-	10	-	-	
MAXIMUM GREEN I (SEC.)	-	40	-	35	-	40	-	35	
MAXIMUM GREEN II (SEC.)	-	40	-	35	-	40	-	35	
YELLOW CHANGE (SEC.)	-	5.1	-	4.3	-	5.1	-	4.3	
ALL RED CLEARANCE (SEC.)	-	1	-	1.4	-	1	-	1.4	
DELAYED GREEN (LPI) (SEC.)	-	6	-	6	-	6	-	6	
FLASHING YELLOW ARROW DELAY^ (SEC.)	-	-	-	-	-	-	-	-	
WALK (SEC.)	-	8	-	9	-	9	-	9	
PEDESTRIAN CLEARANCE (SEC.)	-	14	-	12	-	16	-	19	
RECALL	MAXIMUM (ON/OFF)	-	NO	-	NO	-	NO	-	NO
	MINIMUM (ON/OFF)	-	YES	-	NO	-	YES	-	NO
	PEDESTRIAN (ON/OFF)	-	YES	-	NO	-	YES	-	NO
MEMORY (ON/OFF)	-	NO	-	NO	-	NO	-	NO	

*VOLUME DENSITY CONTROLS
 # FOR CROSSINGS WITH PEDESTRIAN PUSHBUTTONS, LPI'S (LEADING PEDESTRIAN INTERVALS) MAY BE IMPLEMENTED (3-6 SEC.) IN ACCORDANCE WITH LPI DURATION TIME PER THE ODOT SIGNAL CALCULATIONS - CLEARANCE INTERVALS SPREADSHEET
 ^ WHEN IMPLEMENTING FYA, A MINIMUM 3 SEC. DELAY SHALL BE PROGRAMMED PER FYA PHASE.

PHASING DIAGRAM



NOTES:

- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- FOR PROTECTED/PERMISSIVE PHASES, IMPLEMENT CALL OMITTS TO AVOID YELLOW BALL TRAP.
- ENABLE phi 1, 3 & phi 5, 7 DETECTOR SWITCHING TO ALLOW phi 1 & phi 5 TO EXTEND phi 2 & phi 6 OR phi 3 & phi 7 TO EXTEND phi 4 & phi 8, RESPECTIVELY, WHEN ALLOCATED GREEN TIME FOR LEFT TURN PHASES ARE EXHAUSTED.
- COUNTDOWN PEDESTRIAN SIGNALS SHALL GO TO ZERO ON YELLOW PER ODOTCD FIGURE 4E-2.
- RADAR DETECTION UNITS FOR DILEMMA ZONE DETECTION SHALL PLACE A CONSTANT CALL TO THE CONTROLLER WHEN VEHICLES TRAVEL TIMES TO THE STOP BAR ARE BETWEEN 2.5 AND 6 SECONDS. SPEED TRIGGER SHALL BE SET FOR VEHICLES TRAVELING 35 MPH AND GREATER.
- RADAR SHALL HAVE QUEUE DETECTION CONFIGURED AND A ZONE PLACED AT 100-200 FEET FROM STOP BAR FOR SLOW MOVING VEHICLE EXTENSIONS. SPEED TRIGGER SHALL BE SET AT 1-35 MPH.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.
- FOR ANY ENTRY TO FLASHING OPERATION, PROGRAMMING SHALL RUN MINOR STREET GREEN (TYP. phi 4 & phi 8), ALL-RED CLEARANCE, AND THEN FLASHING OPERATION.

Radar Detection Chart

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY PROGRAMMED IN CONTROLLER (SEC.)	EXTENSION PROGRAMMED IN CONTROLLER (SEC.)	DETECTOR NO.	PURPOSE	DETECTION ZONE LENGTH (FT)
-	-	-	-	-	-	-	-	-
RDZ2A	EB LT	PRESENCE	2	-	-	-	STOP-LINE	EX
RDZ2B	EB T-R	PRESENCE	2	5	-	-	STOP-LINE	EX
RDZ4A	SB T-R	PRESENCE	4	5	-	-	STOP-LINE	20
RDZ4B	SB L	PRESENCE	4	-	-	-	STOP-LINE	20
RDZ6A	WB RT	PRESENCE	6	5	-	-	STOP-LINE	EX
RDZ6B	WB T	PRESENCE	6	-	-	-	STOP-LINE	EX
RDZ6C	WB LT	PRESENCE	6	-	-	-	STOP-LINE	EX
RDZ8A	NB T-R	PRESENCE	8	5	-	-	STOP-LINE	20
RDZ8B	NB L	PRESENCE	8	-	-	-	STOP-LINE	20
-	-	-	-	-	-	-	-	-

Note: Advance/Dilemma Zone Speed Threshold: 30 MPH
 Purpose: Stop-Line or Advance Detection

NOB-78-7.15

MODEL: Sheet_SurvFt_PAPER: 17x11 (in.) DATE: 1/31/2024 TIME: 12:56:21 PM USER: lsack
 C:\ODOT_OpV\119134A.00 - STV-Safety Dsgn 2020 PID112342\0009 NOB-78-7.15 Safety PID112342\117279\117279_0001.dgn

TRAFFIC SIGNAL DETAILS
 SR 78 (MCCONNELSVILLE RD) & CR 56 (FAIRGROUNDS RD.)

DESIGN AGENCY

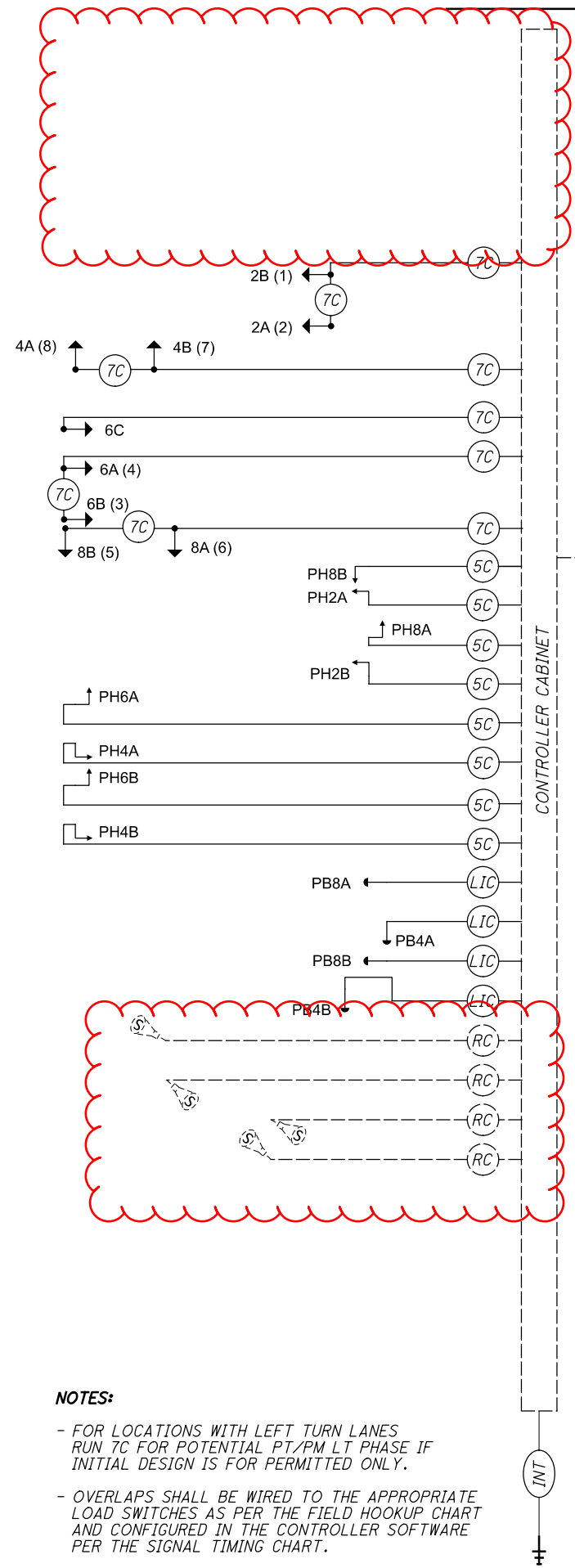
DESIGNER
 LAS

REVIEWER
 TVF 06/21/23

PROJECT ID
 117279

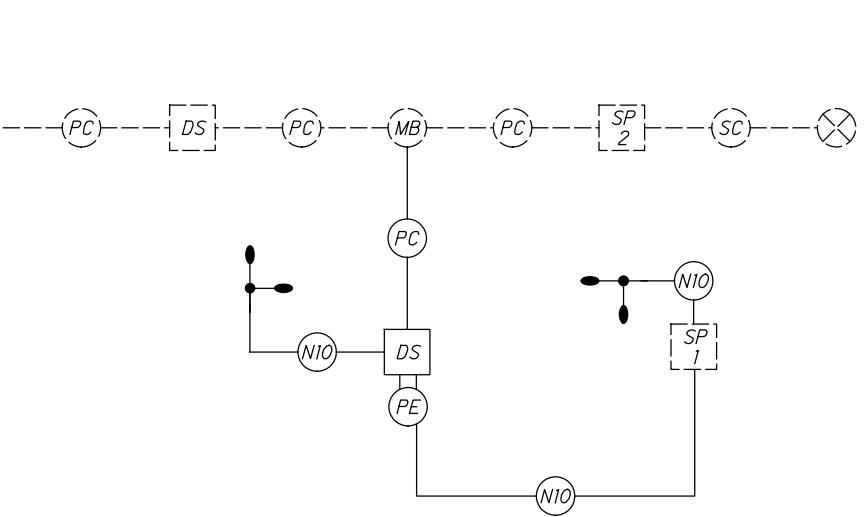
SHEET TOTAL
 36 | 38

WIRING DIAGRAM



"6A (4)" 6A ~ CURRENT SIGNAL HEAD DESIGNATOR
 (4) ~ ORIGINAL SIGNAL HEAD DESIGNATOR

UNINTERRUPTIBLE
 POWER SUPPLY



Field Wiring Hook-up Chart

SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH	SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH
2A 2B (EB)	R	PH 2 RED	R	8A 8B (NB)	R	PH 8 RED	R
	Y	PH 2 YELLOW			Y	PH 8 YELLOW	
	G	PH 2 GREEN			G	PH 8 GREEN	
-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-
4A 4B (SB)	R	PH 4 RED	R	-	-	-	-
	Y	PH 4 YELLOW		-	-	-	-
	G	PH 4 GREEN		-	-	-	-
-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-
6A 6B 6C (WB)	R	PH 6 RED	R	-	-	-	-
	Y	PH 6 YELLOW		-	-	-	-
	G	PH 6 GREEN		-	-	-	-
-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-
PEDESTRIAN MOVEMENTS							
-	-	-	-	PH2 A B SOUTH	DW	PH 2 DONT WALK	OUT
-	-	-	-	-	W	PH 2 WALK	-
-	-	-	-	PH4 A B WEST	DW	PH 4 DONT WALK	OUT
-	-	-	-	-	W	PH 4 WALK	-
-	-	-	-	PH6 A B NORTH	DW	PH 6 DONT WALK	OUT
-	-	-	-	-	W	PH 6 WALK	-
-	-	-	-	PH8 A B EAST	DW	PH 8 DONT WALK	OUT
-	-	-	-	-	W	PH 8 WALK	-
OVERLAPS							
-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-

LS = LOAD SWITCH

LEGEND

	LUMINAIRE, CONVENTIONAL		2/C NO. 14 AWG (LEAD-IN CABLE)		SERVICE CABLE, 3 CONDUCTOR, NO. 6 AWG
	TRAFFIC SIGNAL, 3 UNIT, 12"		VEHICLE LOOP DETECTOR		POWER CABLE, 2 CONDUCTOR, NO. 6 AWG
	PEDESTRIAN SIGNAL		SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG		SIGNAL SUPPORT POLE NO. ...
	PEDESTRIAN PUSH BUTTON		SIGNAL CABLE, 7 CONDUCTOR, NO. 14 AWG		METER BASE
	STOP LINE RADAR DETECTION UNIT		RADAR DETECTION CABLE		NO. 10 AWG POLE & BRACKET CABLE
			PHOTOELECTRIC CELL		DISCONNECT SWITCH
			POWER SOURCE		UNINTERRUPTIBLE POWER SUPPLY CABLE

NOTES:
 - FOR LOCATIONS WITH LEFT TURN LANES RUN 7C FOR POTENTIAL PT/PM LT PHASE IF INITIAL DESIGN IS FOR PERMITTED ONLY.
 - OVERLAPS SHALL BE WIRED TO THE APPROPRIATE LOAD SWITCHES AS PER THE FIELD HOOKUP CHART AND CONFIGURED IN THE CONTROLLER SOFTWARE PER THE SIGNAL TIMING CHART.