

TRAFFIC

TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. THE LENGTH OF RESTRICTED TRAFFIC ZONES SHALL BE KEPT TO A MINIMUM CONSISTENT WITH REQUIREMENTS FOR PROTECTION OF COMPLETED COURSES.

RAILROAD CROSSINGS & BRIDGE TREATMENT

THE NEW SURFACE COURSE SHALL BE FEATHERED OR BUTT JOINTED TO MEET THE PROFILE AS SPECIFIED BY THE ENGINEER. CONCRETE APPROACH SLABS AND BRIDGE DECKS SHALL NOT BE PAVED, UNLESS OTHERWISE NOTED IN THE PLANS (SEE SHEETS 5 THRU 6). THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO

PREVENT ANY ASPHALT CONCRETE MATERIAL FROM FALLING OFF THE EDGE OF A BRIDGE DECK OR EDGE OF A CULVERT DURING ANY CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL IMMEDIATELY REMOVE ANY MATERIAL THAT FALLS INTO THE ROADSIDE DITCHES OR STREAMS THROUGH NON-MECHANICAL MEANS. NO EQUIPMENT SHALL BE PERMITTED IN THE ROADSIDE DITCHES OR STREAMS.

EXTRA AREAS

EXTRA AREAS SHALL INCLUDE DRIVEWAYS, MAILBOX PULL-OFFS AND OTHER SIMILAR AREAS AS DETERMINED BY THE ENGINEER. DRIVEWAYS SHALL BE FEATHERED IN APPROXIMATELY 6 FEET USING ASPHALT CONCRETE. SOME DRIVES MAY REQUIRE MORE THAN 6 FEET TO ALLOW FOR ADEQUATE TRANSITION TO THE

MAINLINE PAVEMENT. THESE TRANSITIONS WILL BE AS DIRECTED BY THE ENGINEER. THE ASPHALT CONCRETE QUANTITIES FOR DRIVES AND MAILBOXES ARE INCLUDED IN THE EXTRA AREA QUANTITIES IN THE PAVEMENT DATA TABLE.

ASPHALT CONCRTE PLACEMENT ON SHOULDERS AND GUTTERS

THE ASPHALT CONCRETE ON THE SHOULDERS AND CONCRETE GUTTERS SHALL BE PLACED AT THE SAME TIME THAT THE ASPHALT CONCRETE IS PLACED ON THE ADJACENT LANES OF PAVEMENT. THE SHOULDER MATERIAL SHALL BE PLACED AT THE SAME CROSS SLOPE AS THE EXISTING SHOULDER OR CONCRETE GUTTER

GRADES. NEW CONCRETE CURB AND GUTTER AT LOCATIONS OF CURB RAMPS SHALL BE COMPLETED PRIOR TO PLACEMENT OF ASPHALT CONCRETE.

MAINTAINING TRAFFIC AT PLANED AREAS

IN SECTIONS OF THE PROJECT WHERE TRAFFIC IS BEING MAINTAINED, THE CONTRACTOR SHALL ARRANGE THEIR OPERATIONS SO THAT TRAFFIC IS RETURNED TO AN AREA WHEN THE PAVING IS COMPLETE. THE PLANED AREA SHALL BE CLEANED TO THE SATISFACTION OF THE ENGINEER PRIOR TO PLACING TEMPORARY MARKINGS. ALL REQUIRED WORK ZONE PAVEMENT MARKINGS SHALL BE PLACED PRIOR TO OPENING THE AREA TO TRAFFIC. NO PLANED SURFACE SHALL REMAIN OPEN TO TRAFFIC MORE THAN (7) DAYS BEFORE BEING COVERED WITH AN ASPHALT COURSE. IF THIS IS NOT DONE, LIQUIDATED DAMAGES WILL BE LEVIED AS PER SECTION SPECIFICATIONS.

IN SECTIONS OF THE PROJECT WHERE THE ROADWAY IS CLOSED TO THRU TRAFFIC, THE CONTRACTOR SHALL ARRANGE THEIR OPERATIONS SO THAT LOCAL TRAFFIC/PROPERTY OWNERS CAN RETURN TO AN AREA WHEN THE PLANING IS COMPLETE. THE PLANED AREA SHALL BE CLEANED TO THE SATISFACTION OF THE ENGINEER. ALSO, THE CONTRACTOR SHALL ARRANGE THEIR OPERATIONS SO THAT WHEN THRU TRAFFIC IS RETURNED NO PLANED SURFACE SHALL REMAIN. IF THIS IS NOT DONE, LIQUIDATED DAMAGES WILL BE LEVIED AS PER SECTION SPECIFICATIONS.

EROSION CONTROL

THE QUANTITY BELOW HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR EROSION CONTROL.

ITEM 832 EROSION CONTROL = 1,000 EACH

ITEM 617 COMPACTED AGGREGATE, AS PER PLAN

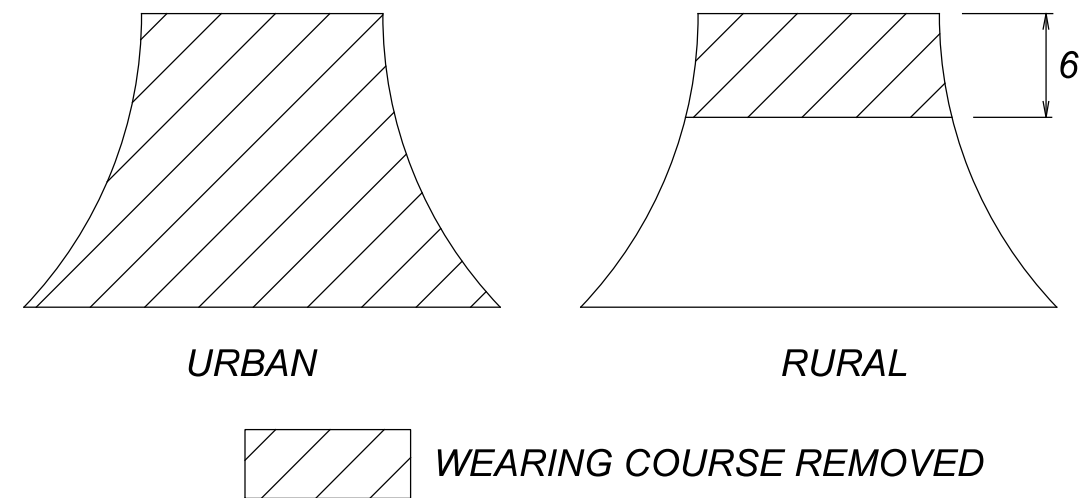
THIS ITEM SHALL MEET ALL REQUIREMENTS FOR ITEM 617 COMPACTED AGGREGATE WITH THE FOLLOWING EXCEPTION:

1) NO RECYCLED ASPHALT CONCRETE PAVEMENT SHALL BE USED IN THIS ITEM

ALL COSTS ASSOCIATED WITH THE EQUIPMENT, LABOR AND MATERIALS NECESSARY FOR SUPPLYING AND PLACING THIS ITEM SHALL BE INCLUDED IN THE PRICE BID PER CUBIC YARD FOR ITEM 617 COMPACTED AGGREGATE, AS PER PLAN.

WEARING COURSE REMOVED AT INTERSECTIONS

TYPICAL WEARING COURSE REMOVED AT INTERSECTIONS AS DETAILED BELOW.



ITEM 253 PAVEMENT REPAIR

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE.

THE ENGINEER SHALL DESIGNATE THE LOCATIONS AND LIMITS OF THE AREAS TO BE REPAIRED. THE REPAIR AREAS SHALL BE ROUGHLY RECTANGULAR IN SHAPE AND CUT OR SAWED TO A NEAT LINE. THE PAVEMENT SHALL BE REMOVED WITHIN THE DESIGNATED AREAS BY METHODS WHICH WILL NOT DAMAGE THE ADJACENT PAVEMENT. THE DEPTH OF REMOVAL, AS DIRECTED BY THE ENGINEER, SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT. THE MATERIALS SO REMOVED SHALL BE DISPOSED OF IN ACCORDANCE WITH 203.01.

THE VERTICAL FACES OF THE REPAIR AREA SHALL BE TACKED PRIOR TO PLACING THE 301 FOR ITEM 253 PAVEMENT REPAIR. THIS MATERIAL SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT EXISTING PAVEMENT SURFACE PRIOR TO PLACING THE PROPOSED ASPHALT CONCRETE OVERLAY. ALL COMPACTION SHALL BE ACHIEVED BY MECHANICAL METHODS TO THE SATISFACTION OF THE ENGINEER.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. AN ESTIMATED QUANTITY IS PROVIDED IN THE SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER. PAYMENT WILL BE MADE AT THE UNIT PRICE BID PER CUBIC YARD OF ITEM 253 PAVEMENT REPAIR.

253 PAVEMENT REPAIR 700 CUBIC YARD

THE ABOVE ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER. IT IS ESTIMATED THE REPAIRS WILL BE APPROXIMATELY 6 INCHES DEEP AND BE MOSTLY LONGITUDINAL REPAIRS. THE ESTIMATED WIDTH OF THESE REPAIRS ARE APPROXIMATELY 4 FEET. THERE ARE SEVERAL LOCATIONS WHERE THE ESTIMATED LENGTHS OF REPAIRS WILL BE BETWEEN APPROXIMATELY 50 FEET TO 150 FEET, AND THERE ARE A FEW LOCATIONS WHERE THE LENGTH OF REPAIRS ARE ESTIMATED TO BE APPROXIMATELY 200 FEET.

WORK ZONE MARKINGS AND SIGNS

ERECT A GROOVED PAVEMENT SIGN 250 FEET IN ADVANCE OF ANY SECTION OF ROADWAY WHERE TRAFFIC MUST TRAVEL ON A PLANED SURFACE. ENSURE THESE SIGNS ARE IN PLACE BEFORE OPENING THE ROADWAY TO TRAFFIC. ERECT THESE SIGNS AT INTERSECTIONS OF THROUGH ROUTES TO WARN TRAFFIC OF THIS SURFACE CONDITION. PAYMENT FOR THESE SIGNS TO BE INCLUDED IN ITEM 614 MAINTAINING TRAFFIC.

ERECT A NO EDGE LINES SIGN IN ADVANCE OF ANY SECTION OF ROADWAY LACKING STANDARD EDGE LINE MARKINGS. ERECT A DO NOT PASS SIGN AT THE BEGINNING AND A PASS WITH CARE SIGN AT THE END OF EACH NO PASSING ZONE LACKING STANDARD CENTER LINE MARKINGS. ENSURE THESE SIGNS ARE IN PLACE BEFORE OPENING THE ROADWAY TO TRAFFIC. PAYMENT FOR THESE SIGNS TO BE INCLUDED IN ITEM 614 WORK ZONE MARKING SIGN.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF CMS 614.04, 614.055 AND 614.11.

ITEM 614, WORK ZONE MARKING SIGN	70 EACH
ITEM 614, WORK ZONE CENTER LINE, CLASS I	13.31 MILES
ITEM 614, WORK ZONE CENTER LINE, CLASS II	26.62 MILES

446 DENSITY ACCEPTANCE WITH FLAGGER CLOSING OF A 2-LANE HIGHWAY FOR PAVING OPERATIONS

THIS PLAN NOTE APPLIES ONLY TO A FLAGGER CLOSURE OF ONE LANE OF A 2-LANE HIGHWAY DURING PAVING OPERATIONS WHEN USING STANDARD CONSTRUCTION DRAWING MT 97.11 OR MT 97.12, AND ALLOWS A PAVING OPERATION TO PROCEED CONCURRENTLY WITH THE MARKING AND CUTTING OF CORE'S REQUIRED FOR 446 DENSITY ACCEPTANCE.

IN ALL CASES THE CONTRACTOR SHOULD LENGTHEN THEIR LANE CLOSURES TO THE MAXIMUM PERMISSIBLE LENGTH DETAILED IN THE ABOVE REFERENCED STANDARD CONSTRUCTION DRAWINGS TO ALLOW THE ENGINEER ADEQUATE TIME TO MARK THE REQUIRED CORE LOCATIONS AND FOR CORE CUTTING OPERATIONS.

THE CONTRACTOR WILL PROVIDE TO THE ENGINEER THE PLANNED QUANTITY THAT WILL BE PLACED FOR THE DAY'S PRODUCTION. EACH DAY'S PRODUCTION WILL BE CONSIDERED ONE LOT AND INCLUDES SHOULDERS. TEN CORES WILL BE OBTAINED BY THE CONTRACTOR FOR EACH LOT AT RANDOM LOCATIONS DETERMINED BY THE ENGINEER. THE ENGINEER WILL DIVIDE A LOT INTO FIVE EQUAL SUBPLOTS AND CALCULATE TWO RANDOM CORE LOCATIONS IN EACH SUBPLOT AS DESCRIBED IN C&MS 446.05.

THE ENGINEER WILL MARK THE CORE LOCATIONS AFTER THE PAVING OPERATION (INCLUDING THE FINISH ROLLER) HAS COMPLETELY PASSED THE RANDOMLY SELECTED CORE LOCATION. THE CORE DRILL OPERATION CAN BEGIN CUTTING CORES WHEN THE NEWLY PLACED PAVEMENT SURFACE TEMPERATURE IS LESS THAN 140 DEGREES F. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE LANE CLOSURE DURING ALL PAVING, MARKING, AND CORING OPERATIONS PER THE REQUIREMENTS OF THE STANDARD CONSTRUCTION DRAWING USED FOR THE PAVING OPERATION.

ITEM 614 MAINTAINING TRAFFIC

A MINIMUM OF 1 LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED 25 CONSECUTIVE DAYS FOR STAGE 1 OF FULL WIDTH (ECHELON) PAVING AND 25 CONSECUTIVE DAYS FOR STAGE 2 OF FULL WIDTH (ECHELON) PAVING, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEET 4. STAGE 1 AND STAGE 2 WORK LOCATIONS SHALL NOT BE CLOSED CONCURRENTLY. A DIS-INCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$4,500 PER

ITEM 614 MAINTAINING TRAFFIC (CONTINUED)

DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE.

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	> = 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

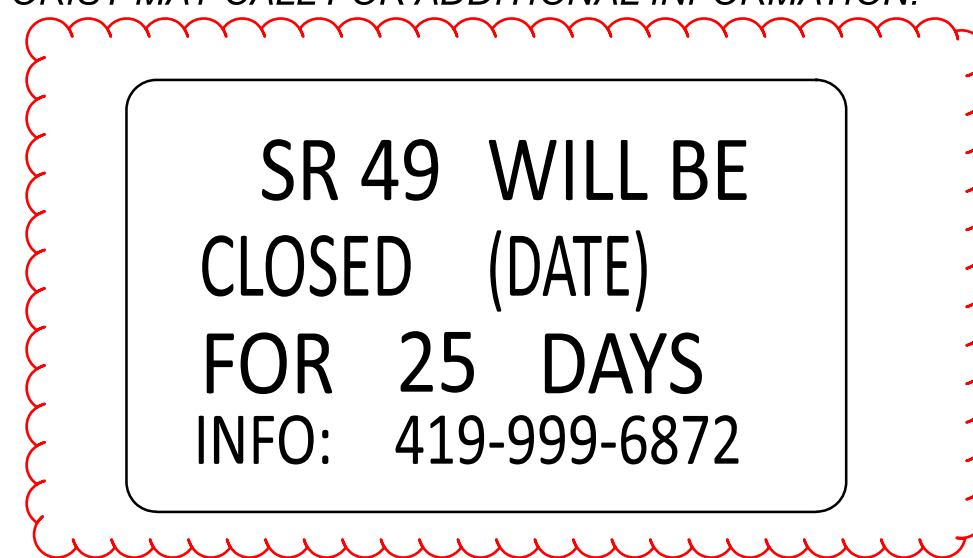
ALIGNMENT AND PROFILE

THE WORK PROPOSED FOR THIS PROJECT IS FOR THE RESURFACING OF THE EXISTING PAVEMENT. PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT.



ITEM 614, MAINTAINING TRAFFIC (CONTINUED)

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.



W20-H13-60

NOTE: THE CONTRACTOR IS TO SUPPLY THE DATE

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE FOLLOWING LOCATIONS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

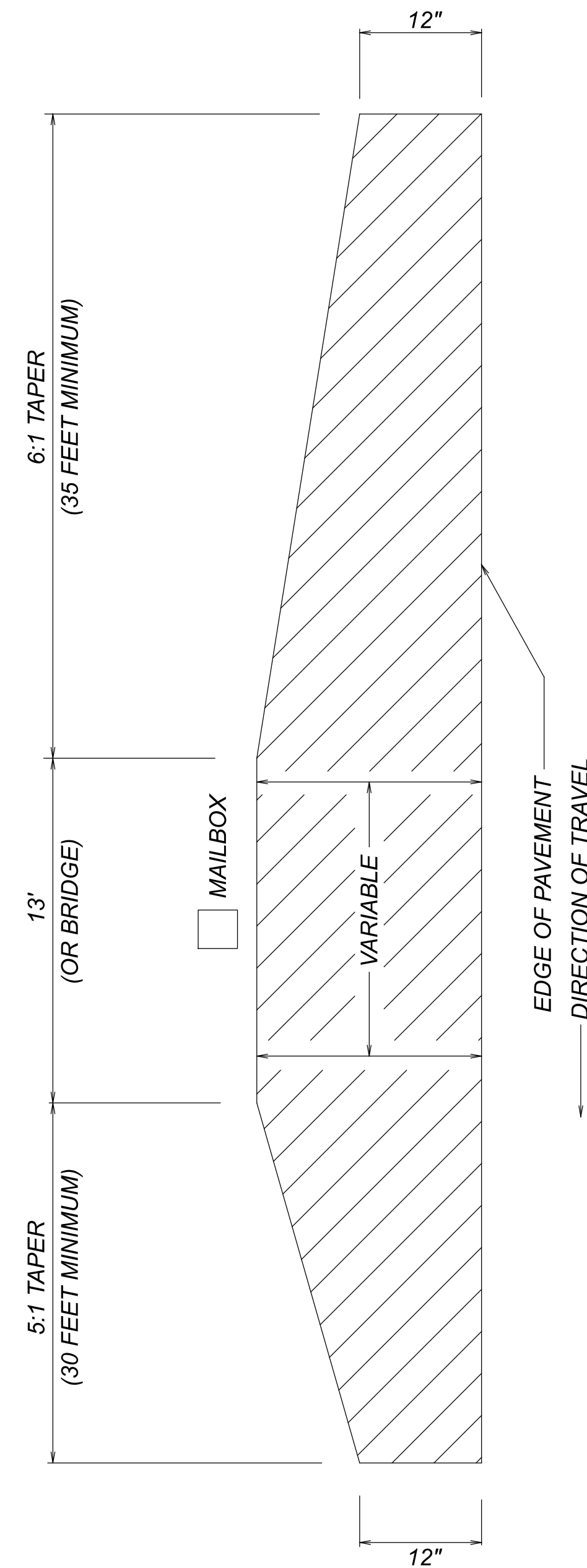
- STAGE 1:
 BEGIN PROJECT SR 49
 INTERSECTION OF SR 49 & CR 94
 INTERSECTION OF SR 49 & TR 96
 INTERSECTION OF SR 49 & CR 106
 INTERSECTION OF SR 49 & CR 124
 INTERSECTION OF SR 49 & SR 111
 INTERSECTION OF SR 49 & CR 144
 INTERSECTION OF SR 49 & TR 152
 INTERSECTION OF SR 49 & TR 162.
 END OF STAGE 1 SR 49

- STAGE 2:
 BEGIN SR 49
 INTERSECTION OF SR 49 & US 24 RAMPS
 INTERSECTION OF SR 49 & CR 424
 INTERSECTION OF SR 49 & CR 192
 INTERSECTION OF SR 49 & CR 204
 INTERSECTION OF SR 49 & TR 214
 INTERSECTION OF SR 49 & CR 220
 INTERSECTION OF SR 49 & CR 230
 INTERSECTION OF SR 49 & DEFIANCE PAULDING COUNTY LINE RD
 INTERSECTION OF SR 49 & JERICHO RD
 END PROJECT

THE DEPARTMENT WILL PROVIDE DETOUR SIGNING AS PER 614.06.A

ACCESS TO ADJACENT PROPERTY WITHIN THE WORK LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR AT ALL TIMES, AS PER 614.02.A.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.



SINGLE MAILBOX TURNOUT & BRIDGE APPROACHES

IF THERE IS A DISTANCE OF 100 FEET OR LESS BETWEEN MAILBOXES, APPROACHES SHALL BE PAVED THRU TO THE LAST MAILBOX.

IF THERE IS A DISTANCE OF 50 FEET OR LESS BETWEEN DRIVEWAY AND MAILBOX, APPROACHES SHALL BE PAVED THRU TO THE LAST MAILBOX. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXCAVATION OF MATERIALS FROM ALL STONE DRIVEWAYS AND MAILBOX APPROACHES TO A DEPTH OF 2 INCHES BELOW EXISTING PAVEMENT. EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY OUTSIDE THE LIMITS OF THE HIGHWAY RIGHT OF WAY.

IF NEITHER OF THE ABOVE CONDITIONS APPLY AND PER THE DIRECTION OF THE PROJECT ENGINEER, A MAILBOX TURNOUT SHALL BE PROVIDED AS PER THE ADJACENT DETAIL.

FOR MAILBOX TURNOUTS, WHEN UNSTABLE MATERIAL IS ENCOUNTERED, EXCAVATION OF THIS MATERIAL SHALL BE TO A DEPTH OF 6 INCHES BELOW EXISTING PAVEMENT ELEVATION. AN ESTIMATED QUANTITY OF 304 AGGREGATE BASE HAS BEEN SET UP FOR BACKFILL OF THESE AREAS. EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY OUTSIDE THE LIMITS OF THE HIGHWAY RIGHT OF WAY.

AT BRIDGES WITH NO EXISTING BRIDGE APPROACHES AND PER THE DIRECTION OF THE PROJECT ENGINEER, BRIDGE APPROACHES SHALL BE PROVIDED PER THE ADJACENT DETAIL. THE AREA SHOWING THE LOCATION OF THE BRIDGE WILL HAVE VARIABLE LENGTH AND NO WORK IS NEEDED WITHIN THIS AREA UNLESS NOTED OTHERWISE IN THE PLANS.

FOR BRIDGE APPROACHES, THE BRIDGE APPROACHES SHALL BE EXCAVATED TO A DEPTH OF 9 INCHES BELOW EXISTING ADJACENT PAVEMENT ELEVATIONS. AN ESTIMATED QUANTITY OF 304 AGGREGATE BASE HAS BEEN SET UP FOR BACKFILL TO A DEPTH OF 6 INCHES FOR THESE AREAS, AND ESTIMATED QUANTITIES OF ASPHALT SURFACE COURSE AND TACK COAT HAVE BEEN SET UP FOR PLACEMENT OF TWO ASPHALT CONCRETE SURFACE COURSES, EACH COURSE BEING 1½ INCHES THICK, FOR THESE AREAS. EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY OUTSIDE THE LIMITS OF THE HIGHWAY RIGHT OF WAY.

AN ADDITIONAL QUANTITY OF ASPHALT CONCRETE HAS BEEN SET UP TO BE USED IN THOSE AREAS EXCAVATED FOR DRIVEWAYS, MAILBOX AND BRIDGE APPROACHES.

ALL WORK, MATERIALS, EXCEPT ITEM 304, LABOR AND EQUIPMENT NECESSARY TO COMPLETE THE ABOVE DESCRIBED WORK SHALL BE INCIDENTAL TO THE PLACEMENT OF THE ASPHALT CONCRETE.

PERSONAL PROTECTIVE EQUIPMENT (PPE)

THE CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF SECTIONS XXIV AND XXXIV OF THE OHIO DEPARTMENT OF TRANSPORTATION SAFETY AND HEALTH STANDARD OPERATING PROCEDURE 220-006(SP) EFFECTIVE: NOVEMBER 1, 2018 (EXCEPT AS AMENDED BELOW) AND ALL SUBSEQUENT UPDATES POSTED AT THE FOLLOWING WEBSITE:

HTTP://WWW.DOT.STATE.OH.US/POLICY/POLICIESANDSOPS/POLICIES/220-006(SP).PDF

AMENDMENTS TO THE REQUIREMENTS OF THIS DOCUMENT ARE:

XXIV. HEAD PROTECTION (HARD HATS)
 ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR APPROPRIATE HEAD PROTECTION. ALL HARD HATS MUST MEET OR EXCEED ANSI Z89.1-2009 TYPE 1, CLASS E-G REQUIREMENTS.

XXXIV. SAFETY APPAREL AND VEST (HIGH VISIBILITY)
 ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR A HIGH-VISIBILITY SAFETY VEST THAT MEETS THE PERFORMANCE CLASS II OR CLASS III REQUIREMENTS OF THE ANSI/ISEA 107-2015 PUBLICATION ENTITLED "AMERICAN NATIONAL STANDARD FOR HIGH-VISIBILITY SAFETY APPAREL AND ACCESSORIES."

WORKERS MAY WEAR AN ANSI CLASS II OR ANSI CLASS III APPROVED RAIN SUIT, JACKET OR OTHER APPAREL WITHOUT A SAFETY VEST OVER IT.

WORKERS MUST WEAR THE REQUIRED PPE AS DESCRIBED IN THE LATEST EDITION OF THE CSXT PUBLIC PROJECTS MANUAL, AT ALL TIMES WHILE WORKING WITHIN THE CSXT RIGHT OF WAY.

ROLLER REQUIREMENTS WITHIN CORPORATION LIMITS

WITHIN THE CORPORATION LIMITS OF THE VILLAGE OF ANTWERP, THE CONTRACTOR SHALL NOT USE THE VIBRATION MODE (VIBRATION SHALL BE TURNED OFF) ON VIBRATORY ROLLERS TO COMPACT THE ASPHALT CONCRETE. 446 DENSITY REQUIREMENTS SHALL APPLY.

SEEDING AND MULCHING

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS.

659, TOPSOIL	1 CU. YD.
659, SEEDING AND MULCHING	30 SQ. YD.
659, COMMERCIAL FERTILIZER	0.1 TON
659, WATER	0.2 M. GAL.

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT OF WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT OF WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

GENERAL NOTES

PAU/DEF-49-6.94/0.00

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DESIGN AGENCY



DESIGNER	MPB
REVIEWER	GLI
PROJECT ID	1-30-24
	113043
SHEET	TOTAL
3	16