

OHIO DEPARTMENT OF TRANSPORTATION

PLAN NO. _____



970087 02-19-97
 87PGS DIST. 06

PIC-762-0.00-17.22



LATITUDE: 39°46'55" LONGITUDE: 83°05'02"

PART	COUNTY	ROUTE	SECTIONS	PROJECT TERMINI		NET LENGTH km	CITY
				BEGIN	END		
1	PIC	762	(0.00 - 17.22)	0.00	17.99	17.99	COMMERCIAL POINT

INDEX OF SHEETS:

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1995 SPECIFICATIONS

The standard specifications of the State of Ohio, Department of Transportation, including changes and supplemental specifications listed in the proposal shall govern this improvement.

I hereby approve these plans and declare that the making of this improvement will not require the closing to traffic of the highway and that provisions for the maintenance and safety will be as set forth on plans and estimates.

DESIGN EXCEPTIONS

DESIGN FEATURE	APPROVAL DATE	SHEET NOS.
LANE WIDTH	8-9-96	2
SHOULDER WIDTH	8-9-96	4
HORIZONTAL ALIGNMENT	8-9-96	
VERTICAL ALIGNMENT	8-9-96	
STOPPING SIGHT DISTANCE	8-9-96	
HORIZONTAL CLEARANCE	8-9-96	
VERTICAL CLEARANCE	8-9-96	

STANDARD DRAWINGS	
BP-3.1M	10-28-94
RM-1.1M	06-30-95
MT-97.10M	04-25-94
MT-97.11M	01-30-95
MT-99.10M	01-30-95
MT-99.20M	01-30-95
MT-105.10M	04-25-94
MT-105.11M	04-25-94
TC-65.10M	11-01-95
TC-65.12M	11-01-95
TC-71.10M	09-01-93

SUPPLEMENTAL SPECIFICATIONS	

UNDERGROUND UTILITIES

TWO WORKING DAYS

BEFORE YOU DIG

CALL 1-800-362-2764 (TOLL FREE)

OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY

PLAN PREPARED BY:
 O.D.O.T.
 DISTRICT SIX
 IN-HOUSE DESIGN

PLANS CERTIFIED BY:

NAME: *D. S. Shover* DATE: 10-30-96

DISTRICT 6
 OHIO DEPT. OF TRANSPORTATION

Approved *[Signature]*
 Date: 10/22/96 District Deputy Director of Transportation

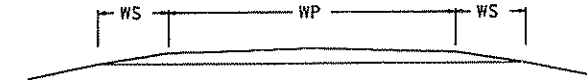
Approved *[Signature]*
 Date: 11/12/96 Director, Department of Transportation

FEDERAL PROJECT NO. STP
 (Surface Transportation Program)
 PID NO. 16015
 CONSTRUCTION PROJECT NO.
 RAILROAD INVOLVEMENT
 PIC-762-0.000

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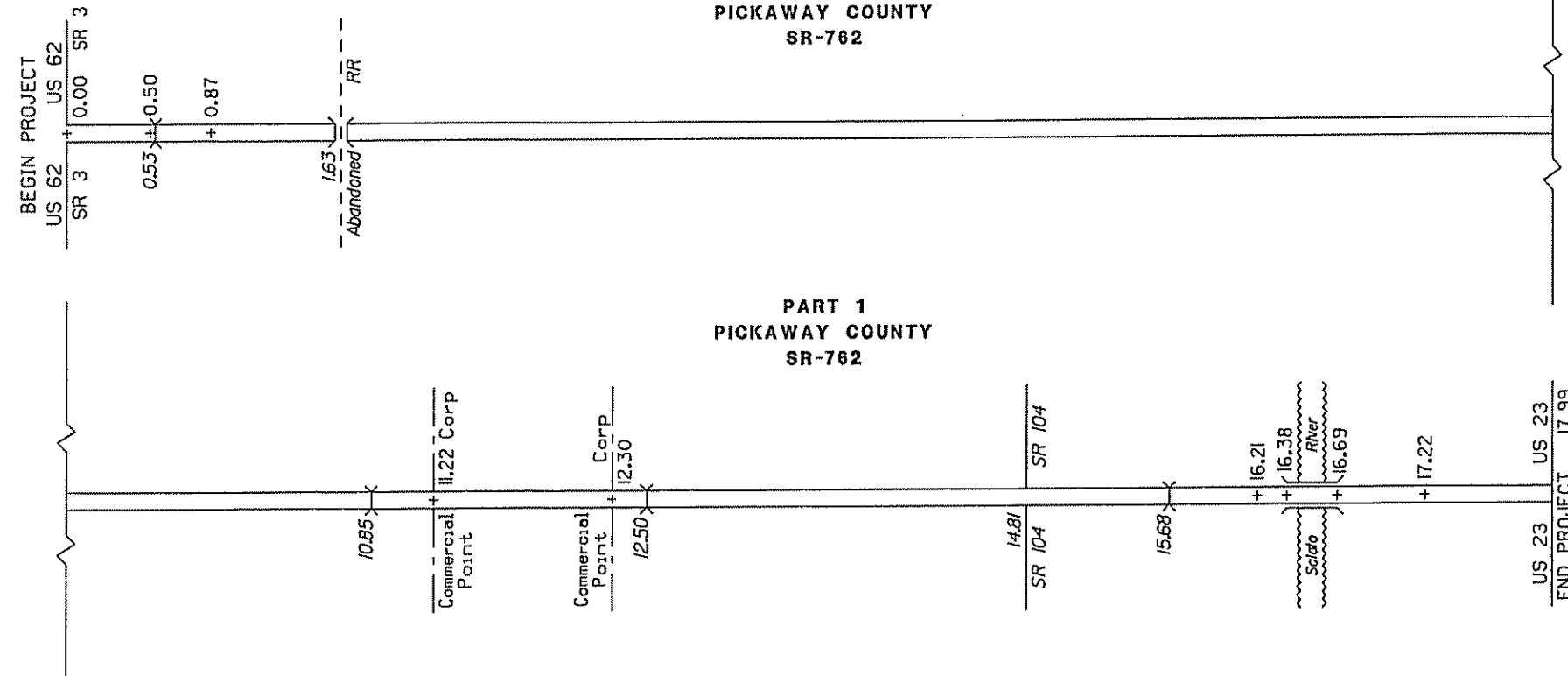
PART 1
PICKAWAY COUNTY
SR-762

TYPICAL I



ITEM 254 PAVEMENT PLANING SHALL BE USED IN LIEU OF ITEM 202 WEARING COURSE REMOVED AS CALLED FOR IN STD. DWG. BP-3.1M

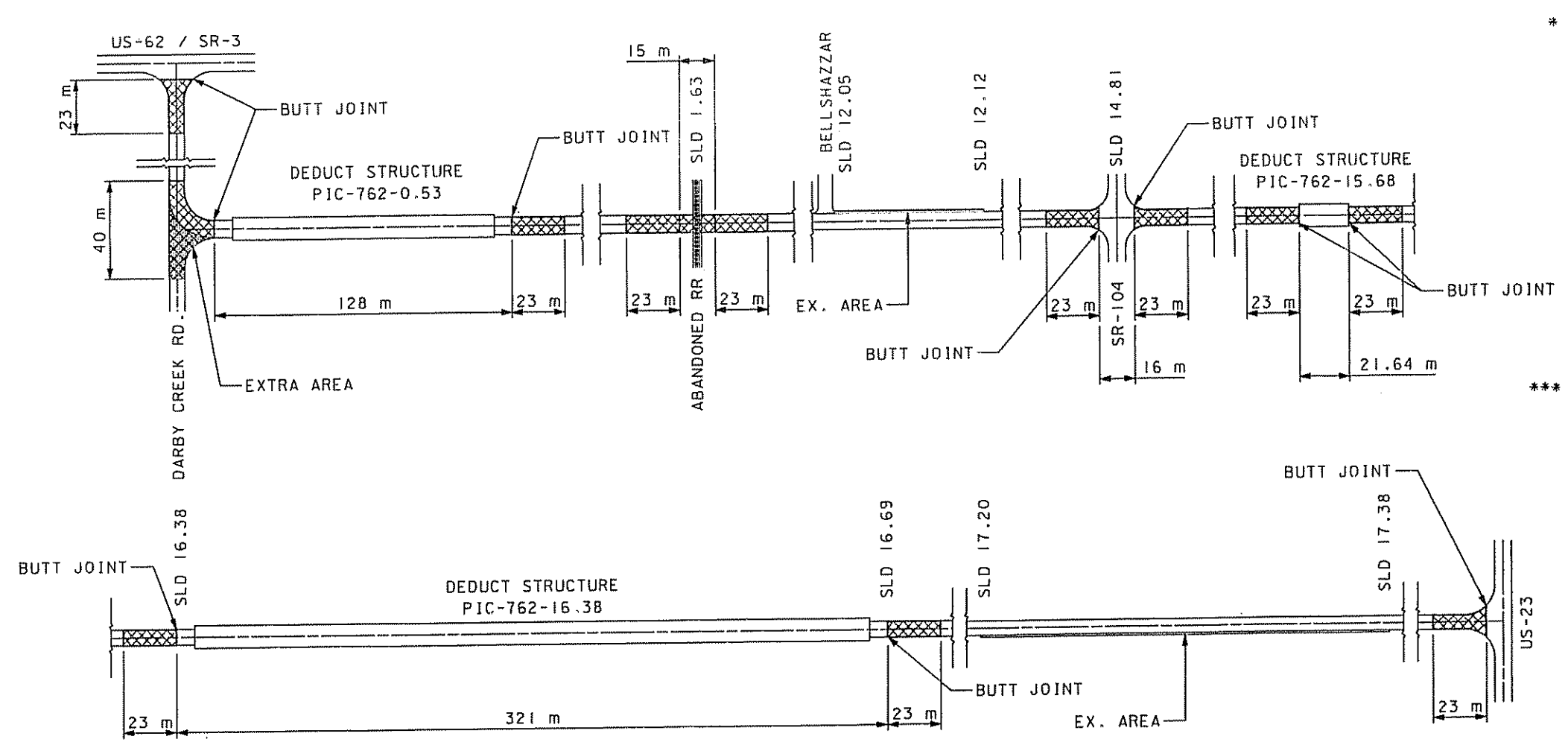
* ITEM 448 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I, AC-20, (SPOT LEVELING): THIS MATERIAL IS TO BE USED, AS DIRECTED BY THE ENGINEER, TO FILL RUTS AND IMPERFECTIONS; AND TO LEVEL AND RE-ESTABLISH THE ORIGINAL ALIGNMENT AND PROFILE OF THE PAVEMENT, PRIOR TO PLACEMENT OF THE SURFACE COURSE. THIS MAY RESULT IN A PORTION OF THE ORIGINAL SURFACE BEING EXPOSED BEFORE THE OVERLAY IS PLACED.



PAVEMENT DATA



PART	COUNTY	ROUTE	FROM	TO	LENGTH		AVERAGE WIDTH OF PAVEMENT	TYPICAL	EX. PAVEMENT TYPE	PAVEMENT AREA	ITEM 448, ASPHALT CONCRETE			254	614	617	604	604		
											TACK COAT AT 0.45 liter/sq. m	INTERMEDIATE COURSE TYPE 1, AC-20							SURFACE COURSE TYPE 1, AC-20	
												AVERAGE THICK 0 mm MIN.	cu m						AVERAGE THICK	cu m
			SLD	SLD	km	m	m			sq m	liter	mm	cu m	mm	cu m	sq m	EACH	cu m	EACH	EACH
I	PIC	762	0.00	0.50	0.50	500	6.71	I	404	3355	1510	*	44	32	107					
I	PIC	762	0.50	0.87	0.37	370	7.32	I	404	2708	1219	*	35	32	87					
I	PIC	762	0.87	11.22	10.35	10350	6.10	I	404	63135	28411	*	821	32	2020					
I	PIC	762	11.22	12.30	1.08	1080	6.71	I	404	7247	3261	*	94	32	232					
I	PIC	762	12.30	16.21	3.91	3910	6.10	I	404	23851	10733	*	310	32	763					
I	PIC	762	16.21	17.22	1.01	1010	7.32	I	404	7393	3327	*	96	32	237					
I	PIC	762	17.22	17.99	0.77	770	6.10	I	404	4697	2114	*	61	32	150					
TO BE USED AS DIRECTED BY THE ENGINEER										400	180	*	5	32	13					
TOTALS											50755		1466		3609					

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* ITEM 448 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, AC-20, (SPOT LEVELING): THIS MATERIAL IS TO BE USED, AS DIRECTED BY THE ENGINEER, TO FILL RUTS AND IMPERFECTIONS; AND TO LEVEL AND RE-ESTABLISH THE ORIGINAL ALIGNMENT AND PROFILE OF THE PAVEMENT, PRIOR TO PLACEMENT OF THE SURFACE COURSE. THIS MAY RESULT IN A PORTION OF THE ORIGINAL SURFACE BEING EXPOSED BEFORE THE OVERLAY IS PLACED.

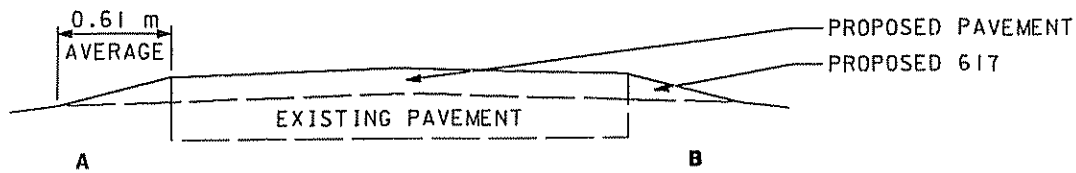
*** ITEM 254 PAVEMENT PLANING SHALL BE USED IN LIEU OF ITEM 202 WEARING COURSE REMOVED AS CALLED FOR IN STD. DWG. BP-3.1M

 PLANING AREAS
 EXTRA AREAS

PART	COUNTY	ROUTE	FROM	TO	SIDE	DESCRIPTION	LENGTH		WIDTH "W"	AREA	ITEM 448, ASPHALT CONCRETE				254		617		REMARKS
							km	m			407 TACK COAT AT 0.45 liter/sq. m	INTERMEDIATE COURSE TYPE 1, AC-20		SURFACE COURSE TYPE 1, AC-20		PAVEMENT PLANING BITUMINOUS		COMPACTED AGGREGATE TYPE A 75 mm	
												AVERAGE THICK 0 mm MIN.	AVERAGE THICK	AVERAGE THICK	DEPTH				
							liter	mm			cu m	mm	cu m	mm	sq m	cu m			
			slid	slid		INTERSECTION - BITUMINOUS				3800	1710	*	49	32	122				
I	PIC	762	VARIOUS			DRIVES - BITUMINOUS				1240	558	*	16	32	40				
I	PIC	762	VARIOUS			DRIVES - AGGREGATE				2340								176	
I	PIC	762	0.00			BEGIN PROJECT PLANING		23.00	VARIES	176						45 TO 0	176		***
I	PIC	762	0.53			APPROACH STRUCTURE PLANING			VARIES	433						0 TO 45	433		***
I	PIC	762	0.53		RT	EXTRA AREA - @ DARBY CREEK ROAD			VARIES	217	98	*	3	32	7				
I	PIC	762	0.53			STRUCTURE - DEDUCT		-128.00	7.32	-937	-422	*	-12	32	-30				
I	PIC	762	0.53			DEPART STRUCTURE PLANING		23.00	7.32	168						45 TO 0	168		***
I	PIC	762	1.63			APPROACH PLANING TO ABANDONED RR		23.00	6.10	140						0 TO 45	140		***
I	PIC	762	1.63			PLANING UNDER ABANDONED RR		15.00	6.10	92						45	92		
I	PIC	762	1.63			DEPART PLANING FROM ABANDONED RR		23.00	6.10	140						45 TO 0	140		***
I	PIC	762	12.05	12.12	LT	EXTRA AREA - WIDTH @ BELLSHAZZAR RD.	0.07	70.00	1.22	85	38	*	1	32	3	0 TO 45	160		***
I	PIC	762	14.81			APPROACH PLANING TO SR-104		23.00	VARIES	160						0 TO 45	160		***
I	PIC	762	14.81			DEDUCT INTERSECTION : SR-104		-16.00	6.10	-98	-44	*	-1	32	-3				***
I	PIC	762	14.81			DEPART PLANING FROM SR-104		23.00	VARIES	160						0 TO 45	140		***
I	PIC	762	15.68			APPROACH STRUCTURE PLANING		23.00	6.10	140						45 TO 0	140		***
I	PIC	762	15.68			STRUCTURE - DEDUCT		-21.64	6.10	-132	-59	*	-2	32	-4				***
I	PIC	762	15.68			DEPART STRUCTURE PLANING		23.00	6.10	140						45 TO 0	140		***
I	PIC	762	16.38			APPROACH STRUCTURE PLANING		23.00	7.32	168						0 TO 45	168		***
I	PIC	762	16.38			STRUCTURE - DEDUCT		321.00	7.32	2350	1058	*	31	32	75	45 TO 0	168		***
I	PIC	762	16.69			DEPART STRUCTURE PLANING		23.00	7.32	168						45 TO 0	168		***
I	PIC	762	17.20	17.38	RT	EXTRA AREA - WIDTH	0.18	180.00	0.91	164	74	*	2	32	5	0 TO 45	160		***
I	PIC	762	17.99			END PROJECT PLANING		23.00	VARIES	160						0 TO 45	160		***
TOTAL											3011		87		215	2245	176		

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TYPICAL 1A



NO DEDUCTIONS FOR STRUCTURES TO ALLOW FOR EXTRA MATERIAL TO BE USED AS DIRECTED.

SHOULDER DATA

PART	COUNTY	ROUTE	FROM	TO	LENGTH		TYPICAL	EXISTING TYPE - WIDTH (meter)								AREA	ITEM 448, ASPHALT CONCRETE						254 PAVEMENT PLANING BITUMINOUS 38 mm DEPTH	617 COMPACTED AGGREGATE TYPE A AVERAGE THICKNESS 75 mm															
								A		B		C		D			407 TACK COAT AT 0.45 liter/sq. m	INTERMEDIATE COURSE TYPE 1, AC-20		SURFACE COURSE TYPE 1, AC-20																			
								TYPE	WIDTH	TYPE	WIDTH	TYPE	WIDTH	TYPE	WIDTH			AVERAGE THICK 0 m MIN.		AVERAGE THICK																			
				km	m								sq m	mm	cu m	mm	cu m	sq m	cu m																				
I	PIC	762	0.00	17.99	17.99	17990	IA														1646																		
TOTALS																																							1646

CALCULATED
CHECKED
PLAN NO.

SHOULDER TREATMENT

PIC-762-0.000

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ITEM 643 - CENTER LINE QUANTITIES

PART	COUNTY	ROUTE	s.l.d.	FROM	s.l.d.	TO	DASHED	SOLID	TOTAL	REMARKS
							km	km	km	
I	PIC	762	0.00	US-62/SR-3	17.99	US-23	12.73	17.76	17.99	
CENTER LINE TOTAL									17.99	

ITEM 643 - EDGE LINE QUANTITIES

PART	COUNTY	ROUTE	s.l.d.	FROM	s.l.d.	TO	WHITE			YELLOW			REMARKS
							HIGHWAY	RAMP	TOTAL	HIGHWAY	RAMP	TOTAL	
							km	km	km	km	km	km	
I	PIC	762	0.00	US-62/SR-3	17.99	US-23	(x2) 17.99		35.98				
EDGE LINE TOTAL									35.98				

ITEM 644 - AUXILIARY MARKING QUANTITIES

PART	COUNTY	ROUTE	FROM	TO	CHANNELIZING LINES	600 mm TRANSVERSE LINES		STOP LINES	CROSSWALK LINES	WORD ON PAVEMENT ONLY		SCHOOL SYMBOL MARKING		LANE ARROWS				REMARKS				
						200 mm	WHITE			YELLOW	600 mm	300 mm	WHITE	1800 mm	2500 mm	1800 mm	2500 mm		TURN		THRU.	COMB.
																			m	m		
I	PIC	762	0.00					3.4										SEE SHEET 9				
I	PIC	762	0.50					9.8										@ DARBY CREEK				
I	PIC	762	11.81					4.9										IN COMMERCIAL POINT				
I	PIC	762	17.99					4.6										SEE SHEET 9				
TOTAL									22.7													

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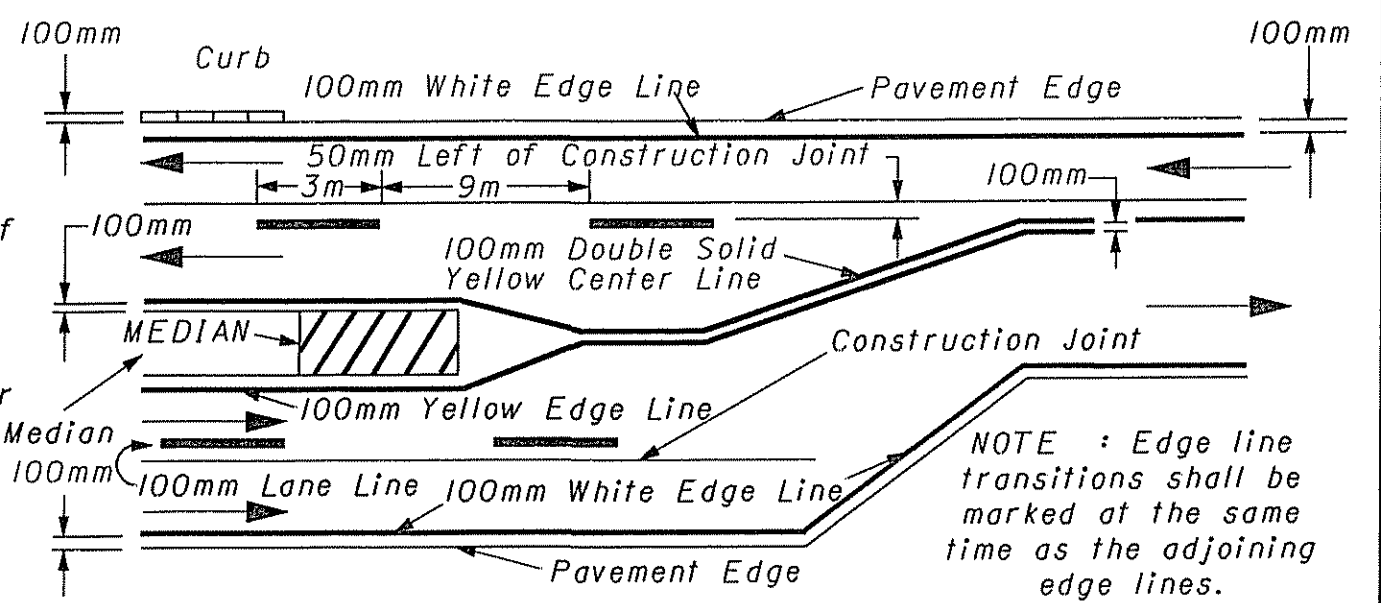
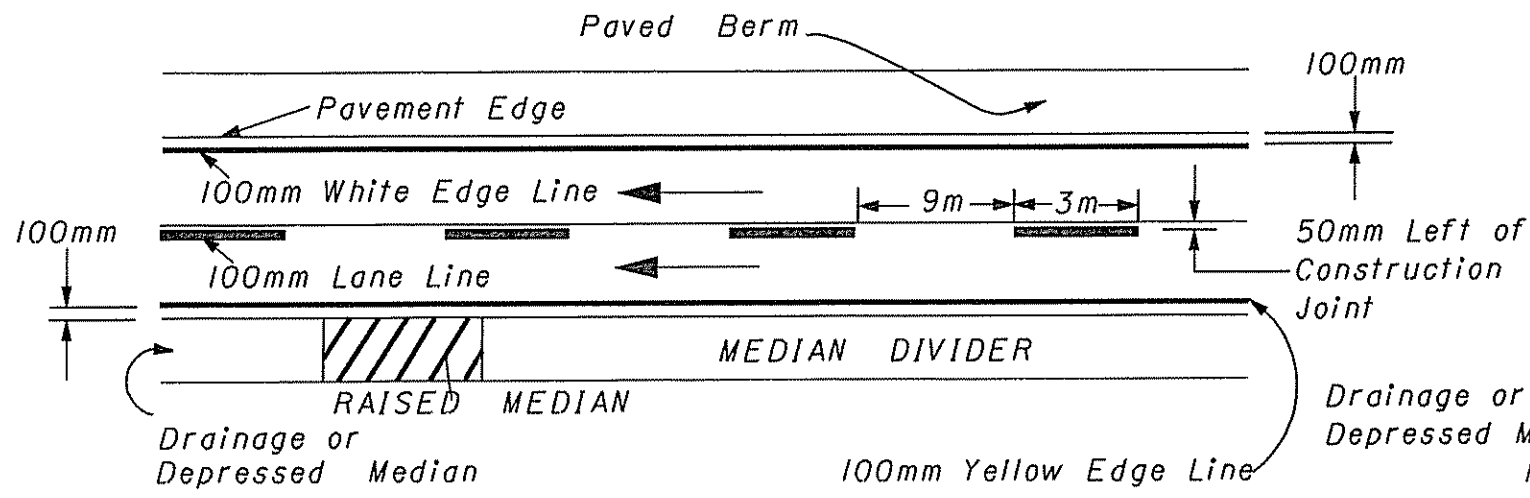
PLAN NO.

PAVEMENT MARKING

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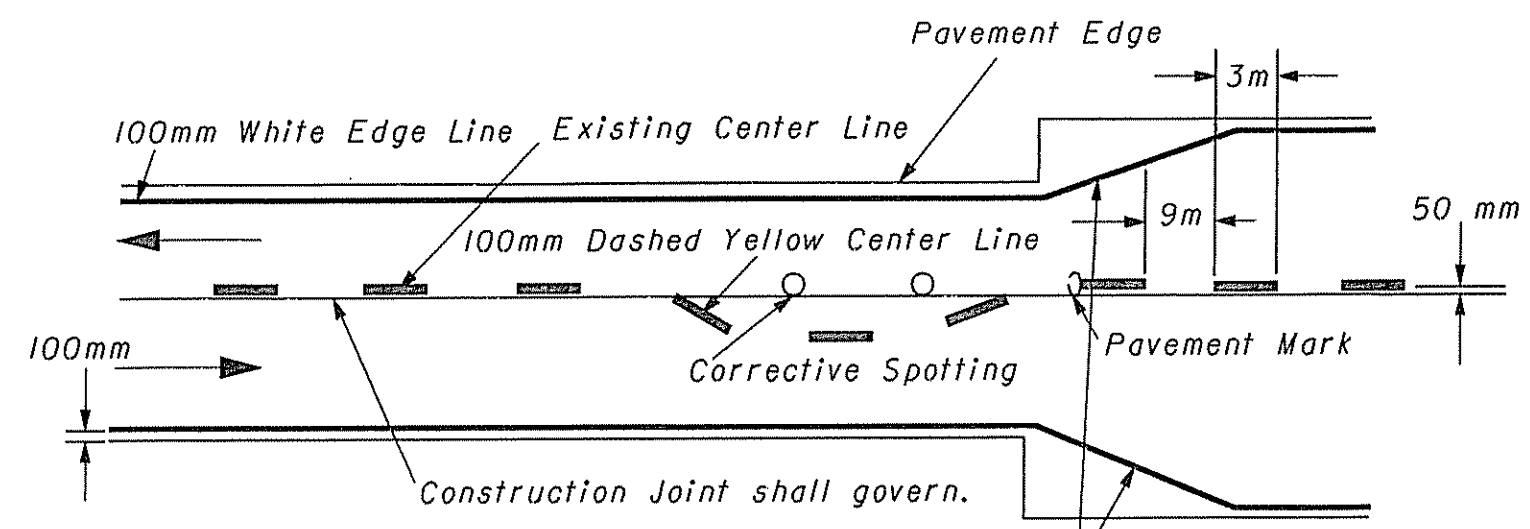
FREEWAY & EXPRESSWAY MAINLINE MARKINGS

MULTILANE DIVIDED & UNDIVIDED HIGHWAY MARKINGS



NOTE : Edge line transitions shall be marked at the same time as the adjoining edge lines.

TWO LANE MARKINGS



Min. 30:1 Taper Both Sides
At all locations where pavement widths change by construction plans.

NOTES :

1. The distance from the pavement edge to the nearside edge of the edgeline may be increased with the approval of the engineer in order to maintain uniform lane width.
2. See TC-72.20M for entrance and exit ramp markings.
3. The cycle length for dashed lines shall be 12 meters plus or minus 150mm. The minimum length of dash shall be sufficiently long to maintain a 3:1 ratio between length of gap and length of dash.

LOCATION SUB-SUMMARY

DETAIL	
1	MAINLINE UNDIVIDED TYPICAL SPACING
2	TAPERED ACCELERATION LANE
3	DECELERATION LANE
4	PARALLEL ACCELERATION LANE
5	MULTILANE DIVIDED/EXPRESSWAY
6	STOP APPROACH

DETAIL	
7	1 LANE APPROACH W/ LT. TURN LANE
8	THRU APPROACH
9	2 LANE APPROACH W/ LT. TURN LANE
10	4 LANE DIVIDED TO 2 LANE TRANSITION
11	4 LANE UNDIVIDED TO 2 LANE TRANS.
12	TWO LANE NARROW BRIDGE
13	TWO WAY LEFT TURN LANE

DETAIL	
14	ONE LANE BRIDGE
15	HORIZONTAL CURVE
16	HORIZONTAL CURVE ALTERNATE
17	STOP APPROACH ALTERNATE
GAP	CENTERLINE AT 24 m TYP.

PART	LOCATION				D E T A I L	PRISMATIC RETRO-REFLECTOR COLORS					202	621	621	REMARKS
	COUNTY	ROUTE	S.L.D. SECTION			ONE-WAY		TWO-WAY			RAISED PAVEMENT MARKER REMOVED FOR STORAGE	RAISED PAVEMENT MARKER	RAISED PAVEMENT MARKER INSTALL ONLY	
			FROM	TO		WHITE	YELLOW	WHITE / WHITE	YELLOW / YELLOW	WHITE / RED	EACH	EACH	EACH	
												EACH	EACH	
I	PIC	762	0.00	0.32	6	39				27	66	39	27	@US-62 TO -----
I	PIC	762	0.32	1.45	15					92	92		92	-----ORIENT
I	PIC	762	2.25	2.74	GAP					20	20		20	ORIENT TO -----
I	PIC	762	2.74	3.38	15					53	53		53	
I	PIC	762	3.38	4.02	GAP					26	26		26	
I	PIC	762	4.02	6.76	15					224	224		224	
I	PIC	762	6.76	8.05	GAP					53	53		53	
I	PIC	762	8.05	8.53	15					40	40		40	
I	PIC	762	8.53	9.82	GAP					53	53		53	
I	PIC	762	9.82	11.22	15					115	115		115	-----COMMERCIAL POINT WCL
I	PIC	762	12.31	14.53	GAP					91	91		91	COMMERCIAL POINT ECL TO DETAIL 6
I	PIC	762	14.53	15.18	6	78				54	132	78	54	@ SR-104
I	PIC	762	15.18	17.67	GAP					102	102		102	SR-104 TO DETAIL 6
I	PIC	762	17.67	17.99	6	39				27	66	39	27	@ US-23
TOTAL											1133	156	977	

CALCULATED
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PLAN NO.

RAISED PAVEMENT MARKER

PIC-762-0.000

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GENERAL:

THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 101.18) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK LISTED IN THE GENERAL SUMMARY FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER.

CONSTRUCTION INITIATION:

THE CONTRACTOR SHALL ADVISE THE DISTRICT COMMUNICATIONS OFFICER AT 1-614-363-1251, EXTENSION 469 AND THE MAINTENANCE OF TRAFFIC ENGINEER, EXTENSION 477 OR BY FAX NO. 1-614-369-7437, FOURTEEN DAYS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES. THE PROJECT ENGINEER WILL PROVIDE ASSISTANCE/CLARIFICATION FOR ANY QUESTIONS.

TRAFFIC:

TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. THE LENGTH OF RESTRICTED TRAFFIC ZONES SHALL BE KEPT TO A MINIMUM CONSISTENT WITH THE SPECIFICATION REQUIREMENTS FOR PROTECTION OF COMPLETED COURSES.

ALIGNMENT AND PROFILE:

THE WORK PROPOSED BY THIS PROJECT IS FOR THE RESURFACING OF THE EXISTING PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED, AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT EXCEPT THAT IT MAY BE RAISED AN AMOUNT EQUAL TO THE THICKNESS OF THE RESURFACING COURSE OR COURSES SPECIFIED IN THESE PLANS.

TACK COAT:

THE TACK COAT OPERATION SHALL BE AS DETERMINED AT A PRE CONSTRUCTION CONFERENCE AS PER 407.05 AND APPLICATION RATES SHALL NOT EXCEED 0.45 LITER PER SQ. METER.

ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR:

THIS ITEM SHALL BE USED WHERE DIRECTED. THE DEPTH OF REPAIRS SHALL BE APPROXIMATELY 75 mm.

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED:

PART 1	3 CUBIC METERS
TOTAL TO GENERAL SUMMARY	- 3 CUBIC METERS

ITEM 253 PAVEMENT REPAIR:

THIS ITEM SHALL BE USED WHERE DIRECTED. THE EDGE OF THE PAVEMENT REMOVAL SHALL BE SAWED FULL DEPTH WITH A DIAMOND SAW PRIOR TO REMOVAL. THE ITEM 301 SHALL BE PLACED IN TWO EQUAL LIFTS.

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED:

PART 1	3 CUBIC METERS
TOTAL TO GENERAL SUMMARY	- 3 CUBIC METERS

ITEM 604 MONUMENT ASSEMBLY, AS PER PLAN:

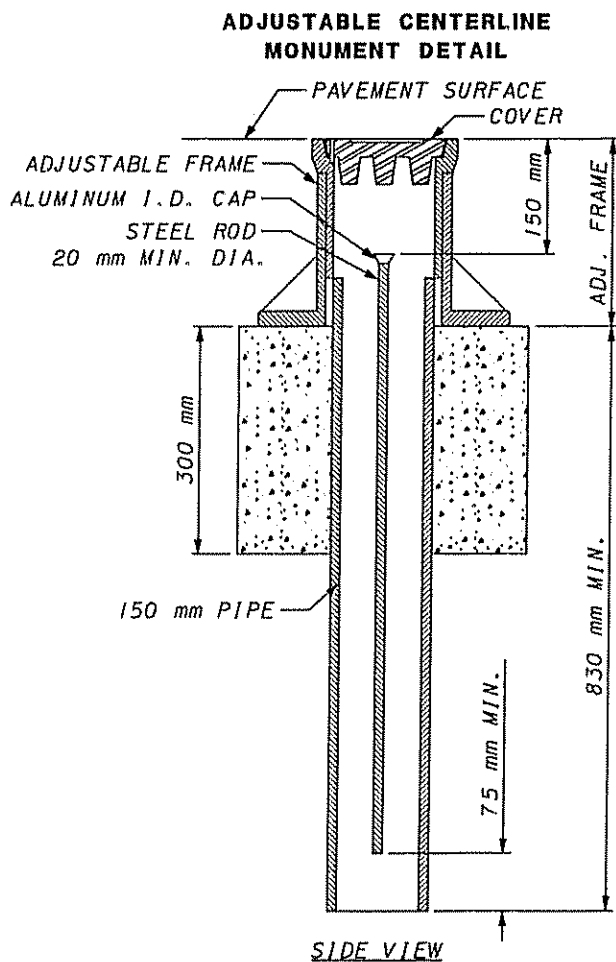
THIS WORK SHALL CONSIST OF FURNISHING AND PLACING CENTERLINE MONUMENTS AT THE FOLLOWING INTERSECTIONS:

PART 1

- 1.) S.R. 762 and Twp. Rd. 154 (Burgeit Rd.)
- 2.) S.R. 762 and Twp. Rd. 33 (Matville Rd.)
- 3.) S.R. 762 and Co. Rd. 143 (Welch Rd.)
- 4.) S.R. 762 and Twp. Rd. 136 (Gibson Rd.)

A REGISTERED SURVEYOR FROM DISTRICT SIX SURVEY DEPARTMENT SHALL BE RESPONSIBLE FOR REFERENCING AND VERIFYING THE LOCATIONS OF THE CENTERLINE MONUMENTS. THE CONTRACTOR SHALL NOTIFY THE SURVEY DEPARTMENT (614-363-1251) 48 HOURS PRIOR TO START OF MONUMENT WORK. PAYMENT FOR THIS ITEM SHALL INCLUDE ALL NECESSARY LABOR, MISCELLANEOUS HARDWARE, AND EQUIPMENT REQUIRED FOR PLACEMENT. PAYMENT WILL BE AT CONTRACT BID PRICE PER EACH.

PART 1 TOTAL CARRIED TO GENERAL SUMMARY - 4 EACH



FOR ADDITIONAL DETAILS SEE STANDARD DRAWING RM-1.1M

ITEM 254 PAVEMENT PLANING, BITUMINOUS:

THE EXISTING WEARING COURSE SHALL BE REMOVED TO A DEPTH EQUAL TO THE DEPTH OF THE PROPOSED NEW PAVEMENT. THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE THAT MAY RESULT FROM THE PLANING OPERATION, INCLUDING CASTINGS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED, TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT. THE PLANED AREA SHALL NOT BE EXPOSED TO TRAFFIC FOR MORE THAN FIVE DAYS PRIOR TO RESURFACING. FAILURE TO COMPLY WITH THE FIVE DAY LIMIT SHALL SUBJECT THE CONTRACTOR TO LIQUIDATED DAMAGES AS PER SECTION 108.07 OF THE CMS.

ITEM 614 - TEMPORARY CENTER LINE CLASS II:

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED

PART 1 S.L.D. 0.00 - S.L.D. 17.99	
17.99 km x 1 LINE x 2 APP.	= 35.98 km
TOTAL CARRIED TO GENERAL SUMMARY	= 35.98 km

ITEM 614 - WORK ZONE MARKING SIGNS:

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED AS PER STANDARD DRAWING MT-99.10M

PART 1 S.L.D. 0.00 - S.L.D. 17.99	
R-33-30 "DO NOT PASS"	= 34
R-34-30 "PASS WITH CARE"	= 33
OW-167-36 "NO EDGE LINES"	= 20
TOTAL CARRIED TO GENERAL SUMMARY	= 87

ITEM 619, FIELD OFFICE TYPE A, AS PER PLAN:

UNDER THIS ITEM, THE CONTRACTOR SHALL PROVIDE A FIELD OFFICE MEETING ALL REQUIREMENTS OF ITEM 619, FIELD OFFICE, TYPE A WITH THE FOLLOWING MODIFICATION. INSTEAD OF PROVIDING ONLY ONE PHONE LINE AS PER CMS, THE CONTRACTOR SHALL PROVIDE TWO SEPARATE PHONE LINES FOR THE FIELD OFFICE ON THIS PROJECT. THE FOLLOWING QUANTITY HAS BEEN CARRIED TO GENERAL SUMMARY: ITEM 619 FIELD OFFICE, TYPE A, AS PER PLAN - LUMP SUM

ITEM 623 - CONSTRUCTION LAYOUT STAKES, AS PER PLAN:

THIS ITEM SHALL CONSIST OF STATIONING USING 1 m LATH STAKES. THE STAKES SHALL BE SPACED AT 50 m INTERVALS AND SHALL EXTEND THROUGHOUT THE LENGTH OF THE PROJECT AND THROUGHOUT THE LENGTH OF ALL RAMPS. PLACEMENT OF THE STAKES SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED OR MISSING STAKES.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH PRICE SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

29-OCT-1996
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CALCULATED
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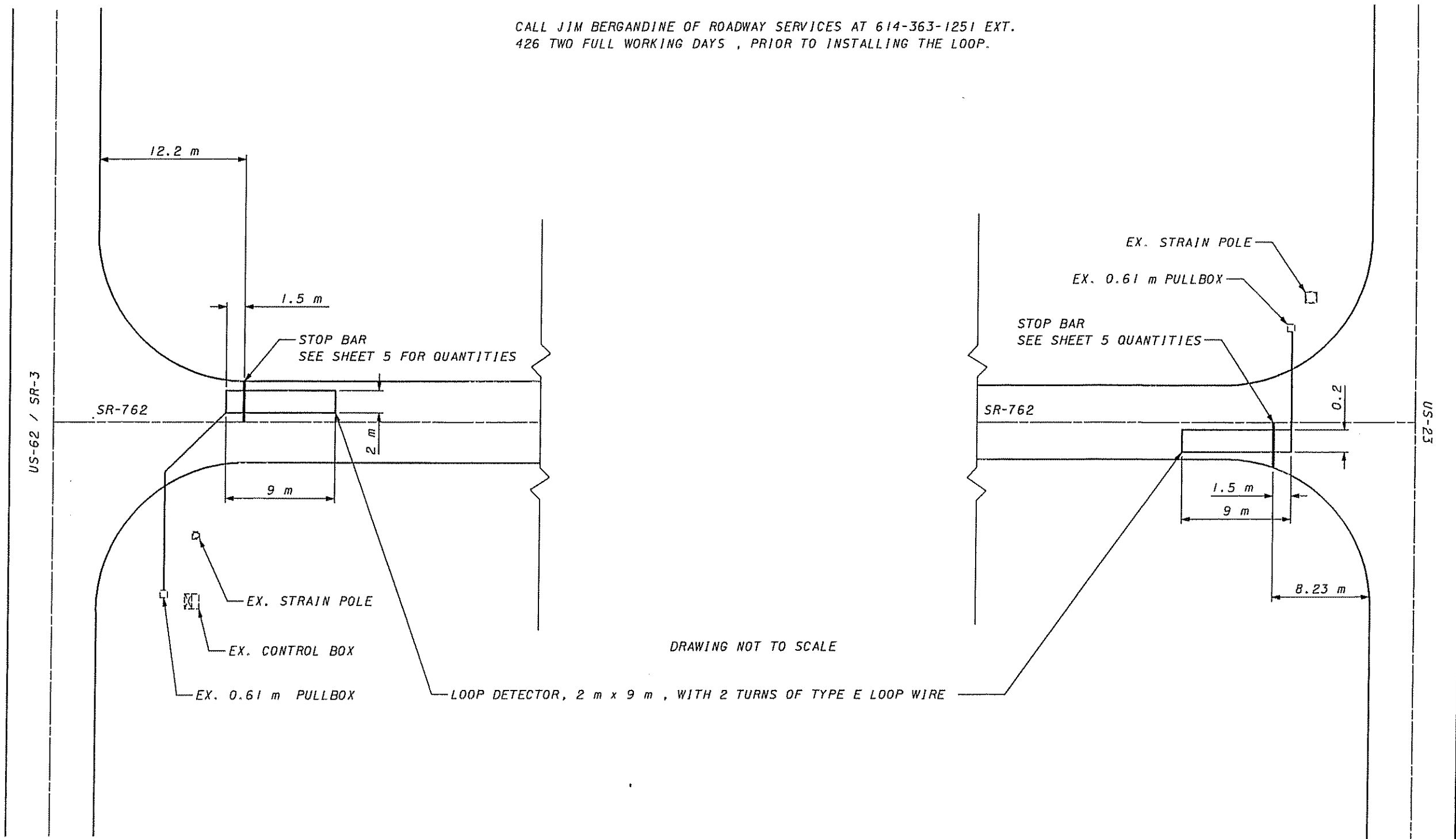
PLAN NO.

GENERAL NOTES

PIC-762-0.000

WIRE, TYPE E
 THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED AND CARRIED TO
 THE GENERAL SUMMARY TO BE USED, AS DIRECTED, TO REPLACE
 DAMAGED LOOPS AT THE BEGINNING AND END OF THIS PROJECT:
 ITEM 632 LOOP DETECTOR PAVEMENT CUTTING 60 m
 ITEM 632 LOOP DETECTOR WIRE, TYPE E 143 m

CALL JIM BERGANDINE OF ROADWAY SERVICES AT 614-363-1251 EXT.
 426 TWO FULL WORKING DAYS , PRIOR TO INSTALLING THE LOOP.



DRAWING NOT TO SCALE

LOOP DETECTOR, 2 m x 9 m , WITH 2 TURNS OF TYPE E LOOP WIRE

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CHECKED
 DESIGNED

PLAN NO.

LOOP DETECTOR REPLACEMENT

PIC-762-0.000

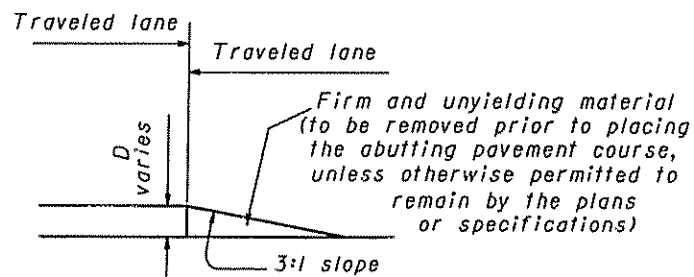
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GENERAL NOTES

1. It is intended that this drawing be used for treatment of drop-offs that develop during construction operations, and that are not otherwise provided for in the construction plans. Where the plans do not provide specific items for labor, equipment, or materials to implement the drop-off treatments specified hereon, they shall be included for payment in the lump sum bid for Item 614 - Maintaining Traffic.
2. While the need for certain advisory signing is noted hereon, it is not intended that this be indicative of all signing that may be required to advise or warn motorists, and all requirements of the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) must be fulfilled.
3. In urban or otherwise heavily developed areas where pedestrians and/or bicyclists may be present in significant numbers, additional signing and protective measures other than those shown hereon may be required.
4. The drop-off treatment selected for use at any given location shall be as appropriate for the prevailing conditions at the site.
5. Where concrete barrier is specified, it shall be in accordance with Standard Construction Drawing MC-9.2M and Item 622.
6. When drums are specified for a dropoff condition, a minimum number of four drums shall be used. Spacing shall be as indicated in the plans or as specified in the OMUTCD.
7. When OW-151 (Low Shoulder) signs or OW-171 (Uneven Lanes) and OWP-171 signs are required, they shall be placed 230 meters in advance of the condition, on all intersecting entrance ramps within the limits of the condition and immediately beyond all intersecting roadways within the limits of the condition. When the dropoff condition extends more than 800 meters, additional signs should be erected at intervals of 1.6 kilometer or less.
8. For locations, such as at ramps, lane shifts, lane closures, etc., where traffic is required to negotiate any difference in elevation between pavements, a 3:1 slope treatment similar to the Optional Wedge Treatment shall be provided.
9. Portable concrete barrier shall be placed on the same level as the traffic surface and shall not encroach on lane width(s) designated as the minimum required for traffic use. Where drums are used, and their presence would reduce traveled lane widths to less than 3.0 m, drums may be placed on the opposite level from that of traffic provided the dropoff depth does not exceed 127 mm and approval is granted by the Project Engineer.
10. Pavement Repairs (or similar work)
 - a. Lengths greater than 20 meters - utilize appropriate treatment from Condition I.
 - b. Lengths of 20 meters or less - repairs shall be effected in accordance with Item 255.08. Drums may be used as a separator adjacent to the traveled lane.

**OPTIONAL WEDGE TREATMENT
(MILLING OR RESURFACING)**

1. This treatment may be used when permitted for Condition I only.
2. OW-171 and OWP-171 signs required.



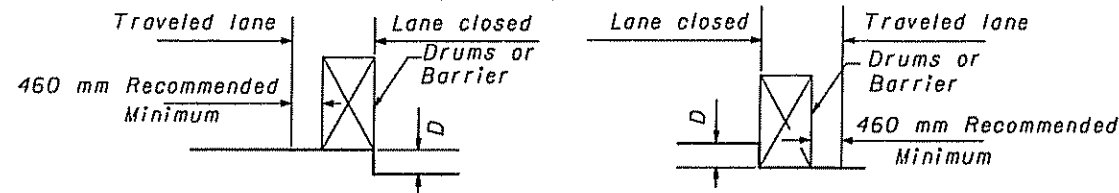
CONDITION I

DROPOFFS BETWEEN TRAVELED LANES

1. These treatments are to be used for resurfacing, pavement planing, excavation, etc. between or within traveled lanes.

D (mm.)	Treatment
<38	Erect OW-171 and OWP-171 signs.
>38-76	1) Lane closure utilizing drums* as shown below OR 2) Optional Wedge Treatment
>76-127	Lane closure utilizing drums as shown below.
>127	Lane closure utilizing portable concrete barrier as shown below.

*Cones may be used for daytime only conditions.



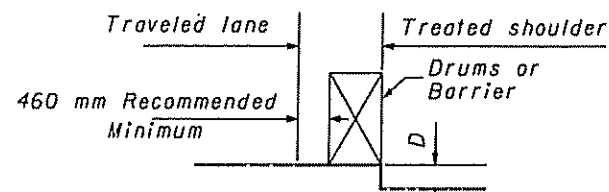
CONDITION II

DROPOFFS WITHIN GRADED SHOULDER AREA

1. The treatments indicated below are for use in conjunction with resurfacing, planing, or excavations within the graded shoulder area.
2. The graded shoulder area is that flat or gradually sloping area between the edge of a normally traveled lane and the more steeply sloping ditch foreslope or embankment area (improved with aggregates, asphaltic materials, or concrete). For the purposes herein, its maximum width shall be considered to be 3.6 meters.

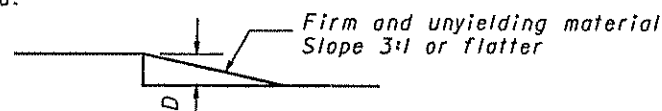
D (mm)	Treatment
<38	1) If edgelines are present, no treatment necessary OR 2) Erect OW-171 and OWP-171 signs.
>38-127	1) If min. lane width* requirements can be met, maintain lanes utilizing drums as shown below OR 2) If min. lane width* requirements cannot be met, close adjacent lane utilizing drums OR 3) Optional Shoulder Treatment.
>76-305 Daylight only	If min. lane width* requirements can be met, maintain lanes utilizing drums as shown below.
>76-610	1) If min. lane width* requirements can be met, maintain lanes utilizing portable concrete barrier as shown below. OR 2) If min. lane width* requirements cannot be met, close adjacent lane utilizing drums.
>610	Lane closure utilizing portable concrete barrier as shown below.

* Minimum lane widths shall be 3.0 meters unless otherwise specified in the plans.



OPTIONAL SHOULDER TREATMENT

1. This treatment may not be used within a bituminous shoulder where a hot longitudinal joint per Item 401.15 is required.
2. OW-151 signs required.



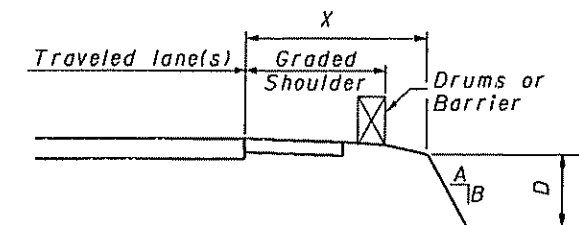
CONDITION III

DROPOFFS BEYOND GRADED SHOULDER OR BACK OF CURB

1. See Note 2 under Condition II.
2. Use Chart A or B below, as applicable.

CHART A

- USE FOR:
1. Uncurbed Facilities.
 2. Curbed Facilities, where:
 - a. Curbs are less than 150 mm in height.
 - b. Curbs are 150 mm or greater in height and the legal speed is 70 km/h or greater.

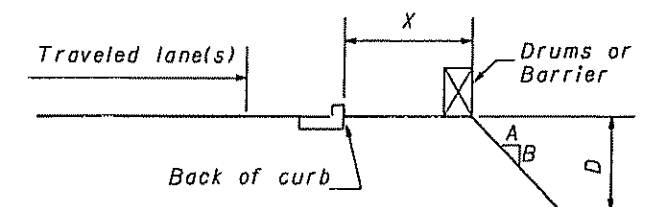


X (m)	D (mm)	A/B	Treatment Required	
			Day	Night
0-1.2	Any	Any	(a)	(a)
1.2-9.1	Any	3:1 or Flatter	None	None
1.2-3.6	<76	Steeper than 3:1	None	None
1.2-3.6	>76-<305	Steeper than 3:1	Drums	Drums
1.2-3.6	>305	Steeper than 3:1	Drums	Barrier
>3.6-6.1	<305	Steeper than 3:1	None	None
>3.6-6.1	>305-<610	Steeper than 3:1	Drums	Drums
>3.6-6.1	>610	Steeper than 3:1	Drums	Barrier
>6.1-9.1	<610	Steeper than 3:1	None	Drums
>6.1-9.1	>610	Steeper than 3:1	Drums	Barrier
>9.1	Any	Any	None	None

(a) Use treatment specified under Condition II.

CHART B

- USE FOR: Curbed facilities, where the curb is 150 mm or greater in height and the legal speed less than 70 km/h.



X (m)	D (mm)	A/B	Treatment Required	
			Day	Night
0-3.0	<305	Any	None	Drums
0-3.0	>305	Any	Drums	Drums
>3.0	Any	Any	None	None

M E T R I C

SHEET NUMBER

2	3	4	5	7	8	9	ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
				1133			202	54100	1133	EACH	RAISED PAVEMENT MARKER REMOVED FOR STORAGE	
					3		251	01002	3	CU METER	PARTIAL DEPTH PAVEMENT REPAIR	
	2245				3		253	02000	3	CU METER	PAVEMENT REPAIR	
							254	01000	2245	SO METER	PAVEMENT PLANING, BITUMINOUS	
50755	3011						407	10000	53766	LITER	TACK COAT	
1466	87						448	14050	1553	CU METER	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, AC-20 (SPOT LEVELING)	
3609	215						448	16000	3824	CU METER	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, AC-20	
					4		604	38501	4	EACH	MONUMENT ASSEMBLY, AS PER PLAN	8
					87		614	12460	87	EACH	WORK ZONE MARKING SIGN	
					35.98		614	21400	35.98	KILOMETER	TEMPORARY CENTER LINE, CLASS II	
	176	1646					617	10100	1822	CU METER	COMPACTED AGGREGATE, TYPE A	
				156			617	25000	50	CU METER	WATER	
				977			621	00100	156	EACH	RAISED PAVEMENT MARKER	
							621	00200	977	EACH	RAISED PAVEMENT MARKER, INSTALLATION ONLY	
						143	632	64900	143	METER	LOOP DETECTOR WIRE, TYPE E	
						60	632	27500	60	METER	LOOP DETECTOR PAVEMENT CUTTING	
			35.98				643	00100	35.98	KILOMETER	EDGE LINE	
			17.99				643	00300	17.99	KILOMETER	CENTER LINE	
			22.7				644	00500	22.7	METER	STOP LINE	
							614	11000	LUMP		MAINTAINING TRAFFIC	
							619	15001	LUMP		FIELD OFFICE, TYPE A, AS PER PLAN	8
							SPECIAL	61925000	LUMP		COMPUTER EQUIPMENT FOR TYPE A OFFICE	
							623	10001	LUMP		CONSTRUCTION LAYOUT STAKES, AS PER PLAN	8
							624	10000	LUMP		MOBILIZATION	

CALCULATED	DELETED
PLAN NO.	
GENERAL SUMMARY	
PIC-762-0.000	

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