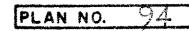
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OHIO DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

PART			CEATIONS	PROJECT T	ERMINII	NET	TOWNSHIP	CITY	VILLAGE
	COUNTY	ROUTE	SECTIONS	BEGIN	END	LENGTH MILES	IOWNShip	GILI	VICEAGE
1	PIC S	R-762	(0.00) (7.65)	0.00	9.23	8.55	-		
2	PIC 5	2-762	(6.97-7.36)	6.97	7.65	0.68			Commercial Point
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The Standard 19 83 Specifications of the State of Ohio, Department of Transportation, including changes and Supplemental Specifications listed in the plans and proposal shall govern these improvements.

I hereby approve these plans and declare that the making of these improvements will require the closing of the highways to traffic on Parts No. will be provided by State forces. The closing to traffic of the highways will not be required on Parts No. I and Z and provisions for the maintenance and safety of traffic

Engineer of Maintenance

Approved Assistant Deputy Director, Program Development Approved Chief Engineer, Construction

Approved Chief Engineer, Design

Assistant Director, Department of Transportation

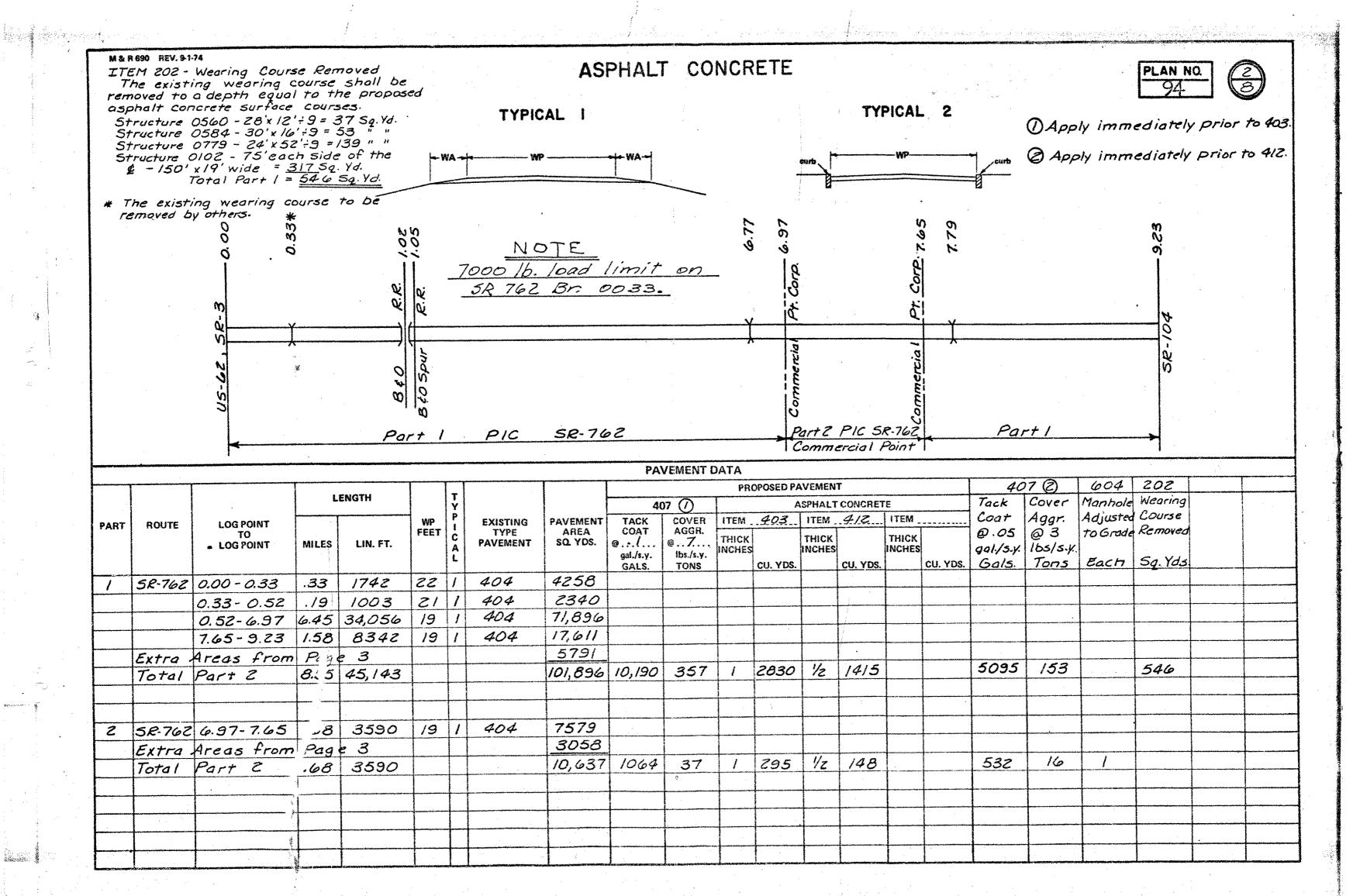
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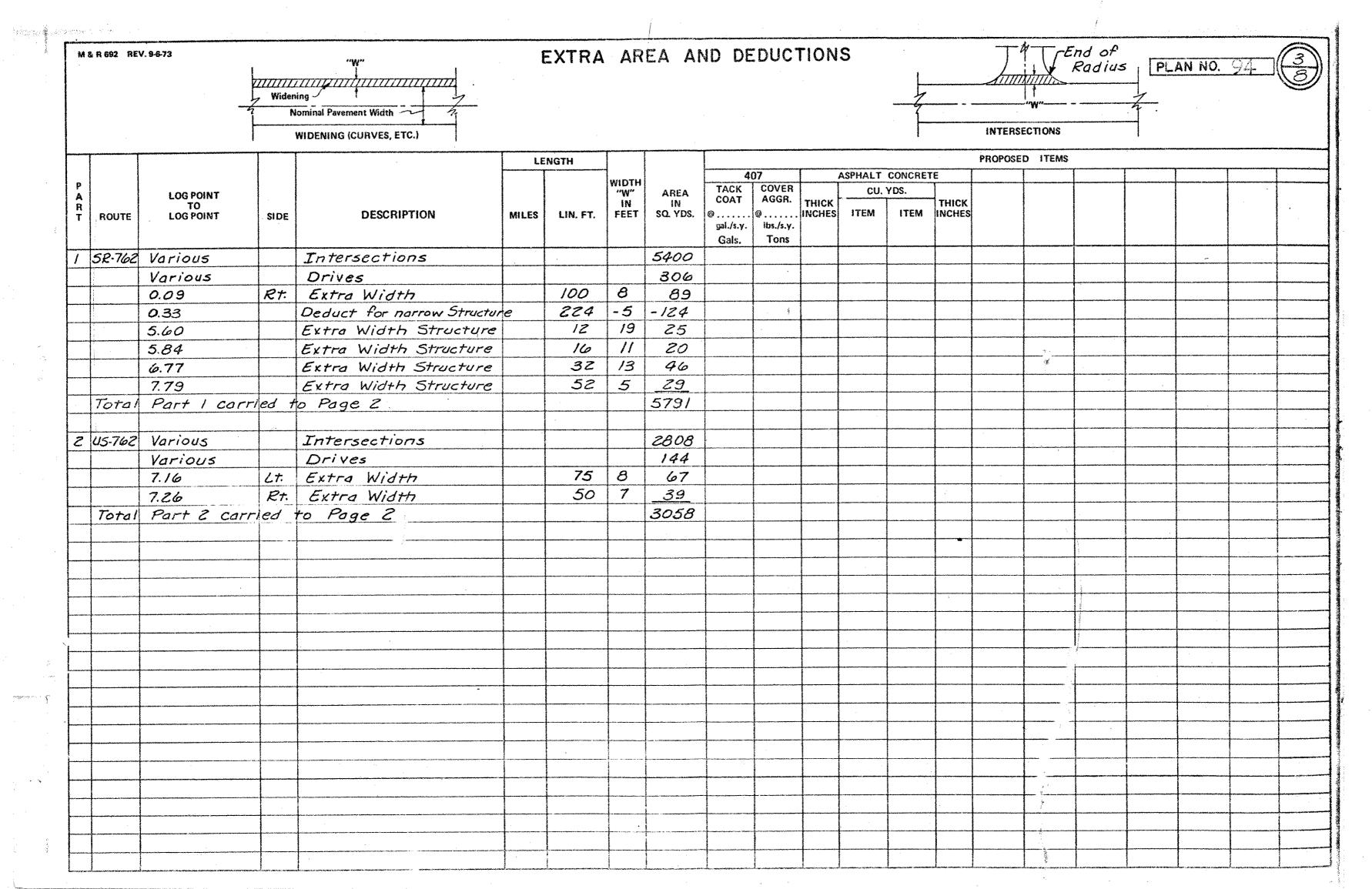
	IDARO VINGS	SUPPLEMENTAL SPECIFICATIONS					
BP-5	7-16-81						
<u></u>							
House and influence Williams representative ANNESS PRO-							

LOCATION MAP

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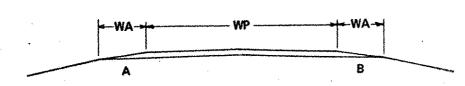


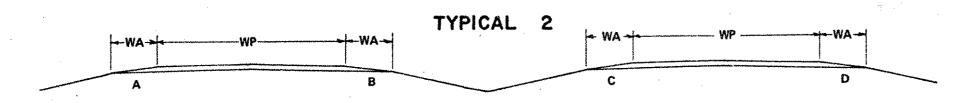


M&R 691 REV. 8-1-78

SHOULDER TREATMENT

TYPICAL I





No deductions made in quantities at Structures. Extra material to be used for low berms, as directed.

*NOTES

- SEAL COAT: After completion of the mix the Seal Coat shall be applied when directed by the Engineer.
- 2. PENETRATION CHOKE: Choke to be applied in two applications; approximately 0.004 cu. yd./sq. yd. shall be applied immediately on the mix after initial rolling. Not earlier than two days nor later than five days following the final rolling the penetration coat and final choke application shall be performed in accordance with the provisions of 409.07 and 409.08.
- 3. MIX BITUMINOUS MATERIAL: Include 0.20 gal./sq. yd. to be applied as a penetration.
- 4. PRIME COAT: A minimum of 36 hours shall elapse after completion of Prime Coat before any subsequent treatment.
- 5. MIX: Mix to be completed on shoulders within days following completion of the adjacent pavement.
- 6. SHIELD: The contractor shall provide a shield to prevent the spraying or drifting of liquid bituminous material onto the edge of the pavement or edgelines. The attention of the contractor is directed to 107.12 of the Specifications.
- 7. APPLICATION® RATE: The rate of application for mix bituminous material shall be gal. per sq. yd. for slag or gal. per sq. yd. for gravel or stone.
- 8. CENTRAL MIXING: When central mixing is used, the mix bituminous material shall be reduced 0.20 0.25 gal./sq. yd. to prevent in-transit drainage and applied as a penetration.



3 Use I'z" average to allow for low berms.

SHOULDER DATA

								£χ	CIST	ING					4	07	The state of the s	405	-	408	Asphal	t Conc	6	17	Avenue Hand
		LOG POINT TO LOG POINT	LEI	NGTH	Т	YPE - WIDTH(ft.)				Marine de la companya	-	ck		IIX	CHOKE	PRIME	403	412	Shoulder Preparation	Compacted Aggregate	*N				
P A R T					Y		:		3	C		C D		AREA	Bit. Matl.	Cover Aggr.	Bit. Matl.	Mix Aggr.	Aggr.	Bit. Mati.	Ava.	Avg.		Aggregate	0
	ROUTE		MILES	LIN. FT.	I C A L	T W I D D T H	W I D T H	TYPE	W I D T H	TYPE	W I D T H	T- Y- P. E	W I D T H	SQ. YDS.	@ :/gal./s.y.	e 7	@ gal./s.y. Gals.	ec.y./s.y.	©c.y./s.y.	@ gal./s.y. Gals.	l" Cu. Yds.	Avg.	Sq. Yds.	③ Cu. Yds.	E
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614 Temporary Pavement Markings

The contractor shall furnish, install, maintain, and, when necessary, remove temporary retroreflective pavement markings on reconstructed resurfaced or temporary roads within the work limits, in accordance with the following requirements

The temporary markings shall be complete on all pavement courses exposed to traffic at the end of each day's operation. Where permanent markings are called for in these plans, the contractor shall furnish and place the permanent markings within 30 calendar days, following completion of all surface courses in a single roadway or prior to the end of the construction season, whichever comes first.

Temporary markings shall be placed in accordance with the following requirements, unless otherwise specified in the plans:

Center lines and lane lines shall consist of 12"x4" segments spaced at a maximum 40' center to center; channelizing lines shall be 12"x4" segments spaced at maximum 20' center to center. Freeway and expressway gore markings shall be two continuous lines, 50' long, 4" wide. Edge lines shall consist of 12"x4" segments spaced at a maximum 20' center to center.

The material furnished shall be flexible retroreflective preformed pressure sensitive tape for pavement lines. It shall be free of cracks with straight edges and consist of pigment and fillers, but have sufficient binder and plasticizer to retain glass beads having an appropriate refractive index to meet minimum reflective intensity standards outlined in the manufacturers information. Naterial shall be Flexolite "Wet Reflective", 3M "S.otchlane," or approved equal.

Glass beads shall be mixed uniformly throughout the marking material with sufficient surface beads to provide optimum reflectorization at all times.

The material shall have a precoated adhesive layer for pavement application without the use of heat, solvents or additional adhesives The adhesive shall be sufficient to retain complete markings on the pavement surface throughout the useful life of the markings.

White marking material shall be free of tint. Yellow material shall conform to Color No. 33538 of Federal Standard 595.

In addition, all applicable manufacturers material and application instructions, in force at the time of placement, shall be adhered to. The contractor shall furnish to the engineer certification that the material supplied meets the properties specified herein.

Markings shall be accurately laid out in conformance with 621.051 and shall be located in a true line on the center line, lane line, for channelizing line where normal permanent marking would lie, unless otherwise specified in the plans. The temporary tape shall be placed by rolling the material into the surface. * edge line

As an alternate material to pavement marking tape, the contractor may furnish and apply painted retroreflective pavement markings conforming to 621. The width and length of painted segment shall be the same as required for temporary tape material. The paint application rate shall be not less than 16 gallons per mile for a solid line nor less than 0.4 gallons per mile for the 12"x4" dashed line.

The contractor shall provide coplete pavement markings for all temporary roads constructed for this project, in a produce with material and performance requirements described herein and in the Ohio anual as defined in 614.03.

In addition to the requireme: of 614.03, the contractor shall, prior to placing temporary markings, remove all extring conflicting markings that are visible to the traveling public during daylight or n. httime hours. When temporary markings are no longer needed, any conflicting markings visible to the traveling public shall be removed by the contractor before the flow of traffic is diverted to the next phases. Removal of existing or temporary markings shall be performed in accordance with 621.134. The cost for removal of conflicting markings shall be incidental to the various pay items unless specically paid for as a separate item.



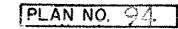
Method of measurement and basis of payment shall be in conformance with 621.15 and 621.16 respectively for:

PLAN NO.

Item	Unit	Description
614	Miles	Temporary Lane Lines
614	Miles	Temporary Center Lines
614	Miles/Lin. Ft	Temporary Channelizing Lines
614	Lin. Ft.	Temperary Gore Marking
614	Lin. Ft.	Removal of Temporary Marking
614	Miles	Temporary Edge Lines

ITEM 614 - Temporary Center Lines

Part 2: 5.L.M. 6.97 to 7.65 x & courses = 1.36 Miles



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·			GENERAL SUN	MMARY		
ITEM	Total Part I	Total Part 2		GRAND TOTAL Parts 1¢2	UNIT	DESCRIPTION
407	15,285	1612		16,897	Gals.	Tack Coat
407	510	54		564	Tons	· Cover Aggregate
403	2830	300		3130	Cu. Yds.	Asphalt Concrete AC-20
404					Cu. Yds.	Asphalt Concrete AC-20
412	1415	150		1565	Cu. Yds.	Asphalt Concrete, as per plan
604		/		1	Each	Monholes Adjusted to Grade
202	546			546		Wearing Course Removed
					-	
				·		
617	/	. /		2	M.Gal.	Water
614	17.10	1.36		18.46	Miles	Temporary Center Lines
624	Lump	Lump		Lump	Lump	Mobilization

TRAFFIC:

617

617

614

Traffic shall be maintained at all times. The length of restricted traffic zones shall be kept to a minimum consistent with the specification requirements for protection of completed corress.

60

Lump

RAILROAD CROSSINGS:

836

Lump

The new surface course shall be franered or butt jointed to meet the rail grades as speriled.

ALIGNMENT AND PROFILE:

The work proposed by this project is for the resurfacing of the existing pavement, the alignment of the existing pavement will not be changed, and the profile of the proposed surface will be similar to that of the existing pavement except that it will be raised an amount equal to the thickness of the resurfacing course or courses specified in these plans.

INTERMEDIATE COURSE, SPOT LEVELING AND

PATCHING:

This material shall be placed in a separate operation where and as directed by the engineer.

TACK COAT:

The tack coat operation shall be as determined at a pre-construction conference as per 407.05, and application rates shall not exceed 0.10 gal. per sq. yd.

COVER AGGREGATE:

Cover aggregate shall conform to 703.06.

GENERAL NOTES

JOINTS

The top surface of the longitudinal and transverse joints shall be painted six (6) inches wide with the same bituminous material used in the 412 mixture as directed. Application rate shall be at least 0.25 gal./s.y. The cost of this operation to be incidental to the Unit Cost of the 412 Asphalt Concrete.

Shoulder Preparation

Compacted Aggregate

Maintaining Traffic

Sq. Yds.

Cu. Yds.

Lump

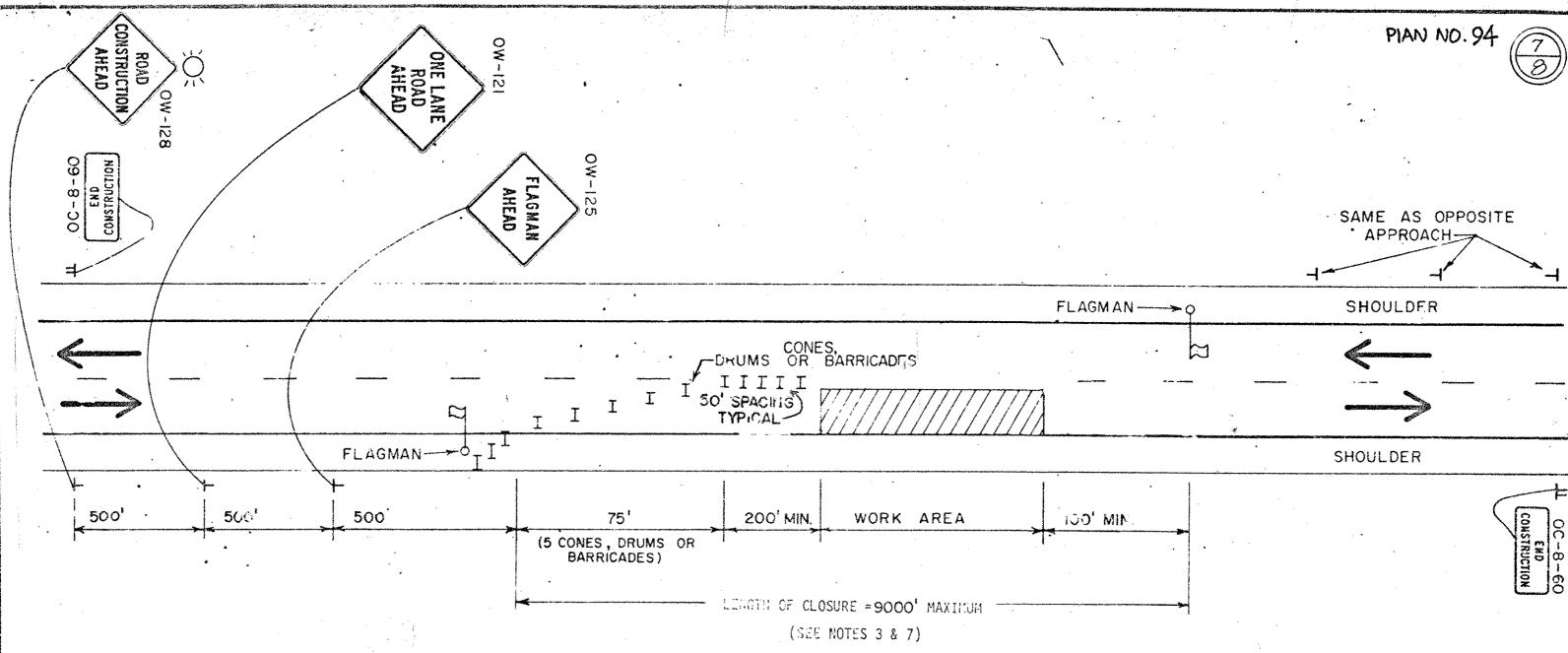
896

Lump

PAVEMENT MARKING

Pavement marking (28'-24" Stop Bar) will be placed at indicated locations. The exact location will be determined at a pre-construction conference. The State will furnish and place the plastic markings at the locations and the Contractor shall roll the plastic markings into the new surface during the finish rolling operation. The cost of rolling shall be included in the Unit Bid Price for Item 404 - Asphalt Concrete.

The Contractor shall provide, erect and remove Fresh Tar signs in accordance with O.M.U.T.C.D. Payment for the above to be included in the Unit Price Bid for Maintaining Traffic.



GENERAL NOTES

- 1. FLAGMEN SHALL BE USED TO CONTROL TRAFFIC CONTINUOUSLY FOR AS LONG AS ONE LANE OPERATION IS IN EFFECT. FLAGMEN SHALL BE ABLE TO COMMUNICATE WITH EACH OTHER AT ALL TIMES EITHER VERBALLY OR BY MEANS OF RADIO OR FIELD TELEPHONES. FLAGMAN STATIONS SHALL BE ADEQUATELY ILLUMINATED FOR NIGHT TIME OPERATIONS BY USE OF A 175 WATT MINIMUM LUMINAIRE.
- 2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS FOR THE LANE CLOSURES DURING DAYLIGHT HOURS ONLY.
- 3: WHEN THE AMBIENT TEMPERATUR: EXCEEDS 80 DEGREES F, THE ENGINEER MAY INCREASE THE LENGTH OF CLOSURE TO ALLOW FOR SUFFICIENT COOLING OF THE NEW PAVEMENT.

THE ENGINEER MAY CHORTEN FM. MAXIMUM ALLOWABLE LENGTH OF CLOSURE TO RELIEVE EXCESSIVE TRAFFIC BACKUPS.

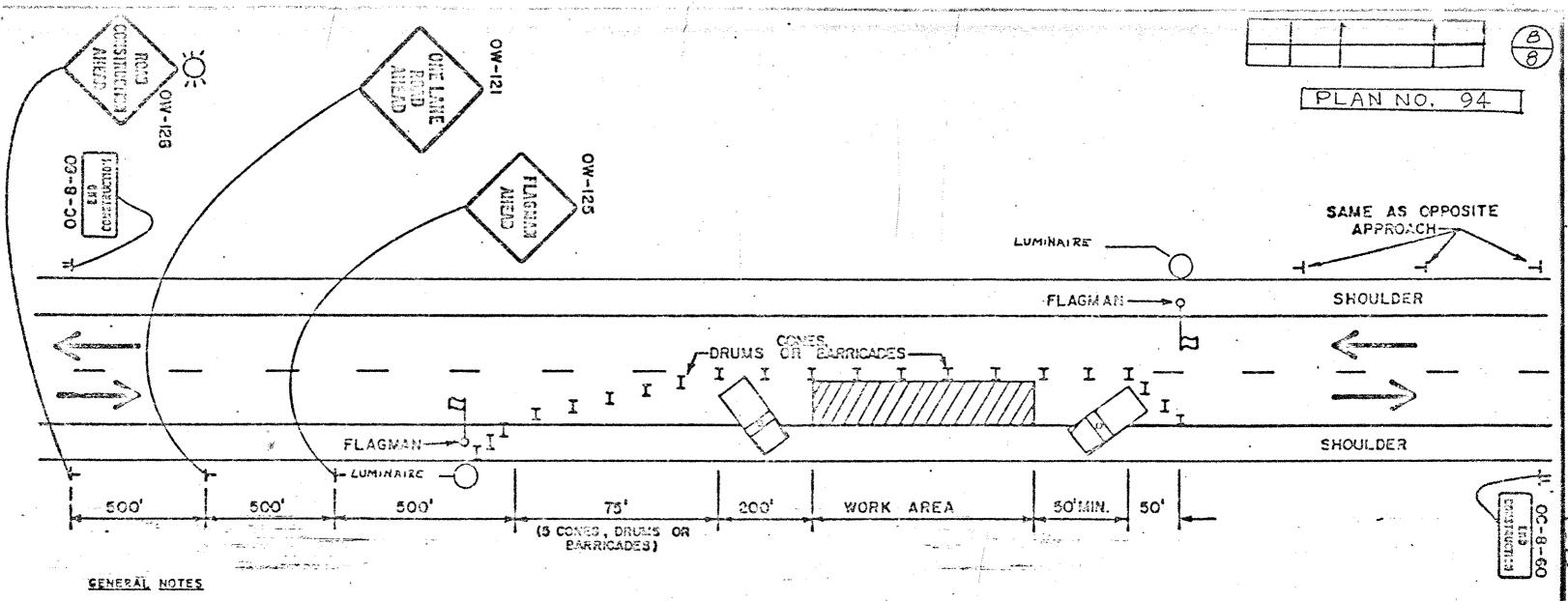
ALL TRAFFIC CONTROL SIGNS, CHANNELIZING DEVICES, AND FLAGMEN SHALL BE MOVED FORWARD BEFORE THE CLOSURE REACHES THE MAXIMUM ALLOWABLE LENGTH. ONLY ONE SIDE OF THE ROAD SHALL BE CLOSED AT ANY TIME IN A WORK AREA.

- 4. THE TYPE B HIGH INTENSITY BARRICADE WARNING LIGHT SHOWN ON THE ROAD CONSTRUCTION AHEAD SIGN IS REQUIRED WHENEVER NIGHT LANE CLOSURE IS NECESSARY.
- TYPE C STEADY BURNING BARRICADE WARNING LIGHTS SHALL BE ERECTED ON DRUMS OR BARRICADES FOR NIGHT LANE CLOSURES.
- 6. THE ADVANCE WARNING SIGNS "OW-128" "OW-121" AND "OW-125" SHALL BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
- . WITHIN THE LENGTH OF CLOSURE, PROVISION SHALL BE MADE TO CONTROL TRAFFIC ENTERING FROM INTERSECTING STREETS AND MAJOR DRIVES AS NECESSARY TO PREVENT WRONG WAY MOVEMENTS AND TO KEEP VEHICLES OFF OF NEW PAVEMENT NOT READY FOR TRAFFIC.

OHIO DEPARTMENT OF TRANSPORTATION

FLAGMEN CLOSING
DATE
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HIGHWAY

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DE CO.



- 1. FLAGMEN SHALL BE USED-TO CONTROL TRAFFIC CONTINUOUSLY FOR AS LONG AS ONE LANE OPERATION IS IN EFFECT. FLAGMAN SHALL COMMUNICATE WITH EACH OTHER AT ALL TIMES AS DESCRISED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES IN THE SECTION "FLAGMAN CONTROL".
- 2. CONES, DRUMS, OR BARRICADES SHALL BE SPACED AT 50 FOOT CENTERS FOR THE FIRST 1000 FEET OF THE WORK AREA AND AT A MAXIMUM OF 100 FEET FOR THE BALANCE CT THE WORK AREA. CONES MAY BE SUBSTITUDED FOR BARRICADES OR ILL DRUMS FOR THE LINE CLOSURES DURING DAYLIGHT HOURS ONLY.
- 3. SEVERAL SMALL WORK SITES CLOSE TOGETHER SHALL BE COMBINED IN 10 ONE WORK AREA TO MAKE A CLOSURE NOT 10 ME THAN 2000 FEET LONG INCLUDING TAPE S. CLOSURES MORE THAN 2000 FEET MAY 3E APPROVED BY THE ENGINEER. THE MIN MUM LENGTH BETWEEN CLOSURES SHALL BE ZOOD FEET. ONLY ONE SIDE OF THE ROAD MALL BE CLOSED IN ANY ONE WORK AREA.

- *. THE WORK TRUCKS SHOWN AT EACH END OF THE WORK AREA SHALL OF IM PLACE AND UNOCCUPIED WHENEVER MEN ARE WORKING WITHIN THE WORK AREA. THESE TRUCKS SHALL BE MOVED FROM THE PAVEMENT WHENEVER WORKHEN ARE NOT IN THE WORK AREA. OTHER PROTECTIVE DEVICES MAY BE USED IN LIEU OF THE WORK TRUCKS SHOWN WHEN APPROVED BY THE ENGINEER.
- 5. THE TYPE B HIGH INTENSITY BARRICADE WARNING LIGHT SHOWN ON THE ROAD CONSTRUCTION AHEAD SIGN, IS REQUIRED WHENEVER WIGHT LANE. CLOSURE IS NECESSARY.
- S. TYPE C STEADY BURNING BARRICADE WARNING LIGHTS SHALL BE ERECTED ON DRUMS OR BARRICADES FOR MIGHT LANE CLOSURES. MAXIMUM SPACING SHALL BE 50' CENTER TO CENTER IN ADVANCE OF THE WORK AREA AND 200' CENTER TO CENTER WITHIN THE WORK AREA.
- 7. ADEQUATE AREA ILLUMINATION TO CLEARLY IDENTIFY THE BEGINNING OF THE TRANSITION AT NIGHT SHALL BE PROVIDED BY USE OF A 175 WATT MINIMUM LUMINAIRE LOCATED ADJACENT TO FLAGMAN STATIONS AS SHOWN ABOVE. THE MOUNTING HEIGHT FOR TEMPORARY ELMINAIRES SHALL BE A MINIMUM OF 27 FEET ABOVE THE PAVEMENT AND THE MINIMUM OVERHEAD CONDUCTOR CLEARANCE SHALL BE 20 FEET ABOVE THE PAVEMENT.

OMO DEPARTMENT OF TRANSPORTATION

4/77

12/81

FLAGMEN CLOSING
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HIGHWAY

DR.GBD CX.RLB