

OHIO DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

1
8

300-83

JAN CAMP

PLAN NO. 94

PART	COUNTY	ROUTE	SECTIONS	PROJECT TERMINII		NET LENGTH MILES	TOWNSHIP	CITY	VILLAGE
				BEGIN	END				
1	PIC	SR-762	(0.00) (7.65)	0.00	9.23	8.55			
2	PIC	SR-762	(6.97- 7.36)	6.97	7.65	0.68			Commercial Point

The Standard 19 83 Specifications of the State of Ohio, Department of Transportation, including changes and Supplemental Specifications listed in the plans and proposal shall govern these improvements.

I hereby approve these plans and declare that the making of these improvements will require the closing of the highways to traffic on Parts No. _____ and that detours will be provided by State forces. The closing to traffic of the highways will not be required on Parts No. 1 and 2 and provisions for the maintenance and safety of traffic will be as indicated in the proposal.

Approved Date 12/6/82 E. J. Burns
District Deputy Director of Transportation

Approved Date 12/15/82 Robert B. Pfeifer
Engineer of Bridges

Approved Date _____
Engineer of Maintenance

Approved Date 2/25/83 James R. Longmeyer
Chief Engineer, Operations

Approved Date _____
Assistant Deputy Director, Program Development

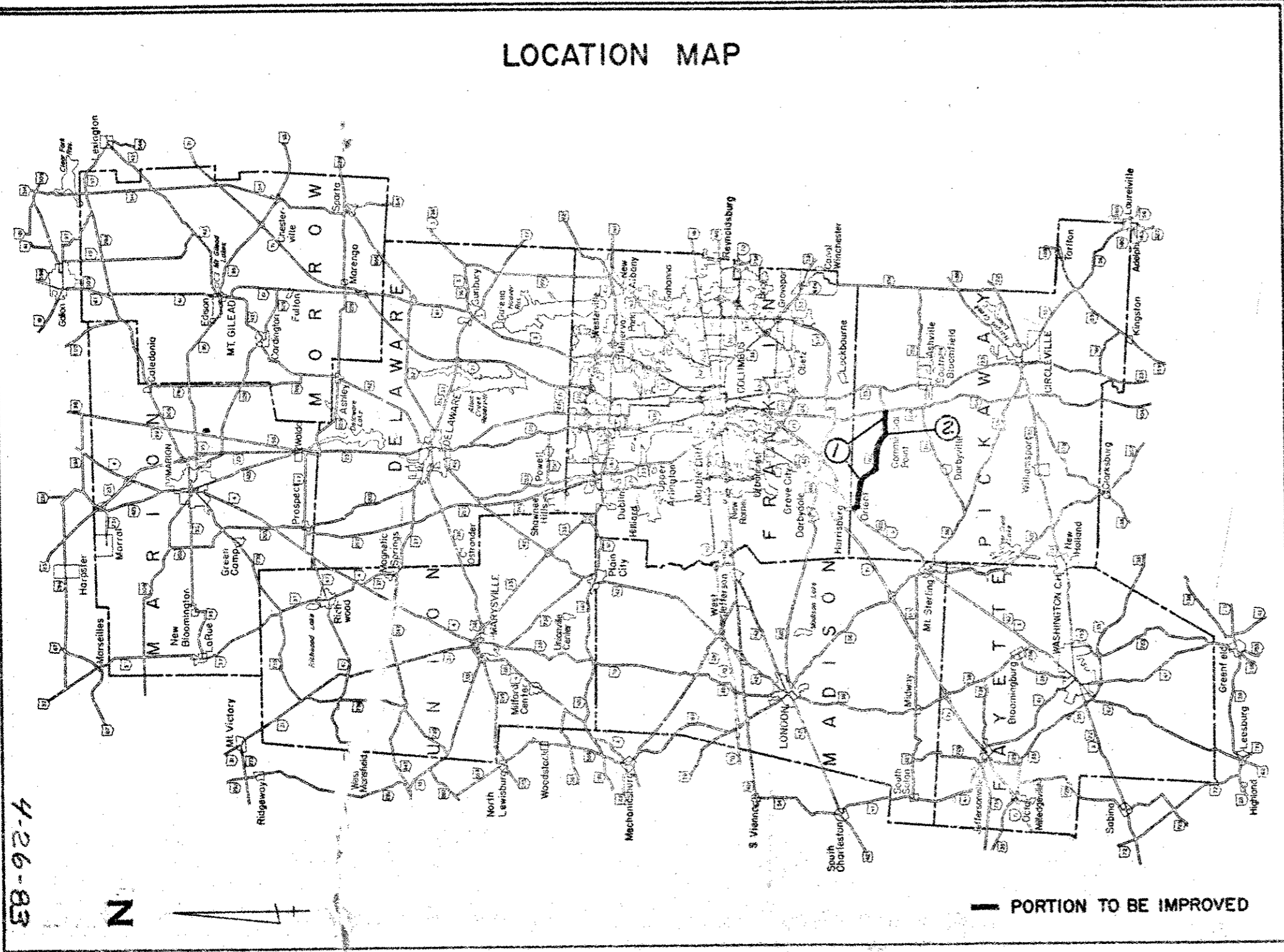
Approved Date _____
Chief Engineer, Construction

Approved Date _____
Chief Engineer, Design

Approved Date _____
Assistant Director, Department of Transportation

Approved Date 2-25-83 Warner J. Smith
Director, Department of Transportation

LOCATION MAP



STANDARD DRAWINGS	SUPPLEMENTAL SPECIFICATIONS
BP-5 7-16-81	

300

4-26-83

BN

ASPHALT CONCRETE

PLAN NO.
94

2
8

ITEM 202 - Wearing Course Removed
The existing wearing course shall be removed to a depth equal to the proposed asphalt concrete surface courses.

Structure 0560 - 28' x 12' ÷ 9 = 37 Sq. Yd.
Structure 0584 - 30' x 16' ÷ 9 = 53 " "
Structure 0779 - 24' x 52' ÷ 9 = 139 " "
Structure 0102 - 75' each side of the
E - 150' x 19' wide = 317 Sq. Yd.
Total Part 1 = 546 Sq. Yd.

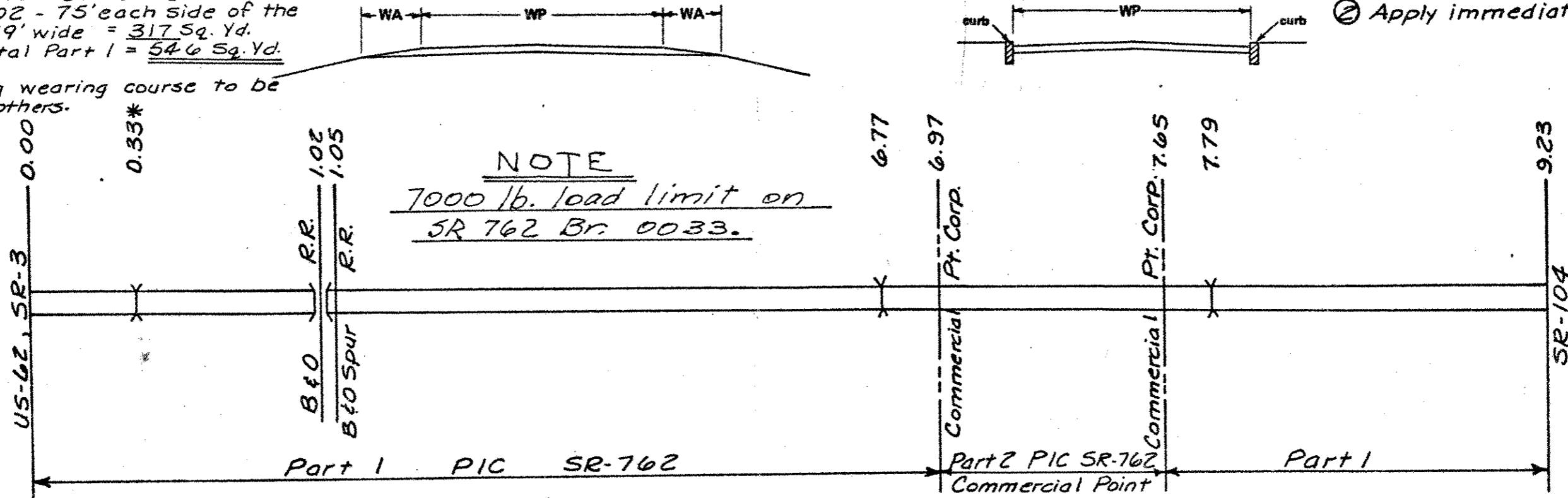
* The existing wearing course to be removed by others.

TYPICAL 1

TYPICAL 2

① Apply immediately prior to 403.

② Apply immediately prior to 412.



PAVEMENT DATA

PART	ROUTE	LOG POINT TO LOG POINT	LENGTH		WP FEET	TYPICAL	EXISTING TYPE PAVEMENT	PAVEMENT AREA SQ. YDS.	PROPOSED PAVEMENT						Tack Coat @ .05 gal./s.y. Gals.	Cover Aggr. @ 3 lbs./s.y. Tons	Manhole Adjusted to Grade Each	Wearing Course Removed Sq. Yds.	
			MILES	LIN. FT.					407 ①		ASPHALT CONCRETE		ITEM 403 THICK INCHES	ITEM 412 THICK INCHES					ITEM THICK INCHES
									TACK COAT @ .1 gal./s.y. GALS.	COVER AGGR. @ .7 lbs./s.y. TONS	CU. YDS.	CU. YDS.							
1	SR-762	0.00-0.33	.33	1742	22	1	404	4258											
		0.33-0.52	.19	1003	21	1	404	2340											
		0.52-6.97	6.45	34,056	19	1	404	71,896											
		7.65-9.23	1.58	8342	19	1	404	17,611											
Extra Areas from Page 3								5791											
Total Part 2			8.5	45,143				101,896	10,190	357	1	2830 1/2	1415		5095	153		546	
2	SR-762	6.97-7.65	.68	3590	19	1	404	7579											
		Extra Areas from Page 3								3058									
		Total Part 2			.68	3590				10,637	1064	37	1	295 1/2	148		532	16	1

614 Temporary Pavement Markings

The contractor shall furnish, install, maintain, and, when necessary, remove temporary retro-reflective pavement markings on reconstructed resurfaced or temporary roads within the work limits, in accordance with the following requirements

The temporary markings shall be complete on all pavement courses exposed to traffic at the end of each day's operation. Where permanent markings are called for in these plans, the contractor shall furnish and place the permanent markings within 30 calendar days, following completion of all surface courses in a single roadway or prior to the end of the construction season, whichever comes first.

Temporary markings shall be placed in accordance with the following requirements, unless otherwise specified in the plans:

Center lines and lane lines shall consist of 12"x4" segments spaced at a maximum 40' center to center; channelizing lines shall be 12"x4" segments spaced at maximum 20' center to center. Freeway and expressway gore markings shall be two continuous lines, 50' long, 4" wide. Edge lines shall consist of 12"x4" segments spaced at a maximum 20' center to center.

The material furnished shall be flexible retroreflective preformed pressure sensitive tape for pavement lines. It shall be free of cracks with straight edges and consist of pigment and fillers, but have sufficient binder and plasticizer to retain glass beads having an appropriate refractive index to meet minimum reflective intensity standards outlined in the manufacturers information. Material shall be Flexolite "Wet Reflective", 3M "Scotchlane," or approved equal.

Glass beads shall be mixed uniformly throughout the marking material with sufficient surface beads to provide optimum reflectorization at all times.

The material shall have a precoated adhesive layer for pavement application without the use of heat, solvents or additional adhesives. The adhesive shall be sufficient to retain complete markings on the pavement surface throughout the useful life of the markings.

White marking material shall be free of tint. Yellow material shall conform to Color No. 33538 of Federal Standard 595.

In addition, all applicable manufacturers material and application instructions, in force at the time of placement, shall be adhered to. The contractor shall furnish to the engineer certification that the material supplied meets the properties specified herein.

Markings shall be accurately laid out in conformance with 621.051 and shall be located in a true line on the center line, lane line, or channelizing line where normal permanent marking would lie, unless otherwise specified in the plans. The temporary tape shall be placed by rolling the material into the surface. * edge line

As an alternate material to pavement marking tape, the contractor may furnish and apply painted retroreflective pavement markings conforming to 621. The width and length of painted segment shall be the same as required for temporary tape material. The paint application rate shall be not less than 16 gallons per mile for a solid line nor less than 0.4 gallons per mile for the 12"x4" dashed line.

The contractor shall provide complete pavement markings for all temporary roads constructed for this project, in accordance with material and performance requirements described herein and in the Ohio Manual as defined in 614.03.

In addition to the requirements of 614.03, the contractor shall, prior to placing temporary markings, remove all existing conflicting markings that are visible to the traveling public during daylight or nighttime hours. When temporary markings are no longer needed, any conflicting markings visible to the traveling public shall be removed by the contractor before the flow of traffic is diverted to the next phases. Removal of existing or temporary markings shall be performed in accordance with 621.134. The cost for removal of conflicting markings shall be incidental to the various pay items unless specifically paid for as a separate item.

Method of measurement and basis of payment shall be in conformance with 621.15 and 621.16 respectively for:

Item	Unit	Description
614	Miles	Temporary Lane Lines
614	Miles	Temporary Center Lines
614	Miles/Lin. Ft	Temporary Channelizing Lines
614	Lin. Ft.	Temporary Gore Marking
614	Lin. Ft.	Removal of Temporary Marking
614	Miles	Temporary Edge Lines

ITEM 614 - Temporary Center Lines

Part 1: S.L.M. 0.00 to 6.97 x 2 courses = 13.94 Miles
 7.65 to 9.23 x 2 courses = 3.16 "
 Total = 17.10 Miles

Part 2: S.L.M. 6.97 to 7.65 x 2 courses = 1.36 Miles

PLAN NO.
C.C.



GENERAL SUMMARY

ITEM	Total Part 1	Total Part 2	GRAND TOTAL Parts 1&2	UNIT	DESCRIPTION
407	15,285	1612	16,897	Gals.	Tack Coat
407	510	54	564	Tons	Cover Aggregate
403	2830	300	3130	Cu. Yds.	Asphalt Concrete AC-20
404				Cu. Yds.	Asphalt Concrete AC-20
412	1415	150	1565	Cu. Yds.	Asphalt Concrete, as per plan
604		1	1	Each	Manholes Adjusted to Grade
202	546		546	Sq. Yds.	Wearing Course Removed
617	1	1	2	M. Gal.	Water
614	17.10	1.36	18.46	Miles	Temporary Center Lines
624	Lump	Lump	Lump	Lump	Mobilization
617				Sq. Yds.	Shoulder Preparation
617	836	60	896	Cu. Yds.	Compacted Aggregate
614	Lump	Lump	Lump	Lump	Maintaining Traffic

GENERAL NOTES

TRAFFIC:

Traffic shall be maintained at all times. The length of restricted traffic zones shall be kept to a minimum consistent with the specification requirements for protection of completed courses.

RAILROAD CROSSINGS:

The new surface course shall be finished or butt jointed to meet the rail grades as specified.

ALIGNMENT AND PROFILE:

The work proposed by this project is for the resurfacing of the existing pavement. The alignment of the existing pavement will not be changed, and the profile of the proposed surface will be similar to that of the existing pavement except that it will be raised an amount equal to the thickness of the resurfacing course or courses specified in these plans.

INTERMEDIATE COURSE, SPOT LEVELING AND PATCHING:

This material shall be placed in a separate operation where and as directed by the engineer.

TACK COAT:

The tack coat operation shall be as determined at a pre-construction conference as per 407.05, and application rates shall not exceed 0.10 gal. per sq. yd.

COVER AGGREGATE:

Cover aggregate shall conform to 703.06.

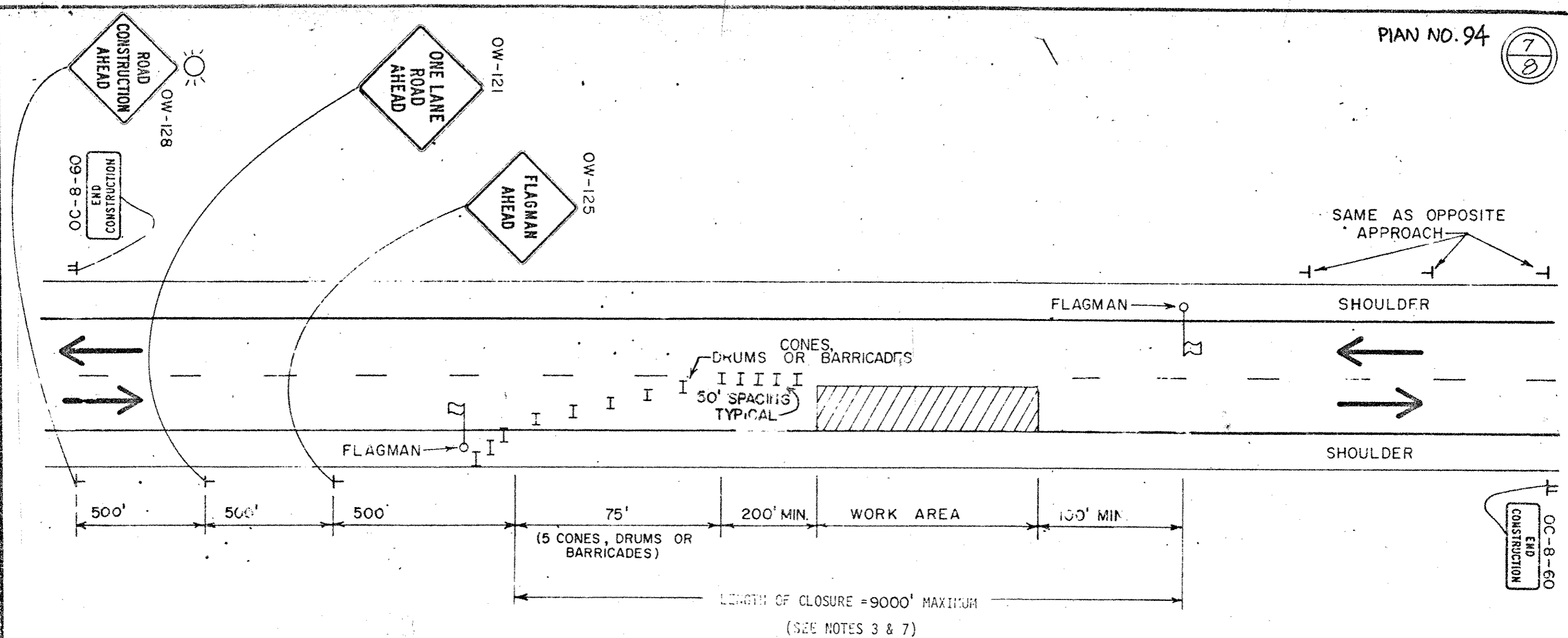
JOINTS

The top surface of the longitudinal and transverse joints shall be painted six (6) inches wide with the same bituminous material used in the 412 mixture as directed. Application rate shall be at least 0.25 gal./s.y. The cost of this operation to be incidental to the Unit Cost of the 412 Asphalt Concrete.

PAVEMENT MARKING

Pavement marking (28'-24" Stop Bar) will be placed at indicated locations. The exact location will be determined at a pre-construction conference. The State will furnish and place the plastic markings at the locations and the Contractor shall roll the plastic markings into the new surface during the finish rolling operation. The cost of rolling shall be included in the Unit Bid Price for Item 404 - Asphalt Concrete.

The Contractor shall provide, erect and remove Fresh Tar signs in accordance with O.M.U.T.C.D. Payment for the above to be included in the Unit Price Bid for Maintaining Traffic.



GENERAL NOTES

1. FLAGMEN SHALL BE USED TO CONTROL TRAFFIC CONTINUOUSLY FOR AS LONG AS ONE LANE OPERATION IS IN EFFECT. FLAGMEN SHALL BE ABLE TO COMMUNICATE WITH EACH OTHER AT ALL TIMES EITHER VERBALLY OR BY MEANS OF RADIO OR FIELD TELEPHONES. FLAGMAN STATIONS SHALL BE ADEQUATELY ILLUMINATED FOR NIGHT TIME OPERATIONS BY USE OF A 175 WATT MINIMUM LUMINAIRE.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS FOR THE LANE CLOSURES DURING DAYLIGHT HOURS ONLY.
3. WHEN THE AMBIENT TEMPERATURE EXCEEDS 80 DEGREES F, THE ENGINEER MAY INCREASE THE LENGTH OF CLOSURE TO ALLOW FOR SUFFICIENT COOLING OF THE NEW PAVEMENT.

THE ENGINEER MAY SHORTEN THE MAXIMUM ALLOWABLE LENGTH OF CLOSURE TO RELIEVE EXCESSIVE TRAFFIC BACKUPS.
4. THE TYPE B HIGH INTENSITY BARRICADE WARNING LIGHT SHOWN ON THE ROAD CONSTRUCTION AHEAD SIGN IS REQUIRED WHENEVER NIGHT LANE CLOSURE IS NECESSARY.
5. TYPE C STEADY BURNING BARRICADE WARNING LIGHTS SHALL BE ERECTED ON DRUMS OR BARRICADES FOR NIGHT LANE CLOSURES.
6. THE ADVANCE WARNING SIGNS "OW-128" "OW-121" AND "OW-125" SHALL BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
7. WITHIN THE LENGTH OF CLOSURE, PROVISION SHALL BE MADE TO CONTROL TRAFFIC ENTERING FROM INTERSECTING STREETS AND MAJOR DRIVES AS NECESSARY TO PREVENT WRONG WAY MOVEMENTS AND TO KEEP VEHICLES OFF OF NEW PAVEMENT NOT READY FOR TRAFFIC.

ALL TRAFFIC CONTROL SIGNS, CHANNELIZING DEVICES, AND FLAGMEN SHALL BE MOVED FORWARD BEFORE THE CLOSURE REACHES THE MAXIMUM ALLOWABLE LENGTH. ONLY ONE SIDE OF THE ROAD SHALL BE CLOSED AT ANY TIME IN A WORK AREA.

OHIO DEPARTMENT OF TRANSPORTATION	
FLAGMEN CLOSING 1 LANE OF A 2 LANE HIGHWAY	DATE 12/80
PAVING OPERATIONS	

