

SCHEMATIC PLAN

**@ CURVE DATA**  
 P.I. = Sta. 102+13.39  
 $\Delta = 30^{\circ}38'00''$  LT  
 $Dc = 05^{\circ}00'00''$   
 $R = 1145.92'$   
 $Ls = 300.00'$   
 $\theta_s = 07^{\circ}30'00''$   
 $LT = 200.18'$   
 $ST = 100.16'$   
 $Lc = 312.67'$   
 $Ts = 464.66'$   
 $Es = 45.59'$   
 $E_{max} = 0.083$   
 $T.S. = 97+48.73$   
 $S.C. = 100+48.73$   
 $C.S. = 103+61.40$   
 $S.T. = 106+61.40$

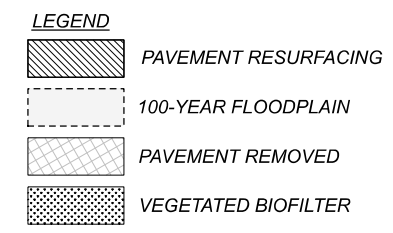
**SPIRAL DATA**  
 P.I. = Sta. 104+61.56  
 $Ls = 300.00'$   
 $\theta_s = 07^{\circ}30'00''$   
 $LT = 200.18'$   
 $ST = 100.16'$   
 $x = 298.63'$   
 $y = 26.13'$   
 $k = 149.91'$   
 $\rho = 3.27'$   
 $C = 299.77'$   
 $Start = Sta. 103+61.40$   
 $End = Sta. 106+61.40$   
 $C.B. = N64^{\circ}41'06''E$

SPLIT#	CATEGORY	GROUP	PLAN SPLIT CODE	DESCRIPTION
1	STP < 200,000	Pavement	01/S<2/PV	This split will cover pavement replacement and associated work outside the City of Circleville.
2	STP 50,000 - 200,000	Pavement	02/S50/PV	Items associated with resurfacing within the City of Circleville.
3	STP 50,000 - 200,000	Pavement	03/S50/PV	100% City items - full depth pavement within City, sidewalk, curbs, curb ramp, and pavement repairs.

**C1 CURVE DATA**  
 P.I. = Sta. 114+45.69  
 $\Delta = 04^{\circ}00'00''$  LT  
 $Dc = 01^{\circ}44'41''$   
 $R = 3284.00'$   
 $T = 114.68'$   
 $L = 229.27'$   
 $E = 2.00'$

**C2 CURVE DATA**  
 P.I. = Sta. 118+35.83  
 $\Delta = 04^{\circ}00'00''$  RT  
 $Dc = 01^{\circ}44'41''$   
 $R = 3284.00'$   
 $T = 114.68'$   
 $L = 229.23'$   
 $E = 2.00'$

**C3 CURVE DATA**  
 P.I. = Sta. 123+88.86  
 $\Delta = 49^{\circ}37'00''$  RT  
 $Dc = 08^{\circ}09'34''$   
 $R = 702.20'$   
 $T = 324.59'$   
 $L = 608.09'$   
 $E = 71.39'$



**UTILITIES**

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

AEP DISTRIBUTION  
2881 STATE ROUTE 7  
REEDSVILLE, OH 45772  
740-985-3054  
CLARKE SAUNDERS  
CMSAUNDERS@AEP.COM

AEP TRANSMISSION  
8600 SMITHS MILL ROAD  
NEW ALBANY, OH 43054  
380-205-5072  
MICHAEL CARR  
TL\_PUBLICPROJECTS@AEP.COM

CIRCLEVILLE CITY OF (PUBLIC SERVICE)  
104 E. FRANKLIN STREET  
CIRCLEVILLE, OH 43113  
740-477-8224  
JAMES A. STANLEY  
JSTANLEY@CIRCLEVILLEOH.GOV

CIRCLEVILLE CITY OF (WATER & SEWER)  
802 S. PICKAWAY STREET  
CIRCLEVILLE, OH 43113  
740-477-6764  
JOSH ANDERSON  
JANDERSON@CIRCLEVILLEOH.GOV

COLUMBIA GAS  
843 PIATT STREET  
CHILLICOTHE, OH 45601  
740-637-9378  
HUDSON PARK  
HPARK@NISOURCES.COM

FRONTIER COMMUNICATIONS  
TRAVIS BRANNON  
TRAVIS.L.BRANNON@FTR.COM  
740-835-6825

HORIZON  
68 EAST MAIN STREET  
CHILLICOTHE, OH 45601  
740-993-9793  
ZACH GOEBBEL  
ZACH.GOEBBEL@HORIZONCONNECTS.COM

MOX NETWORKS  
2040 E. MARIPOSA AVENUE  
EL SEGUNDO, CA 90245  
740-794-1678  
DAVE ALLISON  
CWA4324PRES@GMAIL.COM

ODOT TRAFFIC (D6)  
400 EAST WILLIAMS STREET  
DELEWARE, OH 43015  
740-474-7197

PICKAWAY COUNTY ENGINEER  
207 SOUTH COURT STREET  
CIRCLEVILLE, OH 43113  
740-474-3360

T-MOBILE  
3007 TUCUMCARI DRIVE  
SARDINIA, OH 45171  
513-459-5796  
STEVEN HUGHES  
STEVEN.HUGHES1@T-MOBILE.COM

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

**WORK LIMITS**

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**EXISTING PLANS**

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 6 OFFICE IN DELAWARE, OHIO.

**CSXT RAILROAD**

IF ANY ISSUE, INCIDENT, AND/OR SPILL OCCURS WITHIN CSXT RIGHT OF WAY, PLEASE CONTACT THE CSXT PUBLIC SAFETY COORDINATION CENTER AT 800-232-0144.

**CLEARING AND GRUBBING**

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

**CONTRACTION AND/OR EXPANSION JOINTS**

ALTHOUGH SPECIFIC LOCATIONS OF CERTAIN CONTRACTION AND EXPANSION JOINTS HAVE BEEN DETAILED ON THIS PLAN, NO WAIVER OF THE SPECIFICATIONS IS INTENDED. IN ALL CASES, THE PROVISION OF EXPANSION JOINTS AT ALL MAJOR STRUCTURES INCLUDING THE MAXIMUM SPACING BETWEEN CONTRACTION JOINTS IS IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWING BP-2.2 AND THE SPECIFICATIONS.

**CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL**

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

**REVIEW OF DRAINAGE FACILITIES**

PRIOR TO THE START OF WORK AND AGAIN BEFORE FINAL ACCEPTANCE, PERFORM AN INSPECTION WITH REPRESENTATIVES OF THE DEPARTMENT, CONTRACTOR AND LOCALS OF ALL EXISTING DRAINAGE FACILITIES THAT ARE TO REMAIN IN SERVICE WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES IS DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION ARE MAINTAINED BY THE DEPARTMENT.

CONFIRM ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE-MENTIONED PARTIES ARE MAINTAINED AND LEFT IN A CONDITION COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. THE CONTRACTOR IS RESPONSIBLE TO CORRECT ANY CHANGE IN THE CONDITION RESULTING FROM THEIR OPERATIONS AS DIRECTED AND APPROVED BY THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE IS INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

**ITEM 606 - ANCHOR ASSEMBLY, MGS TYPE E (NCHRP 350 OR MASH 2016)**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING'S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE FACE OF THE TYPE E IMPACT HEAD SHALL BE COVERED WITH A SHEET OF TYPE G REFLECTIVE SHEETING, PER CMS 730.19.

REFER TO THE MANUFACTURER'S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE E, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING ALL RELATED TRANSITIONS, REFLECTIVE SHEETING, HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

**CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES**

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, NOTIFY THE ENGINEER BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, NOTIFY THE ENGINEER BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE IS INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEM.

**POST CONSTRUCTION STORM WATER TREATMENT**

THIS PLAN UTILIZES STRUCTURAL BEST MANAGEMENT PRACTICES (BMP'S) FOR POST CONSTRUCTION STORM WATER TREATMENT.

**IMPACT ATTENUATOR, TYPE 1 (BIDIRECTIONAL)**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY ONE OF THE TYPE 1 IMPACT ATTENUATORS AS LISTED ON THE OFFICE OF ROADWAY ENGINEERING'S WEB PAGE. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, IMPACT ATTENUATOR, TYPE 1 (BIDIRECTIONAL), EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED TRANSITIONS, HARDWARE, REFLECTIVE SHEETING AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

**IMPACT ATTENUATOR, TYPE 2 (BIDIRECTIONAL), 35 MPH, 24" WIDE**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE TYPE 2 IMPACT ATTENUATORS AS LISTED ON THE OFFICE OF ROADWAY ENGINEERING'S WEB PAGE (REFER TO THE POSTED SHOP DRAWINGS FOR THE MOST CURRENT APPROVED PRODUCT MODELS). WHEN BI-DIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, IMPACT ATTENUATOR, TYPE 2 (BIDIRECTIONAL), 35 MPH, 24", EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS/BACKSTOPS, TRANSITIONS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

**IMPACT ATTENUATOR, TYPE 2 (BIDIRECTIONAL), 35 MPH, 64" WIDE**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE TYPE 2 IMPACT ATTENUATORS AS LISTED ON THE OFFICE OF ROADWAY ENGINEERING'S WEB PAGE (REFER TO THE POSTED SHOP DRAWINGS FOR THE MOST CURRENT APPROVED PRODUCT MODELS). WHEN BI-DIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, IMPACT ATTENUATOR, TYPE 2 (BIDIRECTIONAL), 35 MPH, 64", EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS/BACKSTOPS, TRANSITIONS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

**LOOP DETECTORS AT U.S. 22-S.R. 56 INTERSECTION**

THESE INTERSECTIONS HAVE RADAR DETECTION AND VIDEO DETECTION SYSTEMS RESPECTIVELY, ANY LOOP DETECTORS ENCOUNTERED IN THE PAVEMENT HAVE BEEN ABANDONED AND SHALL NOT BE RE-INSTALLED.

COSTS OF REMOVING, IF ENCOUNTERED, SHALL BE INCIDENTAL TO ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE.

GENERAL NOTES

DESIGN AGENCY

CHA

DESIGNER	JCW
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	TOTAL
7	96

PIC-22-17-12  
MODEL: Sheet PAPER SIZE: 17x11 (In.) DATE: 1/17/2023 TIME: 3:55:59 PM USER: 4957  
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**AUXILIARY UTILITY NOTE**

BIDDERS ARE ADVISED THAT THE FOLLOWING UTILITY FACILITIES SHALL REMAIN IN PLACE AND IN SERVICE DURING THE CONSTRUCTION OF THE PROJECT AS SET OUT BELOW. ALL STATION LOCATIONS LISTED BELOW ARE APPROXIMATE UNLESS OTHERWISE STATED. STATIONS ARE BASED ON THE CENTERLINE OF CONSTRUCTION FOR U.S. 22.

ACD.NET  
 SHIRLEY YOHNKA  
 517-999-3240  
 YOHNKA.SHIRLEY@ACD.NET

PER COMMUNICATIONS WITH SHIRLEY YOHNKA, ACD.NET AERIAL FIBER IS LOCATED ON THE EAST SIDE OF THE AT-GRADE NORFOLK SOUTHERN RAILROAD TRACKS, EAST OF THE CONSTRUCTION LIMITS. NO RELOCATIONS REQUIRED.

AEP OHIO (DISTRIBUTION)  
 CLARKE SAUNDERS  
 28831 STATE ROUTE 7  
 REEDSVILLE, OH 45772  
 740-985-3054  
 CMSAUNDERS@AEP.COM

AEP OHIO (TRANSMISSION)  
 MICHAEL D. CARR  
 8600 SMITHS MILL ROAD  
 NEW ALBANY, OH 43054  
 380-205-5072  
 TL\_PUBLICPROJECTS@AEP.COM

AEP HAS DISTRIBUTION LINES ON THE SOUTH SIDE OF THE PROJECT LIMITS AND LARGE TRANSMISSION LINES GOING OVER THE PROJECT EAST OF U.S. 23 AND RUNNING NORTH-SOUTH. CLEARANCE BENEATH THE OVERHEAD TRANSMISSION IS EXPECTED TO BE LARGE ENOUGH TO NOT BE AN ISSUE, BUT THE CONTRACTOR SHALL USE CAUTION AND BE AWARE OF THE OVERHEAD TRANSMISSION LINES DURING CONSTRUCTION AND OBSERVE PROPER CLEARANCES. NO RELOCATIONS REQUIRED.

CITY OF CIRCLEVILLE WATER & SEWER  
 JOSH ANDERSON  
 802 S. PICKAWAY STREET  
 CIRCLEVILLE, OH 43113  
 740-477-6764  
 JANDERSON@CIRCLEVILLEOH.GOV

WATERLINE IS PRIMARILY LOCATED ON THE SOUTH SIDE OF U.S. 22 AND NORTHEAST OF THE ISLAND ROAD / U.S. 22 INTERSECTION. BASED ON SUBSURFACE UTILITY ENGINEERING, THE WATERLINE SHOULD BE DEEP ENOUGH TO MAINTAIN THE MINIMUM COVER REQUIREMENTS RELATIVE TO THE PROPOSED CONSTRUCTION. NO RELOCATIONS ARE REQUIRED. CONTRACTOR TO EXERCISE CAUTION AS THE FACILITY WILL REMAIN IN PLACE FOR THE DURATION OF THE PROJECT.

CHARTER COMMUNICATIONS (SPECTRUM)  
 JOSEPH BAILEY  
 740-648-3099 (DESK) / 740-253-0174 (CELL)  
 JOSEPH.BAILEY@CHARTER.COM

CHARTER HAS FACILITIES LOCATED ON TWO POLES: ONE POLE IN THE NORTHWEST CORNER OF THE CARGILL LOT (STA. 122+80, 134' RT) AND ONE POLE APPROXIMATELY 100' NORTH OF U.S. 22 ON THE EAST SIDE OF ISLAND ROAD (STA. 125+15, 161' LT). THESE POLES ARE OUTSIDE OF THE PROJECT. NO RELOCATIONS REQUIRED.

**AUXILIARY UTILITY NOTE (CON'T)**

COLUMBIA GAS  
 HUDSON PARK  
 843 PIATT STREET  
 CHILLICOTHE, OH 45601  
 740-637-9378  
 HPARK@NISOURCE.COM

COLUMBIA GAS HAS GAS LINES RUNNING ALONG THE SOUTH SIDE OF U.S. 22, WITH CROSSINGS UNDER U.S. 22 NEAR STA. 123+40 THAT CONTINUES TO THE NORTH UNDER THE EXISTING SLIP RAMP AS WELL AS ANOTHER CROSSING NEAR STA. 126+25. THE GAS LINE CROSSING AT STA. 123+40 IS AN 8" COATED STEEL HIGH PRESSURE MAIN. ON THE HIGH SIDE OF THE U.S. 22 PAVEMENT (LEFT OF CENTERLINE OF CONSTRUCTION U.S. 22), THE CLEARANCE BETWEEN THE BOTTOM OF THE PROPOSED PAVEMENT BUILDUP AND THE TOP OF THE EXISTING 8" GAS LINE IS AS LITTLE AS 1.6'. ALTHOUGH THE MINIMUM COVER IS EXPECTED TO BE MAINTAINED BETWEEN THE EXISTING GAS LINES AND THE PROPOSED CONSTRUCTION, THE CONTRACTOR SHALL EXERCISE CAUTION NEAR GAS MAINS, ESPECIALLY THE SHALLOW AREAS AS MENTIONED IN THIS NOTE. HAND DIGGING IS SUGGESTED WHERE POSSIBLE. NO RELOCATIONS REQUIRED.

FRONTIER (VERIZON)  
 TRAVIS BRANNON  
 740-835-6825  
 TRAVIS.L.BRANNON@FTR.COM

FRONTIER HAS A CROSSING UNDER U.S. 22 NEAR STA. 113+75 AS WELL AS COPPER/FIBER LINES ON THE NORTH SIDE OF U.S. 22. THE FIBER AND COPPER RUN PARALLEL FROM A MANHOLE JUST WEST OF THE CSX RAILROAD BRIDGE AND CROSSING UNDER THE ON-RAMP/OFF-RAMP TO/FROM U.S. 23 (LOOP RAMP). THE COPPER AND FIBER THEN RUN THROUGH TWO MANHOLES WEST OF THE U.S. 23 BRIDGE (STA. 118+50, 62' LT AND STA. 118+49, 44' LT) AND CONTINUE EASTWARD JUST INSIDE OF THE EXISTING CURB LINE BENEATH THE PAVEMENT TO ANOTHER MANHOLE LOCATED AT STA. 121+25, 57' LT. EAST OF THE U.S. 23 BRIDGE, THE COPPER AND FIBER LINES BEGIN TO SEPARATE AND CONTINUE TOWARDS THE NORTHEAST. THERE IS ALSO AN EXISTING MANHOLE IN THE INFIELD CREATED BY THE SLIP RAMP ONTO U.S. 23 NORTH.

THERE IS POTENTIAL FOR CONFLICT WHERE THE COPPER AND FIBER ARE RUNNING BENEATH THE PAVEMENT BETWEEN THE MANHOLES STA. 118+49, 44' LT AND STA. 121+25, 57' LT AS THERE WILL BE APPROXIMATELY 2' OF EXCAVATION FOR THE PROPOSED PAVEMENT WORK. BASED ON MEASUREMENTS TAKEN AT THE MANHOLES MENTIONED IN THE PREVIOUS SENTENCE, THE ESTIMATED DEPTH OF THE FIBER/COPPER IS AT LEAST 3.5' DEEP. THEREFORE, IT IS ESTIMATED THAT THERE WILL BE 1.5' OF COVER BETWEEN THE FIBER/COPPER LINES AND THE BOTTOM OF THE PROPOSED AGGREGATE BASE FOR THE PAVEMENT.

ALTHOUGH THERE IS NOT EXPECTED TO BE A CONFLICT DURING CONSTRUCTION, THE CONTRACTOR SHALL EXERCISE CAUTION TO AVOID DAMAGING THE COPPER AND FIBER LINES. NO RELOCATIONS REQUIRED.

**AUXILIARY UTILITY NOTE (CON'T)**

HORIZON  
 ZACH GOEBBEL  
 68 E. MAIN STREET  
 CHILLICOTHE, OH, 45601  
 740-993-9793  
 ZACH.GOEBBEL@HORIZONCONNECTS.COM

HORIZON IS LOCATED ON THE SOUTH SIDE OF U.S. 22 AND CROSSES BENEATH THE SOUTHBOUND U.S. 23 ON-RAMP (FOR EASTBOUND U.S. 22 TRAFFIC) AND UNDER MOUND STREET BEFORE CONTINUING SOUTH ON MOUND STREET. THE EXISTING DEPTHS RANGE FROM 7 FEET TO 13 FEET WITHIN THE PROJECT AREA. NO RELOCATIONS REQUIRED.

MOX NETWORKS  
 DAVE ALLISON  
 2040 MARIPOSA AVENUE  
 EL SEGUNDO, CA 90245  
 740-794-1678  
 CWA4324PRES@GMAIL.COM

MOX IS LOCATED ON THE EAST SIDE OF THE CSX RAILROAD TRACKS AND RUNS NORTH-SOUTH PARALLEL WITH THE TRACKS WITH A CROSSING UNDER U.S. 22 NEAR STA. 115+50. THE EXISTING DEPTHS RANGE FROM 4.5 FEET TO 7 FEET WITHIN THE PROJECT AREA. NO RELOCATIONS REQUIRED.

T-MOBILE  
 STEVEN HUGHES  
 3007 TUCUMCARI DRIVE  
 SARDINIA, OH 45171  
 513-459-5796 / 814-553-2300 (CELL)  
 STEVEN.HUGHES1@T-MOBILE.COM

THERE IS A T-MOBILE LINE ON THE WEST SIDE OF THE CSX RAILROAD GOING UNDER U.S. 22 AT STA. 114+14. NO CONFLICTS ARE ANTICIPATED. CONTRACTOR SHALL ESPECIALLY EXERCISE CAUTION NEAR STA. 114+20, 15' LT FOR THE FULL DEPTH SHOULDER CONSTRUCTION, AS THE T-MOBILE LINE PASSES BENEATH AT NEAR THAT LOCATION. NO RELOCATIONS REQUIRED.

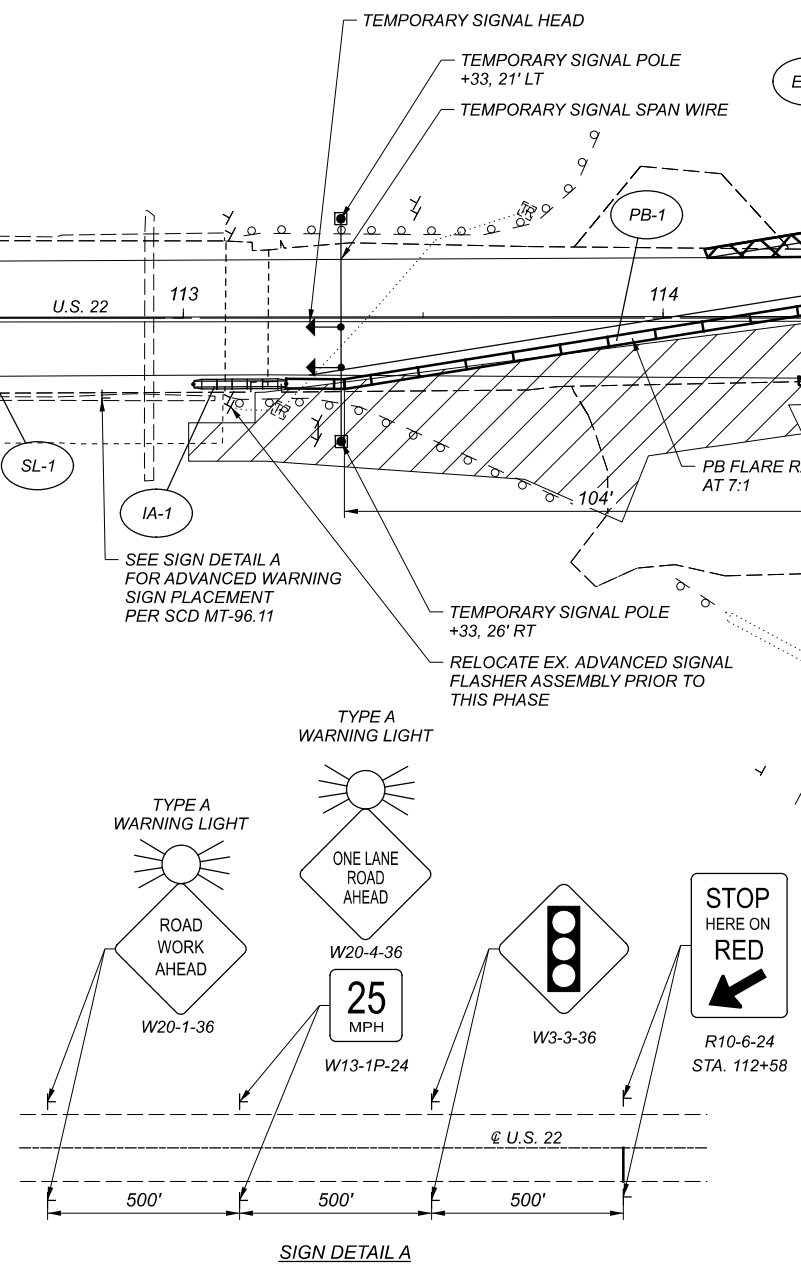
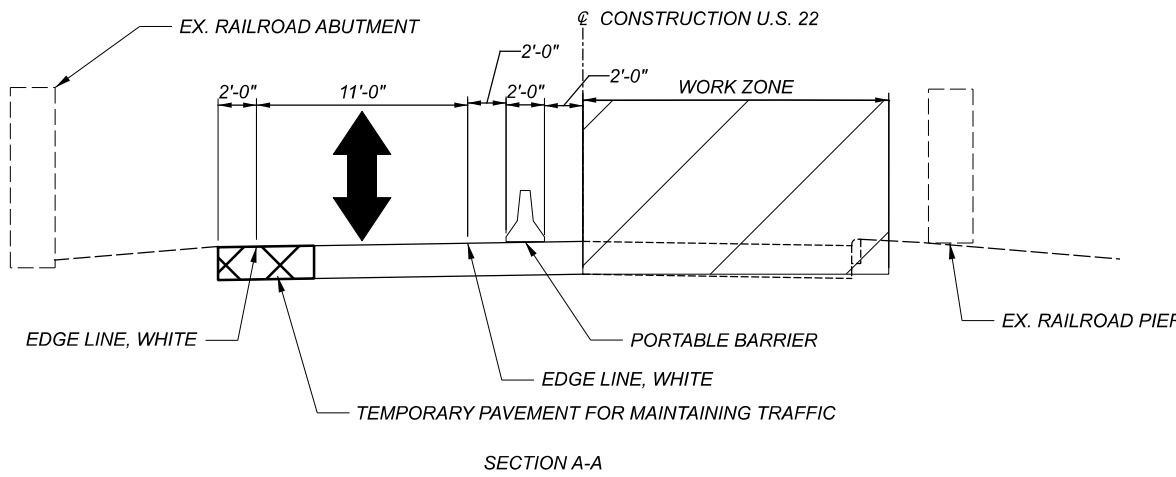
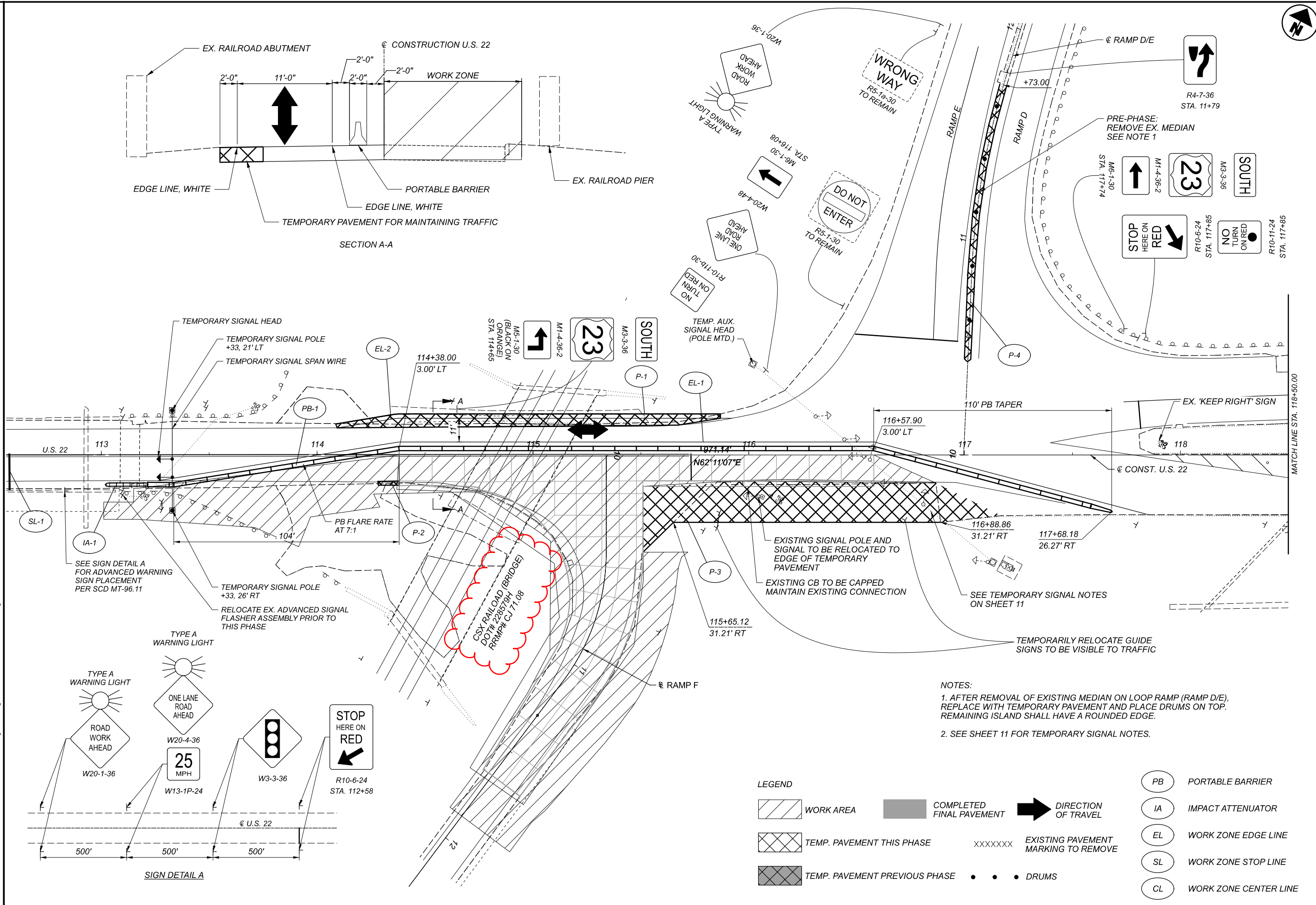
**WINDOW CONTRACT TABLE**

USE THE FOLLOWING TABLE AS REFERRED TO IN THE PROPOSAL:

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE	DISINCENTIVE \$ PER DAY	WORK WINDOW	
			START	END
ALL WORK ASSOCIATED WITH PHASE 1, PHASE 2A & 2B	30	\$12,000 PER DAY	BEGINNING OF PHASE 1 WORK	PRIOR TO STARTING PHASE 2C
ALL WORK ASSOCIATED WITH PHASE 2C, PHASE 2D & PHASE 3	75	\$6,000 PER DAY	COMPLETION OF PHASE 2B	PRIOR TO STARTING PHASE 4

**SHORT-TERM HOURLY CLOSURE WINDOW CONTRACT**

DESCRIPTION OF CRITICAL WORK	HOURS TO COMPLETE	DISINCENTIVE \$ PER HOUR
COMPLETE CLOSURE OF RAMP E AND RAMP D BETWEEN THE HOURS OF 7PM FRIDAY THROUGH 6AM MONDAY TO PERFORM ALL WORK ASSOCIATED WITH PHASE 2A	59 HOURS	475



NOTES:  
 1. AFTER REMOVAL OF EXISTING MEDIAN ON LOOP RAMP (RAMP D/E), REPLACE WITH TEMPORARY PAVEMENT AND PLACE DRUMS ON TOP. REMAINING ISLAND SHALL HAVE A ROUNDED EDGE.  
 2. SEE SHEET 11 FOR TEMPORARY SIGNAL NOTES.

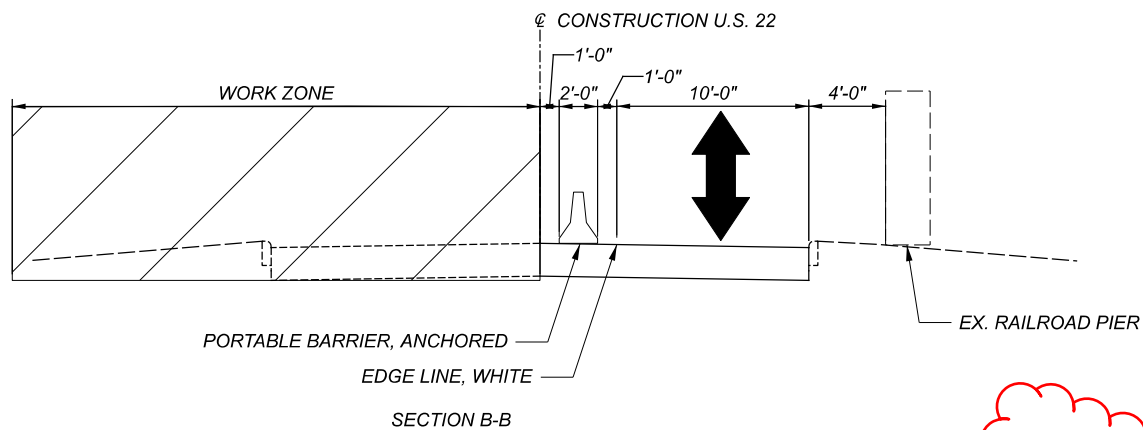
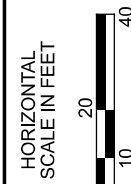
LEGEND			
	WORK AREA		COMPLETED FINAL PAVEMENT
	TEMP. PAVEMENT THIS PHASE		DIRECTION OF TRAVEL
	TEMP. PAVEMENT PREVIOUS PHASE		EXISTING PAVEMENT MARKING TO REMOVE
			DRUMS
	PORTABLE BARRIER		IMPACT ATTENUATOR
	WORK ZONE EDGE LINE		WORK ZONE STOP LINE
	WORK ZONE CENTER LINE		



MAINTENANCE OF TRAFFIC PHASE 1  
 BEGIN PROJECT TO STA. 118+50

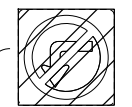
DESIGN AGENCY	CHA
DESIGNER	DTY
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	TOTAL
18	96

NOTES  
 1. DO NOT CONSTRUCT CONCRETE MEDIAN IN PHASE 2A. CONSTRUCT SUBBASE MATERIAL PER SCD RM-3.1 AND PLACE ITEM 304 AGGREGATE ON TOP. PLACE DRUMS ON TOP OF THE AGGREGATE FOR NEXT PHASE.

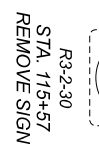


CSX RAILROAD (BRIDGE)  
 DOT# 2285794  
 RAMP# CJ 71.08

SIGN DETAILS



R3-2-30  
 STA. 115+57  
 TO BE COVERED



R3-2-30  
 STA. 115+57  
 REMOVE SIGN



W6-3-36  
 STA. 117+37

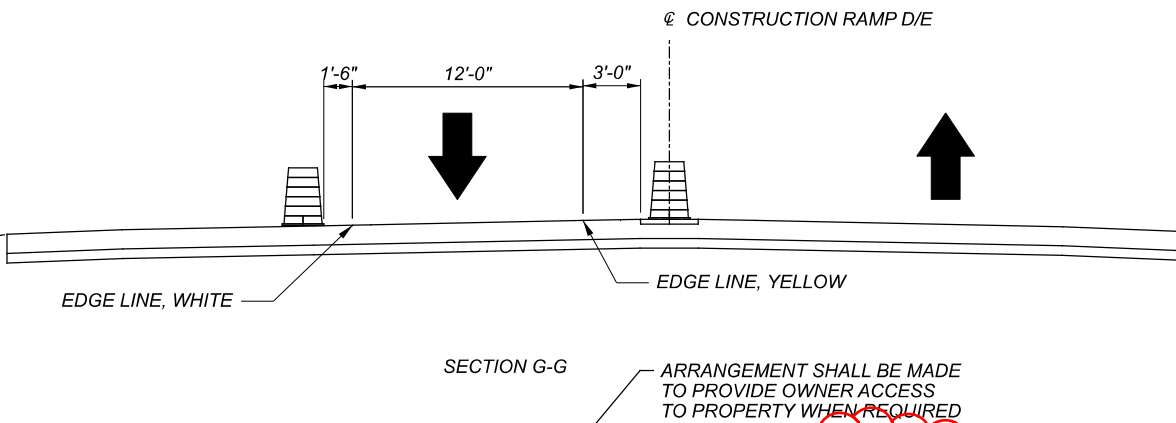
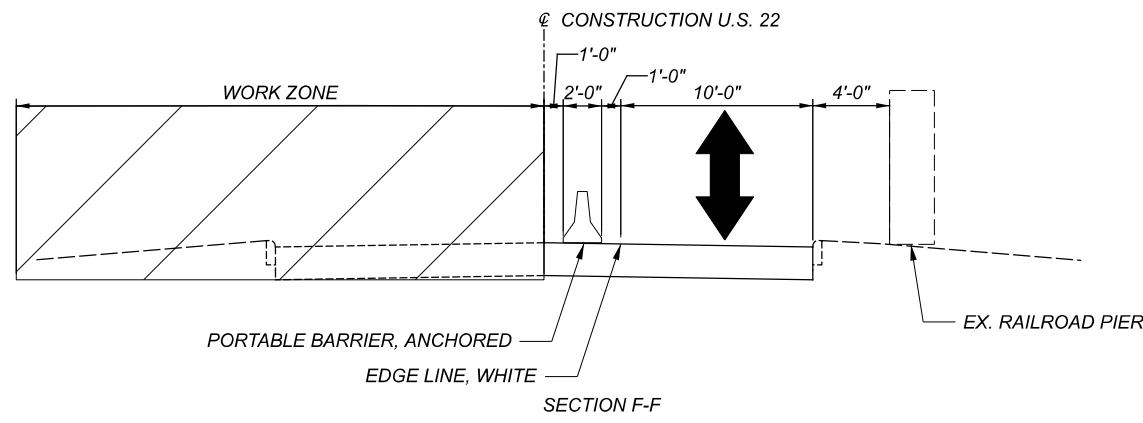
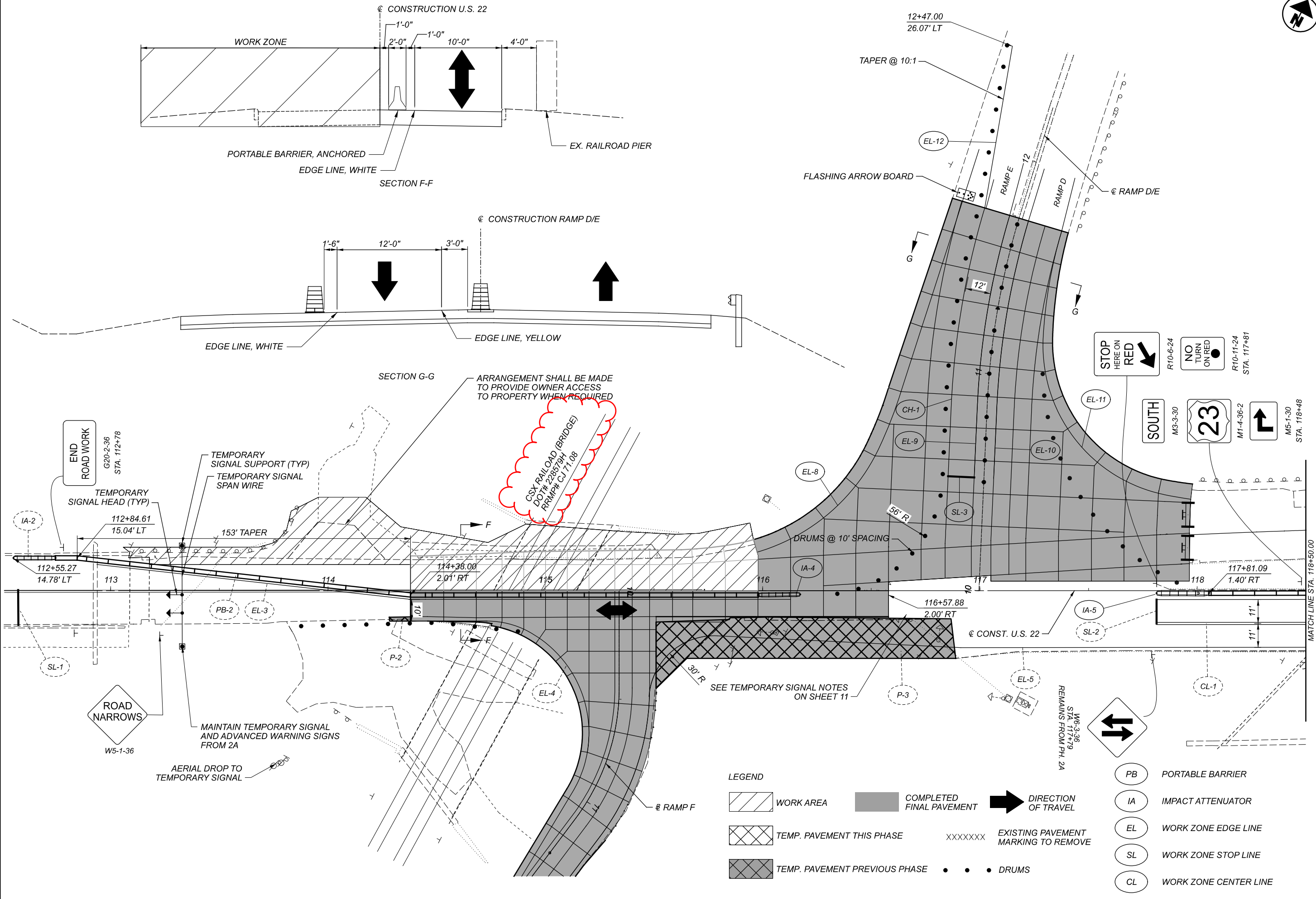
LEGEND

- WORK AREA
- TEMP. PAVEMENT THIS PHASE
- TEMP. PAVEMENT PREVIOUS PHASE
- COMPLETED FINAL PAVEMENT
- DIRECTION OF TRAVEL
- EXISTING PAVEMENT MARKING TO REMOVE
- PORTABLE BARRIER
- IMPACT ATTENUATOR
- WORK ZONE EDGE LINE
- WORK ZONE STOP LINE
- WORK ZONE CENTER LINE

PIC-22-17-12  
 MODEL: MOT Phase 1 - Plan [Sheet#] PAPER SIZE: ITXII (in.) DATE: 1/18/2023 TIME: 12:45:40 PM USER: 4957  
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MAINTENANCE OF TRAFFIC PHASE 2A  
 BEGIN PROJECT TO STA. 118+50

DESIGN AGENCY	
<b>CHA</b>	
DESIGNER	DTY
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	TOTAL
22	96















- LEGEND**
- WORK AREA
  - COMPLETED FINAL PAVEMENT
  - DIRECTION OF TRAVEL
  - TEMP. PAVEMENT THIS PHASE
  - EXISTING PAVEMENT MARKING TO REMOVE
  - TEMP. PAVEMENT PREVIOUS PHASE
  - DRUMS
  - PORTABLE BARRIER
  - IMPACT ATTENUATOR
  - WORK ZONE EDGE LINE
  - WORK ZONE STOP LINE
  - WORK ZONE CENTER LINE

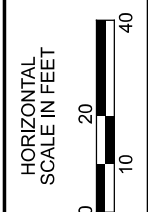
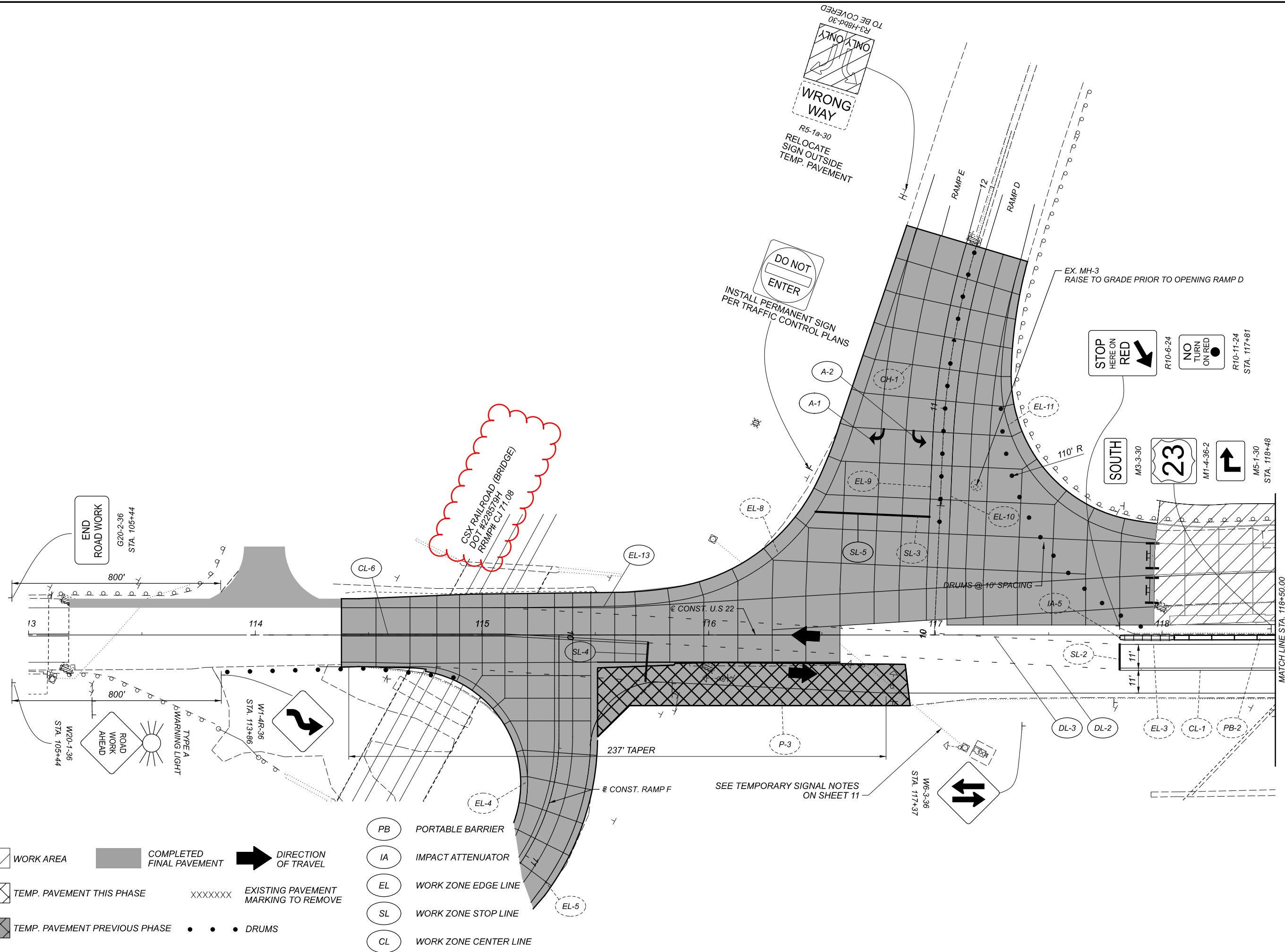


**MAINTENANCE OF TRAFFIC PHASE 2B**  
**BEGIN PROJECT TO 118+50**

DESIGN AGENCY	
<b>CHA</b>	
DESIGNER	DTY
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	TOTAL
25	96

LEGEND

-  WORK AREA
-  COMPLETED FINAL PAVEMENT
-  DIRECTION OF TRAVEL
-  TEMP. PAVEMENT THIS PHASE
-  EXISTING PAVEMENT MARKING TO REMOVE
-  TEMP. PAVEMENT PREVIOUS PHASE
-  DRUMS
-  PORTABLE BARRIER
-  IMPACT ATTENUATOR
-  WORK ZONE EDGE LINE
-  WORK ZONE STOP LINE
-  WORK ZONE CENTER LINE

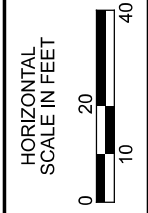
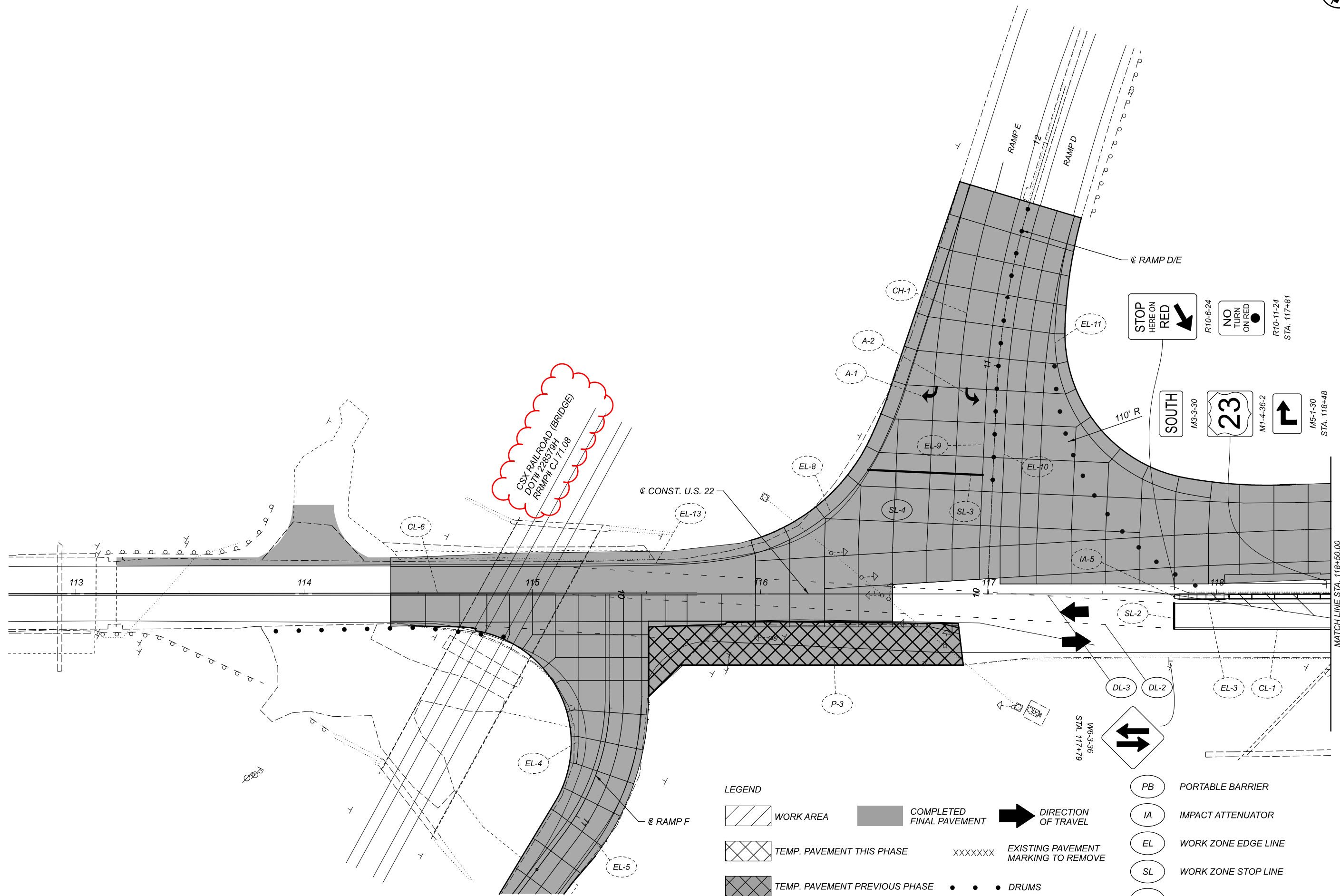


MAINTENANCE OF TRAFFIC PHASE 2C  
 BEGIN PROJECT TO STA. 118+50

DESIGN AGENCY



DESIGNER	DTY
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	TOTAL
26	96

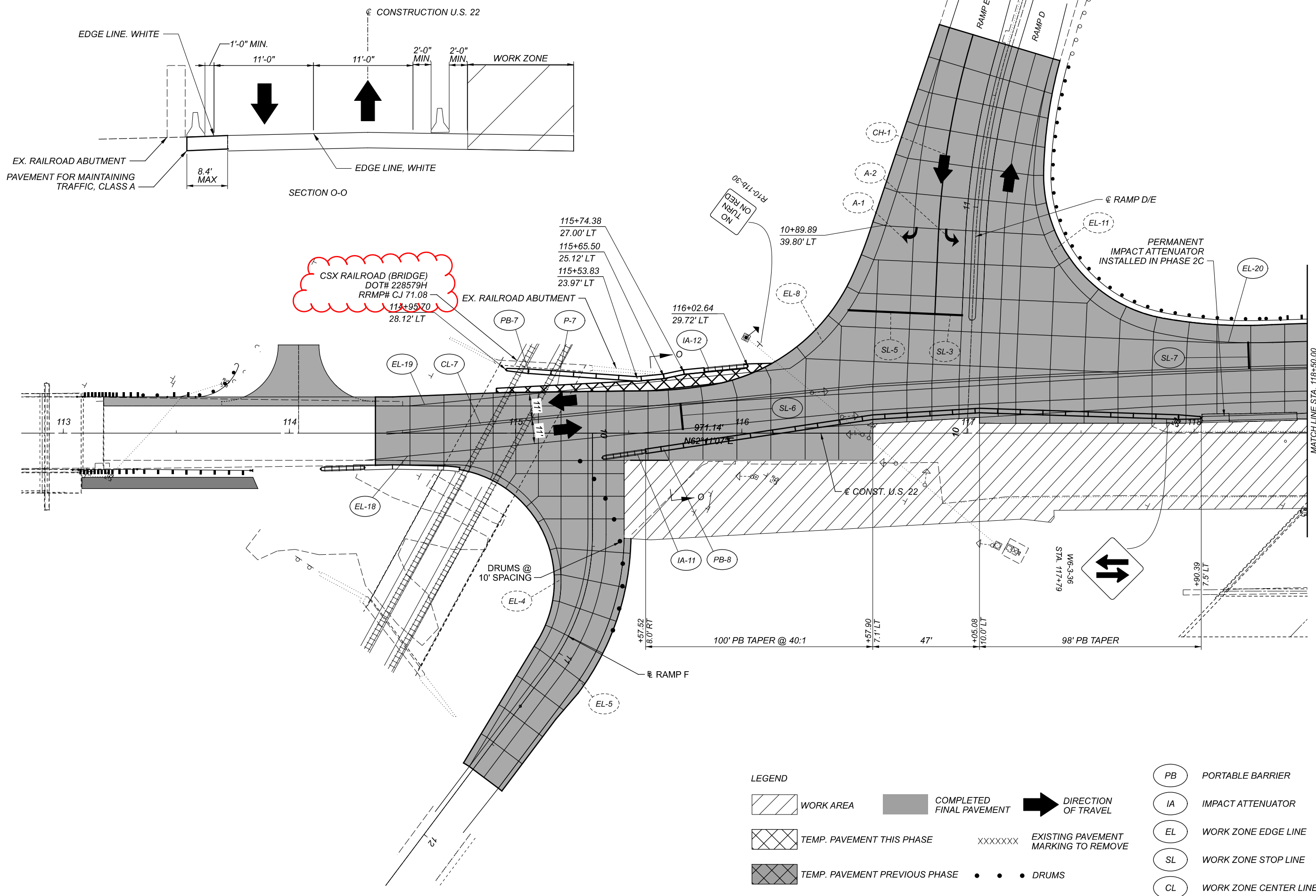
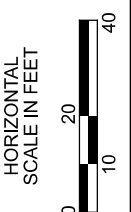


MAINTENANCE OF TRAFFIC PHASE 2D  
 BEGIN PROJECT TO 118+50

DESIGN AGENCY	
<b>CHA</b>	
DESIGNER	DTY
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	TOTAL
29	96

- LEGEND**
- WORK AREA
  - COMPLETED FINAL PAVEMENT
  - DIRECTION OF TRAVEL
  - TEMP. PAVEMENT THIS PHASE
  - EXISTING PAVEMENT MARKING TO REMOVE
  - TEMP. PAVEMENT PREVIOUS PHASE
  - DRUMS
  - PORTABLE BARRIER
  - IMPACT ATTENUATOR
  - WORK ZONE EDGE LINE
  - WORK ZONE STOP LINE
  - WORK ZONE CENTER LINE



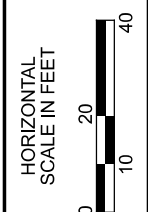
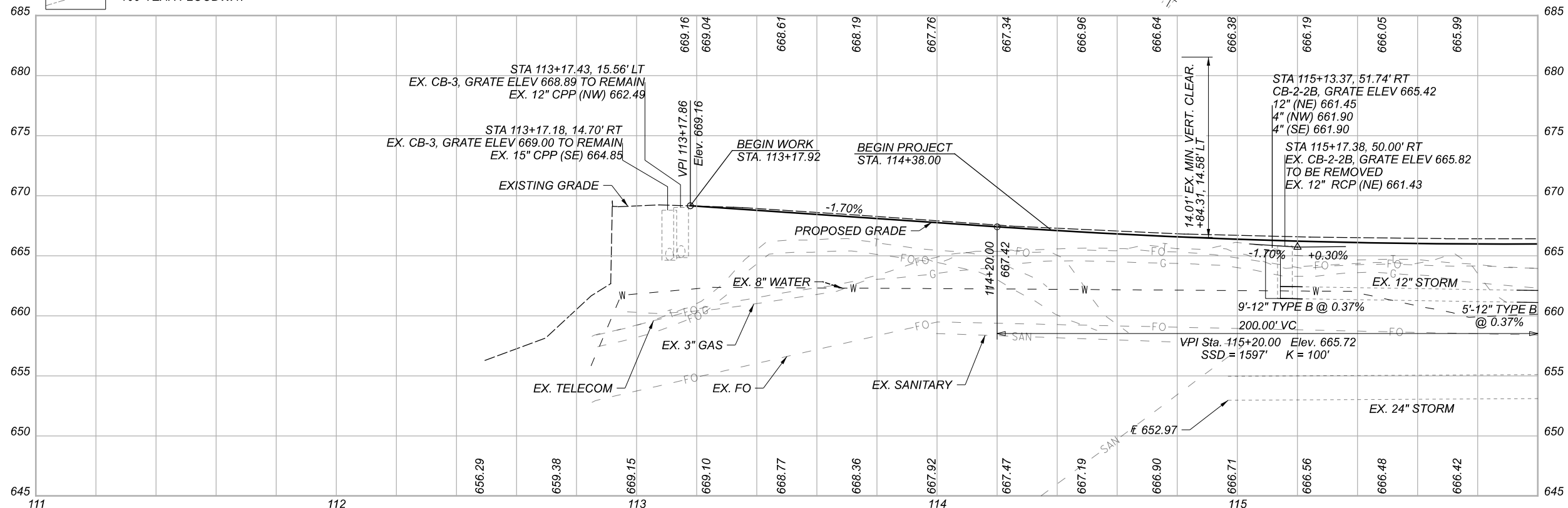
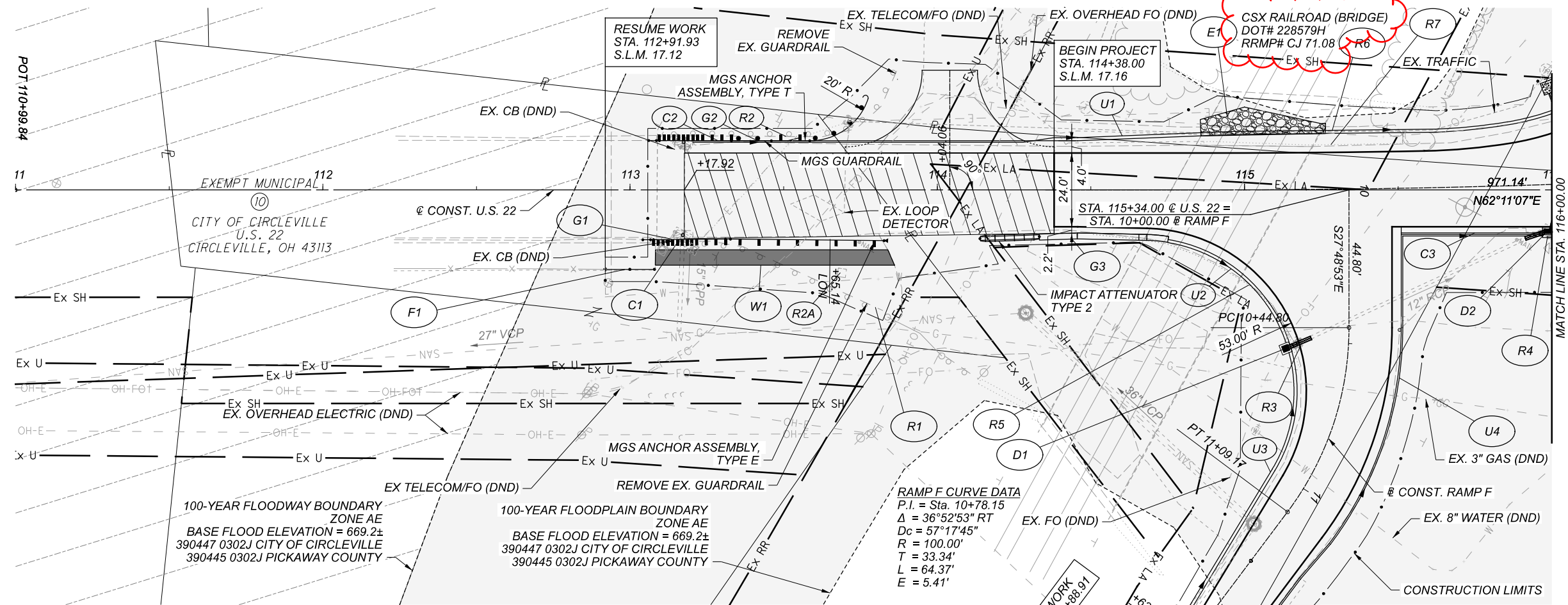


CSX RAILROAD (BRIDGE)  
 DOT# 228579H  
 RRMP# CJ 71.08  
 114+95.70  
 28.12' LT

<b>LEGEND</b>			
	WORK AREA		COMPLETED FINAL PAVEMENT
	TEMP. PAVEMENT THIS PHASE		DIRECTION OF TRAVEL
	TEMP. PAVEMENT PREVIOUS PHASE		EXISTING PAVEMENT MARKING TO REMOVE
			DRUMS
	PORTABLE BARRIER		IMPACT ATTENUATOR
	WORK ZONE EDGE LINE		WORK ZONE STOP LINE
	WORK ZONE CENTER LINE		

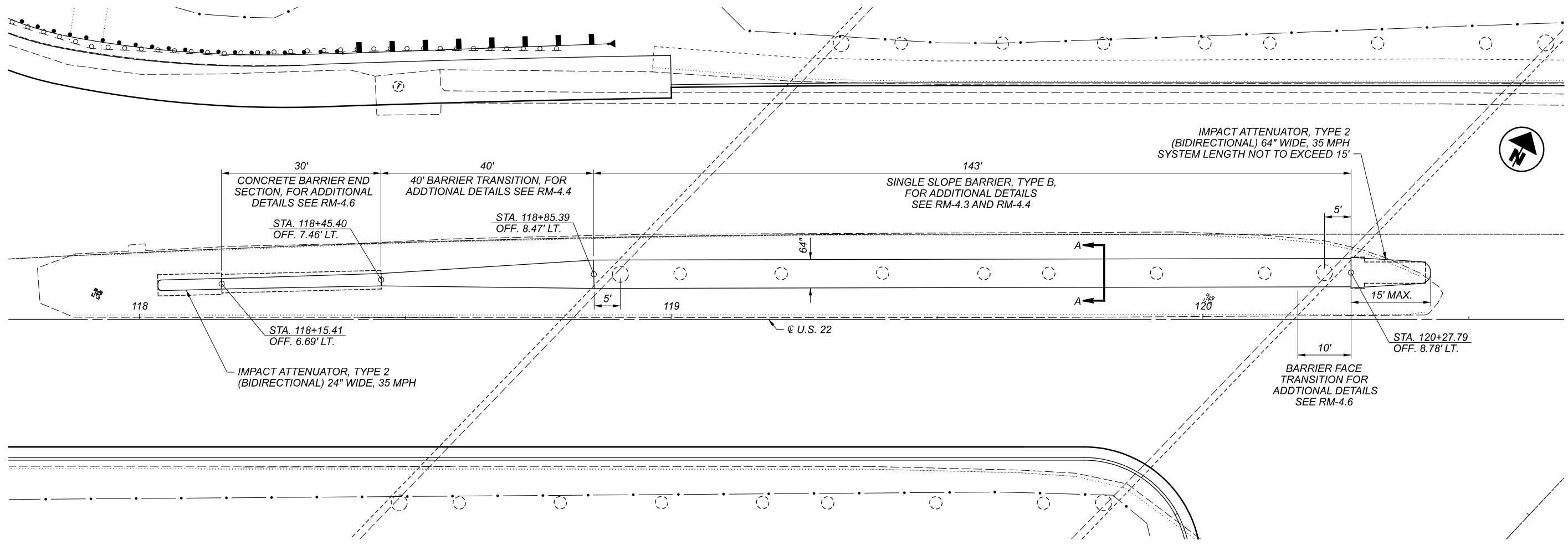
MAINTENANCE OF TRAFFIC PHASE 3  
 BEGIN PROJECT TO STA. 118+50

DESIGN AGENCY	<b>CHA</b>
DESIGNER	DTY
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	TOTAL
33	96

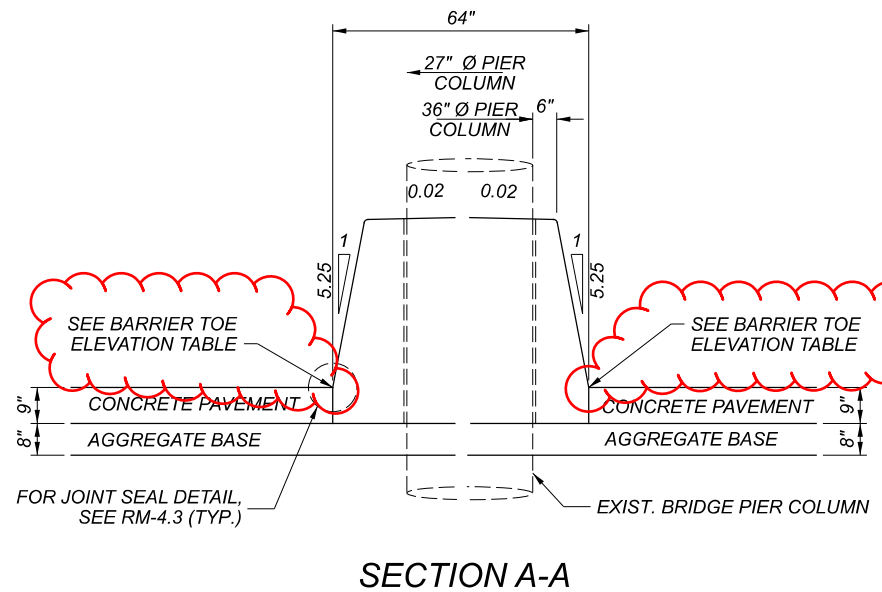


**PLAN AND PROFILE - U.S. 22**  
**BEGIN WORK TO STA. 116+00**

DESIGN AGENCY	CHA
DESIGNER	CAM
REVIEWER	BJA 09/12/22
PROJECT ID	113673
SHEET	43
TOTAL	96



REF No.	STATION TO STATION	SIDE	622		606		626
			CONCRETE BARRIER, SINGLE SLOPE, TYPE B	CONCRETE BARRIER END SECTION, TYPE B	CONCRETE BARRIER, END ANCHORAGE, REINFORCED, TYPE B	IMPACT ATTENUATOR, TYPE 2 (BIDIRECTIONAL), 24" WIDE, 35 MPH	IMPACT ATTENUATOR, TYPE 2 (BIDIRECTIONAL), 64" WIDE, 35 MPH
			FT	EACH	EACH	EACH	EACH
B1	118+15.41	LT				1	2
B2	118+15.41 TO 118+45.40	LT		1	1		1
B3	118+45.40 TO 118+85.39	LT	40				1
B4	118+85.39 TO 120+27.79	LT	143				4
B5	120+27.79	LT				1	2
TOTALS CARRIED TO GENERAL SUMMARY			183	1	1	1	10



### BARRIER TOE ELEVATIONS

STATION	LEFT TOE OF BARRIER		RIGHT TOE OF BARRIER	
	OFFSET (FROM CL U.S. 22)	ELEVATION	OFFSET (FROM CL U.S. 22)	ELEVATION
118+45.40	-8.63	666.79	-6.29	666.80
118+50	-8.91	666.80	-6.24	666.81
118+60	-9.54	666.82	-6.12	666.84
118+70	-10.17	666.85	-6.00	666.87
118+80	-10.80	666.87	-5.87	666.89
118+90	-11.15	666.90	-5.82	666.92
119+00	-11.17	666.92	-5.84	666.95
119+10	-11.19	666.94	-5.86	666.98
119+20	-11.21	666.96	-5.88	667.01
119+30	-11.24	666.98	-5.90	667.04
119+40	-11.26	666.99	-5.92	667.06
119+50	-11.28	667.01	-5.95	667.10
119+60	-11.30	667.03	-5.97	667.13
119+70	-11.32	667.08	-5.99	667.16
119+80	-11.34	667.13	-6.01	667.19
119+90	-11.36	667.19	-6.03	667.22
120+00	-11.38	667.25	-6.06	667.25
120+10	-11.41	667.30	-6.08	667.28
120+20	-11.43	667.36	-6.10	667.31
120+27.79	-11.45	667.40	-6.11	667.34