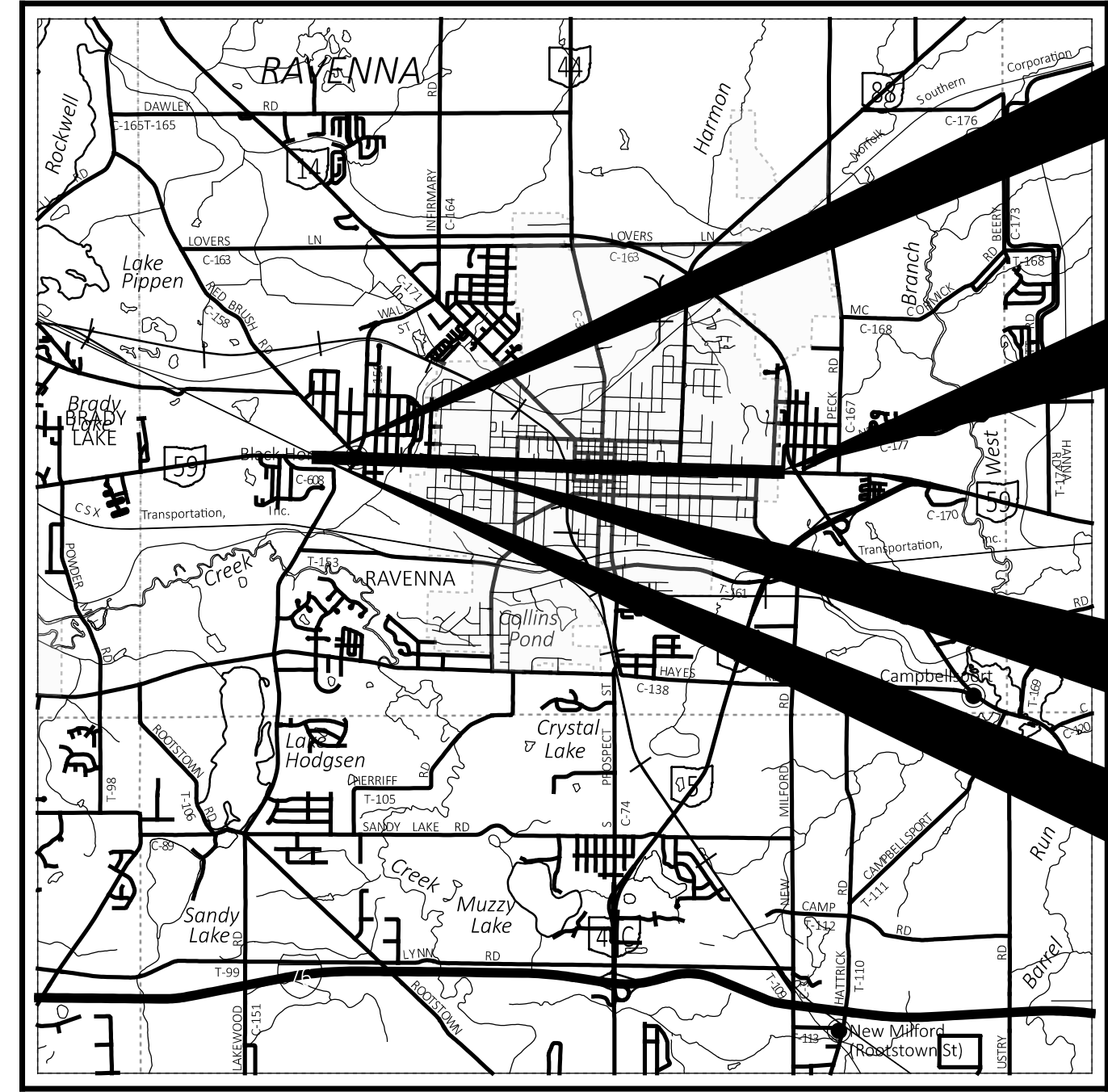


POR-59-(6.05)(6.80)

MODEL: Sheet PAPER: 34x22 (in.) DATE: 2023-08-14 TIME: 11:24:33 USER: cslumpe
 p:\ohio\dot-pw-bentley.com\ohio\dot-pw-02\Documents\01 Active Projects\District 04\Portage\102732\400-Engineering\Roadway\Sheets\102732_G1001.dgn



LOCATION MAP

LATITUDE: 41° 09' 28" N LONGITUDE: 81° 14' 35" W



PORTION TO BE IMPROVED	=====
INTERSTATE HIGHWAY	=====
FEDERAL ROUTES	=====
STATE ROUTES	=====
COUNTY & TOWNSHIP ROADS	=====
OTHER ROADS	-----

DESIGN DESIGNATION

CURRENT ADT (2023)	11,350
HOURLY VOLUME (2023)	1,050
DIRECTIONAL DISTRIBUTION	55%
TRUCKS (24 HOUR B&C)	2%
LEGAL SPEED	25/35
DESIGN FUNCTIONAL CLASSIFICATION:	03 PRINCIPAL ARTERIAL (URBAN)
NHS PROJECT	YES
CURRENT TDMS DATA FOR INFORMATION ONLY	

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

REQUIRED

UNDERGROUND UTILITIES
 Contact Two Working Days
 Before You Dig

OHIO811.org
 Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
 (Non members must be called directly)

PLAN PREPARED BY:
 ODOT DISTRICT 4 CAPITAL PLANNING
 2088 S. ARLINGTON RD.
 AKRON, OH 44306

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

POR-59-(6.05)(6.80)

CITY OF RAVENNA
 RAVENNA TOWNSHIP
 PORTAGE COUNTY

INDEX OF SHEETS:

TITLE SHEET	P.1
TYPICAL SECTIONS	P.2-4
GENERAL NOTES	P.5-6
MAINTENANCE OF TRAFFIC	P.7-14
GENERAL SUMMARY	P.15-16
PAVEMENT CALCULATIONS	P.17
CURB RAMP SUBSUMMARY	P.18-25
PAVEMENT MARKING SUBSUMMARY	P.26-27
STRUCTURES	P.28-35

FEDERAL PROJECT NUMBER

E170(129)

RAILROAD INVOLVEMENT

NORFOLK SOUTHERN RR

PROJECT DESCRIPTION

RESURFACING OF SR 59 FROM SLM 6.80 TO SLM 9.05 IN THE CITY OF RAVENNA. INCLUDES MINOR WORK ON ONE STRUCTURE.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.0 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.3 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A (NOI NOT REQUIRED)*
	*ROUTINE MAINTENANCE PROJECT

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Arthur G. Noirot Jr.
 Arthur G. Noirot Jr., P.E.
 District 04 Deputy Director

Jack Marchbanks
 Jack Marchbanks, PhD
 Director, Department of Transportation

ENGINEER'S SEAL

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-3.1	1/21/22	MT-97.10	4/19/19	800-2023 4/21/23	
BP-5.1	7/15/22	MT-97.12	1/20/17	821 4/20/12	
BP-7.1	1/20/23	MT-99.20	4/19/19	832 7/15/22	
		MT-101.90	7/17/20	843 10/18/19	
DM-1.1	7/17/20	MT-105.10	1/17/20	921 4/20/12	
DM-4.3	1/15/16	MT-110.10	7/19/13		
DM-4.4	1/15/16				
		TC-41.20	10/18/13		
AS-1-15	1/20/23	TC-42.10	10/18/13		
EXJ-4-87	1/20/23	TC-42.20	10/18/13		
VPF-1-90	1/20/23	TC-52.10	10/18/13		
		TC-52.20	1/15/21		
MT-95.31	7/19/19	TC-65.10	1/17/14		
MT-95.32	4/19/19	TC-65.11	7/15/22		
MT-95.50	7/21/17	TC-71.10	4/26/23		
MT-95.60	4/19/19	TC-74.10	1/20/23		

TITLE SHEET

DESIGN AGENCY

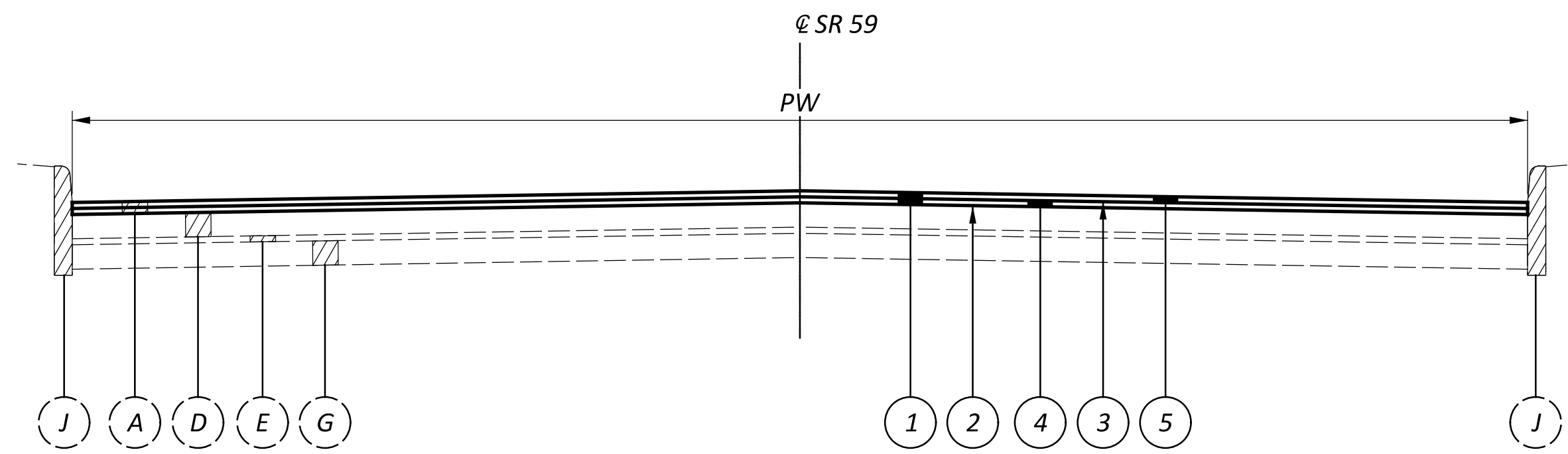


DESIGNER
 CMS

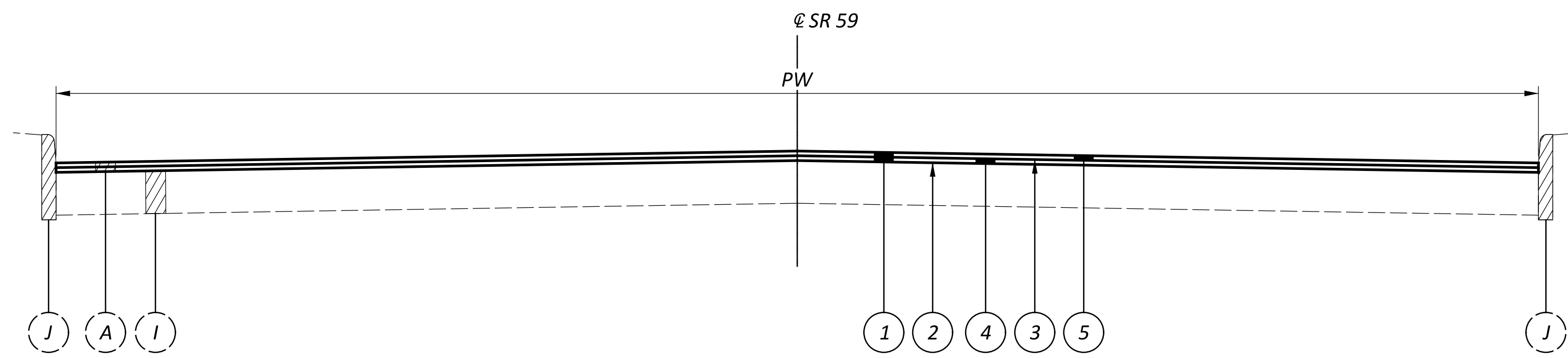
REVIEWER
 MJP 04/27/23

PROJECT ID
 102732

SHEET TOTAL
 P.1 35



TYPICAL SECTION 1				
ROUTE	SLM		AVG. PW (FEET)	LENGTH (MILES)
	FROM	TO		
SR 59	7.21	7.58	42	0.37
SR 59	7.58	7.73	47	0.15
SR 59	7.99	8.08	69	0.09
SR 59	8.08	8.16	40	0.08
SR 59	8.16	8.61	29	0.45



TYPICAL SECTION 2				
ROUTE	SLM		AVG. PW (FEET)	LENGTH (MILES)
	FROM	TO		
SR 59	8.61	9.05	55	0.42

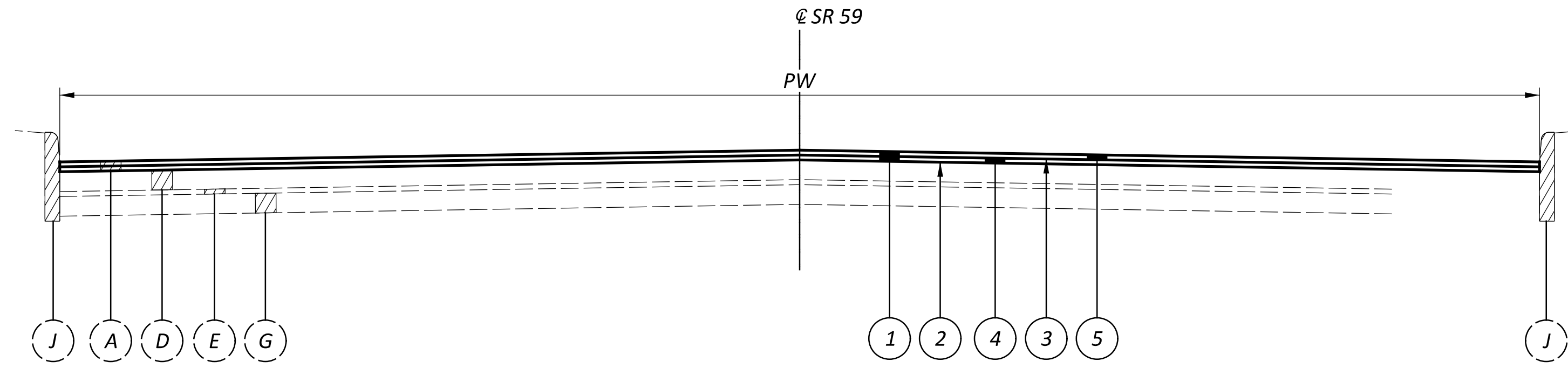
LEGEND

- (A) EXISTING ASPHALT CONCRETE (T = 2"±)
- (B) EXISTING ASPHALT CONCRETE (T = 3"±)
- (C) EXISTING ASPHALT CONCRETE (T = 4"±)
- (D) EXISTING RIGID BRICK BASE (T = 4"±)
- (E) EXISTING SAND CUSHION (T = 1"±)
- (F) EXISTING GRANULATED SLAG SUBBASE (T = 8"±)
- (G) EXISTING PLAIN CONCRETE BASE (T = 3"±)
- (H) EXISTING PLAIN CONCRETE BASE (T = 6"±)
- (I) EXISTING PLAIN CONCRETE BASE (T = 9"±)
- (J) EXISTING CURB
- (K) EXISTING CURB AND GUTTER
- (1) ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (T = 2")
- (2) ITEM 407, TACK COAT, 702.13 @ 0.09 GAL/SY
- (3) ITEM 407, TACK COAT, 702.13 @ 0.06 GAL/SY
- (4) ITEM 441, ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448) (T = 1.25")
- (5) ITEM 424, FINE GRADED POLYMER ASPHALT CONCRETE, TYPE A, (449), AS PER PLAN* (T = 0.75") *ADTT < 1,500

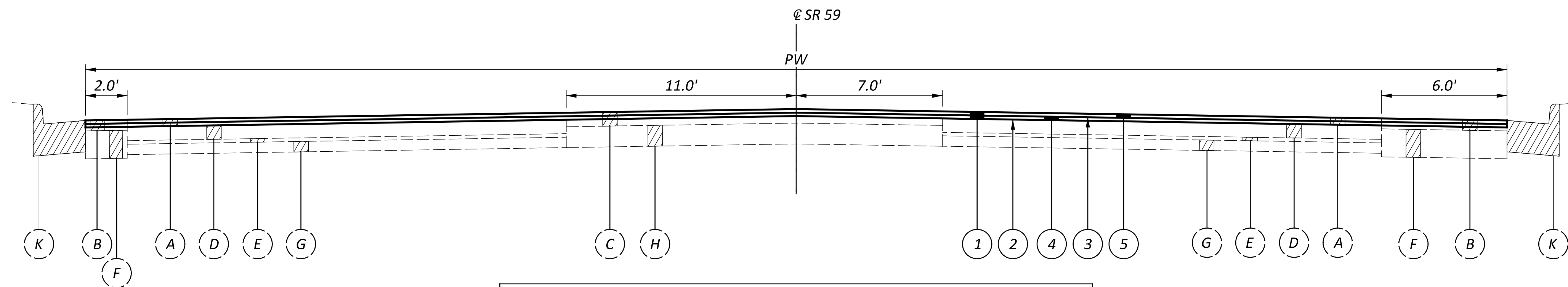
DESIGN AGENCY



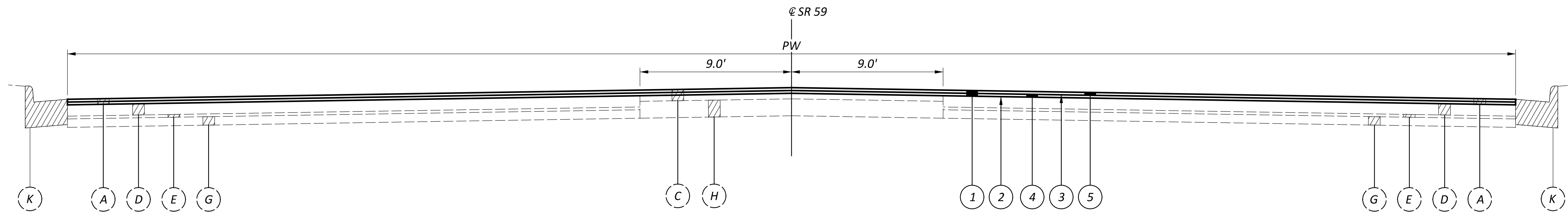
DESIGNER	CMS
REVIEWER	MJP
PROJECT ID	102732
SHEET	P.2
TOTAL	35



TYPICAL SECTION 3				
ROUTE	SLM		AVG. PW (FEET)	LENGTH (MILES)
	FROM	TO		
SR 59	6.80	7.21	50	0.41

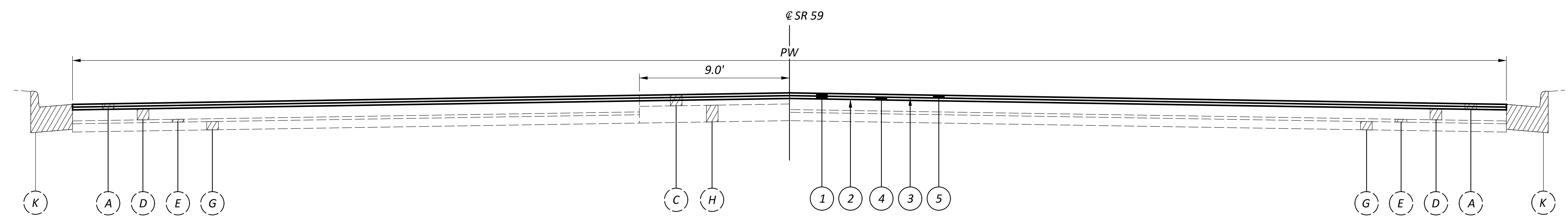


TYPICAL SECTION 4				
ROUTE	SLM		AVG. PW (FEET)	LENGTH (MILES)
	FROM	TO		
SR 59	7.73	7.86	71	0.13

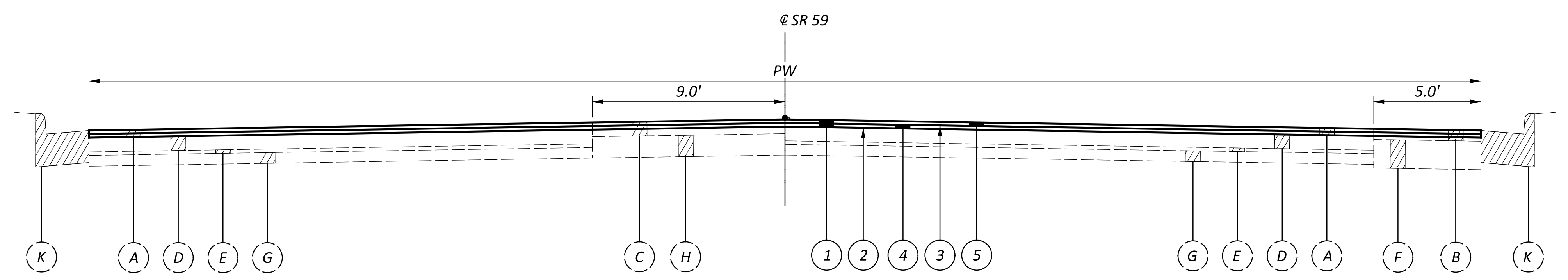


TYPICAL SECTION 5				
ROUTE	SLM		AVG. PW (FEET)	LENGTH (MILES)
	FROM	TO		
SR 59	7.86	7.91	71	0.05





TYPICAL SECTION 6				
ROUTE	SLM		AVG. PW (FEET)	LENGTH (MILES)
	FROM	TO		
SR 59	7.91	7.95	86	0.04



TYPICAL SECTION 7				
ROUTE	SLM		AVG. PW (FEET)	LENGTH (MILES)
	FROM	TO		
SR 59	7.95	7.99	69	0.04

TYPICAL SECTIONS

DESIGN AGENCY



DESIGNER
CMS

REVIEWER
MJP 04/27/23

PROJECT ID
102732

SHEET TOTAL
P.4 35

UTILITIES

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, OHIO811, THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 4 HEADQUARTERS (MICHELLE CHANEY AT 330-786-2267), AND ALL NON-REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN ALL AREAS.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO THE UTILITIES.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN IN THE TYPICAL SECTIONS.

PAVEMENT MARKING LANE WIDTHS

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKINGS ON THIS PROJECT SHALL BE AS FOLLOWS:

ROUTE	SLM TO SLM	LANE WIDTH
SR 59	6.80 TO 8.08	11 FT
SR 59	8.08 TO 8.16	12 FT
SR 59	8.16 TO 8.61	10 FT
SR 59	8.61 TO 9.05	11 FT

PAVEMENT MARKING DETAILS

THE PAVEMENT MARKING DETAIL SHEETS HAVE BEEN SUPPLIED AS REFERENCE DOCUMENTS FOR THIS PROJECT AND ARE AVAILBLE ON THE ODOT FTP SITE AT <https://ftp.dot.state.oh.us/pub/contracts/attach/> FOR THIS PROJECT. FOR ANY LOCATIONS THAT PAVEMENT MARKING DETAILS HAVE NOT BEEN MADE AVAILABLE TO THE CONTRACTOR, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL NEW PAVEMENT MARKINGS IN THE ORIGINAL LOCATIONS.

CURB RAMPS / DETECTABLE WARNINGS

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, INSTALLATION OF THE CURB RAMPS AND DETECTABLE WARNINGS SHALL BE PERFORMED PRIOR TO MAINLINE RESURFACING.

INTERSECTIONS

INTERSECTIONS WILL BE RESURFACED BEYOND THE EDGE LINE TO INCLUDE CROSSWALKS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR INDICATED IN THE PLAN. INTERSECTIONS SHALL BE PAVED AFTER COMPLETION OF THE SURFACE COURSE OR WITH THE MAINLINE PAVEMENT IF THIS CAN BE ACCOMPLISHED WITHOUT CHANGING THE VELOCITY AND DIRECTION OF THE PAVER. USE THE SAME ASPHALT CONCRETE AS THE MAINLINE PAVEMENT. A BUTT JOINT, AS PER STANDARD CONSTRUCTION DRAWING BP-3.1, SHALL BE USED TO PROVIDE A SMOOTH TRANSITION TO THE EXISTING PAVEMENT. ANY GRADING OR PRIME NECESSARY TO ACCOMPLISH THIS WORK SHALL BE INCLUDED IN THE COST OF THE ASPHALT SURFACE COURSE.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN

THIS ITEM OF WORK SHALL BE PERFORMED IN CONFORMANCE WITH ITEM 254 IN THE CMS EXCEPT THE DEPTH SHALL VARY FROM 2 INCHES TO THE TOP OF THE BRICK OR CONCRETE, WHICHEVER IS FIRST. THIS WORK SHALL BE PERFORMED SO THAT THE BRICK OR CONCRETE BASE IS NOT DISTURBED. ALL EQUIPMENT, LABOR, TOOLS, AND OTHER INCIDENTALS REQUIRED TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN.

ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE A (449), AS PER PLAN

703.05 DO NOT USE ANY AGGREGATE FROM A SOURCE DESIGNATED 'SR' OR 'SRH' ACCORDING TO THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM.

CATCH BASIN ADJUSTED TO GRADE

AN ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR ADJUSTING CATCH BASINS TO GRADE.

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF REQUIRED TYPE, SIZE, AND STRENGTH. ENSURE ALL MATERIAL MEETS CMS ITEM 611 AND HAS PRIOR APPROVAL OF THE ENGINEER.

ITEM 611, CATCH BASIN ADJUSTED TO GRADE, 25 EACH
 ITEM SPECIAL, MISCELLANEOUS METAL, 1250 LB

CATCH BASIN RECONSTRUCTED TO GRADE

AN ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR RECONSTRUCTING CATCH BASINS TO GRADE.

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF REQUIRED TYPE, SIZE, AND STRENGTH. ENSURE ALL MATERIAL MEETS CMS ITEM 611 AND HAS PRIOR APPROVAL OF THE ENGINEER.

ITEM 611, CATCH BASIN RECONSTRUCTED TO GRADE, 5 EACH
 ITEM SPECIAL, MISCELLANEOUS METAL, 250 LB

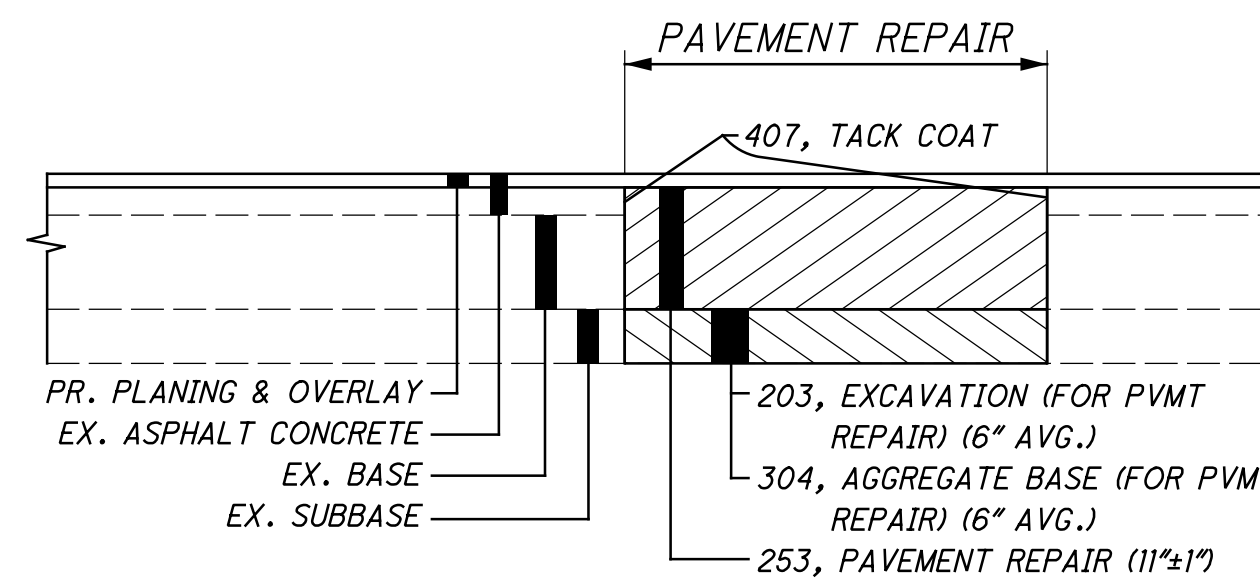
ITEM 253 - PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING THE FULL DEPTH OF DETERIORATED PAVEMENT AND PLACING 11 INCHES ±1 INCH OF 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE SURFACE PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT CONCRETE ON THE MILLED SURFACE.

IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 253, PAVEMENT REPAIR, 750 SQ YD
 ITEM 252, FULL DEPTH PAVEMENT SAWING, 4600 FT



ITEM 203 - EXCAVATION (FOR PAVEMENT REPAIR)

THIS ITEM OF WORK SHALL CONSIST OF REMOVING AND DISPOSING OF ALL UNSUITABLE MATERIAL BY EXCAVATING THE EXISTING SUBGRADE AND SUBBASE TO AN AVERAGE DEPTH OF 6 INCHES OR AS DIRECTED BY THE ENGINEER. EXACT LIMITS OF REMOVAL SHALL BE DETERMINED BY THE ENGINEER. ALL EQUIPMENT, LABOR, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203, EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 203, EXCAVATION (FOR PAVEMENT REPAIR), 125 CU YD

ITEM 304 - AGGREGATE BASE (FOR PAVEMENT REPAIR)

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO BACKFILL AREAS WHICH WERE EXCAVATED UNDER ITEM 203, EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 304, AGGREGATE BASE (FOR PAVEMENT REPAIR), 125 CU YD

**ITEM 611 – MANHOLE ADJUSTED TO GRADE, AS PER PLAN
 ITEM 623 – MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN
 ITEM 638 – VALVE BOX ADJUSTED TO GRADE, AS PER PLAN**

IN ADDITION TO THE REQUIREMENTS OF CMS 611.10.D FOR MANHOLES, 623.05 FOR MONUMENT ASSEMBLIES, OR 638.18 FOR VALVE BOXES, THE CONTRACTOR WILL MAKE A CLEAN CIRCULAR CUT AROUND THE CASTING (48" DIAMETER FOR STORM AND SANITARY MANHOLE CASTINGS, 24"-28" FOR VALVE BOXES AND MONUMENT ASSEMBLIES, AND 2' IN DIAMETER LARGER THAN THE CASTING DIAMETER FOR ANY CASTINGS THAT ARE LARGER THAN STANDARD MANHOLES) AND REMOVE AND DISCARD THE EXISTING CASTING. INSTALL A NEW CASTING TO GRADE (ACCORDING TO TOLERANCES AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1) AFTER THE PAVEMENT SURFACE COURSE HAS BEEN REPLACED.

CMS 499 CLASS QCMS CONCRETE (DYE THE CONCRETE SUCH THAT ITS COLOR CLOSELY MATCHES THE COLOR OF THE SURROUNDING PAVEMENT) WILL BE USED FOR BACKFILLING THE FULL PAVEMENT SECTION AND THE JOINT BETWEEN THE ASPHALT AND CONCRETE WILL BE SEALED WITH CMS 702.01 PG BINDER. EPOXY COATED REBAR SHALL BE PLACED IN THE CONCRETE AT 6" MAXIMUM ON CENTER AND A MINIMUM OF 3.5" CLEARANCE FROM THE TOP, BOTTOM, AND SIDES. THE CONCRETE WILL BE VIBRATED SUFFICIENTLY TO ELIMINATE AIR POCKETS UNDER THE FRAME.

PAYMENT WILL INCLUDE REMOVAL OF THE EXISTING MATERIAL, INSTALLATION AND FURNISHING OF A NEW CASTING, AND ALL LABOR AND MATERIALS REQUIRED TO COMPLETE THIS ITEM OF WORK AS DESCRIBED.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 611, MANHOLE ADJUSTED TO GRADE, AS PER PLAN, 20 EACH
 ITEM 623, MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN, 10 EACH
 ITEM 638, VALVE BOX ADJUSTED TO GRADE, AS PER PLAN, 15 EACH

ENDANGERED SPECIES HABITAT - INDIANA BAT/NORTHERN LONG-EARED BAT

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE AND WITH A MINIMUM HEIGHT OF 13 FEET.

ITEM 254 - PATCHING PLANED SURFACE

THIS ITEM HAS BEEN PROVIDED TO PATCH SPALLED OR UNSOUND AREAS OF THE PLANED SURFACE. THE ESTIMATED QUANTITY PROVIDED FOR IN THESE PLANS COVERS 10 PERCENT OF THE TOTAL PAVING AREA OF THE PROJECT. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 254, PATCHING PLANED SURFACE, 6641 SQ YD

DESIGN AGENCY



DESIGNER
 CMS

REVIEWER
 MJP 04/27/23

PROJECT ID
 102732

SHEET TOTAL
 P.5 35

ITEM SPECIAL - AS-BUILT CONSTRUCTION RECORD DRAWINGS

PRIOR TO FINAL ACCEPTANCE OF THE WORK, THE CONTRACTOR SHALL FURNISH THE DEPARTMENT FORMAL AS-BUILT CONSTRUCTION RECORD-DRAWING PLANS. THE FORMAL AS-BUILT CONSTRUCTION RECORD-DRAWING SHALL INCLUDE ALL RED-LINED CHANGES. RED-LINE CHANGE SHALL BE DENOTED UTILIZING CLOUDING IN MICROSTATION (OR OTHER CAD SOFTWARE) OR CLOUDING IN PDF EDITING SOFTWARE. THE AS-BUILT CONSTRUCTION RECORD-DRAWING SHALL HAVE A SIGNED VERIFICATION ON THE TITLE SHEET FROM THE CONTRACTOR INDICATING THAT ALL RED-LINED AND FIELD CHANGES HAVE BEEN INCORPORATED INTO AS-BUILT CONSTRUCTION RECORD-DRAWINGS.

THE CONTRACTOR'S VERIFICATION STATEMENT INDICATES ALL KNOWN FIELD MODIFICATIONS MADE HAVE BEEN INCLUDED IN THE FORMAL RECORD-DRAWING. THE CONTRACTOR'S VERIFICATION STATEMENT SHALL BE SIGNED BY THE CONTRACTOR'S PROJECT MANAGER (OR ACCEPTABLE REPRESENTATIVE).

IN ADDITION TO THE INFORMATION SHOWN ON THE CONSTRUCTION PLANS, THE AS-BUILT CONSTRUCTION RECORD-DRAWINGS SHALL SHOW THE FOLLOWING:

1. ALL DEVIATIONS FROM THE ORIGINAL APPROVED CONSTRUCTION PLANS WHICH RESULT IN A CHANGE OF LOCATION, MATERIAL, TYPE OR SIZE OF WORK.
2. ANY UTILITIES, PIPES, WELLHEADS, ABANDONED PAVEMENTS, FOUNDATIONS OR OTHER MAJOR OBSTRUCTIONS DISCOVERED AND REMAINING IN PLACE WHICH ARE NOT SHOWN, OR DO NOT CONFORM TO LOCATIONS OR DEPTHS SHOWN IN THE PLANS. UNDERGROUND FEATURES SHALL BE SHOWN AND LABELED ON THE RECORD-DRAWING PLAN IN TERMS OF STATION, OFFSET AND ELEVATION.
3. THE FINAL OPTION AND SPECIFICATION NUMBER SELECTED FOR THOSE ITEMS WHICH ALLOW SEVERAL MATERIAL OPTIONS UNDER THE SPECIFICATION (E.G., CONDUIT).
4. CHANGES TO THE PAY ITEMS AND FINAL QUANTITIES AS PAID SHALL BE SHOWN ON THE GENERAL SUMMARY AND SUBSUMMARIES.
5. ADDITIONAL PLAN SHEETS MAY BE NEEDED IF NECESSARY TO SHOW WORK NOT INCLUDED IN THE CONSTRUCTION PLANS. IF ADDITIONAL PLAN SHEETS ARE NEEDED, THEY ARE REQUIRED TO BE PREPARED IN CONFORMANCE WITH THE LOCATION AND DESIGN MANUAL, VOLUME 3, SECTION 1200 - PLAN PREPARATION.

NOTATION SHALL ALSO BE MADE OF LOCATIONS AND THE EXTENT OF USE OF MATERIALS, OTHER THAN SOIL, FOR EMBANKMENT CONSTRUCTION (ROCK, BROKEN CONCRETE WITHOUT REINFORCING STEEL, ETC.).

THE PLAN INDEX SHALL SHOW THE PLAN SHEETS WHICH HAVE CHANGES APPEARING ON THEM.

TWO COPIES OF THE AS-BUILT CONSTRUCTION RECORD-DRAWINGS SHALL BE DELIVERED TO THE PROJECT ENGINEER FOR APPROVAL UPON COMPLETION OF THE PHYSICAL WORK BUT PRIOR TO THE REQUEST FOR FINAL PAYMENT. AFTER THE DEPARTMENT HAS APPROVED THE AS-BUILT CONSTRUCTION RECORD-DRAWINGS, THE ASSOCIATED ELECTRONIC FILES SHALL BE DELIVERED TO THE DISTRICT CAPITAL PROGRAMS ADMINISTRATOR. ACCEPTANCE OF THESE PLANS AND DELIVERY OF THE ASSOCIATED ELECTRONIC FILES IS REQUIRED PRIOR TO THE WORK BEING ACCEPTED AND THE FINAL ESTIMATE APPROVED.

PAYMENT FOR ALL THE ABOVE SHALL BE LUMP SUM UPON PROPER EXECUTION OF ALL WORK OF THIS ITEM AS DETERMINED BY THE PROJECT ENGINEER.

RAILROAD FLAGGING SERVICE

FLAGGING FOR WORK ON RAILROAD RIGHT OF WAY SHALL BE COORDINATED, OBTAINED, AND PAID FOR BY THE CONTRACTOR. FLAGGING SHALL BE PROVIDED BY THE CONTRACTOR WHENEVER REQUIRED BY THE NORFOLK SOUTHERN SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTEREST. NORFOLK SOUTHERN SHALL APPROVE THE FLAGGING SERVICE PROVIDER AND THEIR STAFF.

NORFOLK SOUTHERN HAS THE SOLE AUTHORITY TO DETERMINE THE NEED FOR PROTECTION SERVICES TO PROTECT ITS OPERATIONS IN GENERAL. THE REQUIREMENTS OF SUCH SERVICES WILL BE WHENEVER THE CONTRACTOR'S PERSONNEL OR EQUIPMENT ARE OR ARE LIKELY TO BE WORKING ON THE RAILROAD'S RIGHT OF WAY OR ACROSS, OVER, ADJACENT TO, OR UNDER A TRACK OR WHEN SUCH WORK HAS DISTURBED OR IS LIKELY TO DISTURB A RAILROAD STRUCTURE OR THE RAILROAD ROADBED OR SURFACE AND ALIGNMENT OF ANY TRACK TO SUCH AN EXTENT THAT THE MOVEMENT OF TRAINS MUST BE CONTROLLED BY FLAGGING.

THE TOTAL DOLLARS IN THE ESTIMATED QUANTITIES IS BASED UPON AN ESTIMATE OF TOTAL FLAGGING DOLLARS NEEDED TO COMPLETE THE PLANNED WORK.

ONLY THE FOLLOWING CERTIFIED FLAGGING PROVIDERS ARE ACCEPTABLE TO NORFOLK SOUTHERN:

Railroad Consultants
Steve Lloyd (VP Business Development)
(615) 542-8901

RailPros
1320 Greenway Dr., Suite 490
Irving, TX 75038
(877) 315-0513
<http://www.railpros.com/services-category/field-services/>

PAYMENT FOR CERTIFIED FLAGGING PROVIDERS WILL BE MADE PER ITEM 900, RAILROAD FLAGGING SERVICES BASED UPON THE INVOICES RECEIVED FROM THE FLAGGING SERVICE FOR THE DOLLARS USED, INCLUDING A FIVE PERCENT MARKUP FOR CONTRACTOR OVERHEAD FOR ADMINISTERING THE CONTRACT WITH THE FLAGGING SERVICE.

IN THE EVENT THAT THE PROJECT IS DELAYED DUE TO RAILROAD FLAGGER AVAILABILITY, THE CONTRACTOR WILL PROVIDE DOCUMENTATION SUPPORTING THEIR EFFORTS TO SCHEDULE A FLAGGER FROM THE FLAGGING SERVICE.

OBJECT MARKERS AND STRUCTURE IDENTIFICATION SIGNS

OBJECT MARKERS WILL BE PLACED ON EACH APPROACH OFF OF THE LEFT AND RIGHT SHOULDER, FACING TRAFFIC, AND BEHIND THE GUARDRAIL IF APPLICABLE. ONE OM-3L AND ONE OM-3R WILL BE INSTALLED AT EACH APPROACH. THE SIGNS WILL BE MOUNTED ON NEW NO. 2 POSTS AND SHALL BE INSTALLED AS PER STANDARD CONSTRUCTION DRAWING TC-41.20, MOST CURRENT REVISION. EACH POST WILL BE 10.5 FT IN LENGTH.

STRUCTURE IDENTIFICATION SIGNS (I-H25b) WILL BE INSTALLED ON THE SAME POST AND DIRECTLY BELOW THE OBJECT MARKER OFF OF THE RIGHT SHOULDER ON EACH APPROACH. A QUANTITY OF ONE SIGN WILL BE INSTALLED AT EACH APPROACH. THE SIGNS WILL HAVE A NON-REFLECTIVE WHITE SHEETING BACKGROUND.

INSTALL SIGNS FOR THE FOLLOWING STRUCTURES:

POR-59-0605 (TWO APPROACHES)

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED FOR EACH APPROACH:

- ITEM 630, SIGN, FLAT SHEET, 730.20, 1 SQ FT
- ITEM 630, SIGN, FLAT SHEET, 6 SQ FT
- ITEM 630, GROUND MOUNTED SUPPORT, NO. 2 POST, 21 FT
- ITEM 630, REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL, 3 EACH
- ITEM 630, REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL, 2 EACH

DESIGN AGENCY



DESIGNER

CMS

REVIEWER

MJP 04/27/23

PROJECT ID

102732

SHEET TOTAL

P.6 35

MAINTENANCE OF TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, AND THE FOLLOWING:

1. IN SECTIONS WITH 2 LANES, A MINIMUM OF ONE TEN (10) FOOT BIDIRECTIONAL LANE SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.

IN SECTIONS WITH 3 OR MORE LANES, A MINIMUM OF ONE TEN (10) FOOT LANE IN EACH DIRECTION SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.

2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208 EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.

3. LANE RESTRICTIONS OR REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.

4. ALL FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS SHALL BE COMPLETED ON THE SAME DAY THE EXCAVATION IS MADE. IF THE CONTRACTOR CANNOT COMPLETE THE WORK, THE EXCAVATION SHALL BE BACKFILLED OR PROTECTED AS PER STANDARD CONSTRUCTION DRAWING MT-101.90.

5. TRUCK-MOUNTED ATTENUATORS (TMA'S) SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.

6. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES, OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE (1) MILE URBAN.

7. FOR ROUTES NOT ON THE PERMITTED LANE CLOSURE CHART, ONLY DURING OFF-PEAK PERIODS (I.E. ANY PERIOD OTHER THAN 6-8 AM AND 3-6 PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.

8. IN ADDITION TO THE REQUIREMENTS OF C&MS 614.11, WORK ZONE PAVEMENT MARKINGS, THE CONTRACTOR SHALL, AT THE END OF EACH DAY OF WORK, REPLACE WITH WORK ZONE MARKINGS ALL LANE, CENTER, STOP, AND CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.

9. A QUANTITY OF 20 CU. YDS. OF ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS AS DIRECTED BY THE ENGINEER.

10. PRIOR TO OPENING TO TRAFFIC, EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.

11. THE CONTRACTOR SHALL PLACE THE SIGNS W8-1 (BUMP) PER OMUTCD 2C.28, W8-11 (UNEVEN LANES) PER OMUTCD 6F.45, AND W6-3 (TWO-WAY TRAFFIC) PER OMUTCD 6F.32. PAYMENT FOR THESE SIGNS SHALL BE INCIDENTAL TO THE LUMP SUM ITEM 614, MAINTAINING TRAFFIC. A QUANTITY OF ITEM 614, WORK ZONE MARKING SIGNS HAS BEEN INCLUDED IN THE PLANS PER C&MS 614.04.

12. THE CONTRACTOR SHALL SET A WORK ZONE AT THE REQUEST OF THE ENGINEER TO ALLOW THE LAYOUT OF THE FULL DEPTH PAVEMENT REPAIR AREAS. THIS WORK IS INCIDENTAL TO ITEM 614, MAINTAINING TRAFFIC.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

ALL PHASES:
ITEM 614, WORK ZONE MARKING SIGN, 4 EACH

PHASE I: PLANED SURFACE
ITEM 614, WORK ZONE CENTERLINE, CLASS I, 2.25 MILE
ITEM 614, WORK ZONE LANE LINE, CLASS I, 6", 2.57 MILE
ITEM 614, WORK ZONE STOP LINE, CLASS I, 794 FT
ITEM 614, WORK ZONE CHANNELIZING LINE, CLASS I, 12", 2187 FT
ITEM 614, WORK ZONE SCHOOL SYMBOL MARKING, 96", CLASS I, 3 EACH

PHASE II: INTERMEDIATE COURSE
ITEM 614, WORK ZONE CENTERLINE, CLASS I, 2.25 MILE
ITEM 614, WORK ZONE LANE LINE, CLASS I, 6", 2.57 MILE
ITEM 614, WORK ZONE STOP LINE, CLASS I, 794 FT
ITEM 614, WORK ZONE CHANNELIZING LINE, CLASS I, 12", 2187 FT
ITEM 614, WORK ZONE SCHOOL SYMBOL MARKING, 96", CLASS I, 3 EACH

PHASE III: SURFACE COURSE
ITEM 614, WORK ZONE CENTERLINE, CLASS III, 642 PAINT 2.25 MILE
ITEM 614, WORK ZONE LANE LINE, 6", CLASS III, 642 PAINT 2.57 MILE
ITEM 614, WORK ZONE STOP LINE, CLASS III, 642 PAINT 794 FT
ITEM 614, WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT, 2187 FT
ITEM 614, WORK ZONE SCHOOL SYMBOL MARKING, 96", CLASS III, 642 PAINT, 3 EACH

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND ALL OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD). PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER FIFTEEN (15) DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT FOR MINIMAL PERIODS OF TIME THAT ONE-WAY TRAFFIC WILL BE PERMITTED CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

DROPOFFS

THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE MILLED SURFACES OR ASPHALT SURFACE COURSE AND SIDE STREET APPROACHES OR DRIVEWAYS GREATER THAN 1.25 INCH. THE CONTRACTOR SHALL PLACE A 12:1 ASPHALT WEDGE FOR ALL RESULTING ELEVATION DIFFERENCES GREATER THAN 1.25 INCH PRIOR TO OPENING TO TRAFFIC. THE PAVING OF INTERSECTION APPROACHES AND DRIVEWAYS, PER THE NOTES ON SHEET XX, SHALL BE PERFORMED WITHIN SEVEN (7) DAYS OF THE MAINLINE SURFACE COURSE BEING APPLIED AND A DROPOFF BEING CREATED BETWEEN THE NEW SURFACE COURSE AND THE MILLED/EXISTING SIDE ROAD OR DRIVEWAY SURFACE. THE CONTRACTOR MAY ELECT TO PLACE A 12:1 ASPHALT WEDGE IN LIEU OF COMPLETING THE PAVING, HOWEVER THE ASPHALT CONCRETE USED FOR THE WEDGE SHALL BE CONSIDERED INCIDENTAL TO ITEM 614, MAINTAINING TRAFFIC AND SHALL INCLUDE THE REMOVAL OF THE WEDGE BEFORE THE INTERSECTION/DRIVEWAY IS PAVED.

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE SEVEN (7) CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$1,000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
ROAD & RAMP CLOSURES	≥ 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	≥ 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERNS CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

TIME LIMITATION, CURB RAMP

THE MAXIMUM ALLOWABLE TIME FOR THE CONTRACTOR TO HAVE AN INDIVIDUAL CURB RAMP AND ASSOCIATED SIDEWALK LEADING INTO THE CURB RAMP OUT OF SERVICE FOR THE REMOVAL AND REPLACEMENT SHALL BE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS (THIS TIME PERIOD INCLUDES ALL WORK AND CURING TIME).

AT THE CONCLUSION OF THE CONSTRUCTION OF THE CURB RAMP AND PRIOR TO OPENING THE CURB RAMP TO PEDESTRIAN TRAFFIC, THE CONTRACTOR SHALL ENSURE THAT THE REQUIREMENTS OF STANDARD CONSTRUCTION DRAWING BP-7.1 ARE MET. THE CONTRACTOR SHALL USE ASPHALT AS A WEDGE, OR SUBMIT ANOTHER METHOD APPROVED BY THE ENGINEER TO ENSURE THE TRANSITION FROM THE CURB RAMP TO THE ROADWAY IS PER STANDARD CONSTRUCTION DRAWING BP-7.1. ALL COSTS TO PERFORM THIS WORK SHALL BE INCIDENTAL TO THE ASSOCIATED PAY ITEMS FOR THE INSTALLATION OF THE CURB RAMP.

SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THEY SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$1000 PER DAY PER AFFECTED CURB RAMP THAT REMAINS OUT OF SERVICE BEYOND FOURTEEN (14) CONSECUTIVE CALENDAR DAYS.

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED)	LABOR DAY
TOTAL SOLAR ECLIPSE (04/08/24)	BALLOON A-FAIR (09/14/24-09/17/24)
MEMORIAL DAY	GENERAL/REGULAR ELECTION (NOV)
FOURTH OF JULY (OBSERVED)	THANKSGIVING
RAVENNA CRUISE-IN (AUG)	CHRISTMAS (OBSERVED)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
MONDAY	12:00N MONDAY THROUGH 12:00 AM WEDNESDAY (TOTAL SOLAR ECLIPSE)
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY	5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY (GEN./REG. ELECTION)
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY (THANKSGIVING)
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRIAN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LANE VALUE CONTRACT			
DESCRIPTION OF CRITICAL LANE TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME PERIOD
SR 59	PER MAINTAINING TRAFFIC NOTE	PER LANE/ PER MINUTE	\$50



ASPHALT PAVING LIMITATION

THE CONTRACTOR SHALL NOT ANTICIPATE OR SCHEDULE TO PLACE ASPHALT (ASPHALT SURFACE COURSE, ASPHALT INTERMEDIATE COURSE, ASPHALT CONCRETE BASE, ETC.) BETWEEN NOVEMBER 1 AND APRIL 1 WHEN SUBMITTING THEIR INITIAL BAR CHART PROGRESS SCHEDULE TO THE DISTRICT CONSTRUCTION ENGINEER (DCE) AS SPECIFIED IN CMS 108.02A. THIS LIMITATION SHALL ALSO INCLUDE INITIAL BASE LINE SCHEDULES AND ALL UPDATES IF A CPM SCHEDULE IS REQUIRED.

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC CONTROL DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614 – LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS FOR USES OTHER THAN THOSE SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G. DIRECTING MOTORISTS THROUGH A RED LIGHT).

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT, OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES OR SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, PURSUIT OF THE MOTORIST IS APPROPRIATE.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN THE WORK ZONE.

THE LEOS WORK AT THE DIRECTION OF THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO THE DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE DUTIES OF THE LEOS AND THEIR PLACEMENT AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING THEIR SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF THEIR SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF THEIR SHIFT. WHEN THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON THEIR SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF THE SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS DESCRIBED ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE, 100 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE.

ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN, AND REMOVE WHEN NO LONGER NEEDED, A PORTABLE CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON THE LIST OF PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND CLASS B UNITS WITH MINIMUM LEGIBILITY DISTANCE OF 800 FT AND 650 FT RESPECTIVELY.

EACH SIGN SHALL BE TRAILER MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM TO DIM THE SIGN DURING DARKNESS AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHOULD BE DELINEATED.

PLACEMENT, OPERATION, MAINTENANCE, AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE PCMS SHOULD NOT BE LOCATED IN THE MEDIUM OF A HIGHWAY UNLESS IT IS PROTECTED FROM BOTH DIRECTIONS OF TRAFFIC. THE CONTRACTOR SHALL, AT THE DISCRETION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR TO ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS WILL BE OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED TO FACE AWAY FROM ALL TRAFFIC AND SHALL DISPLAY ONE OR MORE TYPE G YELLOW REFLECTIVE SHEETING SURFACES OF 9 INCH BY 15 INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT AND TO CHANGE SIGN MESSAGES IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE CONTRACTOR. A LIST OF ALL PROPOSED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED, OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR AREAS) ALLOW REMOTE SIGN ACTIVATION, DEACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS, AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC ACCRUED BY THE DEPARTMENT WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON THEIR CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24 HOURS PER DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THEIR USE. THE REQUIREMENT TO FURNISH, INSTALL, MAINTAIN, AND REMOVE A PCMS UNIT ON THIS PROJECT SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITIES AS OUTLINED IN CMS 614.02.

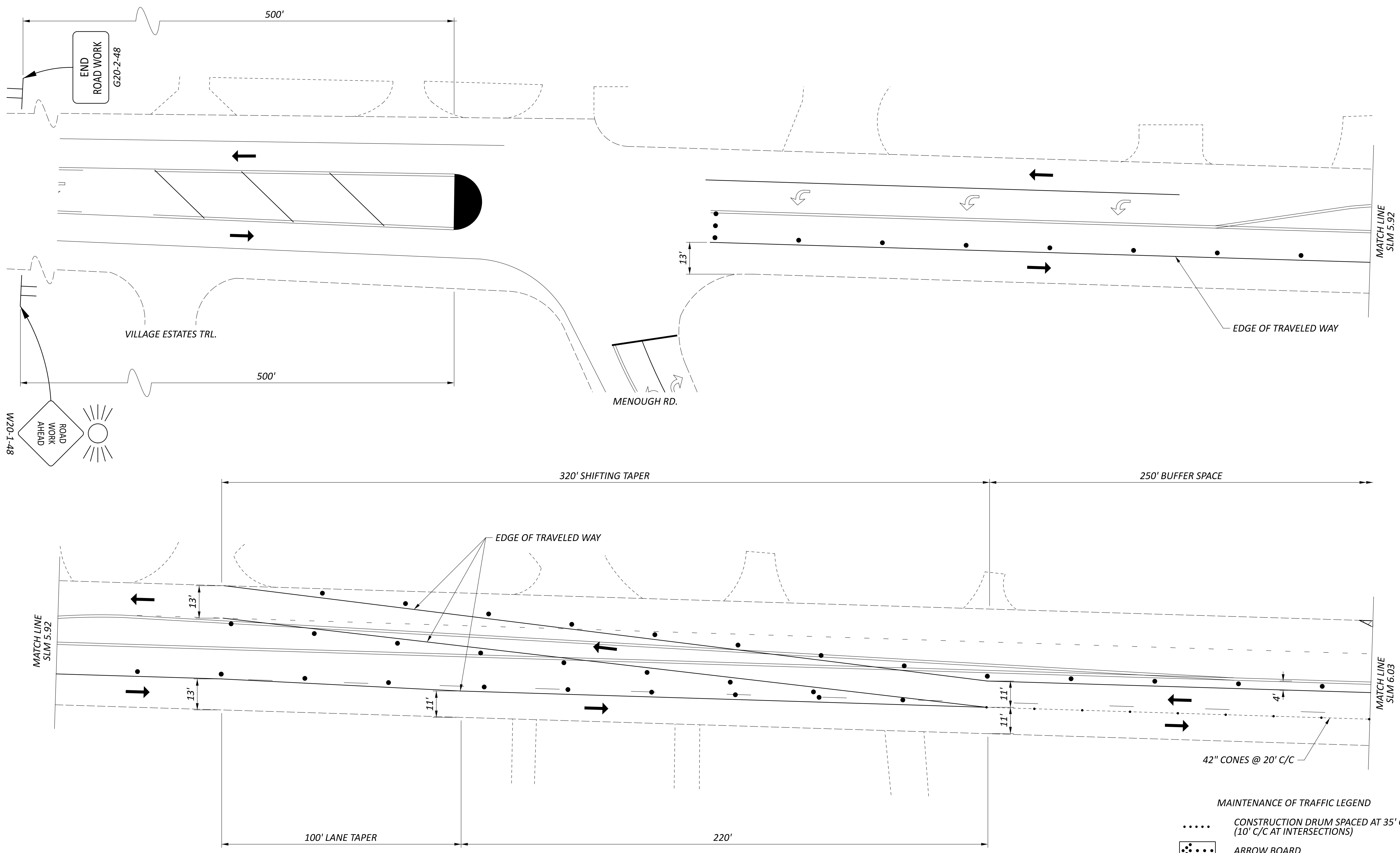
PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN, 14 SIGN MONTH, ASSUMING 2 SIGNS FOR 7 MONTHS

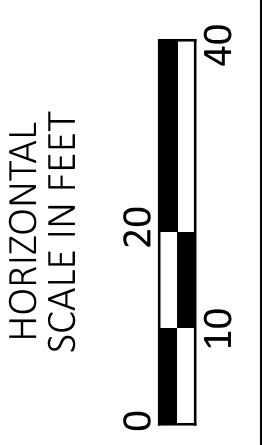
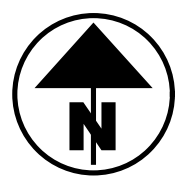
STRUCTURE EXPANSION JOINT REPLACEMENT AND BACKWALL REPAIRS

BRIDGE JOINT REPLACEMENT WORK AND BACKWALL REPAIR WORK SHALL BE PROTECTED FROM TRAFFIC AS DIRECTED BY THE MAINTENANCE OF TRAFFIC NOTES AND THE SCHEMATIC DETAIL ON SHEETS 9-14. THE CONTRACTOR IS PERMITTED TO UTILIZE TWO SEPARATE THREE-DAY PHASE WORK ZONES TO PERFORM THIS WORK BY CONSTRUCTING ONE SIDE OF THE BRIDGE REPAIRS AT A TIME. THE DURATION OF EACH WORK ZONE AND LANE CLOSURE SHALL NOT EXCEED THREE CONSECUTIVE DAYS. THE DATES FOR THESE LANE CLOSURES SHALL BE COORDINATED WITH THE ENGINEER EIGHTEEN (18) DAYS PRIOR TO PERFORMING THE WORK. ALL MAINTENANCE OF TRAFFIC ASSOCIATED WITH THIS ITEM OF WORK SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.





- MAINTENANCE OF TRAFFIC LEGEND**
- CONSTRUCTION DRUM SPACED AT 35' C/C (10' C/C AT INTERSECTIONS)
 - ARROW BOARD
 - DIRECTION OF TRAFFIC FLOW
 - WORK AREA

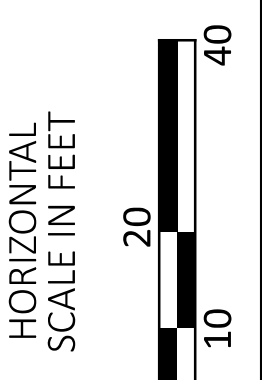
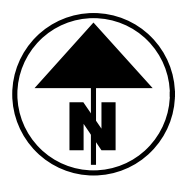
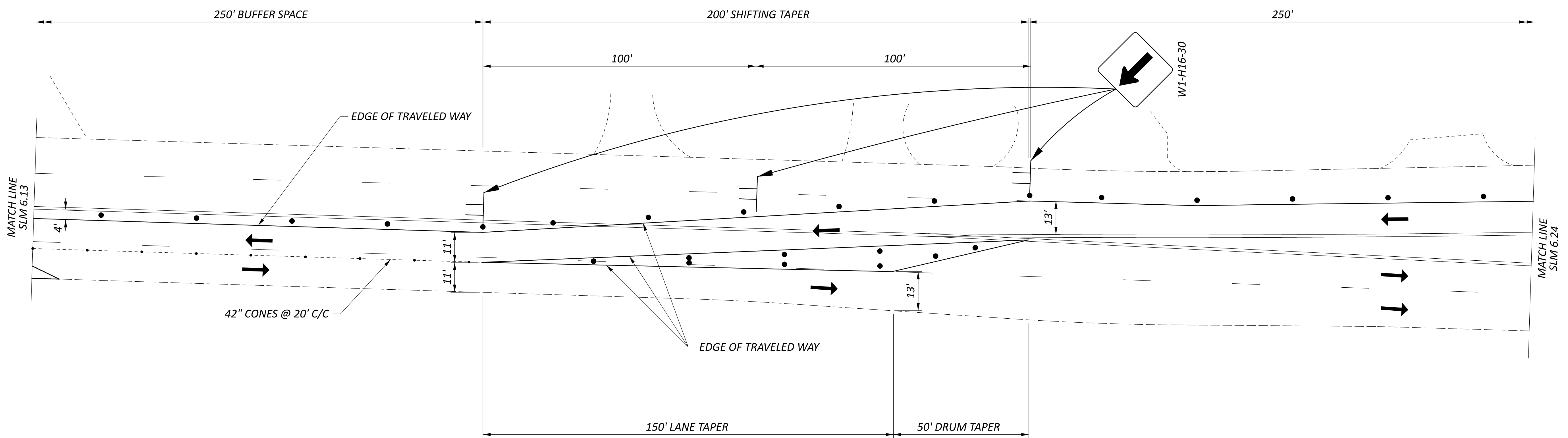
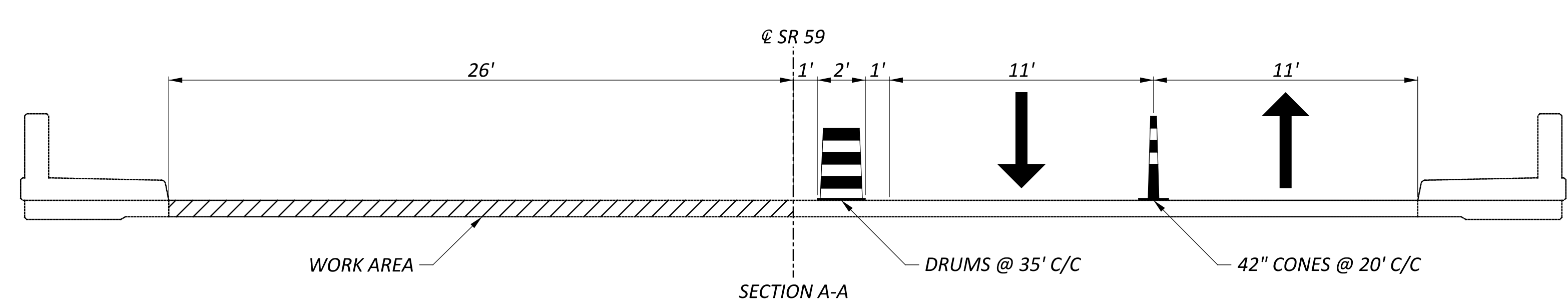
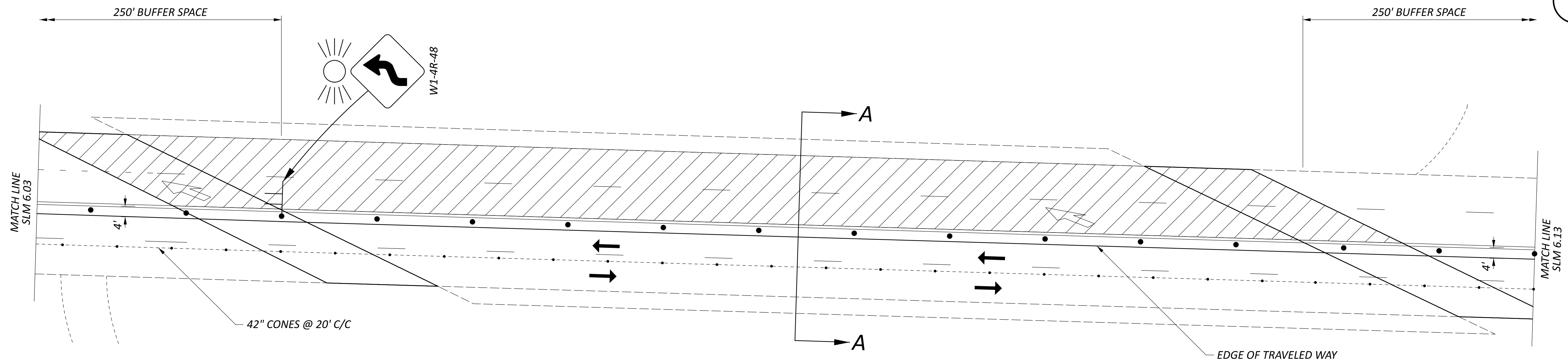


**MAINTENANCE OF TRAFFIC PLAN
 PHASE 1**

DESIGN AGENCY



DESIGNER	CMS
REVIEWER	MJP 08/10/23
PROJECT ID	102732
SHEET	TOTAL
P.9	35



**MAINTENANCE OF TRAFFIC PLAN
PHASE 1**

DESIGN AGENCY



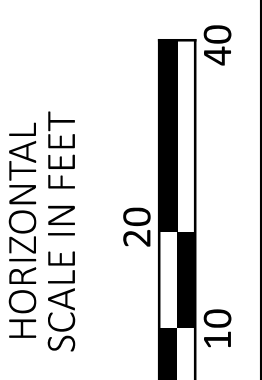
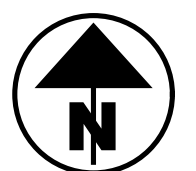
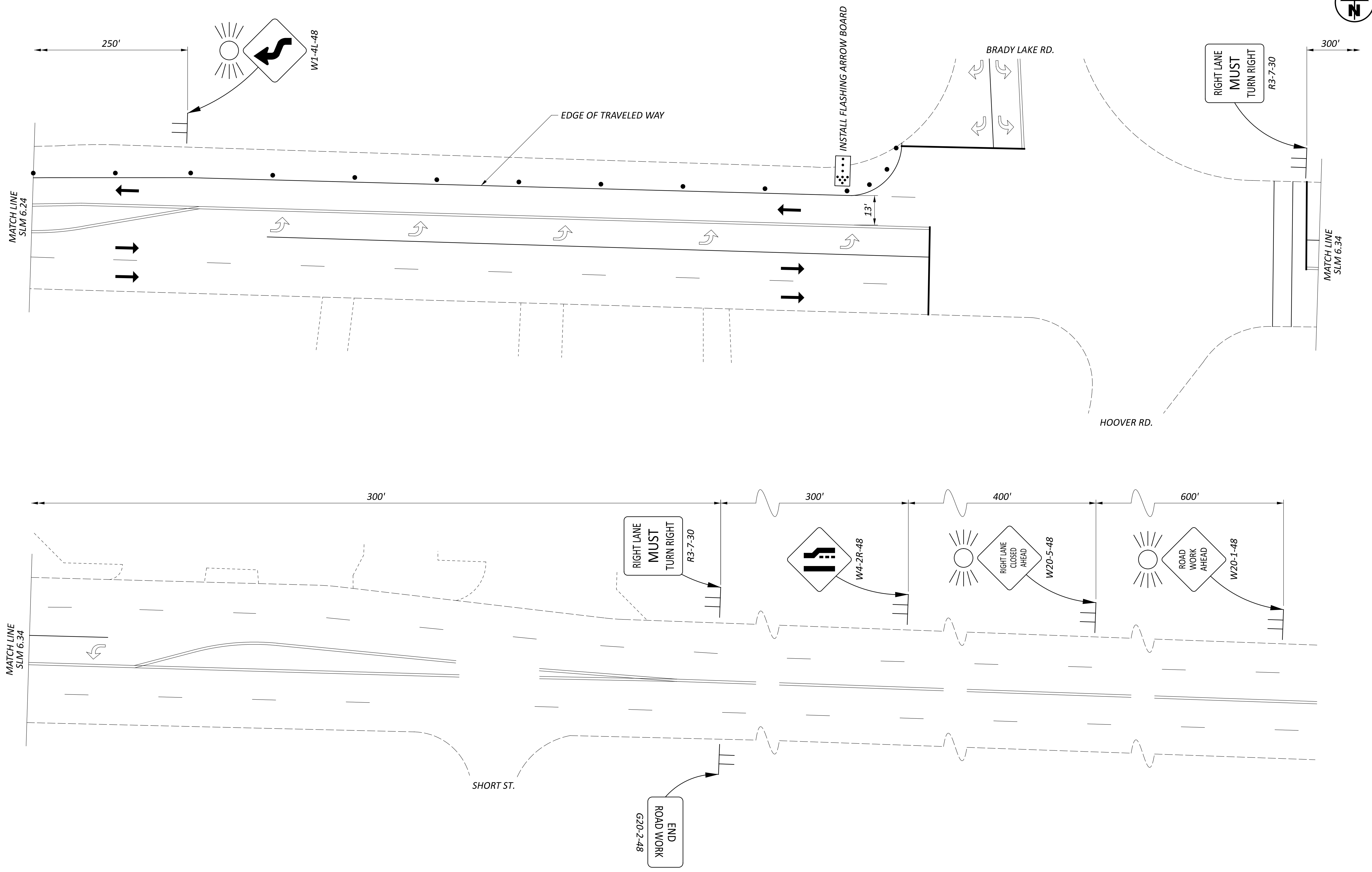
DESIGNER
CMS

REVIEWER
MJP 08/10/23

PROJECT ID
102732

SHEET TOTAL
P.10 | 35

SEE SHEET 9 FOR MAINTENANCE OF TRAFFIC LEGEND



MAINTENANCE OF TRAFFIC PLAN
 PHASE 1

DESIGN AGENCY



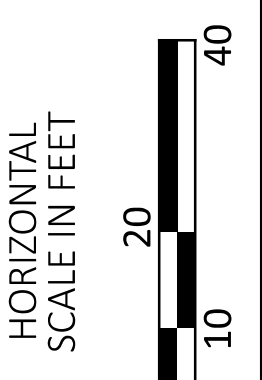
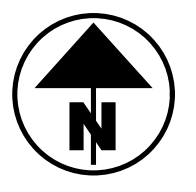
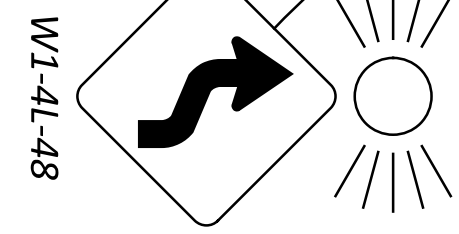
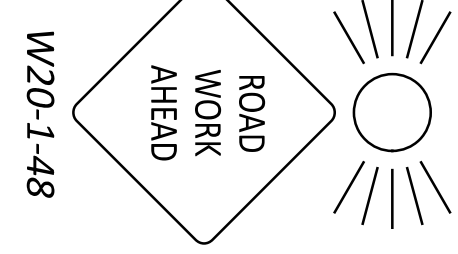
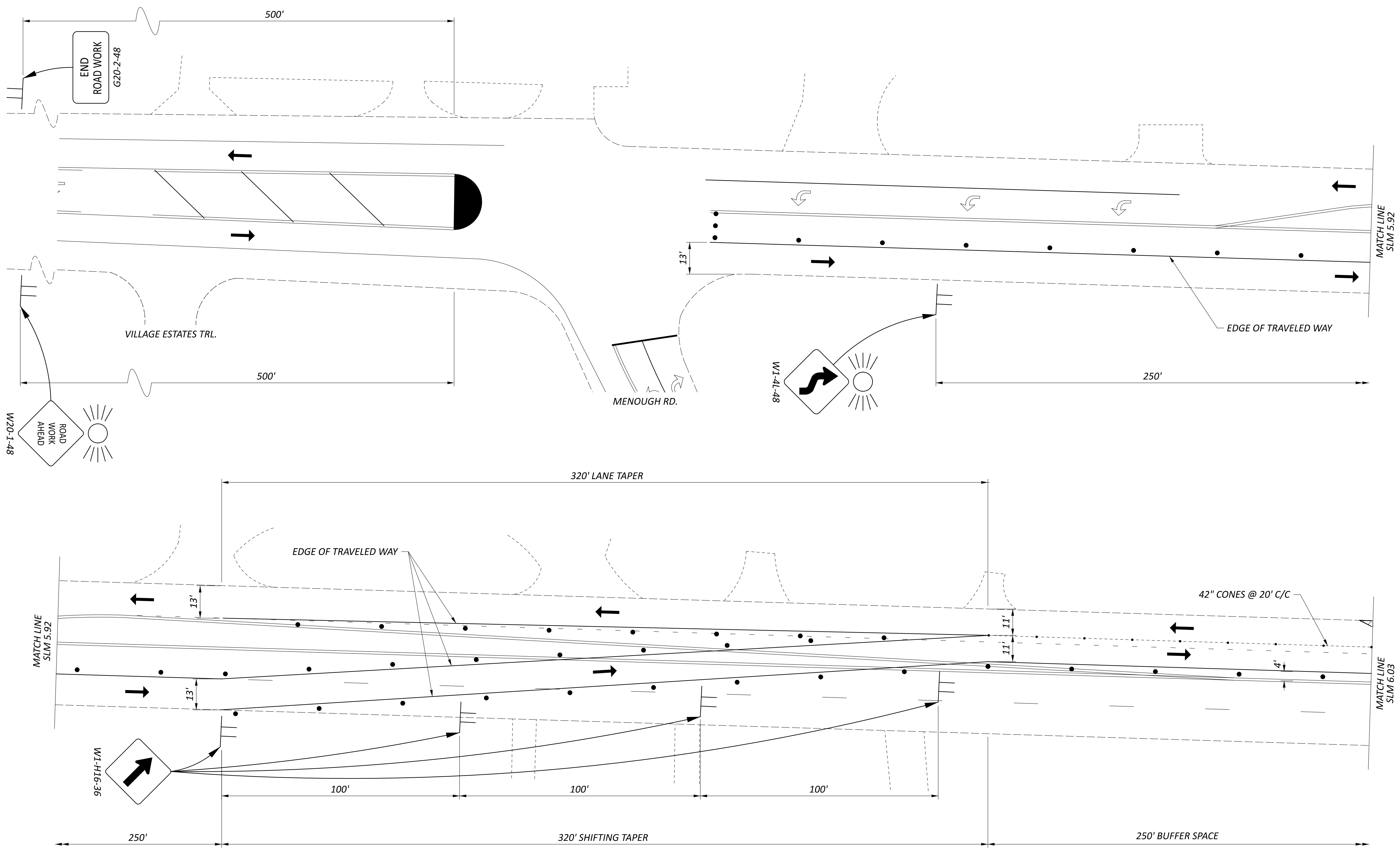
DESIGNER
 CMS

REVIEWER
 MJP 08/10/23

PROJECT ID
 102732

SHEET	TOTAL
P.11	35

SEE SHEET 9 FOR MAINTENANCE OF TRAFFIC LEGEND



MAINTENANCE OF TRAFFIC PLAN
PHASE 2

DESIGN AGENCY



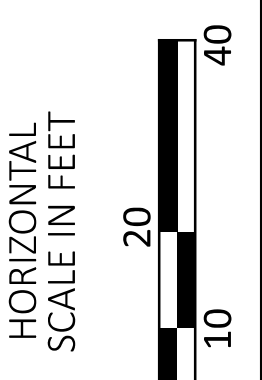
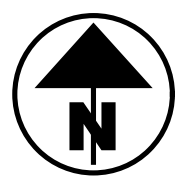
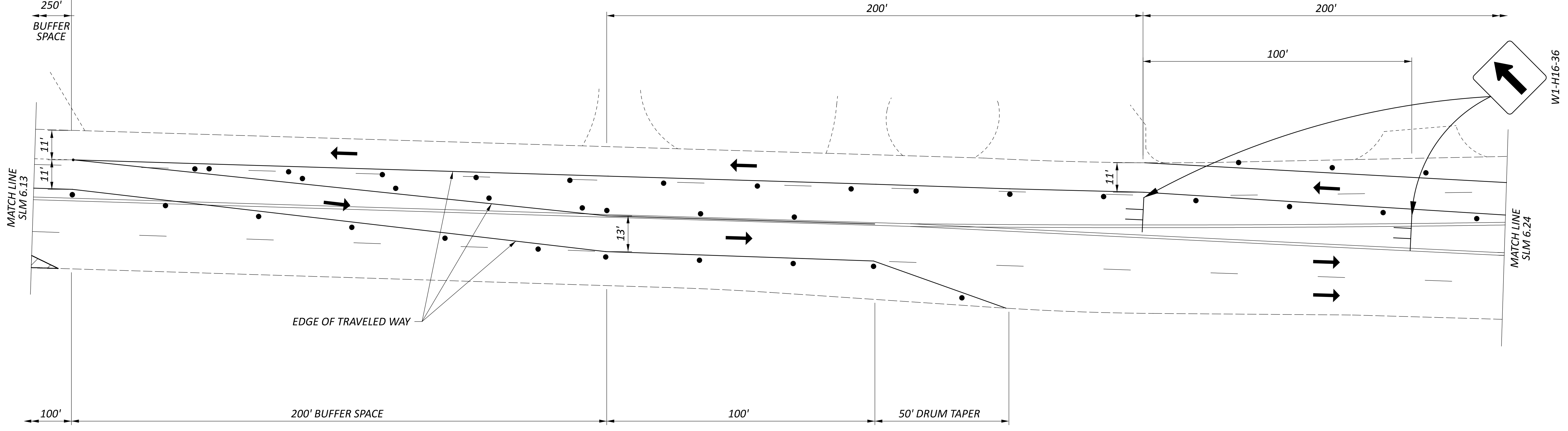
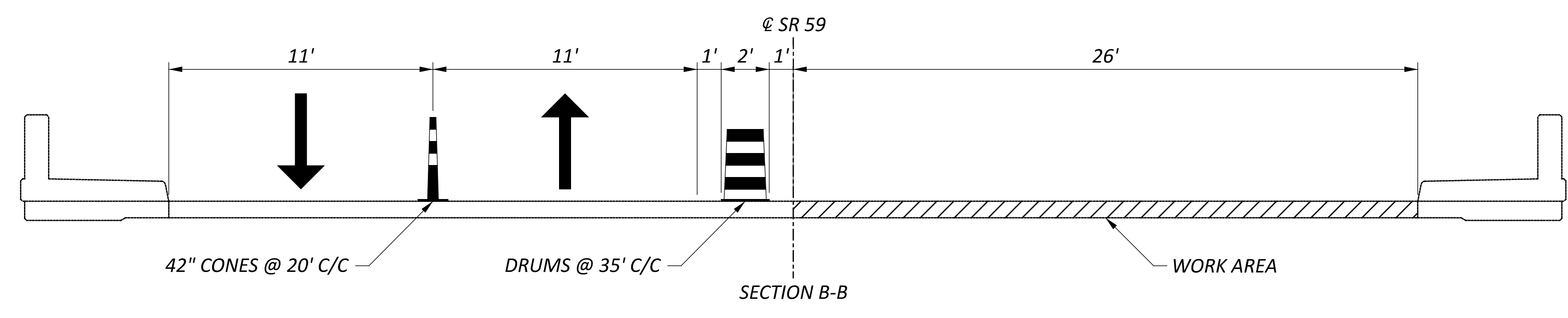
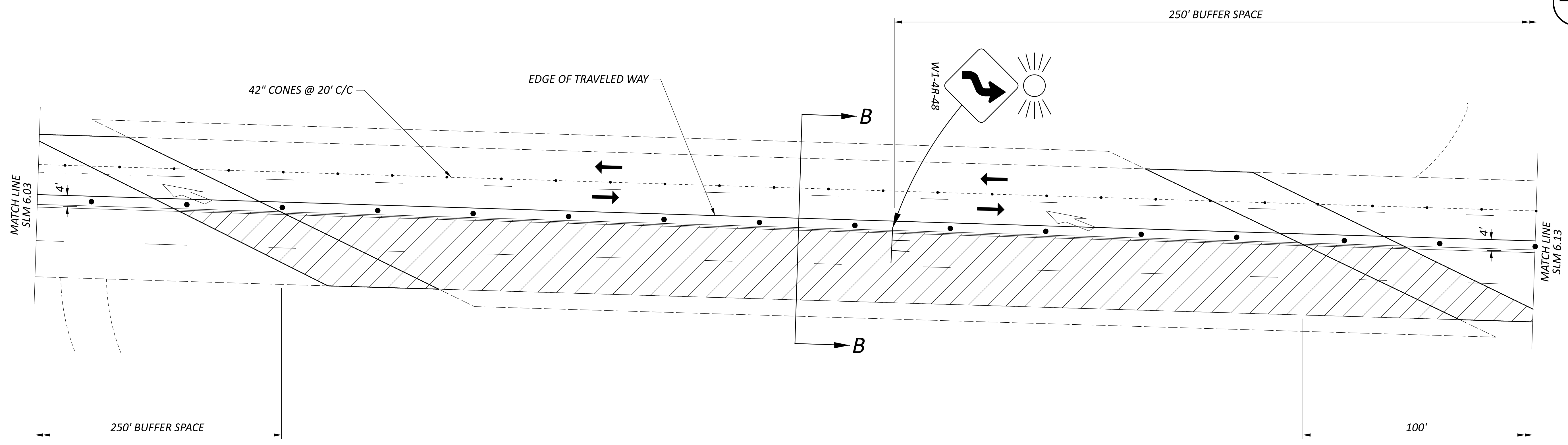
DESIGNER
CMS

REVIEWER
MJP 08/10/23

PROJECT ID
102732

SHEET TOTAL
P.12 35

SEE SHEET 9 FOR MAINTENANCE OF TRAFFIC LEGEND



MAINTENANCE OF TRAFFIC PLAN
PHASE 2

DESIGN AGENCY



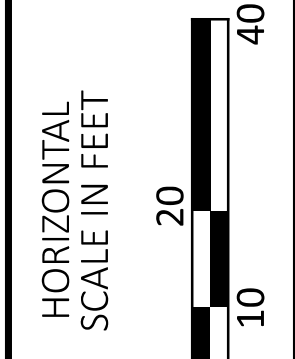
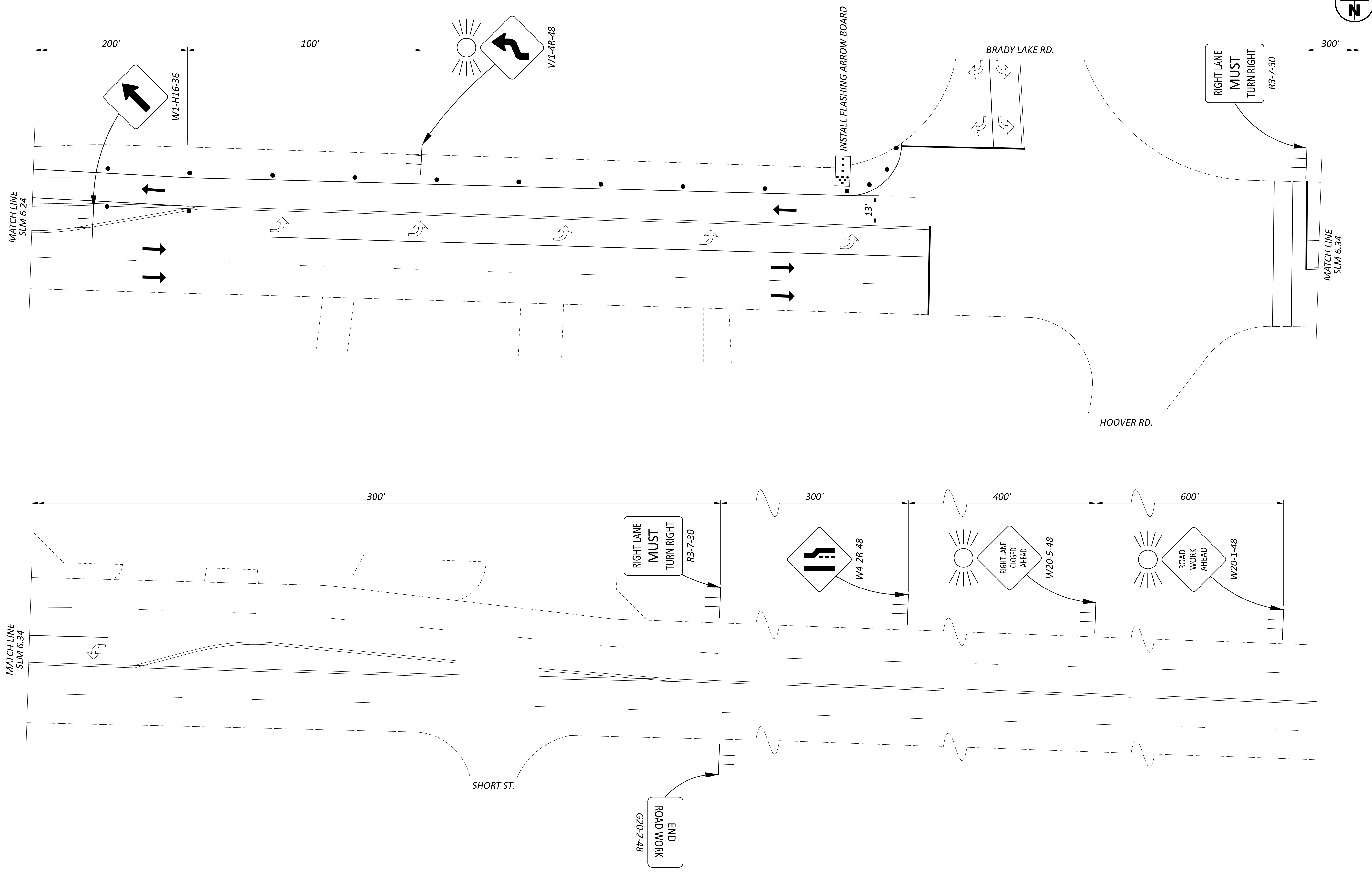
DESIGNER
CMS

REVIEWER
MJP 08/10/23

PROJECT ID
102732

SHEET	TOTAL
P.13	35

SEE SHEET 9 FOR MAINTENANCE OF TRAFFIC LEGEND



MAINTENANCE OF TRAFFIC PLAN
PHASE 2

DESIGN AGENCY



DESIGNER
CMS

REVIEWER
MJP 08/10/23


PROJECT ID
102732

SHEET TOTAL
P.14 35

SEE SHEET 9 FOR MAINTENANCE OF TRAFFIC LEGEND


SHEET NUM.										PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
5	6	7	8	17	18	19	26	27	29	01/NHS/05/RAVE	02/NHS/05/RAVE	03/NHS/13							
ROADWAY																			
					6,104	4,178					10,282		202	30000	10,282	SF	WALK REMOVED		
					107	300					407		202	32000	407	FT	CURB REMOVED		
					159	5					164		202	32500	164	FT	CURB AND GUTTER REMOVED		
125											125		203	10000	125	CY	EXCAVATION (FOR PAVEMENT REPAIR)		
						1					1		203	10000	1	CY	EXCAVATION (FOR WALK OR CURB RAMP INSTALLATION)		
					1,330	1,525					2,855		608	10000	2,855	SF	4" CONCRETE WALK		
					4,195	2,746					6,941		608	52000	6,941	SF	CURB RAMP		
10										10			623	39501	10	EACH	MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN	5	
	LUMP									LUMP			SPECIAL	69091000	LS		AS-BUILT CONSTRUCTION PLANS	6	
EROSION CONTROL																			
										3,000			832	30000	3,000	EACH	EROSION CONTROL		
DRAINAGE																			
25										25			611	98630	25	EACH	CATCH BASIN ADJUSTED TO GRADE		
5											5		611	98634	5	EACH	CATCH BASIN RECONSTRUCTED TO GRADE		
20										20			611	99655	20	EACH	MANHOLE ADJUSTED TO GRADE, AS PER PLAN	5	
1,250										1,250			SPECIAL	61199820	1,250	LB	MISCELLANEOUS METAL	5	
PAVEMENT																			
4,600											4,600		252	01500	4,600	FT	FULL DEPTH PAVEMENT SAWING		
750											750		253	01001	750	SY	PAVEMENT REPAIR, AS PER PLAN	5	
				66,413							66,413		254	01001	66,413	SY	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (T = 2.0")	5	
											6,641		254	01600	6,641	SY	PATCHING PLANED SURFACE		
125											125		304	20000	125	CY	AGGREGATE BASE (FOR PAVEMENT REPAIR)		
					9,963						9,963		407	13900	9,963	GAL	TACK COAT, 702.13 @ 0.08 GAL/SY		
					1,384						1,384		424	13101	1,384	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE A, (449), AS PER PLAN	5	
					2,306						2,306		441	50200	2,306	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448)		
						199	7				206		609	12000	206	FT	COMBINATION CURB AND GUTTER, TYPE 2		
						134	375				509		609	26000	509	FT	CURB, TYPE 6		
WATER WORK																			
15						1					15	1	638	10801	16	EACH	VALVE BOX ADJUSTED TO GRADE, AS PER PLAN	5	
TRAFFIC CONTROL																			
	42												42	630	02100	42	FT	GROUND MOUNTED SUPPORT, NO. 2 POST	
	12												12	630	80100	12	SF	SIGN, FLAT SHEET	
	2												2	630	80100	2	SF	SIGN, FLAT SHEET, 730.20	
	6												6	630	84900	6	EACH	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	
	4												4	630	86002	4	EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	
							2.73				2.73	0.16	646	10110	2.73	MILE	LANE LINE, 6"		
							2.33				2.25	0.08	646	10200	2.33	MILE	CENTER LINE		
							218	1,969			2,187		646	10310	2,187	FT	CHANNELIZING LINE, 12"		
							301	493			794		646	10400	794	FT	STOP LINE		
							1,897	3,658			5,555		646	10520	5,555	FT	CROSSWALK LINE, 24"		
								763			763		646	10600	763	FT	TRANSVERSE/DIAGONAL LINE, WHITE		
								258			258		646	10600	258	FT	TRANSVERSE/DIAGONAL LINE, YELLOW		
								317			317		646	20370	317	EACH	TWO WAY LEFT TURN ARROW		
								12			12		646	10900	12	EACH	HANDICAP SYMBOL MARKING		
							3				3		646	20110	3	EACH	SCHOOL SYMBOL MARKING, 96"		
								2,621			2,621		646	20200	2,621	FT	PARKING LOT STALL MARKING		
							6	41			47		646	20300	47	EACH	LANE ARROW		
							2				2		646	20350	2	EACH	LANE REDUCTION ARROW		
								3			3		646	20370	3	EACH	TWO WAY LEFT TURN ARROW		
STRUCTURE REPAIRS																			
																		29	FOR POR-59-0605 ESTIMATED QUANTITIES
MAINTENANCE OF TRAFFIC																			
										100			614	11110	100	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	8	
										4			614	12460	4	EACH	WORK ZONE MARKING SIGN		
										20			614	13000	20	CY	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC		
										14			614	18601	14	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	7	
										5.14			614	20010	5.14	MILE	WORK ZONE LANE LINE, CLASS 1, 6"		

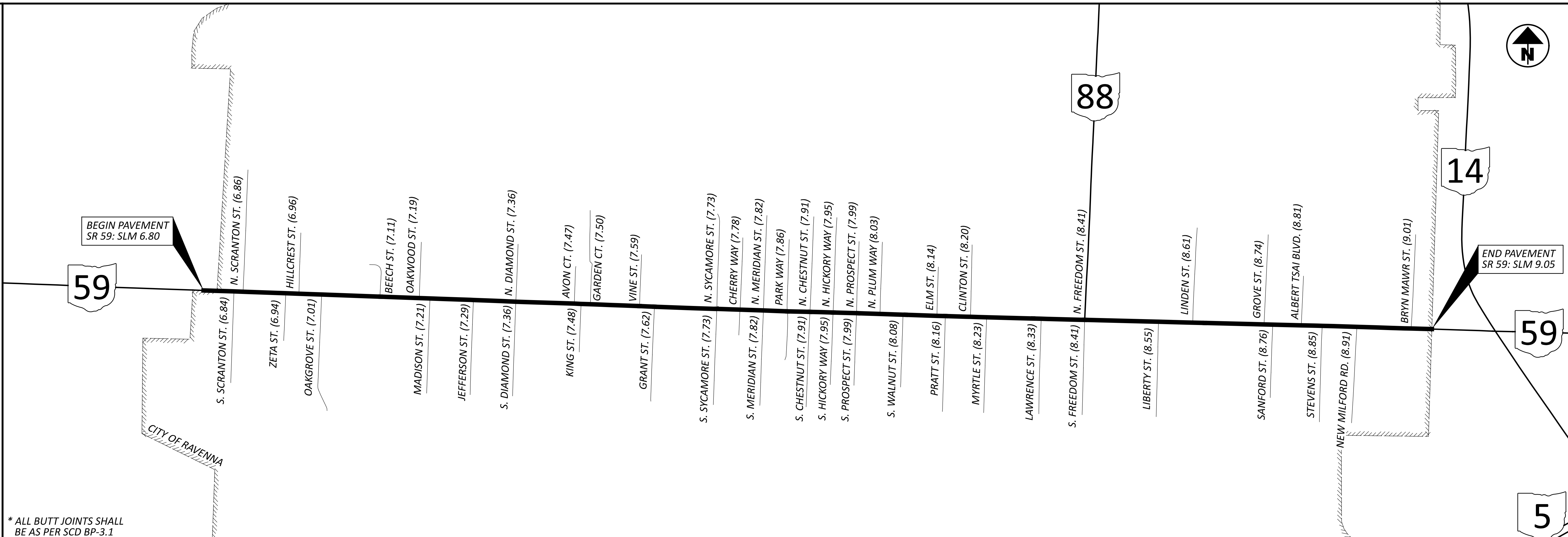
GENERAL SUMMARY

DESIGN AGENCY

 DESIGNER
CMS
 REVIEWER
MJP 04/27/23
 PROJECT ID
102732
 SHEET TOTAL
P.15 | 35

SHEET NUM.										PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
5	6	7	8	17	18	19	26	27	29	01/NHS/05/RAVE	02/NHS/05/RAVE	03/NHS/13						
		2.57								2.57			614	20560	2.57	MILE	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	
		4.5								4.5			614	21000	4.5	MILE	WORK ZONE CENTER LINE, CLASS I	
		2.25								2.25			614	21550	2.25	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT	
		4,374								4,374			614	23010	4,374	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	
		2,187								2,187			614	23690	2,187	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
		1,588								1,588			614	26000	1,588	FT	WORK ZONE STOP LINE, CLASS I	
		794								794			614	26610	794	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
		6								6			614	31742	6	EACH	WORK ZONE SCHOOL SYMBOL MARKING, 96", CLASS I	
		3								3			614	31750	3	EACH	WORK ZONE SCHOOL SYMBOL MARKING, 96", CLASS III, 642 PAINT	
																	INCIDENTALS	
										LS			614	11000	LS		MAINTAINING TRAFFIC	
										6			619	16010	6	MNTH	FIELD OFFICE, TYPE B	
										LS			623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
										LS			624	10000	LS		MOBILIZATION	
	243,000											243,000	900	00100	243,000	EACH	RAILROAD FLAGGING SERVICES	


GENERAL SUMMARY

DESIGN AGENCY

 DESIGNER
 CMS
 REVIEWER
 MJP 04/27/23
 PROJECT ID
 102732
 SHEET TOTAL
 P.16 | 35



SLM RANGE	TYPICAL SECTION	SIDE	DISTANCE (D)	AVERAGE WIDTH (W)	SURFACE AREA (A) A=DxW/9	CADD GENERATED AREA	254	407	407	424	441														
							PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (T = 2.0")	TACK COAT, 702.13 @ 0.09 GAL/SY	TACK COAT, 702.13 @ 0.06 GAL/SY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE A, (449), AS PER PLAN (T = 0.75")	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448) (T = 1.25")														
			FT	FT	SQ YD	SQ YD	SY	GAL	GAL	CY	CY														
MAINLINE SR 59																									
6.80	TO	7.21	3	L/R	2164.80	50.00	12026.67																		
7.21	TO	7.58	1	L/R	1953.60	42.00	9116.80																		
7.58	TO	7.73	1	L/R	792.00	47.00	4136.00																		
7.73	TO	7.86	4	L/R	686.40	71.00	5414.93																		
7.86	TO	7.91	5	L/R	264.00	71.00	2082.67																		
7.91	TO	7.95	6	L/R	211.20	86.00	2018.13																		
7.95	TO	7.99	7	L/R	211.20	69.00	1619.20																		
7.99	TO	8.08	1	L/R	475.20	69.00	3643.20																		
8.08	TO	8.16	1	L/R	422.40	40.00	1877.33																		
8.16	TO	8.61	1	L/R	2376.00	29.00	7656.00																		
8.61	TO	9.05	2	L/R	2323.20	55.00	14197.33																		
INTERSECTIONS																									
6.80	TO	9.05			VARIES	VARIES	2624.03	2624.03	236.16	157.44	54.67	91.11													
SUBTOTALS							66412.30	5977.11	3984.74	1383.59	2305.98														
TOTALS CARRIED TO GENERAL SUMMARY							66413	5978	3985	1384	2306														

PAVEMENT CALCULATIONS


DESIGN AGENCY

 DESIGNER
 CMS
 REVIEWER
 MJP 04/27/23
 PROJECT ID
 102732
 SHEET TOTAL
 P.17 35

POR-59-(6.05)(6.80)

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MAIN ROUTE	INTERSECTING ROUTE	DESIGN SHEET	QUADRANT RL=REAR LT, RR=REAR RT FL=FWD LT, FR=FWD RT (LOOKING UPSTATION)	CURB RAMP TYPE (SCD BP-7.1, SHEET 2/3) (BT = BLENDED TRANSITION)	202	202	202	203	608	608	609	609	638					COMMENTS
					WALK REMOVED	CURB REMOVED	CURB AND GUTTER REMOVED	EXCAVATION (FOR WALK OR CURB RAMP INSTALLATION)	4" CONCRETE WALK	CURB RAMP	CURB, TYPE 6	COMBINATION CURB AND GUTTER, TYPE 2	VALVE BOX ADJUSTED TO GRADE, AS PER PLAN					
					SF	FT	FT	CY	SF	SF	FT	FT	EACH					
SR 59	S. SCRANTON ST.	16	RL	A2/C2	73.75	8.00			40.00	33.75	10.00							
SR 59	N. SCRANTON ST.	16	FL	B2	83.00	4.00			20.00	63.00	5.00							
SR 59	ZETA ST.	16	RL	A2/C2	115.00	9.00			45.00	70.00	11.25							
SR 59	HILLCREST ST.	16	FL	B2	113.50	10.00			50.00	63.50	12.50							POSSIBLE WAIVER LOCATION
SR 59	BEECH ST.	15	RL	A2-1	102.50	7.00			35.00	67.50	8.75							
		15	FL	A2-1	110.00	7.00			35.00	75.00	8.75							
SR 59	OAKWOOD ST.	15	RL	A2-3	91.75	9.00			45.00	46.75	11.25							POSSIBLE WAIVER LOCATION
		17	FL	D-A1	172.50	8.00			40.00	132.50	10.00							
		16	FR	A2/C2	110.00	10.00			50.00	60.00	12.50							
SR 59	GARDEN CT.	15	RL	A2-1	66.00	6.00			30.00	36.00	7.50		1.00					
		15	FL	A2-1	75.00	6.00			30.00	45.00	7.50							
SR 59	SYCAMORE ST.		RL	BT	172.99	10.00			50.00	122.99	12.50							
			RR	BT	201.29	8.00			40.00	161.29	10.00							
			FL	BT	132.32	5.00	3.00		40.00	92.32	6.25	3.75						
			FR	BT	196.43		10.00		50.00	146.43		12.50						
SR 59	CHERRY WAY		RL	BT	75.47					75.47								
			RR	BT	87.84					87.84								
			FL	BT	49.41					49.41								
			FR	BT	92.08					92.08								
SR 59	MERIDIAN ST.		RL	BT	195.35		10.00		50.00	145.35		12.50						
			RR	BT	198.88		10.00		50.00	148.88		12.50						
			FL	BT	215.78		10.00		50.00	165.78		12.50						
			FR	BT	214.18		10.00		50.00	164.18		12.50						
SR 59	PARK WAY		RL1	BT	91.58		6.00		30.00	61.58		7.50						CROSSING SR 59
			RL2	BT	77.39					77.39								CROSSING PARK WAY
			RR	BT	188.60		10.00		50.00	138.60		12.50						
			FL	BT	172.40					172.40								
			FR	BT	131.25		10.00		50.00	81.25		12.50						
SR 59	CHESTNUT ST.		RL	BT	300.36		10.00		50.00	250.36		12.50						
			RR	BT	327.60		10.00		50.00	277.60		12.50						
			FL	BT	203.06		10.00		50.00	153.06		12.50						
			FR	BT	314.23		10.00		50.00	264.23		12.50						
SR 59	HICKORY WAY		RL	BT	69.40													
			RR	BT	184.62													
			FL	BT	79.92													CROSSING HICKORY WAY
			FR	BT	78.05													CROSSING HICKORY WAY
SR 59	PROSPECT ST.		RL	BT	205.91		10.00		50.00	155.91		12.50						
			RR	BT	223.10		10.00		50.00	173.10		12.50						
			FL	BT	189.54		10.00		50.00	139.54		12.50						
			FR	BT	154.12		10.00		50.00	104.12		12.50						
SR 59	PLUM WAY		RL	BT	78.41													
			FL	BT	88.87													
SUBTOTALS					6103.40	107.00	159.00	0.00	1330.00	4194.14	133.75	198.75	1.00	0.00	0.00	0.00	0.00	
TOTALS CARRIED TO GENERAL SUMMARY					6104	107	159	0	1330	4195	134	199	1	0	0	0	0	

CURB RAMP SUBSUMMARY


DESIGN AGENCY

 DESIGNER
CMS
 REVIEWER
MJP 04/27/23
 PROJECT ID
102732
 SHEET TOTAL
P.18 | 35

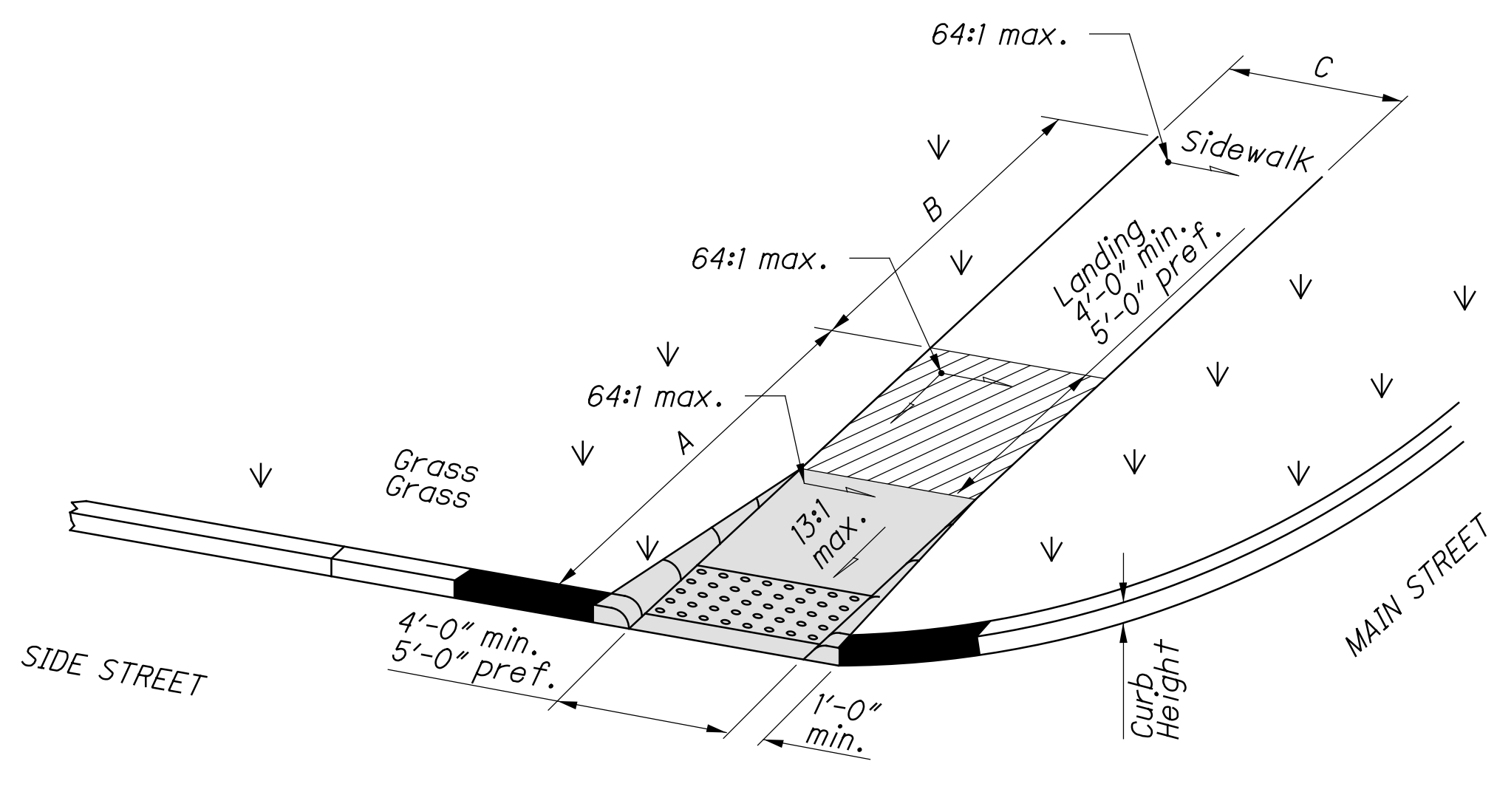
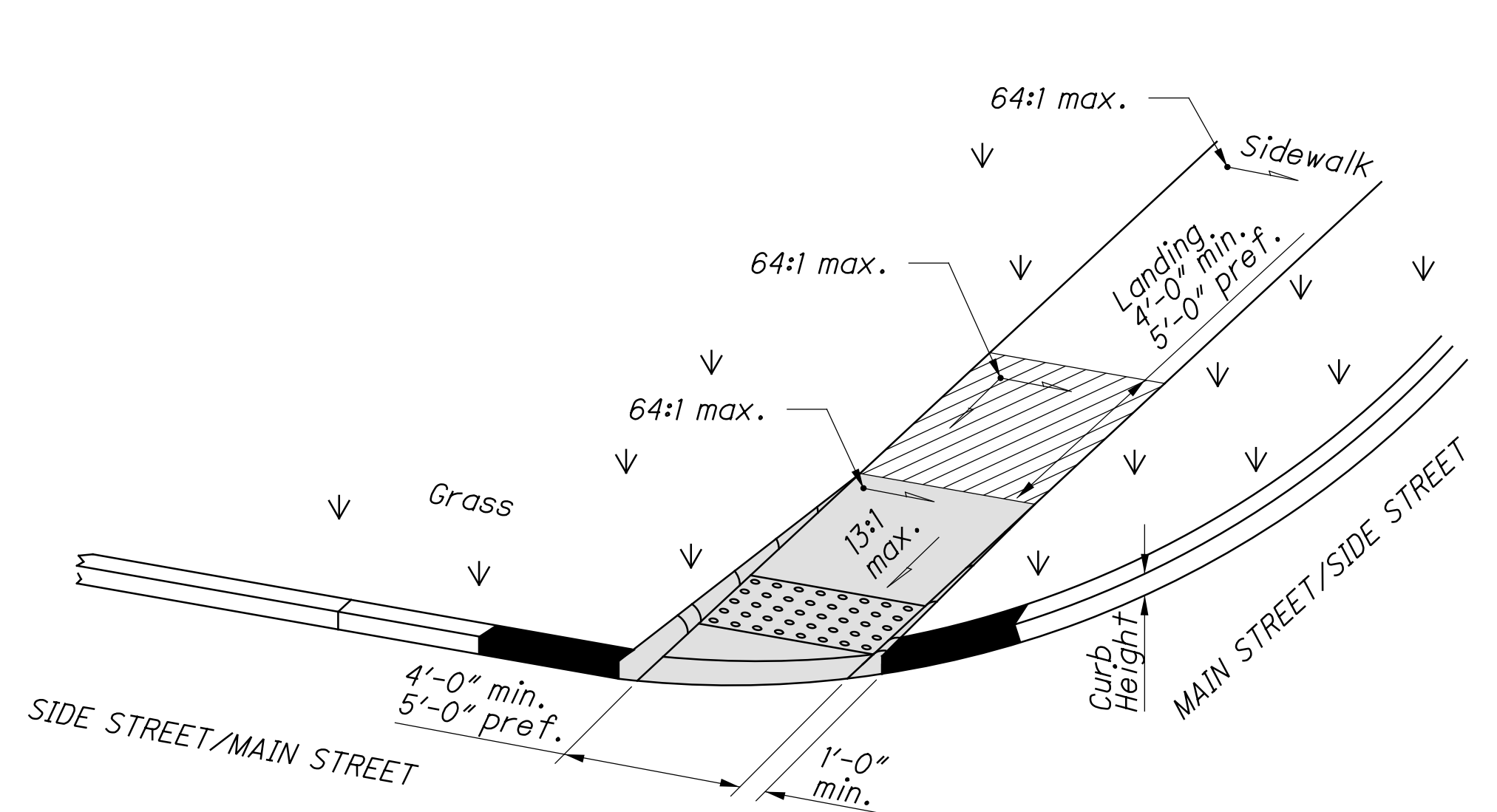
POR-59-(6.05)(6.80)

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MAIN ROUTE	INTERSECTING ROUTE	DESIGN SHEET	QUADRANT RL=REAR LT, RR=REAR RT FL=FWD LT, FR=FWD RT (LOOKING UPSTATION)	CURB RAMP TYPE (SCD BP-7.1, SHEET 2/3)	202	202	202	203	608	608	609	609	638					COMMENTS
					WALK REMOVED	CURB REMOVED	CURB AND GUTTER REMOVED	EXCAVATION (FOR WALK OR CURB RAMP INSTALLATION)	4" CONCRETE WALK	CURB RAMP	CURB, TYPE 6	COMBINATION CURB AND GUTTER, TYPE 2	VALVE BOX ADJUSTED TO GRADE, AS PER PLAN					
					SF	FT	FT	CY	SF	SF	FT	FT	EACH					
SR 59	WALNUT ST.		RR	BT	142.81		5.00		25.00	117.81		6.25						
			FL	BT	352.77	12.00			60.00	292.77	15.00							
			FR	BT	215.44	6.00			30.00	185.44	7.50							
SR 59	ELM ST.	15	RL	A2-5	197.18	12.00			60.00	147.18	15.00					POSSIBLE WAIVER LOCATION		
			RR	A2/C2	122.82	8.00			40.00	72.82	10.00							
			FL	A2-1	58.96	6.00			30.00	33.96	7.50					POSSIBLE WAIVER LOCATION		
SR 59	PRATT ST.	19	RR	C1	109.13	10.00			50.00	59.13	12.50							
			FR	B2	96.75	12.00			60.00	120.00	15.00							
SR 59	CLINTON ST.	15	RL	A2-3	108.07	5.00			25.00	58.07	6.25							
			FL	A2-5	215.11	12.00			60.00	165.11	15.00							
SR 59	MYRTLE ST.	15	RR	A2-3	143.93	10.00			50.00	93.93	12.50							
			FR	A2-3	145.79	10.00			50.00	95.79	12.50							
SR 59	LAWRENCE ST.	15	RR	A2-3	105.12	10.00			50.00	65.12	12.50							
			FR	A2-3	97.37	10.00			50.00	57.37	12.50							
SR 59	S. FREEDOM ST. / SR 88	19	RL	D-C1	215.50	15.00			75.00	140.50	18.75							
			FL	A2-5	170.00	6.00			30.00	140.00	7.50							
SR 59	LINDEN ST.	15	RL	A2-3	90.00	8.00			40.00	50.00	10.00							
			FL	B3	105.00	10.00			50.00	55.00	12.50							
SR 59	GROVE ST.	16	RL	B3	130.00	12.00			60.00	70.00	15.00							
			FL	D-B3	112.50	6.00			30.00	82.50	7.50							
			FR	C2	72.80	8.00			40.00	32.80	10.00							
SR 59	SANFORD ST.	15	RR	A2-3	140.15	10.00			50.00	90.15	12.50							
			FR	A2-3	61.80	5.00			25.00	36.80	6.25							
SR 59	ALBERT TSAI BLVD.	16	RL	B3	95.00	10.00			50.00	45.00	12.50							
			FL	D-C1	120.00	12.00			60.00	60.00	15.00							
SR 59	STEVENS ST.	15	RR	A2-3	68.00	8.00			40.00	28.00	10.00							
			FR	A2-3	57.00	5.00			25.00	32.00	6.25							
SR 59	NEW MILFORD RD.	16	RL	A2/C2	85.00	10.00		0.37	50.00	35.00	12.50							
			RR1	A2/C2	85.00	10.00		0.37	50.00	35.00	12.50					CROSSING SR 59		
			RR2	A2-3	92.50	10.00			50.00	42.50	12.50					CROSSING NEW MILFORD RD.		
			FR	B3	115.00	10.00			50.00	65.00	12.50							
SR 59	BRYN MAWR ST.	15	RL	A2-3	126.00	12.00			60.00	66.00	15.00							
			FL	B3	125.00	10.00			50.00	75.00	12.50							
SUBTOTALS					4177.50	300.00	5.00	0.74	1525.00	2745.75	375.00	6.25	0.00	0.00	0.00	0.00	0.00	
TOTALS CARRIED TO GENERAL SUMMARY					4178	300	5	1	1525	2746	375	7	0	0	0	0	0	

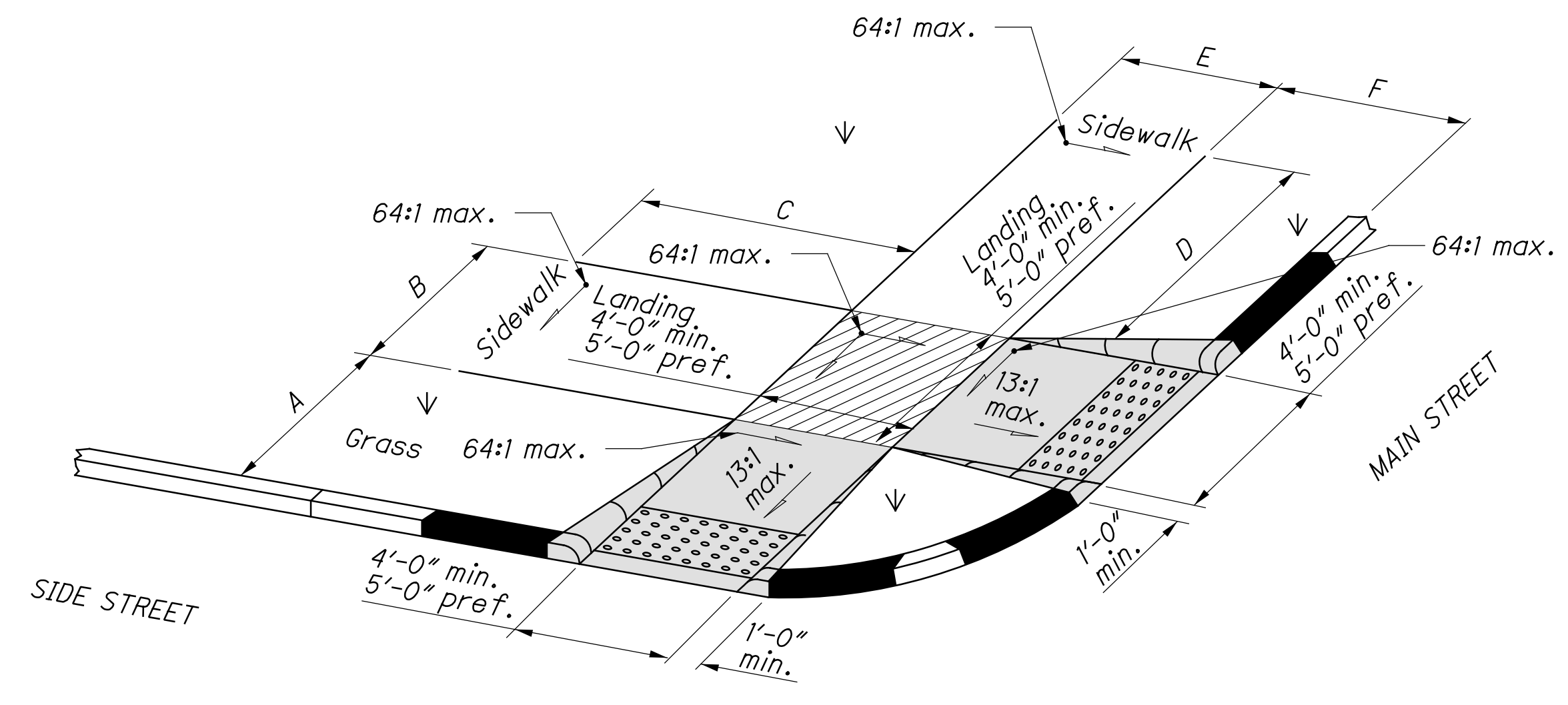
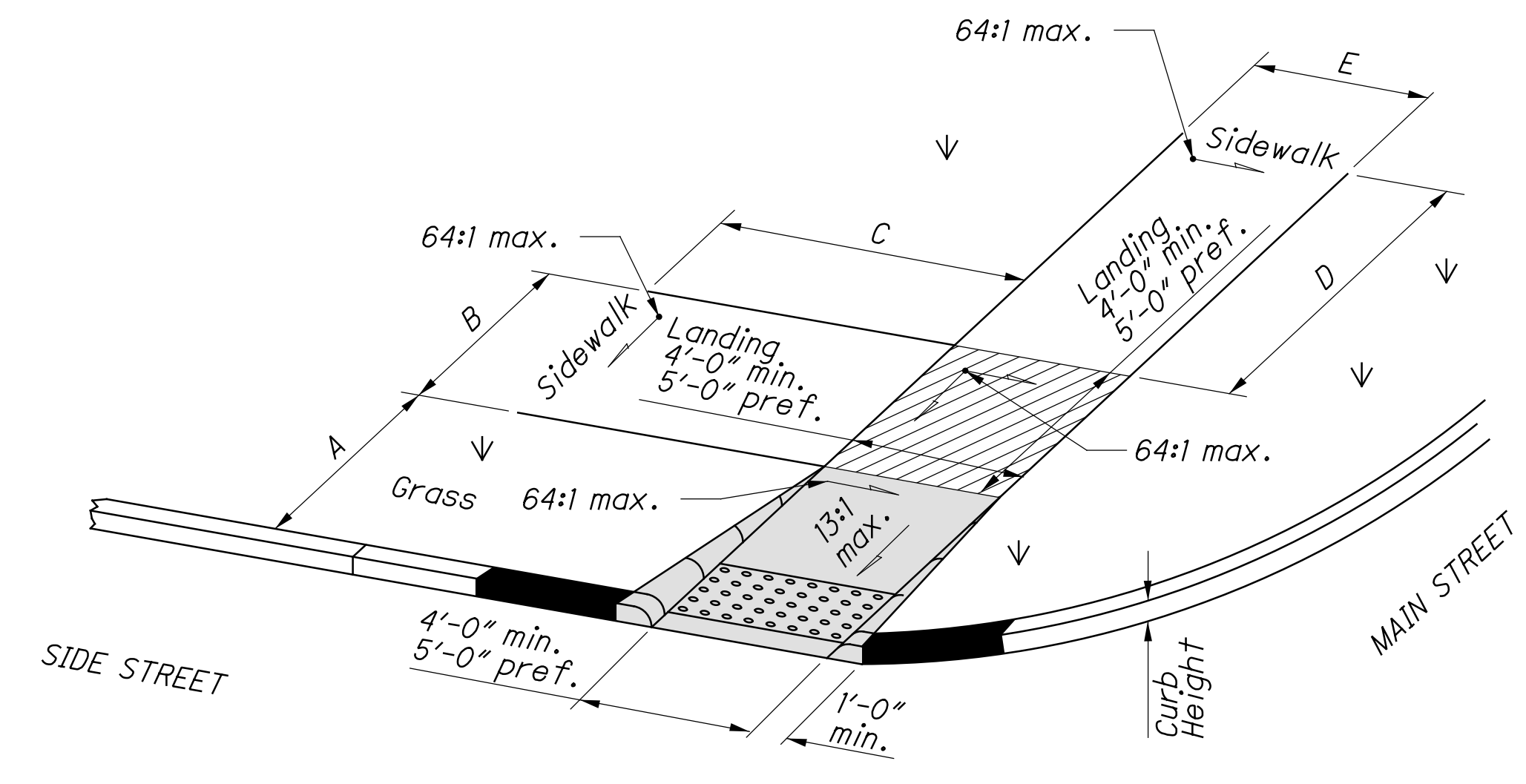
CURB RAMP SUBSUMMARY

DESIGN AGENCY

 DESIGNER
CMS
 REVIEWER
MJP 04/27/23
 PROJECT ID
102732
 SHEET TOTAL
P.19 | 35



* ALIGN TRUNCATED DOMES WITH THE PRIMARY DIRECTION OF THE RAMP FOR SKEWED CONDITIONS

Type A2 - 1



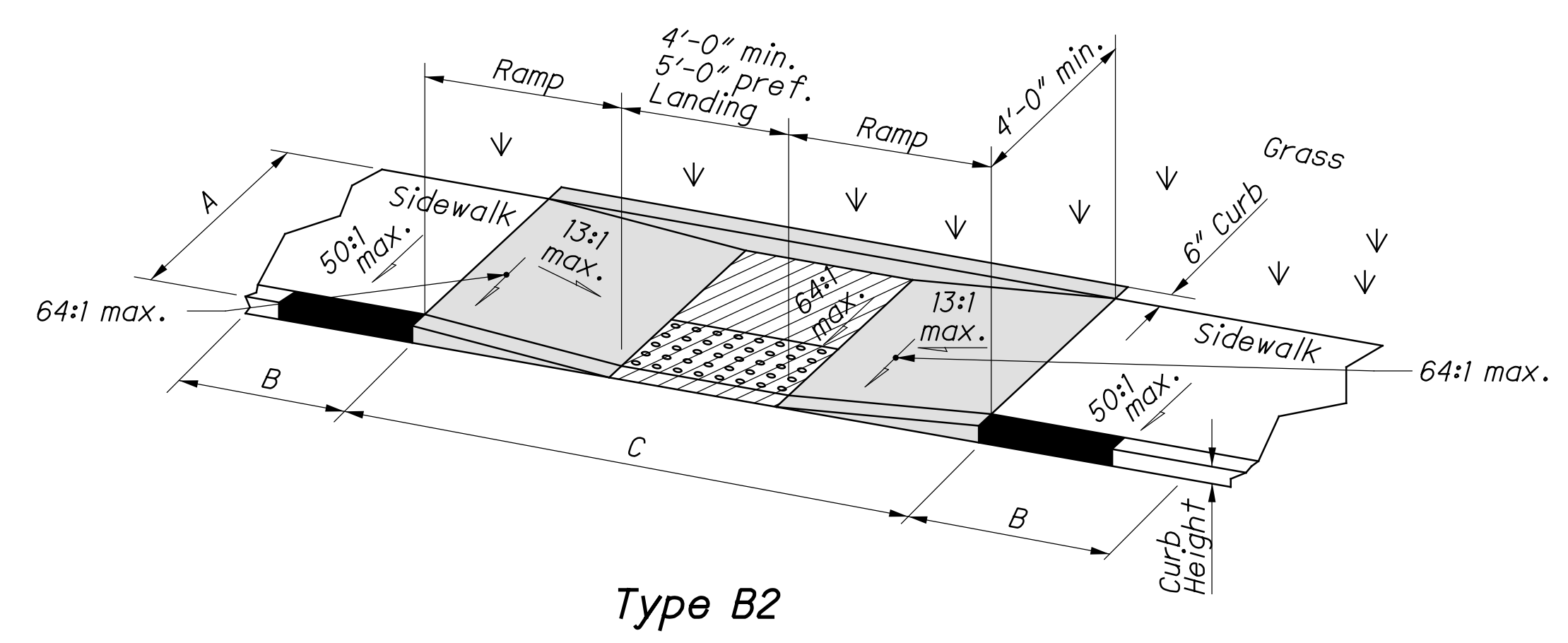
Type A2 - 3

Type A2 - 5

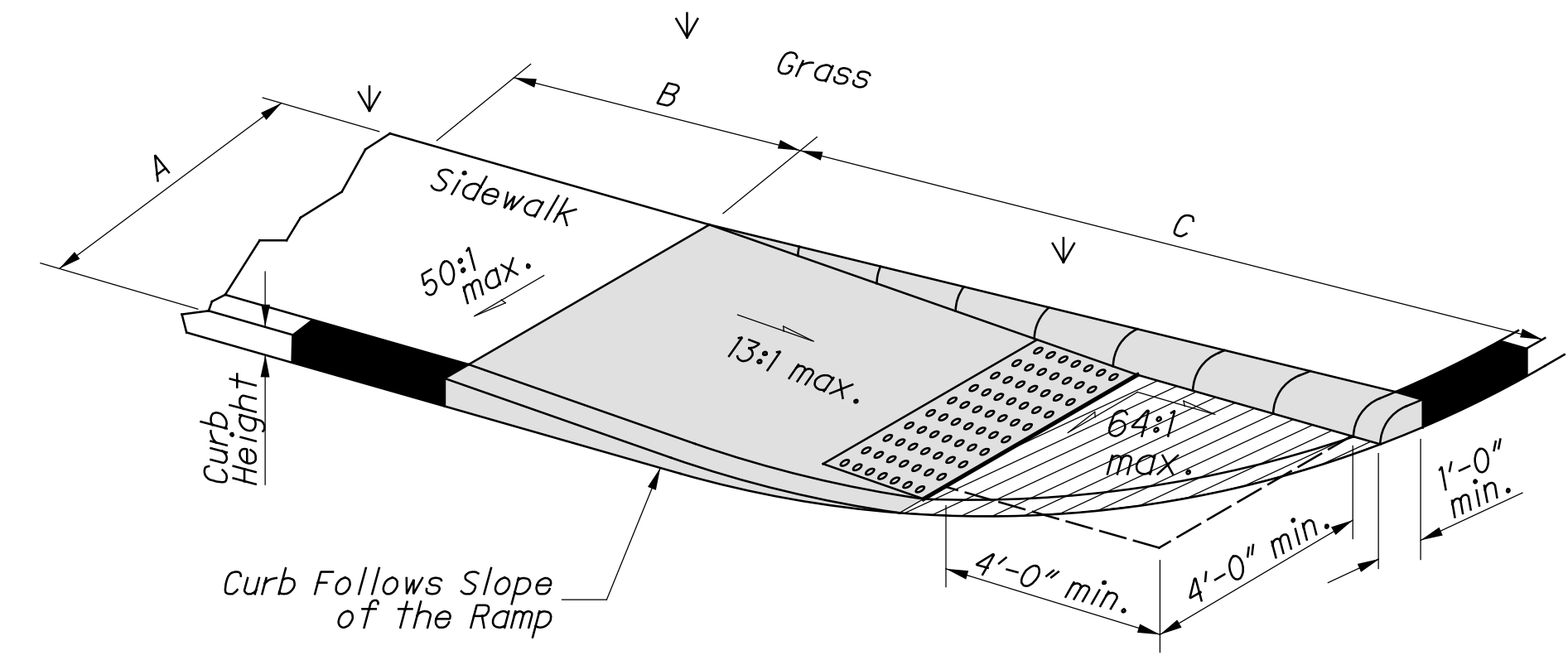
- LEGEND
- CONCRETE WALK
 - LANDING PAD
 - CURB RAMP
 - CURB

- NOTES
- * SEE SHEET -- FOR DIMENSIONS
 - * SEE SCD BP-7.1 FOR ALL OTHER DETAILS
 - * 10:1 FLARES ONLY REQUIRED WHERE CONCRETE WALK IS LOCATED BEHIND THE CURB

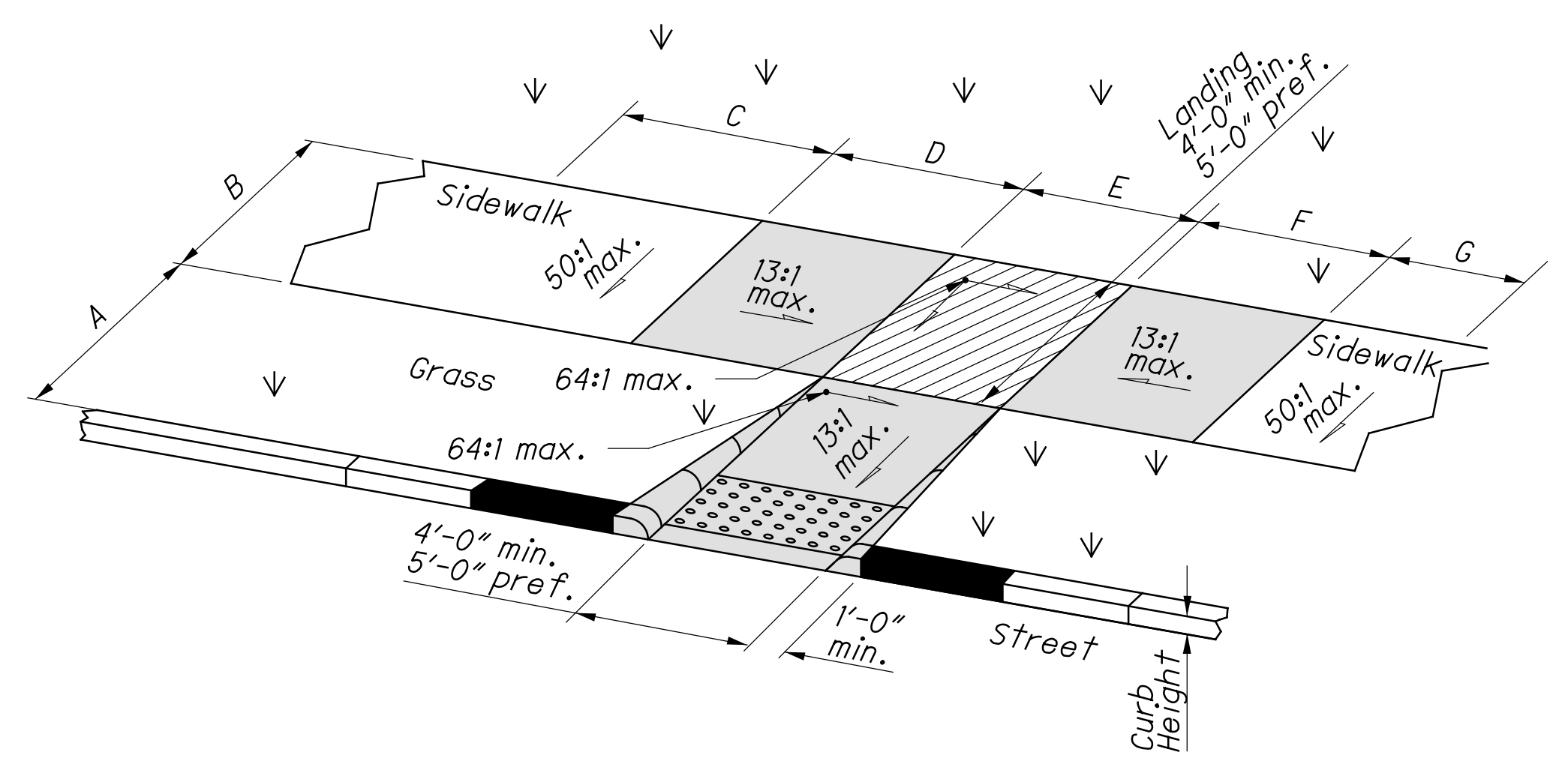
DESIGN AGENCY	
DESIGNER	CMS
REVIEWER	MJP 04/27/23
PROJECT ID	102732
SHEET	P.21
TOTAL	35



Type B2



Type B3



Type A2/C2

- LEGEND
- CONCRETE WALK
 - LANDING PAD
 - CURB RAMP
 - CURB

- NOTES
- * SEE SHEET -- FOR DIMENSIONS
 - * SEE SCD BP-7.1 FOR ALL OTHER DETAILS
 - * 10:1 FLARES ONLY REQUIRED WHERE CONCRETE WALK IS LOCATED BEHIND THE CURB

DESIGN AGENCY



DESIGNER

CMS

REVIEWER

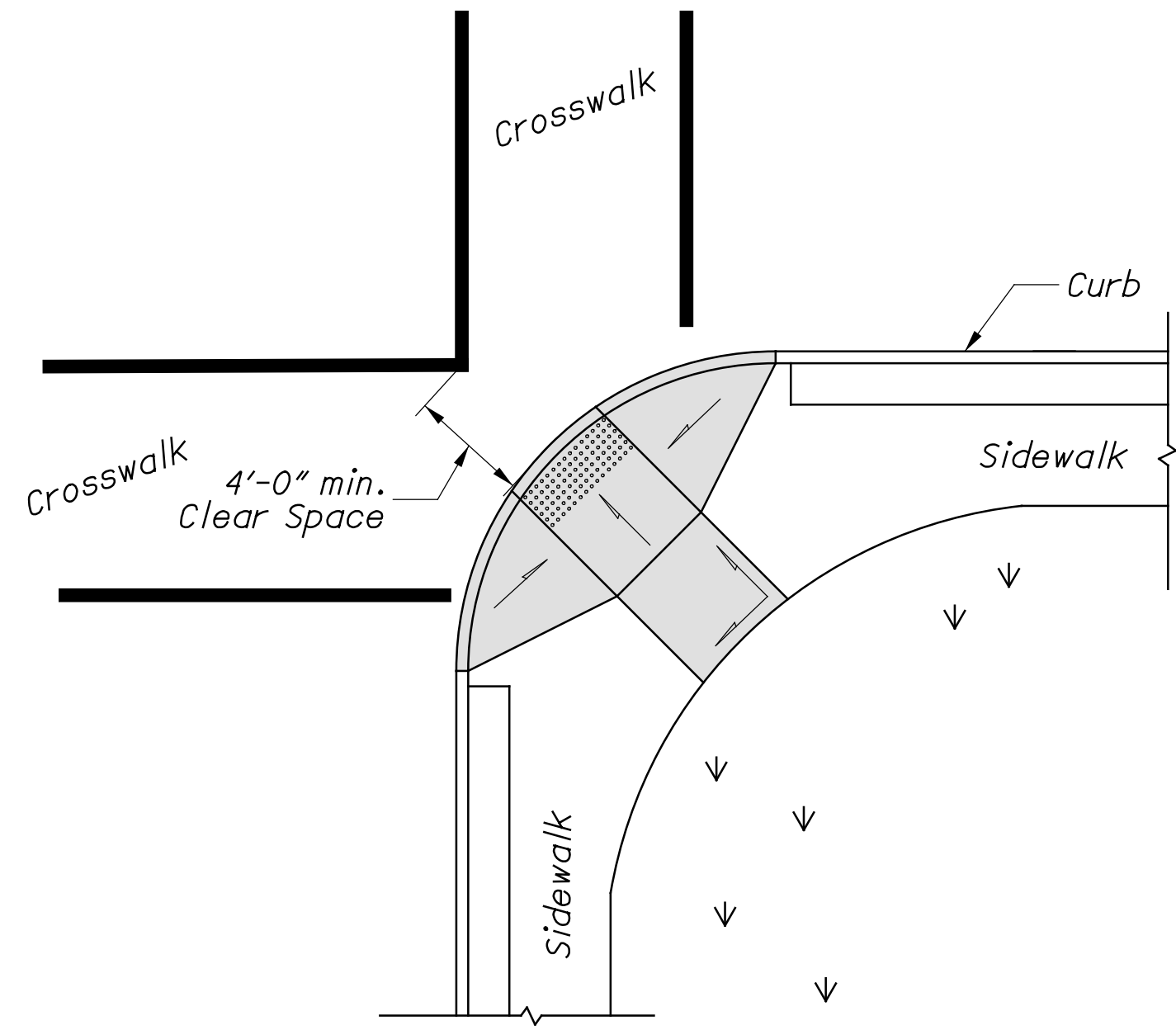
MJP 04/27/23

PROJECT ID

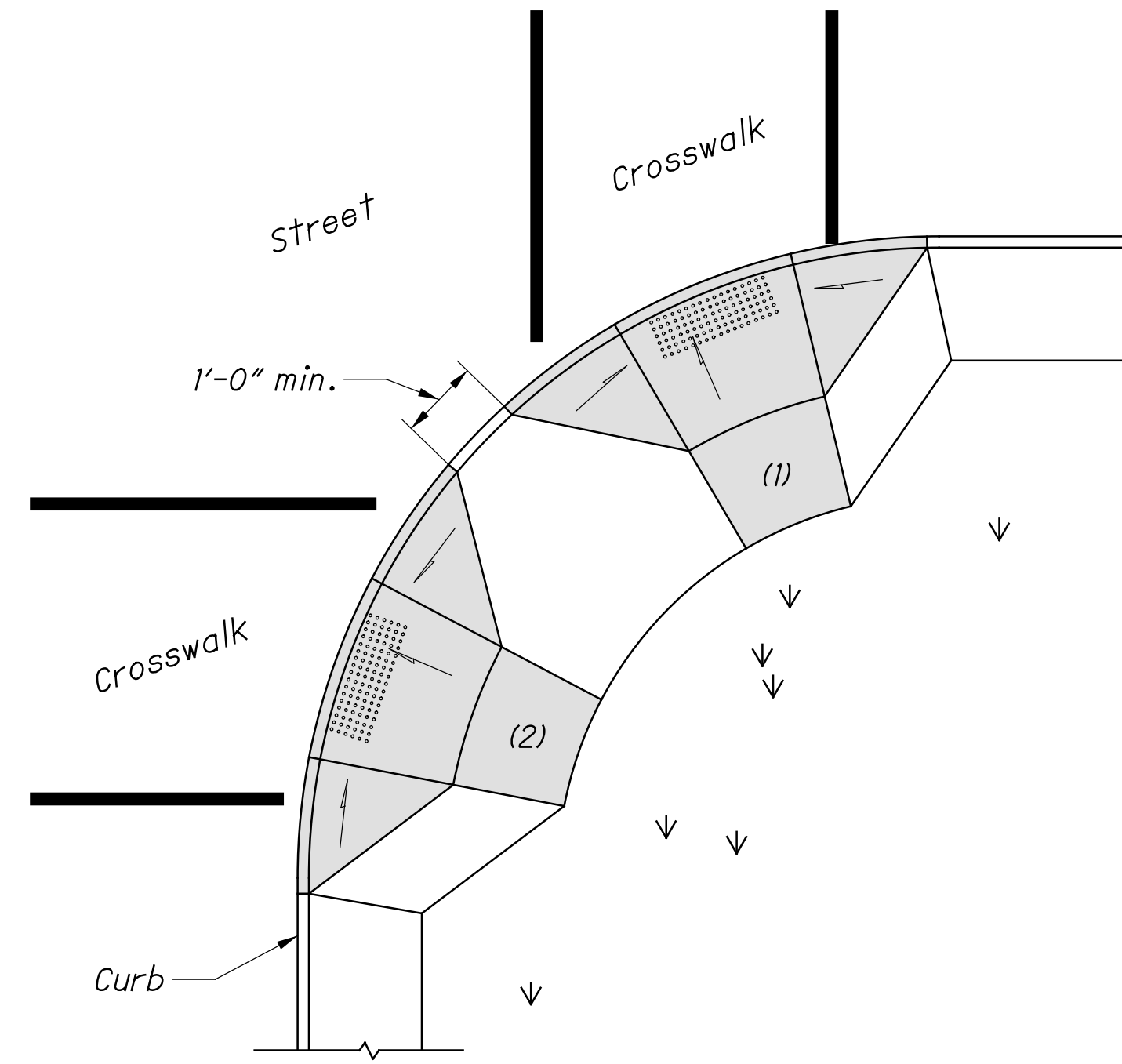
102732

SHEET TOTAL

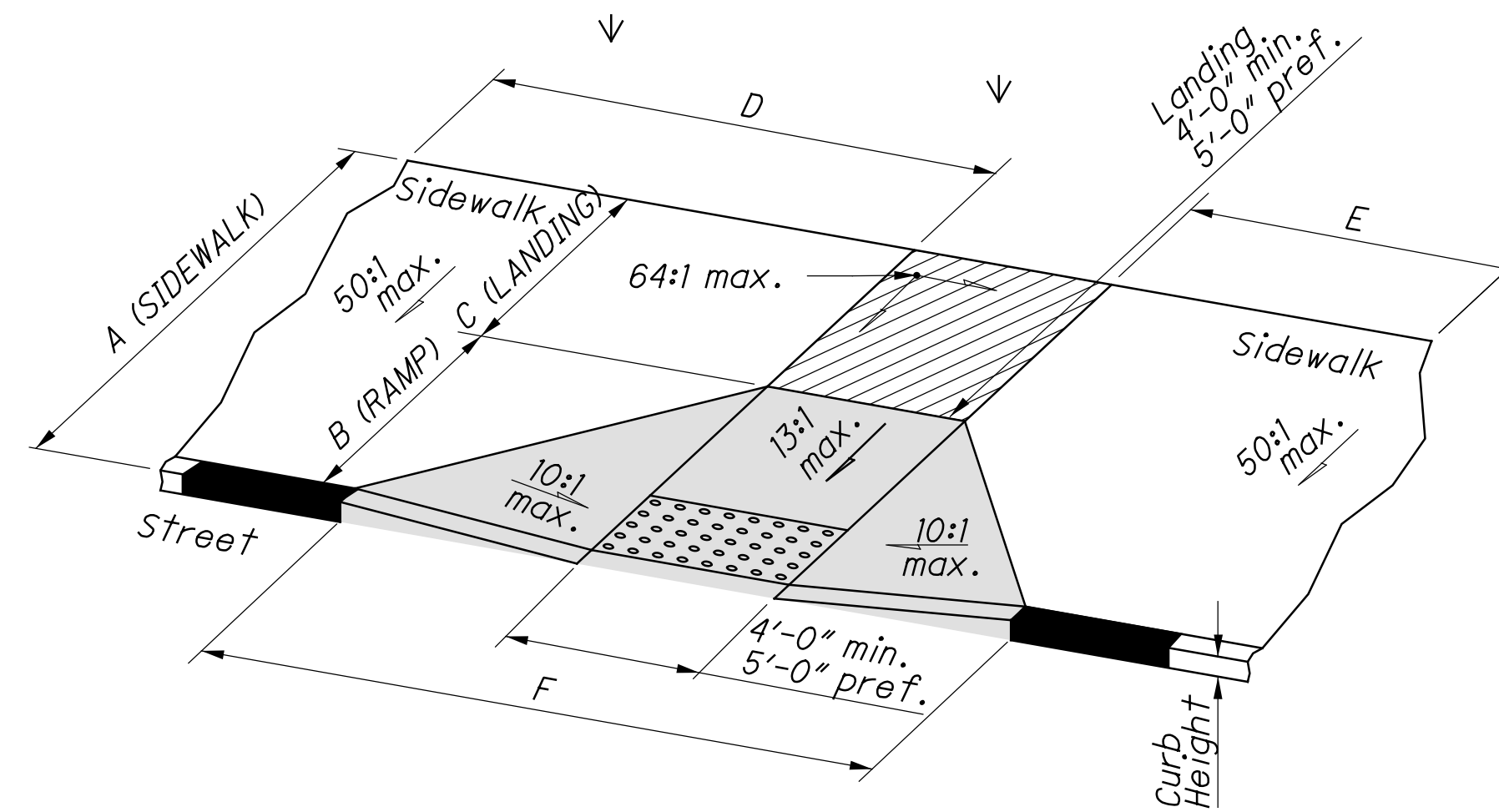
P.22 35



Type D - DIAGONAL CURB RAMP (TYPE A1)



Type D - PERPENDICULAR CURB RAMPS (TYPE A1)



Type A1

- LEGEND
- CONCRETE WALK
 - LANDING PAD
 - CURB RAMP
 - CURB

- NOTES
- * SEE SHEET -- FOR DIMENSIONS
 - * SEE SCD BP-7.1 FOR ALL OTHER DETAILS
 - * 10:1 FLARES ONLY REQUIRED WHERE CONCRETE WALK IS LOCATED BEHIND THE CURB

POR-59-(6.05)(6.80)

MODEL: Sheet 7 PAPER SIZE: 34x22 (in.) DATE: 2023-08-14 TIME: 11:26:00 USER: cslumper
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CURB RAMP SUBSUMMARY

DESIGN AGENCY



DESIGNER

CMS

REVIEWER

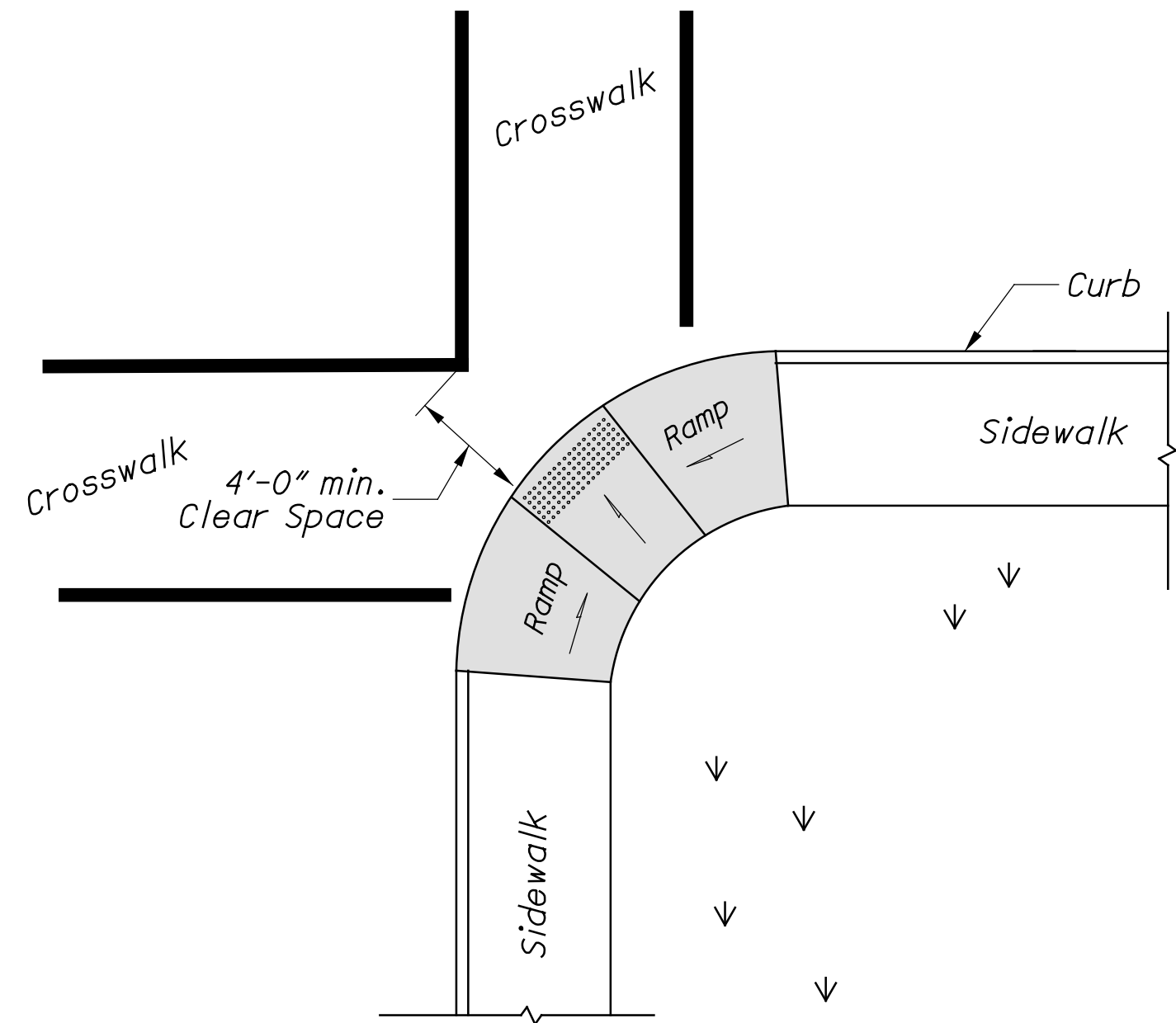
MJP 04/27/23

PROJECT ID

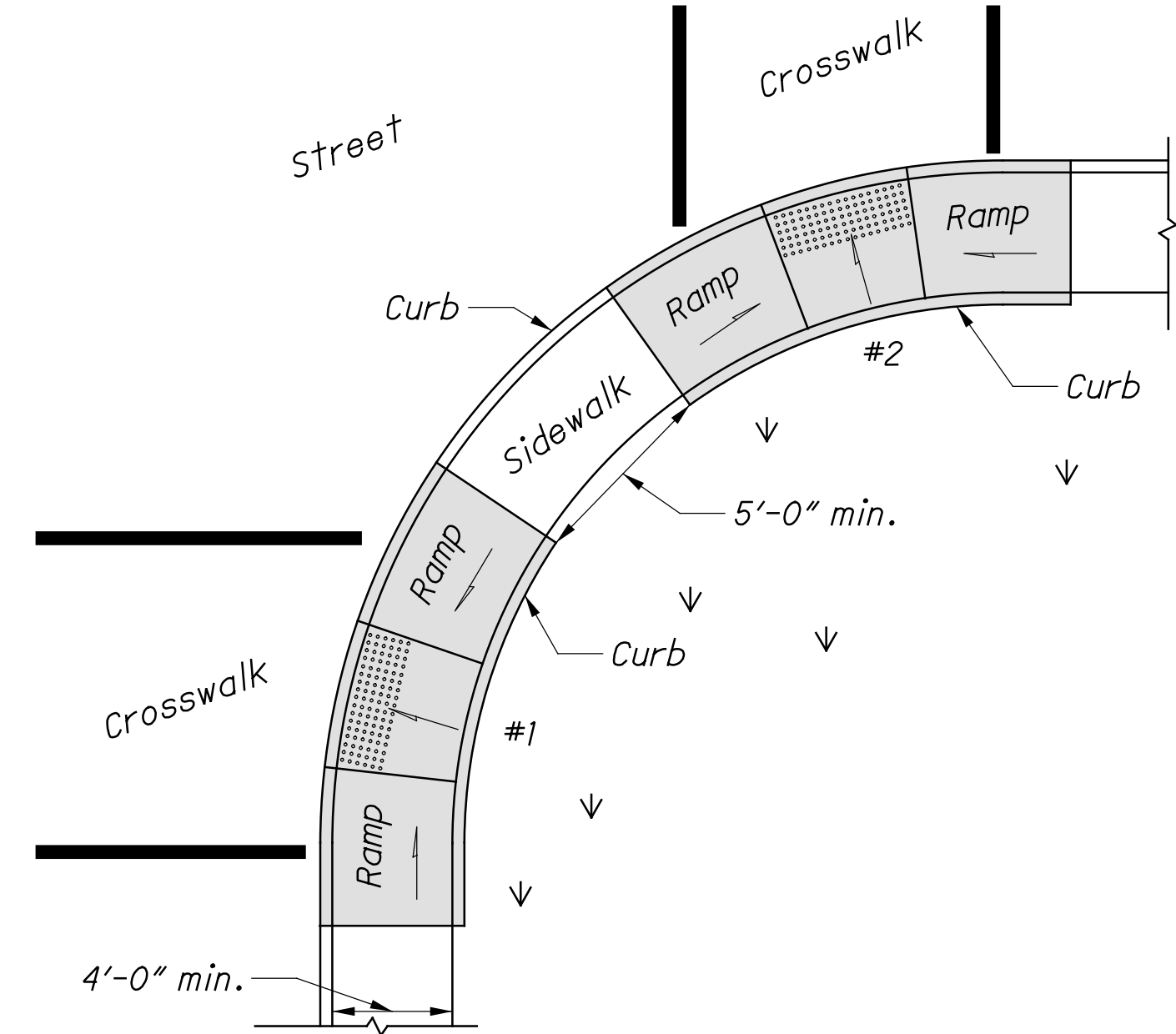
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SHEET TOTAL

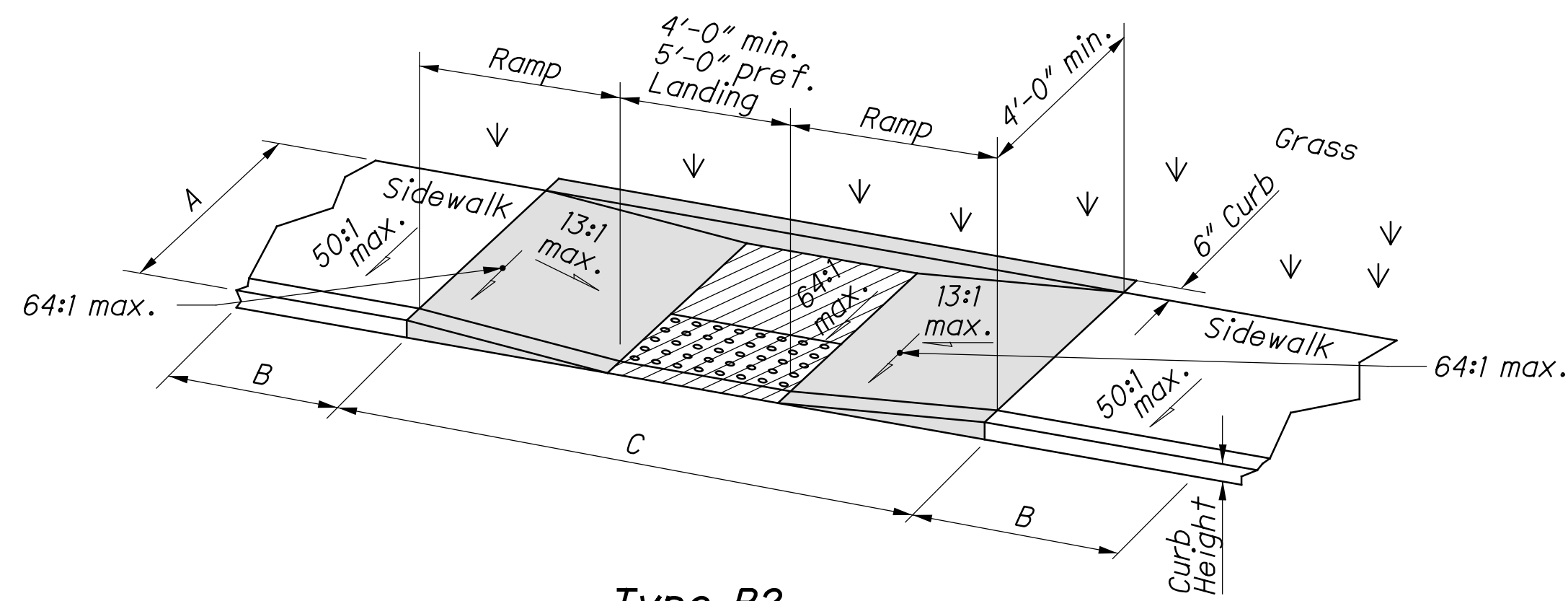
P.23 35



Type D - PARALLEL CURB RAMP (TYPE B2)



TYPE D - PARALLEL CURB RAMPS (TYPE B2)



Type B2

- LEGEND
- CONCRETE WALK
 - LANDING PAD
 - CURB RAMP
 - CURB

- NOTES
- * SEE SHEET -- FOR DIMENSIONS
 - * SEE SCD BP-7.1 FOR ALL OTHER DETAILS
 - * 10:1 FLARES ONLY REQUIRED WHERE CONCRETE WALK IS LOCATED BEHIND THE CURB

POR-59-(6.05)(6.80)

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CURB RAMP SUBSUMMARY

DESIGN AGENCY

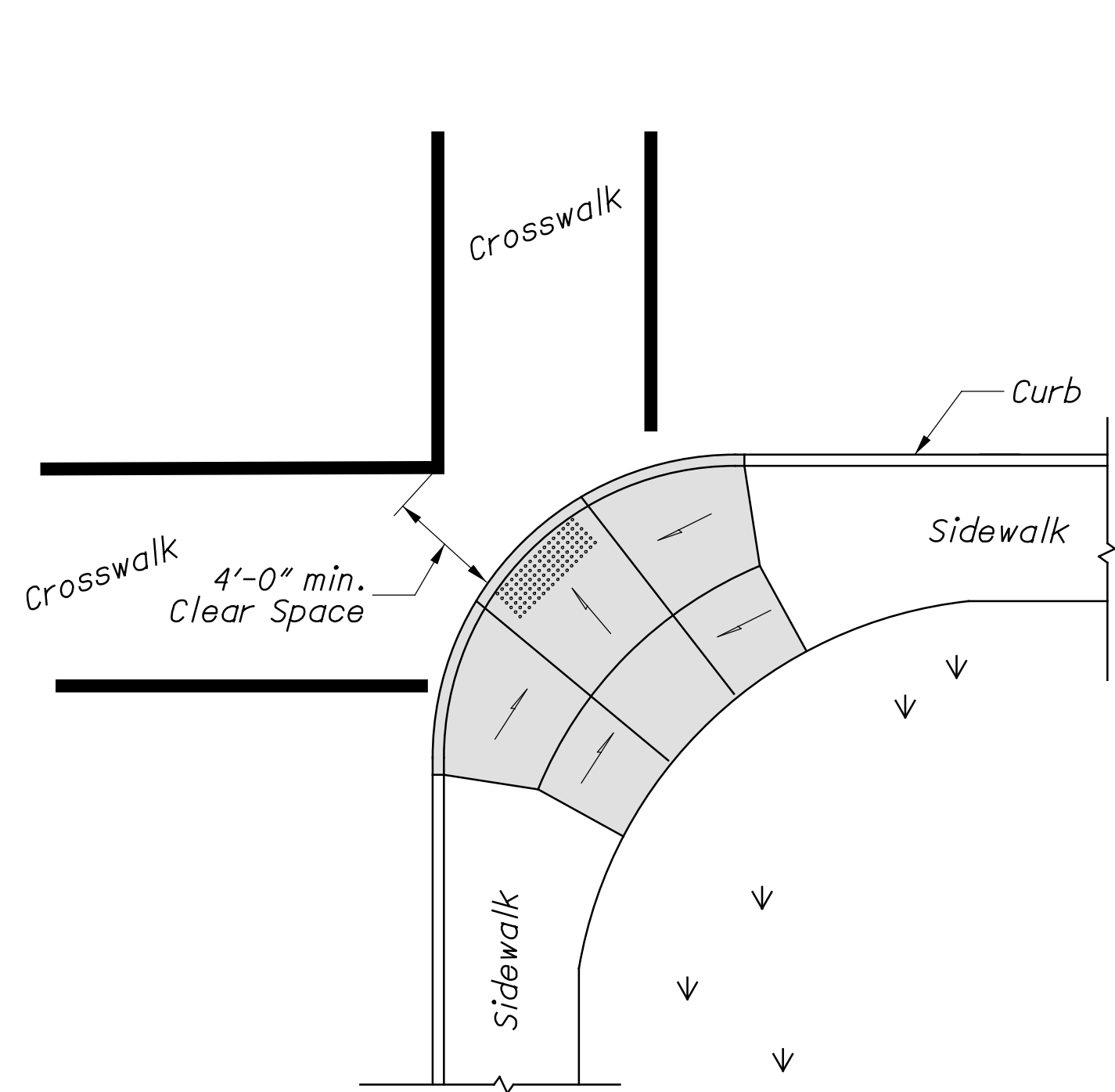


DESIGNER
CMS

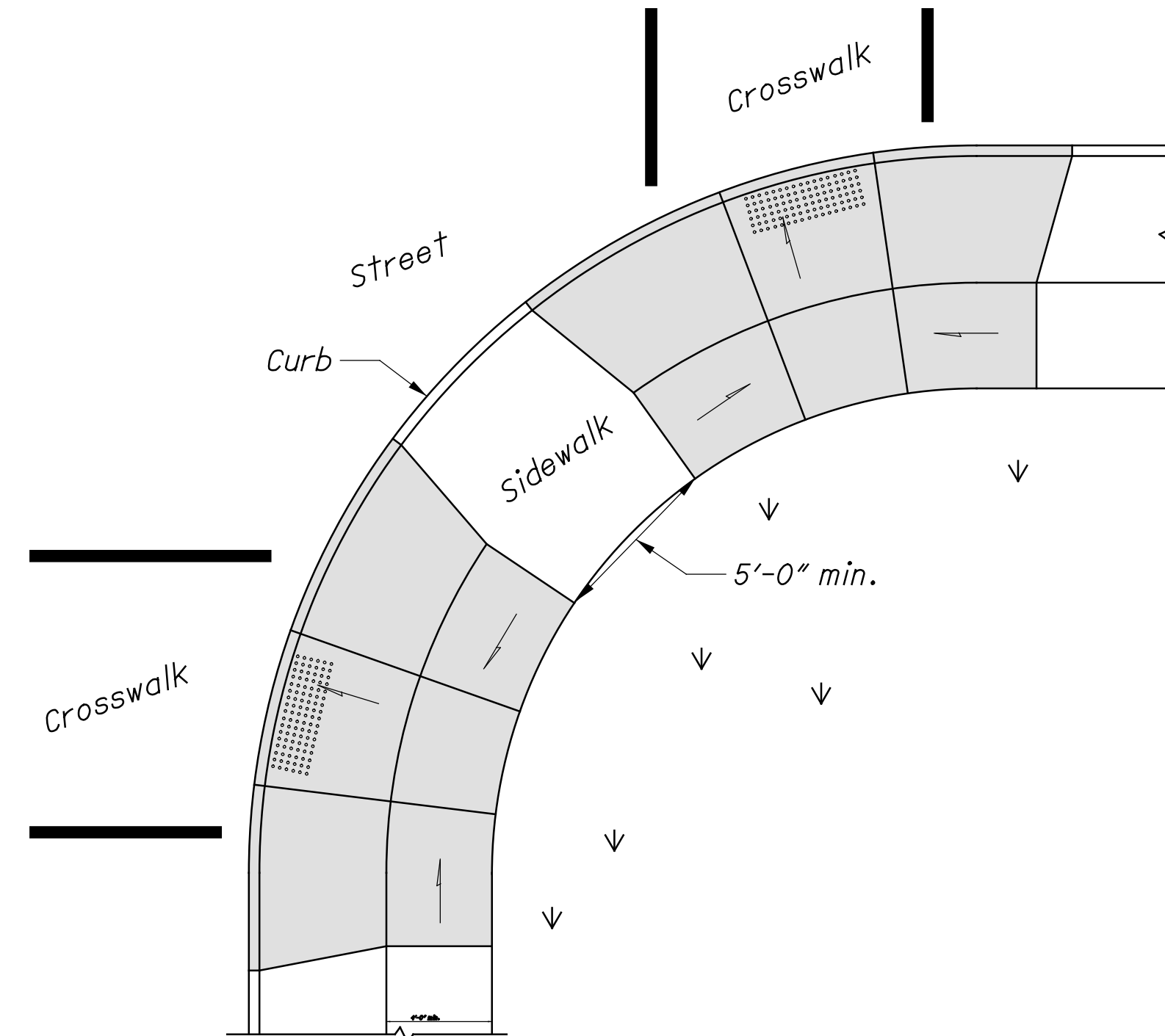
REVIEWER
MJP 04/27/23

PROJECT ID
102732

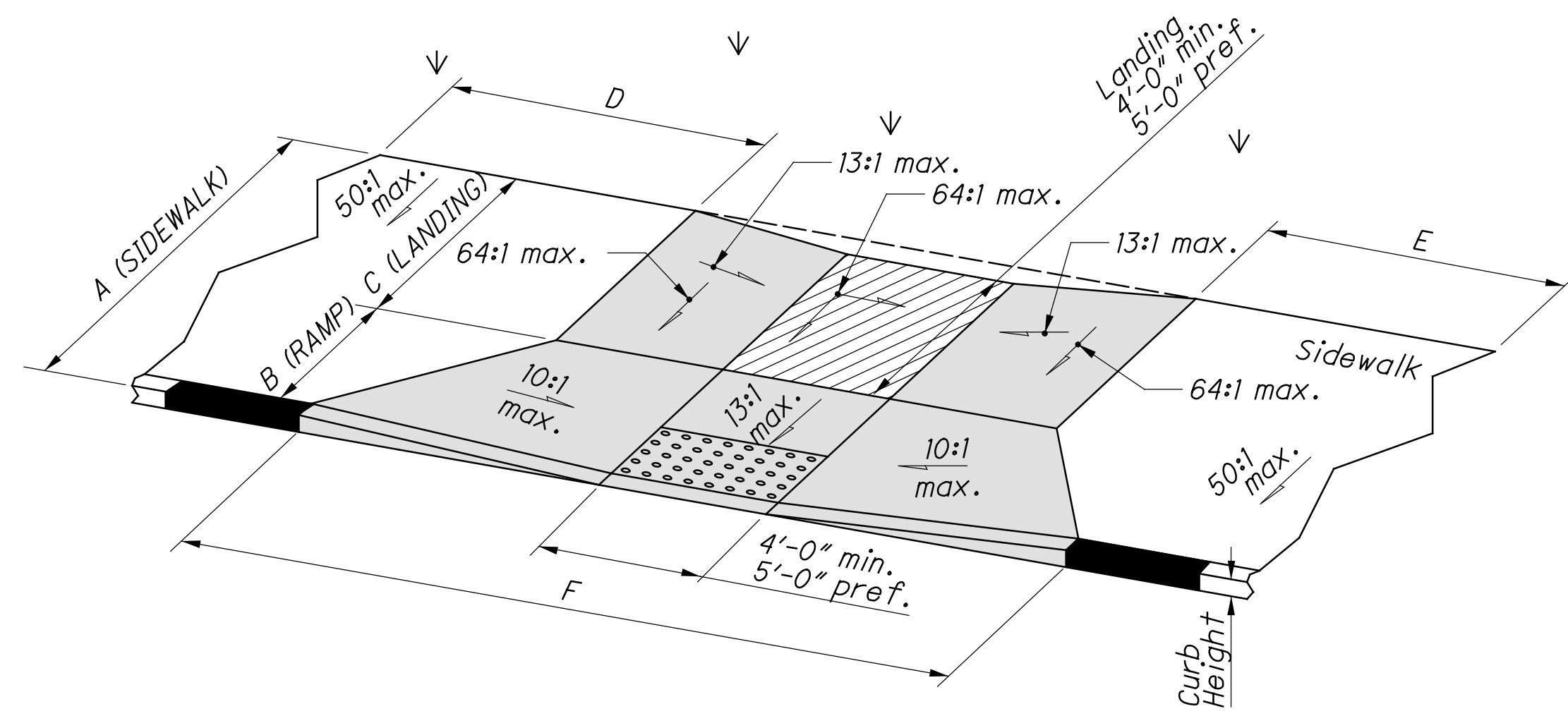
SHEET TOTAL
P.24 35



Type D - COMBINATION CURB RAMP (TYPE C1)



TYPE D - COMBINATION CURB RAMPS (TYPE C1)



Type C1

- LEGEND
- CONCRETE WALK
 - LANDING PAD
 - CURB RAMP
 - CURB

- NOTES
- * SEE SHEET -- FOR DIMENSIONS
 - * SEE SCD BP-7.1 FOR ALL OTHER DETAILS
 - * 10:1 FLARES ONLY REQUIRED WHERE CONCRETE WALK IS LOCATED BEHIND THE CURB

POR-59-(6.05)(6.80)

MODEL: Sheet 9 PAPER SIZE: 34x22 (in.) DATE: 2023-08-14 TIME: 11:26:01 USER: cslumper
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CURB RAMP SUBSUMMARY

DESIGN AGENCY



DESIGNER
CMS

REVIEWER
MJP 04/27/23

PROJECT ID
102732

SHEET TOTAL
P.25 35

POR-59-(6.05)(6.80)

MODEL: Sheet 1 PAPER SIZE: 34x22 (in.) DATE: 2023-08-14 TIME: 11:26:11 USER: cstumper
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EDGE LINE											GENERAL SPEC:	640
											MATERIAL TYPE:	646
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	WHITE EDGE LINE			YELLOW EDGE LINE			COMMENTS
						TOTAL	HIGHWAY	RAMP	TOTAL	HIGHWAY	RAMP	
TOTAL						0			0			

LANE LINE										COMMENTS	
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	6" LANE LINE				
							DASHED	SOLID			
POR	SR 59	6.05	W. BRIDGE LIMIT (POR-59-0605)	6.13	E. BRIDGE LIMIT (POR-59-0605)	0.16	0.16				
POR	SR 59	6.80	MORGAN RD.	7.67	UNNAMED PATH	1.74	1.74				
POR	SR 59	7.67	UNNAMED PATH	7.70	0.03 MILE W. OF SYCAMORE ST.	0.03	0.03				
POR	SR 59	8.65	0.04 MILE E. OF LINDEN ST.	9.05	E. CORP. LIMIT RAVENNA	0.80	0.80				
TOTAL						2.73	2.73				

CENTER LINE									COMMENTS	
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	EQUIVALENT SOLID LINE			
POR	SR 59	6.05	W. BRIDGE LIMIT (POR-59-0605)	6.13	E. BRIDGE LIMIT (POR-59-0605)	0.08	0.16			
POR	SR 59	6.80	MORGAN RD.	9.05	E. CORP. LIMIT RAVENNA	2.25	4.28			
TOTAL						2.33	4.44			

AUXILIARY																						
CTY	ROUTE LOCATION	TRUE LOG	CHANNEL LINE, 12"	STOP LINE	CROSS WALK LINE, 24"	TRANSVERSE DIAGONAL LINES		ISLAND MARKING	SYMBOL MARKINGS			PARKING STALL MARKING	HANDICAP MARKING	LANE ARROWS					REDUCT. ARROW	COMMENTS		
						WHITE	YELLOW		R x R	SCHOOL				TURN LEFT	TURN RIGHT	THRU	COMB.	TWO WAY LEFT				
										FT	FT										72"	96"
POR	SR 59 @ BRIDGE POR-59-0605	6.05																		2	WESTBOUND LANES	
POR	SR 59 @ S. SCRANTON ST.	6.84		43	155																	
POR	SR 59 @ N. SCRANTON ST.	6.86		21	50																	
POR	SR 59 @ SLM 6.91	6.91																				
POR	SR 59 @ ZETA ST.	6.94			166																	
POR	SR 59 @ HILLCREST ST.	6.96			70																	
POR	SR 59 @ OAKGROVE ST.	7.00			56																	
POR	SR 59 @ BEECH ST.	7.11			72																	
POR	SR 59 @ OAKWOOD ST.	7.12		44	146																	
POR	SR 59 @ MADISON ST.	7.21		20	56																	
POR	SR 59 @ JEFFERSON ST.	7.29			56																	
POR	SR 59 @ SLM 7.31	7.31																			1	EASTBOUND LANES
POR	SR 59 @ DIAMOND ST.	7.36		44	292																	
POR	SR 59 @ AVON CT.	7.47		20	24																	
POR	SR 59 @ KING ST.	7.48		42	146																	
POR	SR 59 @ GARDEN CT.	7.50			40																	
POR	SR 59 @ SLM 7.55	7.55																			1	WESTBOUND LANES
POR	SR 59 @ VINE ST.	7.59			72																	
POR	SR 59 @ GRANT ST.	7.62			56																	
POR	SR 59 @ SYCAMORE ST.	7.73	218	67	440															4	2	
TOTAL			218	301	1897															4	2	2

PAVEMENT MARKING SUBSUMMARY

DESIGN AGENCY



DESIGNER
CMS

REVIEWER
MJP 04/27/23

PROJECT ID
102732

SHEET TOTAL
P.26 35


POR-59-(6.05)(6.80)

MODEL: Sheet 2 PAPER SIZE: 34x22 (in.) DATE: 2023-08-14 TIME: 11:26:12 USER: cslumper
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AUXILIARY

CTY	ROUTE LOCATION	TRUE LOG	CHANNEL LINE, 12" FT	STOP LINE FT	CROSS WALK LINE, 24" FT	TRANSVERSE DIAGONAL LINES		ISLAND MARKING SQ.FT	SYMBOL MARKINGS			PARKING STALL MARKING FT	HANDICAP MARKING EACH	LANE ARROWS			REDUCT. ARROW EACH	COMMENTS		
						WHITE FT	YELLOW FT		RxR EACH	SCHOOL				TURN LEFT EACH	TURN RIGHT EACH	THRU EACH			COMB. EACH	TWO WAY LEFT EACH
										72" EACH	96" EACH									
POR	SR 59 @ CHERRY WAY	7.78				260	217				601	3								
POR	SR 59 @ MERIDIAN ST.	7.82	160	68	520															
POR	SR 59 @ PARK WAY	7.86	36		240	318		64			640	4	1							
POR	SR 59 @ CHESTNUT ST.	7.91	307	83	500								4	2						
POR	SR 59 @ HICKORY WAY	7.95			170	135		131			820	4								
POR	SR 59 @ PROSPECT ST.	7.99	301	72	520								4	2						
POR	SR 59 @ N. PLUM WAY	8.03	65			50		74			560	1	1							
POR	SR 59 @ S. WALNUT ST.	8.08	92		160									2						
POR	SR 59 @ ELM ST.	8.14	96	49	162									1						
POR	SR 59 @ PRATT ST.	8.16	52	23	72									3						
POR	SR 59 @ CLINTON ST.	8.20	140		146															
POR	SR 59 @ MYRTLE ST.	8.23			40															
POR	SR 59 @ LAWRENCE ST.	8.33			56															
POR	SR 59 @ S. FREEDOM ST. / SR 88	8.41	238	43	326									4						
POR	SR 59 @ LIBERTY ST.	8.55	102		56		41	48						2						
POR	SR 59 @ LINDEN ST.	8.61	380		56									2	5					
POR	SR 59 @ GROVE ST.	8.74		62	280									2						
POR	SR 59 @ SANFORD ST.	8.76		31	56															
POR	SR 59 @ ALBERT TSAI BLVD.	8.81															1			
POR	SR 59 @ STEVENS ST.	8.85			56															
POR	SR 59 @ NEW MILFORD RD.	8.91		62	186									4						
POR	SR 59 @ SLM 8.97	8.97															1			
POR	SR 59 @ BRYN MAWR ST.	9.01			56															
POR	SR 59 @ SLM 9.03	9.03															1			
TOTAL			1969	493	3658	763	258	317			2621	12	30	11			3			

PAVEMENT MARKING SUBSUMMARY

DESIGN AGENCY

 DESIGNER: CMS
 REVIEWER: MJP 04/27/23
 PROJECT ID: 102732
 SHEET: P.27 TOTAL: 35

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

REFER TO THE FOLLOWING STANDARD BRIDGE DRAWINGS:

- AS-1-15 REVISED 1/20/23
- EXJ-4-87 REVISED 1/20/23
- VPF-1-90 REVISED 1/20/23

DESIGN SPECIFICATIONS

THIS STRUCTURE CONFORMS TO THE 9TH EDITION OF THE "LRFD BRIDGE DESIGN SPECIFICATIONS" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS AND THE ODOT BRIDGE DESIGN MANUAL, 2020.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS, SECTIONS 102.05, 105.02, AND 513.04. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

PROPOSED WORK

- POR-59-0605 (OVER NORFOLK SOUTHERN RR)
 - SEAL EXISTING WEARING SURFACE AND APPROACH SLABS WITH GRAVITY-FED RESIN
 - REMOVE AND REPLACE EXISTING ELASTOMERIC STRIP SEALS
 - PATCH UNSOUND AREAS OF CONCRETE SUBSTRUCTURE AND SEAL WITH EPOXY-URETHANE
 - PERFORM BACKWALL REPAIR ON FORWARD AND REAR ABUTMENTS
 - SEAL ALL CONCRETE SURFACES WITH EPOXY-URETHANE, INCLUDING PARAPETS, PIERS, WINGWALLS, AND BACKWALLS
 - REMOVE AND REPLACE EXISTING VANDAL PROTECTION FENCE
 - PERFORM CLEARING AND GRUBBING OF ALL VEGETATION WITHIN THE RIGHT OF WAY LIMITS AROUND THE STRUCTURE
 - PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

STRIP SEALS

MINIMUM JOINT OPENING AT TIME OF SEAL GLAND INSTALLATION SHALL NOT BE LESS THAN 1-1/2". THE EXPANSION JOINT OPENINGS, AS PER SCD EXJ-4-87 ARE AS FOLLOWS:

TEMPERATURE (°F)	DIMENSION "A"
30	1-11/16"
40	1-5/8"
50	1-9/16"
60	1-1/2"
70	1-7/16"
80	1-3/8"
90	1-5/16"

ENDANGERED SPECIES HABITAT - INDIANA BAT/NORTHERN LONG-EARED BAT

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE AND WITH A MINIMUM HEIGHT OF 13 FEET.

ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS

ALTHOUGH NO TREES OR STUMPS ARE SPECIFICALLY MARKED FOR REMOVAL WITHIN THE PLANS, A LUMP SUM QUANTITY IS INCLUDED IN THE STRUCTURE GENERAL SUMMARY FOR ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS. SCALPING IS NOT REQUIRED FOR THIS ITEM OF WORK. ALL VEGETATION SHALL BE REMOVED TO THE R/W LIMITS OF THE HEADWALLS, ABUTMENTS, AND/OR PIERS.

ALL OTHER PROVISIONS AS SET FORTH IN THE C&MS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS.

ITEM 512 - REMOVAL OF EXISTING PAVEMENT MARKINGS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE STRUCTURE ESTIMATED QUANTITIES FOR THE REMOVAL OF EXISTING PAVEMENT MARKINGS PRIOR TO CONCRETE DECK SEALING:

- ITEM 512, REMOVAL OF EXISTING PAVEMENT MARKING, 1350 FT
- ITEM 512, REMOVAL OF EXISTING PAVEMENT MARKING, 2 EACH

ITEM 519 - PATCHING CONCRETE STRUCTURES, AS PER PLAN

PRIOR TO THE SURFACE CLEANING SPECIFIED IN C&MS 519.04 AND WITHIN 24 HOURS OF PLACING PATCHING MATERIAL, BLAST CLEAN ALL SURFACES TO BE PATCHED INCLUDING THE EXPOSED REINFORCING STEEL. ACCEPTABLE METHODS INCLUDE HIGH-PRESSURE WATER BLASTING WITH OR WITHOUT ABRASIVES IN THE WATER, ABRASIVE BLASTING WITH CONTAINMENT, OR VACUUM ABRASIVE BLASTING.

ITEM 607 - VANDAL PROTECTION FENCE, 12' CURVED, COATED FABRIC

THE VANDAL PROTECTION FENCE TO BE PLACED ON STRUCTURE POR-59-0605 WILL BE 12' CURVED SECTION FENCE, POST SECTION PS-1, WITH BASE PLATE BP-3 AS DETAILED IN STANDARD CONSTRUCTION DRAWING VPF-1-90.

PRIOR TO ORDERING MATERIALS, THE CONTRACTOR WILL LAY OUT THE POST LOCATIONS AND THE PROJECT ENGINEER WILL APPROVE THE POST SPACING TO BE USED. THE POST SPACING WILL BE AS PER STANDARD CONSTRUCTION DRAWING VPF-1-90.

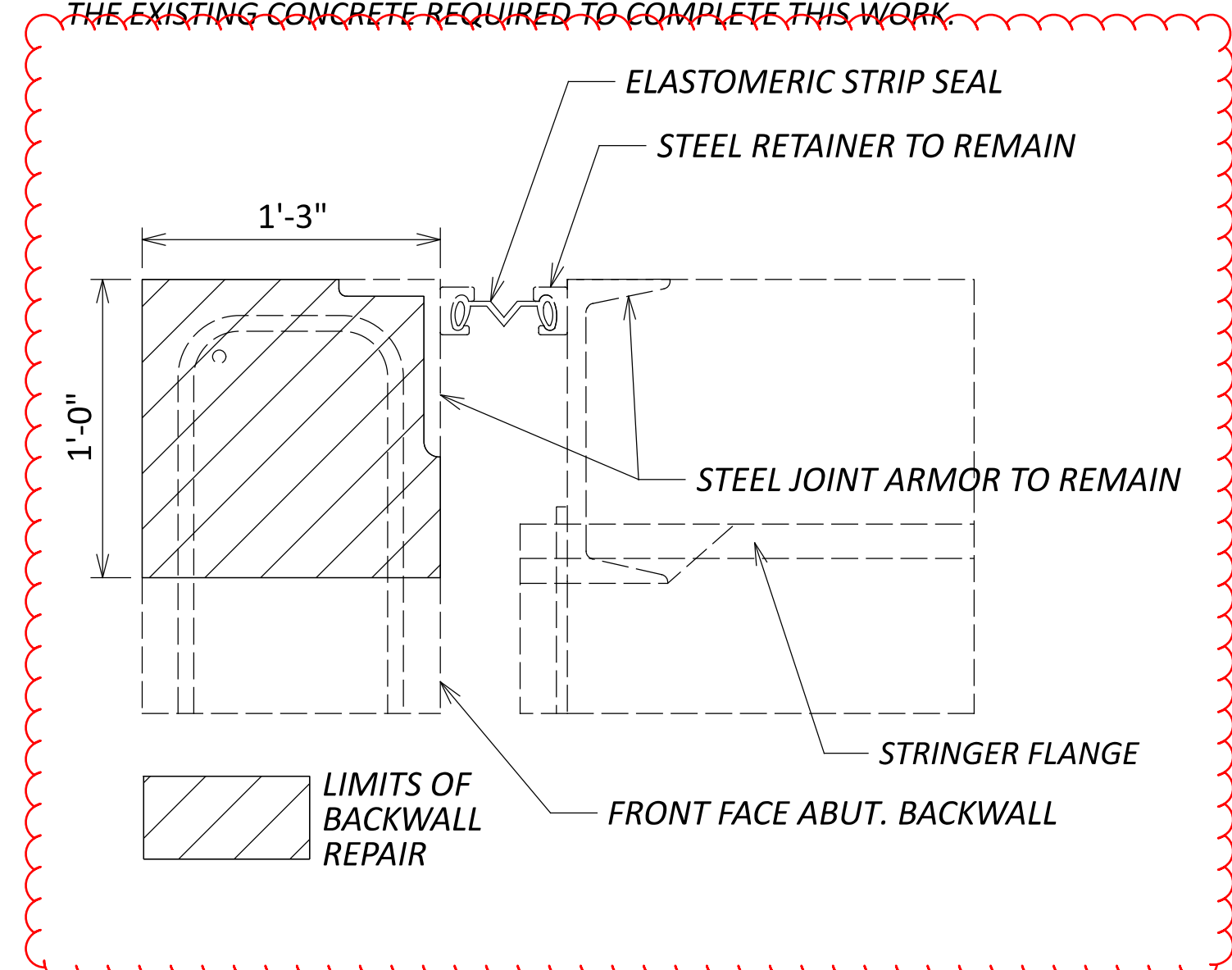
THE EXISTING VANDAL FENCE IS TO BE REMOVED UNDER ITEM 202, VANDAL PROTECTION FENCE REMOVED. ONCE THE EXISTING VANDAL FENCE HAS BEEN REMOVED, THE NEW VANDAL PROTECTION FENCE MUST BE IN PLACE WITHIN SEVEN (7) DAYS.

ITEM 511 - CONCRETE MISC.: BACKWALL REPAIR

THIS ITEM OF WORK CONSISTS OF THE REMOVAL OF ALL UNSOUND CONCRETE AT THE BACKWALLS OF STRUCTURE POR-59-0605 TO THE LIMITS SHOWN BELOW OR AS DIRECTED BY THE ENGINEER, THE PREPARATION OF THE SURFACE, FORMS, TEMPORARY SUPPORTS OF THE EXPANSION JOINT, AND PROVIDING AND PLACING OF CLASS QC MS CONCRETE, SUBSTRUCTURE.

TEMPORARY SUPPORTS OF THE EXPANSION JOINT WILL BE USED TO MAINTAIN THE PROPER ALIGNMENT AND GRADE OF THE JOINT DURING REMOVAL AND REPLACEMENT OF BACKWALL CONCRETE. THE COST OF THIS TEMPORARY SUPPORT WILL BE INCIDENTAL TO THIS ITEM.

PAYMENT WILL BE MADE AT THE CONTRACT PRICE PER CUBIC YARD FOR ITEM 511, CONCRETE MISC.: BACKWALL REPAIR, WHICH WILL INCLUDE ALL MATERIALS AND LABOR INCLUDING REMOVAL AND DISPOSAL OF THE EXISTING CONCRETE REQUIRED TO COMPLETE THIS WORK.



STRUCTURE GENERAL NOTES
 POR-59-0605
 OVER NORFOLK SOUTHERN RR

SFN
 6701841
 DESIGN AGENCY

DESIGNER: CMS
 CHECKER: MJP
 REVIEWER: MJP
 PROJECT ID: 102732


SUBSET	TOTAL
1	8
SHEET	TOTAL
P.28	35

ESTIMATED QUANTITIES (03/NHS/13)

ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUT.	PIERS	SUPER.	GEN.	SEE SHEET
201	11001	LS		CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS				LS	1 / 8
202	75260	760	FT	VANDAL PROTECTION FENCE REMOVED				760	
509	20000	1808	LB	REINFORCING STEEL, REPLACEMENT OF EXISTING REINFORCING STEEL				1808	
511	71100	11	CY	CONCRETE, MISC.: BACKWALL REPAIR				11	
512	10100	2648	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)				2648	
512	73500	2618	SY	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN				2618	
512	74000	2648	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES				2648	
512	74500	1350	FT	REMOVAL OF EXISTING PAVEMENT MARKING				1350	
512	74520	2	EACH	REMOVAL OF EXISTING PAVEMENT MARKING				2	
516	01300	289	FT	ELASTOMERIC STRIP SEAL WITHOUT STEEL EXTRUSIONS				289	
519	11101	100	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN				100	1 / 8
607	39930	760	FT	VANDAL PROTECTION FENCE, 12' CURVED, COATED FABRIC				760	

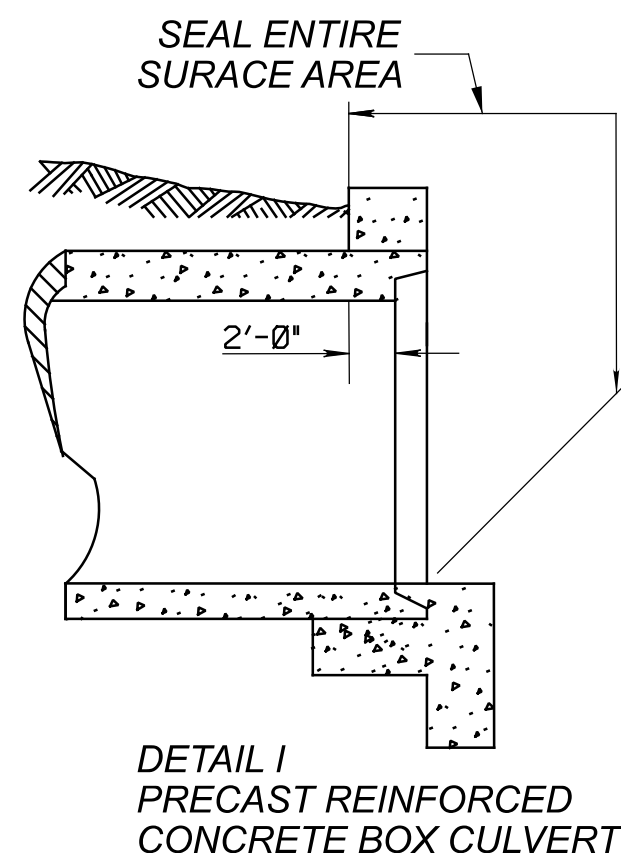
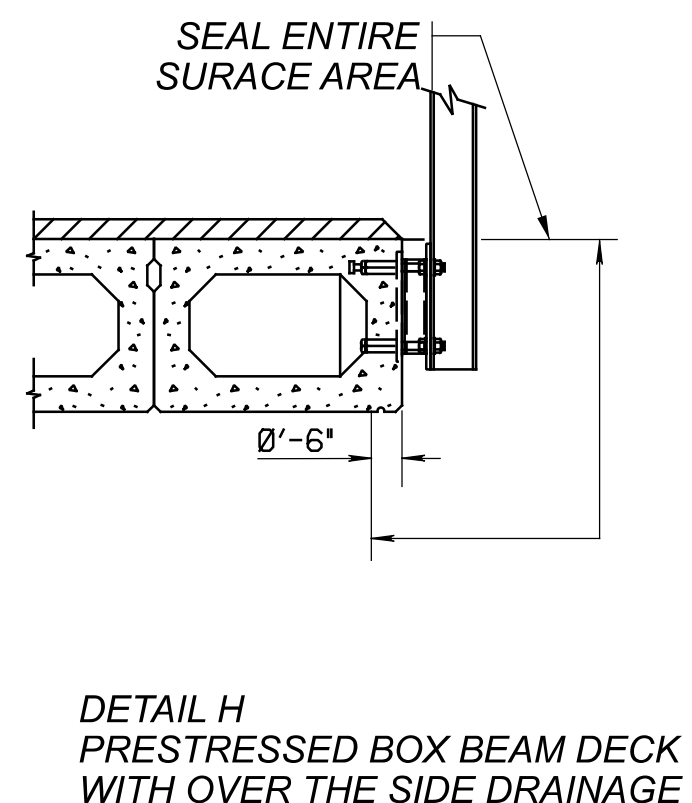
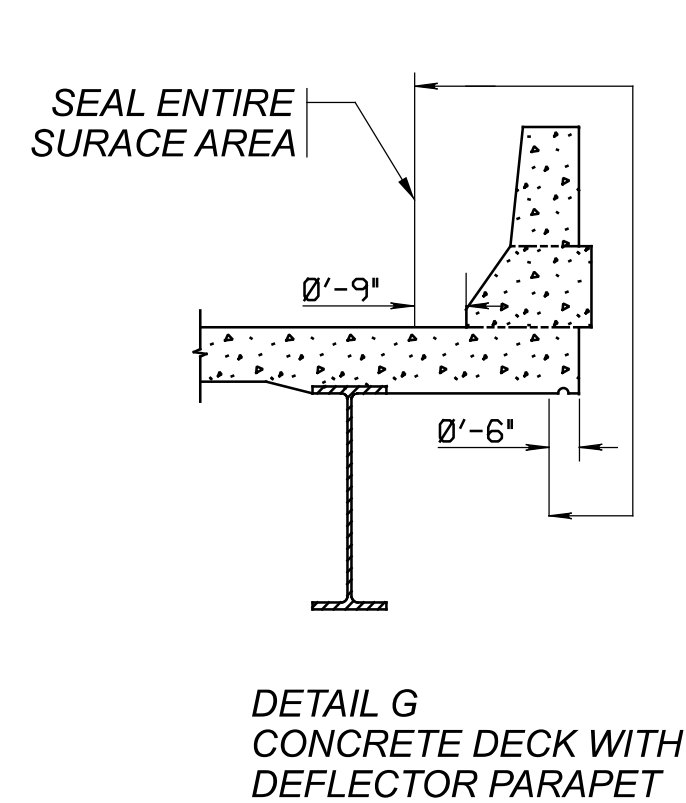
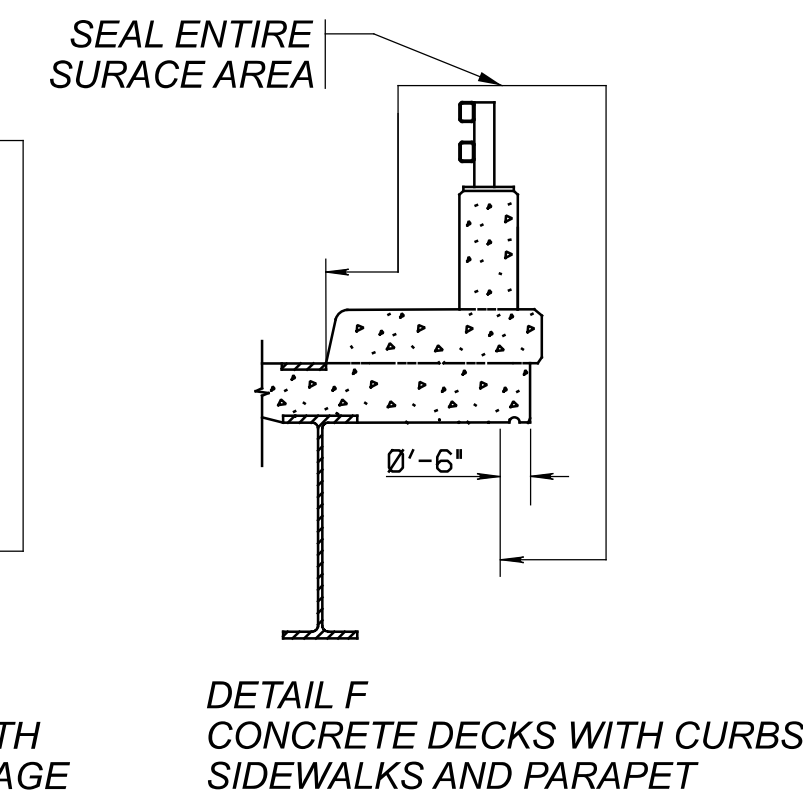
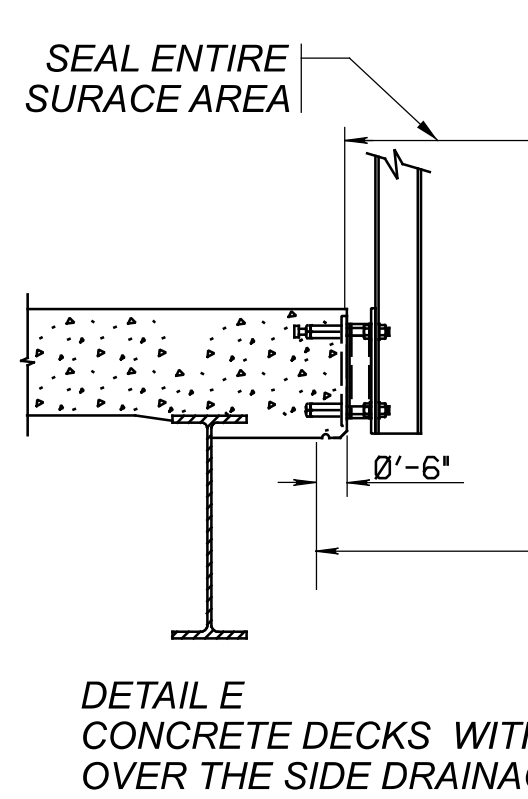
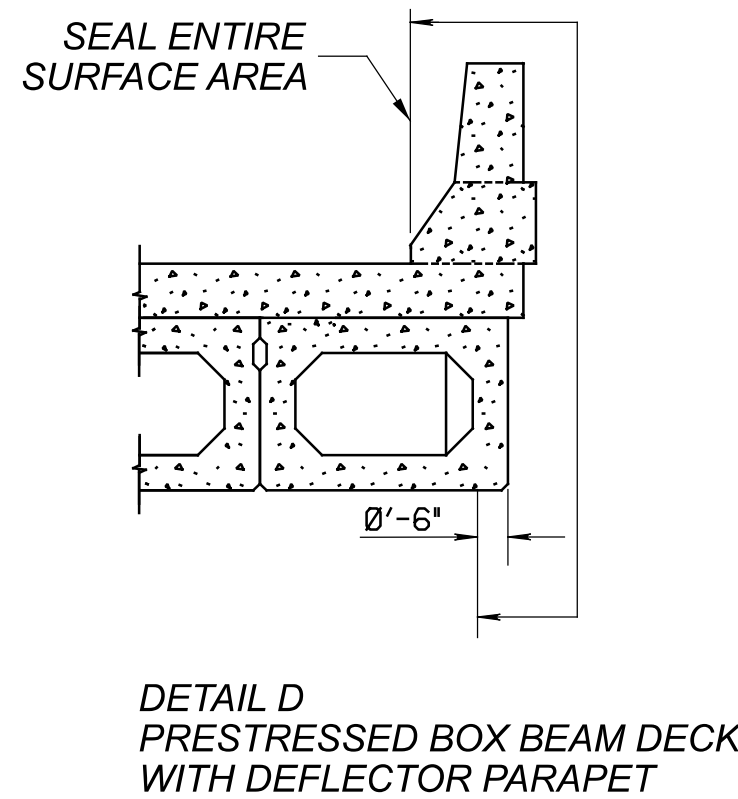
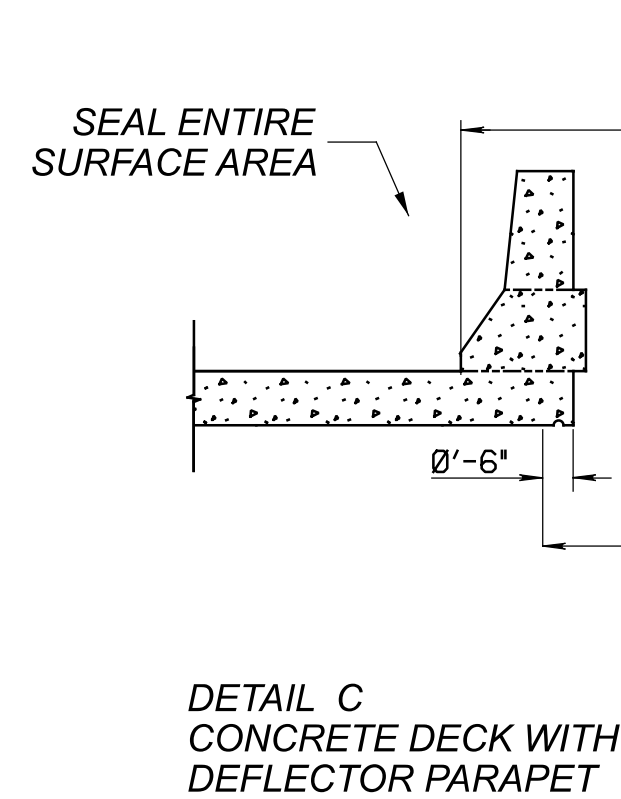
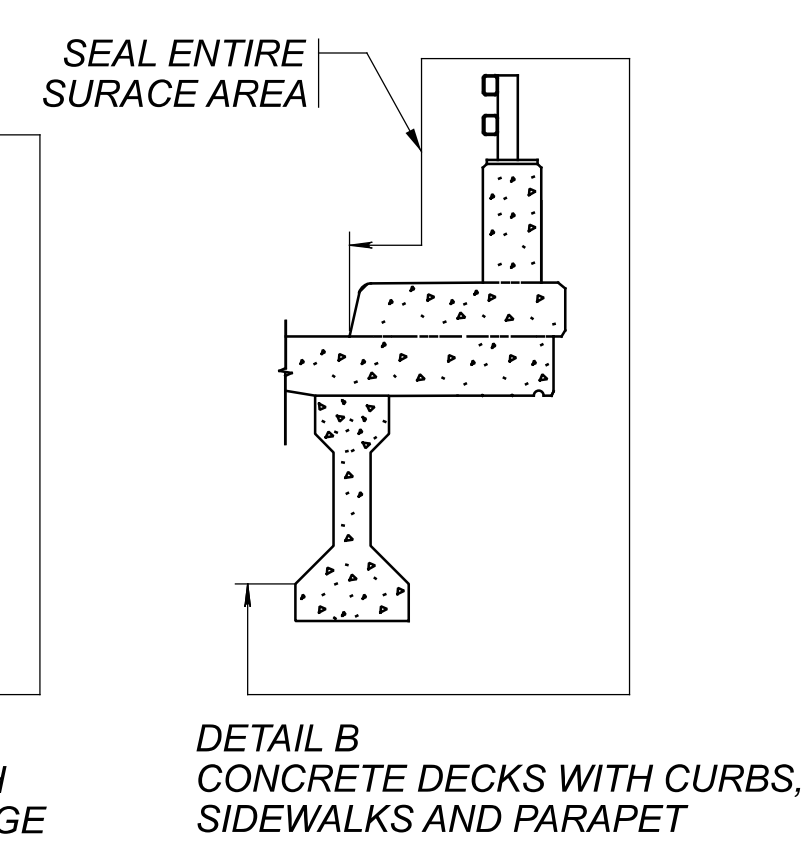
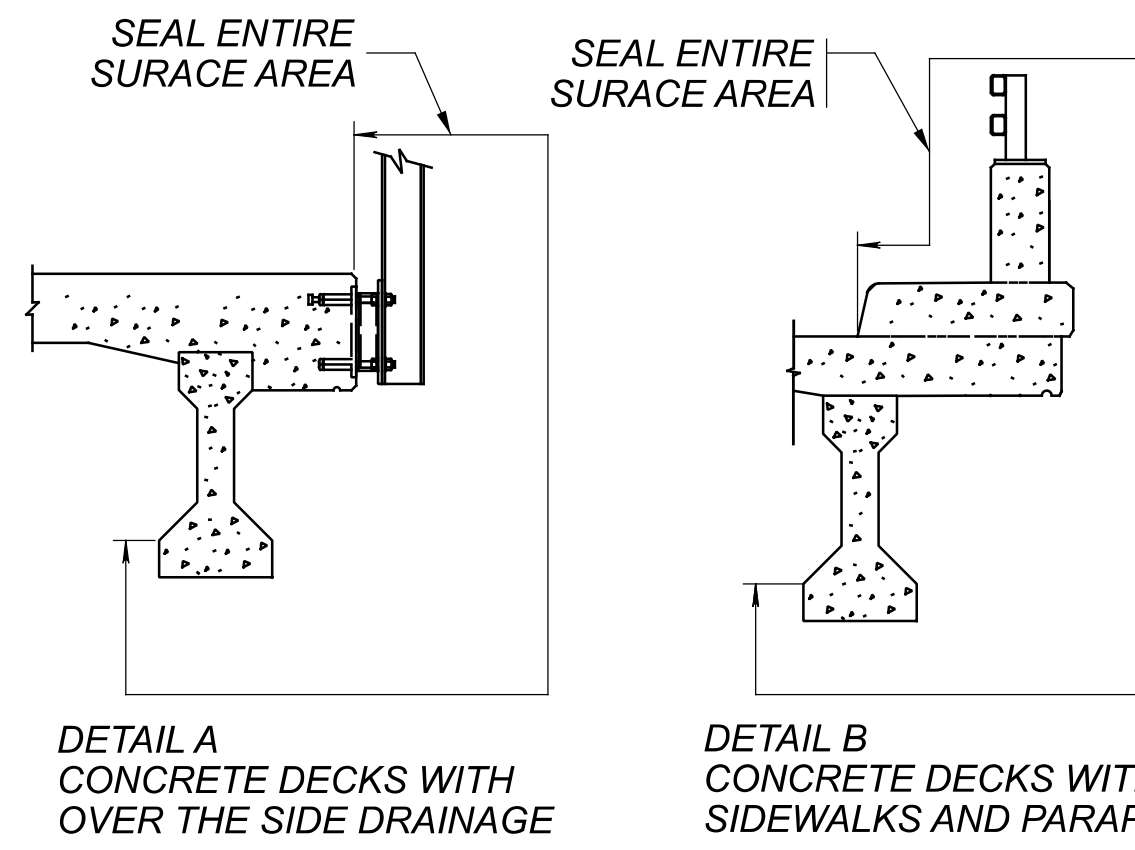
STRUCTURE ESTIMATED QUANTITIES
 POR-59-0605
 OVER NORFOLK SOUTHERN RR

SFN
 6701841
 DESIGN AGENCY



DESIGNER: CMS
 CHECKER: MJP
 REVIEWER: MJP
 DATE: 04/27/23
 PROJECT ID: 102732

SUBSET	TOTAL
2	8
SHEET	TOTAL
P.29	35



BRIDGE NUMBER	SEALING PAY ITEM	STRUCTURE TYPE	PROPOSED SEALING	FEDERAL COLOR NUMBER OF TOP COAT	ESTIMATED QUANTITIES				
					ABUT (SQ YD)	PIER (SQ YD)	SUPER (SQ YD)	GENERAL (SQ YD)	TOTAL (SQ YD)
POR-59-0605	ITEM 512, SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	STEEL CONTINUOUS MULTI-BEAM	SEAL PIERS, WINGWALLS, AND BACKWALLS SEAL CONCRETE SURFACES AS PER DETAIL F SEAL SIDEWALKS WITH CLEAR SEALANT	MATCH EXISTING	453	739	1456		2648


NOTES:
 -ALL EPOXY-URETHANE SEALING WORK MUST BE PERFORMED AFTER REMOVAL OF THE EXISTING VANDAL FENCE AND PRIOR TO INSTALLATION OF THE NEW VANDAL FENCE.

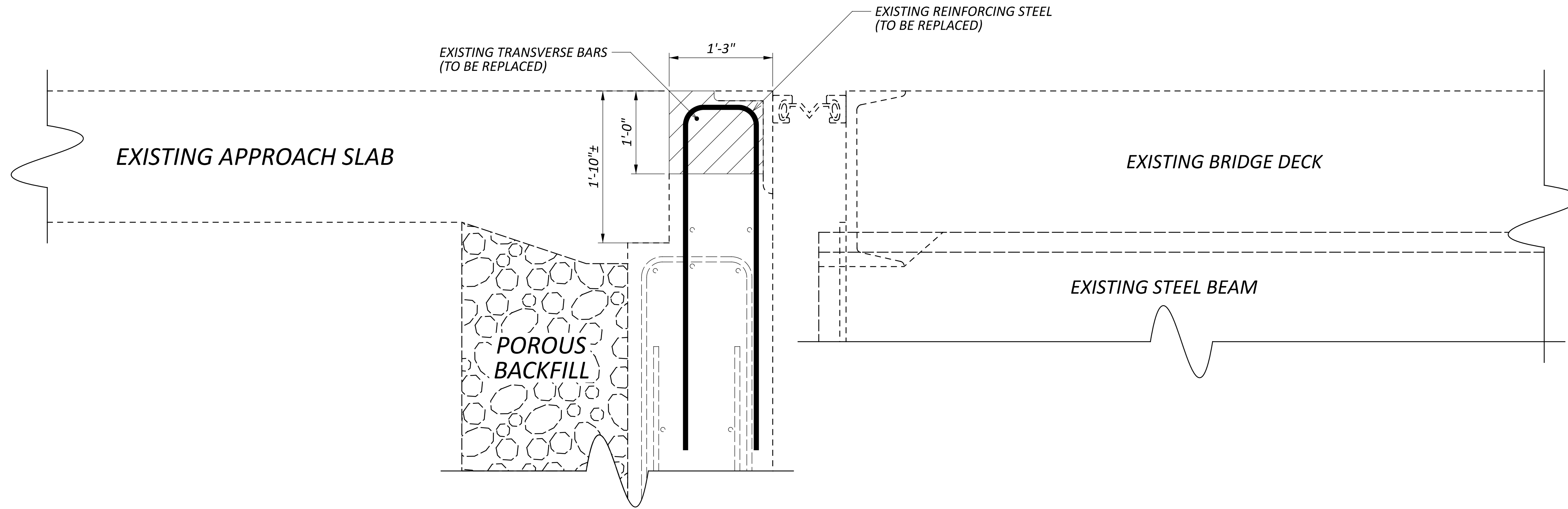
SFN
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 DESIGN AGENCY

DESIGNER: CMS
 CHECKER: MJP
 REVIEWER: MJP 04/27/23
 PROJECT ID: 102732

SUBSET	TOTAL
4	8
SHEET	TOTAL
P.31	35

NOTES:

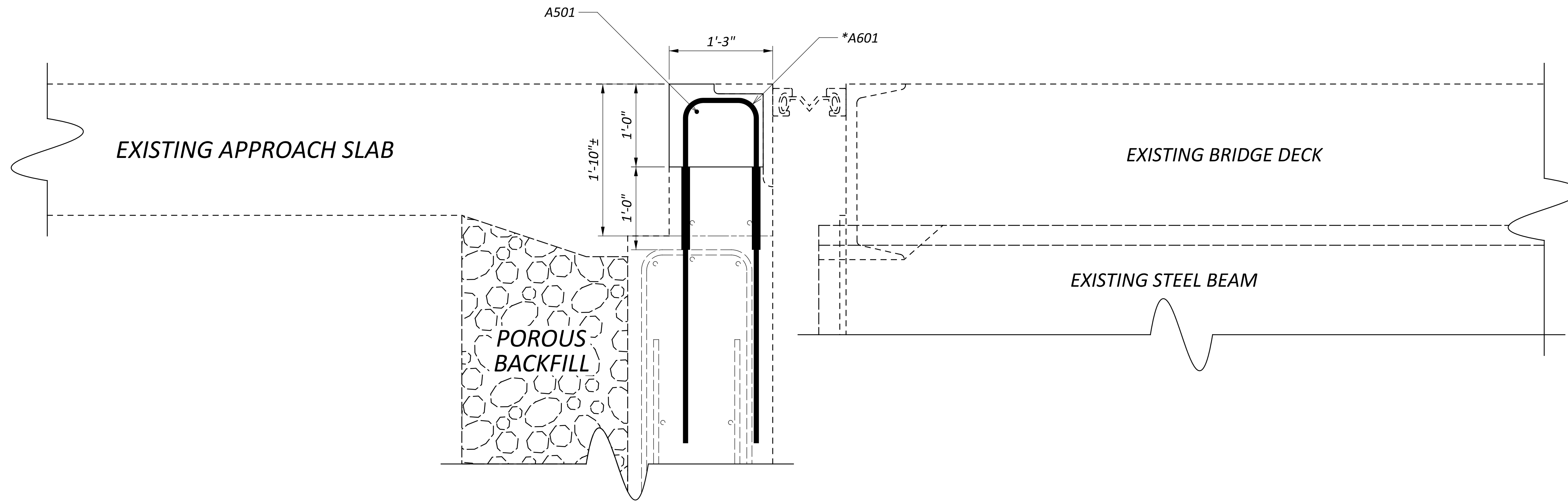
1.  ITEM 202 - PORTIONS OF STRUCTURE TO BE REMOVED
2. REMOVAL OF EXISTING JOINTS, DECK CONCRETE, AND BACKWALL CONCRETE WILL BE PAID FOR UNDER ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN. CARE WILL BE TAKEN TO SALVAGE ALL EXISTING LONGITUDINAL DECK REINFORCING STEEL DURING CONCRETE REMOVAL.



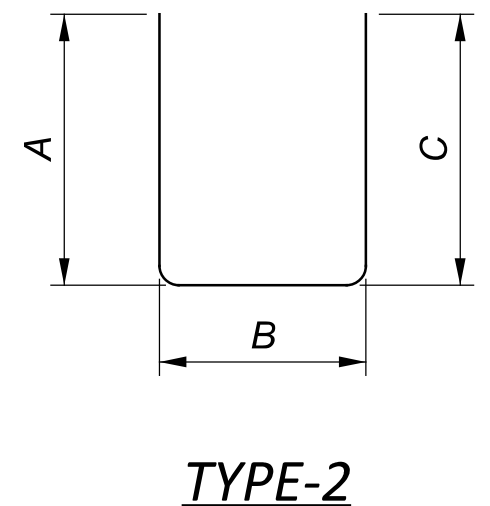
NOTES:

1. REBUILD PORTION OF DECK AND ABUTMENT BACKWALL PER THE DETAIL SHOWN BELOW. ALL MATERIAL, LABOR, EQUIPMENT, AND INCIDENTALS WILL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 511 - CONCRETE, MISC.: BACKWALL REPAIR.
2. ALL REINFORCING STEEL REQUIRED TO COMPLETE THE CONSTRUCTION OF THE NEW JOINT WILL BE PAID FOR UNDER ITEM 509 - EPOXY COATED REINFORCING STEEL, AS PER PLAN.
3. PROVIDE A 2" MINIMUM REINFORCING STEEL CLEARANCE.

*12" DOWEL



MARK	NUMBER				LENGTH	WEIGHT (LBS)	TYPE	DIMENSIONS												
	REAR ABUT	FWD ABUT	SUPER	TOTAL				A	B	C	D	E								
SUPERSTRUCTURE SUB-TOTAL						0														
A501	2	2		4	27'-0"	113	ST													
*A601	139	143		282	4'-0"	1695	2	1'-8"	11"	1'-8"										
ABUTMENT SUB-TOTAL						1808														
GRAND TOTAL						1808														



THE BAR SIZE NUMBER IS SPECIFIED ON THE PLANS IN THE BAR MARK COLUMN. THE FIRST DIGIT WHERE THREE DIGITS ARE USED, OR THE FIRST TWO DIGITS WHERE FOUR ARE USED, INDICATES THE BAR SIZE NUMBER. FOR EXAMPLE, P601 IS A NO. 6 BAR. BAR DIMENSIONS SHOWN ARE OUT TO OUT UNLESS OTHERWISE INDICATED. "R" INDICATES INSIDE RADIUS, UNLESS OTHERWISE NOTED. "STD." WRITTEN IN PLACE OF A DIMENSION INDICATES A STANDARD BEND AT THE END OF THE BAR.

ALL REINFORCING STEEL IS TO BE EPOXY COATED

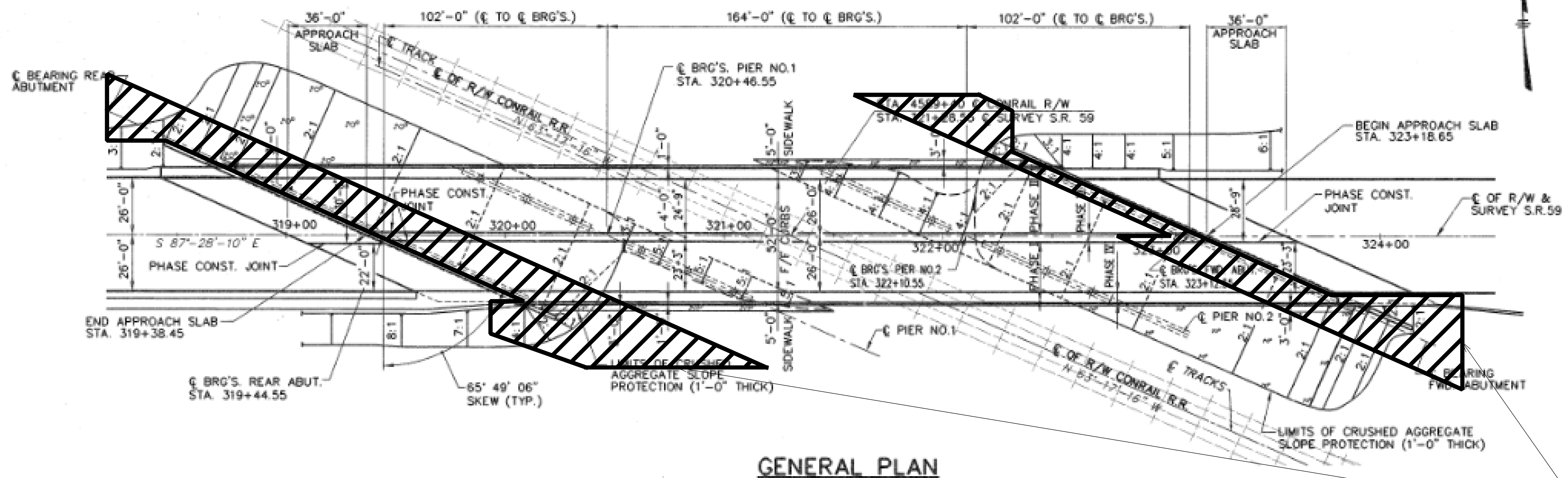
*12" DOWEL

SFN	
6701841	
DESIGN AGENCY	
DESIGNER	CHECKER
CMS	MJP
REVIEWER	
MJP 08/10/23	
PROJECT ID	
102732	
SUBSET	TOTAL
7	8
SHEET	TOTAL
P.34	35

FHWA REGION	STATE	PROJECT	
5	OHIO		

92
141

PORTAGE COUNTY
 POR-59-5.78

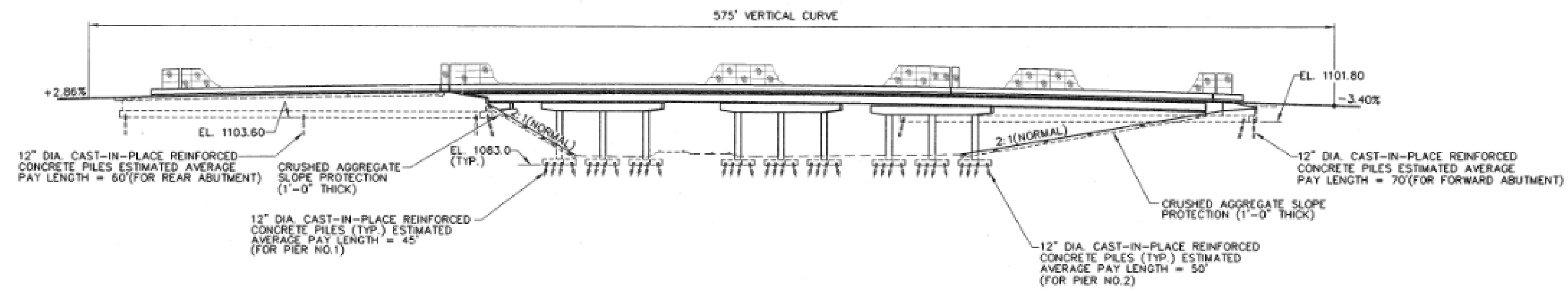


GENERAL PLAN

NOTES

- ① FOR PHASE CONSTRUCTION NOTES AND DETAILS SEE SHEETS 7 / 29 AND 8 / 29 .

APPROXIMATE CLEARING AND GRUBBING LIMITS*
 * SEE NOTE ON STRUCTURE GENERAL NOTES SHEET 1/5



ELEVATION

FINKBEINER, PETTIS & STROUT, LTD. 5 / 29
 CONSULTING ENGINEERS
 AKRON TOLEDO GREENSBORO

GENERAL PLAN & ELEVATION

BRIDGE NO. POR-59-0605
 OVER CONRAIL TRACKS

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
R.A.H.	J.D.P.		R.B.B.	T.E.N.	12/94	

STRUCTURE PLAN (FOR REFERENCE ONLY)
 POR-59-0605
 OVER NORFOLK SOUTHERN RR

SFN
 6701841
 DESIGN AGENCY



DESIGNER: CMS
 CHECKER: MJP
 REVIEWER: MJP
 DATE: 04/27/23

PROJECT ID: 102732

SUBSET	TOTAL
8	8
SHEET	TOTAL
P.35	35