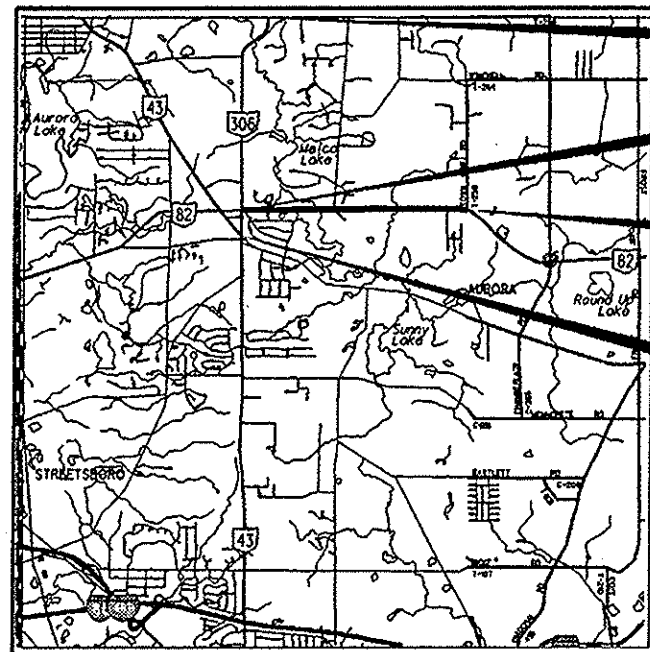


POR - SR-82/306-2.38/0.00
100590 PID - 77832
Dist 4 12/16/2010



LOCATION MAP

LATITUDE: N41°19'02" LONGITUDE: W81°20'44"



PORTION TO BE IMPROVED -----
INTERSTATE HIGHWAY -----
STATE & FEDERAL ROUTES -----
COUNTY & TOWNSHIP ROADS -----
OTHER ROADS -----

DESIGN DESIGNATION

DESIGN FUNCTIONAL CLASSIFICATION:
RURAL PRINCIPLE ARTERIAL SR 306 SLM 0.00 TO SLM 2.49
RURAL MINOR ARTERIAL SR 82 SLM 2.38 TO SLM 5.07

NHS PROJECT ----- NO

DESIGN EXCEPTIONS

NONE

UNDERGROUND UTILITIES	
CONTACT BOTH SERVICES CALL TWO WORKING DAYS BEFORE YOU DIG	
CALL 1-800-362-2764 (TOLL FREE)	
OHIO UTILITIES PROTECTION SERVICE NON-MEMBERS MUST BE CALLED DIRECTLY	
OIL GAS PRODUCERS PROTECTIVE SERVICE CALL 1-800-925-0988	

PLAN PREPARED BY:
ODOT---DISTRICT 4
2088 S. ARLINGTON ROAD
AKRON, OHIO 44306

END SR 306
SLM 2.49

BEGIN SR 82
SLM 2.38

END SR 82
SLM 5.07

BEGIN SR 306
SLM 0.00

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

POR-82/ 306-2.38 / 0.00

CITY OF AURORA PORTAGE COUNTY

INDEX OF SHEETS:

TITLE SHEET	1
TYPICAL SECTIONS	2
GENERAL NOTES	3-4
MAINTENANCE OF TRAFFIC	5-7
GENERAL SUMMARY	8
ASPHALT CONCRETE SUB-SUMMARY	9-10
PAVEMENT MARKING SUB-SUMMARY	11
STRUCTURES OVER 20'	12-15

PROJECT DESCRIPTION

IMPROVEMENT OF 2.69 MILES OF SR 82 AND 2.49 MILES OF SR306 BY PLANING AND RESURFACING THE ROADWAY. ALSO MAKING IMPROVEMENTS ON STRUCTURE POR-306-0217.

PROJECT EARTH DISTURBED AREA: 0 ACRES
ESTIMATED CONTRACTOR EDA: (N/A MAINTENANCE PROJECT)
NOTICE OF INTENT EDA: (N/A MAINTENANCE PROJECT)

2010 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVED THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

ENGINEERS SEAL:	STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS
	BP-3.1	10/19/07	TC-52.10	1/19/07			800-2010 10/15/2010
	BP-4.1	7/16/04	TC-52.20	1/19/07			832 5/05/2009
			TC-71.10	1/15/10			843 4/18/2003
	DM-4.3	4/17/09	TC-73.10	1/19/07			
	DM-4.4	4/17/09					
	MT-35.10	4/20/07					
	MT-97.10	4/17/09					
	MT-97.11	4/17/09					
	MT-99.20	1/16/09					
	MT-101.90	1/16/09					
	MT-105.10	1/16/09					
SIGNED:	TC-41.20	1/19/07					SPECIAL PROVISIONS
DATE: 9/13/10	TC-42.20	7/16/04					

APPROVED
DATE 9-13-2010 DISTRICT DEPUTY DIRECTOR

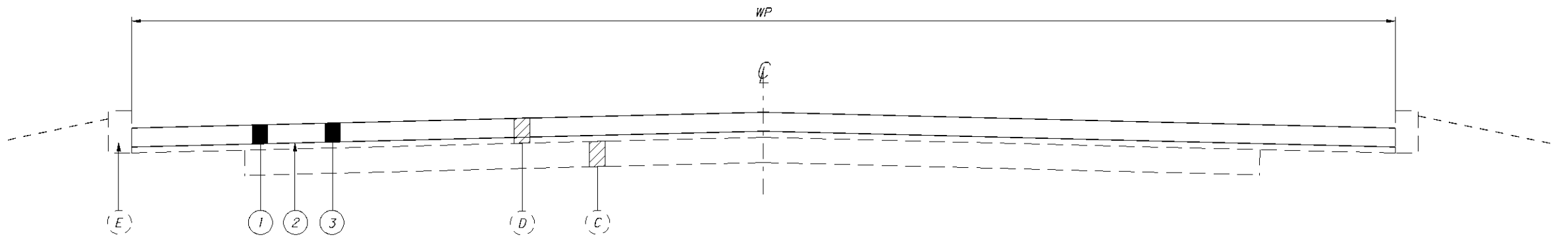
APPROVED
DATE 9-22-10 DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. E100(824)
PID NO. 77832
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
POR-82/ 306-2.38 / 0.00
1
15

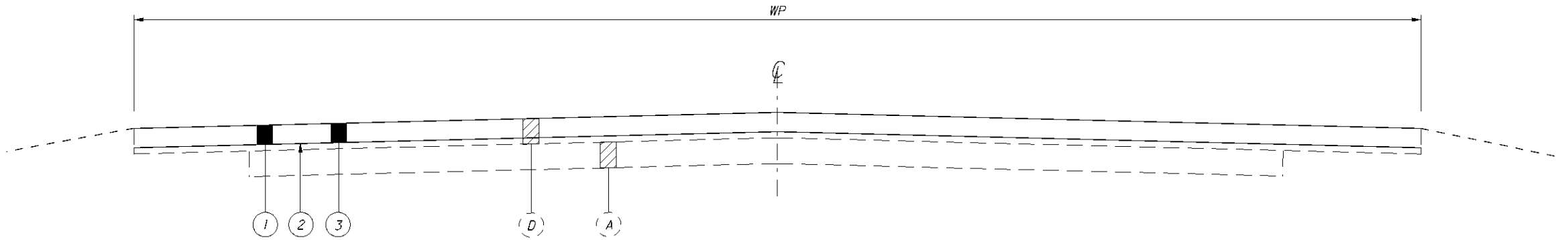
ROUTE	SLM	SLM	WIDTH OF PAVEMENT, WP
82	2.38	2.41	49'
82	2.41	3.45	26'
82	3.45	3.71	40'
82	3.71	5.07	26'

CURB LOCATIONS		
ROUTE	SLM	SLM
82	3.53	3.67

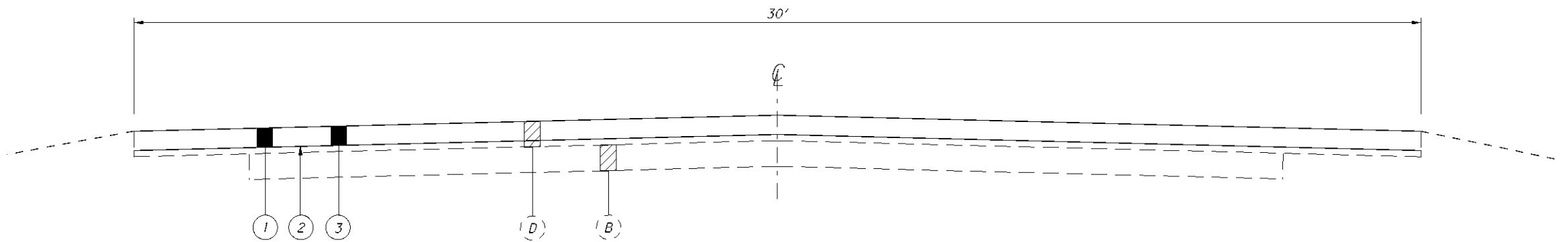
ROUTE	SLM	SLM	WIDTH OF PAVEMENT, WP
306	0.00	0.24	46'
306	0.24	0.28	35'
306	0.28	0.40	30'
306	0.40	0.45	44'
306	0.45	0.63	30'
306	0.63	0.65	38'
306	0.65	0.79	30'
306	1.89	1.93	30'
306	1.93	1.99	48'
306	1.99	2.49	30'



SR 82
SLM 2.38 TO SLM 5.07



SR 306
SLM 0.00 TO SLM 0.79
SLM 1.89 TO SLM 2.49



SR 306
SLM 0.79 TO SLM 1.89

LEGEND

- 1 254, PAVEMENT PLANING, ASPHALT CONCRETE (T=1 1/2")
- 2 407, TACK COAT @0.15 GAL/SY
- 3 448, ASPHALT CONCRETE SURFACE COURSE, TYPE 1 PG70-22M, (T=1 1/2")

- A WATER BOUND MACADAM
- B AGGREGATE BASE
- C PLANT MIX BITUMINOUS CONCRETE
- D EXISTING ASPHALT CONCRETE SURFACE COURSE
- E EXISTING CURB (SEE SHEET FOR LOCATIONS)

UTILITIES

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, THE OHIO UTILITIES PROTECTION SERVICE (OUPS), THE OHIO & GAS PROCEDURES UNDERGROUND PROTECTION SERVICE (OGPUPS), THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 4 HEAD-QUARTERS AND ALL NON REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION IN ALL AREAS.

OUPS 1-800-362-2764 (CONTACT LIMITED BASIS PARTICIPANTS DIRECTLY)
OGPUPS 1-800-925-0988
ODOT 330-786-3145 KEN GREENE

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PAVEMENT MARKING LANE WIDTHS

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKINGS ON THIS PROJECT SHALL BE AS FOLLOWS (AT LEAST 3 DAYS PRIOR TO PERFORMING THE WORK CONTACT THE TRAFFIC OFFICE AT 330-786-3147 TO CONFIRM THE WIDTHS):

ROUTE	S.L.M. TO	S.L.M. FROM	LANE WIDTH
SR 82	2.38	2.65	12'
SR 82	2.65	5.07	10'
SR 306	0.00	2.49	10'

PAVEMENT MARKING DETAILS

THE PAVEMENT MARKING DETAIL SHEETS WILL BE SUPPLIED TO THE CONTRACTOR AT THE PRE-CONSTRUCTION MEETING.

INTERSECTIONS

INTERSECTIONS WILL BE RESURFACED 2 FT. BEYOND THE EDGE LINE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR INDICATED IN THE PLAN. INTERSECTIONS SHALL BE PAVED AFTER COMPLETION OF THE SURFACE COURSE OR WITH THE MAINLINE PAVEMENT IF THIS CAN BE ACCOMPLISHED WITHOUT CHANGING THE VELOCITY AND DIRECTION OF THE PAVER. USE THE SAME ASPHALT CONCRETE AS THE MAINLINE PAVEMENT. PROVIDE A SMOOTH TRANSITION TO THE EXISTING PAVEMENT. ANY GRADING OR PRIME NECESSARY TO ACCOMPLISH THIS WORK SHALL BE INCLUDED IN THE COST OF THE PERTINENT BID ITEM.

ITEM 408 - PRIME COAT, AS PER PLAN

THE CONTRACTOR WILL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER.

THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID PRIME COAT MATERIAL ONTO THE EDGE OF THE PAVEMENT OR EDGELINE. CARE ALSO SHALL BE TAKEN TO AVOID SPRAYING LIQUID PRIME COAT MATERIAL ONTO DRIVEWAY APRONS, MAILBOX APPROACHES OR ANY PEDESTRIAN AREAS. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

AN ESTIMATED QUANTITY OF 1265 GAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

IN LOW SHOULDER AREAS EXCEEDING 1", OR AS DIRECTED BY THE ENGINEER, RECYCLED ASPHALT PAVEMENT (RAP) SHALL BE USED IN AREAS ADJACENT TO THE PAVED BERM. THE RAP SHALL HAVE A MINIMUM PG CONTENT OF 4.5% AND MEET THE FOLLOWING GRADATION. ONCE THE STOCKPILE MEETS THE GRADATION, THE PG CONTENT OF THE RAP SHALL BE DETERMINED PER 441.03. THE RAP ANALYSIS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL 2 WEEKS PRIOR TO USE. METHOD OF MEASUREMENT SHALL BE AS PER 617.06. PLACEMENT AND COMPACTION SHALL MEET THE REQUIREMENTS OF ITEM 617. ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 617 COMPACTED AGGREGATE, AS PER PLAN.

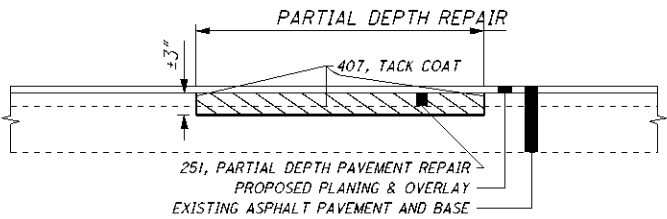
MODIFIED GRADATION SHALL APPLY:

SIEVE	TOTAL PERCENT PASSING
1-1/2"	100
3/4"	50-100
NO. 4	35-70
NO. 30	9-33
NO. 200	0-13

AN ESTIMATED QUANTITY OF 175 CU.YD. HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 448 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE I PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING. ALSO, THIS ITEM SHALL COMMENCE WITHIN 7 DAYS OF THE COMPLETION OF MAINLINE PAVEMENT PLANING. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:



AN ESTIMATED QUANTITY OF 1800 SQ.YD. HAS BEEN CARRIED TO THE GENERAL SUMMARY.

DRIVEWAYS

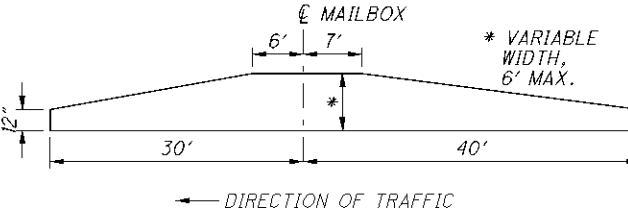
THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE ASPHALT SURFACE COURSE AND THE EXISTING DRIVEWAYS. IF APPROVED BY THE ENGINEER, AN ASPHALT WEDGE WITH A WIDTH OF APPROX 2' MAY BE PLACED EITHER ON THE ROADWAY SHOULDER OR DRIVEWAY DEPENDENT UPON WHICH SIDE IS HIGH. A QUANTITY OF MAINLINE SURFACE COURSE ASPHALT HAS BEEN PROVIDED IN THE CALCULATIONS AND GENERAL SUMMARY TO PERFORM THIS ITEM OF WORK.

PAVED MAILBOX APPROACHES

ALL EXISTING MAIL BOX APPROACHES WILL BE PAVED WITH ASPHALT CONCRETE AS PER TYPICAL SHOWN OR AS NEAR AS PRACTICAL. AGGREGATE APPROACHES SHALL HAVE A 2 IN. MIN. THICKNESS; IMPROVED APPROACHES SHALL HAVE A 2 IN. MIN. THICKNESS. THE CONTRACTOR SHALL HAVE THE OPTION OF PAVING THE MAILBOX APPROACHES WITH EITHER THE PAVING OF THE DRIVEWAYS OR THE PAVING OF THE MAINLINE AND SHOULDERS. PAYMENT SHALL BE AS FOLLOWS:

1. SHOULD THE CONTRACTOR ELECT TO PAVE THE MAILBOX APPROACHES WITH THE DRIVEWAYS THEN ALL GRADING, TACK, TOOLS, EQUIPMENT, MATERIAL AND INCIDENTALS REQUIRED FOR THE CONTRACTOR TO LAYOUT AND CONSTRUCT THE MAILBOX APPROACHES SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 448, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG-64-22 (DRIVEWAYS).

2. SHOULD THE CONTRACTOR ELECT TO PAVE THE MAILBOX APPROACHES WITH THE MAINLINE AND SHOULDERS, THEN ALL GRADING, TACK, TOOLS, EQUIPMENT, MATERIAL AND INCIDENTALS REQUIRED TO LAYOUT AND CONSTRUCT THE MAILBOX APPROACHES SHALL BE INCLUDED IN THE UNIT BID FOR ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M.



FIELD DRIVES

THIS ITEM OF WORK WILL CONSIST OF PLACING ITEM 304, AGGREGATE BASE FOR ALL FIELD DRIVES. FIELD DRIVES ARE TO BE PLACED A DISTANCE OF 10 FT FROM THE EDGE OF PAVED SHOULDER UNLESS OTHERWISE DIRECTED BY THE ENGINEER. FIELD DRIVES WILL BE PLACED AFTER THE COMPLETION OF THE SURFACE COURSE. AVERAGE THICKNESS WILL BE 2 IN. ALL GRADING, TOOLS, EQUIPMENT, MATERIAL, AND INCIDENTALS REQUIRED TO LAYOUT AND CONSTRUCT THE FIELD DRIVES WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 304, AGGREGATE BASE.

AN ESTIMATED QUANTITY OF 35 CU.YD. HAS BEEN CARRIED TO THE GENERAL SUMMARY.

CALCULATED
JMW
CHECKED

GENERAL NOTES

POR-82/ 306 - 2.38 / 0.00

3
15

ITEM SPECIAL - MAILBOX SUPPORT

THIS WORK SHALL CONSIST OF FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED MOUNTING HARDWARE IN ACCORDANCE WITH PLAN DETAILS, AND ATTACHING AN OWNER-SUPPLIED MAILBOX AT LOCATIONS SPECIFIED IN THE PLAN, OR OTHERWISE ESTABLISHED BY THE ENGINEER.

WOOD POSTS SHALL BE NOMINAL 4" BY 4" SQUARE OR 4-1/2" DIAMETER ROUND, AND CONFORM TO 710.14.

STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2" I.D., AND CONFORM TO AASHTO M 181.

ALL HARDWARE INCLUDING BUT NOT LIMITED TO PLATES, SCREWS, BOLTS, AND ETC. SHALL BE COMMERCIAL-GRADE GALVANIZED STEEL.

POSTS SHALL BE SET PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO BOXES MAY BE MOUNTED ON A SINGLE POST.

THE MAILBOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS, AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION.

IN THE ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER, THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POST MASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO A NEW LOCATION.

PAYMENT UNDER THIS ITEM SHALL BE LIMITED TO FINAL PERMANENT INSTALLATIONS. TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 107.10. HOWEVER, THE SAME MATERIAL AND SIZE LIMITATIONS AS FOR PERMANENT INSTALLATIONS SHALL APPLY.

MAILBOX SUPPORTS, COMPLETE IN PLACE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH, FOR ITEM SPECIAL MAILBOX SUPPORT SYSTEM, (SINGLE) (DOUBLE).

690, MAILBOX SUPPORT SYSTEM, SINGLE 8 EACH
690, MAILBOX SUPPORT SYSTEM, DOUBLE 5 EACH

ENDANGERED SPECIES HABITAT

THIS PROJECT IS WITHIN THE RANGE OF THE FEDERALLY ENDANGERED INDIANA BAT (MYOTIS SODALIS) AND MAY IMPACT SUMMER ROOSTING HABITAT FOR THIS SPECIES. THE SUMMER ROOSTING HABITAT FOR THE INDIANA BAT CONSISTS OF LIVING OR DEAD TREES OR SNAGS WITH EXFOLIATING, PEELING OR LOOSE BARK, SPLIT TRUNKS AND/OR BRANCHES OR CAVITIES. THEREFORE, ANY UNAVOIDABLE CUTTING OF SUCH TREES WILL BE PERFORMED ONLY AFTER SEPTEMBER 30 AND BEFORE APRIL 1.

AURORA BRANCH OF THE CHAGRIN STATE SCENIC RIVER AVOIDANCE

THE PROJECT PROPOSES TO REPAIR AND RESURFACE THE STATE ROUTE 82 ROADWAY IN PROXIMITY TO THE STATE ROUTE 82 BRIDGE SPANNING THE AURORA BRANCH OF THE CHAGRIN RIVER, REPAIR AND RESURFACE THE STATE ROUTE 306 ROADWAY NORTH AND SOUTH OF THE AURORA BRANCH OF THE CHAGRIN RIVER AND PERFORM MAINTENANCE ON THE STATE ROUTE 306 BRIDGE (STRUCTURE NO. POR-306-0217) SPANNING THE AURORA BRANCH OF THE CHAGRIN RIVER. THE AURORA BRANCH OF THE CHAGRIN RIVER IS DESIGNATED A STATE SCENIC RIVER.

IDLE EQUIPMENT, PETROCHEMICALS AND TOXIC/HAZARDOUS MATERIALS SHALL NOT BE STORED WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER, FLOOD PLAINS OR NEAR ANY DRAINAGE WAYS, DITCHES, STREAMS, WETLANDS OR OTHER WATERS OF THE UNITED STATES. REFUELING OF EQUIPMENT SHALL NOT OCCUR WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER, FLOOD PLAINS OR NEAR ANY DRAINAGE WAYS, DITCHES, STREAMS, WETLANDS OR OTHER WATERS OF THE UNITED STATES

UNDER NO CIRCUMSTANCES SHALL ANY EQUIPMENT (BACKHOE, EARTH MOVING EQUIPMENT, ETC.), SCAFFOLDING, FILL AND/OR OTHER MATERIALS ENTER THE AURORA BRANCH OF THE CHAGRIN RIVER OR ANY OTHER STREAMS, WETLANDS OR OTHER WATERS OF THE UNITED STATES. SHOULD ANY MATERIALS AND/OR DEMOLITION DEBRIS FALL INTO THE AURORA BRANCH OF THE CHAGRIN RIVER, ALL WORK SHALL BE STOPPED AND ALL DEBRIS/MATERIALS SHALL BE REMOVED IMMEDIATELY, IN SUCH A WAY AS TO MINIMIZE TURBIDITY THAT COULD DEGRADE WATER QUALITY AND ADVERSELY AFFECT AQUATIC PLANT AND ANIMAL LIFE.

IN EACH CASE WHERE THERE IS INCIDENT OF DEBRIS AND/OR MATERIALS THAT FALL OR MIGRATE INTO THE AURORA BRANCH OF THE CHAGRIN RIVER, THE CONTRACTOR SHALL, AS SOON AS POSSIBLE, NOTIFY THE PROJECT ENGINEER. IN EACH CASE WHERE THERE IS AN INCIDENT OF HAZARDOUS MATERIAL FALLING OR MIGRATING INTO THE AURORA BRANCH OF THE CHAGRIN RIVER, THE CONTRACTOR SHALL, AS SOON AS POSSIBLE, NOTIFY THE PROJECT ENGINEER AND THE FOLLOWING AGENCIES:

OHIO DEPARTMENT OF NATURAL RESOURCES
DIVISION OF WATERCRAFT
(440) 225-5582
ATTN: MATTHEW SMITH, NE OHIO SCENIC RIVERS MANAGER

CITY OF AURORA DIVISION OF FIRE
(330) 562-7171

OHIO EPA SPILL REPORTING
24-HOUR EMERGENCY SERVICE
CALL: 1-800-282-9378

PROVIDE AS MUCH OF THE FOLLOWING INFORMATION AS POSSIBLE:

- 1. TIME OBSERVED
- 2. LOCATION
- 3. MATERIAL RELEASED
- 4. PROBABLE SOURCE
- 5. VOLUME AND DURATION
- 6. PRESENT AND ANTICIPATED MOVEMENT OF CONTAMINATE
- 7. PERSONNEL ON SCENE
- 8. ACTIONS ALREADY INITIATED
- 9. PERSON(S) ON SCENE TO CONTACT

ALL PAVEMENT REPAIR AND RESURFACING WORK WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER SHALL BE PERFORMED WITHIN THE FOOTPRINT OF THE EXISTING PAVEMENT AND SHOULDERS OF STATE ROUTE 82 AND STATE ROUTE 306.

ALL ACCESS AND WORK TO BE PERFORMED ON BRIDGE STRUCTURE NUMBER POR-306-0217 (SFN: 6704743) SHALL BE PERFORMED WITHIN 15- FEET OF THE EXISTING STRUCTURE. THE CONTRACTOR SHALL LIMIT THE DISTURBANCE OF EXISTING VEGETATED AREA TO THE MAXIMUM EXTENT PRACTICABLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR IMPACT REMAINING VEGETATED DITCHES AND/OR OTHER VEGETATED AREAS WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER BEYOND THE LIMITS NECESSARY TO CONSTRUCT THE PROJECT.

TO PROTECT AND DELINEATE AREAS LOCATED BEYOND THE CONSTRUCTION LIMITS AND WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER, A QUANTITY OF ITEM 832 PERIMETER FILTER FABRIC FENCE SHALL BE INCLUDED IN THE PLAN. FURTHER, TO PROTECT AND DELINEATE AREAS LOCATED BEYOND THE CONSTRUCTION LIMITS AT BRIDGE STRUCTURE NUMBER POR-306-0217 (SFN: 6704743), A QUANTITY OF ITEM 832 EROSION CONTROL HAS BEEN INCLUDED IN THE PLAN FOR CONSTRUCTION FENCE AND PERIMETER FILTER FABRIC FENCE. ALL CONSTRUCTION FENCE AND PERIMETER FILTER FABRIC FENCE SHALL BE INSTALLED BY THE CONTRACTOR AT THE CONSTRUCTION LIMITS WITHIN THE RIGHT-OF-WAY PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES INCLUDING ANY NECESSARY CLEARING AND GRUBBING ACTIVITIES. THE FENCE SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT.

SPECIFICATION SET FORTH IN THE MOST CURRENT VERSION OF ODOT S CONSTRUCTION AND MATERIAL SPECIFICATIONS, LOCATION AND DESIGN MANUAL AND STANDARD DRAWINGS SHALL BE USED TO ENSURE ADEQUATE EROSION AND SEDIMENT CONTROL, ALONG WITH ADDITIONAL PROTECTIVE MEASURES TO AVOID IMPACTS TO ADJACENT PROPERTIES. ALL SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN PLACE PRIOR TO ANY CONSTRUCTION ACTIVITIES INCLUDING ANY NECESSARY CLEARING AND GRUBBING ACTIVITIES. THEY SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE AND THE AREA IS STABILIZED AS ACCEPTED BY THE ENGINEER. THEY SHALL ALSO COMPLY WITH ODOT S "HANDBOOK FOR SEDIMENT AND EROSION CONTROL .

VEGETATED AREAS WITHIN THE CONSTRUCTION LIMITS SHALL BE MAINTAINED TO THE MAXIMUM EXTENT PRACTICABLE. CLEARING AND GRUBBING AT BRIDGE STRUCTURE NUMBER POR-306-0217 (SFN: 6704743) SHALL BE LIMITED TO THE REMOVAL AND TRIMMING OF SHRUBS, TREES AND/OR OTHER ABOVE GROUND VEGETATION TO THE EXTENT NECESSARY TO GAIN ACCESS AND PROPERLY TREAT AND SEAL THE POR-306-0217 BRIDGE STRUCTURE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR IMPACT AREAS BEYOND 15 FEET OF THE POR-306-0217 BRIDGE STRUCTURE. ALL AREAS LOCATED WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER DISTURBED DURING CONSTRUCTION OPERATIONS SHALL BE STABILIZED AND RESTORED USING APPROPRIATE NATIVE OHIO GRASS SPECIES SUITABLE FOR PLANTING WITHIN 1,000 FEET OF STATE SCENIC RIVERS.

ALL WASTE MATERIALS GENERATED DURING CONSTRUCTION ACTIVITIES WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER SHALL BE IMMEDIATELY REMOVED FROM THE CONSTRUCTION SITE. IMMEDIATE REMOVAL IS DEFINED AS DEPOSITING THE REMOVED MATERIALS DIRECTLY INTO A TRUCK AND REMOVING THE MATERIALS FROM THE SITE; PLACEMENT OF WASTE MATERIALS WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER, FLOOD PLAINS, STREAMS, WETLANDS AND/OR WATERS OF THE UNITED STATES EVEN TEMPORARILY IS CONSIDERED A FILL AND IS PROHIBITED.

THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PREVENT EPOXY-URETHANE SEALER, PAINT, OR OTHER MATERIALS USED TO REPAIR, CLEAN, SEAL, OR TREAT ANY BRIDGE STRUCTURE FROM ENTERING THE AURORA BRANCH OF THE CHAGRIN RIVER AND/OR ANY OTHER STREAMS, WETLANDS OR OTHER WATERS OF THE UNITED STATES AND TAKE THE APPROPRIATE ACTIONS IN THE EVENT OF A RELEASE.

AN UNDER-DECK APRON OR SIMILAR DEVICE SHALL BE INSTALLED TO PREVENT ACCIDENTAL SPILLAGE OR MATERIALS FROM ENTERING THE AURORA BRANCH OF THE CHAGRIN RIVER. FULL CONTAINMENT SHALL BE ACHIEVED TO PREVENT IMPACTS TO THE AQUATIC LIFE AND WATER QUALITY. THE CONTRACTOR SHALL CONTAIN ALL WASTEWATER AND OTHER WASTE MATERIALS ASSOCIATED WITH THE BRIDGE CONSTRUCTION OPERATIONS. ALL WASTE MATERIALS GENERATED DURING CONSTRUCTION ACTIVITIES WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER SHALL BE IMMEDIATELY REMOVED FROM THE CONSTRUCTION SITE.

IF SAND OR WATER BLASTING ANY PORTION OF THE BRIDGE IS NECESSARY THEN APPROPRIATE APRONS SHALL BE UTILIZED TO PROVIDE FOR CONTAINMENT OF ALL BLASTING MATERIALS, PAINT DEBRIS PARTICLES AND OTHER DEBRIS. ANY SUCH MATERIALS AND DEBRIS SHALL BE REMOVED AND DISPOSED OF IN AN APPROVED MANNER. APPROPRIATE APRONS SHALL BE USED TO PROVIDE FOR COMPLETE CONTAINMENT OF ALL PAINT AND/OR SEALANT OVER-SPRAY.

DISCHARGE OF WASTEWATER INTO THE AURORA BRANCH OF THE CHAGRIN RIVER, FLOOD PLAINS, STREAMS, WETLANDS OR OTHER WATERS OF THE UNITED STATES IS PROHIBITED. ALL WASTEWATER SHALL BE CONTAINED AND PROPERLY TESTED, TRANSPORTED, AND DISPOSED OF IN A LICENSED WASTE FACILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS AND TO TRANSPORT THE WASTEWATER TO A DISPOSAL FACILITY. THE CONTRACTOR SHALL CONTACT THE FACILITY TO DETERMINE IF ANY ADDITIONAL TESTING IS REQUIRED FOR DISPOSAL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONDUCTING ANY ADDITIONAL SAMPLING AND ANALYSIS OF WASTE MATERIAL(S).

THE CONTRACTOR SHALL LIMIT THE AMOUNT OF OPEN PAINT/SEALER, HAZARDOUS AND/OR OTHER POTENTIALLY HAZARDOUS MATERIALS TO THE EXTENT PRACTICABLE TO PERFORM THE REQUIRED WORK. DISCARDED CONTAINERS SHALL BE IMMEDIATELY REMOVED FROM WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER AND UNDER NO CIRCUMSTANCES SHALL ANY PAINT, SEALER, HAZARDOUS AND/OR POTENTIALLY HAZARDOUS MATERIALS BE STORED WITHIN 1,000 FEET OF THE AURORA BRANCH OF THE CHAGRIN RIVER.

THE OHIO DEPARTMENT OF NATURAL RESOURCES, WATERCRAFT DIVISION, SHALL RECEIVE PRECONSTRUCTION NOTIFICATION, AT LEAST FIFTEEN (15) CALENDAR DAYS PRIOR TO THE START OF ANY WORK, INCLUDING INSTALLATION OF MAINTENANCE OF TRAFFIC SIGNAGE, STAGING OF EQUIPMENT AND/OR MATERIALS, ETC. WITHIN 1,000 OF THE AURORA BRANCH OF THE CHAGRIN RIVER. THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO THE OHIO DEPARTMENT OF NATURAL RESOURCES, WATERCRAFT DIVISION. INFORMATION REQUIRED AS PART OF THE PRECONSTRUCTION NOTIFICATION SHALL INCLUDE:

- 1. THE CONTRACTOR S NAME AND ADDRESS.
- 2. CONTRACTOR AND ODOT, DISTRICT 4 CONSTRUCTION REPRESENTATIVE CONTACT INFORMATION
- 3. THE SCHEDULED DATES FOR THE PRECONSTRUCTION MEETING
- 4. THE SCHEDULED DATES FOR START AND COMPLETION OF THE POR-306-0217 BRIDGE MAINTENANCE OPERATIONS.
- 4. ONE COPY OF THE CONSTRUCTION PLAN.

THE CONTRACTOR SHALL COMPILE THE ABOVE INFORMATION AND SUBMIT IT TO FOLLOWING:

OHIO DEPARTMENT OF NATURAL RESOURCES DIVISION OF WATERCRAFT
2045 MORSE ROAD
BUILDING A
COLUMBUS, OH 43229
ATTN: OHIO SCENIC RIVERS MANAGER FOR THE CHAGRIN RIVER

AND

OHIO DEPARTMENT OF NATURAL RESOURCES DIVISION OF WATERCRAFT
3441 NORTH RIDGE WEST
ASHTABULA, OH 44004
ATTN: MATTHEW SMITH, NE OHIO SCENIC RIVERS MANAGER

A COPY OF THE NOTIFICATIONS SHALL BE PROVIDED TO THE ODOT PROJECT ENGINEER.

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2211 AND THE CITY OF AURORA (330) 995-9116, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.

4. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.

6. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL.

8. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.

10. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

614, WORK ZONE MARKING SIGN,	20 EACH
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614, WORK ZONE CENTER LINE, CLASS II, 5.33 MILE
614, WORK ZONE CHANNELIZING LINE, CLASS I, 1473 FT
614, WORK ZONE STOP LINE, CLASS I, 128 FT

614, WORK ZONE CENTER LINE, CLASS III, 642 PAINT,	5.33 MILE
614, WORK ZONE STOP LINE, CLASS III, 642 PAINT,	128 FT
614, WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT,	1473 FT

SURFACE CONDITION SIGNS

WINTER TRAFFIC LIMITATIONS

CONTRACTOR'S EQUIPMENT - OPERATION AND STORAGE

TRAFFIC CONTROL INSPECTOR

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$7500 FOR EACH HOUR THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE ODOT INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE ODOT, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 40 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ITEM 632 - DETECTOR LOOP, AS PER PLAN

THE CONTRACTOR SHALL CONTACT THE DISTRICT OFFICE (330-786-3146) THREE WORKING DAYS PRIOR TO ANY PLANING OR TRENCHING AT THE INTERSECTIONS SHOWN BELOW. LOOP DETECTORS DISTURBED BY PAVEMENT PLANING OR TRENCHING SHALL BE ABANDONED IN PLACE. THE LOOP DETECTOR WIRE WILL BE CUT INTO THE PAVEMENT AFTER THE PROPOSED SURFACE COURSE HAS BEEN PLACED. ALL STOP LINE INDUCTANCE DETECTOR LOOPS SHALL BE THE POWERHEAD CONFIGURATION SHOWN ON TC-82.10. THE WIDTH SHALL BE AS SPECIFIED ON TC-82.10 AND THE LENGTH SHALL BE AS SPECIFIED BELOW. THE LOCATION OF THESE LOOPS SHALL BE SUCH THAT THE POWERHEAD IS LOCATED AT THE STOP LINE, NOT PAST IT. ALL DILEMMA ZONE INDUCTANCE DETECTOR LOOPS CALLED FOR IN THE PLANS SHALL BE THE ANGULAR DESIGN DETECTION (ADD) LOOP AS SHOWN ON TC-82.10. DIMENSIONS SHALL BE AS SPECIFIED ON TC-82.10 AND THE LOOP SHALL BE PLACED AT THE SAME LOCATION AS THE EXISTING LOOPS.

THE QUANTITIES LISTED BELOW HAVE BEEN CARRIED TO THE GENERAL SUMMARY. THE NEW LOOP DETECTOR WIRES SHALL BE RUN INTO THE EXISTING CONTROL BOX OR THE EXISTING PULLBOX. INCLUDED IN THIS ITEM IS THE POURED EPOXY TYPE CABLE SPLICE KIT (CONFORMING TO 725.15E) THAT MUST BE USED IN MAKING THESE CONNECTIONS. ALL NECESSARY MATERIAL, LABOR, SPLICE KITS AND EQUIPMENT SHALL BE INCIDENTAL TO PAYMENT OF THESE ITEMS.

632 DETECTOR LOOP, AS PER PLAN, 15 EACH

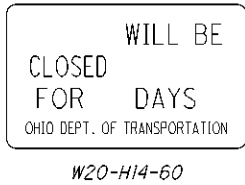
SR 82/SR 43	1 - (8 X 30) 1 - (6 X 24) 1 - (6 X 6)
SR 82/SR 306	3 - (8 X 30) 5 - (6 X 24) 2 - (6 X 6)
SR 306/SR 43	1 - (8 X 30) 1 - (6 X 6)

DETOUR NOTIFICATION [CITY/COUNTY]

THE CONTRACTOR SHALL ADVISE THE ODOT DISTRICT OFFICE (330-786-3148) AND THE CITY OF AURORA (330-844-3614) EIGHTEEN (18) DAYS IN ADVANCE OF WHEN THE DETOUR ROUTE SHOULD BE IN EFFECT. ALL WORK ZONE DEVICES REQUIRED SHALL BE FURNISHED, ERECTED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. PAYMENT FOR ALL WORK ASSOCIATED WITH THE DETOUR SHALL BE INCLUDED UNDER THE LUMP SUM BID FOR ITEM 614, DETOUR SIGNING.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS, AS DETAILED IN THESE PLANS, SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD OR RAMP CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.



ITEM 614, MAINTAINING TRAFFIC (TIME LIMITATION ON A DETOUR)

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED ONE WEEKEND (A WEEKEND IS 7PM FRIDAY TO 6 AM MONDAY) WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEET 7. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$1200 FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

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DETOUR SIGNING

A

ROAD CLOSED
MILES AHEAD
LOCAL TRAFFIC ONLY
R11-3A-60
← DETOUR
M4-10L-48

B

ROAD CLOSED
MILES AHEAD
LOCAL TRAFFIC ONLY
R11-3A-60
DETOUR →
M4-10R-48

C

DETOUR
M4-8-24
NORTH
M3-1-24
306
MI-5-30-3
→
M5-1R-21

D

DETOUR
M4-8-24
NORTH
M3-1-24
306
MI-5-30-3
→
M6-1R-21

E

DETOUR
M4-8-24
306
MI-5-30-3

F

DETOUR
M4-8-24
NORTH
M3-1-24
306
MI-5-30-3
422 EAST
W e
→

G

SR 306
CLOSED
AHEAD
W20-3-36

H

DETOUR
M4-8-24
SOUTH
M3-3-24
306
MI-5-30-3
→
M5-1R-21

I

DETOUR
M4-8-24
SOUTH
M3-3-24
306
MI-5-30-3
422 WEST
C e e
→

J

DETOUR
M4-8-24
SOUTH
M3-3-24
306
MI-5-30-3

K

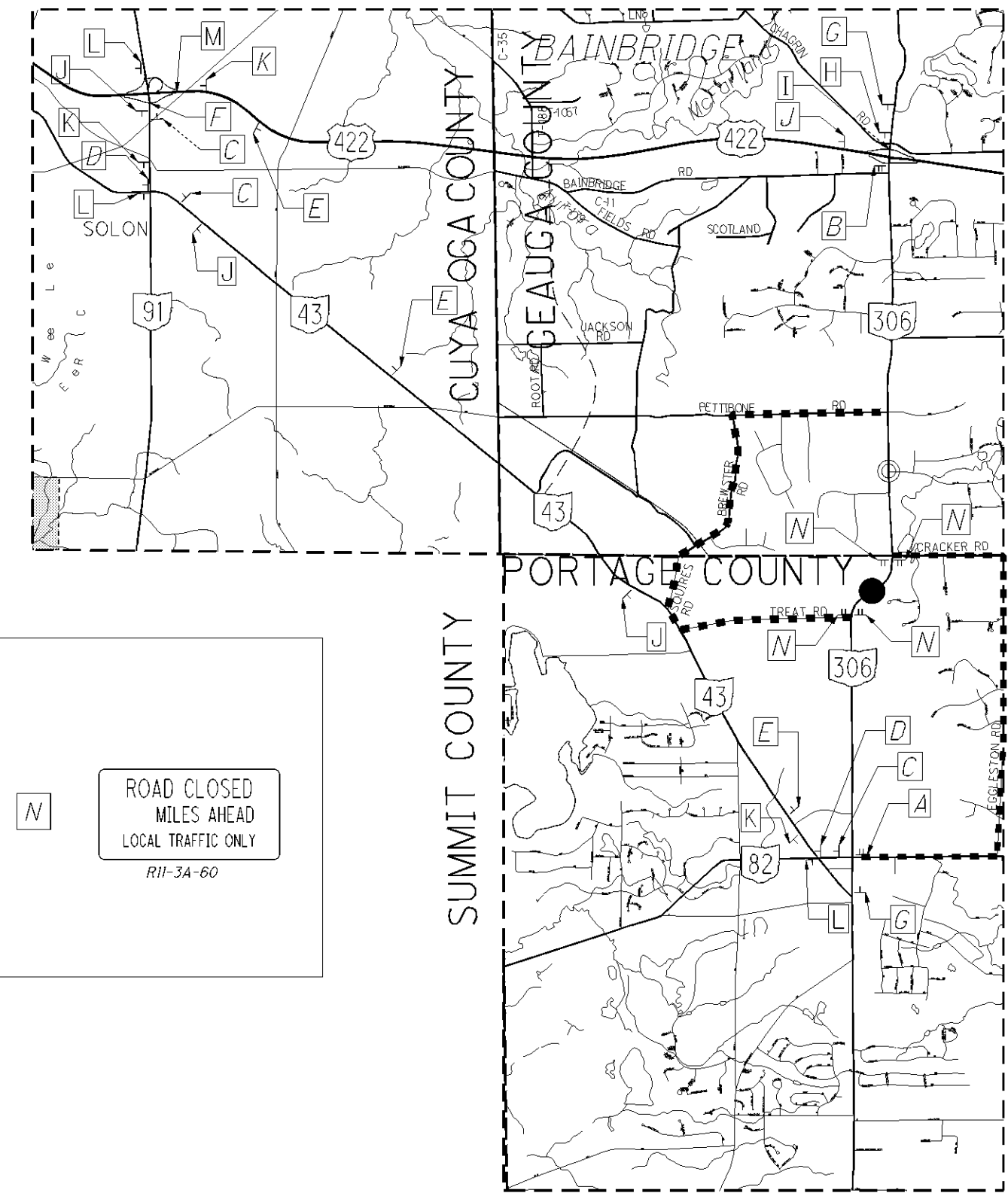
DETOUR
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DETOUR
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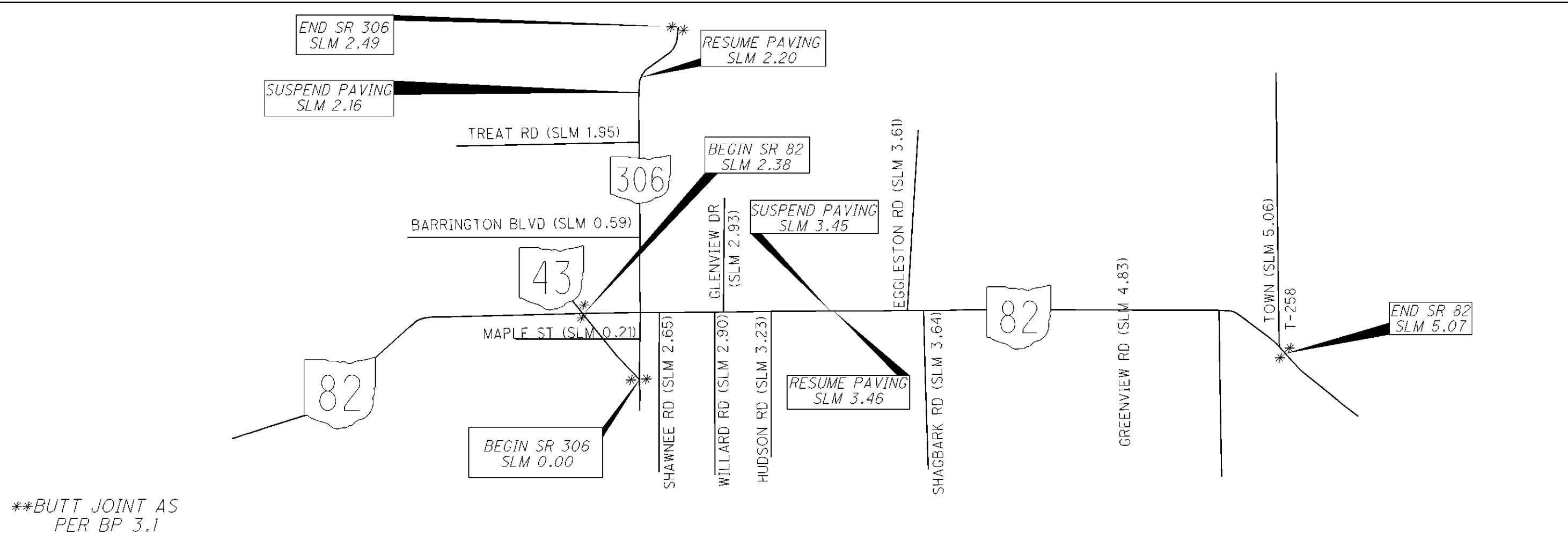
ROAD CLOSED
MILES AHEAD
LOCAL TRAFFIC ONLY
R11-3A-60

DETOUR PLAN FOR STRUCTURE (POR-306-0217)

- CLOSED PER MT-101.60
- OFFICIAL DETOUR ROUTE: SR 82 / SR 43 / SR 91 / US 422
- - - LOCAL ALTERNATE DETOUR ROUTE:
NB- TREAT RD / SR 43 / SQUIRES RD / BREWSTER [TH 184] / PETTIBONE [TH 185]
SB- CRACKER RD [TH 259] / EGGLESTON / SR 82

NOTE: REFER TO THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, TYPICAL APPLICATION 8, FOR SIGN SPACING.

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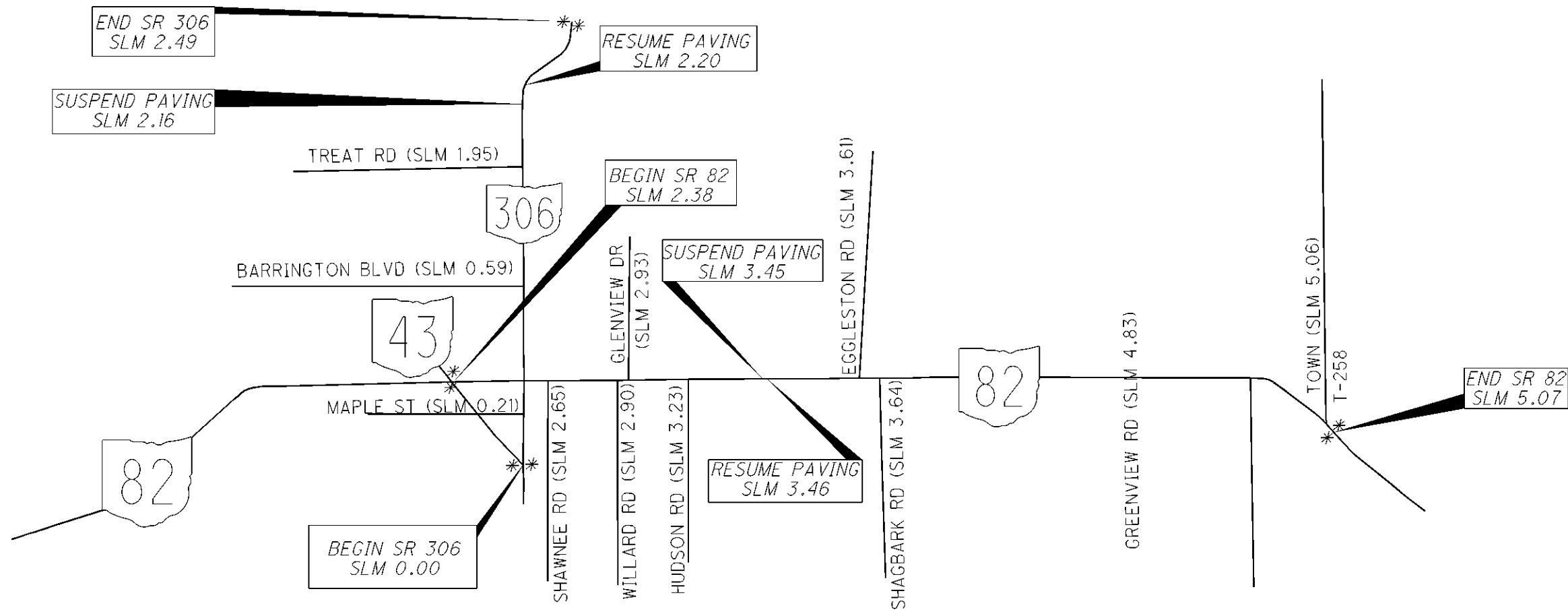


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PER BP 3.1

SLM RANGE			TYPICAL SECTION	SIDE	DISTANCE (D)	AVERAGE WIDTH (W)	SURFACE AREA (A) A=DxW/9	CADD GENERATED AREA	202	254	407	448	448										
									WEARING COURSE REMOVED	PAVEMENT PLANING, ASPHALT CONCRETE (T=1 1/2")	TACK COAT @ 0.15 GAL/SY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M (T=1 1/2")	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 (DRIVEWAYS)										
					FT	FT	SQ YD	SQ YD	SQ YD	SQ YD	GALLON	CU YD	CU YD										
SR 82																							
2.38	TO	2.41			158.40	49.00	862.40		54.44	862.40	129.36	35.93											
2.41	TO	3.45			5491.20	26.00	15863.47			15863.47	2379.52	660.98											
3.46	TO	3.71			1320.00	40.00	5866.67			5866.67	880.00	244.44											
3.71	TO	5.07			7180.80	26.00	20744.53		28.89	20744.53	3111.68	864.36											
SR 306																							
0.00	TO	0.24			1267.20	46.00	6476.80		51.11	6476.80	971.52	269.87											
0.24	TO	0.28			211.20	35.00	821.33			821.33	123.20	34.22											
0.28	TO	0.40			633.60	30.00	2112.00			2112.00	316.80	88.00											
0.40	TO	0.45			264.00	44.00	1290.67			1290.67	193.60	53.78											
0.45	TO	0.63			950.40	30.00	3168.00			3168.00	475.20	132.00											
0.63	TO	0.65			105.60	38.00	445.87			445.87	66.88	18.58											
0.65	TO	1.93			6758.40	30.00	22528.00			22528.00	3379.20	938.67											
1.93	TO	1.99			316.80	48.00	1689.60			1689.60	253.44	70.40											
1.99	TO	2.16			897.60	30.00	2992.00			2992.00	448.80	124.67											
2.20	TO	2.49			1531.20	30.00	5104.00		33.33	5104.00	765.60	212.67											
INTERSECTIONS																							
SR 82																							
2.38	TO	5.07			2.00	VARIES	93.78		93.78		14.07	3.91											
SR306																							
0.00	TO	2.49			2.00	VARIES	76.67		76.67		11.50	3.19											
SUBTOTALS									338.22	89965.33	13520.37	3755.66	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
TOTALS CARRIED TO GENERAL SUMMARY									339	89966	13521	3756	0	0	0	0	0	0	0	0	0	0	

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SLM RANGE			TYPICAL SECTION	SIDE	DISTANCE (D)	AVERAGE WIDTH (W)	SURFACE AREA (A) A=DxW/9	CADD GENERATED AREA	202	254	407	448	448											
									WEARING COURSE REMOVED	PAVEMENT PLANING, ASPHALT CONCRETE (T=1 1/2")	TACK COAT @ 0.15 GAL/SY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M (T=1 1/2")	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 (DRIVEWAYS)											
					FT	FT	SQ YD	SQ YD	SQ YD	SQ YD	GALLON	CU YD	CU YD											
MAILBOX APPROACHES																								
SR 82																								
2.38	TO	5.07			14203.20		1275.00						61.98											
SR 306																								
0.00	TO	2.49			13147.20		0.00	1190.00					57.85											
DRIVEWAYS																								
SR 82																								
2.38	TO	5.07			10.00	VARIES	2133.33					59.26												
SR 306																								
0.00	TO	2.49			10.00	VARIES	1666.67					46.30												
SUBTOTALS									0.00	0.00	0.00	105.56	119.83	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
TOTALS CARRIED TO GENERAL SUMMARY									0	0	0	106	120	0	0	0	0	0	0	0	0	0	0	0

PAVEMENT DETAILS

POR-82 / 306-2.38 / 0.00

CENTER LINE															GENERAL SPEC: 640						
															MATERIAL TYPE: 642						
CTY	ROUTE	TRUE LOG	FROM		TRUE LOG	TO		TOTAL MILES	EQUIVALENT SOLID LINE		COMMENTS										
POR	82	2.38	JCT. SR 43		5.07	EAST CORP. AURORA CITY		2.84	4.21												
POR	306	0.00	JCT. SR 43		2.49	GEAUGA COUNTY LINE		2.49	3.97												
TOTAL								5.33	8.18												
LANE LINE																					
CTY	ROUTE	TRUE LOG	FROM		TRUE LOG	TO		TOTAL MILES	4" LANE LINE		COMMENTS										
									DASHED	SOLID											
TOTAL																					
EDGE LINE																					
CTY	ROUTE	TRUE LOG	FROM		TRUE LOG	TO		WHITE EDGE LINE			YELLOW EDGE LINE			COMMENTS							
								TOTAL	HIGHWAY	RAMP	TOTAL	HIGHWAY	RAMP								
POR	82	2.38	JCT. 43		5.07	EAST CORP. AURORA CITY		5.38	5.38												
POR	306	0.00	JCT. 43		2.49	GEAUGA COUNTY LINE		4.98	4.98												
TOTAL								10.36	10.36												
AUXILIARY																					
CTY	ROUTE LOCATION		TRUE LOG	CHANNEL LINE	STOP LINE	TRANSVERSE DIAGONAL LINES		CROSS WALK LINES	WORD ON PVMT ONLY		LANE ARROWS				SYMBOL MARKINGS				ISLAND MARKING	DOTTED LINES	COMMENTS
						WHITE	YELLOW		72"	96"	LEFT	RIGHT	THRU	COMB.	RxR	SCHOOL					
																FT	FT	EACH			
POR	SR 82 @ SR 43		2.374	185	24						3	1					57				
POR	SR 82 @ HEINENS		2.498	340							3	3					57				
POR	SR 82 @ SR 306		2.611	155	48			78			4										
POR	SR 82 @ CRADDOCK SCHOOL		2.801														2				
POR	SR 82 @ EGGLESTON DR.		3.609	120			185				2						40				
POR	SR 306 @ SR 43		0.000		10																
POR	SR 306 @ SR 82		0.276	155	46		86				2	2		2							
POR	SR 306 @ HEINENS SOUTH		0.412	238							2	3					40				
POR	SR 306 @ BARRINGTON CC		0.578	280								4									

PAVEMENT MARKING SUB-SUMMARY

POR-82/ 306-2.38 / 0.00

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STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

THE FOLLOWING SUPPLEMENTAL SPECIFICATION(S):
843 DATED 04/18/2003

DESIGN SPECIFICATIONS

DESIGN SPECIFICATIONS: THIS STRUCTURE CONFORMS TO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 17TH EDITION, INCLUDING THE 2002 INTERIM SPECIFICATIONS AND THE ODOT BRIDGE DESIGN MANUAL.

EXISTING STRUCTURE VERIFICATION

EXISTING STRUCTURE VERIFICATION: DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02 AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD.

ITEM 519 - PATCHING CONCRETE STRUCTURES, AS PER PLAN

PRIOR TO THE SURFACE CLEANING SPECIFIED IN 519.04 AND WITHIN 24 HOURS OF PLACING PATCHING MATERIAL, BLAST CLEAN ALL SURFACES TO BE PATCHED INCLUDING THE EXPOSED REINFORCING STEEL. ACCEPTABLE METHODS INCLUDE HIGH-PRESSURE WATER BLASTING WITH OR WITHOUT ABRASIVES IN THE WATER, ABRASIVE BLASTING WITH CONTAINMENT, OR VACUUM ABRASIVE BLASTING.

PROPOSED WORK

- POR-306-0217
- REMOVE EXISTING ASPHALT CONCRETE OVERLAY ON THE APPROACH SLABS
 - PATCH ALL UNSOUND AREAS OF THE EXISTING CONCRETE DECK AND APPROACH SLABS
 - PLACE AN ASPHALT CONCRETE OVERLAY WITH TYPE 3 WATERPROOFING ON THE PATCHED CONCRETE DECK
 - PLACE AN ASPHALT CONCRETE OVERLAY ON THE PATCHED CONCRETE APPROACH SLABS
 - PATCH ALL UNSOUND AREAS OF THE SUBSTRUCTURE
 - SEAL ALL EXPOSED CONCRETE OF THE ABUTEMENTS, DECK EDGES, WINGWALLS, AND PIERS WITH EPOXY-URETHANE
 - CLEARING AND GRUBBING 15' AROUND ABUTMENTS FOR SEALING OPERATIONS
 - PROVIDE NEW STRUCTURE IDENTIFICATION SIGNS

- POR-82-0345
- SEALL ALL CONCRETE SURFACES OF THE ABUTMENTS

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

STRUCTURE IDENTIFICATION SIGNS

STRUCTURE IDENTIFICATION SIGNS (1-H25a) WILL BE PLACED ON EACH APPROACH OFF THE RIGHT SHOULDER, FACING TRAFFIC, AND BEHIND THE GUARDRAIL IF APPLICABLE. A QUANTITY OF ONE SIGN PER APPROACH WILL BE INSTALLED. THE SIGNS WILL BE HAVE A NON-REFLECTIVE WHITE SHEETING BACKGROUND.

THE SIGNS WILL BE MOUNTED ON NEW NO. 2 POSTS AND WILL BE INSTALLED AS PER STANDARD CONSTRUCTION DRAWING TC-41.20, MOST CURRENT REVISION. EACH POST WILL BE 7.5' IN LENGTH.

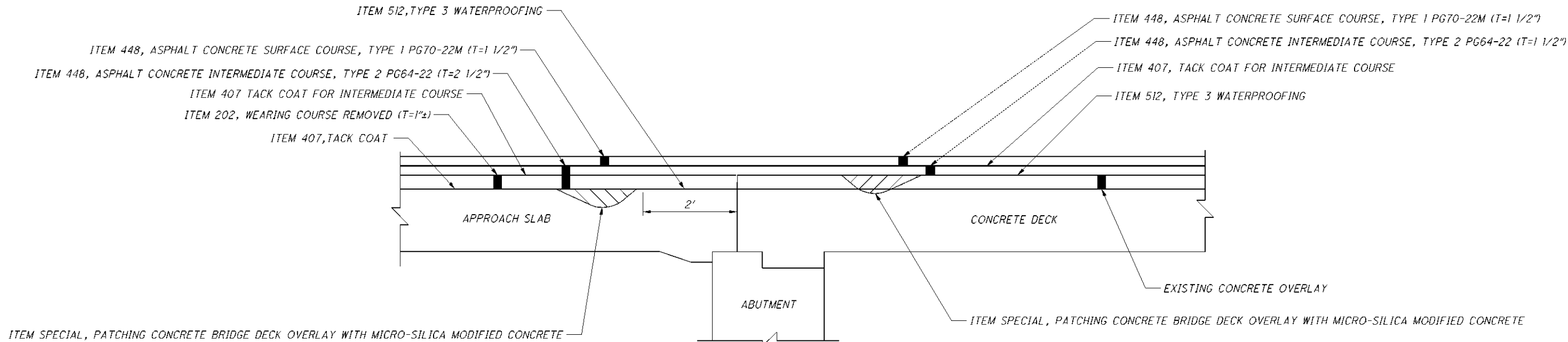
INSTALL SIGNS FOR THE FOLLOWING STRUCTURES:
POR-306-0217 (2 APPROACHES)

- THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED FOR EACH APPROACH:
- ITEM 630 - SIGN, FLAT SHEET, 730.20, 1 SQ FT
 - ITEM 630 - GROUND MOUNTED SUPPORT, NO. 2 POST, 7.5 FT
 - ITEM 630 - REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL, 1 EACH
 - ITEM 630 - REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL, 1 EACH

1 / 4		PID N . 77832		POR - 82 / 306 - 2.38 / 0.00		STRUCTURE GENERAL NOTES BRIDGE NO POR-306-0217 POR-82-0345 BRANCH CHAGRIN RIVER		DESIGNED JMW	DRAWN JMW	REVIEWED LMP	DATE 08-10-10	DESIGN AGENCY ODOT---DISTRICT 4 PRODUCTION	
12 / 15								CHECKED LMP	REVISED	STRUCTURE FILE NUMBER			

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POR-306-0217
APPROACH SHOWN, TRAILING SIMILAR

BRIDGE NUMBER	BRIDGE DECK												APPROACH SLABS												
	LENGTH (BRIDGE LIMITS)	BRIDGE WIDTH	DECK AREA	407	448	448	512	SPEC					LENGTH (APPROACH SLABS)	APPROACH SLAB WIDTH	APPROACH SLAB AREA	APPROACH (FORWARD / REAR)	202	407	407	448	448	512	SPEC		
				TACK COAT FOR INTERMEDIATE COURSE @0.04 GAL/SY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22 (T=1 1/2")	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M (T=1 1/2")	TYPE 3 WATERPROOFING	PATCHING CONCRETE BRIDGE DECK OVERLAY WITH MICRO- SILICA MODIFIED CONCRETE									WEARING COURSE REMOVED (T = 1"±)	TACK COAT @ 0.15 GAL/SY	TACK COAT FOR INTERMEDIATE COURSE @0.04 GAL/SY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22 (T=2 1/2")	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M (T=1 1/2")	TYPE 3 WATERPROOFING	PATCHING CONCRETE BRIDGE DECK OVERLAY WITH MICRO- SILICA MODIFIED CONCRETE		
				GALLON	CU YD	CU YD	SQ YD	SQ YD									SQ YD	GALLON	GALLON	CU YD	CU YD	SQ YD	SQ YD		
POR-306-0217	136.00	36.60	553.07	22.12	23.04	23.04	553.07	27.65					15.00	36.60	61.00	FWD	61.00	2.54	2.44	4.24	2.54	8.13	3.05		
													15.00	36.60	61.00	REAR	61.00	2.54	2.44	4.24	2.54	8.13	3.05		

DESIGN AGENCY
ODOT---DISTRICT 4
PRODUCTION

DATE
08-10-10

REVIEWED
LMP

DRAWN
JMW

CHECKED
LMP

STRUCTURE FILE NUMBER
6704743

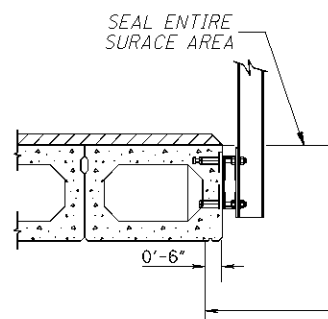
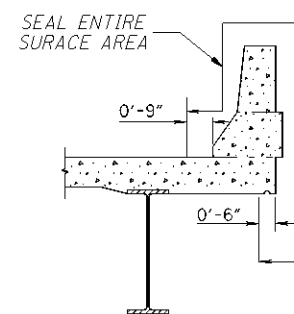
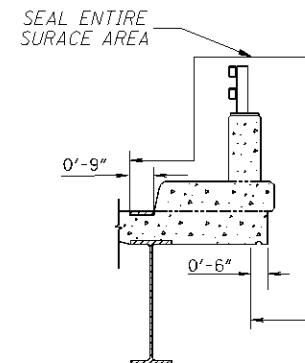
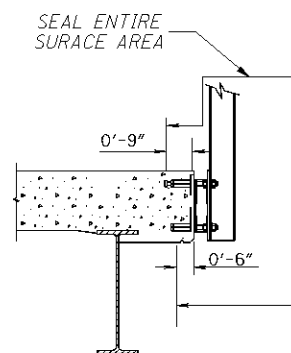
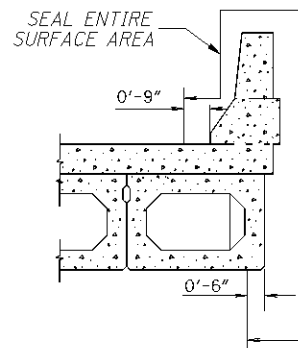
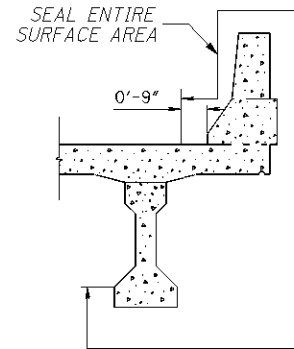
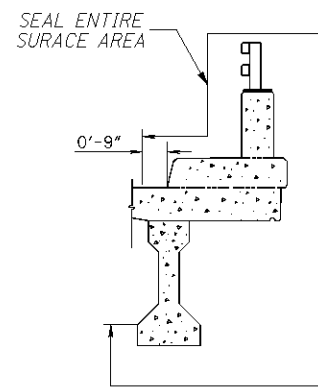
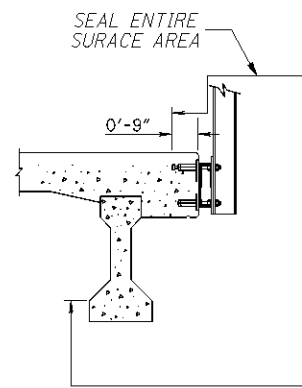
REVISED

SUPERSTRUCTURE DETAILS
BRIDGE NO. POR-306-0217
BRANCH CHAGRIN RIVER

POR - 82 / 306 - 2.38 / 0.00
PID N . 77832

3 / 4

14
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[illegible]

NOTES:

- EPOXY-URETHANE SEALER SHALL BE USED UNLESS SHOWN OTHERWISE
- DETAILS E, F, G AND H ALSO APPLY TO CONCRETE SLAB BRIDGES

SEALING OF BEAM SEATS

SEALING OF BEAM SEATS: IF THE BEAMS SEATS ARE SEALED WITH AN EPOXY OR NON-EPOXY SEALER PRIOR TO SETTING THE BEARINGS, DO NOT APPLY SEALER TO THE CONCRETE SURFACES UNDER THE PROPOSED BEARING LOCATIONS. IF THESE LOCATIONS ARE SEALED, REMOVE THE SEALER TO THE SATISFACTION OF THE ENGINEER PRIOR TO SETTING THE BEARINGS. THE DEPARTMENT WILL NOT PAY FOR THIS REMOVAL.