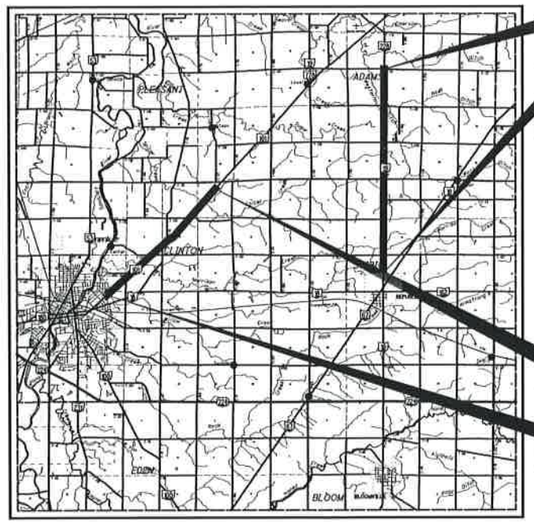


LOCATION MAP

LATITUDE: 41°7'46" LONGITUDE: 83°8'59"

SCALE IN MILES

0 1 2 3 4

N

PORTION TO BE IMPROVED
INTERSTATE HIGHWAY
FEDERAL ROUTES
STATE ROUTES
COUNTY & TOWNSHIP ROADS
OTHER ROADS

DESIGN DESIGNATION

	SEN-19-9.57	SEN-101-1.64
CURRENT ADT (2018)	880	6300
DESIGN YEAR ADT (2030)	880	6700
DESIGN HOURLY VOLUME (2030)	80	670
DIRECTIONAL DISTRIBUTION	54%	58%
TRUCKS (24 HOUR B&C)	15%	6%
DESIGN SPEED	60 MPH	VARIES
LEGAL SPEED	55 MPH	VARIES
DESIGN FUNCTIONAL CLASSIFICATION:	RURAL MAJOR COLLECTOR	VARIES
NHS PROJECT	NO	NO

DESIGN EXCEPTIONS

UNDERGROUND UTILITIES

CONTACT BOTH SERVICES TWO WORKING DAYS
BEFORE YOU DIG.

OHIO Utilities Protection SERVICE
Call Before You Dig
1-800-362-2764
(Non-members must be called directly)

OIL & GAS PRODUCERS
UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
OHIO DEPARTMENT OF TRANSPORTATION
BOWLING GREEN, OH
DISTRICT 2

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

SEN 19/101-9.57/1.64

SCIPIO TOWNSHIP
SENECA COUNTY

INDEX OF SHEETS:

PROJECT DESCRIPTION

RESURFACE SR-19 FROM REPUBLIC EAST CORP. LINE TO SR-228 AND SR-101 FROM TIFFIN EAST CORP. LINE TO 0.43 MILES WEST OF CR-38. WORK ALSO INCLUDES IMPROVEMENTS TO MULTIPLE BRIDGES AND CULVERT REPLACEMENT AT SLM 2.28.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.44 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.10 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	0.54 ACRES

2016 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THESE PLANS AND ESTIMATES.

STRUCTURES	ROADWAY
DAVID J. GECKLE E-72729 PROFESSIONAL ENGINEER STATE OF OHIO	JULIE M. FAHY E-63302 PROFESSIONAL ENGINEER STATE OF OHIO
SIGNED:	SIGNED:
DATE: 11/3/17	DATE: 11-3-2017

STANDARD CONSTRUCTION DRAWINGS

BP-3.1	7/18/14	DM-1.1	7/21/17				
BP-4.1	7/19/13						
BP-9.1	7/19/13	AS-1-15	7/17/15				
		AS-2-15	7/17/15				
HW-1.1	1/18/13						
		DBR-2-73	7/19/02				
RM-1.1	7/18/14	DBR-3-11	7/15/11				
MT-97.12	1/20/17	DS-1-92	7/18/03				
TC-64.10	1/20/17	TST-1-99	7/15/16				
TC-65.10	1/17/14						
TC-65.11	7/15/16						

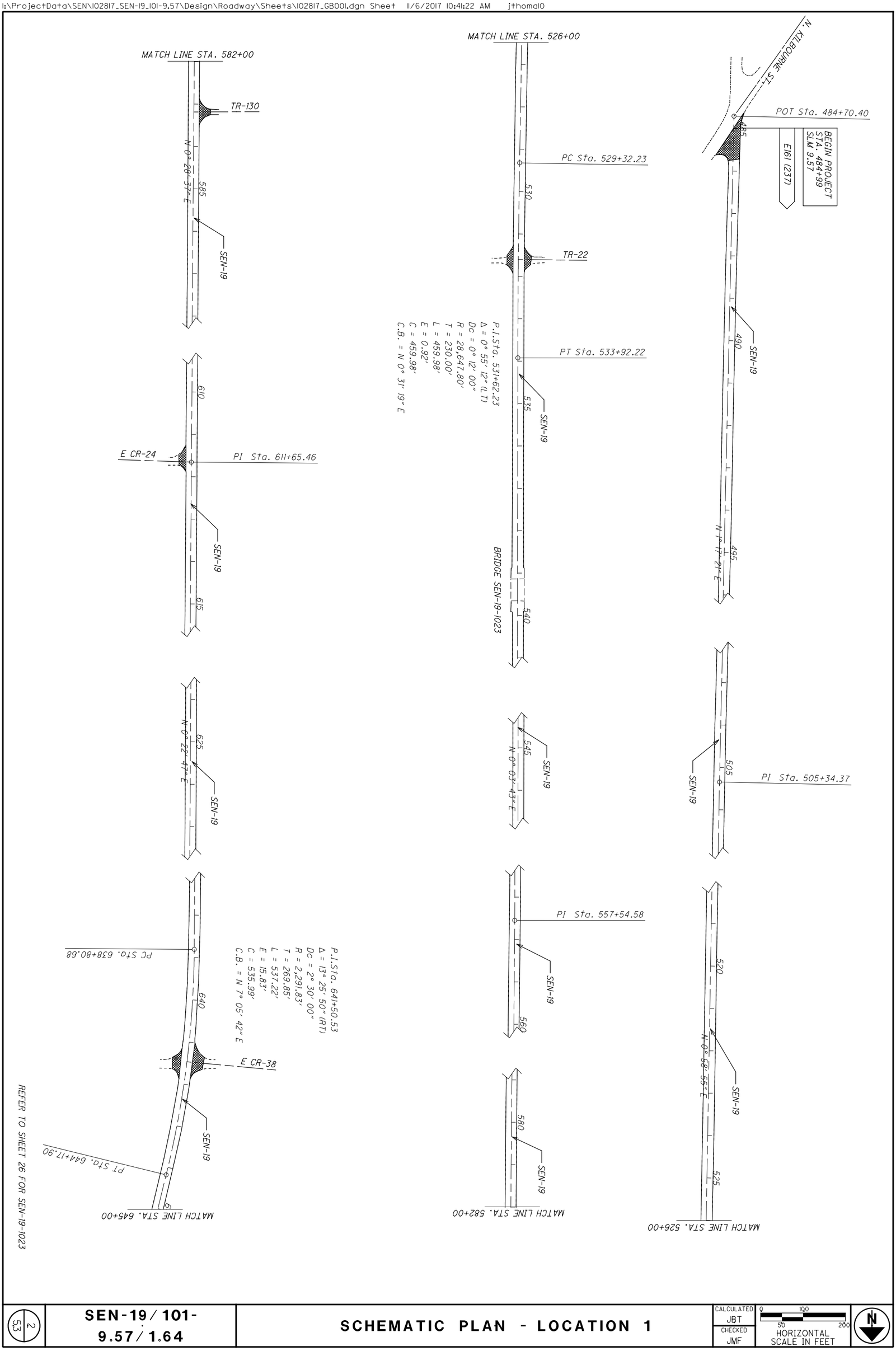
SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS
800	10/20/17	WATERWAY PERMIT 5-5-2017
832	1/17/14	
848	1/20/17	ASBESTOS SURVEY 9-13-2017
875	1/17/14	

APPROVED
DATE 11/6/17 DISTRICT DEPUTY DIRECTOR

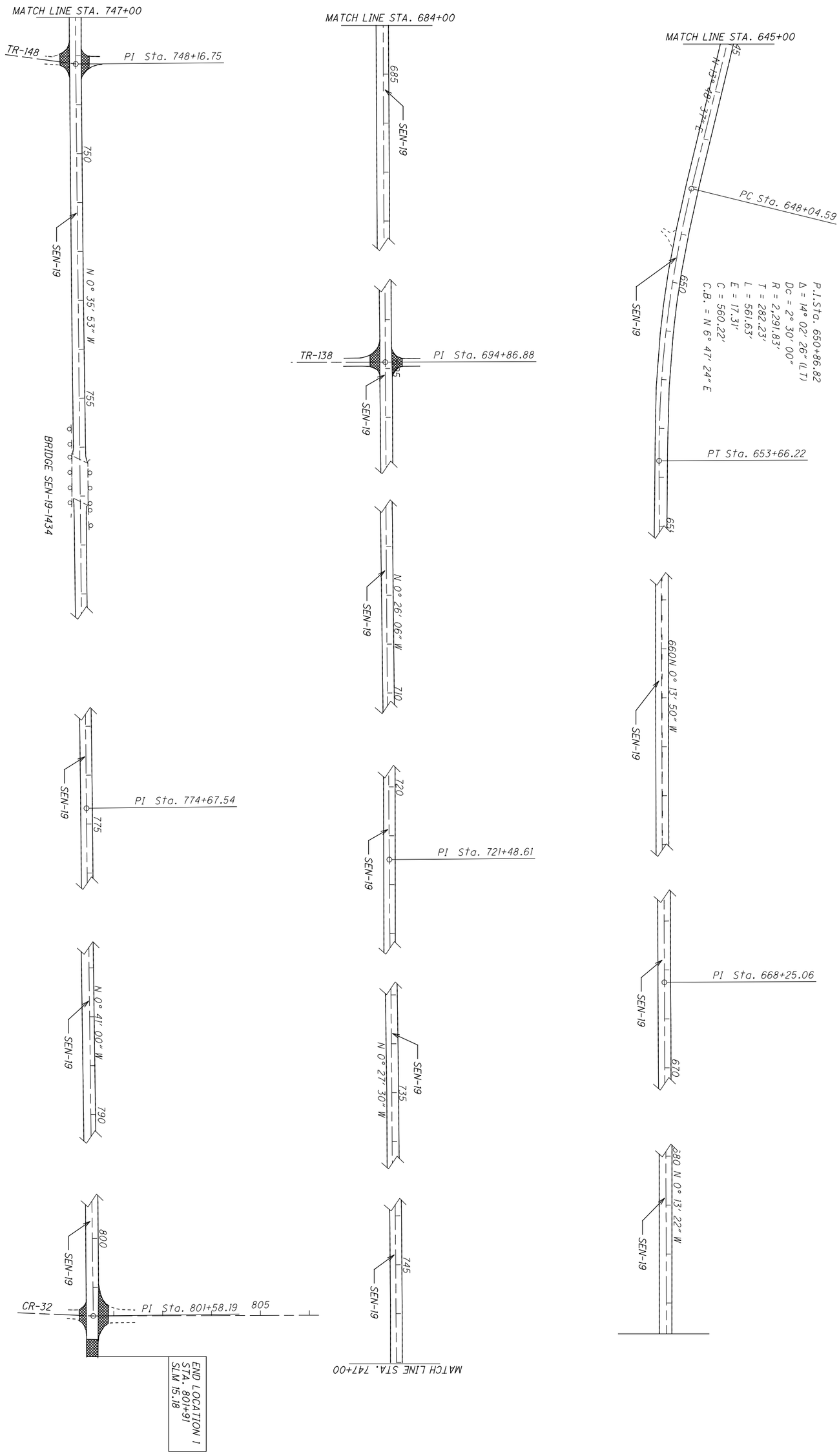
APPROVED
DATE DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. E161237
PID NO. 102817
CONSTRUCTION PROJECT NO. 00000
RAILROAD INVOLVEMENT NONE
SEN-19/101-9.57/1.64
1 / 53

I:\Projects\Data\SEN\102817_SEN-19_101-9.57\Design\Roadway\Sheets\102817_GT001.dgn Sheet 11/3/2017 1:22:22 PM jthomal0

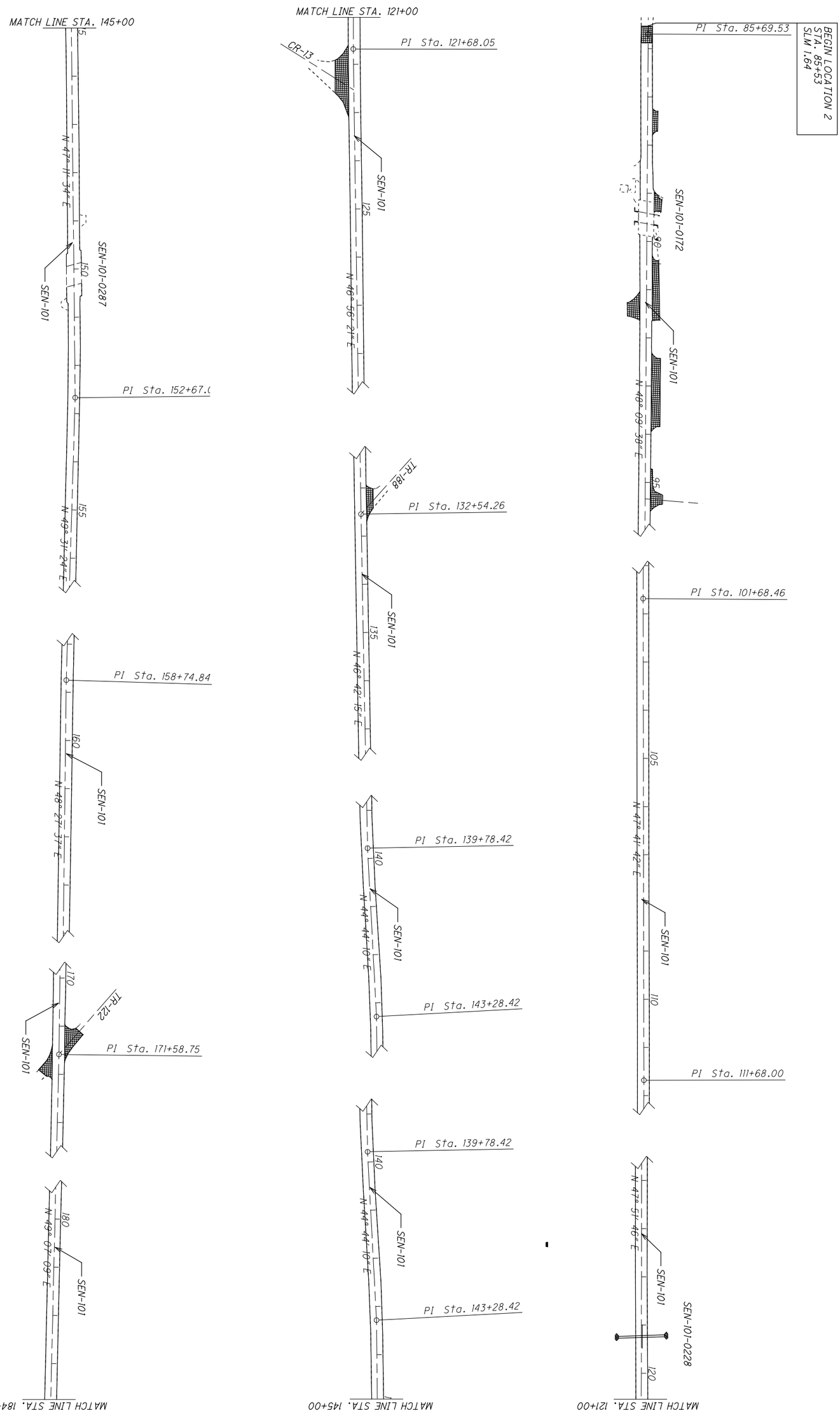


2 / 53	**SEN-19 / 101-9.57 / 1.64**	**SCHEMATIC PLAN - LOCATION 1**	CALCULATED JBT / CHECKED JMF	0 100 50 200 HORIZONTAL SCALE IN FEET



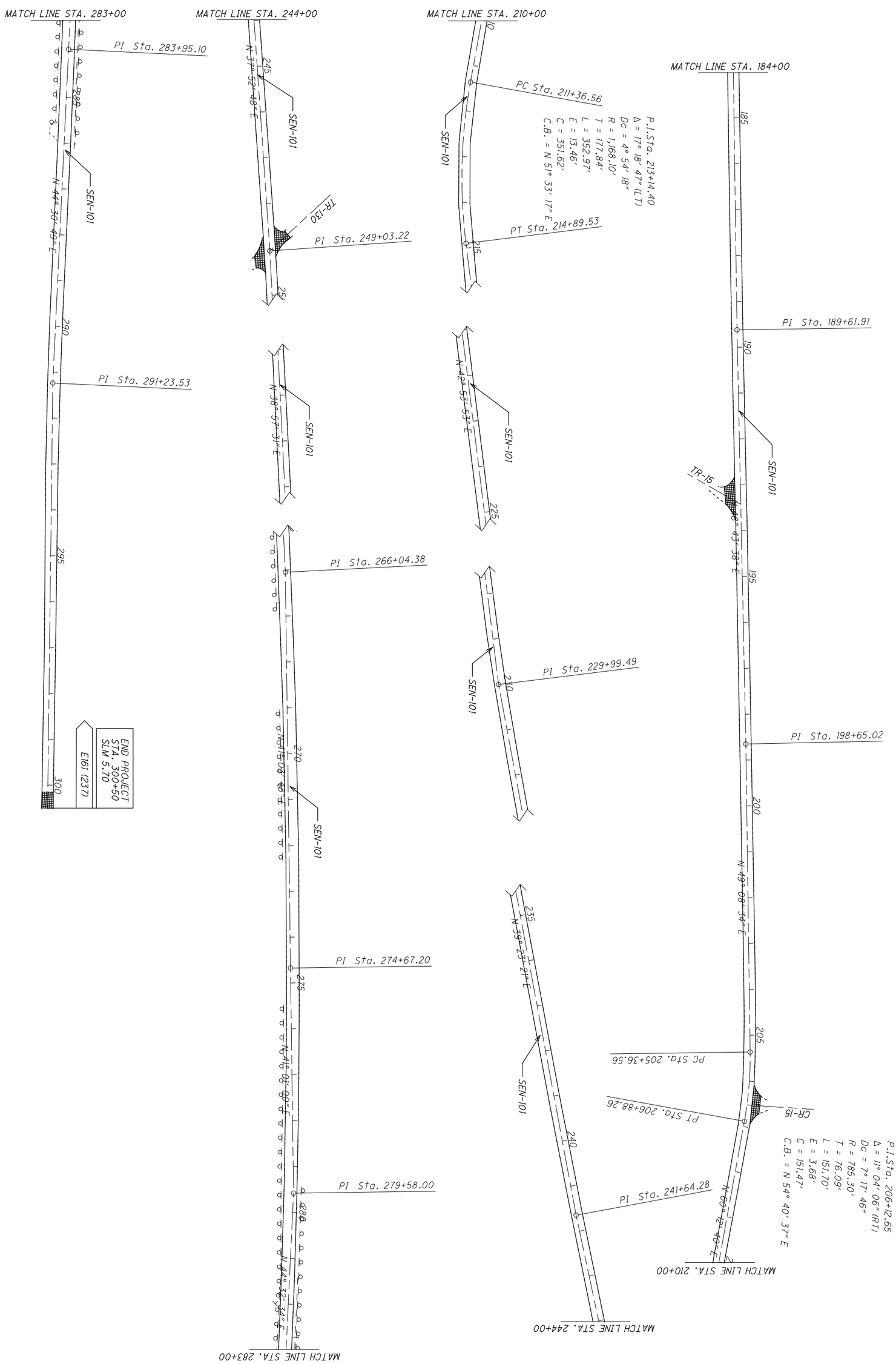
REFER TO SHEET 29 FOR SEN-19-1434

3 / 53	SEN-19 / 101-9.57 / 1.64	SCHEMATIC PLAN - LOCATION 1	CALCULATED JBT / CHECKED JMF	0 100 50 200 HORIZONTAL SCALE IN FEET



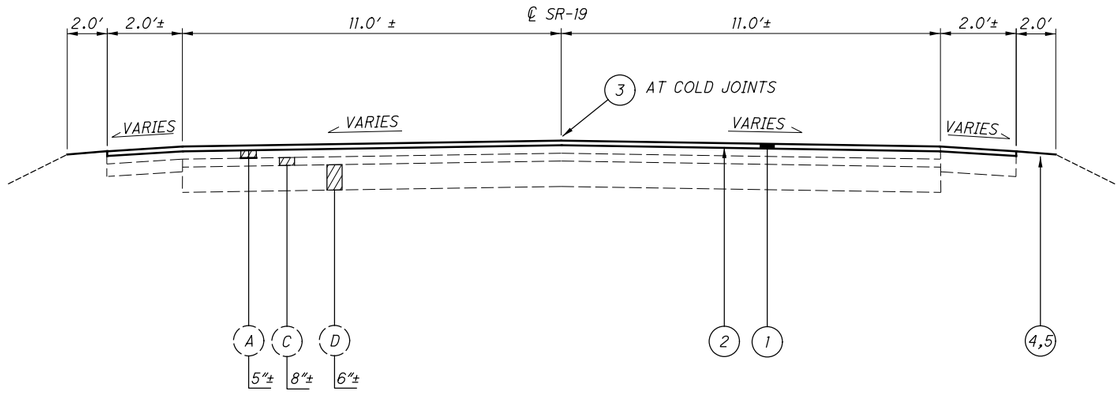
REFER TO SHEET 30 FOR SEN-101-0172
REFER TO SHEET 40 FOR SEN-101-0228
REFER TO SHEET 43 FOR SEN-101-0287

4 / 53	**SEN-19 / 101-9.57 / 1.64**	**SCHEMATIC PLAN - LOCATION 2**	CALCULATED JBT / CHECKED JMF	0 100 50 200 HORIZONTAL SCALE IN FEET



5 / 53	**SEN-19 / 101-9.57 / 1.64**	**SCHEMATIC PLAN - LOCATION 2**	CALCULATED JBT / CHECKED JMF	0 100 / 50 200 HORIZONTAL SCALE IN FEET

TYPICAL SECTION A



SECTION APPLIES: STA. 484+99 TO STA. 538+35
STA. 540+40 TO STA. 756+10
STA. 757+24 TO STA. 801+91

PROPOSED LEGEND

1. Item 424 - 1" Fine Graded Polymer Asphalt Concrete, Type B, As Per Plan
2. Item 407 - Non-Tracking Tack Coat
3. Item 875 - Longitudinal Joint Adhesive
4. Item 617 - Compacted Aggregate
5. Item 209 - Linear Grading

EXISTING LEGEND

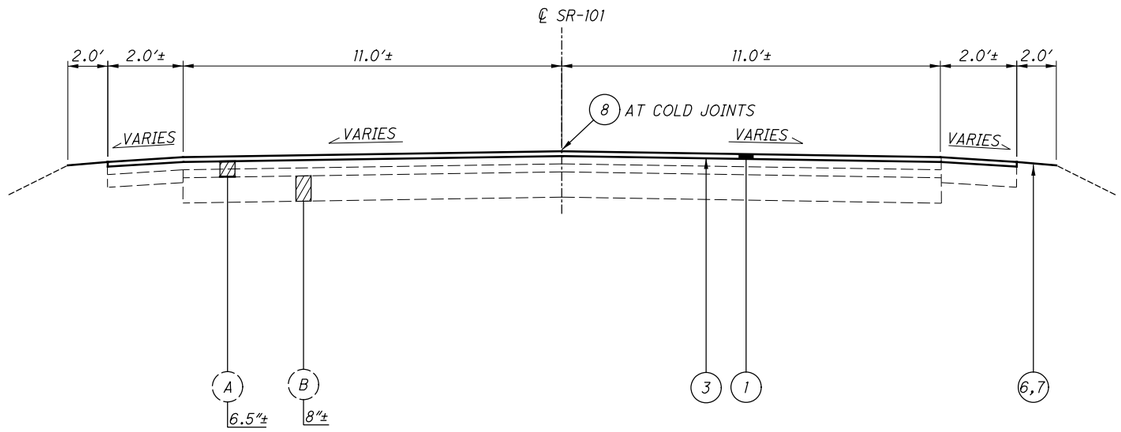
- A: EXISTING ASPHALT (THICKNESS AS SHOWN)
- B: WATERBOUND MACADAM BASE (THICKNESS AS SHOWN)
- C: BASE (THICKNESS AS SHOWN)
- D: AGGREGATE (THICKNESS AS SHOWN)
- E: UNDERDRAIN

TYPICAL SECTIONS - LOCATION 1

SEN-19 / 101-
9.57 / 1.64

6
5.3

TYPICAL SECTION B



SECTION APPLIES STA. 85+53 TO STA. 89+15.95
STA. 89+87.78 TO STA. 118+99.3
STA. 119+48.3 TO STA. 148+55
STA. 151+65 TO STA. 249+00

PROPOSED LEGEND

1. Item 424 - 1" Fine Graded Polymer Asphalt Concrete, Type B, As Per Plan
2. Item 441 - Asphalt Concrete Intermediate Course, Type 2, (448), PG64-22 (t=2")
3. Item 407 - Non-Tracking Tack Coat
4. Item 301 - Asphalt Concrete Base, PG64-22 (t=8")
5. Item 304 - Aggregate Base (t=6")
6. Item 617 - Compacted Aggregate
7. Item 209 - Linear Grading
8. Item 875 - Longitudinal Joint Adhesive

EXISTING LEGEND

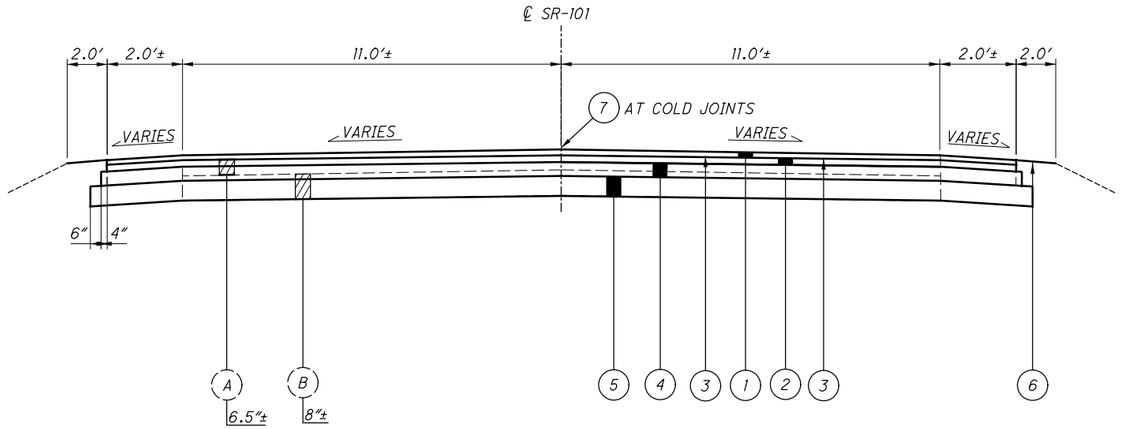
- A EXISTING ASPHALT (THICKNESS AS SHOWN)
- B WATERBOUND MACADAM BASE (THICKNESS AS SHOWN)
- C BASE (THICKNESS AS SHOWN)
- D AGGREGATE (THICKNESS AS SHOWN)
- E UNDERDRAIN

TYPICAL SECTIONS - LOCATION 2

SEN-19 / 101-
9.57 / 1.64

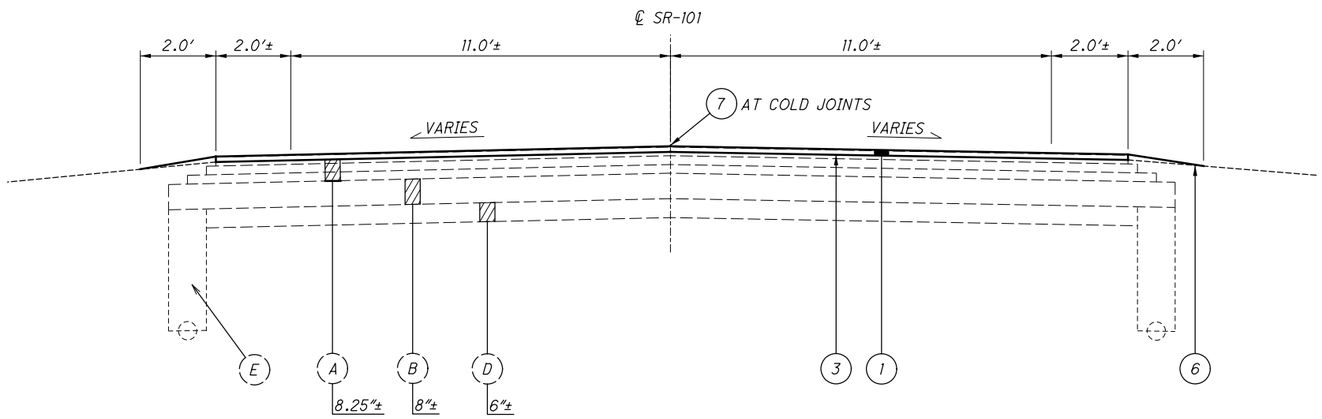
7
53

CULVERT SEN-101-0228 TYPICAL SECTION



SECTION APPLIES STA. 118+99.3 TO STA. 119+48.3

TYPICAL SECTION C



SECTION APPLIES STA. 249+00 TO STA. 300+50

TYPICAL SECTIONS - LOCATION 2

SEN-19 / 101-9.57 / 1.64

8
5.3

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

AMERICAN ELECTRIC POWER
2622 SR 100
TIFFIN, OH 44883
419-209-5583

ASPIRE ENERGY
300 TRACY BRIDGE RD.
ORRVILLE, OH 44667
330-682-7726

AT&T
130 N. ERIE ST.
TOLEDO, OH 43624
419-245-7304

AQUA AMERICA
1630 SOUTH SR 53
TIFFIN, OH 44883
877-987-2782

CENTURYLINK
175 ASHLAND RD.
MANSFIELD, OH 44902
419-755-7183

NORTH CENTRAL ELECTRIC CO-OP
P.O. BOX 475
ATTICA, OH 44807
800-426-3072

CITY OF TIFFIN
51 E. MARKET ST.
TIFFIN, OH 44883
419-448-5425

COLUMBIA GAS OF OHIO
2901 E. MANHATTAN BLVD.
TOLEDO, OH 43611
419-539-6066

FRONTIER
300 W GYPSY LANE RD
BOWLING GREEN, OH 43402
419-354-9452

CHARTER
3760 INTERCHANGE DR.
COLUMBUS, OH 43204
614-255-6340

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

SURVEYING PARAMETERS

USE THE FOLLOWING PROJECT CONTROL, VERTICAL POSITIONING, AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

VERTICAL POSITIONING

ORTHOMETRIC HEIGHT DATUM: NAVD88 (ODOT VRS DERIVED)
GEOID: 2012A

HORIZONTAL POSITIONING

REFERENCE FRAME: NAD 83 2011
ELLIPSOID: GRS 80
MAP PROJECTION: LAMBERT CONFORMAL CONIC
COORDINATE SYSTEM: OHIO STATE PLANE NORTH
COMBINED SCALE FACTOR: GRID=1.000000000

USE THE POSITIONING METHODS AND MONUMENT TYPE USED IN THE ORIGINAL SURVEY TO RESTORE ALL MONUMENTS RELATED TO PRIMARY PROJECT CONTROL THAT ARE DAMAGED OR DESTROYED BY CONSTRUCTION ACTIVITIES. RESTORE THE DAMAGED OR DESTROYED MONUMENTS IN ACCORDANCE WITH CMS 623.

UNITS ARE IN U.S. SURVEY FEET. USE THE FOLLOWING CONVERSION FACTOR: 1 METER = 3.280833333 U.S. SURVEY FEET.

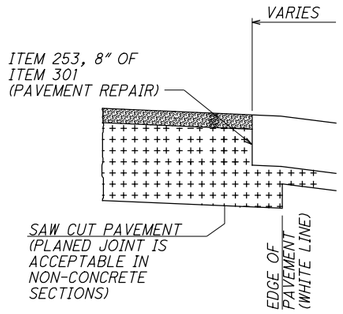
ITEM 253, PAVEMENT REPAIR:

ALL EXISTING PAVEMENT AREAS WHICH WILL BE IN CONTACT WITH THE PAVEMENT REPAIR SHALL BE COATED WITH PG GRADE LIQUID ASPHALT (SIDES AND BOTTOM) AT AN APPLICATION RATE OF 0.25 GAL. PER SQ YD.)

THE FOLLOWING ESTIMATED QUANTITY IS TO BE USED FOR 8" PAVEMENT REPAIRS AS DIRECTED BY THE ENGINEER AND BASED ON THE PERCENTAGE SHOWN BELOW.

SR 19 - 10% = 1969 CU YD
SR 101 - 3% = 404 CU YD

QUANTITY CARRIED TO THE GENERAL SUMMARY.



NOTE: THE ENGINEER SHALL FIELD VERIFY ALL LOCATIONS PRIOR TO THE BEGINNING OF WORK. ANY ADJUSTMENTS NECESSARY SHALL BE AS DIRECTED BY THE ENGINEER.

TRAFFIC CONTROL QUANTITIES
THE FOLLOWING ARE FOR INFORMATION ONLY:

PAVEMENT MARKINGS

THE CONTRACTOR WILL BE PROVIDED THE "NO PASSING ZONE LOG" FOR THE CENTER LINE PAVEMENT MARKING UPON REQUEST. THE FOLLOWING QUANTITIES ARE FOR INFORMATION ONLY:

YELLOW CENTER LINE

DASHED	5.67 MILE
DASHED SOLID	3.38 MILE
DOUBLE SOLID	1.05 MILE

RAISED PAVEMENT MARKERS

TWO WAY YELLOW/YELLOW 645 EACH

ITEM	QTY	UNIT	DESCRIPTION
621	645	EACH	RPM
621	645	EACH	RAISED PAVEMENT MARKER REMOVED
642	20.20	MILE	EDGE LINE, 6"
642	10.10	MILE	CENTER LINE

ALL TRAFFIC CONTROL QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY.

ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN

PER CMS 424.08, 448 DENSITY APPLIES TO THIS PROJECT. DENSITY WILL BE TESTED ACCORDING TO SUPPLEMENT 1055 PER CMS 448.02. THE DISINCENTIVE PORTION OF S-1055 (TABLE 1055.01-1 AND TABLE 1055.04) WILL BE WAIVED PROVIDING THAT THE CONTRACTOR MAKES EVERY EFFORT TO OBTAIN DENSITY AND DOES NOT USE VIBRATORY ROLLERS.

AN ESTIMATED QUANTITY OF 150 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO ACCOUNT FOR SURFACE IRREGULARITIES.

RUMBLE STRIPE REMOVAL BEFORE PAVING

RUMBLE STRIPES WILL BE PLANED WITH ITEM 441 THE QUANTITIES FOR PLANING AND PAVING THE RUMBLE STRIPES ARE PROVIDED BELOW. QUANTITIES ARE BASED ON 2' WIDE MILL. QUANTITIES TO BE CARRIED TO THE GENERAL SUMMARY.

SR 101 EXISTING LENGTH OF RUMBLE STRIPE:
LENGTH = 40,213 FT
TOTAL 40,213 FT

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1½"
SR 101 8937 SY
ITEM 254 TOTAL = 8937 SY

ITEM 407 - NON TRACKING TACK COAT
SR 101 760 GAL
ITEM 407 TOTAL = 760 GAL

ITEM 441- ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22, 1½"
SR 101 376 CY
ITEM 441 TOTAL = 376 CY

ENVIRONMENTAL

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PREVENT ANY AND ALL MATERIAL FROM GOING OFF THE EDGE OF THE BRIDGE DECK(S) AND EDGE OF THE CULVERT(S) DURING ALL CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL IMMEDIATELY REMOVE ANY MATERIAL THAT FALLS INTO THE ROADSIDE DITCHES, STREAMS, WETLANDS, OR OTHER WATERS THROUGH NON-MECHANICAL MEANS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR WORK IN OR STORE EQUIPMENT AND/OR MATERIALS IN ANY WETLANDS, STREAMS, OR OTHER WATERS. NO WORK IS PERMITTED BELOW THE TOP OF BANK OF ANY STREAM, EXCEPT THE SEN-101-2.28 CULVERT OVER STREAM 1 (UNNAMED TRIBUTARY TO MORRISON CREEK).

THE PROJECT REQUIRES A WATERWAY PERMIT FOR WORK WITHIN STREAM 1 (TRIBUTARY OF MORRISON CREEK) (STA 119+23.8) WHICH IS ATTACHED TO THE PLANS AS SPECIAL PROVISIONS THAT SHALL BE FOLLOWED THROUGHOUT CONSTRUCTION BY THE CONTRACTOR.

THIS PROJECT IS LOCATED IN OR NEAR PUBLIC DRINKING WATER SOURCES. IN ORDER TO MINIMIZE THE POTENTIAL FOR A RELEASE IN THIS SENSITIVE AREA DO NOT PERFORM EQUIPMENT FUELING WITHIN THE LOCATIONS IDENTIFIED IN THE IN THE TABLE BELOW. IF REFUELING OF IMMOBILE EQUIPMENT IS NECESSARY WITHIN THESE LIMITS, PROVIDE SECONDARY CONTAINMENT WITH ENOUGH CAPACITY TO COMPLETELY CONTAIN AND COLLECT ALL POTENTIAL LIQUID WASTES IN THE EVENT OF A SPILL. DO NOT PERFORM MAINTENANCE ACTIVITIES ASSOCIATED WITH ANY VISCOUS MATERIAL THAT HAS POTENTIAL TO CONTAMINATE THE GROUNDWATER. ALL EQUIPMENT OR LIQUID CONTAINERS SHALL BE REGULARLY CHECKED FOR LEAKS PRIOR TO ENTERING THE RESTRICTED AREAS. THE CONTRACTOR SHALL DEVELOP A SPILL PREVENTION CONTROL AND COUNTERMEASURE PLAN (SPCC) WHICH SHALL ALSO INCLUDE ALL AREAS OF FUEL STORAGE, EQUIPMENT MAINTENANCE, AND SPILL KITS. ALL AREAS UTILIZED BY THE CONTRACTOR NOT INCLUDED WITHIN THE TABLE BELOW AND NOT IN THE PROJECT LIMITS SHALL BE ASSESSED FOR POTENTIAL GROUNDWATER CONTAMINATION AND BE INDICATED ON THE SPILL PREVENTION CONTROL AND COUNTERMEASURES PLAN. THE CONTRACTOR SHALL IMMEDIATELY TAKE STEPS TO MITIGATE ANY EVENT, SUCH AS A SPILL OF FUELS, OILS, OR CHEMICALS. ANY SUCH SPILL OR EVENT SHALL BE REPORTED IMMEDIATELY TO THE EMERGENCY CONTACT LISTED BELOW FOR EACH DRINKING WATER SOURCE. IF THE SPILL IS A REPORTABLE AMOUNT, THE CONTRACTOR SHOULD CONTACT SENECA COUNTY EMERGENCY MANAGEMENT (419-447-0266) AND OHIO EPA ENVIRONMENTAL RESPONSE AND REVITALIZATION (419-373-3031 OR 800-282-9378) FOR CLEANUP OF THE SPILL.

DRINKING WATER SOURCE	RESTRICTED REFUELING AND MAINTENANCE AREA	EMERGENCY CONTACT INFORMATION
VILLAGE OF CLYDE	SR-19 FROM SLM 14.16 (TR-148) TO 14.46	419-447-0266

CALCULATED JBT
CHECKED JMF

GENERAL NOTES

SEN-19 / 101 - 9.57 / 1.64

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING
EASTER	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF $50 FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614, MAINTAINING TRAFFIC (LANE CLOSURE/REDUCTION REQUIRED)

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

STRUCTURE NO. SEN-101-0172

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED 45 CONSECUTIVE CALENDAR DAYS. WHEN THROUGH TRAFFIC SHALL BE DETOURED AS SHOWN BELOW. DAMAGES IN THE AMOUNT OF $3000 SHALL BE ASSESSED FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

STRUCTURE NO. SEN-101-0228

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED 30 CONSECUTIVE CALENDAR DAYS. WHEN THROUGH TRAFFIC SHALL BE DETOURED AS SHOWN BELOW. DAMAGES IN THE AMOUNT OF $3000 SHALL BE ASSESSED FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

* THE CLOSURE FOR SEN-101-0172 AND THE CLOSURE FOR SEN-101-0228 SHALL NOT OVERLAP IN DURATION.

DETOUR ROUTE AS FOLLOWS:
1. SR-101 TO SR-18
2. SR-18 TO SR-19
3. SR-19 TO SR-101

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13),SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
CLOSURES	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
CLOSURES	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES		14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

THE PROJECT ENGINEER WILL FORWARD ALL INFORMATION TO THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICER (PIO) BY PHONE AT: (419) 373-4428 OR EMAIL AT: d02.pio@dot.ohio.gov
DISTRICT PERMIT SECTION BY PHONE AT: (419) 373-4301 OR EMAIL AT: d02.permits@dot.ohio.gov
CENTRAL OFFICE SPECIAL HAULING PERMITS SECTION BY PHONE AT: (614) 351-2300 OR EMAIL AT: hauling.permits@dot.ohio.gov

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614, WORK ZONE CENTER LINE, CLASS I

SR-19	12.0 MILE
SR-101	8.20 MILE

ITEM 614, WORK ZONE CENTER LINE, CLASS II

SR-19	6.0 MILE
SR-101	4.1 MILE

ITEM 614, WORK ZONE MARKING SIGN

SR-19	16 EACH
SR-101	12 EACH

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

CALCULATED JBT
CHECKED JMF

MAINTENANCE OF TRAFFIC NOTES

SEN-19 / 101-
9.57 / 1.64

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53

SHEET NUM.									PART.					ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
8	9	12	13	44					01/STR/PV	02/STR/BR	03/S<2/PV	04/S<2/BR	05/STR/CV						
																		ROADWAY	
		1,440	2,032						2,350		1,122			202	23500	3,472	SY	WEARING COURSE REMOVED	
		12	8						18		2			209	60500	20	MILE	LINEAR GRADING	
				2									2	623	38500	2	EACH	MONUMENT ASSEMBLY	
																		EROSION CONTROL	
				237									237	659	00300	237	CY	TOPSOIL	
				2,129.6									2,129.6	659	10000	2,129.6	SY	SEEDING AND MULCHING	
				106.48									106.48	659	14000	106.48	SY	REPAIR SEEDING AND MULCHING	
				0.29									0.29	659	20000	0.29	TON	COMMERCIAL FERTILIZER	
				0.44									0.44	659	31000	0.44	ACRE	LIME	
				11.5									11.5	659	35000	11.5	MGAL	WATER	
																		DRAINAGE	
				30									30	611	05200	30	FT	12" CONDUIT, TYPE F, DRAIN TILE	
																		PAVEMENT	
2,373									2,373					253	02000	2,373	CY	PAVEMENT REPAIR	
8,937									8,937					254	01000	8,937	SY	PAVEMENT PLANING, ASPHALT CONCRETE(1-1/2")	
760		7,607	5,040						12,564		843			407	20000	13,407	GAL	NON-TRACKING TACK COAT	
150		2,446	1,647						3,967		276			424	12001	4,243	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	11
376		40	52						439		29			441	50000	468	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	
		387	261						603		45			617	10100	648	CY	COMPACTED AGGREGATE	
		5,217	3,512						8,123		606			875	10000	8,729	LB	LONGITUDINAL JOINT ADHESIVE (L/6)	
																		TRAFFIC CONTROL	
645									645					621	00100	645	EACH	RPM	
645									645					621	54000	645	EACH	RAISED PAVEMENT MARKER REMOVED	
20.2									20.2					642	00094	20.2	MILE	EDGE LINE, 6"	
10.1									10.1					642	00290	10.1	MILE	CENTER LINE	
																		STRUCTURE REPAIRS	
																		FOR SEN-101-0172 ESTIMATED QUANTITIES	31
																		FOR SEN-101-0228 ESTIMATED QUANTITIES	44
																		FOR SEN-101-0287 ESTIMATED QUANTITIES	41
																		FOR SEN-19-1023 ESTIMATED QUANTITIES	27
																		FOR SEN-19-1434 ESTIMATED QUANTITIES	29
																		MAINTENANCE OF TRAFFIC	
	10								10					614	12460	10	EACH	WORK ZONE MARKING SIGN	
	5								5					614	12500	5	EACH	REPLACEMENT SIGN	
	5								5					614	12600	5	EACH	REPLACEMENT DRUM	
	20.2								20.2					614	21000	20.2	MILE	WORK ZONE CENTER LINE, CLASS I	
	10.1								10.1					614	21400	10.1	MILE	WORK ZONE CENTER LINE, CLASS II	
																		INCIDENTALS	
									LS					614	11000	LS		MAINTAINING TRAFFIC	
									LS					623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
									LS					624	10000	LS		MOBILIZATION	
									3,000					832	30000	3,000	EACH	EROSION CONTROL	

CALCULATED JBT
CHECKED JMF

GENERAL SUMMARY

SEN-19 / 101-
9.57 / 1.64

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5.3

STATION RANGE			TYPICAL SECTION/SIDE	DISTANCE (D)	AVERAGE WIDTH (W)	CADD GENERATED AREA	407 NON-TRACKING TACK COAT		424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	441 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22		617 COMPACTED AGGREGATE		875 LONGITUDINAL JOINT ADHESIVE (L/6)		202 WEARING COURSE REMOVED		209 LINEAR GRADING
				FT	FT	SY	GAL		CY	CY		CY		LB		SY		MILE
SEN-19 MAINLINE																		
485+74.00		496+00.00	A	1026.00	25.83	2945.05	250.33		81.81			12.67		171.00				0.39
496+00.00		506+00.00	A	1000.00	25.84	2870.85	244.02		79.75			12.35		166.67				0.38
506+00.00		516+00.00	A	1000.00	26.03	2892.24	245.84		80.34			12.35		166.67				0.38
516+00.00		526+00.00	A	1000.00	26.43	2937.08	249.65		81.59			12.35		166.67				0.38
526+00.00		536+00.00	A	1000.00	26.38	2931.28	249.16		81.42			12.35		166.67				0.38
536+00.00		538+35.00	A	235.00	26.52	692.43	58.86		19.23			2.90		39.17				0.09
BRIDGE SEN-19-1023																		
540+40.00		550+00.00	A	960.00	25.90	2763.05	234.86		76.75			11.85		160.00				0.36
550+00.00		560+00.00	A	1000.00	25.72	2857.24	242.87		79.37			12.35		166.67				0.38
560+00.00		570+00.00	A	1000.00	25.50	2833.37	240.84		78.70			12.35		166.67				0.38
570+00.00		580+00.00	A	1000.00	25.52	2835.22	240.99		78.76			12.35		166.67				0.38
580+00.00		590+00.00	A	1000.00	25.68	2853.64	242.56		79.27			12.35		166.67				0.38
590+00.00		600+00.00	A	1000.00	25.86	2873.05	244.21		79.81			12.35		166.67				0.38
600+00.00		610+00.00	A	1000.00	25.88	2876.04	244.46		79.89			12.35		166.67				0.38
610+00.00		620+00.00	A	1000.00	26.04	2893.61	245.96		80.38			12.35		166.67				0.38
620+00.00		630+00.00	A	1000.00	25.74	2860.41	243.13		79.46			12.35		166.67				0.38
630+00.00		640+00.00	A	1000.00	26.03	2892.56	245.87		80.35			12.35		166.67				0.38
640+00.00		650+00.00	A	1000.00	26.47	2940.96	249.98		81.69			12.35		166.67				0.38
650+00.00		660+00.00	A	1000.00	25.94	2882.25	244.99		80.06			12.35		166.67				0.38
660+00.00		670+00.00	A	1000.00	25.93	2881.49	244.93		80.04			12.35		166.67				0.38
670+00.00		680+00.00	A	1000.00	25.83	2869.53	243.91		79.71			12.35		166.67				0.38
680+00.00		690+00.00	A	1000.00	25.62	2847.17	242.01		79.09			12.35		166.67				0.38
690+00.00		700+00.00	A	1000.00	24.59	2731.70	232.19		75.88			12.35		166.67				0.38
700+00.00		710+00.00	A	1000.00	23.91	2657.22	225.86		73.81			12.35		166.67				0.38
710+00.00		720+00.00	A	1000.00	23.82	2646.48	224.95		73.51			12.35		166.67				0.38
720+00.00		730+00.00	A	1000.00	23.52	2613.11	222.11		72.59			12.35		166.67				0.38
730+00.00		740+00.00	A	1000.00	24.17	2685.68	228.28		74.60			12.35		166.67				0.38
740+00.00		750+00.00	A	1000.00	24.07	2673.97	227.29		74.28			12.35		166.67				0.38
750+00.00		756+09.88	A	609.88	24.37	1651.44	140.37		45.87			7.53		101.65				0.23
BRIDGE SEN-19-1434																		
757+24.38		760+00.00	A	275.62	24.58	752.82	63.99		20.91			3.40		45.94				0.10
760+00.00		770+00.00	A	1000.00	24.16	2683.99	228.14		74.56			12.35		166.67				0.38
770+00.00		780+00.00	A	1000.00	23.64	2626.61	223.26		72.96			12.35		166.67				0.38
780+00.00		790+00.00	A	1000.00	24.32	2702.24	229.69		75.06			12.35		166.67				0.38
790+00.00		800+00.00	A	1000.00	24.69	2743.38	233.19		76.21			12.35		166.67				0.38
800+00.00		801+91.00	A	191.00	30.92	656.15	55.77		18.23			2.36		31.83				0.07
INTERSECTIONS																		
SR-18			C			321.24	27.31			8.92						321.24		
E. TR-122			L			63.57	5.40			1.77						63.57		
E. TR-122			R			66.83	5.68			1.86						66.83		
E. TR-130			L			84.39	7.17			2.34						84.39		
E. CR-24			R			74.33	6.32			2.06						74.33		
CR-38			L			141.84	12.06			3.94						141.84		
CR-38			R			91.05	7.74			2.53						91.05		
TR-138			L			66.49	5.65			1.85						66.49		
TR-138			R			85.37	7.26			2.37						85.37		
E. TR-148			L			67.76	5.76			1.88						67.76		
E. TR-148			R			84.21	7.16			2.34						84.21		
SR-19			L			143.18	12.17			3.98						143.18		
CR-32			R			59.38	5.05			1.65						59.38		
N. SR-228			C			90.11	7.66			2.50						90.11		
SUBTOTALS							7606.91		2445.93	39.99		386.39		5216.25		1439.75		11.86
TOTALS CARRIED TO GENERAL SUMMARY							7607		2446	40		387		5217		1440		12

CALCULATED JBT
CHECKED JMF

PAVEMENT SUBSUMMARY - LOCATION 1

SEN-19 / 101-9.57 / 1.64

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5.3

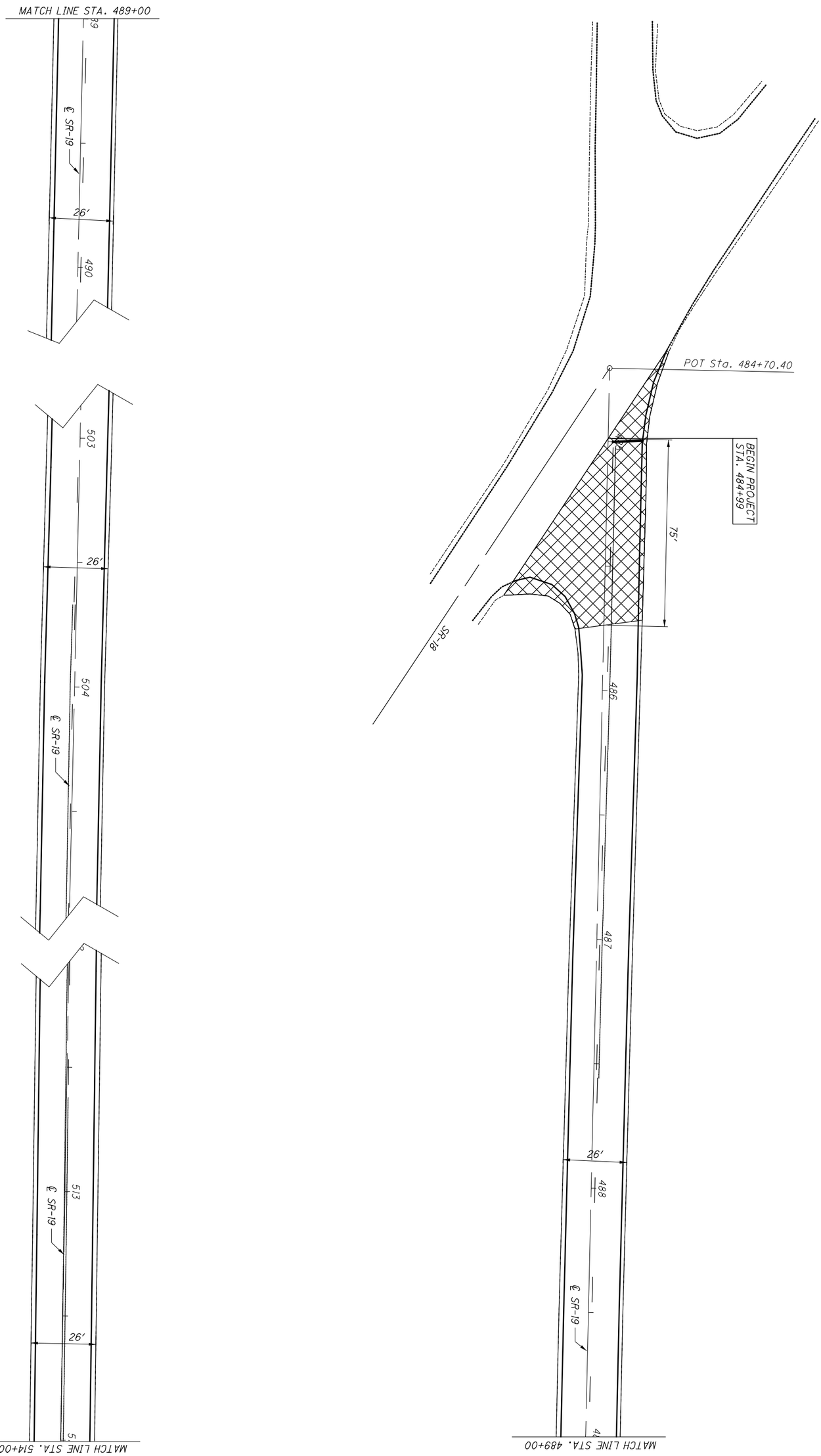
STATION RANGE			TYPICAL SECTION/SIDE	DISTANCE (D)	AVERAGE WIDTH (W)	CADD GENERATED AREA	407 NON-TRACKING TACK COAT		424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	441 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22		617 COMPACTED AGGREGATE		875 LONGITUDINAL JOINT ADHESIVE (L/6)		202 WEARING COURSE REMOVED		209 LINEAR GRADING
				FT	FT	SY	GAL		CY	CY		CY		LB		SY		MILE
SEN-101 MAINLINE																			
85+53.00		85+88.00	B	35.00	23.49	91.35	7.76		2.54			0.43		5.83		91.35		0.01
85+88.00		89+15.95	B	327.95	24.95	909.27	77.29		25.26			4.05		54.66				0.12
BRIDGE SEN-101-0172																			
89+87.78		100+00.00	B	1012.22	24.70	2777.53	236.09		77.15			12.50		168.70				0.38
100+00.00		110+00.00	B	1000.00	24.36	2706.90	230.09		75.19			12.35		166.67				0.38
110+00.00		118+99.30	B	899.30	24.65	2463.46	209.39		68.43			11.10		149.88				0.34
CULVERT SEN-101-0228																			
119+48.30		123+02.40	B	354.10	24.54	965.61	82.08		26.82			4.37		59.02				0.13
123+02.40		130+00.00	B	697.60	24.74	1917.37	162.98		53.26			8.61		116.27				0.26
130+00.00		140+00.00	B	1000.00	24.75	2749.97	233.75		76.39			12.35		166.67				0.38
140+00.00		148+55.00	B	855.00	28.03	2663.29	226.38		73.98			10.56		142.50				0.32
BRIDGE SEN-101-0287																			
151+65.00		160+00.00	B	835.00	25.44	2360.26	200.62		65.56			10.31		139.17				0.32
160+00.00		170+00.00	B	1000.00	24.90	2766.60	235.16		76.85			12.35		166.67				0.38
170+00.00		180+00.00	B	1000.00	24.88	2764.16	234.95		76.78			12.35		166.67				0.38
180+00.00		190+00.00	B	1000.00	24.62	2735.93	232.55		76.00			12.35		166.67				0.38
190+00.00		200+00.00	B	1000.00	24.71	2745.05	233.33		76.25			12.35		166.67				0.38
200+00.00		210+00.00	B	1000.00	24.66	2740.53	232.95		76.13			12.35		166.67				0.38
210+00.00		220+00.00	B	1000.00	24.73	2747.77	233.56		76.33			12.35		166.67				0.38
220+00.00		230+00.00	B	1000.00	24.81	2756.81	234.33		76.58			12.35		166.67				0.38
230+00.00		240+00.00	B	1000.00	24.58	2731.08	232.14		75.86			12.35		166.67				0.38
240+00.00		250+00.00	B,C	1000.00	24.50	2722.28	231.39		75.62			12.35		166.67				0.38
250+00.00		260+00.00	C	1000.00	24.62	2735.74	232.54		75.99			12.35		166.67				0.38
260+00.00		270+00.00	C	1000.00	26.80	2977.39	253.08		82.71			12.35		166.67				0.38
270+00.00		280+00.00	C	1000.00	27.99	3110.20	264.37		86.39			12.35		166.67				0.38
280+00.00		290+00.00	C	1000.00	28.25	3138.57	266.78		87.18			12.35		166.67				0.38
290+00.00		300+15.00	C	1015.00	25.82	2911.44	247.47		80.87			12.53		169.17				0.38
300+15.00		300+50.00	C	35.00	24.65	95.87	8.15		2.66			0.43		5.83		95.87		0.01
INTERSECTIONS																			
Clinton Resort W Dr.			L			55.22				1.53						55.22		
Clinton Resort E Dr.			R			55.10				1.53						55.10		
Clinton Mobile Home Park Office			L			198.53				5.51						198.53		
Hi-Way Dr.			R			115.81				3.22						115.81		
McClintock Inurance Agency Dr.			L			300.34				8.34						300.34		
CR-13			R			305.65				8.49						305.65		
TR-188			L			86.44				2.40						86.44		
TR-122			L			149.45				4.15						149.45		
TR-122			R			91.06				2.53						91.06		
TR-15			R			144.87				4.02						144.87		
CR-15			L			113.91				3.16						113.91		
TR-130			L			115.12				3.20						115.12		
TR-130			R			113.05				3.14						113.05		
SUBTOTALS							5039.18		1646.79	51.24		260.08		3511.03		2031.77		7.98
TOTALS CARRIED TO GENERAL SUMMARY							5040		1647	52		261		3512		2032		8

CALCULATED JBT
CHECKED JMF

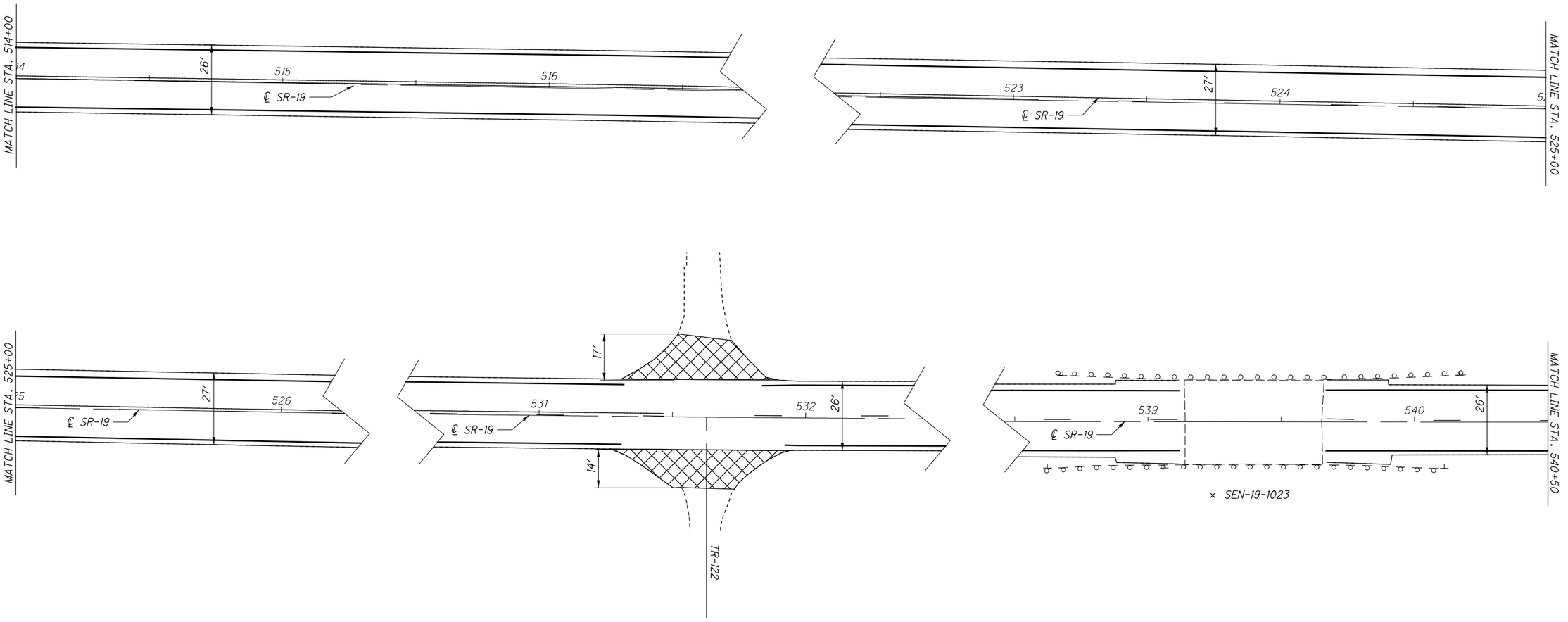
PAVEMENT SUBSUMMARY - LOCATION 2

SEN-19 / 101-
9.57 / 1.64

13
5.3



14 / 53	**SEN-19 / 101-9.57 / 1.64**	**LOCATION 1** **STA. 484+99 TO STA. 514+00**	CALCULATED JBT CHECKED JMF	0 10 20 40 HORIZONTAL SCALE IN FEET



SEE SHEETS 26-28 FOR SEN-19-1023 BRIDGE WORK

N

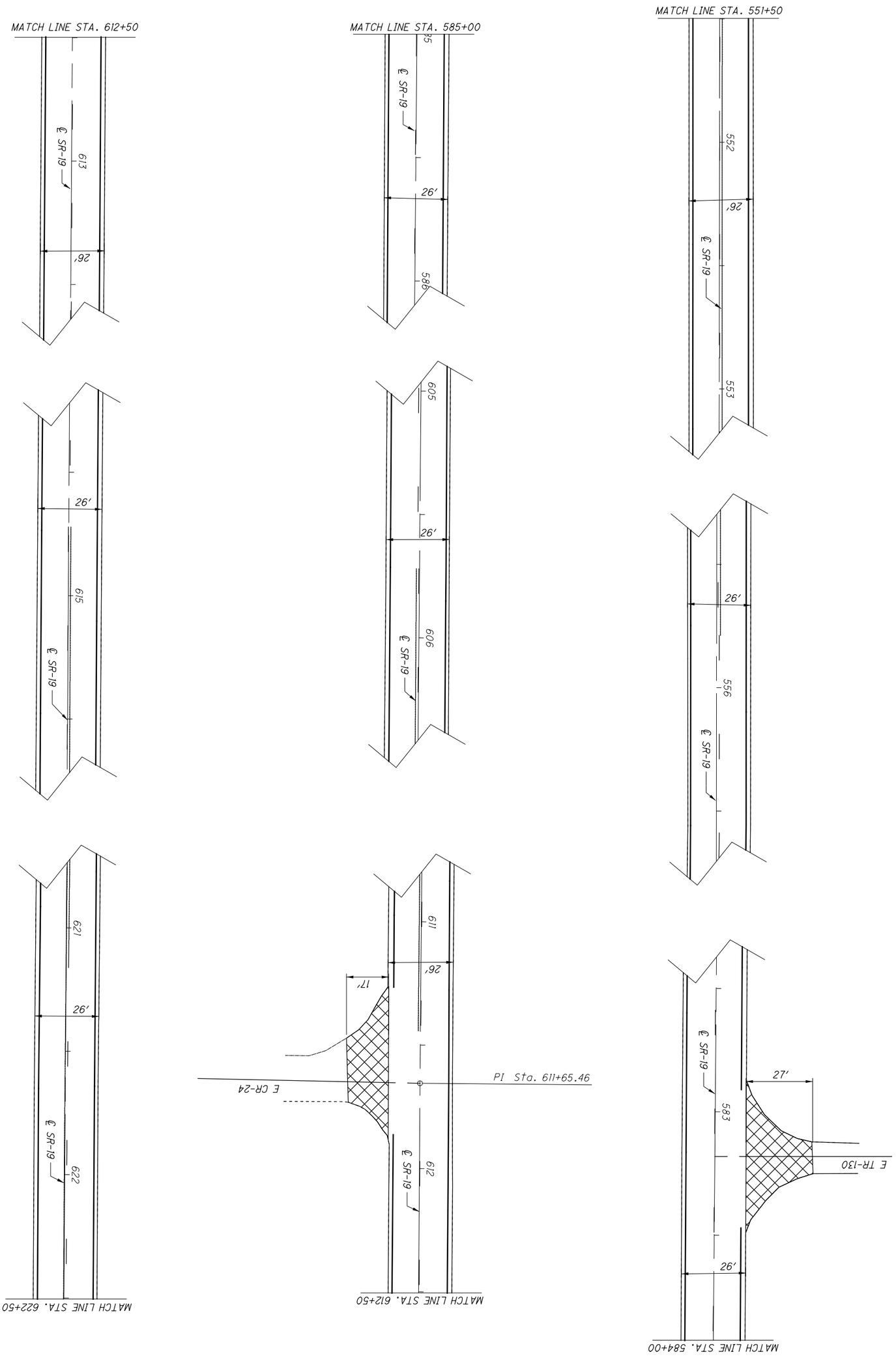
0 10 20 40
HORIZONTAL SCALE IN FEET

CALCULATED JBT
CHECKED JMF

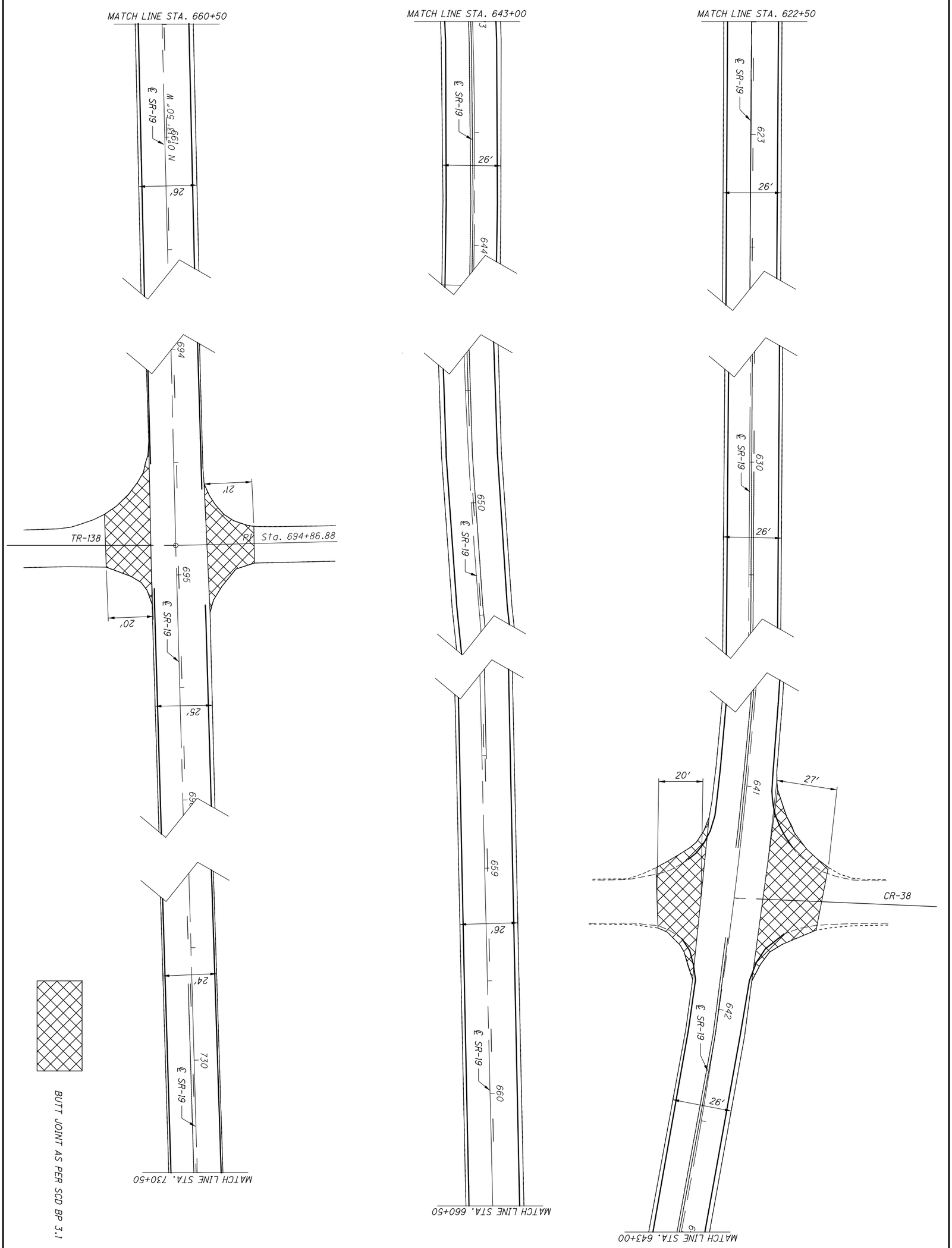
LOCATION 1
STA. 514+00 TO STA. 551+50

SEN-19 / 101-
9.57 / 1.64

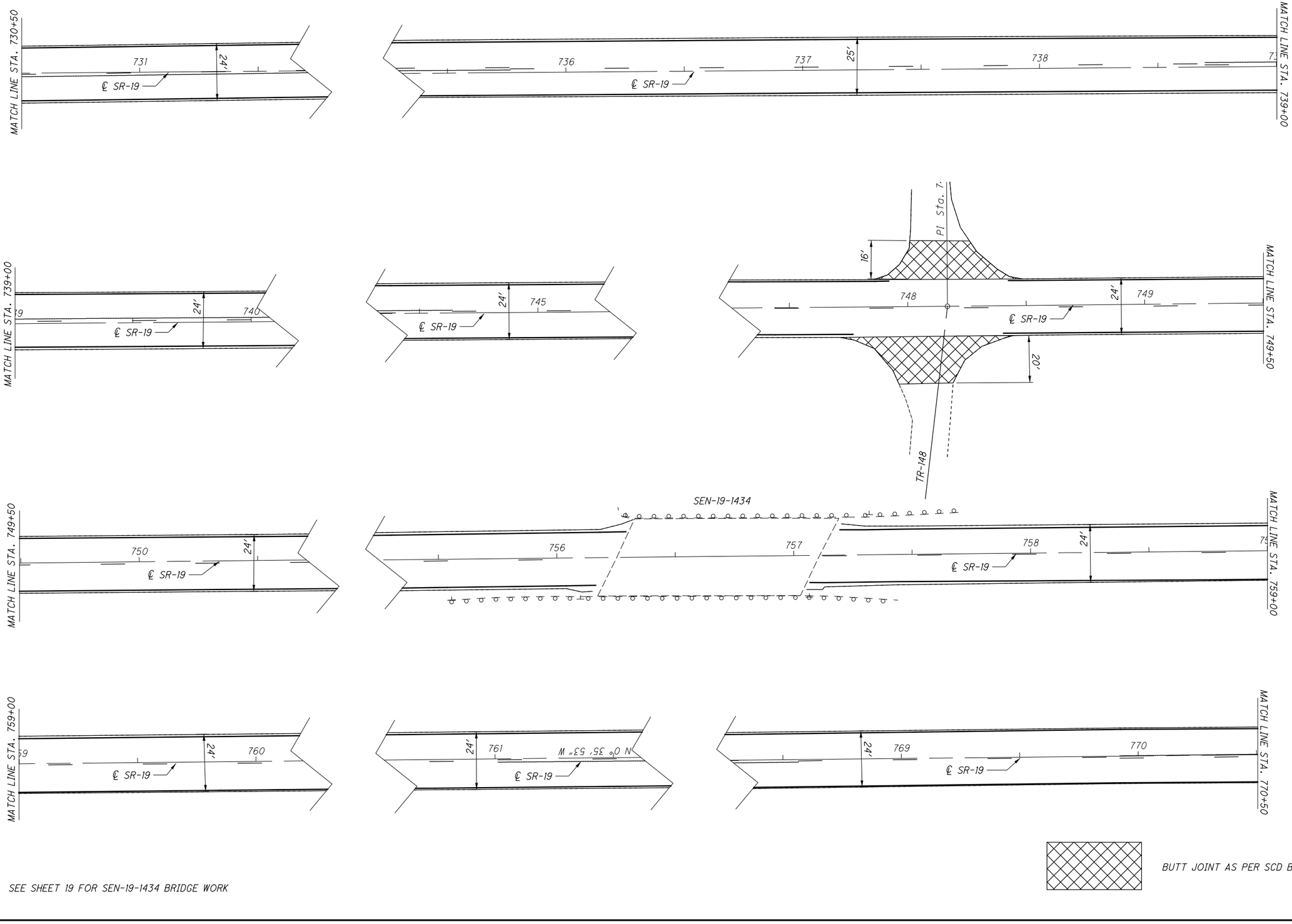
15
53



16 / 53	SEN-19 / 101-9.57 / 1.64	LOCATION 1 STA. 551+50 TO STA. 622+50	CALCULATED JBT CHECKED JMF	0 20 10 40 HORIZONTAL SCALE IN FEET



17 / 53	**SEN-19 / 101-9.57 / 1.64**	**LOCATION 1** **STA. 622+50 TO STA. 730+50**	CALCULATED JBT CHECKED JMF	0 20 10 40 HORIZONTAL SCALE IN FEET



SEE SHEET 19 FOR SEN-19-1434 BRIDGE WORK

BUTT JOINT AS PER SCD BP 3.1

N

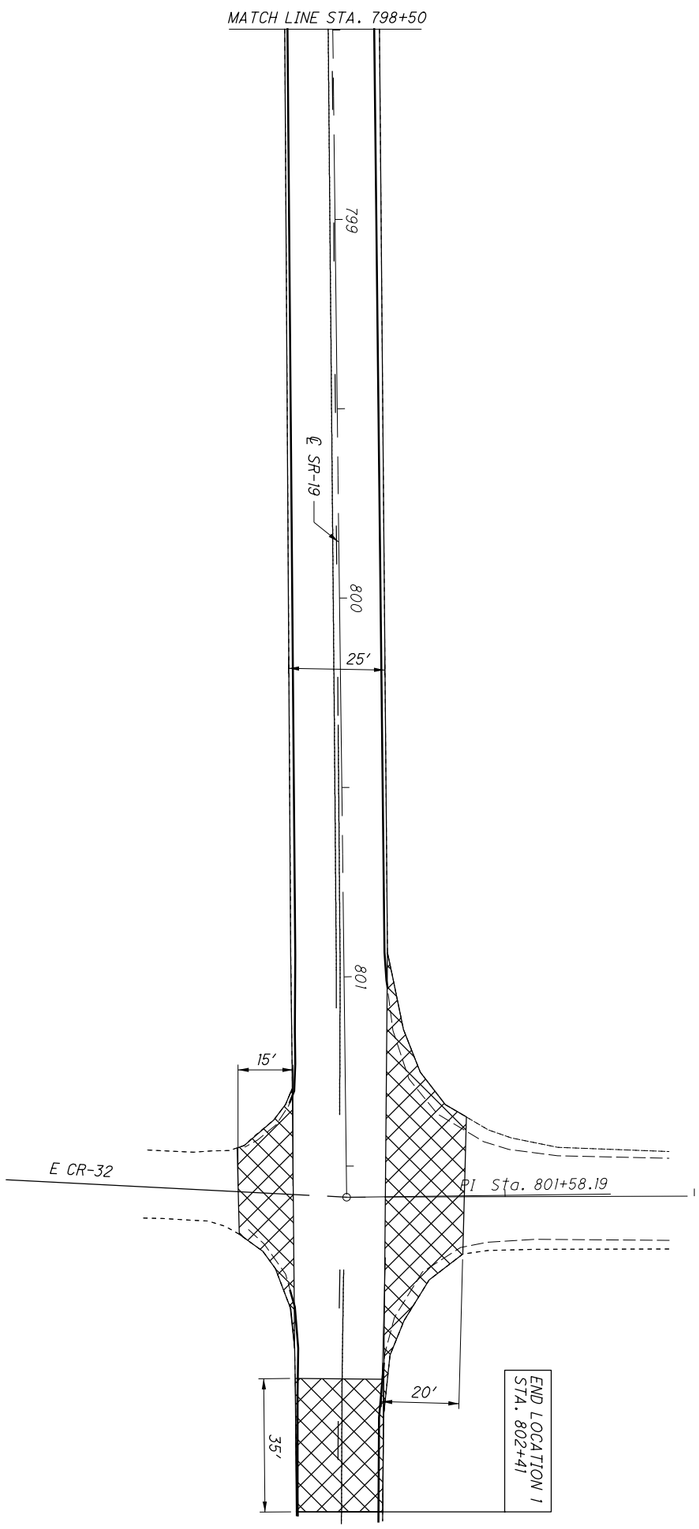
HORIZONTAL SCALE IN FEET
0 10 20 40

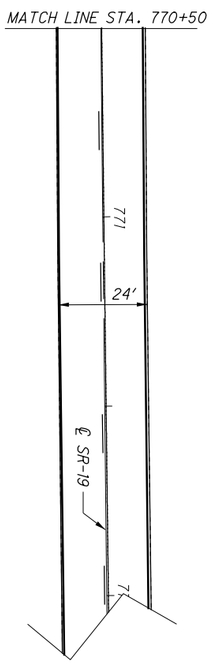
CALCULATED JBT
CHECKED JMF

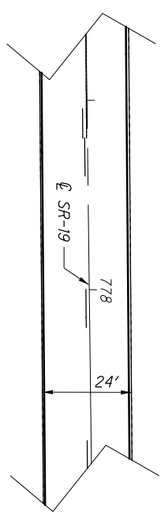
LOCATION 1
STA. 730+50 TO STA. 770+50

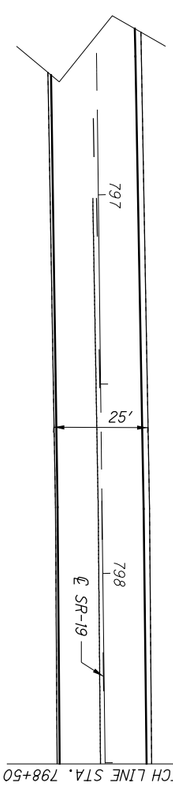
SEN-19 / 101-
9.57 / 1.64

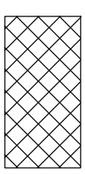
18
53











BUTT JOINT AS PER SCD BP 3.1

19 / 53	**SEN-19 / 101-9.57 / 1.64**	**LOCATION 1** **STA. 770+50 TO STA. 802+41**	CALCULATED JBT CHECKED JMF	0 20 10 40 HORIZONTAL SCALE IN FEET

BEGIN LOCATION 2
STA. 85+53

PI Sta. 85+69.53

SEN-101-0172

MATCH LINE STA. 91+00

MATCH LINE STA. 91+00

MATCH LINE STA. 117+50

N 48° 09' 38" E

N 47° 51' 46" E

℄ SR-101

SEN-101-0228

PI Sta. 12

MATCH LINE STA. 118+00

MATCH LINE STA. 123+50

CR-13

SEE SHEETS 30-39 FOR SEN-101-0172 BRIDGE WORK
SEE SHEETS 43-48 FOR SEN-101-0228 CULVERT WORK

BUTT JOINT AS PER SCD BP 3.1

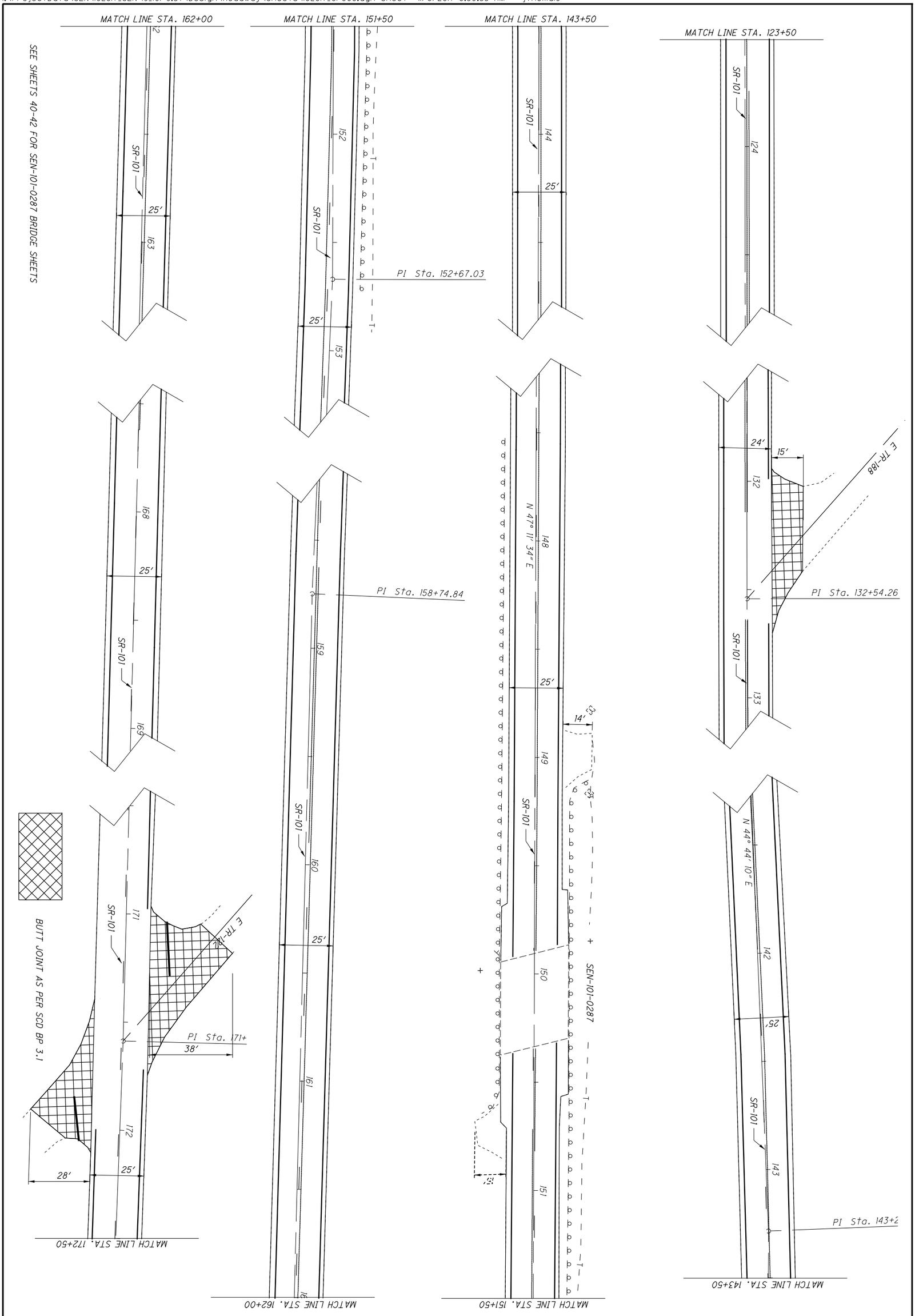
HORIZONTAL SCALE IN FEET
0 10 20 40

CALCULATED JBT
CHECKED JMF

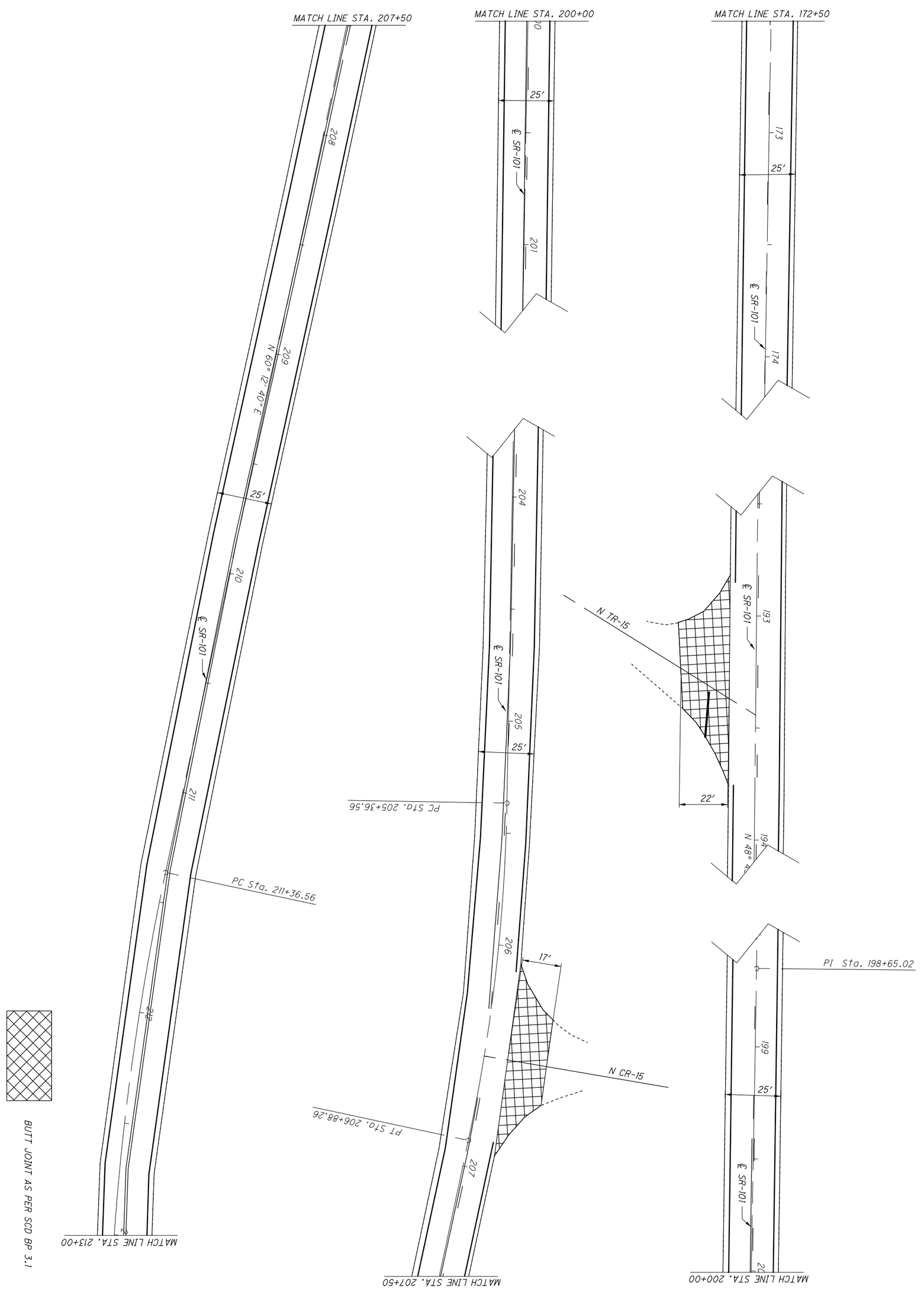
LOCATION 2
STA. 85+53 TO STA. 123+00

SEN-19 / 101-9.57 / 1.64

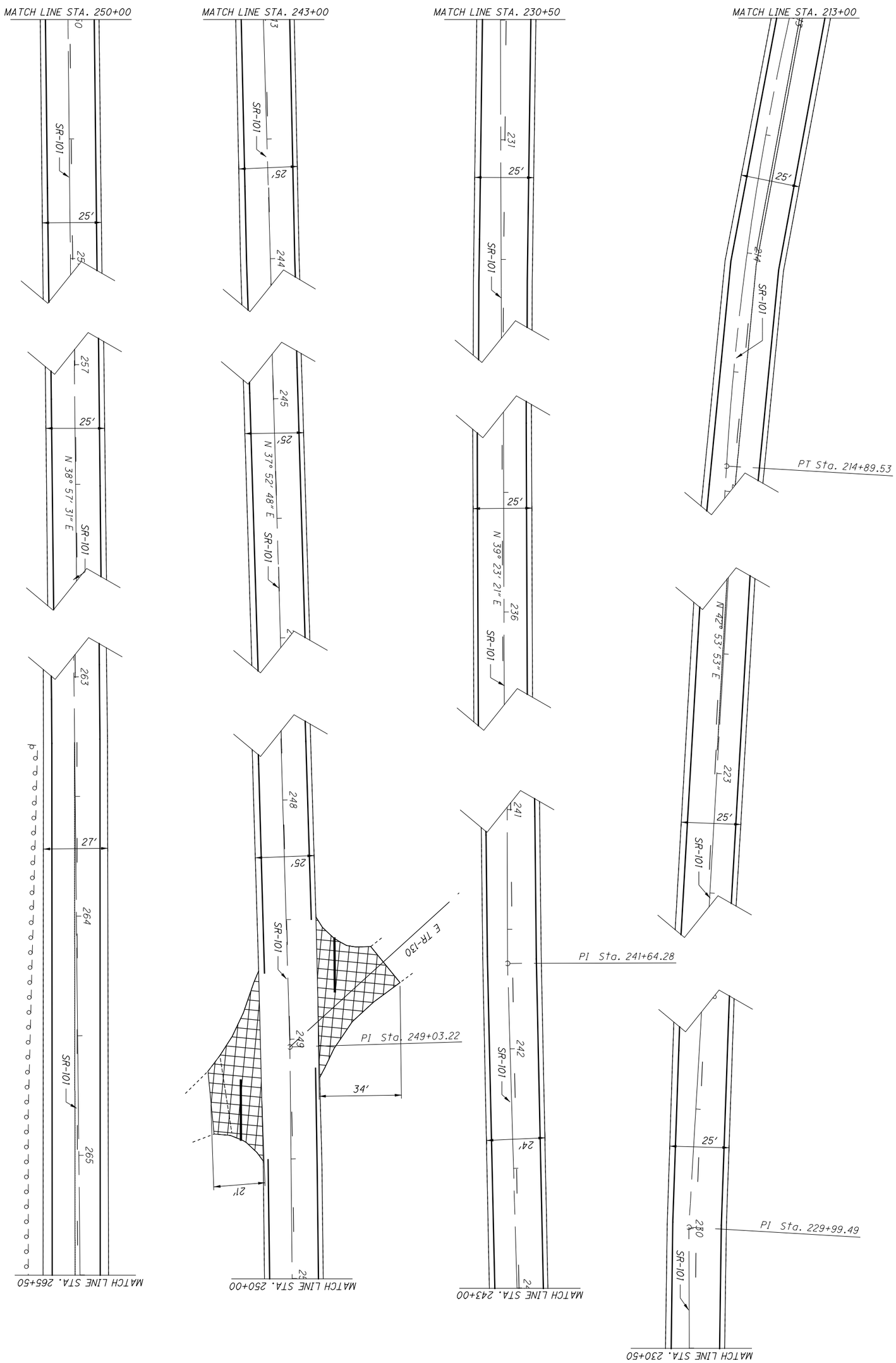
2.0
5.3



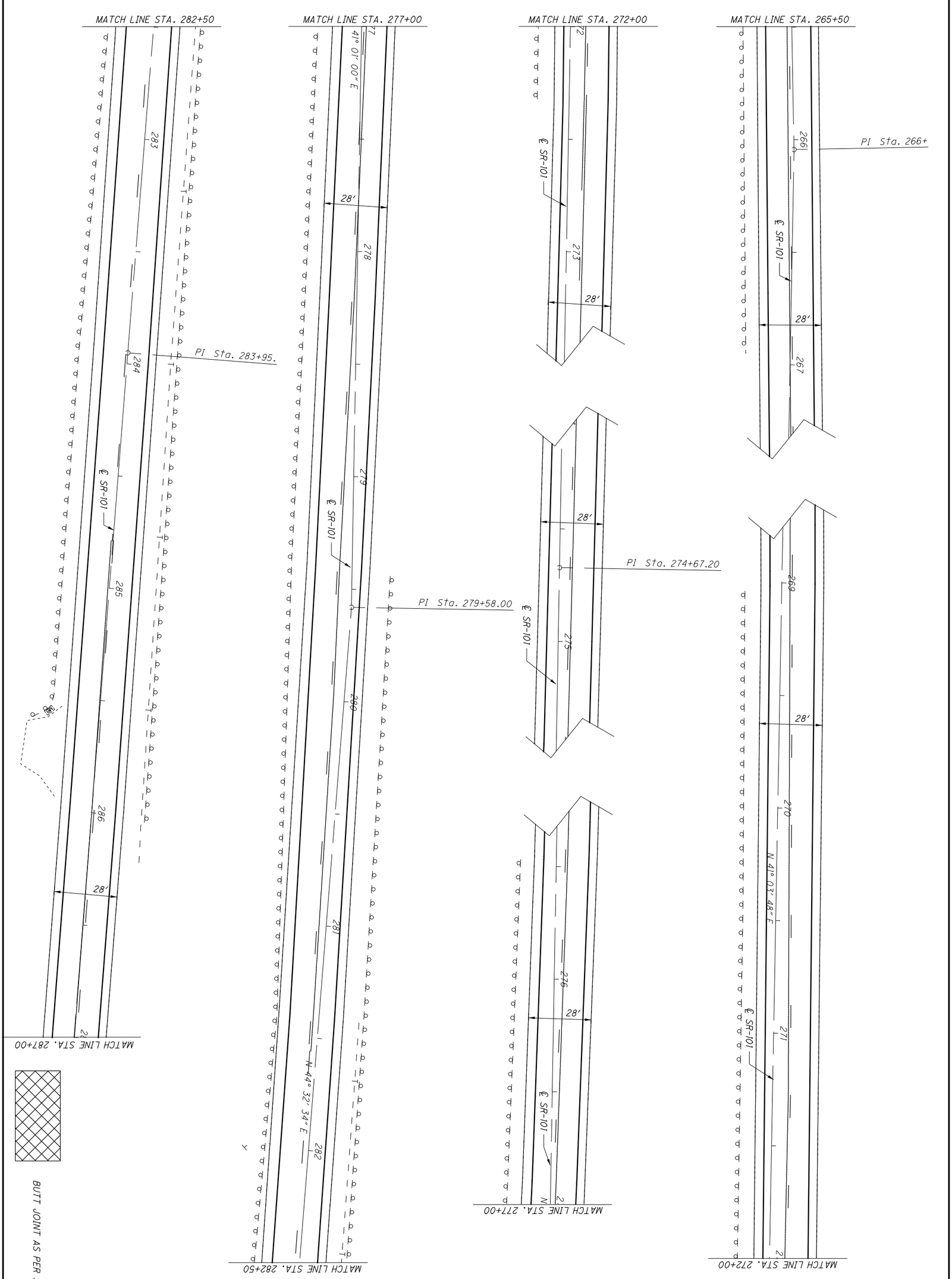
21 / 53	SEN-19 / 101-9.57 / 1.64	LOCATION 2 STA. 123+50 TO STA. 172+50	CALCULATED JBT CHECKED JMF	0 10 20 40 HORIZONTAL SCALE IN FEET



22 / 53	SEN-19 / 101-9.57 / 1.64	LOCATION 2 STA. 172+50 TO STA. 213+00	CALCULATED JBT CHECKED JMF	0 20 10 40 HORIZONTAL SCALE IN FEET

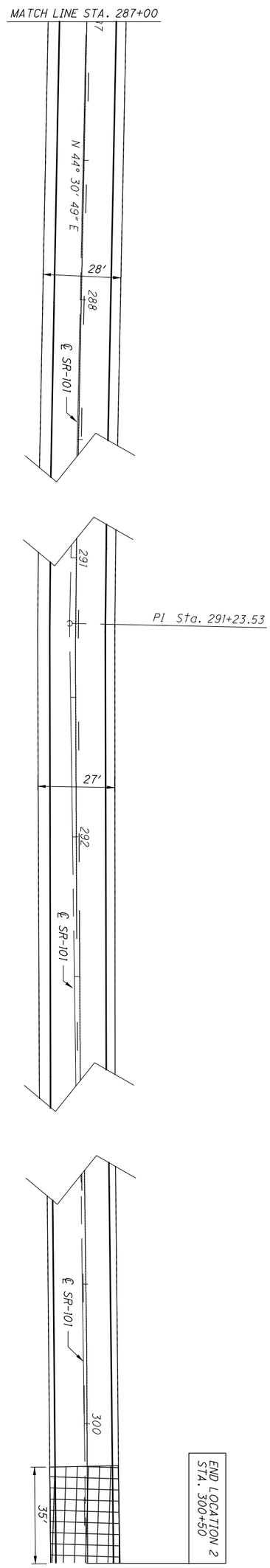


23 / 53	SEN-19 / 101-9.57 / 1.64	LOCATION 2 STA. 213+00 TO STA. 265+50	CALCULATED JBT CHECKED JMF	0 20 10 40 HORIZONTAL SCALE IN FEET

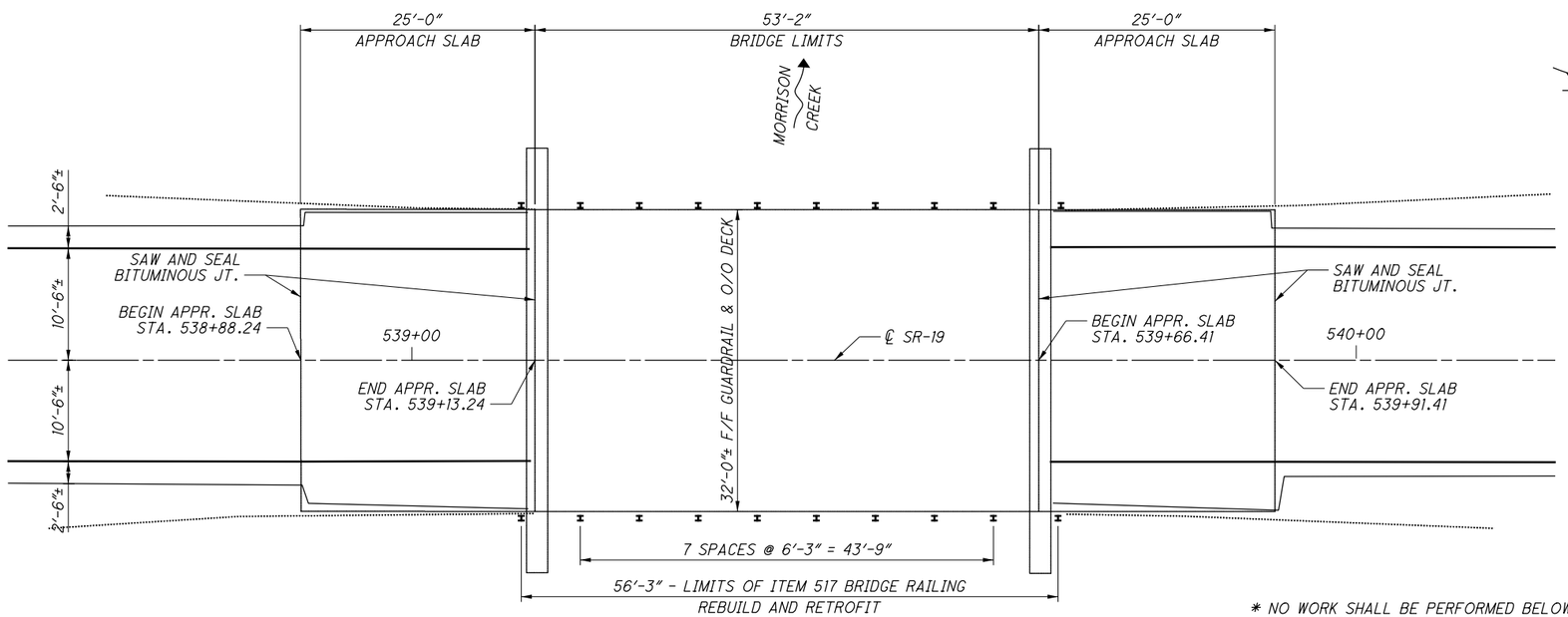


BUTT JOINT AS PER SCD BP 3.1

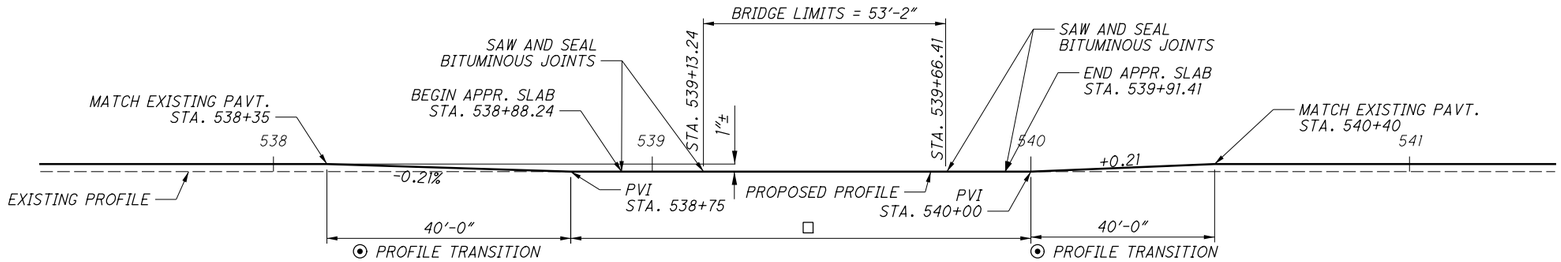
24 / 53	SEN-19 / 101- 9.57 / 1.64	LOCATION 2 STA. 265+50 TO STA. 287+00	CALCULATED JBT CHECKED JMF	0 20 10 40 HORIZONTAL SCALE IN FEET



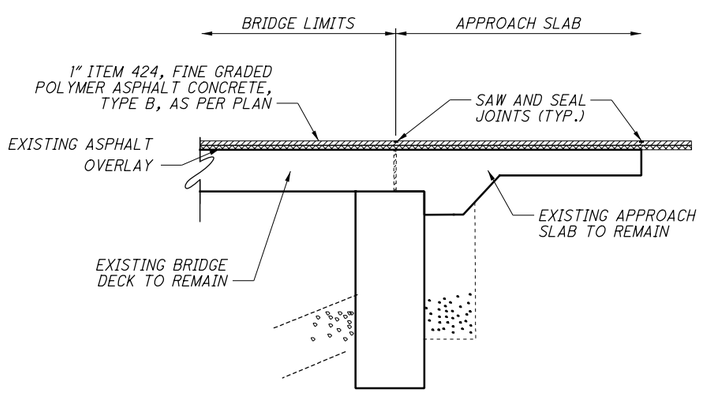
SEN-19/101- 9.57/1.64	LOCATION 2 STA. 287+00 TO STA. 300+50	CALCULATED JBT CHECKED JMF	0 20 10 40 HORIZONTAL SCALE IN FEET



* NO WORK SHALL BE PERFORMED BELOW THE OHWM ELEVATION 815.4'.



PROFILE ADJUSTMENT DETAIL



SEALING OF JOINTS AT ABUTMENTS AND ENDS OF APPROACH SLABS

⊙ PLANING SHALL VARY FROM 0" AT STATION 538+35 TO 1" AT STATION 538+75 AND 1" AT STATION 540+00 TO 0" AT STATION 540+40.

1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN SHALL BE APPLIED IN THESE SECTIONS.

□ PLANING SHALL BE A CONSTANT 1" FROM STATION 538+75 TO 540+00.

1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN SHALL BE APPLIED IN THIS SECTION.

DESIGN TRAFFIC:

2018 ADT - 880 2018 ADTT - 132

2030 ADT - 880 2030 ADTT - 132

DIRECTIONAL DISTRIBUTION - 54%

BENCHMARK DATA

BM #1 STA. 539+12.96, ELEV. 824.19, OFFSET 20.0' RT
ODOT DISK IN SE BRIDGE ABUTMENT

PROPOSED WORK

1. MAINTAIN TRAFFIC WITH FLAGGERS AS PER STANDARD DRAWING MT-97.10.
2. REMOVE 1" OF EXISTING ASPHALT WEARING SURFACE AS PER ITEM 254, PAVEMENT PLANING.
3. APPLY 1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN AND RETROFIT BRIDGE RAILINGS AS PER STANDARD DRAWING DBR-3-11.
4. SAW AND SEAL BITUMINOUS JOINTS.

EXISTING STRUCTURE

TYPE: SINGLE SPAN PRESTRESSED CONCRETE BOX BEAM WITH CAPPED PILE REINFORCED CONCRETE SUBSTRUCTURE

SPAN: 52'-0" C/C BEARINGS

ROADWAY: 32'-0" F/F GUARDRAIL

LOADING: HS20-44 + ALTERNATE MILITARY LOADING

SKEW: NONE

APPROACH SLABS: AS-1-81 (25' LONG)

ALIGNMENT: TANGENT

WEARING SURFACE: ASPHALT CONCRETE

STRUCTURAL FILE NUMBER: 7401159

DATE BUILT: 1983

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
VAP	DdG	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
DdG	XXX	7401159	

SITE PLAN
BRIDGE NO. SEN-19-1023
OVER MORRISON CREEK

SEN-19/101-9.57/1.64
PID No. 102817

1 / 3

2.6
5.3

ESTIMATED QUANTITIES (02/STR/BR)

ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUT.	SUPER.	GEN.	SEE SHEET
202	38603	112.5	FT	BRIDGE RAILING REMOVED FOR REUSE, AS PER PLAN		112.5		2
254	01000	661	SY	PAVEMENT PLANING, ASPHALT CONCRETE, VARIES		189	472 *	
407	20000	57	GAL	NON-TRACKING TACK COAT		17	40 *	
424	12001	19	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN		6	13 *	
SPECIAL	51631200	116	FT	SAWING AND SEALING BITUMINOUS CONCRETE JOINTS		64	52 *	
517	75501	112.5	FT	BRIDGE RAILING REBUILT, AS PER PLAN		112.5		2
517	75600	112.5	FT	DEEP BEAM BRIDGE RETROFIT RAILING		112.5		
875	10000	34	LB	LONGITUDINAL JOINT ADHESIVE		9	25	

* APPROACH SLAB AND APPROACH PAVEMENT

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

REFER TO THE FOLLOWING STANDARD DRAWING(S):

DBR-2-73	DATED/REVISED	7-19-02
DBR-3-11	DATED/REVISED	7-15-11

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASURMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05 AND 513.04.

CONTRACT BID PRICES SHALL BE BASED UPON RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE BY THE CONTRACTOR. HOWEVER, ALL PROJECT WORK SHALL BE BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED BY THE CONTRACTOR IN THE FIELD.

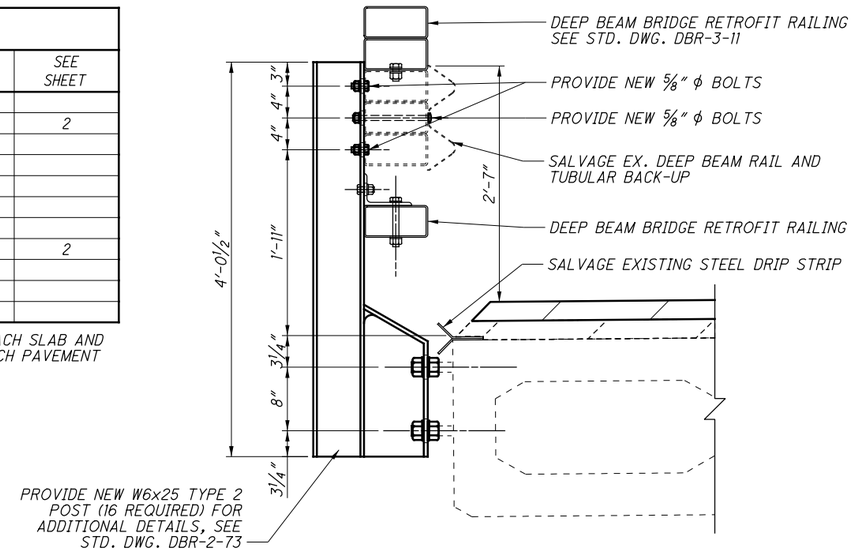
ITEM 517, BRIDGE RAILING REBUILT, AS PER PLAN

THE CONTRACTOR SHALL FURNISH AND INSTALL 16 NEW RAILING POSTS AS SHOWN IN THE PLAN UTILIZING EXISTING BRIDGE BEAM ANCHOR BOLTS, DEEP BEAM RAIL AND STEEL TUBULAR BACKUP. ALL MOUNTING HARDWARE TO INSTALL THE NEW POSTS SHALL BE REPLACED WITH NEW. REBUILT BRIDGE RAILING SHALL CONFORM TO STANDARD DRAWING DBR-2-73. PAYMENT FOR REMOVAL OF OLD POSTS SHALL BE INCLUDED WITH ITEM 202, BRIDGE RAILING REMOVED FOR REUSE.

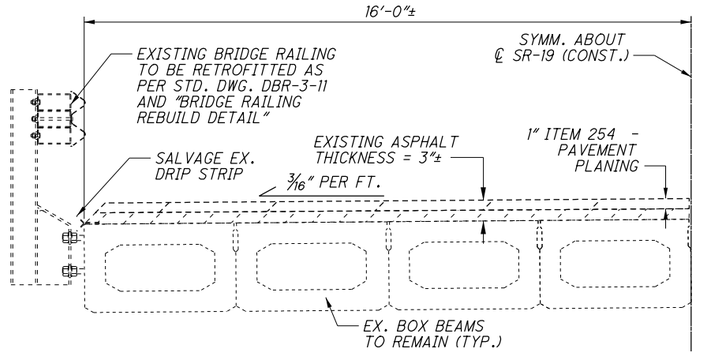
PAYMENT FOR BRIDGE RAILING REBUILD SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL MATERIALS, LABOR AND EQUIPMENT TO COMPLETE THE WORK TO THE SATISFACTION OF THE ENGINEER.

EXISTING BRIDGE PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 2 OFFICE AT 317 EAST POE RD., BOWLING GREEN, OHIO.

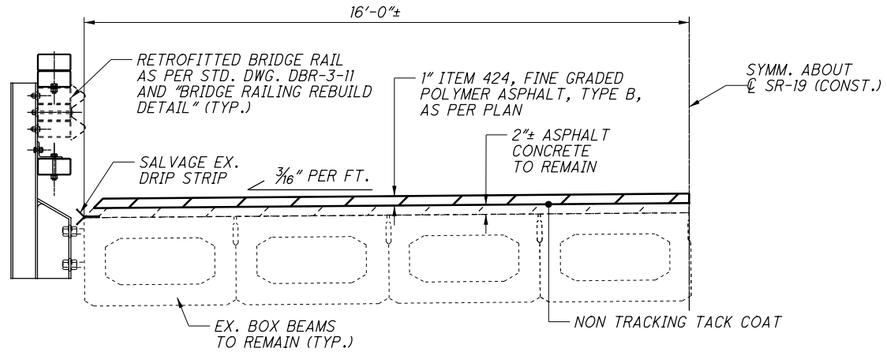


BRIDGE RAILING REBUILD DETAIL



HALF-TRANSVERSE SECTION

(EXISTING)



HALF-TRANSVERSE SECTION

(PROPOSED)

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
VAP	VAP	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
DJG	XXX	7401159	

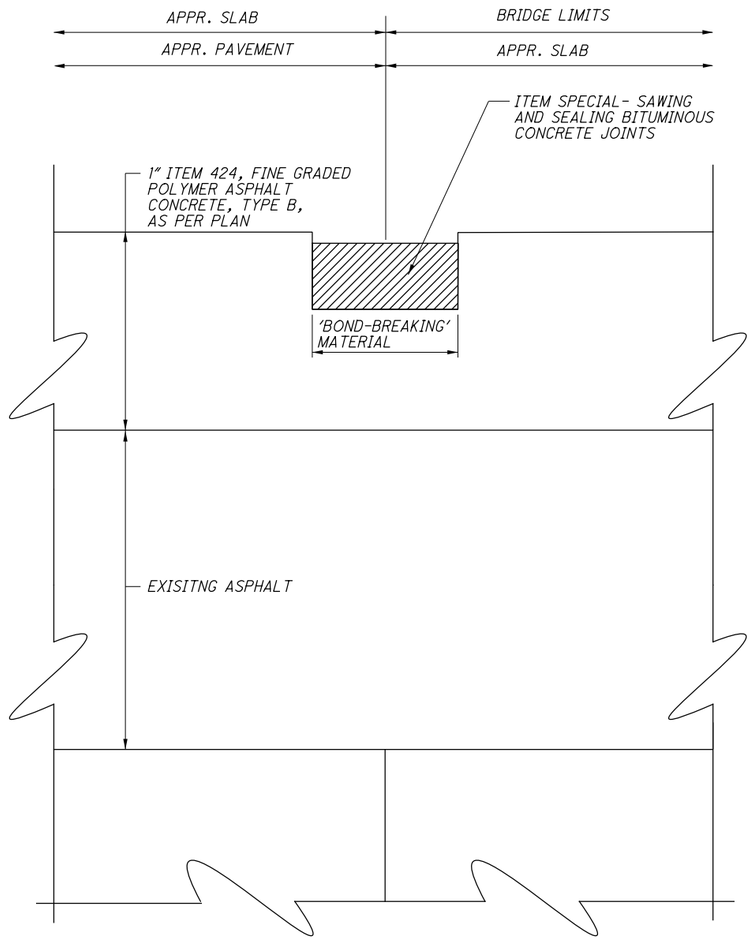
ESTIMATED QUANTITIES AND NOTES

BRIDGE NO. SEN-19-1023
OVER MORRISON CREEK

SEN-19/101-9.57/1.64
PID No. 102817

2 / 3

2.7
5.3



SEALING OF JOINTS AT ABUTMENTS
AND ENDS OF APPROACH SLABS
(APPLIES TO SEN-19-1023 STRUCTURE)

ITEM SPECIAL - SAVING AND SEALING BITUMINOUS CONCRETE JOINTS

1. DESCRIPTION

THIS WORK SHALL CONSIST OF CUTTING AND SEALING TRANSVERSE JOINTS IN THE NEW BITUMINOUS CONCRETE OVERLAY. BITUMINOUS CONCRETE JOINTS SHALL BE CONSTRUCTED DIRECTLY OVER, AND IN LINE WITH, THE EXISTING UNDERLYING TRANSVERSE JOINT OF THE APPROACH SLAB & APPROACH PAVEMENT.

2. MATERIALS

THE JOINT SEALENT SHALL MEET THE REQUIRMENTS OF ITEM 905.04, JOINT SEALENTS, HOT-POURED, FOR CONCRETE AND ASPHALT PAVEMENTS. ACCEPTABLE ALTERNATE MATERIALS ARE. A SILICONE SEALENT MEETING FEDERAL SPECIFICATIONS TT-S-001543a CLASS A -ONE-PART SILICONE SEALENTS. AND TT-S-00230C CLASS A -ONE-COMPONENT SEALENTS; SUCH AS THOSE MANUFACTURED BY GENERAL ELECTRIC, , SILICONE PRODUCTS DIVISIONS, 4015 EXECUTIVE DRIVE, CINCINNATI, OHIO 45242 (513-246-1953) OR DOW CORNING, 400 RECHNE CENTER, SUITE 103, MILFORD, OHIO 45150 (513-831-3586), OR SOF-SEAL. A COLD-APPLIED, LOW MODULUS, TWO--COMPONENT POLYMERIC COMPOUND HORIZONTAL SEALENT AS MANUFACTURED BY W.R.MEADOWS, INC., P.O. BOX 543, ELGIN, ILLINOIS-60121 (800-342-5976).

3. CONSTRUCTION DETAILS

A) GENERAL, THE CONTRACTOR SHALL CONDUCT HIS OPERATION SO THAT THE CUTTING, CLEANING AND SEALING OF TRANSVERSE JOINTS IS A CONTINUOUS OPERATION THAT WILL BE PERFORMED AS SOON AS PRACTICAL AFTER THE PAVING. BUT NO LATER THAN FOUR *4* DAYS AFTER PLACEMENT OF ASPHALT CONCRETE SURFACE COURSE. TRAFFIC SHALL NOT BE ALLOWED TO KNEAD TOGETHER OR DAMAGE JOINT CUT PRIOR TO SEALING

B) CUTTING OF TRANSVERSE JOINTS: THE CONTRACTOR SHALL SAW OR ROUT TRANSVERSE JOINTS TO THE DIMENSIONS SHOWN IN THE DETAILS ON THIS SHEET. THE CUT JOINTS SHALL LIE DIRECTLY ABOVE EACH APPROACH SLAB END. THE BLADE OR BLADES SHALL BE OF SUCH SIZE THAT THE FULL WIDTH AND DEPTH OF THE CUT CAN BE MADE WITH ONE PASS. DRY OR WET CUTTING WILL BE ALLOWED. JOINTS SHALL EXTEND THE FULL WIDTH OF BRIDGE.

C) CLEANING JOINTS: DRY SAWED JOINTS SHALL BE THOROUGHLY CLEANED WITH A SUFFICIENT AMOUNT OF COMPRESSED AIR TO REMOVE ANY DIRT, DUST, OR DELETERIOUS MATTER. WET SAWED JOINTS SHALL BE WASHED CLEAN OF ALL CUTTINGS BY FLUSHING WITH A JET OF WATER AND WITH OTHER TOOLS AS NECESSARY. AFTER FLUSHING, THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR . WHEN THE SURFACES ARE THOROUGHLY CLEAN AND DRY, AND JUST PRIOR TO PLACING THE JOINT SEALER, COMPRESSED AIR HAVING A PRESSURE OF AT LEAST 90 PSI SHALL BE USED TO BLOW OUT THE JOINT AND REMOVE ALL TRACES OF DUST. IN THE EVENT FRESHLY CUT JOINTS BECOME CONTAMINATED BEFORE THEY ARE SEALED, THEY SHALL BE RECLEANED OF ALL FOREIGN MATERIAL BY HIGH PRESSURE WATER JET.

D) SEALING JOINTS: THE JOINT SHALL BE THOROUGHLY DRY WHEN THE SEALANT IS PLACED. AFTER CLEANING AND DRYING, A BOND-BREAKER MATERIAL SHALL BE APPLIED TO THE BOTTOM OF THE GROOVE.

HOT-POURED JOINT SEALANT MATERIAL SHALL BE HEATED IN A KETTLE OR MELTER CONSTRUCTED AS A DOUBLE BOILER, WITH THE SPACE BETWEEN THE INNER AND OUTER SHELLS FILLED WITH OIL OR OTHER HEAT TRANSFER MEDIUM. POSITIVE TEMPERATURE CONTROL AND MECHANICAL AGITATION SHALL BE PROVIDED. HEATING MUST BE IN STRICT ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION. JOINT SEALER MATERIAL SHALL NEVER BE KEPT HEATED AT THE POURING TEMPERATURE FOR MORE THAN FOUR (4) HOURS AND SHALL NEVER BE REHEATED. SEALER LEFT IN THE APPLICATION AT THE END OF A DAY'S WORK SHALL NOT BE USED.

HOT-POURED SEALANT SHALL BE APPLIED IMMEDIATELY THROUGH A NOZZLE, WHICH MUST PROJECT INTO THE SAWED JOINT, FILLING FROM THE BOTTOM UP. THE SEALANT SHALL COMPLETELY FILL THE JOINT IN SUCH A MANNER THAT, AFTER COOLING, THE LEVEL OF THE SEALANT WILL NOT BE HIGHER THAN 1/8" BELOW THE PAVEMENT SURFACE. ANY DEPRESSION IN THE COOLED SEAL GREATER THAN 3/16" SHALL BE BROUGHT UP TO THE SPECIFIED LIMIT BY FURTHER ADDITION OF HOT-POURED SEALANT. CARE SHALL BE TAKEN IN THE SEALING OF THE JOINTS SO THAT THE FINAL APPEARANCE WILL PRESENT A NEAT FINE LINE.

THE COLD APPLIED SEALANT MATERIALS (POLYURETHANE, SILICONE, AND POLYMERIC COMPOUNDS) SHALL BE INSTALLED AS PER MANUFACTURERS' RECOMMENDATIONS, EXCEPT AS MODIFIED BY THIS DRAWING. THE SEALANT SHALL BE INSTALLED WHEN THE AMBIENT TEMPERATURE IS 40 DEGREES F OR HIGHER. TRAFFIC SHALL NOT BE ALLOWED ON THE JOINT FOR ONE HOUR AFTER THE APPLICATION OF THE SEALANT.

4. METHOD OF MEASUREMENT

THE QUANTITY TO BE PAID FOR UNDER THIS ITEM WILL BE THE NUMBER OF LINEAR FEET OF JOINTS SAWED AND SEALED AS PER THE ABOVE REQUIREMENTS.

5. BASIS OF PAYMENT

THE UNIT PRICE PER LINEAR FOOT FOR ITEM SPECIAL- "SAWING AND SEALING BITUMINOUS CONCRETE JOINTS" SHALL INCLUDE THE COST OF ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK, INCLUDING THE FURNISHING AND PLACING OF THE JOINT SEALER MATERIAL.

THIS ITEM SHALL MEET THE MATERIAL (SECTION 2) AND SEALING (SECTION 3D) SPECIFICATIONS OF ITEM SPECIAL- SAWING AND SEALING BITUMINOUS CONCRETE JOINTS.

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

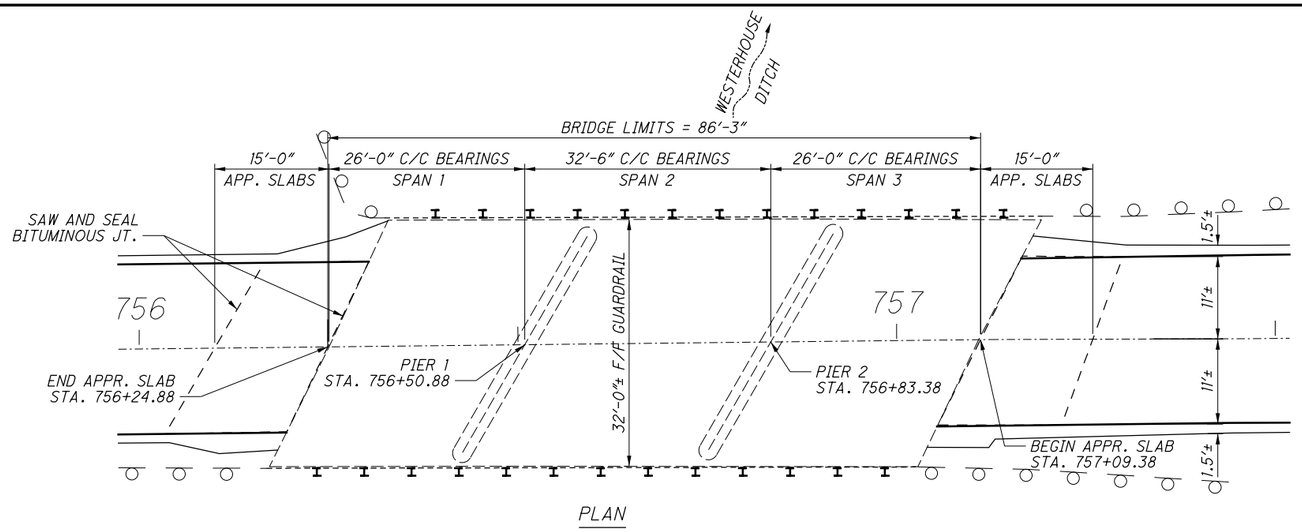
DESIGNED DdG | DRAWN DdG | REVIEWED XXX | DATE MM/DD/YY
CHECKED XXX | REVISED XXX | STRUCTURE FILE NUMBER 7401159

MISCELLANEOUS DETAILS
BRIDGE NO. SEN-19-1023
OVER MORRISON CREEK

SEN-19/101-9.57/1.64
PID No. 102817

3 / 3

2.8
5.3



PLAN

ESTIMATED QUANTITIES (02/STR/BR)

ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUT.	PIERS	SUPER.	GEN.	SEE SHEET
407	20000	36	GAL	NON-TRACKING TACK COAT			28	8 *	
424	12001	12	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN			9	3 *	
SPECIAL	51631200	134	FT	SAWING AND SEALING BITUMINOUS CONCRETE JOINTS			74	60*	
875	10000	19	LB	LONGITUDINAL JOINT ADHESIVE			14	5 *	

* APPROACH SLABS

PROPOSED WORK:

1. MAINTAIN TRAFFIC WITH FLAGGERS AS PER STANDARD DRAWING MT-97.10.
2. APPLY 1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN
3. SAW AND SEAL BITUMINOUS JOINTS.

EXISTING STRUCTURE

TYPE: CONTINUOUS REINFORCED CONCRETE SLAB WITH CAPPED PILE PIERS AND ABUTMENTS

SPANS: 26'-0", 32'-6", 26'-0" C/C BEARINGS

ROADWAY: 32'-0" F/F GUARDRAIL

LOADING: S-15-46

SKEW: 30°00'-00" LEFT FWD.

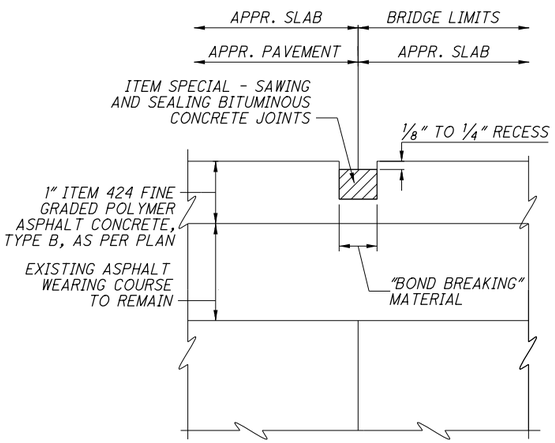
APPROACH SLABS: AS-1-47 (15' LONG)

ALIGNMENT: TANGENT

WEARING SURFACE: ASPHALT CONCRETE

STRUCTURAL FILE NUMBER: 7401175

DATE BUILT: 1951



SEALING BITUMINOUS JOINTS

(AT ABUTMENTS AND ENDS OF APPROACH SLABS)

ITEM SPECIAL - SAWING AND SEALING BITUMINOUS CONCRETE JOINTS

1. DESCRIPTION

THIS WORK SHALL CONSIST OF CUTTING AND SEALING TRANSVERSE JOINTS IN THE NEW BITUMINOUS CONCRETE OVERLAY. BITUMINOUS CONCRETE JOINTS SHALL BE CONSTRUCTED DIRECTLY OVER, AND IN LINE WITH, THE EXISTING UNDERLYING TRANSVERSE JOINT OF THE APPROACH SLAB & APPROACH PAVEMENT.

2. MATERIALS

THE JOINT SEALENT SHALL MEET THE REQUIRMENTS OF ITEM 905.04, JOINT SEALENTS, HOT-POURED, FOR CONCRETE AND ASPHALT PAVEMENTS. ACCEPTABLE ALTERNATE MATERIALS ARE. A SILICONE SEALENT MEETING FEDERAL SPECIFICATIONS TT-S-001543a CLASS A -ONE-PART SILICONE SEALENTS. AND TT-S-00230C CLASS A -ONE-COMPONENT SEALENTS; SUCH AS THOSE MANUFACTURED BY GENERAL ELECTRIC, , SILICONE PRODUCTS DIVISIONS, 4015 EXECUTIVE DRIVE, CINCINNATI, OHIO 45242 (513-246-1953) OR DOW CORNING, 400 RECHNE CENTER, SUITE 103, MILFORD, OHIO 45150 (513-831-3586), OR SOF-SEAL. A COLD-APPLIED, LOW MODULUS, TWO--COMPONENT POLYMERIC COMPOUND HORIZONTAL SEALENT AS MANUFACTURED BY W.R.MEADOWS, INC., P.O. BOX 543, ELGIN, ILLINOIS-60121 (800-342-5976).

3. CONSTRUCTION DETAILS

A) GENERAL, THE CONTRACTOR SHALL CONDUCT HIS OPERATION SO THAT THE CUTTING, CLEANING AND SEALING OF TRANSVERSE JOINTS IS A CONTINUOUS OPERATION THAT WILL BE PERFORMED AS SOON AS PRACTICAL AFTER THE PAVING. BUT NO LATER THAN FOUR *4* DAYS AFTER PLACEMENT OF ASPHALT CONCRETE SURFACE COURSE. TRAFFIC SHALL NOT BE ALLOWED TO KNEAD TOGETHER OR DAMAGE JOINT CUT PRIOR TO SEALING

B) CUTTING OF TRANSVERSE JOINTS: THE CONTRACTOR SHALL SAW OR ROUT TRANSVERSE JOINTS TO THE DIMENSIONS SHOWN IN THE DETAILS ON THIS SHEET. THE CUT JOINTS SHALL LIE DIRECTLY ABOVE EACH APPROACH SLAB END. THE BLADE OR BLADES SHALL BE OF SUCH SIZE THAT THE FULL WIDTH AND DEPTH OF THE CUT CAN BE MADE WITH ONE PASS. DRY OR WET CUTTING WILL BE ALLOWED. JOINTS SHALL EXTEND THE FULL WIDTH OF BRIDGE.

C) CLEANING JOINTS: DRY SAWED JOINTS SHALL BE THOROUGHLY CLEANED WITH A SUFFICIENT AMOUNT OF COMPRESSED AIR TO REMOVE ANY DIRT, DUST, OR DELETERIOUS MATTER. WET SAWED JOINTS SHALL BE WASHED CLEAN OF ALL CUTTINGS BY FLUSHING WITH A JET OF WATER AND WITH OTHER TOOLS AS NECESSARY. AFTER FLUSHING, THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR . WHEN THE SURFACES ARE THOROUGHLY CLEAN AND DRY, AND JUST PRIOR TO PLACING THE JOINT SEALER, COMPRESSED AIR HAVING A PRESSURE OF AT LEAST 90 PSI SHALL BE USED TO BLOW OUT THE JOINT AND REMOVE ALL TRACES OF DUST. IN THE EVENT FRESHLY CUT JOINTS BECOME CONTAMINATED BEFORE THEY ARE SEALED, THEY SHALL BE RECLEANED OF ALL FOREIGN MATERIAL BY HIGH PRESSURE WATER JET.

D) SEALING JOINTS: THE JOINT SHALL BE THOROUGHLY DRY WHEN THE SEALANT IS PLACED. AFTER CLEANING AND DRYING, A BOND-BREAKER MATERIAL SHALL BE APPLIED TO THE BOTTOM OF THE GROOVE.

HOT-POURED JOINT SEALANT MATERIAL SHALL BE HEATED IN A KETTLE OR MELTER CONSTRUCTED AS A DOUBLE BOILER, WITH THE SPACE BETWEEN THE INNER AND OUTER SHELLS FILLED WITH OIL OR OTHER HEAT TRANSFER MEDIUM. POSITIVE TEMPERATURE CONTROL AND MECHANICAL AGITATION SHALL BE PROVIDED. HEATING MUST BE IN STRICT ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION. JOINT SEALER MATERIAL SHALL NEVER BE KEPT HEATED AT THE POURING TEMPERATURE FOR MORE THAN FOUR (4) HOURS AND SHALL NEVER BE REHEATED. SEALER LEFT IN THE APPLICATION AT THE END OF A DAY'S WORK SHALL NOT BE USED.

HOT-POURED SEALANT SHALL BE APPLIED IMMEDIATELY THROUGH A NOZZLE, WHICH MUST PROJECT INTO THE SAWED JOINT, FILLING FROM THE BOTTOM UP. THE SEALANT SHALL COMPLETELY FILL THE JOINT IN SUCH A MANNER THAT, AFTER COOLING, THE LEVEL OF THE SEALANT WILL NOT BE HIGHER THAN 1/8" BELOW THE PAVEMENT SURFACE. ANY DEPRESSION IN THE COOLED SEAL GREATER THAN 3/16" SHALL BE BROUGHT UP TO THE SPECIFIED LIMIT BY FURTHER ADDITION OF HOT-POURED SEALANT. CARE SHALL BE TAKEN IN THE SEALING OF THE JOINTS SO THAT THE FINAL APPEARANCE WILL PRESENT A NEAT FINE LINE.

THE COLD APPLIED SEALANT MATERIALS (POLYURETHANE, SILICONE, AND POLYMERIC COMPOUNDS) SHALL BE INSTALLED AS PER MANUFACTURERS' RECOMMENDATIONS, EXCEPT AS MODIFIED BY THIS DRAWING. THE SEALANT SHALL BE INSTALLED WHEN THE AMBIENT TEMPERATURE IS 40 DEGREES F OR HIGHER. TRAFFIC SHALL NOT BE ALLOWED ON THE JOINT FOR ONE HOUR AFTER THE APPLICATION OF THE SEALANT.

4. METHOD OF MEASUREMENT

THE QUANTITY TO BE PAID FOR UNDER THIS ITEM WILL BE THE NUMBER OF LINEAR FEET OF JOINTS SAWED AND SEALED AS PER THE ABOVE REQUIREMENTS.

5. BASIS OF PAYMENT

THE UNIT PRICE PER LINEAR FOOT FOR ITEM SPECIAL- "SAWING AND SEALING BITUMINOUS CONCRETE JOINTS" SHALL INCLUDE THE COST OF ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK, INCLUDING THE FURNISHING AND PLACING OF THE JOINT SEALER MATERIAL.

THIS ITEM SHALL MEET THE MATERIAL (SECTION 2) AND SEALING (SECTION 3D) SPECIFICATIONS OF ITEM SPECIAL- SAWING AND SEALING BITUMINOUS CONCRETE JOINTS.

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED GLH | DRAWN GLH | REVIEWED XXX | DATE MM/DD/YY
CHECKED XXX | REVISED XXX | STRUCTURE FILE NUMBER 7401175

SENECA
STA. 0+00
STA. 0+00

SITE PLAN
BRIDGE NO.-SEN-19-1434
OVER WESTERHOUSE DITCH

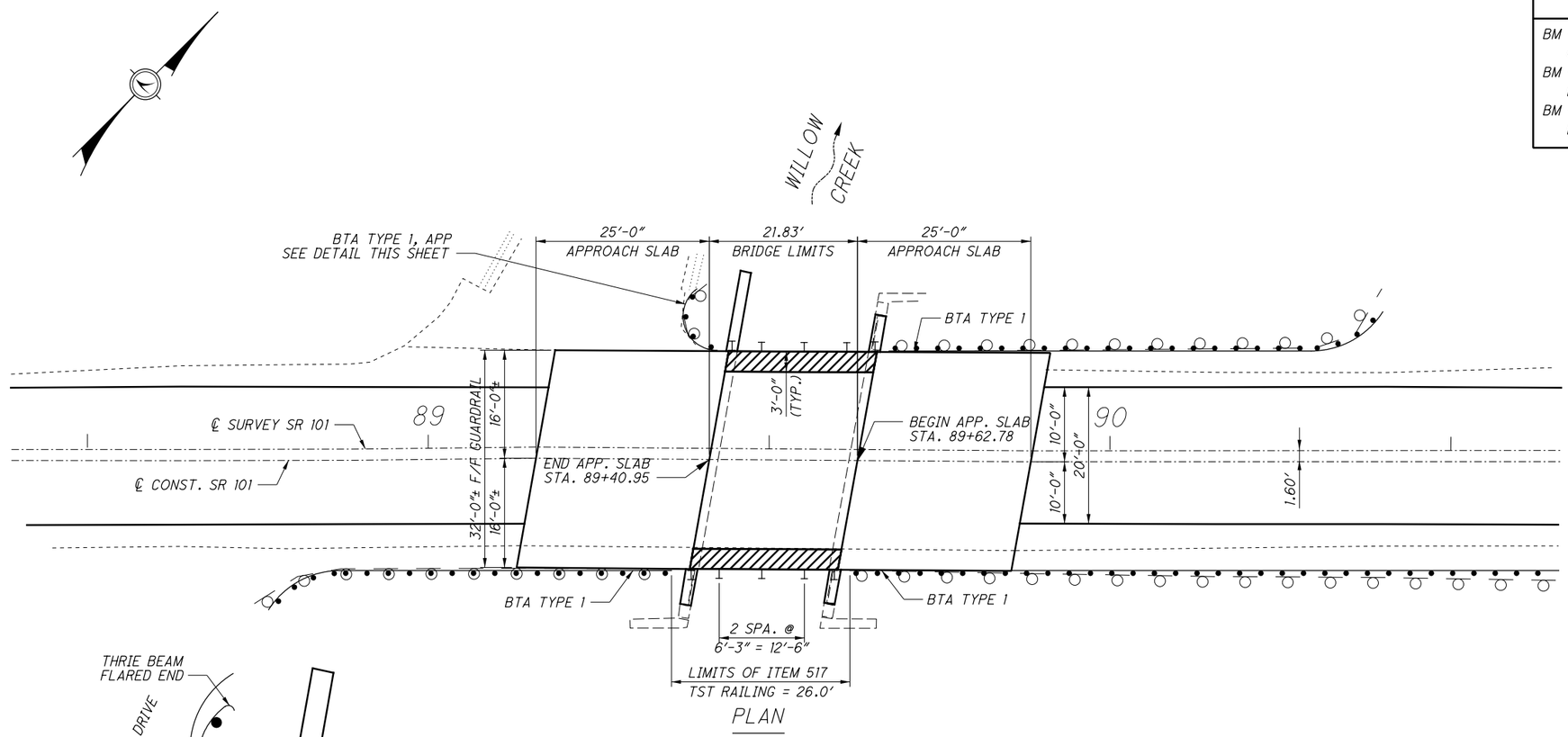
SEN-19/101-
9.57/1.64
PID No.-102817

1 / 1

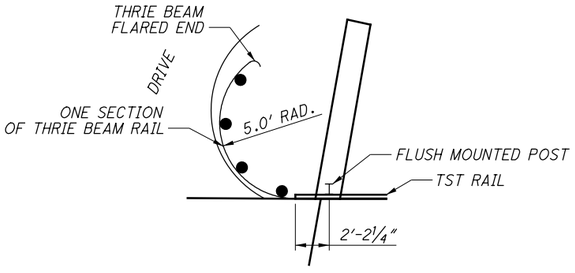
29
53

BENCHMARK DATA

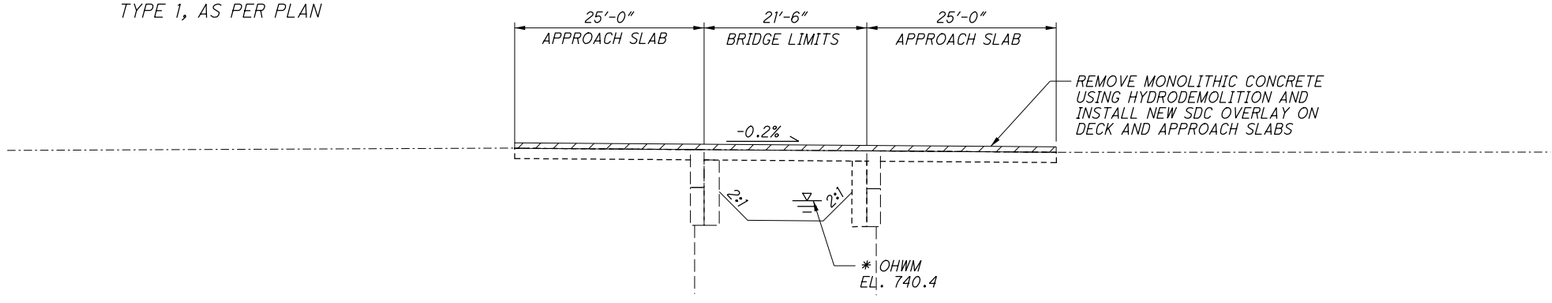
BM #1 STA. 89+37.71, ELEV. 748.63, OFFSET 21.8' RT
ODOT DISK IN SE ABUTMENT WALL
BM #2 STA. 89+20.34, ELEV. 749.82, OFFSET 21.0' RT
BENCH TIE SET IN POWER POLE, NW FACE
BM #3 STA. 87+57.87, ELEV. 750.98, OFFSET 22.6' RT
BENCH TIE SET IN POWER POLE, NW FACE



PLAN



BRIDGE TERMINAL ASSEMBLY
TYPE 1, AS PER PLAN



ELEVATION

FIRST GUARDRAIL POST STATIONING ON WINGWALL		
LOCATION	STATION	SIDE
REAR ABUT.	89+44.21	LT.
REAR ABUT.	89+38.45	RT.
FWD ABUT.	89+65.29	LT.
FWD ABUT.	89+59.53	RT.

PROPOSED WORK:

1. ESTABLISH DETOUR AND CLOSE STRUCTURE TO THRU TRAFFIC.
2. REMOVE PORTIONS OF STRUCTURE AS PER PLAN.
3. RECONSTRUCT DECK EDGE AND PORTIONS OF ABUTMENTS.
4. PLACE SDC CONCRETE OVERLAY ON DECK AND APPROACH SLABS.
5. SEAL CONCRETE SURFACES.
6. INSTALL BRIDGE RAILINGS AND APPROACH GUARDRAIL, PLACE PAVEMENT MARKINGS.
7. OPEN STRUCTURE TO TRAFFIC.

EXISTING STRUCTURE

TYPE: SINGLE SPAN REINFORCED CONCRETE SLAB ON REINFORCED CONCRETE WALL TYPE ABUTMENTS
SPANS: 19'-3½" CLEAR
ROADWAY: 32'-0" F/F GUARDRAIL
LOADING: HS 20-44 & ALTERNATE MILITARY LOADING
SKEW: 10° L.F.
APPROACH SLABS: AS-1-81 (25'-0" LONG)
ALIGNMENT: TANGENT
WEARING SURFACE: MONOLITHIC CONCRETE
STRUCTURAL FILE NUMBER: 7402368
DATE BUILT: 1983

* NO WORK SHALL BE PERFORMED BELOW THE OHWM ELEVATION OF 740.4'.

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED XXX | DRAWN GLH | REVIEWED XXX | DATE MM/DD/YY
CHECKED XXX | REVISED XXX | STRUCTURE FILE NUMBER 7402368

COUNTY
STA. 89+00
STA. 90+00

SITE PLAN
BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

SEN-19/101-9.57/1.64
PID No. 102817

1 / 10

3.0
5.3

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STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

REFER TO THE FOLLOWING STANDARD BRIDGE DRAWING(S):

AS-1-15	DATED/REVISED	7-17-15
AS-2-15	DATED/REVISED	7-17-15
DS-1-92	DATED/REVISED	7-18-03
TST-1-99	DATED/REVISED	7-15-16

AND TO THE FOLLOWING SUPPLEMENTAL SPECIFICATION(S):

848 DATED 1-20-17

DESIGN DATA

CLASS QC2 CONCRETE WITH QC/QA - COMPRESSIVE STRENGTH 4.5 KSI (SUPERSTRUCTURE)
CLASS QC1 CONCRETE WITH QC/QA - COMPRESSIVE STRENGTH 4.0 KSI (SUBSTRUCTURE)
REINFORCING STEEL - ASTM A615 OR A996, GRADE 60, MINIMUM YIELD STRENGTH 60,000 PSI

DECK PROTECTION METHOD

SUPERPLASTICIZED DENSE CONCRETE OVERLAY
STEEL DRIP STRIP

EXISTING BRIDGE PLANS

EXISTING PLANS MAY BE INSPECTED, UPON REQUEST, AT THE ODOT DISTRICT 2 OFFICE AT 317 EAST POE ROAD, BOWLING GREEN, OHIO.

ITEM 202, PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN

THIS ITEM SHALL INCLUDE THE ELEMENTS INDICATED IN THE PLANS AND GENERAL NOTES THAT ARE NOT SEPARATELY LISTED FOR PAYMENT, EXCEPT FOR WEARING COURSE REMOVAL. ITEMS TO BE REMOVED INCLUDE ALL EXISTING MATERIALS BEING REPLACED BY NEW CONSTRUCTION AND MISCELLANEOUS ITEMS THAT ARE NOT SHOWN TO BE INCORPORATED INTO THE FINAL CONSTRUCTION AND ARE DIRECTED TO BE REMOVED BY THE ENGINEER. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL AND THE WEIGHT OF HAMMER SHALL BE APPROVED BY THE ENGINEER. PERFORM ALL WORK IN A MANNER THAT WILL NOT CUT, ELONGATE OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL NOT BE HEAVIER THAN THE NOMINAL 90-POUND CLASS. PNEUMATIC HAMMERS SHALL NOT BE PLACED IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. SUBMIT CONSTRUCTION PLANS ACCORDING TO CMS 501.05.

CUT LINE CONSTRUCTION JOINT PREPARATION

SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 1 INCH DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL, IF REQUIRED IN THE PLANS, IN PLACE. INSTALL DOWEL BARS IF SPECIFIED. PRIOR TO CONCRETE PLACEMENT ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH, BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

SUBSTRUCTURE CONCRETE REMOVAL

REMOVE CONCRETE BY MEANS OF APPROVED PNEUMATIC HAMMERS EMPLOYING POINTED AND BLUNT CHISEL TOOLS. HYDRAULIC HOE-RAM TYPE HAMMERS WILL NOT BE PERMITTED. THE WEIGHT OF THE HAMMER SHALL NOT BE MORE THAN 35 POUNDS FOR REMOVAL WITHIN 18 INCHES OF PORTIONS TO BE PRESERVED. OUTSIDE THE 18 INCH LIMIT, THE CONTRACTOR MAY USE HAMMERS NOT EXCEEDING 90 POUNDS UPON THE APPROVAL OF THE ENGINEER. DO NOT PLACE PNEUMATIC HAMMERS IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02 AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD.

ITEM 509 REINFORCING STEEL, REPLACEMENT OF EXISTING REINFORCING STEEL, AS PER PLAN

REPLACE ALL EXISTING REINFORCING BARS DEEMED BY THE ENGINEER TO BE UNUSABLE BECAUSE OF CORROSION. THE DEPARTMENT WILL MEASURE THE REPLACEMENT REINFORCING STEEL BY THE NUMBER OF POUNDS ACCEPTED IN PLACE.

REPLACE ALL EXISTING REINFORCING STEEL BARS WHICH ARE TO BE INCORPORATED INTO THE NEW WORK AND ARE DEEMED BY THE ENGINEER TO BE MADE UNUSABLE BY CONCRETE REMOVAL OPERATIONS WITH NEW EPOXY COATED REINFORCING STEEL OF THE SAME SIZE AT NO COST TO THE DEPARTMENT.

ITEM 848, SUPERPLASTICIZED DENSE CONCRETE OVERLAY, AS PER PLAN

THE FINAL BRIDGE DECK AND APPROACH SURFACE SMOOTHNESS SHALL COMPLY WITH PROPOSAL NOTE 555, DATED 4/17/2015.

STRUCTURE BENCHMARK

THE ENGINEER SHALL FURNISH TO THE CONTRACTOR A BENCH MARK DISK TO BE INSTALLED IN THE LEFT WINGWALL OF THE FORWARD BRIDGE ABUTMENT. PAYMENT SHALL BE INCLUDED WITH ITEM 511, CLASS QC1 CONCRETE WITH QC/QA, ABUTMENT.

DECK OVERLAY PLACEMENT DESIGN ASSUMPTIONS

THE FOLLOWING ASSUMPTIONS OF CONSTRUCTION MEANS AND METHODS WERE MADE FOR THE ANALYSIS AND DESIGN OF THE SUPERSTRUCTURE. THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN OF THE FALSEWORK SUPPORT SYSTEM WITHIN THESE PARAMETERS AND WILL ASSUME RESPONSIBILITY FOR THE SUPERSTRUCTURE ANALYSIS FOR DEVIATION FROM THESE DESIGN ASSUMPTIONS.

AN EIGHT WHEEL FINISHING MACHINE WITH A MAXIMUM WHEEL LOAD OF 2.2 KIPS.

A MINIMUM OUT-TO-OUT WHEEL SPACING AT EACH END OF THE MACHINE OF 103 INCHES.

ITEM SPECIAL, STRUCTURES, SURVEY OF EXISTING STRUCTURE

PRIOR TO PERFORMING ANY STRUCTURAL OR PAVEMENT REMOVAL OPERATIONS AND BEFORE ANY BRIDGE DEMOLITION, THE CONTRACTOR SHALL MAKE A SURVEY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING:

1. TAKE ELEVATIONS OF THE BRIDGE DECK ALONG ALL EDGES, CROWNS AND CONSTRUCTION JOINTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVELOPING THE DECK SCREED TABLE.

2. TAKE ELEVATIONS AT BRIDGE APPROACHES. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THE APPROACH PAVEMENT PROFILES MEET CRITERIA IN THE ODOT LOCATION AND DESIGN MANUAL.

ALL SURVEY AND FIELD INFORMATION SHALL BE SUBMITTED TO THE ENGINEER PRIOR TO THE FINAL PLACEMENT OF THE BRIDGE DECK OVERLAY AND APPROACH SLABS. THE ENGINEER SHALL HAVE THE AUTHORITY TO MAKE REVISIONS TO THE FINAL PLANS.

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION
DESIGNED XXX DRAWN GLH REVIEWED XXX DATE MM/DD/YY
CHECKED XXX REVISED XXX STRUCTURE FILE NUMBER 7402368

GENERAL NOTES
BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

SEN-19/101-9.57/1.64
PID No. 102817

3.1
5.3

ESTIMATED QUANTITIES (04/S<2/BR)

ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUT.	SUPER.	GEN.	SEE SHEET
202	11203	LS		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN			LUMP	2
503	21100	3	CY	UNCLASSIFIED EXCAVATION	3			
509	10000	1996	LB	EPOXY COATED REINFORCING STEEL	240	1756		
509	20001	100	LB	REINFORCING STEEL, REPLACEMENT OF EXISTING REINFORCING STEEL, AS PER PLAN			100 *	2
510	10000	158	EACH	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	52	106		
511	34412	7	CY	CLASS QC2 CONCRETE WITH QC/QA, SUPERSTRUCTURE		7		
511	45712	3	CY	CLASS QC1 CONCRETE WITH QC/QA, ABUTMENT	3			
SPECIAL	51160000	240	SY	BRIDGE DECK GROOVING		73	167 *	
512	10050	28	SY	SEALING OF CONCRETE SURFACES (NON-EPOXY)	16	12		
516	13600	10	SF	1" PREFORMED EXPANSION JOINT FILLER	10			
517	70000	52	FT	RAILING (TWIN STEEL TUBE)		52		
SPECIAL	51822300	48	FT	STEEL DRIP STRIP		48		
SPECIAL	53000200	LS		STRUCTURES, SURVEY OF EXISTING STRUCTURE			LUMP	
848	10201	256	SY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN, 1.75" THICK		78	178 *	2
848	20000	256	SY	SURFACE PREPARATION USING HYDRODEMOLITION		78	178 *	
848	30200	3	CY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY		3		
848	50000	15	SY	HAND CHIPPING		5	10 *	
848	50100	LS		TEST SLAB			LUMP	

* APPROACH SLABS

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

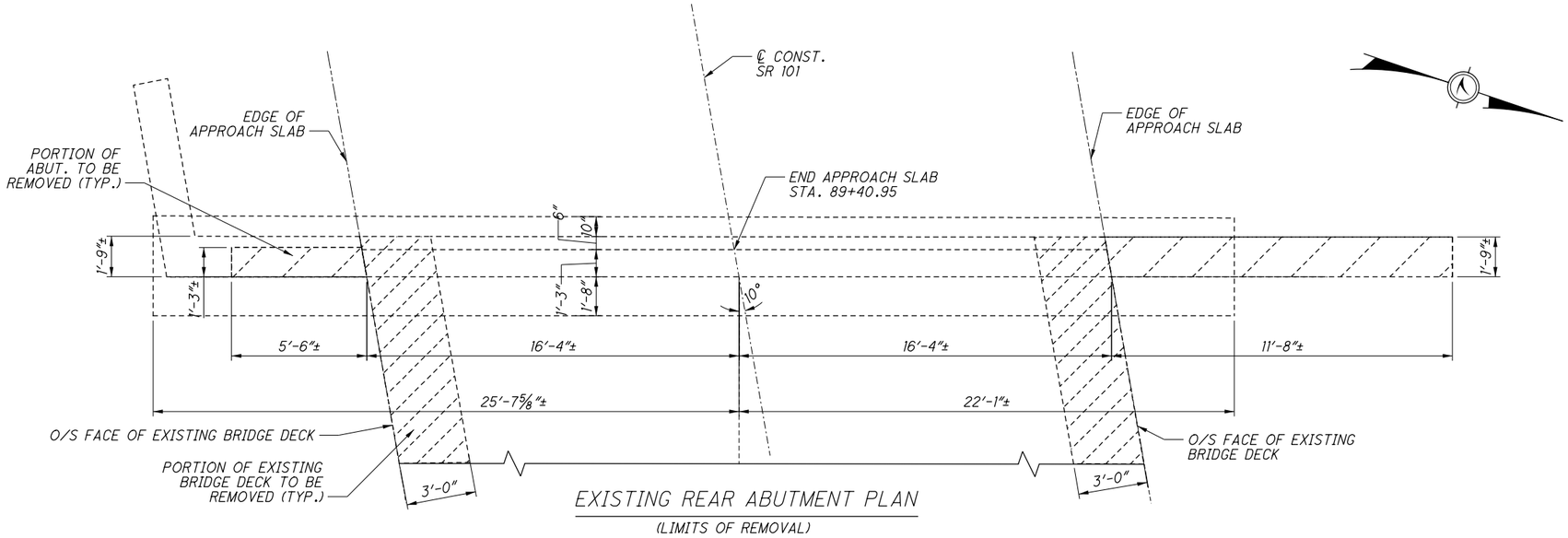
DESIGNED	DRAWN	REVIEWED	DATE
GLH	GLH	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
DJG	XXX	7402368	

SEN-19/101-9.57/1.64
PID No. 102817

ESTIMATED QUANTITIES
BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

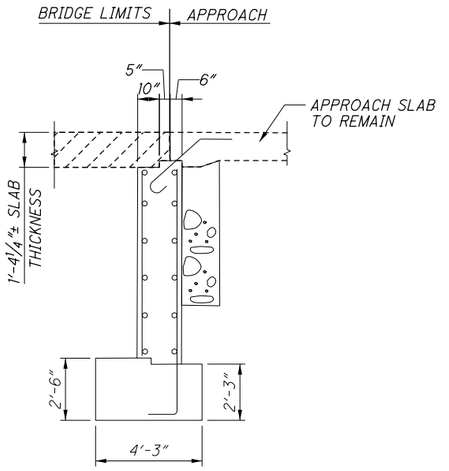
3 / 10

3.2
5.3

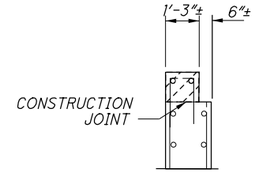


EXISTING REAR ABUTMENT PLAN

(LIMITS OF REMOVAL)



SECTION A-A



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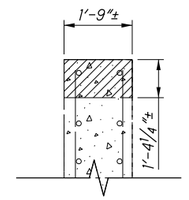

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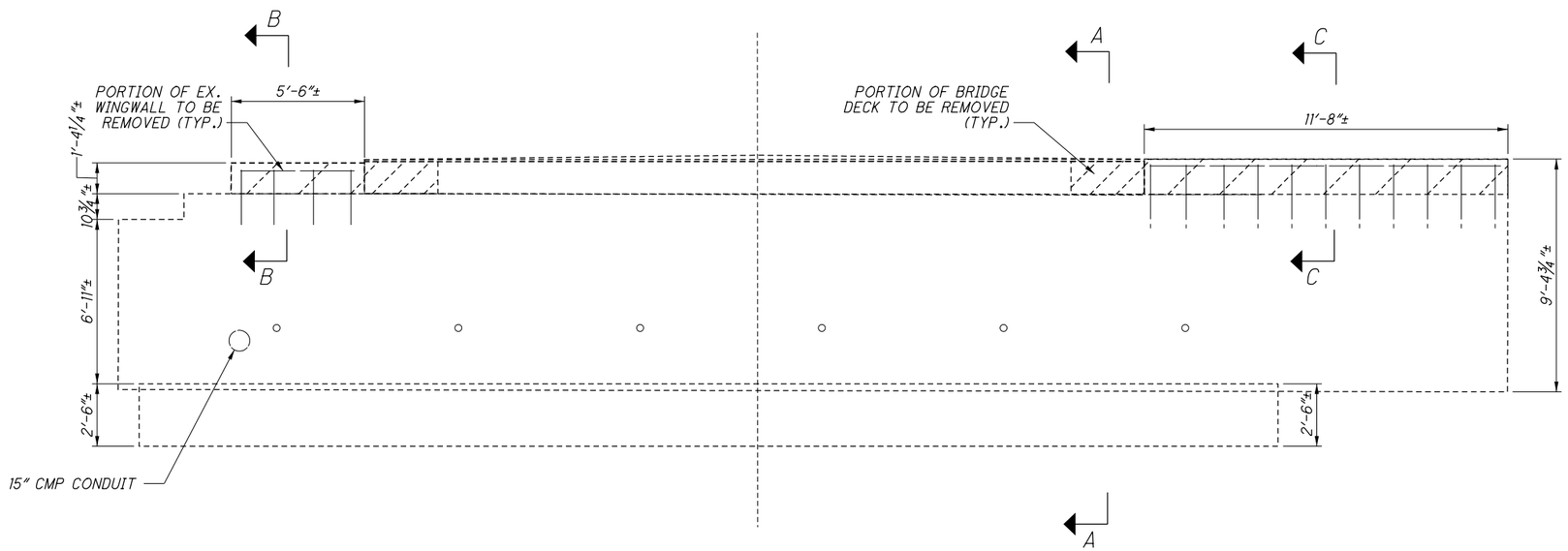

EXISTING REAR ABUTMENT ELEVATION

(LIMITS OF REMOVAL)

DESIGN AGENCY
OHIO DEPARTMENT
OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
XXX	GLH	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
XXX	XXX	7402368	

REAR ABUTMENT: REMOVAL PLAN

BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

SEN-19/101-
9.57/1.64
PID No. 102817

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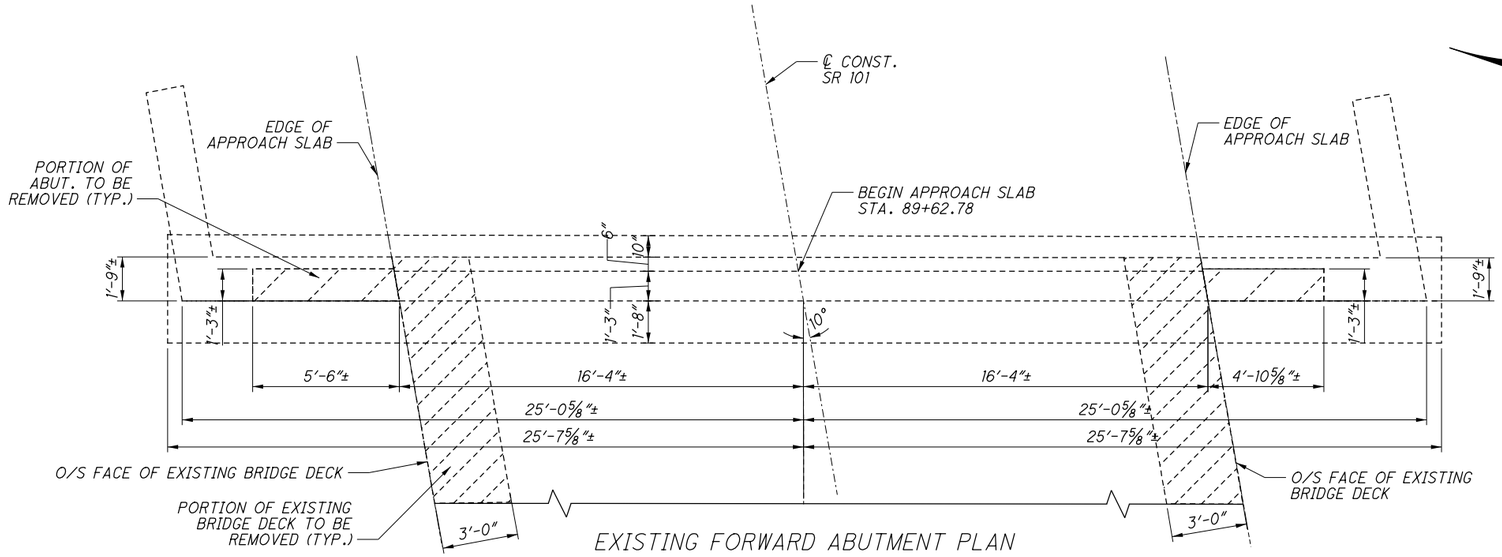
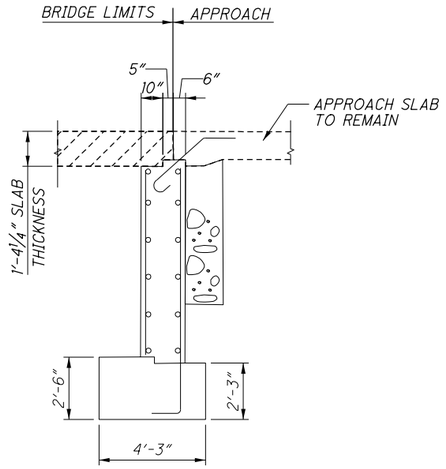
3.3
5.3



EXISTING FORWARD ABUTMENT PLAN

(LIMITS OF REMOVAL)



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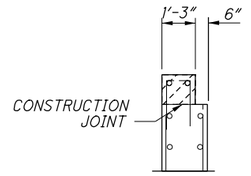

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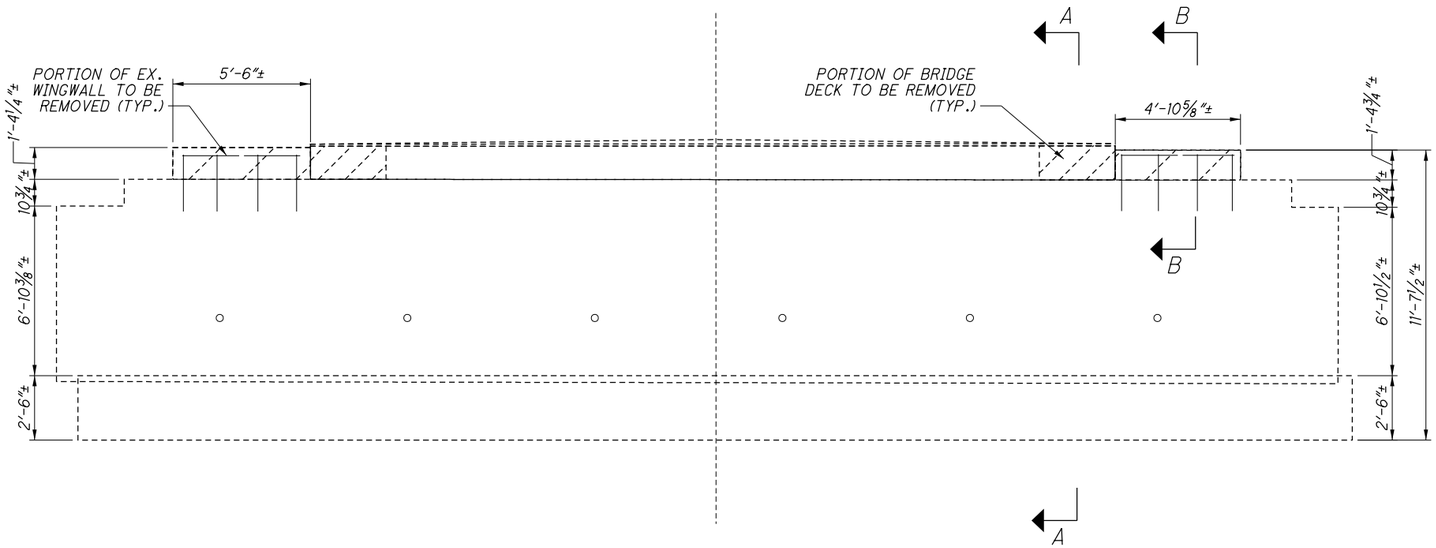

EXISTING FORWARD ABUTMENT ELEVATION

(LIMITS OF REMOVAL)

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
XXX	GLH	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
XXX	XXX	7402368	

FORWARD ABUTMENT REMOVAL PLAN
BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

SEN-19/101-9.57/1.64
PID No. 102817

5 / 10

3.4
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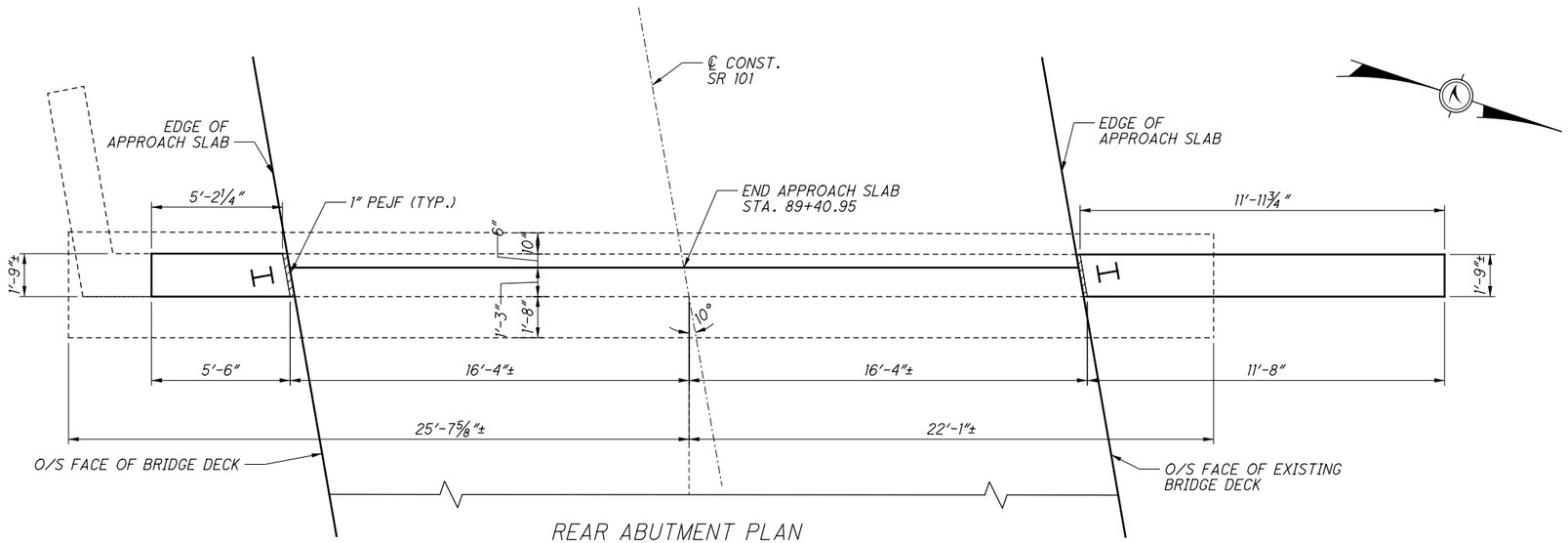
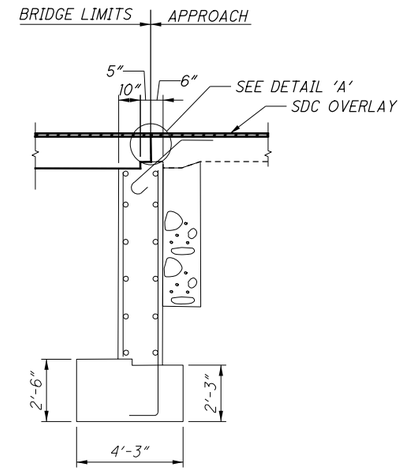


REAR ABUTMENT PLAN



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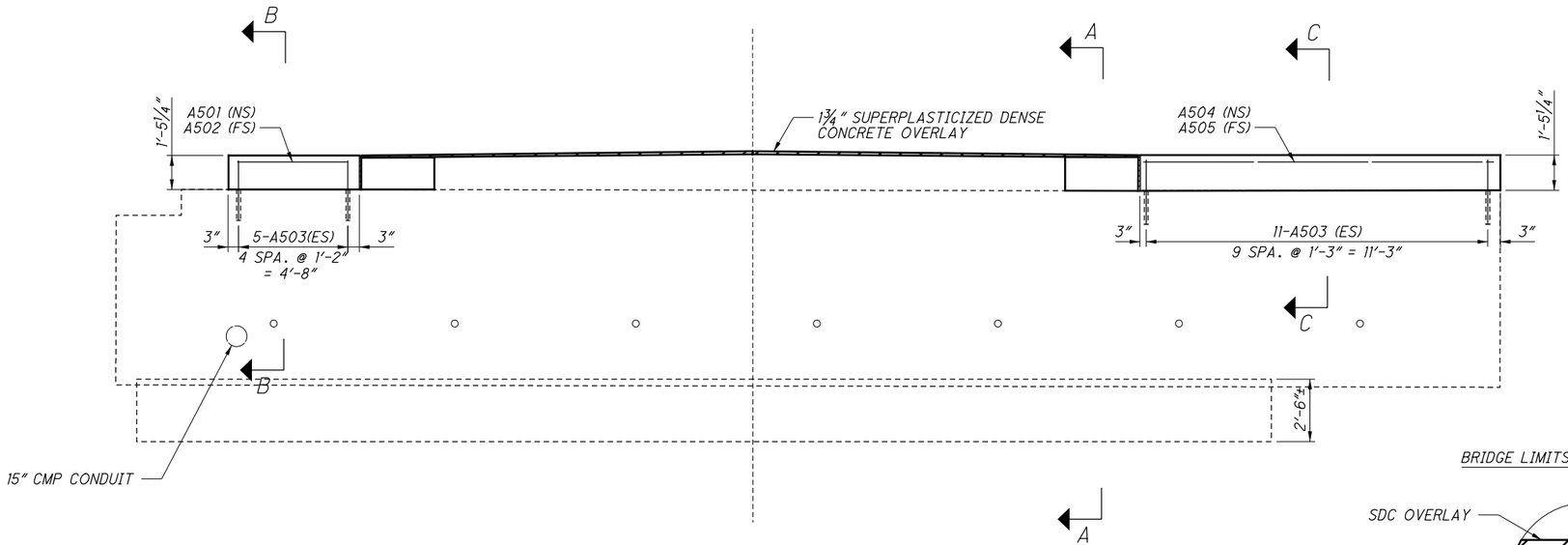
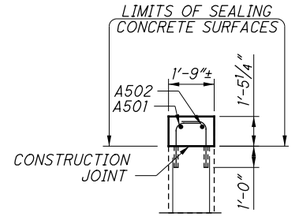
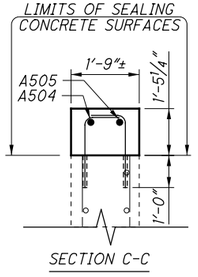


REAR ABUTMENT ELEVATION

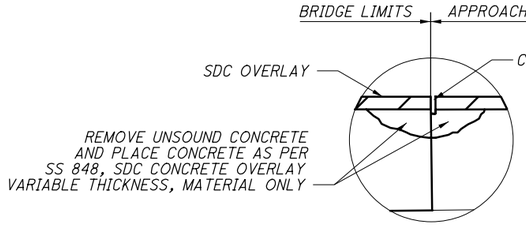
(LIMITS OF REMOVAL)



SECTION B-B



SECTION C-C



DETAIL 'A'

NOTE: THE SUPERPLASTICIZED DENSE CONCRETE OVERLAY FOR THE APPROACH SLABS SHALL BE PLACED A MINIMUM OF 24 HOURS AFTER PLACEMENT OF THE DECK OVERLAY. AFTER THE CURING PERIOD, THE JOINT SHALL BE SAWCUT TO ½" WIDE X 2¼" DEEP AND A PREFORMED ELASTOMERIC COMPRESSION JOINT SEAL, AS PER 705.11 SHALL BE PLACED IN THE GROOVE.

DESIGN AGENCY
OHIO DEPARTMENT
OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
DdG	DdG	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
XXX	XXX	7402368	

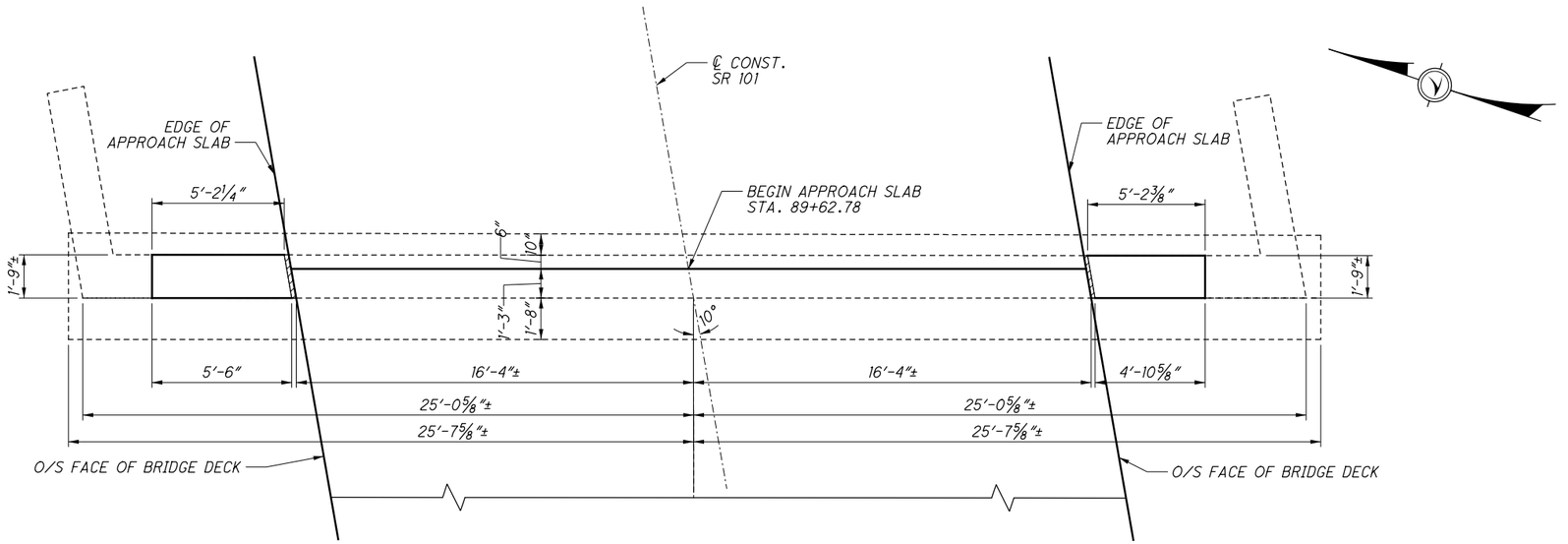
REAR ABUTMENT DETAILS

BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

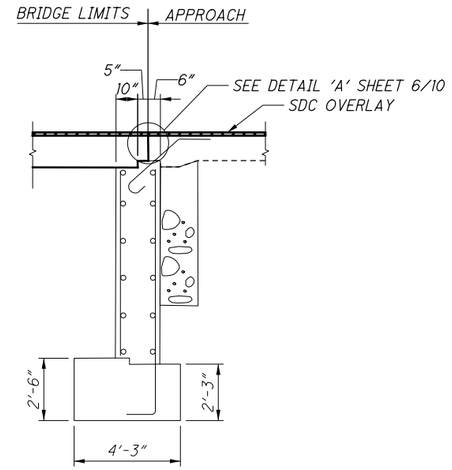
SEN-19/101-
9.57/1.64
PID No. 102817

6 / 10

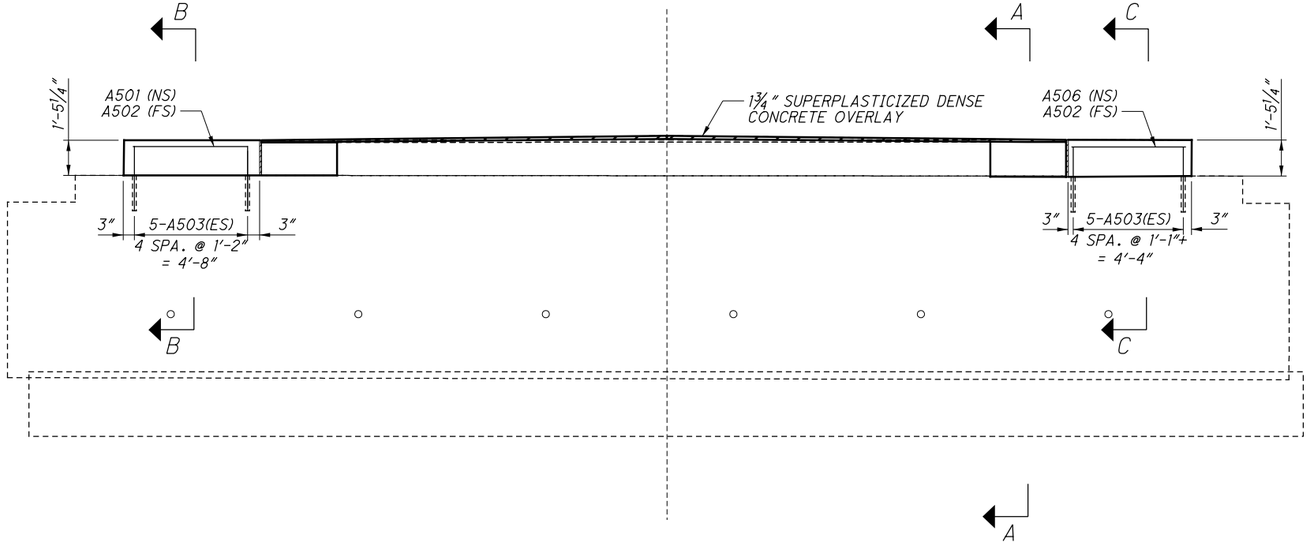
3,5
5,3



FORWARD ABUTMENT PLAN

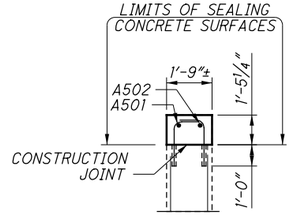


SECTION A-A

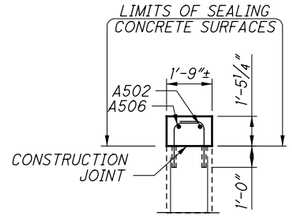


EXISTING FORWARD ABUTMENT ELEVATION

(LIMITS OF REMOVAL)



SECTION B-B



SECTION C-C

DESIGN AGENCY
OHIO DEPARTMENT
OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
DdG	DdG	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
XXX	XXX	7402368	

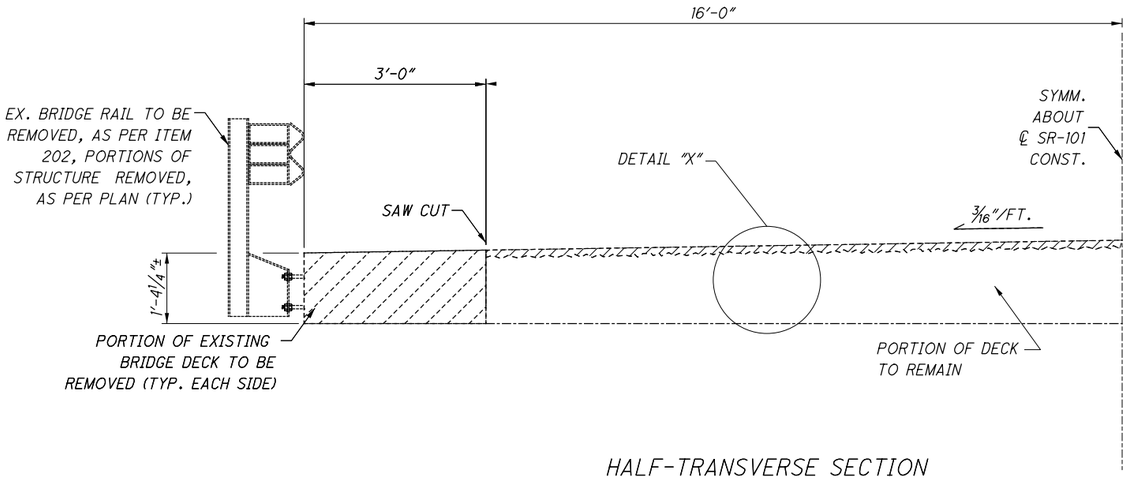
FORWARD ABUTMENT DETAILS

BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

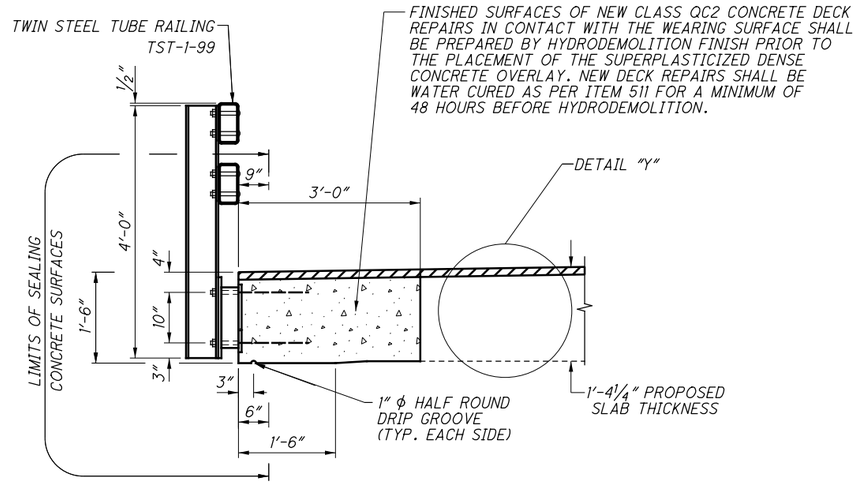
SEN-19/101-9.57/1.64
PID No. 102817

7 / 10

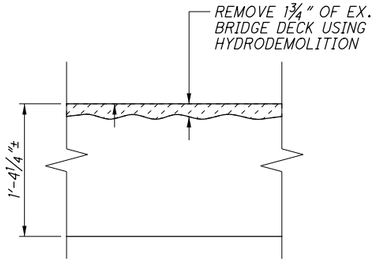
3.6
5.3



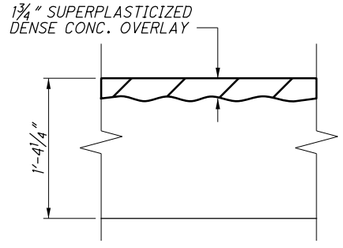
HALF-TRANSVERSE SECTION
(REMOVAL)



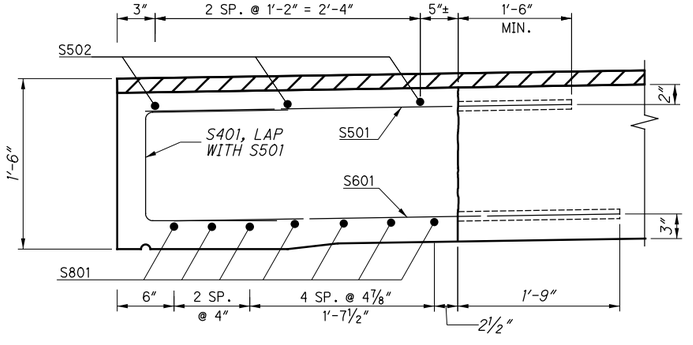
PROPOSED DECK EDGE SECTION



DETAIL "X"
(EXISTING)



DETAIL "Y"
(PROPOSED)



SECTION F-F

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
XXX	G.L.H	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
XXX	XXX	7402368	

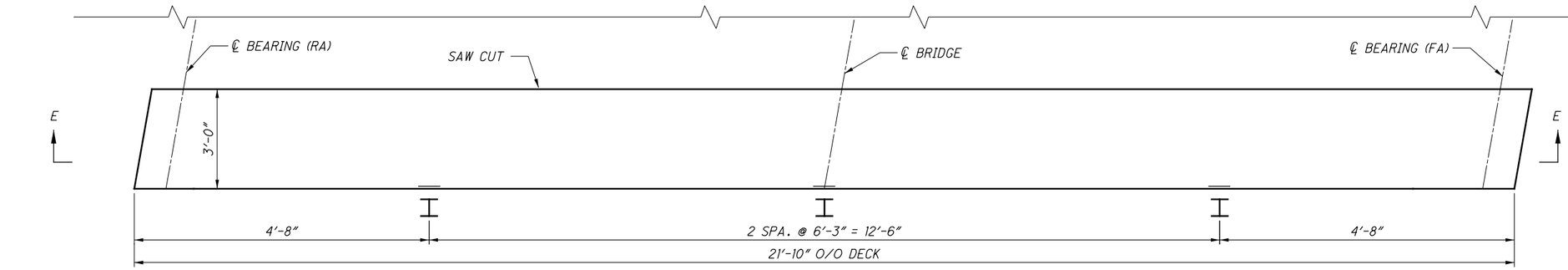
SUPERSTRUCTURE DETAILS
BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

SEN-19/101-9.57/1.64
PID No. 102817

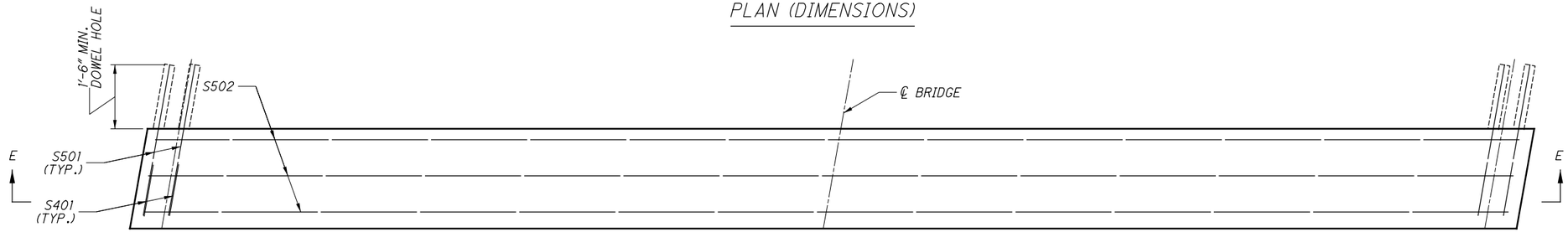
8 / 10

3.7
5.3

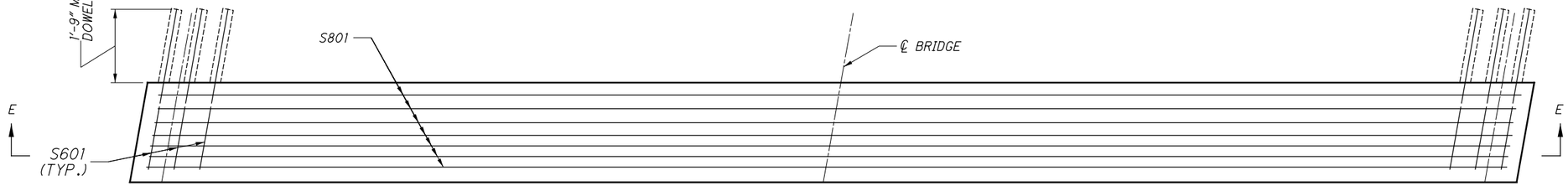
I:\ProjectData\SEN\102817_SEN-19_101-9.57\Design\Structures\SEN101_0172C\Sheets\101_0172C_SS001.dgn Sheet 11/6/2017 8:31:05 AM jthomal0



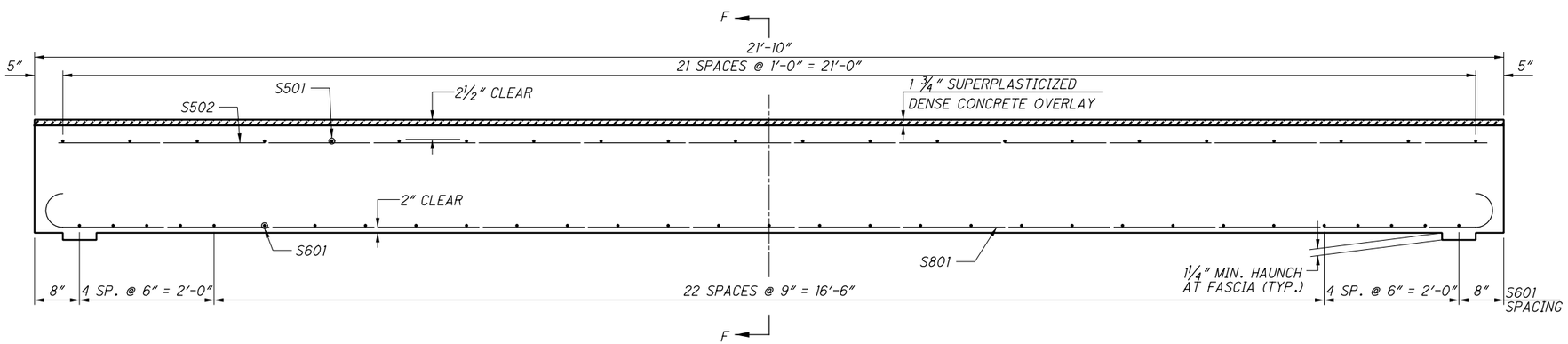
PLAN (DIMENSIONS)



PLAN (TOP REINFORCEMENT)



PLAN (BOTTOM REINFORCEMENT)



SECION E-E

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
XXX	G.L.H	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
XXX	XXX	7402368	

SUPERSTRUCTURE DETAILS

BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

SEN-19/101-9.57/1.64
PID No. 102817

9 / 10

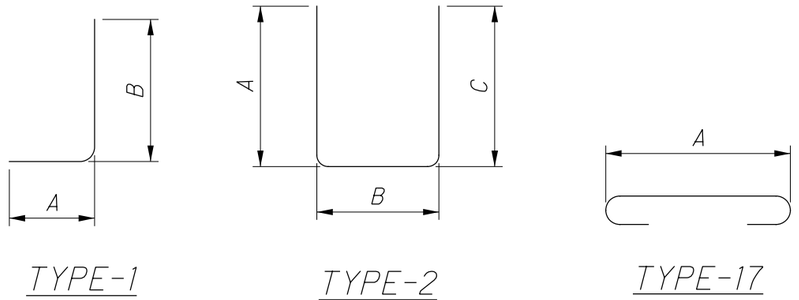
3.8
5.3

ABUTMENTS

MARK	REAR ABUT.	FWD. ABUT.	TOTAL	LENGTH	TYPE	WEIGHT	DIMENSIONS A	B	C	D	E	R
A501	1	1	2	5'-2"	STR.	11						
A502	1	2	3	4'-10"	STR.	15						
A503	32	20	52	3'-5"	1	185	1'-3"	2'-3"				
A504	1		1	11'-4"	STR.	12						
A505	1		1	11'-8"	STR.	12						
A506		1	1	4'-6"	STR.	5						
TOTAL FOR ABUTMENTS = 240 LBS												

SUPERSTRUCTURE

MARK	NUMBER	LENGTH	TYPE	WEIGHT	DIMENSIONS A	B	C	D	E	R
S401	44	4'-7"	2	135	2'-0"	9"	2'-0"			
S501	44	4'-3"	STR.	195						
S502	6	21'-6"	STR.	135						
S601	62	4'-6"	STR.	419						
S801	14	23'-4"	17	872	21'-6"					
TOTAL FOR SUPERSTRUCTURE = 1,756 LBS										



TYPE-1 TYPE-2 TYPE-17

NOTE:

ALL REINFORCING STEEL SHALL BE EPOXY COATED.

THE BAR SIZE IS INDICATED IN THE BAR MARK. THE FIRST DIGIT INDICATES THE BAR SIZE. FOR EXAMPLE, AN A501 IS A #5 BAR. DIMENSIONS SHOWN ARE OUT TO OUT UNLESS OTHERWISE INDICATED. "R" INDICATES THE INSIDE RADIUS.

REINFORCING STEEL MAY REQUIRE FIELD CUTTING OR BENDING TO BE PROPERLY FITTED. PAYMENT SHALL BE INCLUDED WITH THE ASSOCIATED CONCRETE ITEM.

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

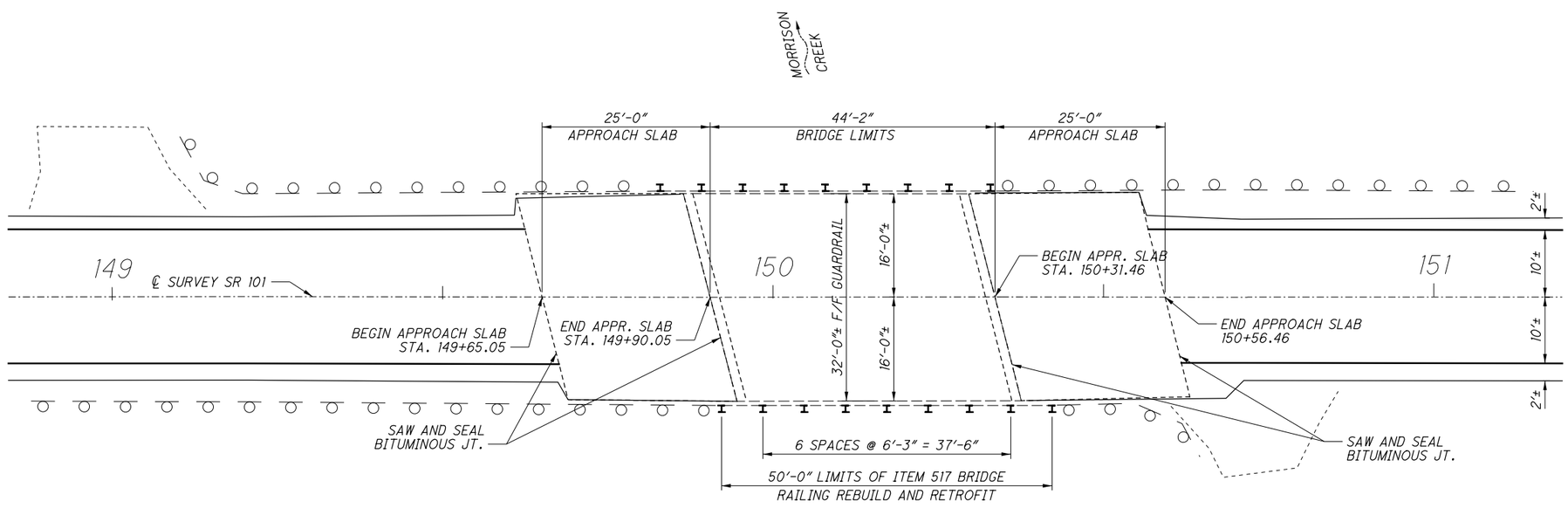
DESIGNED XXX | DRAWN GLH | REVIEWED XXX | DATE MM/DD/YY
CHECKED XXX | REVISED XXX | STRUCTURE FILE NUMBER 7402368

REINFORCING STEEL LIST
BRIDGE NO. SEN-101-0172
OVER WILLOW CREEK

SEN-19/101-9.57/1.64
PID No. 102817

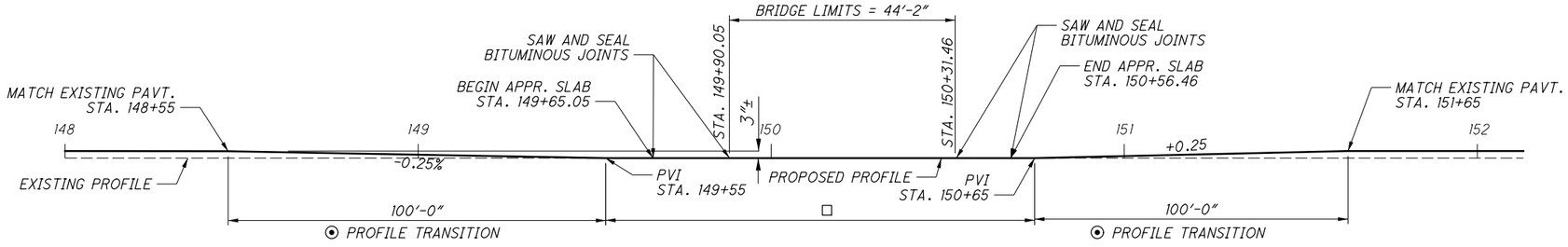
10 / 10

3.9
5.3

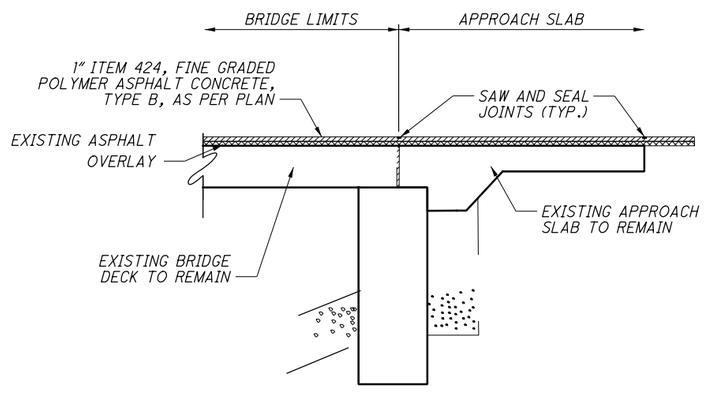


PLAN

* NO WORK SHALL BE PERFORMED BELOW THE OHWM ELEVATION OF 732.0'.



PROFILE ADJUSTMENT DETAIL



SEALING OF JOINTS AT ABUTMENTS AND ENDS OF APPROACH SLABS

⊙ PLANING SHALL VARY FROM 0" AT STATION 148+55 TO 3" AT STATION 149+55 AND 3" AT STATION 150+65 TO 0" AT STATION 151+65.

1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN SHALL BE APPLIED IN THESE SECTIONS.

□ PLANING SHALL BE A CONSTANT 3" FROM STATION 149+55 TO 150+65.

1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN SHALL BE APPLIED IN THIS SECTION.

DESIGN TRAFFIC:

2018 ADT - 6300 2018 ADTT - 380

2030 ADT - 6700 2030 ADTT - 400

DIRECTIONAL DISTRIBUTION - 58%

PROPOSED WORK

1. MAINTAIN TRAFFIC WITH FLAGGERS AS PER STANDARD DRAWING MT-97.10.
2. REMOVE 3" OF EXISTING ASPHALT WEARING SURFACE AS PER ITEM 254, PAVEMENT PLANING.
3. APPLY 1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN AND RETROFIT BRIDGE RAILINGS AS PER STANDARD DRAWING DBR-3-11.
4. SAW AND SEAL BITUMINOUS JOINTS.

EXISTING STRUCTURE

TYPE: PRESTRESSED CONCRETE BOX BEAMS ON CONCRETE WALL TYPE ABUTMENTS

SPAN: 42'-11⅝" C/C BEARINGS

ROADWAY: 32'-0" F/F GUARDRAIL

LOADING: HS 20-44 & ALTERNATE MILITARY LOADING

SKEW: 15° R.F.

APPROACH SLABS: AS-1-81 (25' LONG)

ALIGNMENT: TANGENT

WEARING SURFACE: ASPHALT CONCRETE

STRUCTURAL FILE NUMBER: 7402384

YEAR BUILT: 1985

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED	DRAWN	REVIEWED	DATE
GLH	GLH	XXX	MM/DD/YY
CHECKED	REVISED	STRUCTURE FILE NUMBER	
DJG	XXX	7402384	

COUNTY
STA. 149+90
STA. 150+31

SITE PLAN
BRIDGE NO. SEN-101-0287
OVER MORRISON CREEK

SEN-19/101-9.57/1.64
PID No. 102817

1 / 3

4.0
5.3

ESTIMATED QUANTITIES (02/STR/BR)

ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUT.	SUPER.	GEN.	SEE SHEET
202	38602	100	FT	BRIDGE RAILING REMOVED FOR REUSE		100		
254	01000	918	SY	PAVEMENT PLANING, ASPHALT CONCRETE, VARIES		157	761 *	
407	20000	86	GAL	NON-TRACKING TACK COAT		17	69 *	
424	12001	26	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN		5	21 *	
SPECIAL	51631200	132	FT	SAWING AND SEALING BITUMINOUS CONCRETE JOINTS		66	66 *	
517	75501	100	FT	BRIDGE RAILING REBUILT, AS PER PLAN		100		2
517	75601	100	FT	DEEP BEAM BRIDGE RETROFIT RAILING, AS PER PLAN		100		2
875	10000	53	LB	LONGITUDINAL JOINT ADHESIVE		8	45	

* APPROACH SLABS AND APPROACH PAVEMENT

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

REFER TO THE FOLLOWING STANDARD BRIDGE DRAWING(S):

DBR-2-73 DATED/REVISED 7-19-02
DBR-3-11 DATED/REVISED 7-15-11

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05 AND 513.04.

CONTRACT BID PRICES SHALL BE BASED UPON RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE BY THE CONTRACTOR. HOWEVER, ALL PROJECT WORK SHALL BE BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED BY THE CONTRACTOR IN THE FIELD.

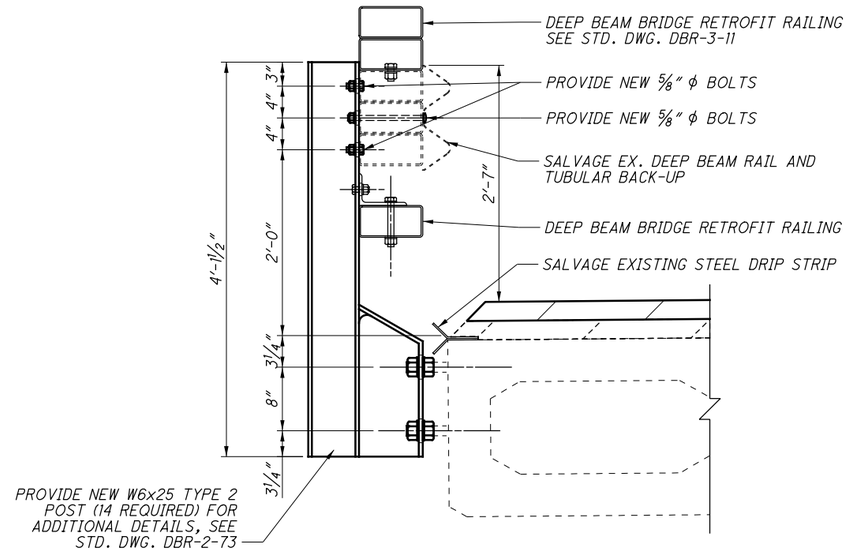
EXISTING BRIDGE PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 2 OFFICE AT 317 EAST POE RD., BOWLING GREEN, OHIO.

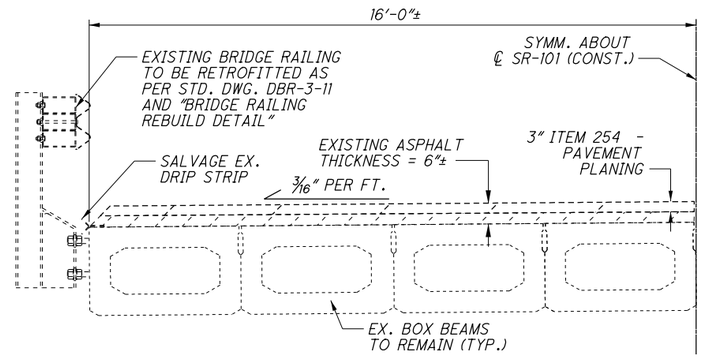
ITEM 517, BRIDGE RAILING REBUILT, AS PER PLAN

THE CONTRACTOR SHALL FURNISH AND INSTALL 14 NEW RAILING POSTS AS SHOWN IN THE PLAN UTILIZING EXISTING BRIDGE BEAM ANCHOR BOLTS, DEEP BEAM RAIL AND STEEL TUBULAR BACKUP. ALL MOUNTING HARDWARE TO INSTALL THE NEW POSTS SHALL BE REPLACED WITH NEW. REBUILT BRIDGE RAILING SHALL CONFORM TO STANDARD DRAWING DBR-2-73. PAYMENT FOR REMOVAL OF OLD POSTS SHALL BE INCLUDED WITH ITEM 202, BRIDGE RAILING REMOVED FOR REUSE.

PAYMENT FOR BRIDGE RAILING REBUILT SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL MATERIALS, LABOR AND EQUIPMENT TO COMPLETE THE WORK TO THE SATISFACTION OF THE ENGINEER.

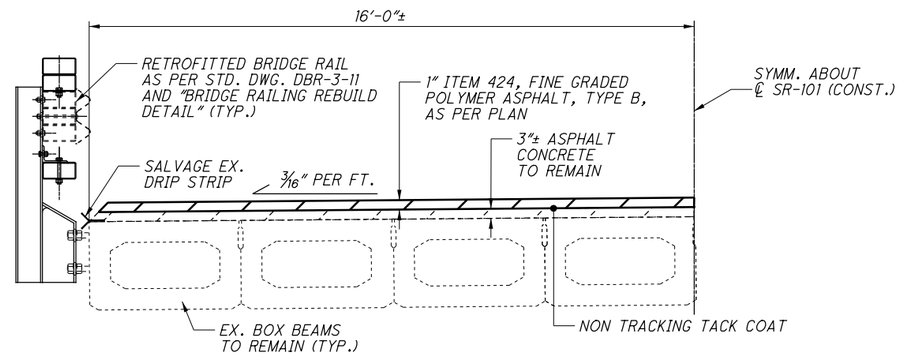


BRIDGE RAILING REBUILD DETAIL



HALF-TRANSVERSE SECTION

(EXISTING)



HALF-TRANSVERSE SECTION

(PROPOSED)

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

DESIGNED GLH | DRAWN GLH | REVIEWED XXX | DATE MM/DD/YY
CHECKED DJG | REVISED XXX | STRUCTURE FILE NUMBER 7402384

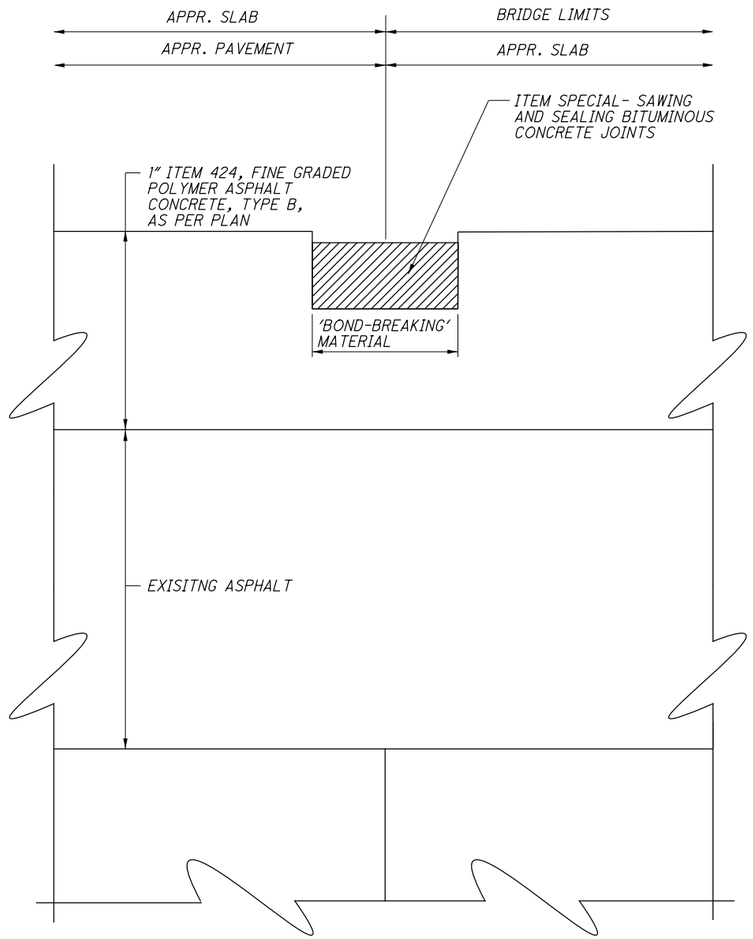
ESTIMATED QUANTITIES AND NOTES

BRIDGE NO. SEN-101-0287
OVER MORRISON CREEK

SEN-19/101-9.57/1.64
PID No. 102817

2 / 3

41
5.3



SEALING OF JOINTS AT ABUTMENTS
AND ENDS OF APPROACH SLABS
(APPLIES TO SEN-101-0287 STRUCTURE)

ITEM SPECIAL - SAVING AND SEALING BITUMINOUS CONCRETE JOINTS

1. DESCRIPTION

THIS WORK SHALL CONSIST OF CUTTING AND SEALING TRANSVERSE JOINTS IN THE NEW BITUMINOUS CONCRETE OVERLAY. BITUMINOUS CONCRETE JOINTS SHALL BE CONSTRUCTED DIRECTLY OVER, AND IN LINE WITH, THE EXISTING UNDERLYING TRANSVERSE JOINT OF THE APPROACH SLAB & APPROACH PAVEMENT.

2. MATERIALS

THE JOINT SEALENT SHALL MEET THE REQUIRMENTS OF ITEM 905.04, JOINT SEALENTS, HOT-POURED, FOR CONCRETE AND ASPHALT PAVEMENTS. ACCEPTABLE ALTERNATE MATERIALS ARE. A SILICONE SEALENT MEETING FEDERAL SPECIFICATIONS TT-S-001543a CLASS A -ONE-PART SILICONE SEALENTS. AND TT-S-00230C CLASS A -ONE-COMPONENT SEALENTS; SUCH AS THOSE MANUFACTURED BY GENERAL ELECTRIC, , SILICONE PRODUCTS DIVISIONS, 4015 EXECUTIVE DRIVE, CINCINNATI, OHIO 45242 (513-246-1953) OR DOW CORNING, 400 RECHNE CENTER, SUITE 103, MILFORD, OHIO 45150 (513-831-3586), OR SOF-SEAL. A COLD-APPLIED, LOW MODULUS, TWO--COMPONENT POLYMERIC COMPOUND HORIZONTAL SEALENT AS MANUFACTURED BY W.R.MEADOWS, INC., P.O. BOX 543, ELGIN, ILLINOIS-60121 (800-342-5976).

3. CONSTRUCTION DETAILS

A) GENERAL, THE CONTRACTOR SHALL CONDUCT HIS OPERATION SO THAT THE CUTTING, CLEANING AND SEALING OF TRANSVERSE JOINTS IS A CONTINUOUS OPERATION THAT WILL BE PERFORMED AS SOON AS PRACTICAL AFTER THE PAVING. BUT NO LATER THAN FOUR *4* DAYS AFTER PLACEMENT OF ASPHALT CONCRETE SURFACE COURSE. TRAFFIC SHALL NOT BE ALLOWED TO KNEAD TOGETHER OR DAMAGE JOINT CUT PRIOR TO SEALING

B) CUTTING OF TRANSVERSE JOINTS: THE CONTRACTOR SHALL SAW OR ROUT TRANSVERSE JOINTS TO THE DIMENSIONS SHOWN IN THE DETAILS ON THIS SHEET. THE CUT JOINTS SHALL LIE DIRECTLY ABOVE EACH APPROACH SLAB END. THE BLADE OR BLADES SHALL BE OF SUCH SIZE THAT THE FULL WIDTH AND DEPTH OF THE CUT CAN BE MADE WITH ONE PASS. DRY OR WET CUTTING WILL BE ALLOWED. JOINTS SHALL EXTEND THE FULL WIDTH OF BRIDGE.

C) CLEANING JOINTS: DRY SAWED JOINTS SHALL BE THOROUGHLY CLEANED WITH A SUFFICIENT AMOUNT OF COMPRESSED AIR TO REMOVE ANY DIRT, DUST, OR DELETERIOUS MATTER. WET SAWED JOINTS SHALL BE WASHED CLEAN OF ALL CUTTINGS BY FLUSHING WITH A JET OF WATER AND WITH OTHER TOOLS AS NECESSARY. AFTER FLUSHING, THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR . WHEN THE SURFACES ARE THOROUGHLY CLEAN AND DRY, AND JUST PRIOR TO PLACING THE JOINT SEALER, COMPRESSED AIR HAVING A PRESSURE OF AT LEAST 90 PSI SHALL BE USED TO BLOW OUT THE JOINT AND REMOVE ALL TRACES OF DUST. IN THE EVENT FRESHLY CUT JOINTS BECOME CONTAMINATED BEFORE THEY ARE SEALED, THEY SHALL BE RECLEANED OF ALL FOREIGN MATERIAL BY HIGH PRESSURE WATER JET.

D) SEALING JOINTS: THE JOINT SHALL BE THOROUGHLY DRY WHEN THE SEALANT IS PLACED. AFTER CLEANING AND DRYING, A BOND-BREAKER MATERIAL SHALL BE APPLIED TO THE BOTTOM OF THE GROOVE.

HOT-POURED JOINT SEALANT MATERIAL SHALL BE HEATED IN A KETTLE OR MELTER CONSTRUCTED AS A DOUBLE BOILER, WITH THE SPACE BETWEEN THE INNER AND OUTER SHELLS FILLED WITH OIL OR OTHER HEAT TRANSFER MEDIUM. POSITIVE TEMPERATURE CONTROL AND MECHANICAL AGITATION SHALL BE PROVIDED. HEATING MUST BE IN STRICT ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION. JOINT SEALER MATERIAL SHALL NEVER BE KEPT HEATED AT THE POURING TEMPERATURE FOR MORE THAN FOUR (4) HOURS AND SHALL NEVER BE REHEATED. SEALER LEFT IN THE APPLICATION AT THE END OF A DAY'S WORK SHALL NOT BE USED.

HOT-POURED SEALANT SHALL BE APPLIED IMMEDIATELY THROUGH A NOZZLE, WHICH MUST PROJECT INTO THE SAWED JOINT, FILLING FROM THE BOTTOM UP. THE SEALANT SHALL COMPLETELY FILL THE JOINT IN SUCH A MANNER THAT, AFTER COOLING, THE LEVEL OF THE SEALANT WILL NOT BE HIGHER THAN 1/8" BELOW THE PAVEMENT SURFACE. ANY DEPRESSION IN THE COOLED SEAL GREATER THAN 3/16" SHALL BE BROUGHT UP TO THE SPECIFIED LIMIT BY FURTHER ADDITION OF HOT-POURED SEALANT. CARE SHALL BE TAKEN IN THE SEALING OF THE JOINTS SO THAT THE FINAL APPEARANCE WILL PRESENT A NEAT FINE LINE.

THE COLD APPLIED SEALANT MATERIALS (POLYURETHANE, SILICONE, AND POLYMERIC COMPOUNDS) SHALL BE INSTALLED AS PER MANUFACTURERS' RECOMMENDATIONS, EXCEPT AS MODIFIED BY THIS DRAWING. THE SEALANT SHALL BE INSTALLED WHEN THE AMBIENT TEMPERATURE IS 40 DEGREES F OR HIGHER. TRAFFIC SHALL NOT BE ALLOWED ON THE JOINT FOR ONE HOUR AFTER THE APPLICATION OF THE SEALANT.

4. METHOD OF MEASUREMENT

THE QUANTITY TO BE PAID FOR UNDER THIS ITEM WILL BE THE NUMBER OF LINEAR FEET OF JOINTS SAWED AND SEALED AS PER THE ABOVE REQUIREMENTS.

5. BASIS OF PAYMENT

THE UNIT PRICE PER LINEAR FOOT FOR ITEM SPECIAL- "SAWING AND SEALING BITUMINOUS CONCRETE JOINTS" SHALL INCLUDE THE COST OF ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK, INCLUDING THE FURNISHING AND PLACING OF THE JOINT SEALER MATERIAL.

THIS ITEM SHALL MEET THE MATERIAL (SECTION 2) AND SEALING (SECTION 3D) SPECIFICATIONS OF ITEM SPECIAL- SAWING AND SEALING BITUMINOUS CONCRETE JOINTS.

DESIGN AGENCY
OHIO DEPARTMENT OF TRANSPORTATION

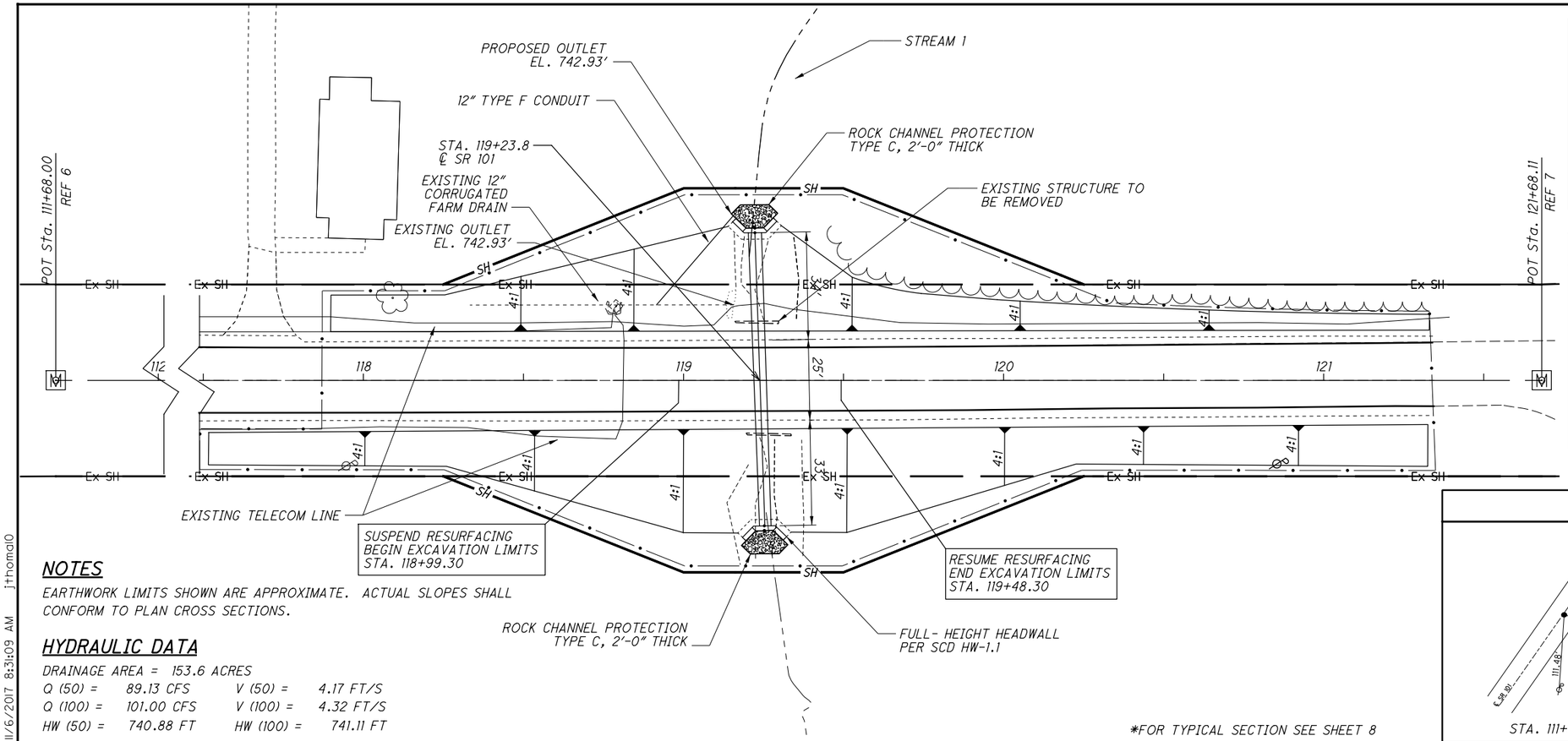
DESIGNED GLH | DRAWN GLH | REVIEWED XXX | DATE MM/DD/YY
CHECKED DJG | REVISED XXX | STRUCTURE FILE NUMBER 7402384

MISCELLANEOUS DETAILS
BRIDGE NO. SEN-101-0287
OVER MORRISON CREEK

SEN-19/101-9.57/1.64
PID No. 102817

3 / 3

4.2
5.3



NOTES

EARTHWORK LIMITS SHOWN ARE APPROXIMATE. ACTUAL SLOPES SHALL CONFORM TO PLAN CROSS SECTIONS.

HYDRAULIC DATA

DRAINAGE AREA = 153.6 ACRES

Q (50) =	89.13 CFS	V (50) =	4.17 FT/S
Q (100) =	101.00 CFS	V (100) =	4.32 FT/S
HW (50) =	740.88 FT	HW (100) =	741.11 FT

EXISTING STRUCTURE

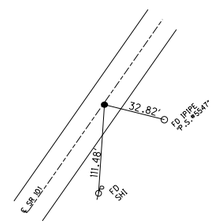
TYPE: 48" X 60" SLAB TOP STONE
SPAN: 36' F/F CULVERT
ROADWAY: 25'0"
ALIGNMENT: STRAIGHT
APPROACH SLABS: NONE
SKEW: 1°53'04.9"
WEARING SURFACE: ASPHALT CONCRETE

PROPOSED STRUCTURE

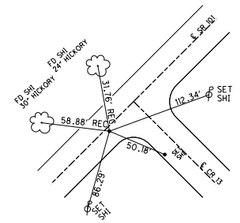
TYPE: 48" RCP WITH FULL HEIGHT HEADWALLS
SPAN: 92'-0" F/F CULVERT
ROADWAY: 25'0"
ALIGNMENT: STRAIGHT
APPROACH SLABS: NONE
SKEW: 1°53'04.9"
WEARING SURFACE: ASPHALT CONCRETE
LOADING: HL-93
CFN: 1843576

*FOR TYPICAL SECTION SEE SHEET 8

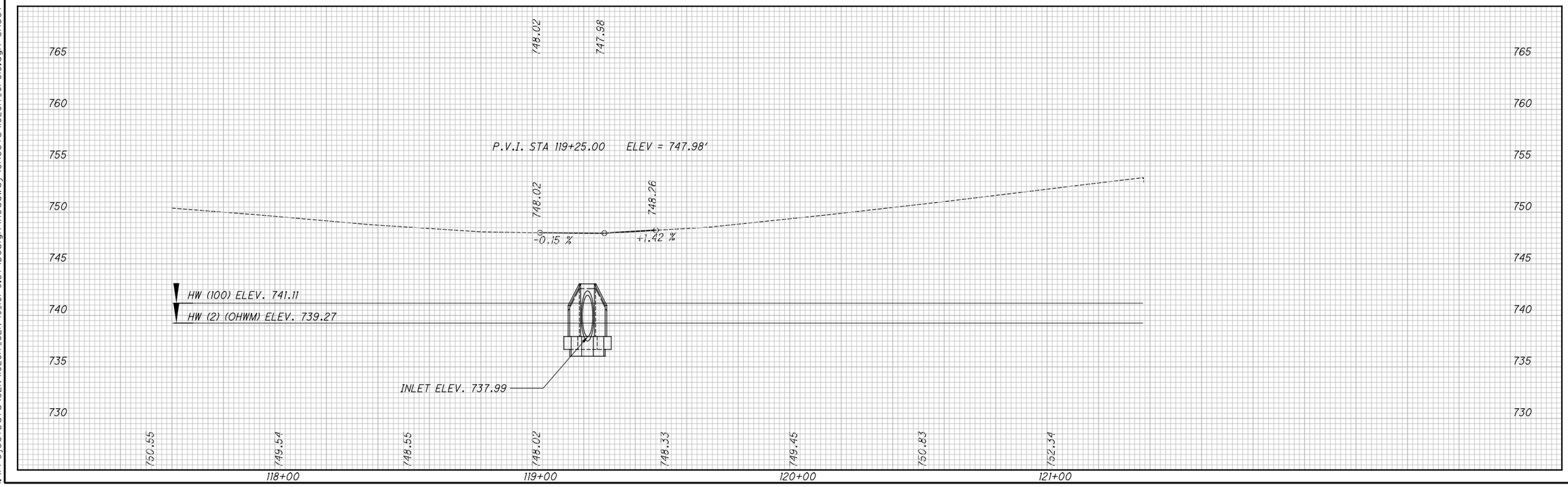
REFERENCE TIES



STA. 111+68.00



STA. 121+68.11



CALCULATED JBT CHECKED JMF

HORIZONTAL SCALE IN FEET
0 10 20 40

PLAN AND PROFILE
SEN-101-0228

SEN-19 / 101-
9.57 / 1.64

4.3
5.3

FARM DRAINS

ALL FARM DRAINS, WHICH ARE ENCOUNTERED DURING CONSTRUCTION, SHALL BE PROVIDED WITH UNOBSTRUCTED OUTLETS. EXISTING COLLECTORS WHICH ARE LOCATED BELOW THE ROADWAY DITCH ELEVATIONS, AND WHICH CROSS THE ROADWAY, SHALL BE REPLACED WITHIN THE (RIGHT OF WAY) (CONSTRUCTION) LIMITS BY ITEM 611 CONDUIT, TYPE B, ONE COMMERCIAL SIZE LARGER THAN THE EXISTING CONDUIT.

EXISTING COLLECTORS AND ISOLATED FARM DRAINS, WHICH ARE ENCOUNTERED ABOVE THE ELEVATION OF ROADWAY DITCHES, SHALL BE OUTLETTED INTO THE ROADWAY DITCH BY 611 TYPE F CONDUIT. THE OUTLET ELEVATION SHALL REMAIN AT THE SAME ELEVATION OR HIGHER THAN THE EXISTING ELEVATION. LATERAL FIELD TILES WHICH CROSS THE ROADWAY SHALL BE INTERCEPTED BY 611, TYPE E CONDUIT, AND CARRIED IN A LONGITUDINAL DIRECTION TO AN ADEQUATE OUTLET OR ROADWAY CROSSING.

THE LOCATION, TYPE, SIZE AND GRADE OF REPLACEMENTS SHALL BE DETERMINED BY THE ENGINEER AND PAYMENT SHALL BE MADE ON FINAL MEASUREMENTS.

EROSION CONTROL PADS SHALL BE PROVIDED AT THE OUTLET END OF ALL FARM DRAINS AS PER STANDARD CONSTRUCTION DRAWING DM-1.1, EXCEPT WHEN THEY OUTLET INTO A DRAINAGE STRUCTURE. PAYMENT FOR THE EROSION CONTROL PADS AND ANY NECESSARY BENDS OR BRANCHES SHALL BE INCLUDED FOR PAYMENT IN THE PERTINENT CONDUIT ITEMS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

611 12" CONDUIT, TYPE F 30 FT.

QUANTITY CARRIED TO GENERAL SUMMARY

MONUMENT ASSEMBLIES

CONSTRUCT MONUMENT ASSEMBLIES IN ACCORDANCE WITH THE DETAILS SHOWN ON THE STANDARD CONSTRUCTION DRAWINGS AND AT THE LOCATIONS SHOWN ON SHEET NO. 43.

623 MONUMENT ASSEMBLY 2 EACH

QUANTITY CARRIED TO GENERAL SUMMARY

Project				
U.S. Survey Feet Grid				
Point	Station	Northing	Easting	Comment
Ref 6	111+68.00	532928.850	1788680.773	MAGF
Ref 7	121+68.11	533599.794	1789422.353	MAGF

SEEDING AND MULCHING

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

659, TOPSOIL 237 CU. YD.

659, SEEDING AND MULCHING 2129.6 SQ. YD.

659, REPAIR SEEDING AND MULCHING 106.48 SQ. YD

659, COMMERCIAL FERTILIZER 0.29 TON

659, LIME 0.44 ACRES

659, WATER 11.5 M. GAL.

QUANTITIES CARRIED TO GENERAL SUMMARY

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

ESTIMATED QUANTITIES (05/STR/CV)

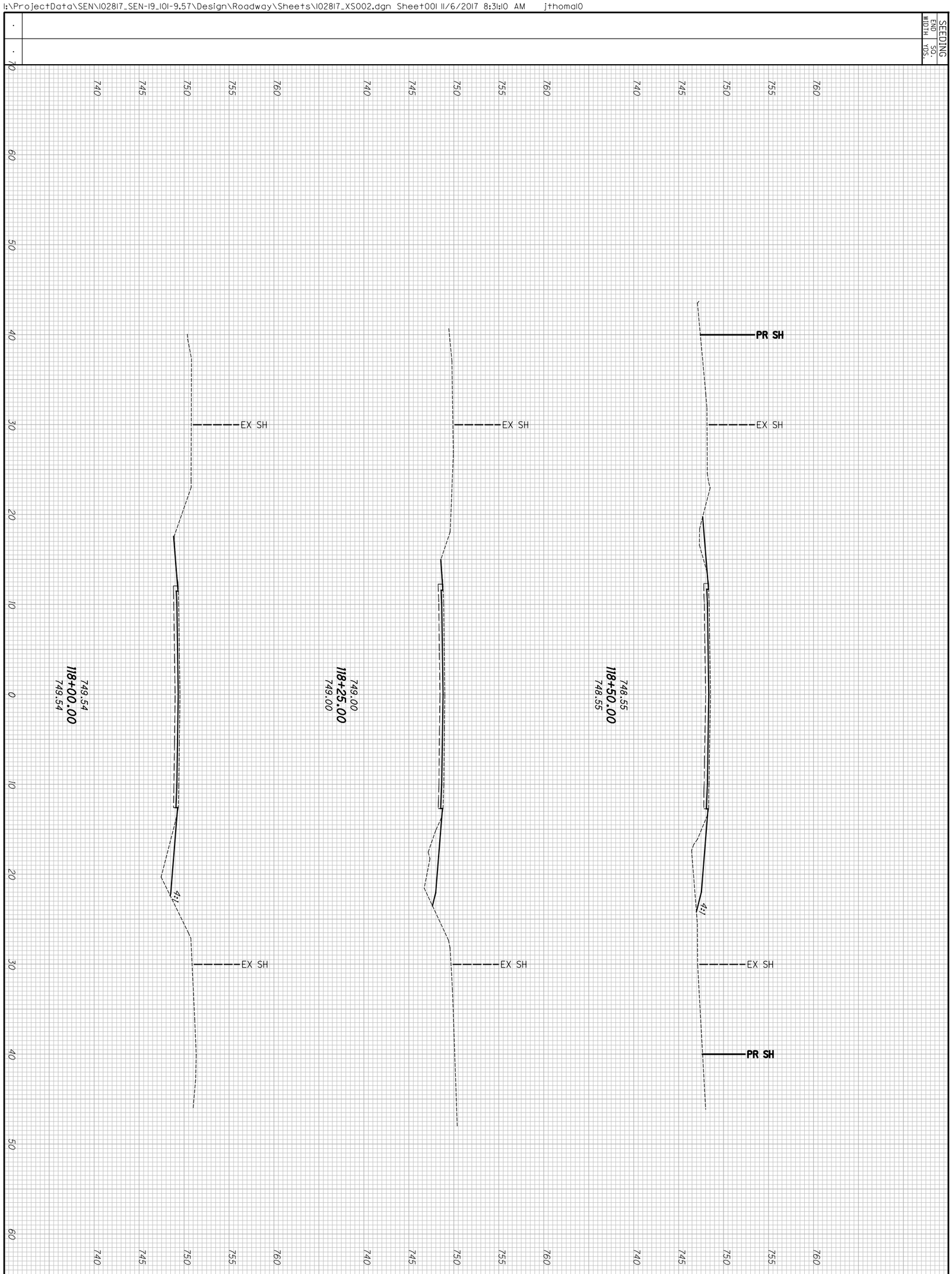
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUT.	SUPER.	GEN.	SEE SHEET
202	11000	LS		STRUCTURE REMOVED				
203	10000	182	CY	EXCAVATION				
203	20000	459	CY	EMBANKMENT				
301	46000	31.46	CY	ASPHALT CONCRETE BASE, PG64-22				
304	20000	23.59	CY	AGGREGATE BASE				
407	20000	178.36	GAL	NON-TRACKING TACK COAT				
424	12001	3.93	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN				
441	50300	7.86	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)				
601	32200	12	CY	ROCK CHANNEL PROTECTION, TYPE C WITH FILTER				
602	20000	8.5	CY	CONCRETE MASONRY				
611	20700	92	FT	48" CONDUIT, TYPE A				

CALCULATED JBT CHECKED JMF

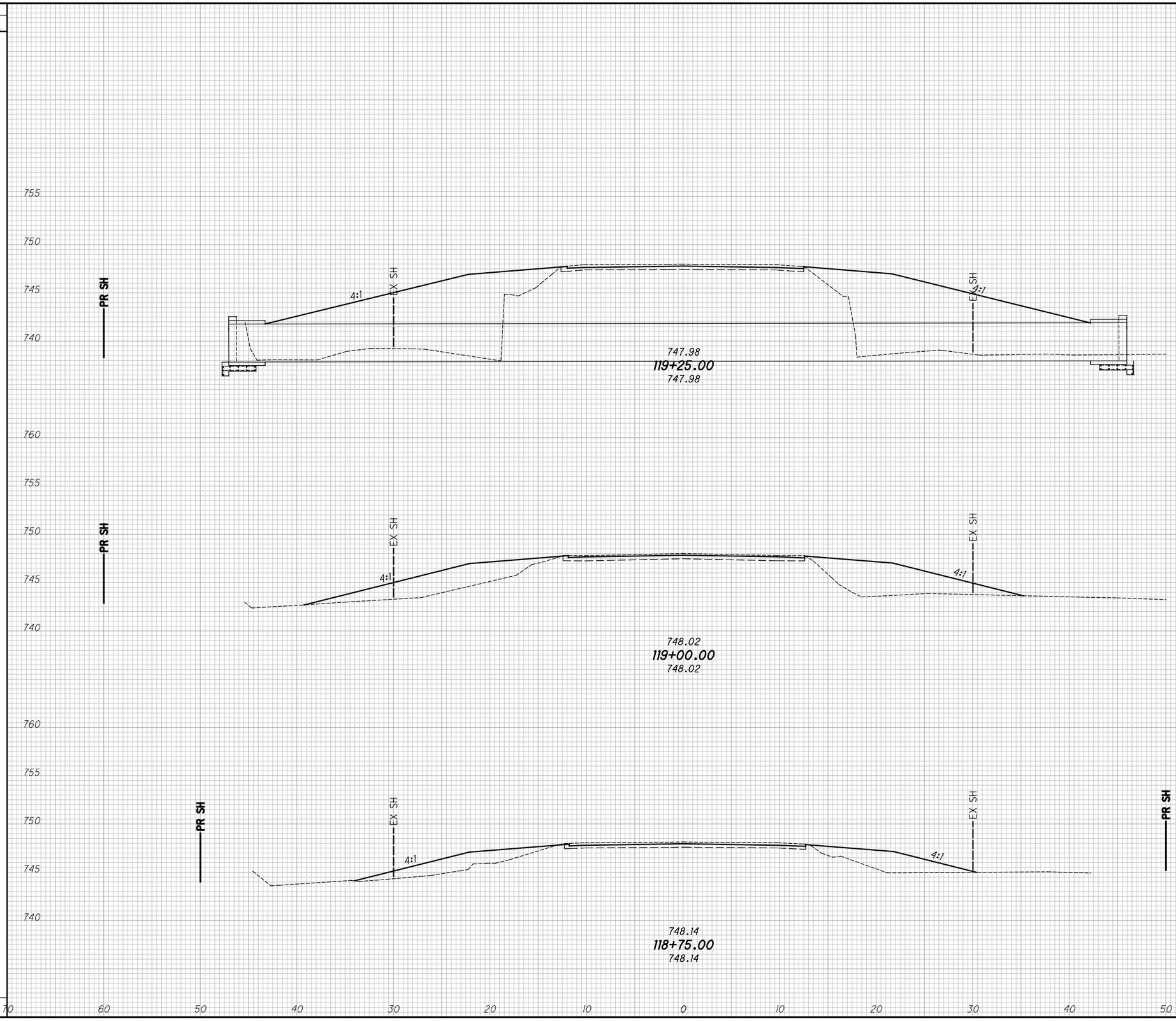
GENERAL NOTES-CULVERT 2.28

SEN-19 / 101-9.57 / 1.64

4.4
5.3



45 / 53	SEN-19 / 101- 9.57 / 1.64	CROSS SECTIONS SR-101 STA. 118+00.00 TO STA. 118+50.00
		CALCULATED JBT CHECKED JMF



SEEDING	
END WIDTH	SQ. YDS.

END AREA		VOLUME	
CUT	FILL	CUT	FILL
		90.7	113.1
195.9	166.1		
		90.7	114.9
0	82.2		
		0	57.2
0	41.4		
195.9	289.7	181.4	285.2

CALCULATED JBT
CHECKED JMF

CROSS SECTIONS SR-101
STA. 118+75.00 TO STA. 119+25.00

SEN-19 / 101-
9.57 / 1.64

4.6
5.3

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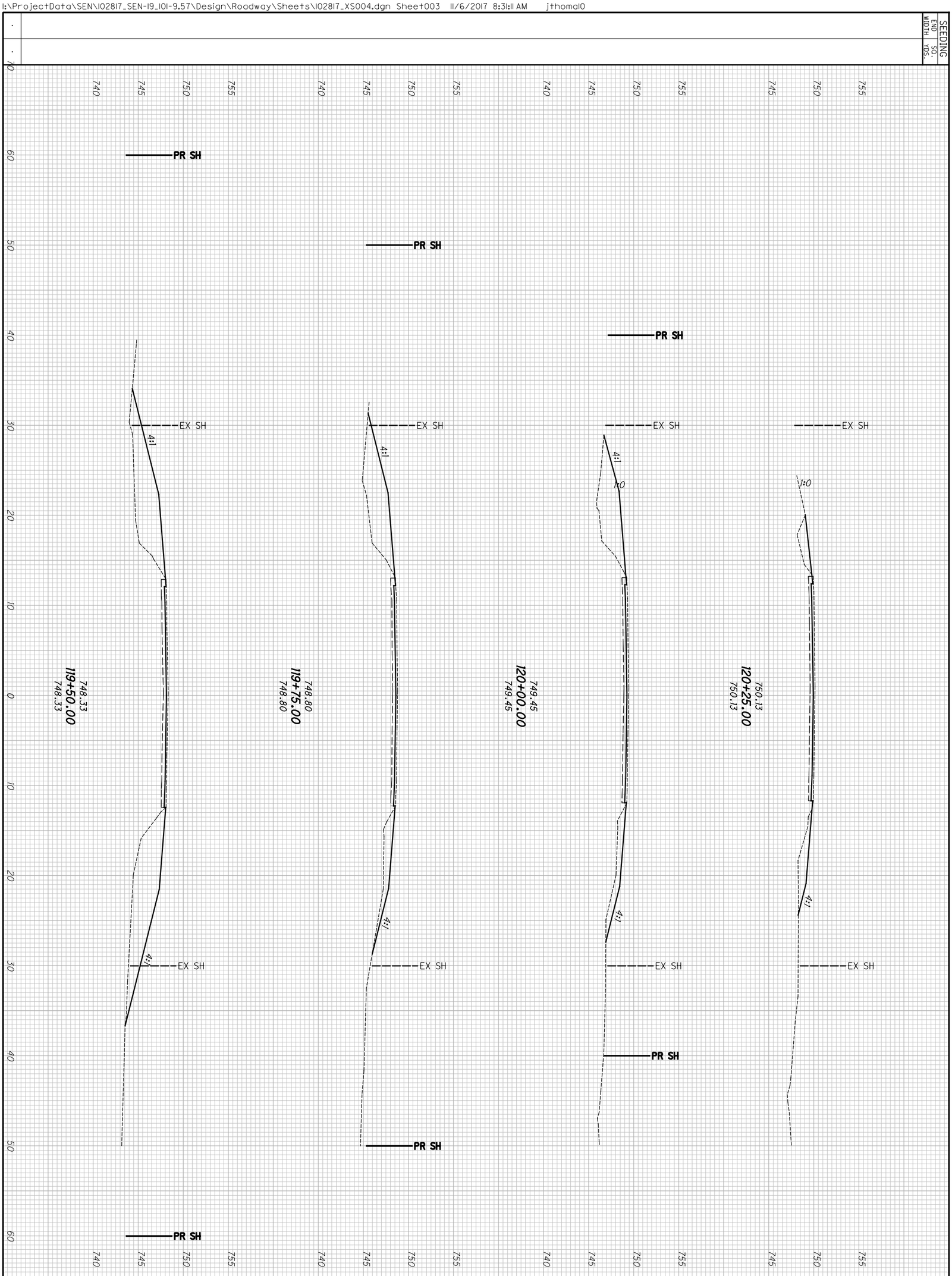

4.7 / 5.3	SEN-19 / 101- 9.57 / 1.64	CROSS SECTIONS SR-101 STA. 119+50.00 TO STA. 120+25.00	CALCULATED JBT CHECKED JMF

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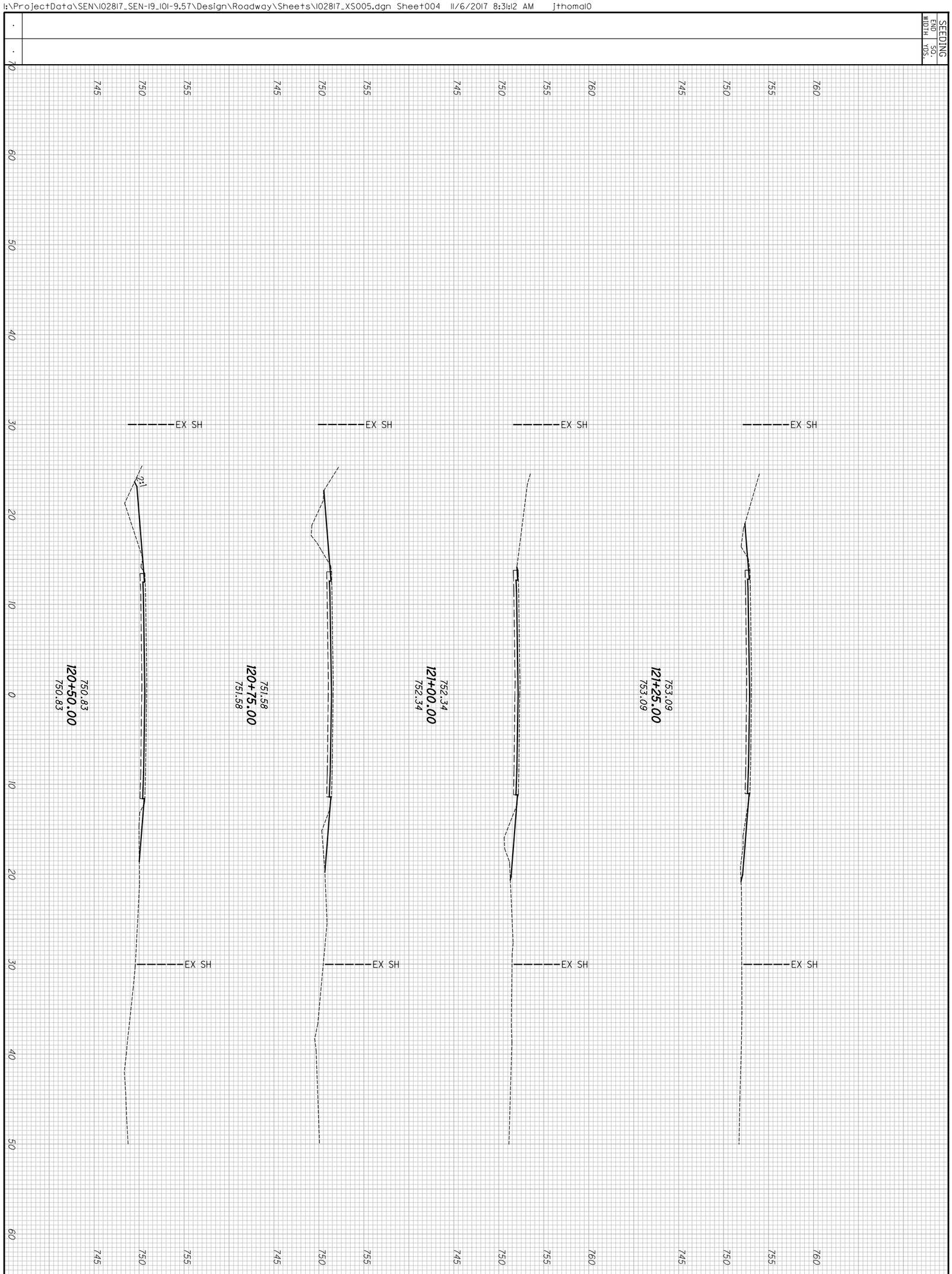
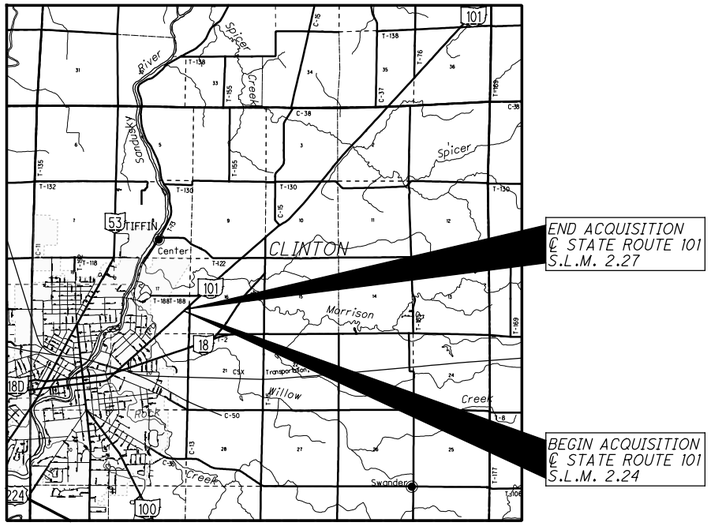


48 / 53	SEN-19 / 101-9.57 / 1.64	CROSS SECTIONS SR-101 STA. 120+50.00 TO STA. 121+25.00

CALCULATED JBT
CHECKED JMF

RIGHT OF WAY LEGEND SHEET
SEN-101-2.25

SENECA COUNTY
CLINTON TOWNSHIP
T-2-N, R-15-E
SECTION 17



LATITUDE: 41°07'45" LONGITUDE: 83°09'03"

SCALE IN MILES
0 1 2 3 4

N

UTILITY LIST		
CITY OF TIFFIN 51 E. MARKET ST. TIFFIN, OH 44883 (419) 448-5425	AT&T 130 N. ERIE ST. TOLEDO, OH 43624 (419) 245-7304	AQUA OHIO 365 E. CENTER ST. MARION, OH 43302 (740) 383-0972
AEP AMERICAN ELECTRIC POWER 2622 STATE ROUTE 100 TIFFIN, OH 44883 (419) 209-5583	COLUMBIA GAS OF OHIO, INC. 2901 E. MANHATTAN BLVD. TOLEDO, OH 43611 (419) 539-6066	

NOTES: THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE OBTAINED FROM THE OWNER OF THE UTILITIES AS REQUIRED BY SECTION 153.64 O.R.C.

CONVENTIONAL SYMBOLS

County Line
Township Line
Section Line
Corporation Line or
Fence Line (Ex) (Pr)
Center Line
Right of Way (Ex) Ex R/W
Right of Way (Pr) R/W
Standard Highway Ease.(Ex) Ex SH
Temporary Right of Way TMP
Channel Ease. (Pr) CH
Utility Ease. (Ex) Ex U
Railroad or
Guardrail (Ex) (Pr)
Construction Limits
Edge of Pavement (Ex)
Edge of Pavement (Pr)
Edge of Shoulder (Ex)
Edge of Shoulder (Pr)

Ditch / Creek (Ex)
Ditch / Creek (Pr)
Tree Line (Ex)
Ownership Hook Symbol , Example
Property Line Symbol , Example
Break Line Symbol , Example
Tree (Pr) , Tree (Ex) , Shrub (Ex)
Tree (Remove) , Shrub (Remove)
Evergreen (Ex) , Stump
Evergreen (Remove) , Stump (Remove)
Wetland (Pr) , Grass (Pr) , Aerial Target
Post (Ex) , Mailbox (Ex) MB , Mailbox (Pr) MB
Light (Ex) , Telephone Marker (Ex) TEL
Fire Hydrant (Ex) , Water Meter (Ex) WM
Water Valve (Ex) , Utility Valve Unknown (Ex.) UV
Telephone Pole (Ex) , Power Pole (Ex)
Light Pole (Ex)

INDEX OF SHEETS:

PROJECT DESCRIPTION

A DISTRICT ALLOCATION FUNDED PROJECT TO RESURFACE SR-101 AND SR-19 IN SENECA COUNTY; PERFORM NECESSARY RELATED WORK. REPLACE CULVERT AT SEN-101-2.24.

PROJECT CONTROL

STATE PLANE GRID - OHIO NORTH NAD 83 (2011)
PROJECT ADJUSTMENT FACTOR - 1.0000000
GEOID MODEL - 2012A

NOTE: THE EXISTING R/W WIDTH AND LOCATION WERE DETERMINED USING O.D.O.T. PLANS TITLED "TIFFIN-BELLEVUE ROAD I.C.H. NO. 271 SEC. 1" FROM 1925, "TIFFIN-BELLEVUE ROAD S.H. 271 SEC. 'I' & TIFFIN (PT.)" FROM 1936,A BOUNDARY SURVEY FOR ATA/MANN PERFORMED BY JAMES G. HOMAN IN JULY 2012, AND RECORDS ON FILE WITH THE SENECA COUNTY ENGINEERS OFFICE FOR SECTION 17, T-2-N, R-15-E, CLINTON TOWNSHIP.

PLANS PREPARED BY:

FIRM NAME : O.D.O.T.
R/W DESIGNER: MARTIN J. FARKAS
R/W REVIEWER: DAVID E. SEASLY
FIELD REVIEWER: MARTIN J. FARKAS
PRELIMINARY FIELD REVIEW DATE: 12OCT16
TRACINGS FIELD REVIEW DATE: 12OCT16
OWNERSHIP UPDATED BY: MARTIN J. FARKAS
DATE COMPLETED: 31 JANUARY 2017
PLAN COMPLETION DATE: 31 JANUARY 2017

TYPES OF TITLE LEGEND:
SH = STANDARD HIGHWAY EASEMENT

STRUCTURE KEY

RESIDENTIAL
COMMERCIAL
OUT-BUILDING

MONUMENT LEGEND

EXISTING R/W MONUMENT BOX
PROPOSED R/W MONUMENT BOX
EXISTING CONCRETE MONUMENT
PROPOSED CONCRETE MONUMENT
RAILROAD SPIKE FOUND
RAILROAD SPIKE SET
I.R.F. IRON PIN FOUND
I.R.F. IRON PIN FOUND W/ ID CAP
I.R.S. IRON PIN SET W/ ID CAP
P.R.F. IRON PIPE FOUND
P.R.S. IRON PIPE SET
P.K.F. P.K. NAIL FOUND
P.K.S. P.K. NAIL SET
M.N.F. MAG NAIL FOUND
M.N.S. MAG NAIL SET

I, David E. Seasly, P. S. have conducted a survey of the existing conditions for the Ohio Department of Transportation on 17 November 2016. The results of that survey are contained herein. The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System North Zone on NAD 83 (2011) datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (US Survey Feet) by a Project Adjustment Factor of 1.0000000. As a part of this project I have reestablished the locations of the existing property lines and the existing centerline of Right of Way for property takes contained herein. As a part of this project I have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire the parcels as shown herein. As a part of this work I have set right of way monuments at the property corners, property line intersection, points along the right of way and/or angle points on the right of way, Section Corners and other points as shown herein. All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as "Minimum Standards for Boundary Surveys in the State of Ohio" unless noted. The words I and my as used herein are to mean either myself or someone working under my direct supervision.

David E. Seasly, Professional Land Surveyor 7050

Date: _____

SURVEYORS SEAL

FEDERAL PROJECT NO. E161237
PID NO. 102817
CALCULATED MJF CHECKED DES

RIGHT OF WAY LEGEND SHEET

SEN-19/101-9.57/1.64

1 / 5

4.9
5.3

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MONUMENT LEGEND

- EXISTING R/W MONUMENT BOX
- PROPOSED R/W MONUMENT BOX
- EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- RAILROAD SPIKE FOUND
- RAILROAD SPIKE SET
- I.R.F. IRON PIN FOUND
- I.R.F. IRON PIN FOUND W/ ID CAP
- I.R.S. IRON PIN SET W/ ID CAP
- P.F. IRON PIPE FOUND
- P.S. IRON PIPE SET
- P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET
- M.N.F. MAG NAIL FOUND
- M.N.S. MAG NAIL SET

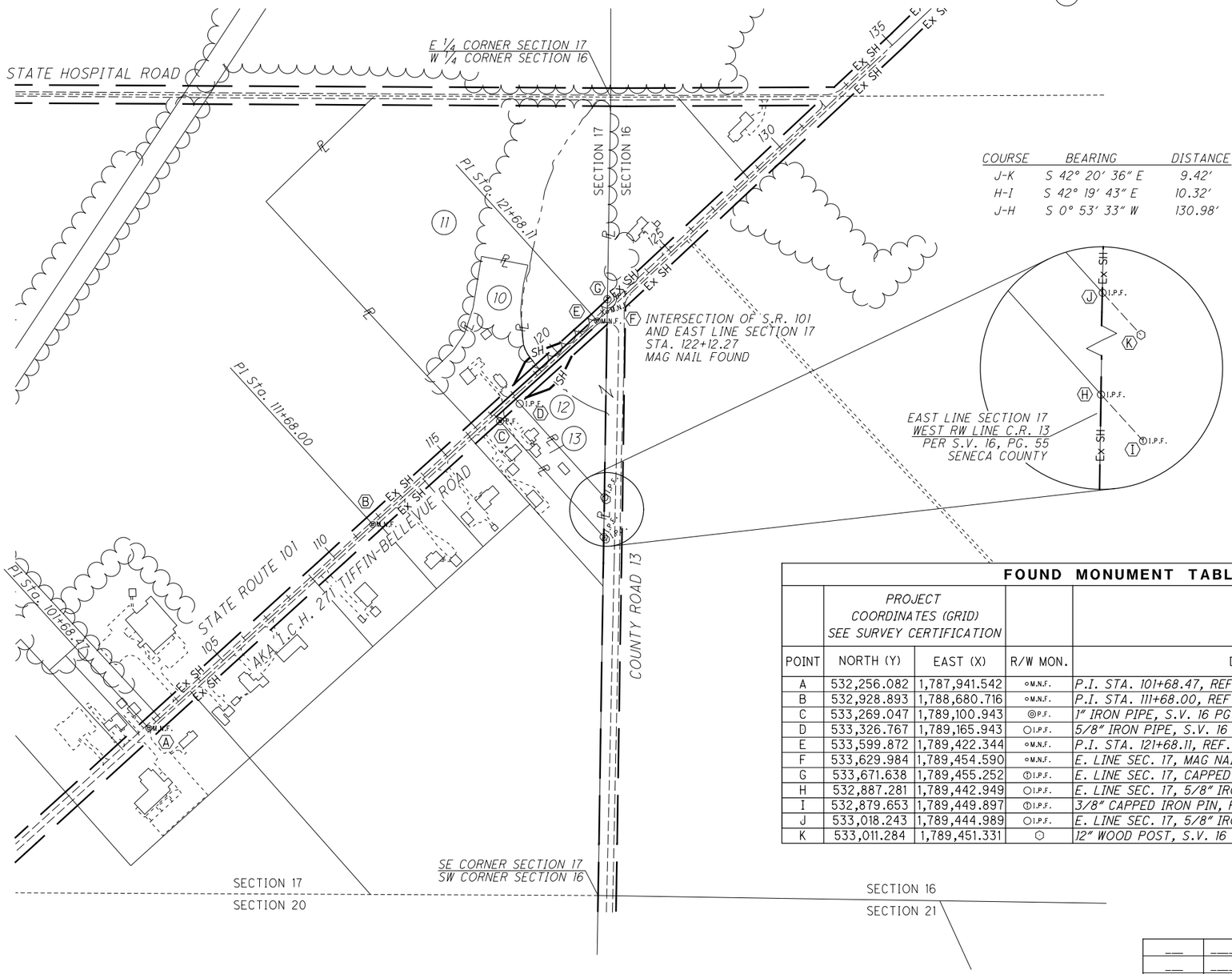
SENECA COUNTY
CLINTON TOWNSHIP
T-2-N, R-15-E
SE 1/4 SECTION 17

(10) DOROTHY M. SCHOLL

(11) ANDREW S. SCHOLL

(12) THE STATE OF OHIO

(13) STEVEN J. CHOLODEWITSCH



COURSE	BEARING	DISTANCE
J-K	S 42° 20′ 36″ E	9.42′
H-I	S 42° 19′ 43″ E	10.32′
J-H	S 0° 53′ 33″ W	130.98′

FOUND MONUMENT TABLE

POINT	PROJECT COORDINATES (GRID) SEE SURVEY CERTIFICATION NORTH (Y)	EAST (X)	R/W MON.	DESCRIPTION
A	532,256.082	1,787,941.542	○M.N.F.	P.I. STA. 101+68.47, REF. 5, SHEET 6 OF 27
B	532,928.893	1,788,680.716	○M.N.F.	P.I. STA. 111+68.00, REF. 6, SHEET 7 OF 27
C	533,269.047	1,789,100.943	◎P.F.	1″ IRON PIPE, S.V. 16 PG. 55, 29.69′ RT. OF ℄
D	533,326.767	1,789,165.943	○I.P.F.	5/8″ IRON PIPE, S.V. 16 PG. 55, 30.50′ RT. OF ℄
E	533,599.872	1,789,422.344	○M.N.F.	P.I. STA. 121+68.11, REF. 7, SHEET 8 OF 27
F	533,629.984	1,789,454.590	○M.N.F.	E. LINE SEC. 17, MAG NAIL FOUND, S.V. 16 PG. 55
G	533,671.638	1,789,455.252	ⓉI.P.F.	E. LINE SEC. 17, CAPPED 5/8″ IRON PIN, HANK & ASSOCIATES
H	532,887.281	1,789,442.949	○I.P.F.	E. LINE SEC. 17, 5/8″ IRON PIN, S.V. 16, PG. 55
I	532,879.653	1,789,449.897	ⓉI.P.F.	3/8″ CAPPED IRON PIN, P.S. 5647
J	533,018.243	1,789,444.989	○I.P.F.	E. LINE SEC. 17, 5/8″ IRON PIN, S.V. 16, PG. 55
K	533,011.284	1,789,451.331	⬡	12″ WOOD POST, S.V. 16 PG. 55

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

REV. BY	DATE	DESCRIPTION
MJF	4/11/17	REVISED S.R. 101 R/W WIDTH & PARCELS
DATE COMPLETED	31 JANUARY 2017	

HORIZONTAL SCALE IN FEET: 0, 100, 200, 400

PID NO. 102817

R/W DESIGNER MJF
R/W REVIEWER DES

PROPERTY MAP (1 OF 2)

SEN-19/101-9.57/1.64

2 / 5

5.0
5.3

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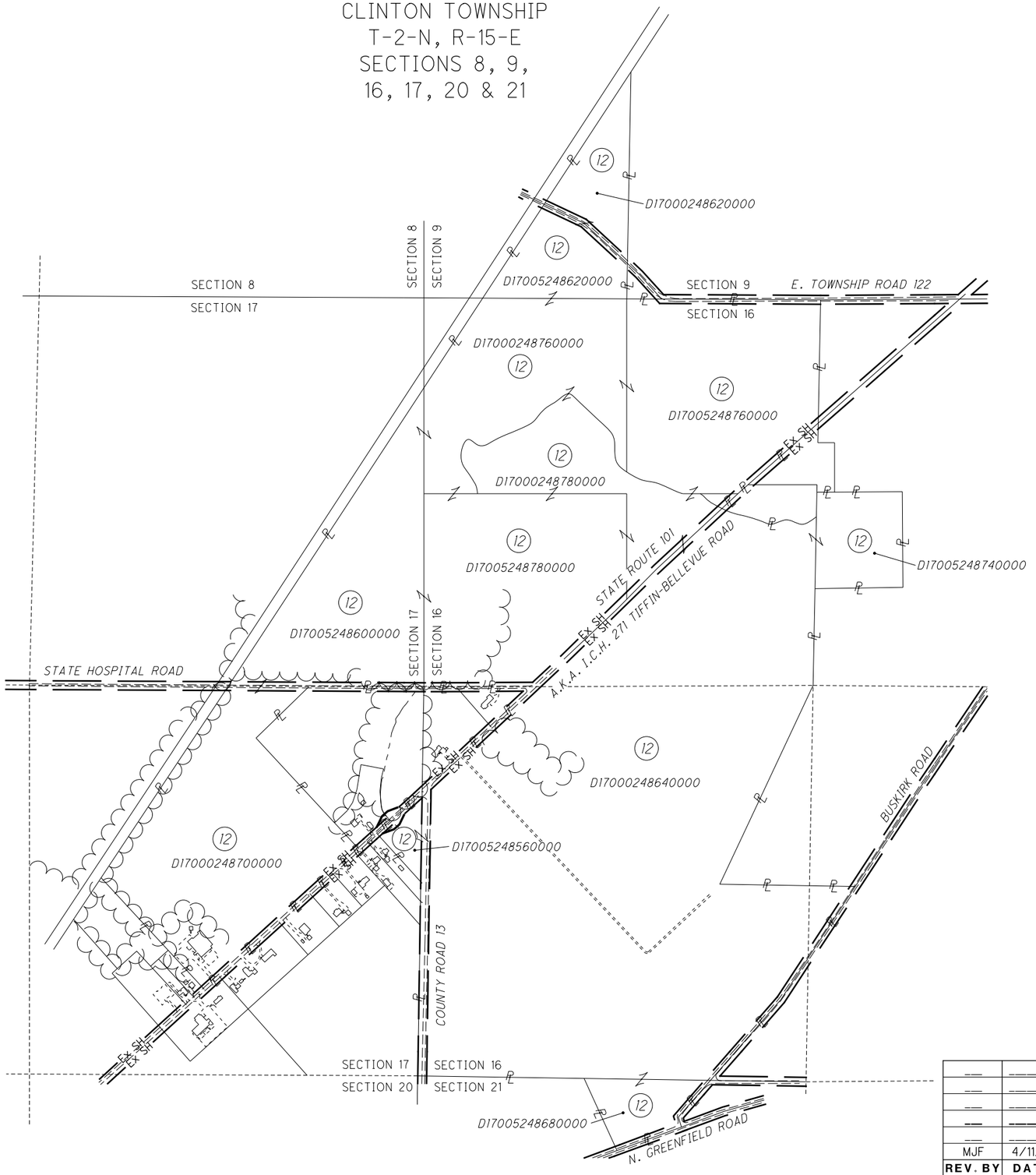
MONUMENT LEGEND

- EXISTING R/W MONUMENT BOX
- PROPOSED R/W MONUMENT BOX
- EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- RAILROAD SPIKE FOUND
- RAILROAD SPIKE SET
- I.R.F. IRON PIN FOUND
- I.R.F. IRON PIN FOUND W/ ID CAP
- I.R.S. IRON PIN SET W/ ID CAP
- P.F. IRON PIPE FOUND
- P.S. IRON PIPE SET
- P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET
- M.N.F. MAG NAIL FOUND
- M.N.S. MAG NAIL SET

SENECA COUNTY
CLINTON TOWNSHIP
T-2-N, R-15-E
SECTIONS 8, 9, 16, 17, 20 & 21



STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

REV. BY	DATE	DESCRIPTION
MJF	4/11/17	REVISED S.R. 101 R/W WIDTH AND PARCELS

DATE COMPLETED 31 JANUARY 2017

HORIZONTAL SCALE IN FEET: 0, 200, 400, 800

PID NO. 102817

R/W DESIGNER MJF
R/W REVIEWER DES

PROPERTY MAP (2 OF 2)

SEN-19/101-9.57/1.64

3 / 5

5.1 / 5.3

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TOTAL NUMBER OF :

3 OWNERSHIPS
3 PARCELS
0 TOTAL TAKES
0 OWNERSHIPS W/ STRUCTURES INVOLVED

NET TAKE = GROSS TAKE - PRO IN TAKE
NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE

(c) = CALCULATED AREA

* DENOTES RIGHT OF WAY ENCROACHMENT

ALL AREAS IN ACRES

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD	AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUCTURE	NET RESIDUE LEFT	NET RESIDUE RIGHT	TYPE FUND	REMARKS	AS ACQUIRED BOOK	AS ACQUIRED PAGE
10-SH	DOROTHY M. SCHOLL	2; 4	D.V. 393, PG. 47	D17000234120100	2:5	0.263	0.040	0.000	0.040		2.197		STATE			
11-SH	ANDREW S. SCHOLL	2; 4	O.R. 312; PG. 1118	D17000234120000	14:34	0.834	0.046	0.000	0.046		13.460					
12-SH	THE STATE OF OHIO	2, 3, 4	D.V. 253; PG. 481	D17005248560000	2.17	0.368	0.086	0.000	0.086			1.736	STATE			
		3		D17005248680000	5.55											
				D17000248640000	171.5											
				D17000248700000	38											
				D17005248600000	32											
				D17005248780000	23:72											
				D17000248780000	19:7											
				D17005248740000	8.75											
				D17005248760000	47:75											
				D17000248760000	29											
				D17005248620000	10											
				D17000248620000	11											
				TOTAL	399.14				0.046			397.384				
13	STEVEN J. CHOLODEWITSCH	2; 4	O.R. 337; PG. 1309	D17000222760000	0.997	0.080						0.917		NO TAKE		

NOTE: ALL TEMPORARY PARCELS TO BE OF 12 MONTH DURATION.

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

GRANTEE:

ALL RIGHT OF WAY ACQUIRED IN THE NAME OF THE OHIO DEPARTMENT OF TRANSPORTATION UNLESS OTHERWISE SHOWN.

TYPES OF TITLE LEGEND:
SH = STANDARD HIGHWAY EASEMENT

REV. BY	DATE	DESCRIPTION
MJF	4/11/17	REVISED PARCELS 10-SH, 11-SH, 12-SH

FIELD REVIEW BY MARTIN J. FARKAS DATE: 12OCT16
OWNERSHIP VERIFIED BY MARTIN J. FARKAS DATE: 31JAN17
DATE COMPLETED 31 JANUARY 2017

FEDERAL PROJECT NO. E161237
PID NO. 102817
STATE JOB NO. 427771
R/W DESIGNER MJF
R/W REVIEWER DES

SUMMARY OF ADDITIONAL RIGHT OF WAY

SEN-19 / 101-9.57 / 1.64

4 / 5

5.2
5.3

I:\ProjectData\SEN\102817_SEN-19_101-9.57\Design\RW\Sheets\102817_RS001.dgn Sheet 11/6/2017 8:31:15 AM jthomo10

MONUMENT LEGEND

- EXISTING R/W MONUMENT BOX
- PROPOSED R/W MONUMENT BOX
- EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- RAILROAD SPIKE FOUND
- RAILROAD SPIKE SET
- I.R.F. IRON PIN FOUND
- I.R.F. IRON PIN FOUND W/ ID CAP
- I.R.S. IRON PIN SET W/ ID CAP
- P.F. IRON PIPE FOUND
- P.S. IRON PIPE SET
- P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET
- M.N.F. MAG NAIL FOUND
- M.N.S. MAG NAIL SET

SENECA COUNTY
CLINTON TOWNSHIP
T-2-N, R-15-E
SE 1/4 SECTION 17

BASIS FOR BEARINGS:

ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY. BEARINGS ON THIS PLAT ARE BASED UPON OHIO STATE NORTH ZONE NAD 83 (2011), GEOID MODEL 2012A. THE CENTERLINE OF STATE ROUTE 101 BETWEEN STATION 111+68.00 AND STATION 121+68.11, SOUTHEAST QUARTER OF SECTION 17, TOWN 2 NORTH, RANGE 15 EAST, CLINTON TOWNSHIP, SENECA COUNTY, WAS ESTABLISHED THROUGH GPS OBSERVATIONS OF FOUND MONUMENTS AND DETERMINED TO BE BEARING NORTH 47° 51′ 47″ EAST.

Ⓐ N 42° 07′ 10″ W 17.68′
Ⓑ N 39° 03′ 06″ W 12.34′
Ⓒ S 39° 03′ 06″ E 30.04′

RESIDENTIAL
(10)
DOROTHY M. SCHOLL
D.V. 393, PG. 47
D17000234120100
370 N SR 101
TIFFIN, OH 44883

AGRICULTURAL
(11)
ANDREW S. SCHOLL
O.R. 312, PG. 1118
D17000234120000
240 OHIO AVE
TIFFIN, OH 44883

PI Sta. 111+68.00

9' ASPH. DRIVE

2 S.F

STA. 119+00.00
60.00′ LT.

SAVE 21.82′

28.18′

STA. 119+21.82
60.00′ LT.

STA. 119+50.00
60.00′ LT.

N 26° 03′ 42″ E
80.78′

N 47° 51′ 47″ E

N 69° 39′ 52″ E
80.78′

10-SH

11-SH

STA. 118+25.00
30.00′ LT.

STA. 119+20.21
30.00′ LT.

STA. 120+25.00
30.00′ LT.

Ex SH

S 47° 51′ 47″ W 95.21′

104.79′

N 42° 08′ 13″ W 30.00′

PI Sta. 121+68.11

BEGIN ACQUISITION
STA. 118+25.00

STA. 119+19.54
17.68′ LT.

℄ STATE ROUTE 101 S 47° 51′ 47″ W

A.K.A. ICH 271 TIFFIN-BELLEVUE ROAD

+19.54

105.46′

248.57′ (10-SH LEAD-IN)

+25.00

S 47° 51′ 47″ W 143.11′

(11, 12-SH LEAD-IN)

44.15′

S 46° 55′ 54″ W

P.O.C. STA. 122+12.26
EAST LINE SECTION 17
MAG NAIL FOUND

END ACQUISITION
STA. 120+25.00

S 42° 08′ 13″ E 30.00′

117 118 119 120 121 122

29.69′ 30.50′

SEE PROPERTY MAP (SHT 2 OF 5) FOR INFO ON PINS FOUND

STA. 118+25.00
30.00′ RT

N 47° 51′ 47″ E

200.00′

STA. 120+25.00
30.00′ RT.

12-SH

80.78′
S 69° 39′ 52″ W

80.78′
S 26° 03′ 42″ W

50.00′

S 47° 51′ 47″ W

STA. 119+00.00
60.00′ RT.

STA. 119+50.00
60.00′ RT.

S 41° 51′ 18″ E

S 42° 07′ 37″ E

RESIDENTIAL
(13)
STEVEN J. CHOLODEWITSCH
O.R. 337, PG. 1309
D17000222760000
369 N SR 101
TIFFIN, OH 44883

1 S.F

1 S.F

AGRICULTURAL
(12)
STATE OF OHIO
D.V. 253, PG. 481
D17005248560000
600 N RIVER RD
TIFFIN, OH 44883

EAST LINE SECTION 17
PER HANK AND ASSOCIATES
SURVEY 20121.122
SURVEY BOOK 16, PAGE 55

SECTION 17
SECTION 16
C.R. 13

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

REV. BY	DATE	DESCRIPTION
MJF	5/23/17	UPDATED CULVERT DESIGN
MJF	4/11/17	REVISED PARCELS 10-SH, 11-SH, 12-SH
MJF	4/11/17	REVISED S.R. 101 R/W WIDTH

DATE COMPLETED 31 JANUARY 2017

HORIZONTAL SCALE IN FEET
0 10 20 40

PID NO. 102817

R/W DESIGNER MJF
R/W REVIEWER DES

RIGHT OF WAY PLAN
STA. 117+00.00 TO STA. 122+00.00

SEN-19 / 101-
9.57 / 1.64

5 / 5

5.3
5.3