

ITEM 614, MAINTAINING TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. ON I.R.-480, A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, AND ITEM 615 ROADS FOR MAINTAINING TRAFFIC. IT IS PERMISSIBLE TO KEEP ONE LANE OF TRAFFIC OPEN ONLY DURING NON-PEAK HOURS. THE CONTRACTOR IS REQUIRED TO MAINTAIN TWO LANES OF TRAFFIC IN EACH DIRECTION DURING THE FOLLOWING TIMES:

MONDAY - FRIDAY 6:00 AM TO 9:00 AM
 MONDAY - FRIDAY 3:00 PM TO 7:00 PM

FOR LANE CLOSURES DURING OFF-PEAK PERIODS, THE CONTRACTOR SHALL INSTALL, REMOVE, AND RESET ALL TRAFFIC CONTROL DEVICES NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE, AND AS PER ODOT SCD'S MT-95.30, MT-95.40 AND MT-101.90. CONFLICTING SIGNS SHALL BE COVERED OR REMOVED. PAYMENT FOR THESE LANE CLOSURES SHALL BE INCIDENTAL TO THE LUMP SUM ITEM 614-MAINTAINING TRAFFIC.

3. THE CONTRACTOR SHALL ADVISE THE ODOT DISTRICT OFFICE (330-786-2208), SUMMIT COUNTY (330-643-2850) THE CITY OF HUDSON (330-342-1770) AND TWINSBURG TOWNSHIP (330-425-4497) EIGHTEEN (18) DAYS IN ADVANCE OF WHEN THE STOW ROAD DETOUR ROUTE SHOULD BE IN EFFECT. ALL WORK ZONE DEVICES REQUIRED SHALL BE FURNISHED, ERECTED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. PAYMENT FOR ALL WORK ASSOCIATED WITH THE DETOUR SHALL BE INCLUDED UNDER THE LUMP SUM BID FOR ITEM 614, DETOUR SIGNING.

4. NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS:

NEW YEAR'S (OBSERVED) MEMORIAL DAY
 FOURTH OF JULY (OBSERVED) LABOR DAY
 GENERAL/REGULAR ELECTION DAY (NOV.) THANKSGIVING
 CHRISTMAS (OBSERVED)

ITEM 614, MAINTAINING TRAFFIC (CONTINUED)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY (GEN./REG. ELECTION)	5:00 AM TUESDAY THROUGH 12: AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LANE VALUE CONTRACT TABLE

DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
2 LANES OF SUM IR 480 FROM MM37 TO MM41	06:00 - 09:00 M-F 15:00 - 19:00 M-F	EACH HOUR	\$10,000

5. LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

6. NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE.

NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A

ITEM 614, MAINTAINING TRAFFIC (CONTINUED)

MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

6. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

SEQUENCE OF CONSTRUCTION

NOTE: THE FINAL PAVEMENT SURFACE COURSE SHALL BE PLACED IN THE FINAL MOT PHASE.

PRE-PHASE

1. REMOVE EXISTING OUTSIDE SHOULDER PAVEMENT AND CONSTRUCT PAVEMENT FOR MAINTAINING TRAFFIC ALONG OUTSIDE SHOULDER OF I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND FOR USE IN PHASES 1 AND 2. MAINTAIN ONE EXISTING LANE IN EACH DIRECTION USING SHORT TERM LANE CLOSURES IN ACCORDANCE WITH SCD MT-95.30 AND THE TIMES AS LISTED IN THE ITEM 614, MAINTAINING TRAFFIC NOTE, SHEET 15.

2. REMOVE EXISTING INSIDE SHOULDER PAVEMENT AND CONSTRUCT PAVEMENT FOR MAINTAINING TRAFFIC ALONG INSIDE SHOULDER OF I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND FOR USE IN PHASES 2 AND 3. MAINTAIN ONE EXISTING LANE IN EACH DIRECTION USING SHORT TERM LANE CLOSURES IN ACCORDANCE WITH SCD MT-95.30 AND THE TIMES AS LISTED IN THE ITEM 614, MAINTAINING TRAFFIC NOTE, SHEET 15.

PHASE 1

1. SHIFT BOTH LANES OF I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND TRAFFIC TO THE OUTSIDE OF EACH BOUND. THE OUTSIDE LANES OF EACH BOUND WILL BE REQUIRED TO USE THE OUTSIDE SHOULDER.

2. DETOUR STOW ROAD TRAFFIC.

3. CONSTRUCT INSIDE PORTIONS OF ROADWAY AND BRIDGE IMPROVEMENTS ALONG I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND.

PHASE 2

CONTRACTOR SHALL NOT IMPLEMENT PHASE 2 PRIOR TO APRIL 1ST.

1. KEEP THE OUTSIDE LANE OF I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND IN ITS CURRENT PATTERN.

2. SHIFT THE INSIDE LANE OF I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND TRAFFIC TO THE INSIDE OF EACH BOUND. THE INSIDE LANES OF EACH BOUND WILL BE REQUIRED TO USE THE INSIDE SHOULDER.

3. CONSTRUCT MIDDLE PORTIONS OF ROADWAY AND BRIDGE IMPROVEMENTS ALONG I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND.

SEQUENCE OF CONSTRUCTION (CONTINUED)

PHASE 3

1. KEEP THE INSIDE LANE OF I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND IN ITS CURRENT PATTERN.

2. SHIFT THE OUTSIDE LANE OF I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND TRAFFIC TO BE ADJACENT TO THE INSIDE LANE OF THEIR RESPECTIVE BOUND.

3. CONSTRUCT OUTSIDE PORTIONS OF ROADWAY AND BRIDGE IMPROVEMENTS ALONG I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND.

FINAL

1. MILL AND RESURFACE STOW ROAD PAVEMENT AND PLACE STOW ROAD FINAL PAVEMENT MARKINGS. OPEN STOW ROAD TO TRAFFIC ONCE ALL IMPROVEMENTS ARE COMPLETE.

3. PLACE I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND TRAFFIC BACK ITS ORIGINAL PATTERN.

4. MILL AND RESURFACE I.R.-480 EASTBOUND LANES AND SHOULDERS FROM STATION 364+90 TO STATION 405+70. MILL AND RESURFACE I.R.-480 WESTBOUND LANES AND SHOULDERS FROM STATION 382+80 TO STATION 423+60. MILL AND RESURFACE THE EXISTING MEDIAN U-TURN AT STA. 388+00. PLACE I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND FINAL SURFACE COURSE. MAINTAIN ONE LANE IN EACH DIRECTION, IN ITS ORIGINAL PATTERN, USING SHORT TERM LANE CLOSURES IN ACCORDANCE WITH SCD MT-95.30 AND THE TIMES AS LISTED IN THE ITEM 614, MAINTAINING TRAFFIC NOTE, SHEET 15.

5. COMPLETE INSTALLATION OF ALL I.R.-480 EASTBOUND AND I.R.-480 WESTBOUND PERMANENT PAVEMENT MARKINGS AND SIGNS.

TRENCH FOR WIDENING (SPEED LIMIT > 45 MPH)

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. AS SHOWN ON MT-101.90. PLACEMENT OF PROPOSED BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND THE EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE BASE WIDENING ON THIS PROJECT WILL BE COMPLETED TO A DEPTH OF 3 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF THE WORK DAY. NO TRENCH WILL BE LEFT OPEN OVERNIGHT. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING WILL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 7 M. GAL.

ITS MESSAGE BOARDS

THE EXISTING ITS MESSAGE BOARDS IN THE VICINITY OF THE PROJECT WILL BE UTILIZED TO PROVIDE SUPPLEMENTAL INFORMATION TO THE TRAVELING PUBLIC. THE CONTRACTOR WILL NOTIFY THE PROJECT ENGINEER ONE (1) WEEK IN ADVANCE OF ANY PHASE CHANGE. THE PROJECT ENGINEER WILL COORDINATE WITH THE DISTRICT 4 PUBLIC INFORMATION OFFICER AT 330-786-2208 FOR ITS MESSAGE BOARD ADJUSTMENTS.

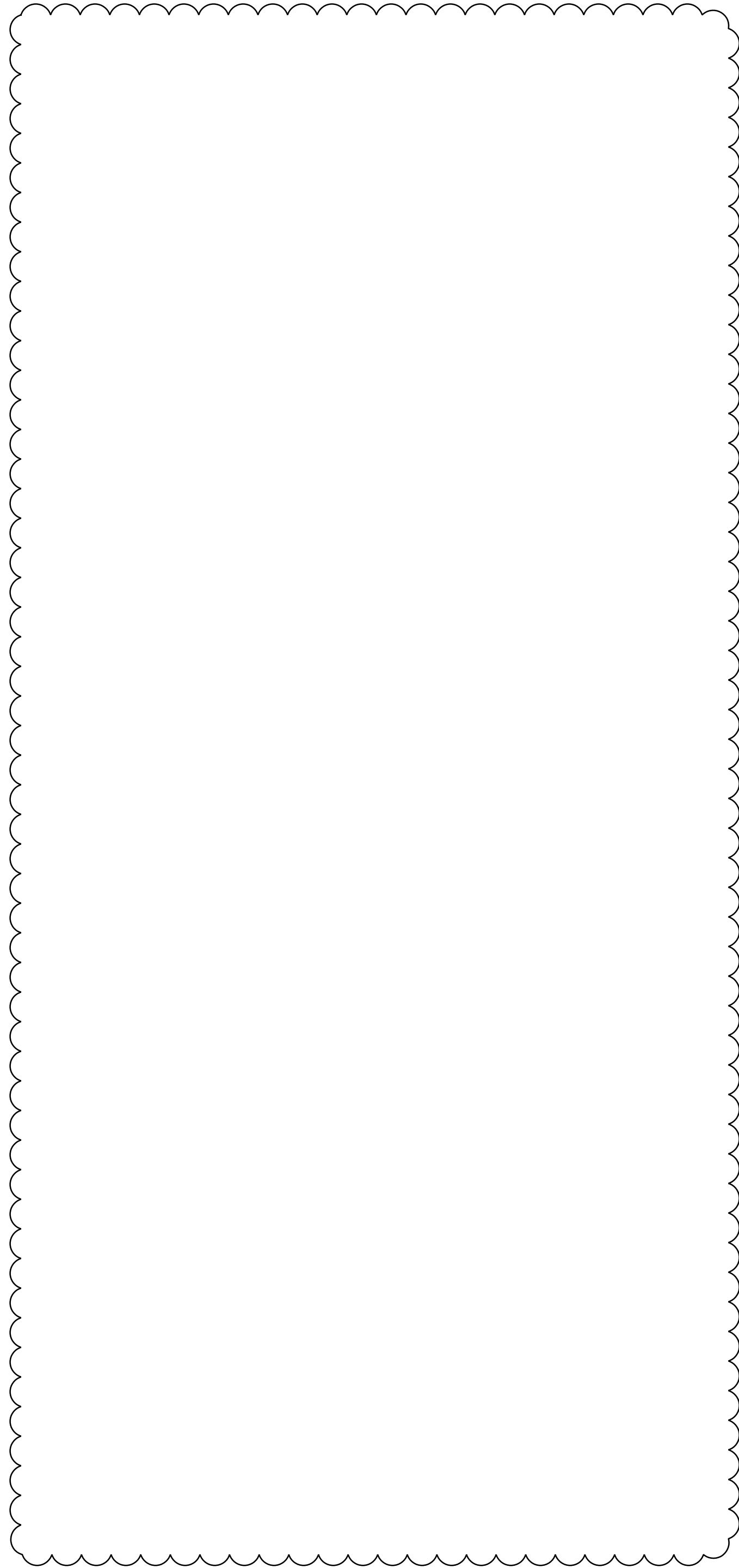
ITEM 614 – MAINTAINING TRAFFIC MISC.: SAFETY REPAIRS

FOR IMPACT ATTENUATORS OR GUARDRAIL DAMAGED BY THE MOTORING PUBLIC, THE CONTRACTOR SHALL FOLLOW THE PROCESS OUTLINED IN 107.15. IF NO ACCIDENT REPORT IS AVAILABLE, THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FROM THE VARIOUS POSSIBLE RESPONDING AGENCIES THAT NO ACCIDENT REPORT IS AVAILABLE. FOR INCIDENTS WITH AN ACCIDENT REPORT AVAILABLE, BUT THE OWNER OR INSURANCE COMPANY IS NON-RESPONSIVE, COPIES OF THE COMMUNICATION SHALL BE SUBMITTED TO THE PROJECT PER 107.15B. FOR BOTH CONDITIONS LISTED ABOVE, THE ENGINEER SHALL DETERMINE THE SAFETY ITEMS THAT MAY BE REPAIRED AND THE SAFETY ITEMS THAT SHALL BE REPLACED. THE WORK WILL BE AS DIRECTED BY THE ENGINEER AND WILL INCLUDE ALL MAINTENANCE OF TRAFFIC COSTS ASSOCIATED WITH THE ACTIVITY. THE COST FOR EACH ITEM SHALL BE \$1.00. THE FIXED AMOUNT SHOWN IN THE PROPOSAL IS INCLUDED (AS ANY OTHER BID ITEMS) IN THE TOTAL BID AMOUNT. THIS FIXED AMOUNT IS THE DEPARTMENT'S ESTIMATE OF THE TOTAL COST FOR THE REPAIR OR REPLACEMENT OF SAFETY ITEMS WITHIN THE WORK LIMITS AS DIRECTED BY THE ENGINEER. CMS TABLE 104.02-2 DOES NOT APPLY TO REDUCTIONS IN THIS CONTRACT ITEM. FORCE ACCOUNT RECORDS SHALL BE KEPT TO TRACK AND ULTIMATELY DETERMINE THE AMOUNT OF THE PAY ITEM USED. THIS ITEM SHALL INCLUDE PAYMENT FOR ALL WORK, INCIDENTALS, AND ALL ASSOCIATED COSTS FOR THE REPAIR OR REPLACEMENT OF DAMAGED SAFETY ITEMS AS DIRECTED BY THE ENGINEER.

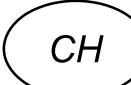


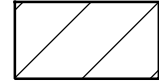











ITEM 614 – MAINTAINING TRAFFIC MISC.: SAFETY REPAIRS,
100,000 EACH

ALTERNATE MAINTENANCE OF TRAFFIC PLANS

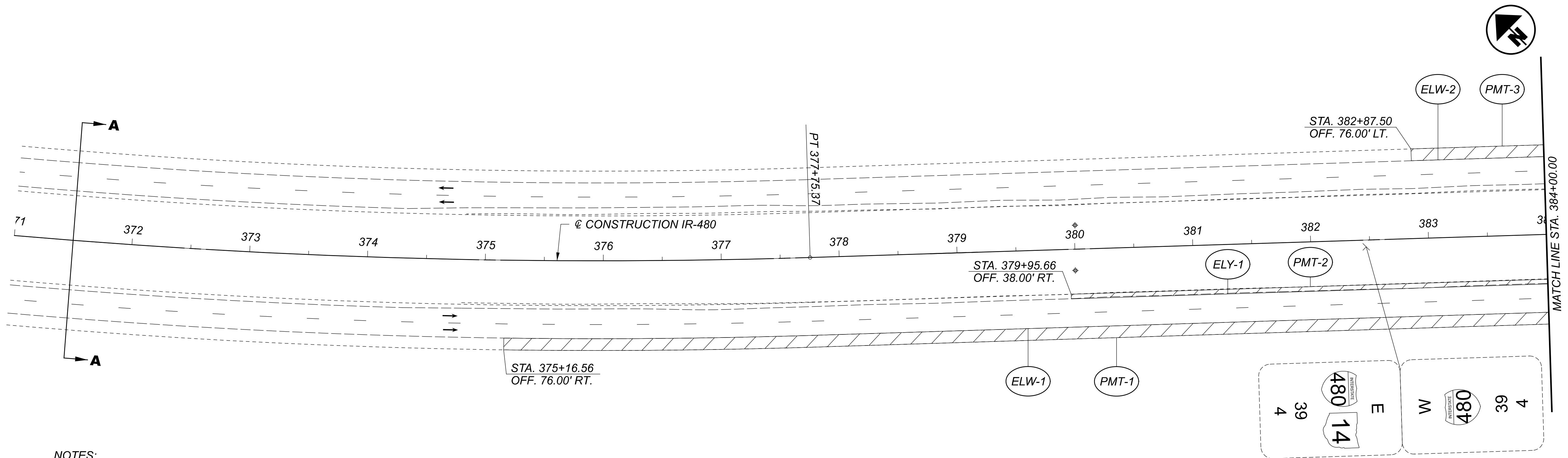
IF THE CONTRACTOR SO ELECTS, HE MAY SUBMIT ALTERNATE METHODS FOR THE MAINTENANCE OF TRAFFIC, PROVIDED THE INTENT OF THE ABOVE PROVISIONS IS FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THERE FROM. NO ALTERNATE PLANS SHALL BE PLACED IN EFFECT UNTIL APPROVAL HAS BEEN GRANTED IN WRITING BY THE ODOT DISTRICT CONSTRUCTION ENGINEER.



MAINTENANCE OF TRAFFIC LEGEND

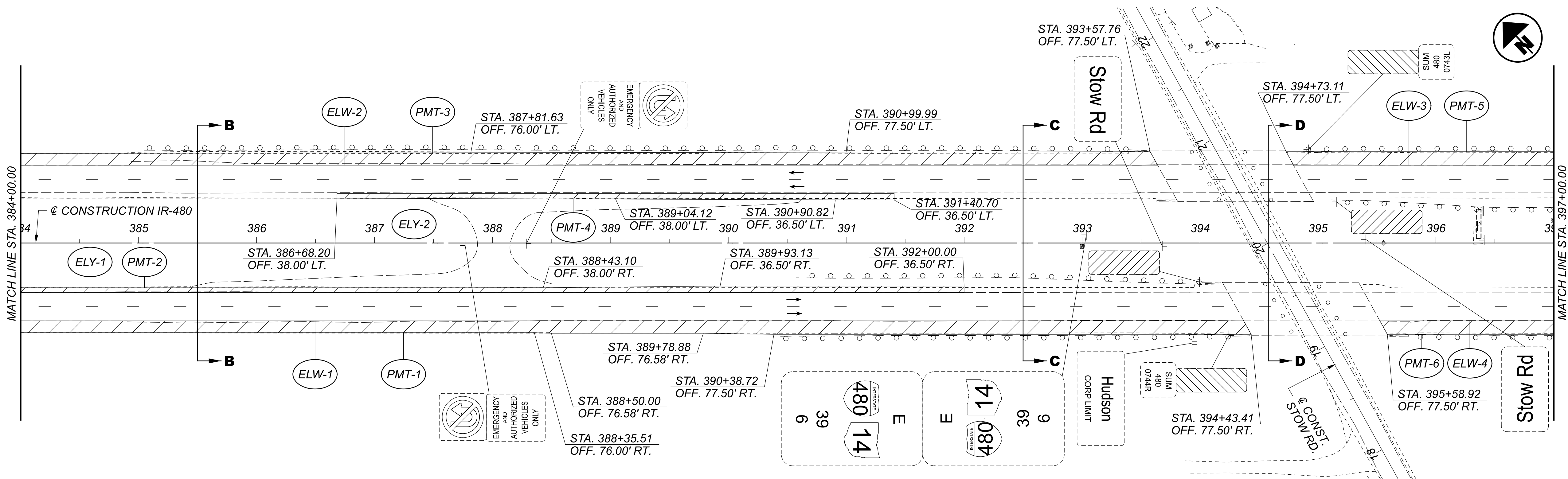
 CH	WORK ZONE CHANNELIZING LINE		PAVEMENT BUILT IN PREVIOUS PHASE
 CHV	WORK ZONE CHEVRON MARKING		PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN
 ELW	WORK ZONE EDGE LINE, WHITE		WORK ZONE
 ELY	WORK ZONE EDGE LINE, YELLOW		DIRECTION OF TRAVEL
 IA	WORK ZONE IMPACT ATTENUATOR		WORK ZONE CHEVRON MARKING
 PB	32" PORTABLE BARRIER		32" PORTABLE BARRIER
 PMT	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN		WORK ZONE IMPACT ATTENUATOR
			WORK ZONE DRUMS

ISL = INSIDE LANE
 OSL = OUTSIDE LANE
 PB = PORTABLE BARRIER



NOTES:

1. FOR MAINTENANCE OF TRAFFIC LEGEND, SEE SHEET 20.
2. UTILITIES NOT SHOWN FOR PLAN CLARITY.
3. MAINTAIN EXISTING TRAFFIC DURING CONSTRUCTION OF PAVEMENT FOR MAINTAINING TRAFFIC. CONSTRUCTION SHALL ONLY BE PERFORMED ON ONE SIDE OF EACH BOUND AT A TIME. TWO LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED DURING THE TIMES LISTED IN THE ITEM 614. MAINTAINING TRAFFIC NOTE, SHEET 15. REFER TO SCD MT-95.30 FOR REQUIRED MOT SETUP AND SIGNAGE.
4. MAINTAIN EXISTING PAVEMENT MARKINGS UNLESS OTHERWISE NOTED. REMOVE CONFLICTING PAVEMENT MARKINGS.
5. IN AREAS OF SHOULDER WIDENING FROM STA. 288+50 TO STA. 400+00, SEE ROADWAY CROSS SECTIONS FOR ADDITIONAL INFORMATION.
6. REINSTALL PAVEMENT MARKINGS DAMAGED BY TEMPORARY PAVEMENT INSTALLATION.



MAINTENANCE OF TRAFFIC PLAN - PRE-PHASE
 I.R.-480 - STA. 371+00 TO STA. 397+00

DESIGN AGENCY



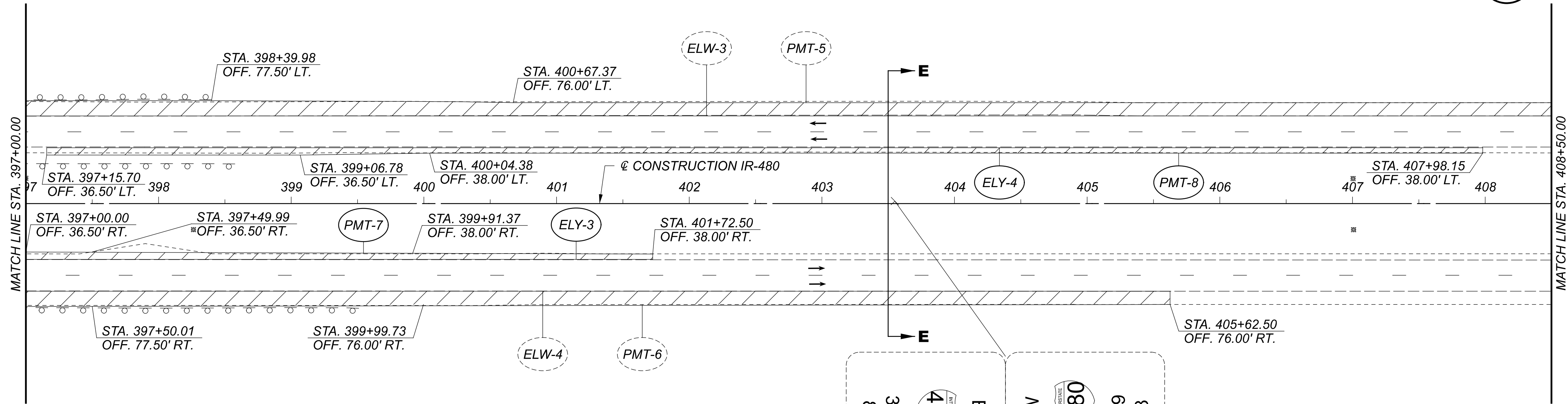
COMPASS
 INFRASTRUCTURE GROUP

DESIGNER
 MBM

REVIEWER
 SSR 06/14/24

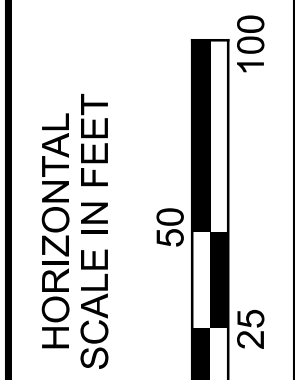
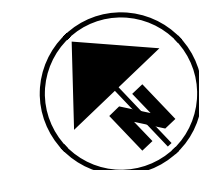
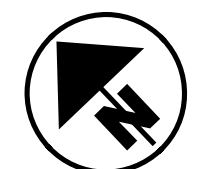
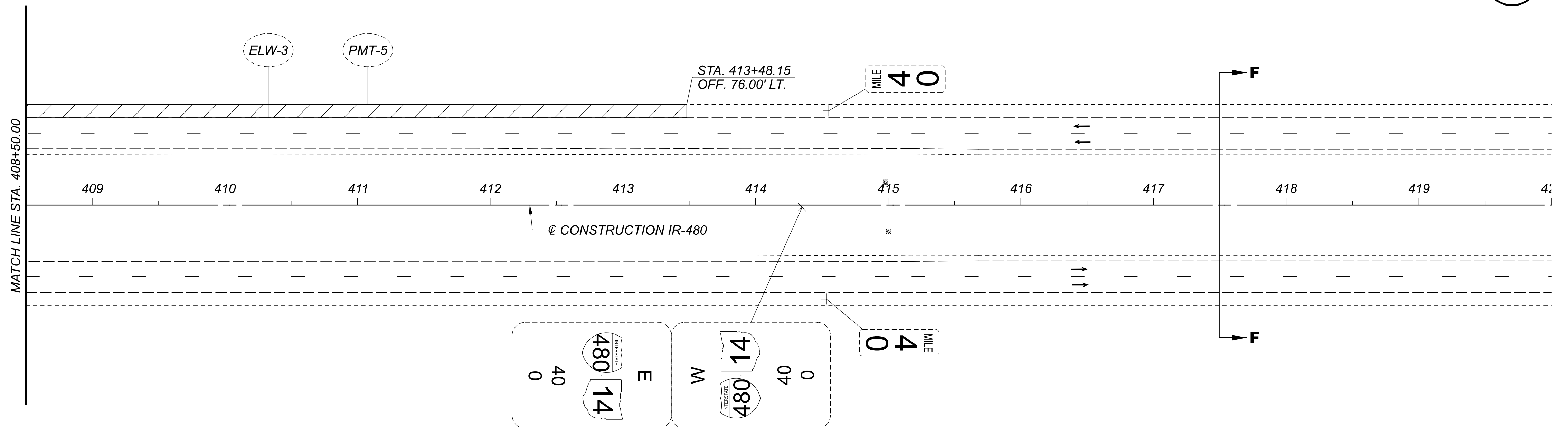
PROJECT ID
 111186

SHEET TOTAL
 30 131



NOTES:

1. FOR MAINTENANCE OF TRAFFIC LEGEND, SEE SHEET 20.
2. UTILITIES NOT SHOWN FOR PLAN CLARITY.
3. MAINTAIN EXISTING TRAFFIC DURING CONSTRUCTION OF PAVEMENT FOR MAINTAINING TRAFFIC. CONSTRUCTION SHALL ONLY BE PERFORMED ON ONE SIDE OF EACH BOUND AT A TIME. TWO LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED DURING THE TIMES LISTED IN THE ITEM 614. MAINTAINING TRAFFIC NOTE, SHEET 15. REFER TO SCD MT 95.30 FOR REQUIRED MOT SETUP AND SIGNAGE.
4. MAINTAIN EXISTING PAVEMENT MARKINGS UNLESS OTHERWISE NOTED. REMOVE CONFLICTING PAVEMENT MARKINGS.
5. IN AREAS OF SHOULDER WIDENING FROM STA. 288+50 TO STA. 400+00, SEE ROADWAY CROSS SECTIONS FOR ADDITIONAL INFORMATION.
6. REINSTALL PAVEMENT MARKINGS DAMAGED BY TEMPORARY PAVEMENT INSTALLATION.



MAINTENANCE OF TRAFFIC PLAN - PRE-PHASE
 I.R.-480 - STA. 397+00 TO STA. 420+00

DESIGN AGENCY



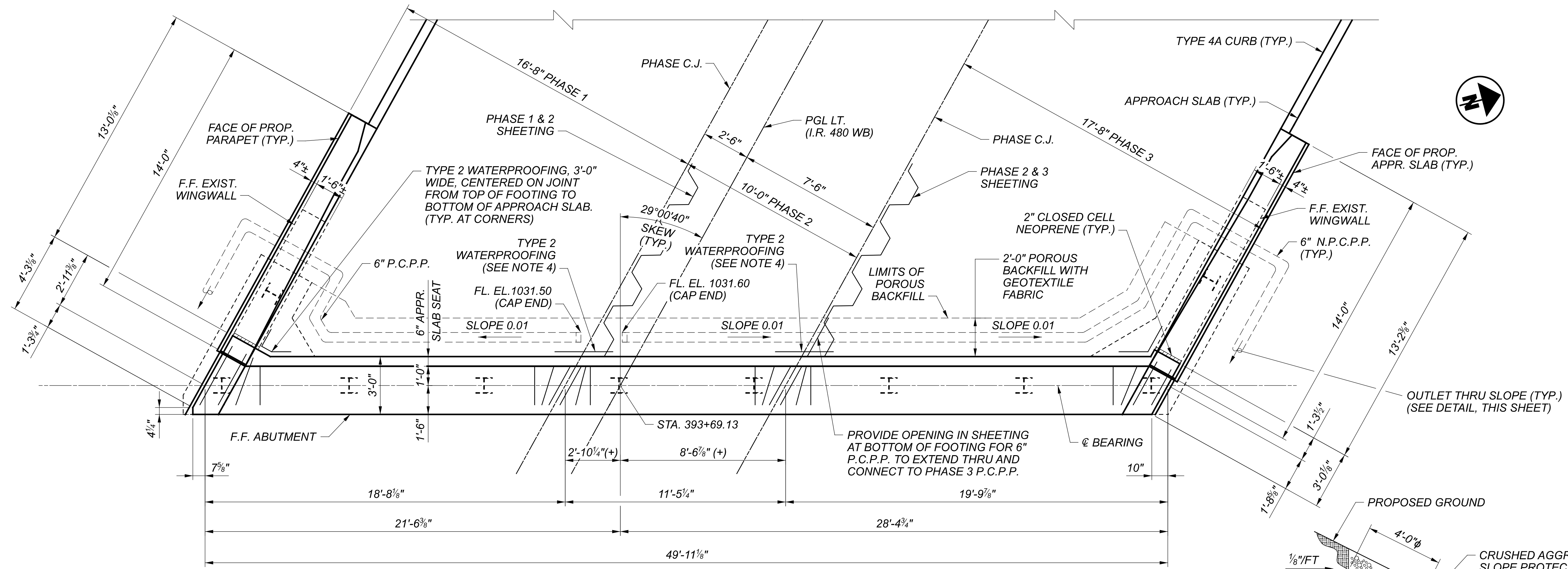
COMPASS
 INFRASTRUCTURE GROUP

DESIGNER
 MBM

REVIEWER
 SSR 06/14/24

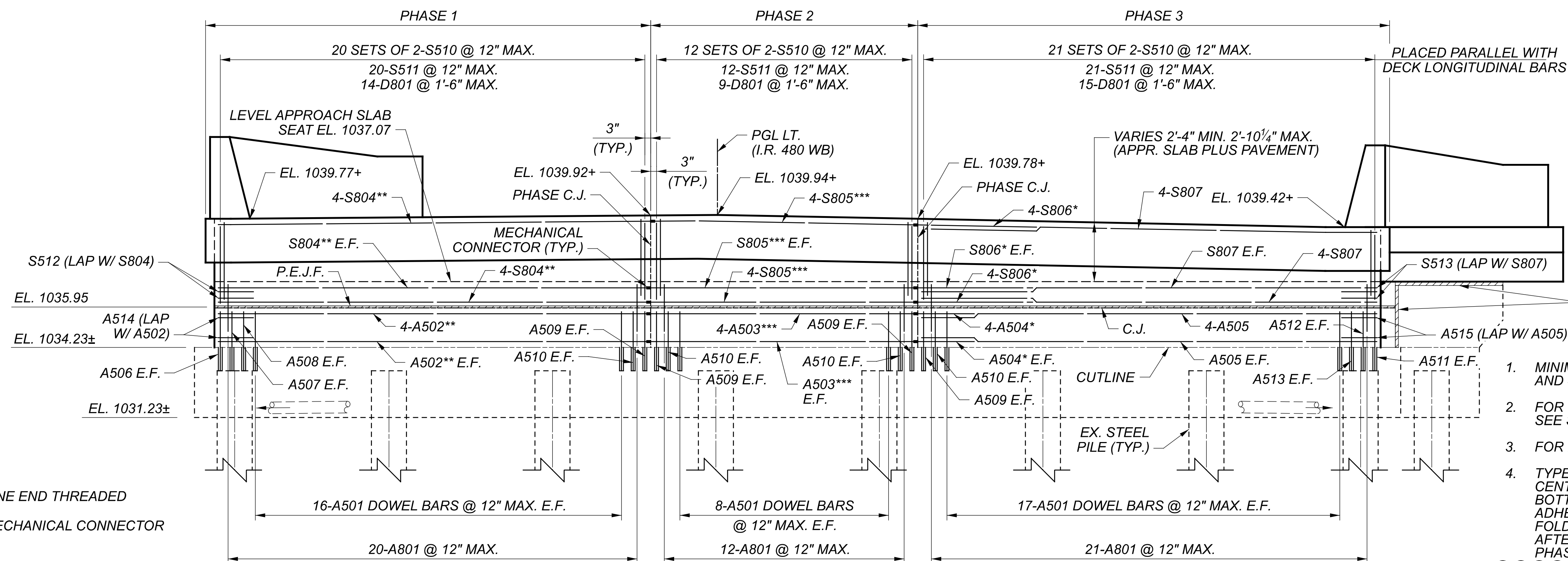
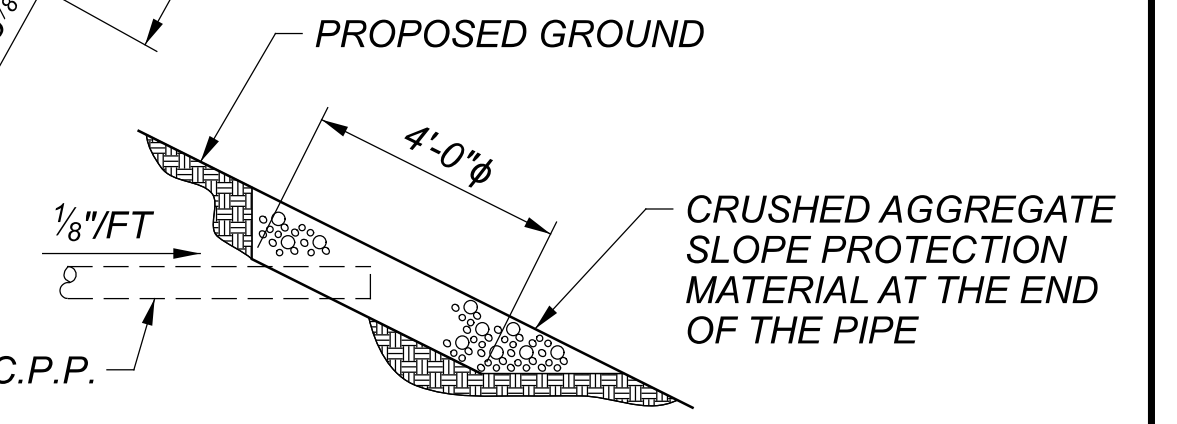
PROJECT ID
 111186

SHEET	TOTAL
31	131



REAR ABUTMENT PLAN

END TREATMENT OF 6" φ NON-PERFORATED CORRUGATED PLASTIC PIPE



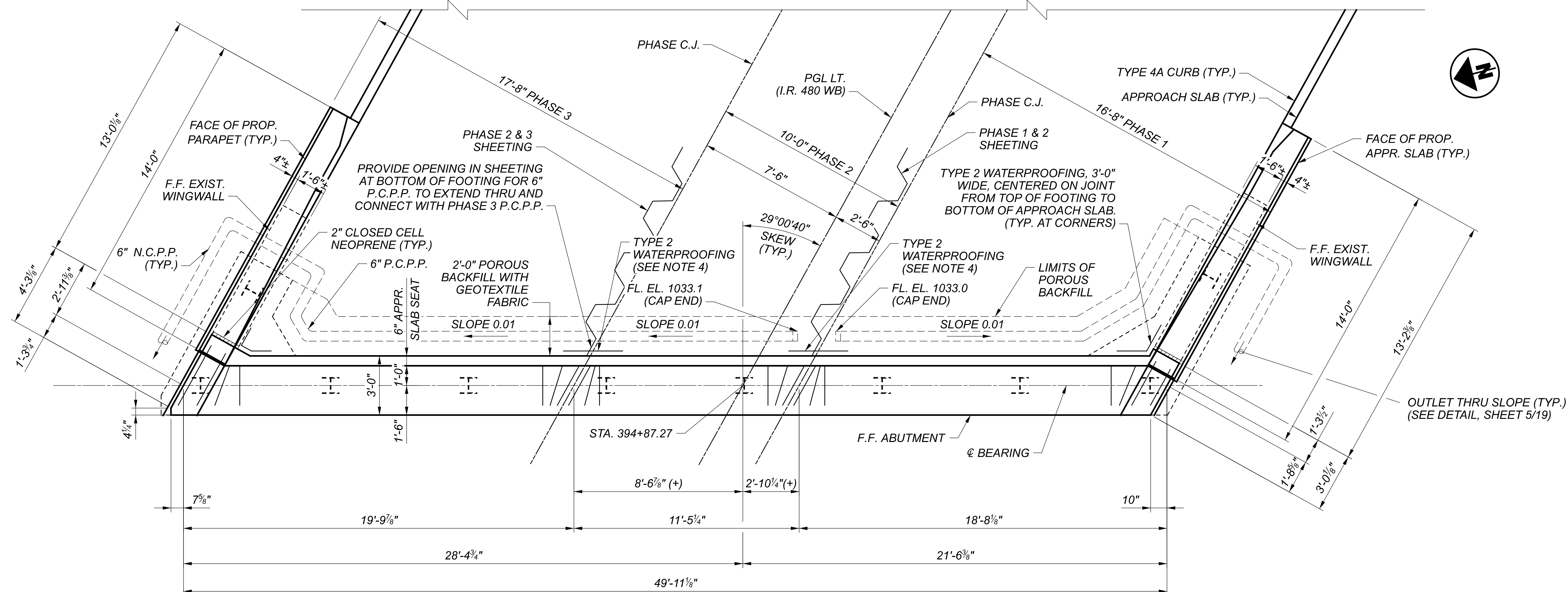
REAR ABUTMENT ELEVATION

LEGEND:

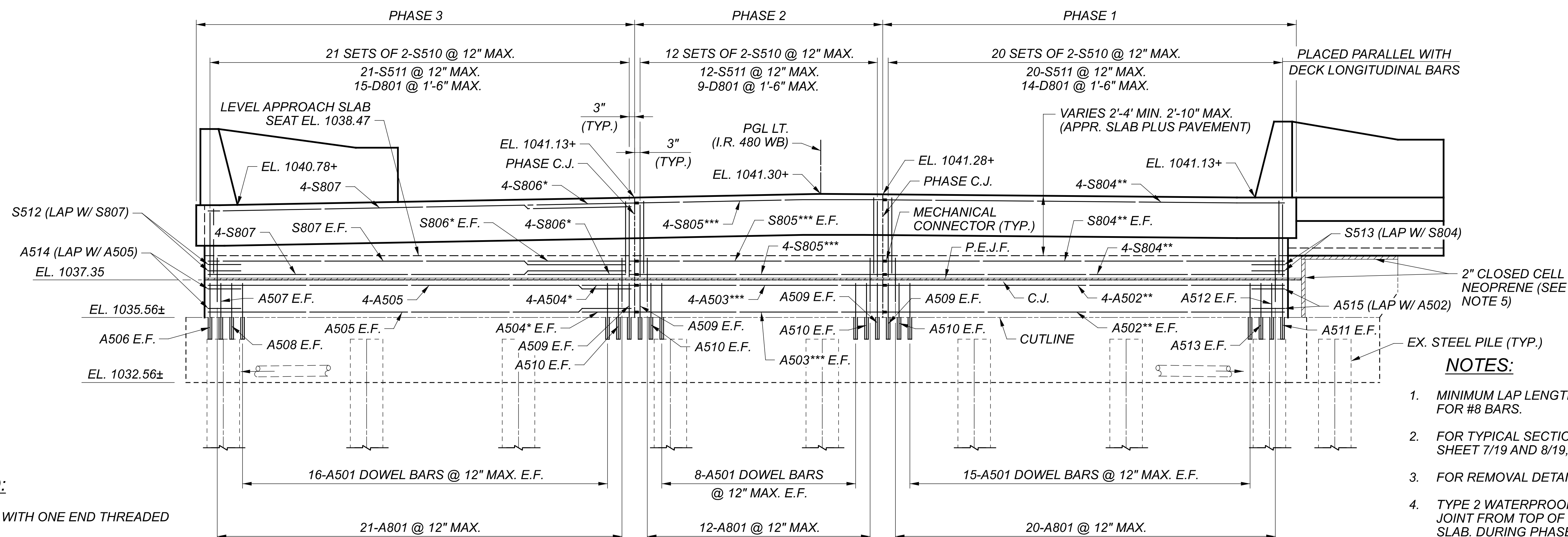
- * - INDICATES BAR WITH ONE END THREADED
- ** - INDICATES BAR WITH MECHANICAL CONNECTOR ON ONE END
- *** - INDICATES BAR WITH MECHANICAL CONNECTOR ON ONE END AND ONE END THREADED
- + - ELEVATIONS GIVEN AT CENTERLINE BEARING

- NOTES:**
1. MINIMUM LAP LENGTH SHALL BE 2'-5" FOR #5 BARS AND 4'-11" FOR #8 BARS.
 2. FOR TYPICAL SECTIONS AND WINGWALL ELEVATIONS, SEE SHEET 7/19 AND 8/19, RESPECTIVELY.
 3. FOR REMOVAL DETAILS, SEE SHEETS 2/19 AND 4/19.
 4. TYPE 2 WATERPROOFING SHALL BE 3'-0" WIDE CENTERED ON JOINT FROM TOP OF FOOTING TO BOTTOM OF APPROACH SLAB, DURING PHASE 1. ADHERE 1'-6" VERTICAL WIDTH OF WATERPROOFING. FOLD BACK AND PROTECT. ADHERE REMAINDER AFTER PHASE 2 IS CONSTRUCTED. SIMILAR FOR PHASE 2 AND 3.
 5. INCLUDE PAYMENT FOR 2" CLOSED CELL NEOPRENE AND 3" WIDE NEOPRENE SHEETING WITH ITEM 516 - INTEGRAL ABUTMENT EXPANSION JOINT SEAL, AS PER PLAN.

SFN	7710445
DESIGN AGENCY	ms consultants, inc.
DESIGNER	DFK
CHECKER	SJR
REVIEWER	WER
PROJECT ID	11186
SUBSET	5
TOTAL	19
SHEET	98
TOTAL	131



FORWARD ABUTMENT PLAN



FORWARD ABUTMENT ELEVATION

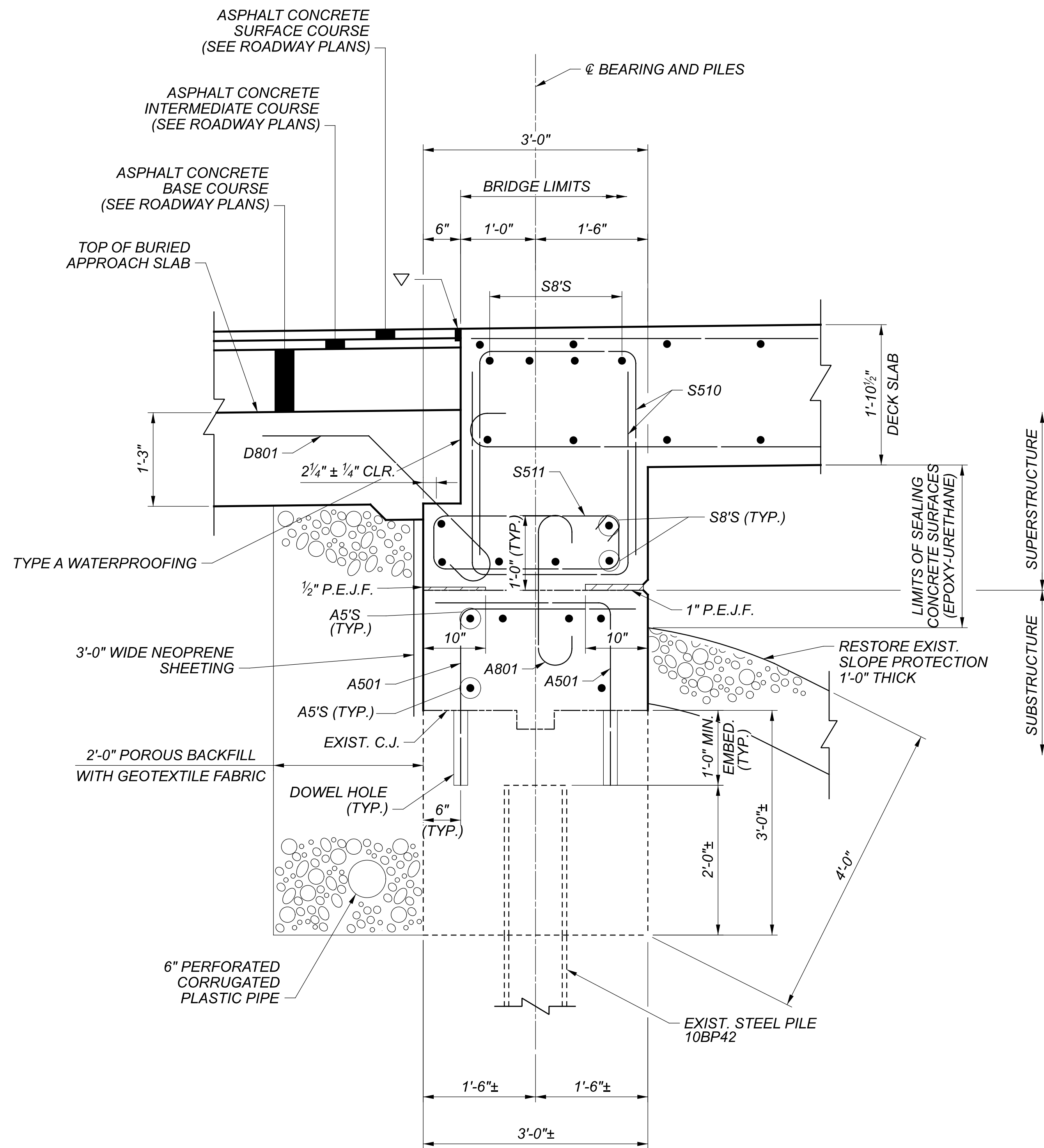
LEGEND:

- * - INDICATES BAR WITH ONE END THREADED
- ** - INDICATES BAR WITH MECHANICAL CONNECTOR ON ONE END
- *** - INDICATES BAR WITH MECHANICAL CONNECTOR ON ONE END AND ONE END THREADED
- + - ELEVATIONS GIVEN AT CENTERLINE BEARING

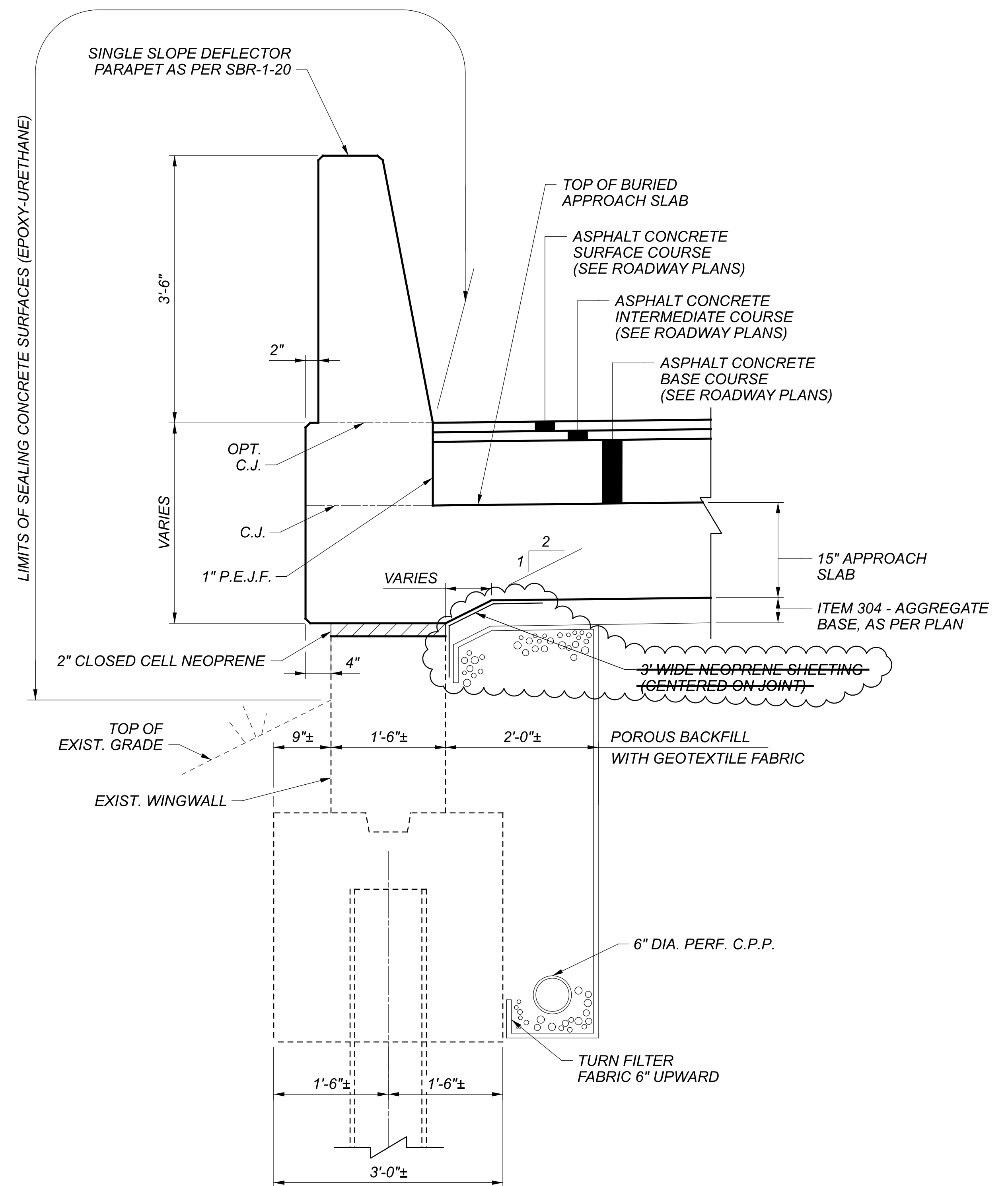
NOTES:

1. MINIMUM LAP LENGTH SHALL BE 2'-5" FOR #5 BARS AND 4'-11" FOR #8 BARS.
2. FOR TYPICAL SECTIONS AND WINGWALL ELEVATIONS, SEE SHEET 7/19 AND 8/19, RESPECTIVELY.
3. FOR REMOVAL DETAILS, SEE SHEETS 3/19 AND 4/19.
4. TYPE 2 WATERPROOFING SHALL BE 3'-0" WIDE CENTERED ON JOINT FROM TOP OF FOOTING TO BOTTOM OF APPROACH SLAB. DURING PHASE 1, ADHERE 1'-6" VERTICAL WIDTH OF WATERPROOFING, FOLD BACK AND PROTECT. ADHERE REMAINDER AFTER PHASE 2 IS CONSTRUCTED. SIMILAR FOR PHASE 2 AND 3.
5. INCLUDE PAYMENT FOR 2" CLOSED CELL NEOPRENE AND 3'-WIDE NEOPRENE SHEETING WITH ITEM 516 - INTEGRAL ABUTMENT EXPANSION JOINT SEAL, AS PER PLAN.

SFN	7710445
DESIGN AGENCY	
DESIGNER	DFK
CHECKER	SJR
REVIEWER	WER
PROJECT ID	111186
SUBSET	6
TOTAL	19
SHEET	99
TOTAL	131



TYPICAL ABUTMENT SECTION



TYPICAL WINGWALL SECTION

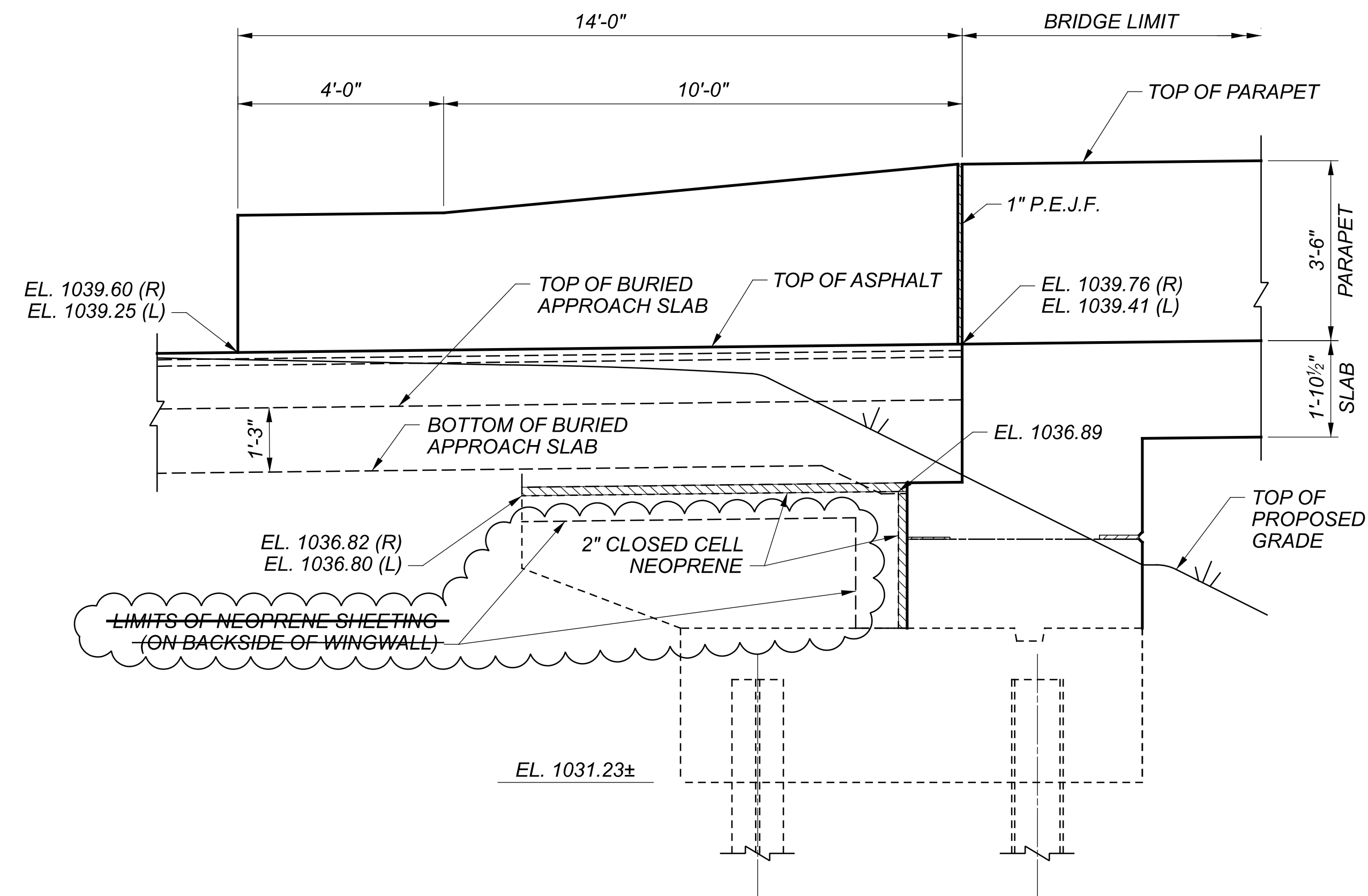
LEGEND:

▽ - 2" DEEP X 1" WIDE HOT APPLIED JOINT SEALER, 705.04.

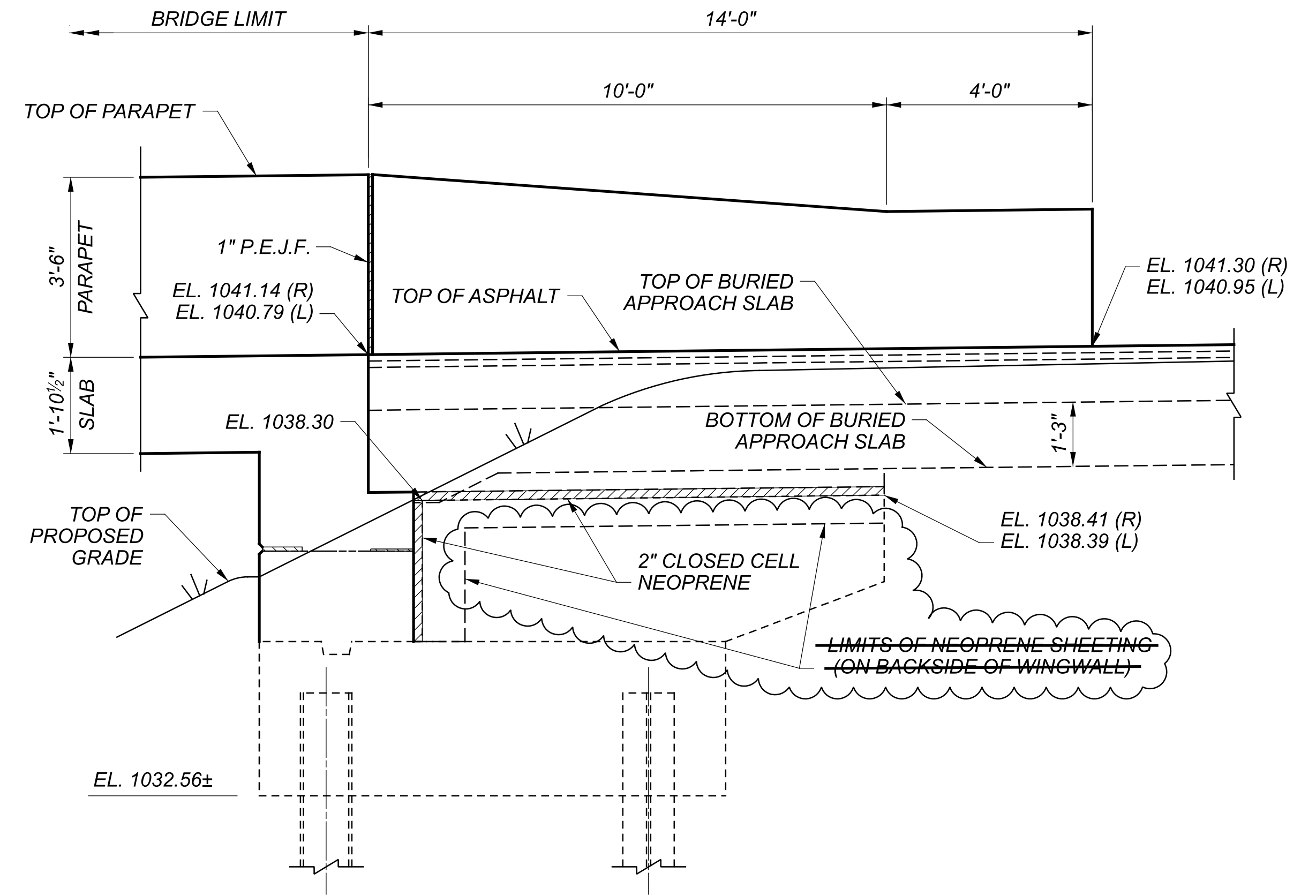
NOTES:

1. REFER TO SCD ICD-1-20, FOR ADDITIONAL DETAILS.

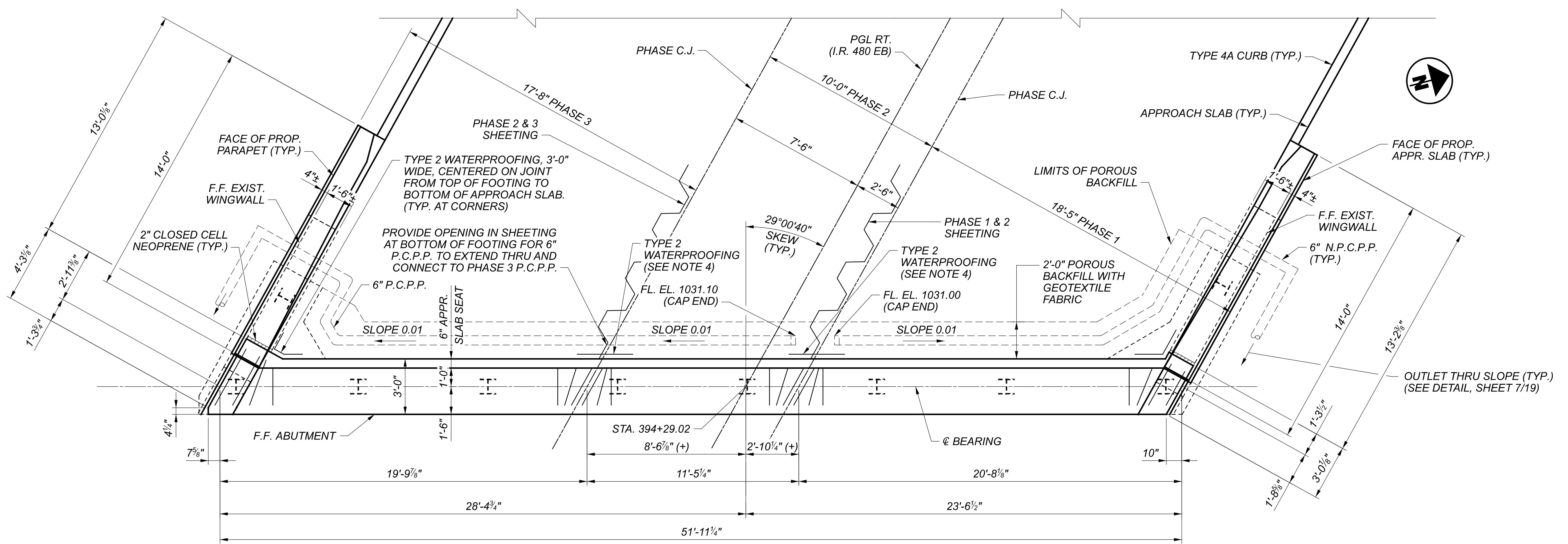
SFN	7710445
DESIGN AGENCY	
DESIGNER	DFK
CHECKER	SJR
REVIEWER	WER
DATE	11/10/23
PROJECT ID	111186
SUBSET	7
TOTAL	19
SHEET	100
TOTAL	131



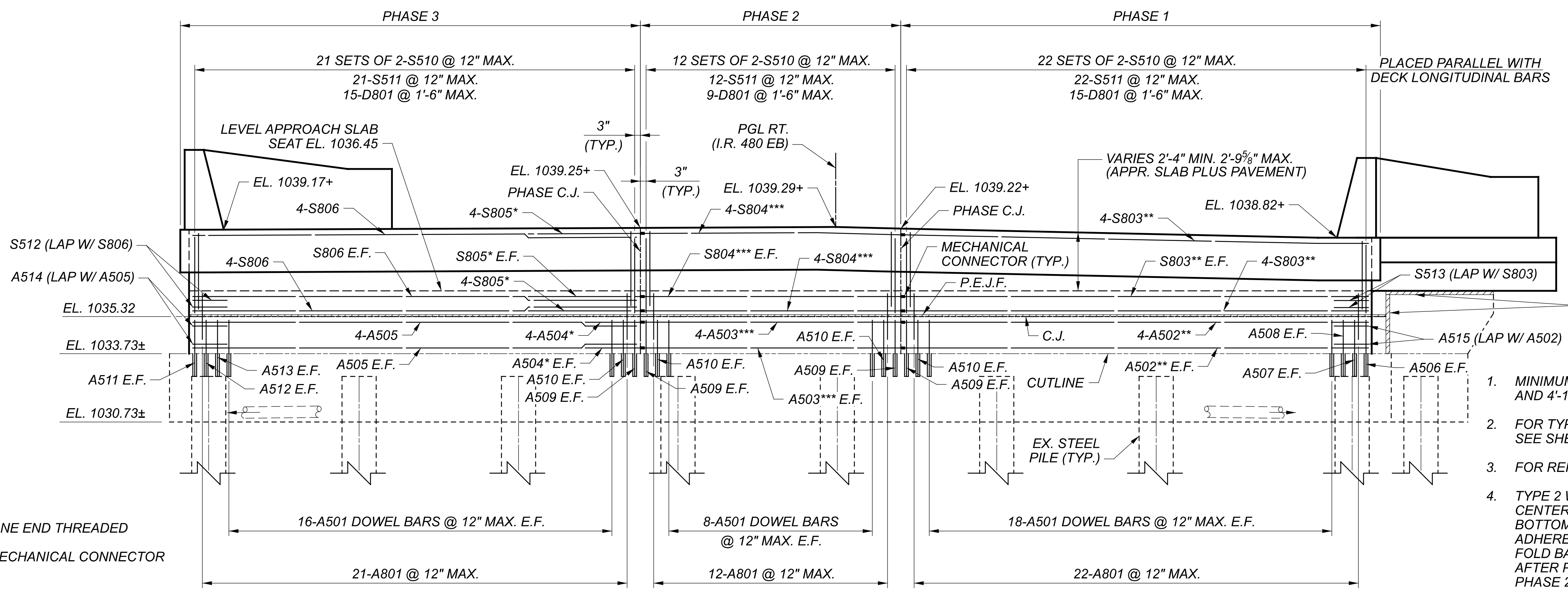
RIGHT WINGWALL ELEVATION - REAR ABUTMENT
 (LEFT WINGWALL ELEVATION OPPOSITE HAND)



RIGHT WINGWALL ELEVATION - FORWARD ABUTMENT
 (LEFT WINGWALL ELEVATION OPPOSITE HAND)



REAR ABUTMENT PLAN



REAR ABUTMENT ELEVATION

LEGEND:

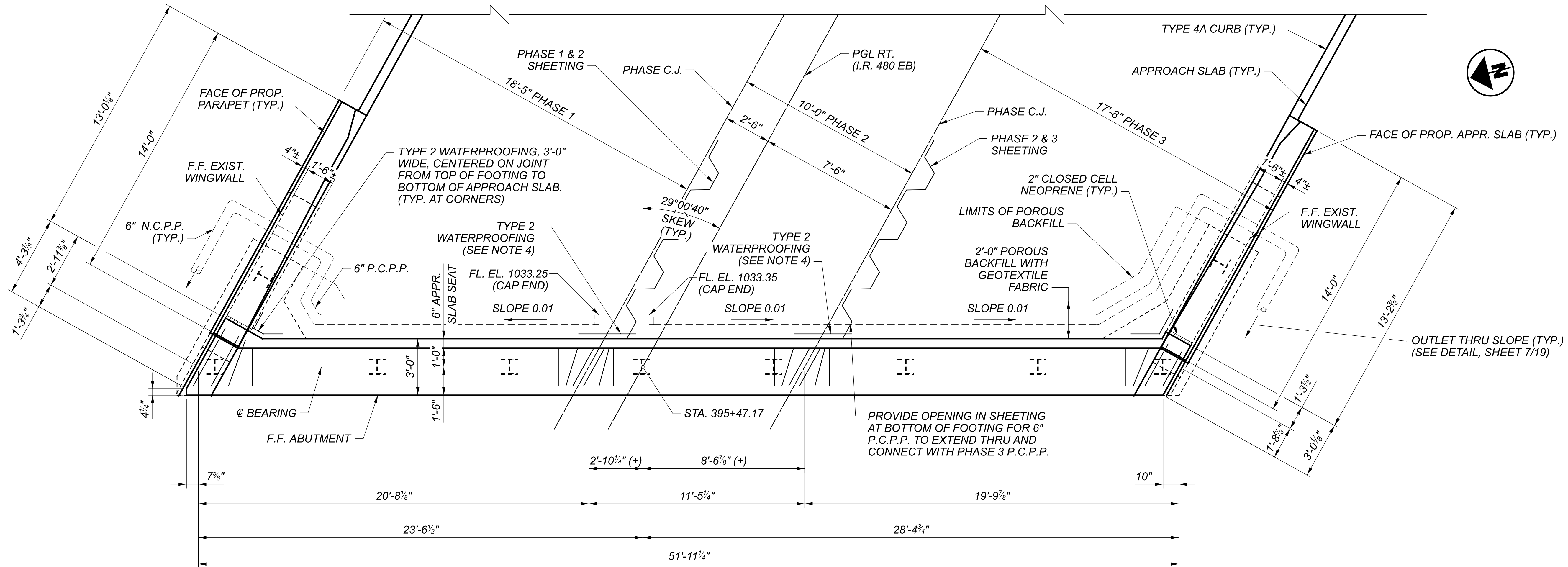
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- + - ELEVATIONS GIVEN AT CENTERLINE BEARING

NOTES:

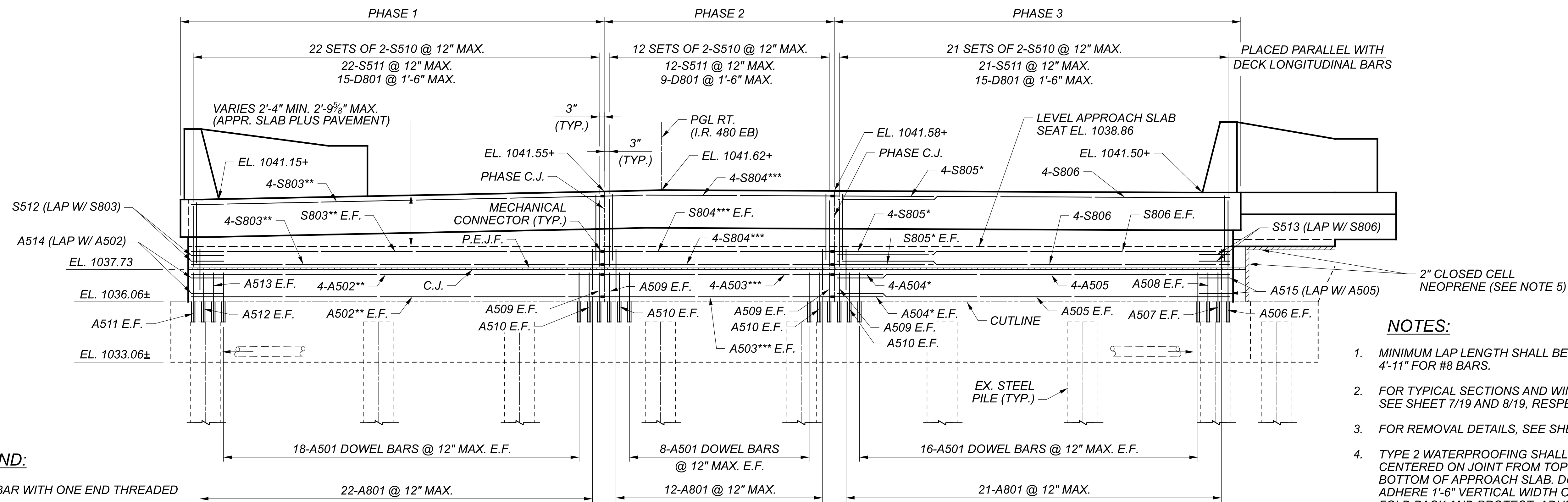
1. MINIMUM LAP LENGTH SHALL BE 2'-5" FOR #5 BARS AND 4'-11" FOR #8 BARS.
2. FOR TYPICAL SECTIONS AND WINGWALL ELEVATIONS, SEE SHEETS 7/19 AND 8/19, RESPECTIVELY.
3. FOR REMOVAL DETAILS, SEE SHEETS 2/19 AND 4/19.
4. TYPE 2 WATERPROOFING SHALL BE 3'-0" WIDE CENTERED ON JOINT FROM TOP OF FOOTING TO BOTTOM OF APPROACH SLAB. DURING PHASE 1, ADHERE 1'-6" VERTICAL WIDTH OF WATERPROOFING, FOLD BACK AND PROTECT. ADHERE REMAINDER AFTER PHASE 2 IS CONSTRUCTED. SIMILAR FOR PHASE 2 AND 3.
5. INCLUDE PAYMENT FOR 2" CLOSED CELL NEOPRENE AND 3" WIDE NEOPRENE SHEETING WITH ITEM 516 - INTEGRAL ABUTMENT EXPANSION JOINT SEAL, AS PER PLAN.

REAR ABUTMENT PLAN AND ELEVATION
 BRIDGE NO. SUM-00480-07.450R
 I.R. 480 OVER STOW ROAD

SFN	7710534
DESIGN AGENCY	
ms consultants, inc.	
DESIGNER	DFK
CHECKER	SJR
REVIEWER	
WER	11/10/23
PROJECT ID	111186
SUBSET	TOTAL
5	19
SHEET	TOTAL
117	131



FORWARD ABUTMENT PLAN



FORWARD ABUTMENT ELEVATION

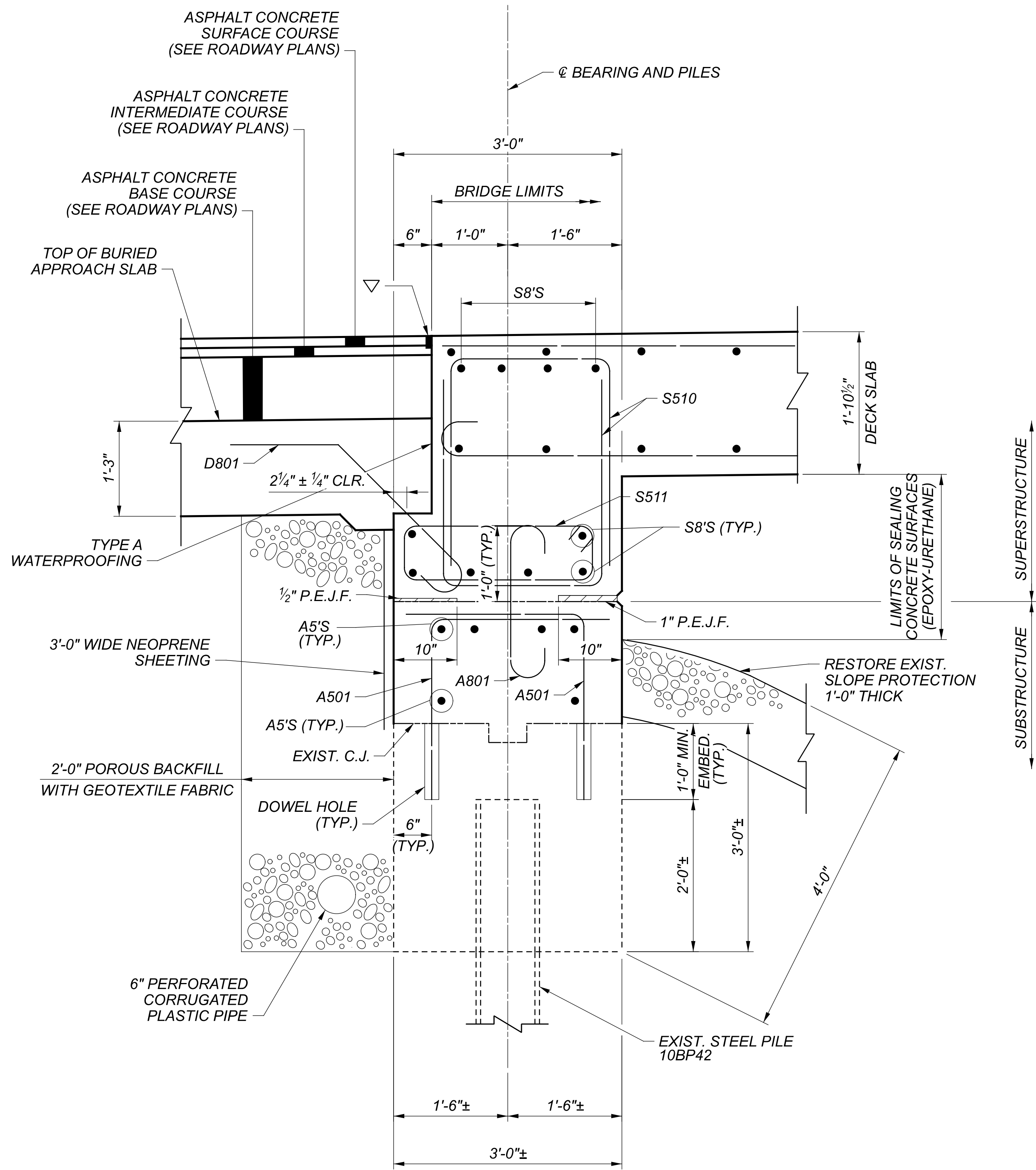
LEGEND:

- * - INDICATES BAR WITH ONE END THREADED
- ** - INDICATES BAR WITH MECHANICAL CONNECTOR ON ONE END
- *** - INDICATES BAR WITH MECHANICAL CONNECTOR ON ONE END AND ONE END THREADED
- + - ELEVATIONS GIVEN AT CENTERLINE BEARING

NOTES:

1. MINIMUM LAP LENGTH SHALL BE 2'-5" FOR #5 BARS AND 4'-11" FOR #8 BARS.
2. FOR TYPICAL SECTIONS AND WINGWALL ELEVATIONS, SEE SHEET 7/19 AND 8/19, RESPECTIVELY.
3. FOR REMOVAL DETAILS, SEE SHEETS 3/19 AND 4/19.
4. TYPE 2 WATERPROOFING SHALL BE 3'-0" WIDE CENTERED ON JOINT FROM TOP OF FOOTING TO BOTTOM OF APPROACH SLAB. DURING PHASE 1, ADHERE 1'-6" VERTICAL WIDTH OF WATERPROOFING, FOLD BACK AND PROTECT. ADHERE REMAINDER AFTER PHASE 2 IS CONSTRUCTED. SIMILAR FOR PHASE 2 AND 3.
5. INCLUDE PAYMENT FOR 2" CLOSED CELL NEOPRENE AND 3" WIDE NEOPRENE SHEETING WITH ITEM 516 - INTEGRAL ABUTMENT EXPANSION JOINT SEAL, AS PER PLAN.

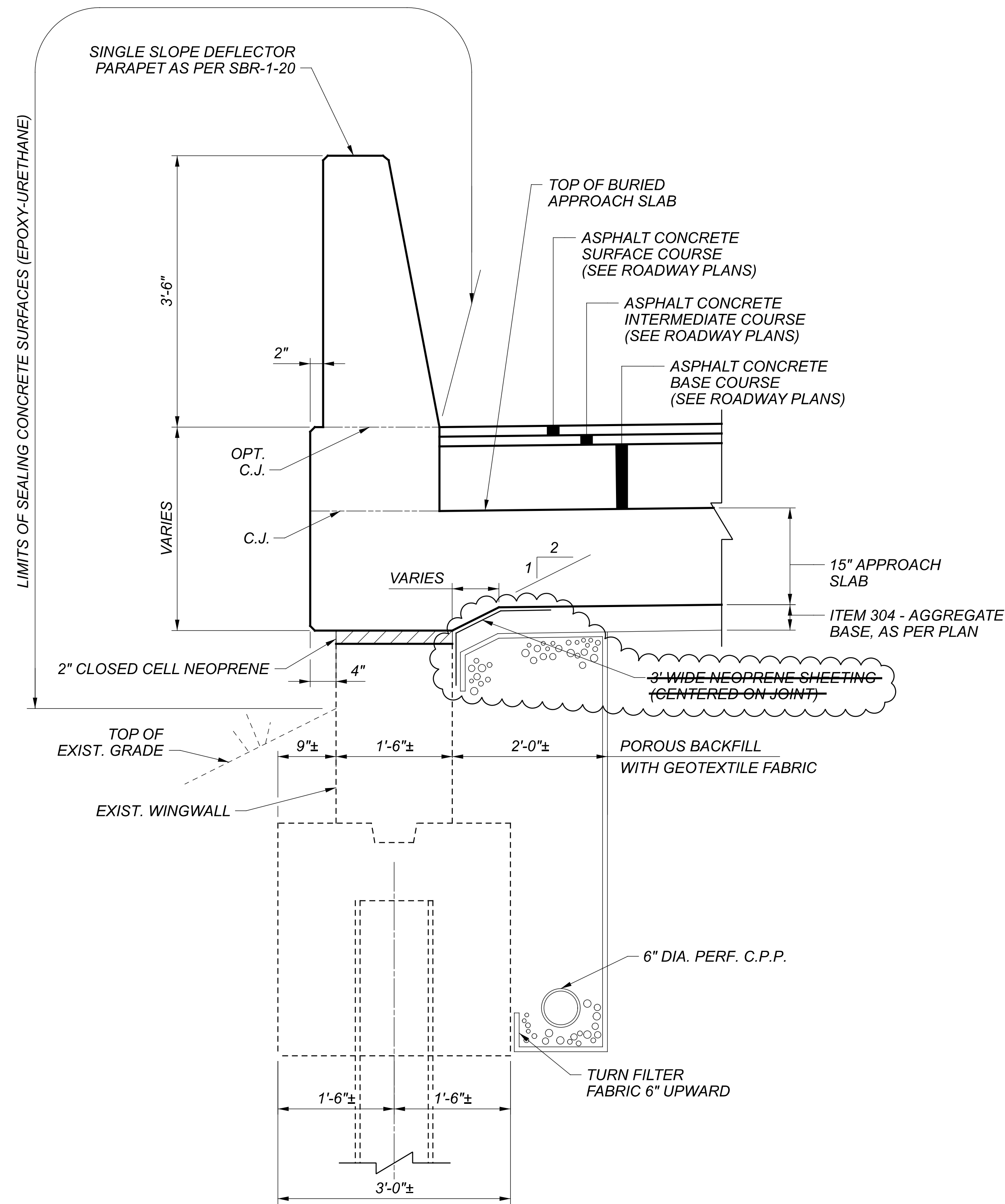
SFN	7710534
DESIGN AGENCY	
DESIGNER	DFK
CHECKER	SJR
REVIEWER	WER
PROJECT ID	111186
SUBSET	6
TOTAL	19
SHEET	118
TOTAL	131



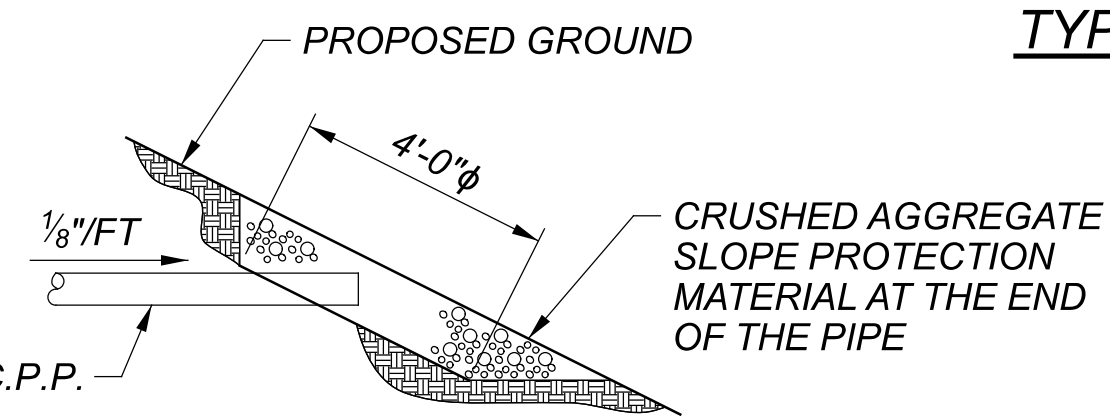
PROPOSED ABUTMENT SECTION
 (REAR ABUTMENT SHOWN)
 (FWD. ABUT. SIMILAR)

LEGEND:

▽ - 2" DEEP X 1" WIDE HOT APPLIED JOINT SEALER, 705.04.



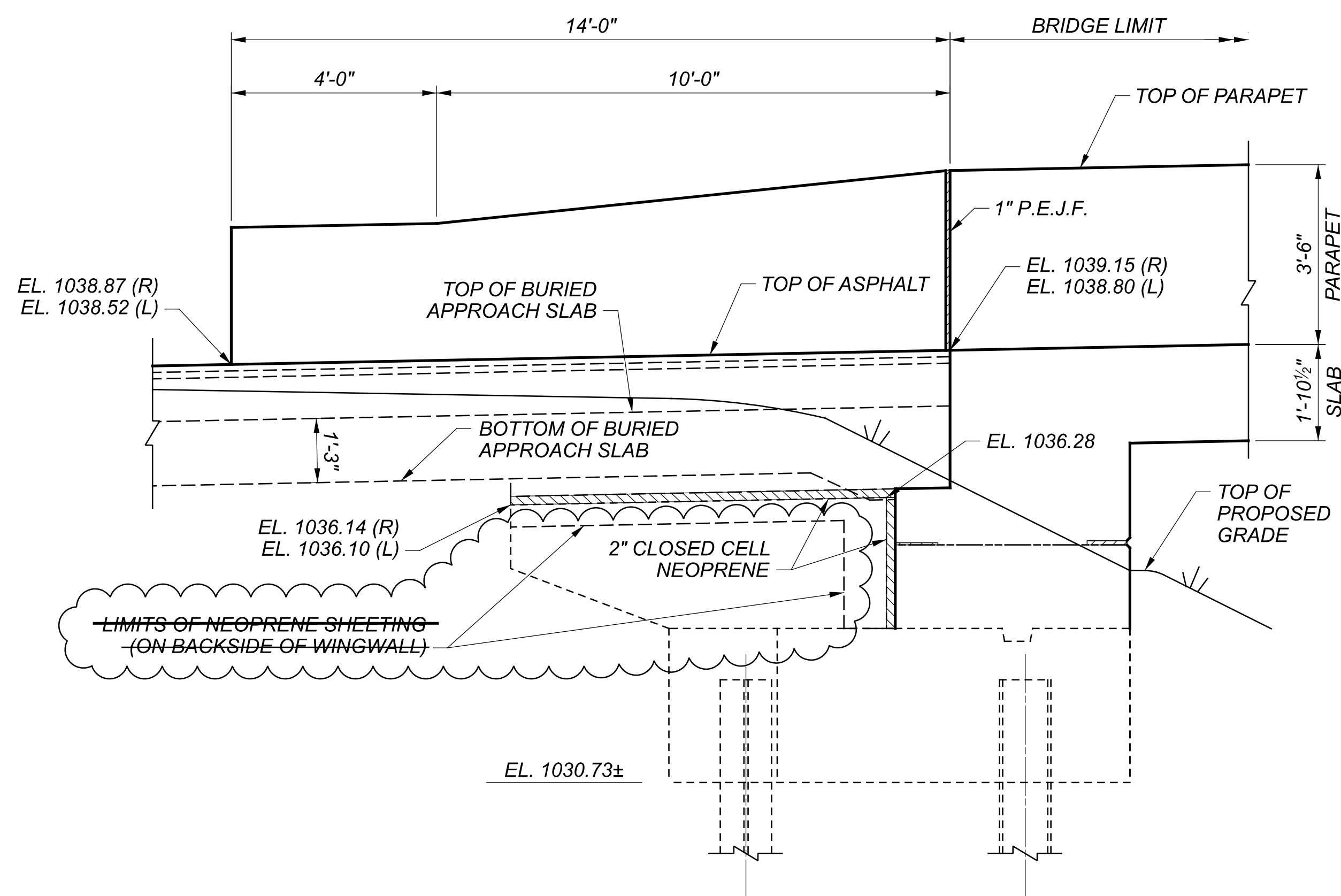
TYPICAL WINGWALL SECTION



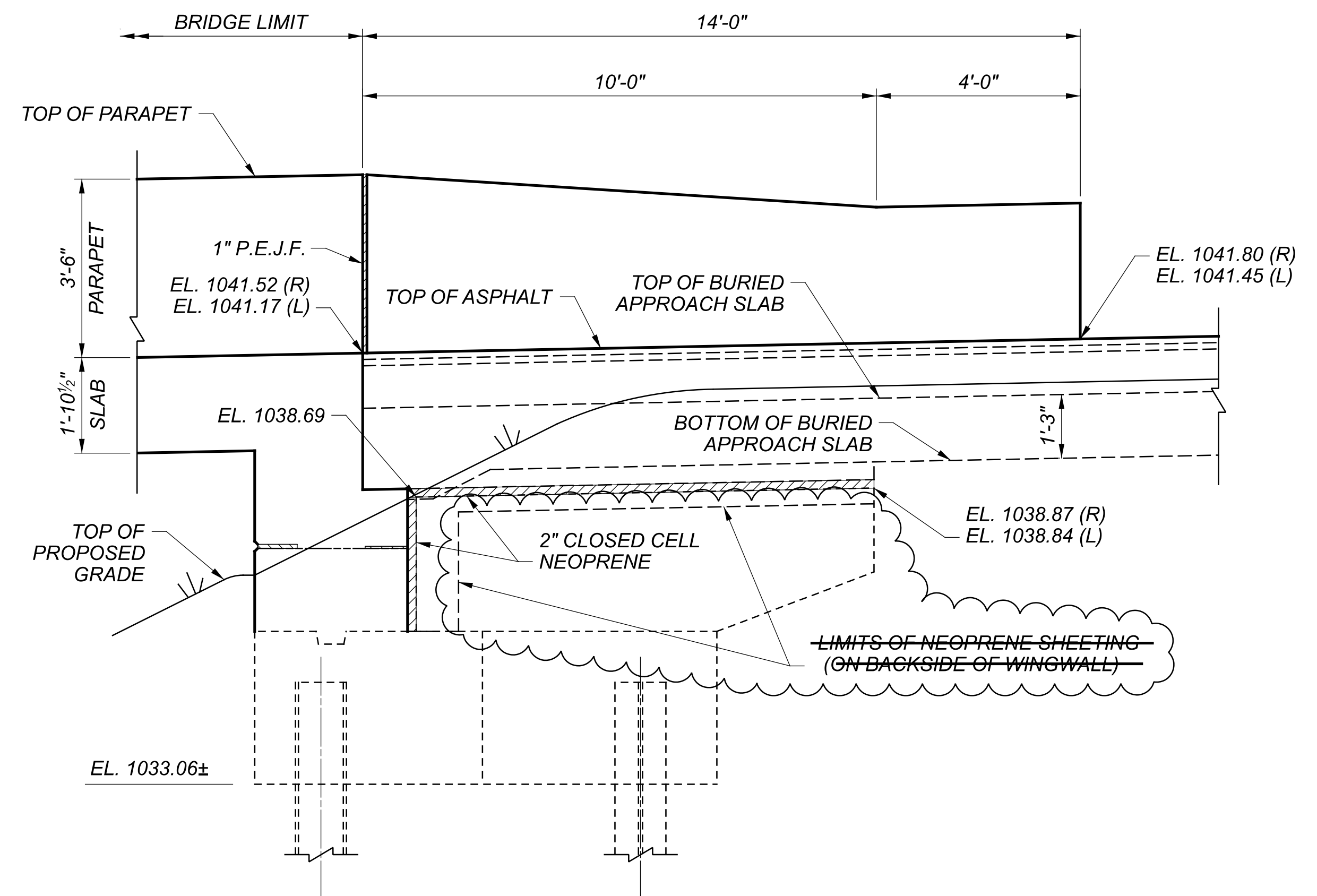
END TREATMENT OF 6" φ NON-PERFORATED CORRUGATED PLASTIC PIPE

NOTES:

1. REFER TO SCD ICD-1-20, FOR ADDITIONAL DETAILS.



RIGHT WINGWALL ELEVATION - REAR ABUTMENT
 (LEFT WINGWALL ELEVATION OPPOSITE HAND)



RIGHT WINGWALL ELEVATION - FORWARD ABUTMENT
 (LEFT WINGWALL ELEVATION OPPOSITE HAND)

