

PAVEMENT

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY (WITH A UNIFORM THICKNESS OF 3 INCHES (AS SHOWN ON THE TYPICAL SECTIONS)).

PART-WIDTH CONSTRUCTION

BECAUSE OF THE NECESSITY TO BUILD THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXERCISE CARE TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN ALL COURSES. LAP LONGITUDINAL JOINTS AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

INTERSECTIONS

INTERSECTIONS WILL BE RESURFACED 2 FT. BEYOND THE EDGE LINE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR AS INDICATED IN THE PLAN. INTERSECTIONS SHALL BE PAVED AFTER COMPLETION OF THE SURFACE COURSE. A BUTT JOINT, AS PER STANDARD CONSTRUCTION DRAWING BP-3.1, SHALL BE USED TO PROVIDE A SMOOTH TRANSITION TO THE EXISTING PAVEMENT. ANY GRADING OR PRIME NECESSARY TO ACCOMPLISH THIS WORK SHALL BE INCLUDED IN THE COST OF THE ASPHALT SURFACE COURSE.

DRIVEWAYS

THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE ASPHALT SURFACE COURSE AND THE EXISTING DRIVEWAYS. IF APPROVED BY THE ENGINEER, AN ASPHALT WEDGE WITH A MINIMUM WIDTH OF 2' MAY BE PLACED EITHER ON THE ROADWAY SHOULDER OR DRIVEWAY DEPENDENT UPON WHICH SIDE IS HIGH. A QUANTITY OF MAINLINE SURFACE COURSE ASPHALT HAS BEEN PROVIDED IN THE CALCULATIONS AND GENERAL SUMMARY TO PERFORM THIS ITEM OF WORK. A QUANTITY OF MAINLINE SURFACE ASPHALT HAS BEEN PROVIDED IN THE CALCULATIONS AND GENERAL SUMMARY TO PERFORM THIS WORK.

IN THE EVENT THAT THE ENGINEER DETERMINES ADDITIONAL WORK IS NECESSARY TO PROPERLY ADDRESS FIELD CONDITIONS, AN ITEM FOR PAVEMENT PLANING HAS BEEN PROVIDED. THE REMOVAL DEPTH IS DEPENDENT UPON THE ELEVATION DIFFERENCE AND ALLOW FOR 1"-2" OF COMPACTED ASPHALT MATERIAL TO BE PLACED.

FIELD DRIVEWAYS

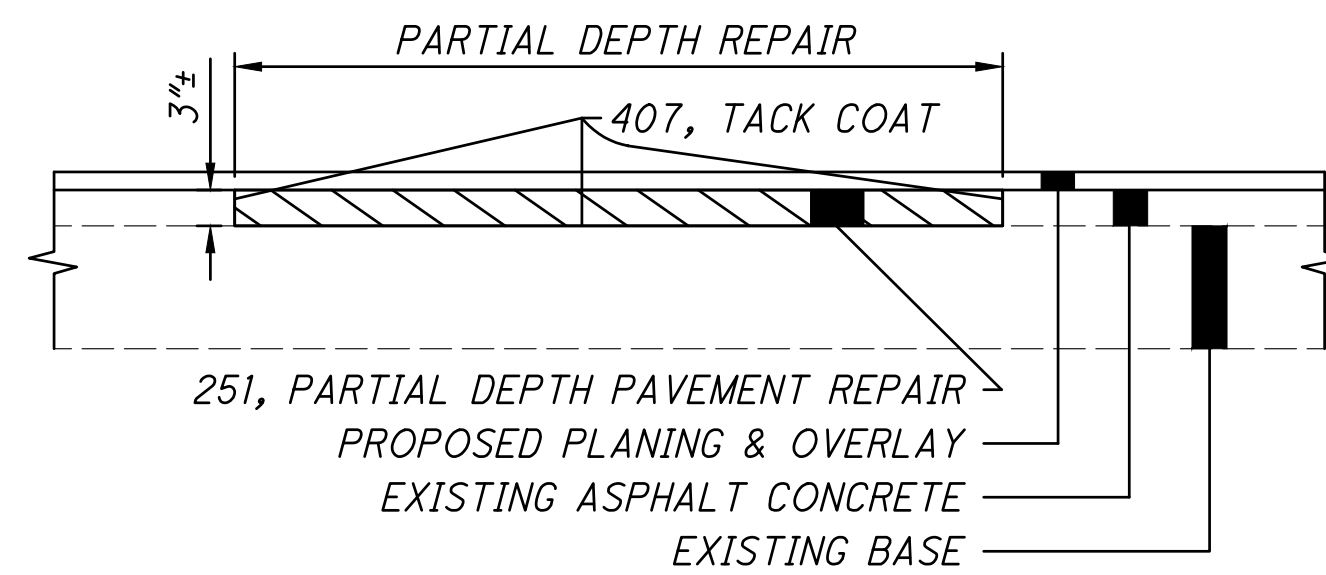
THIS ITEM WILL CONSIST OF PLACING ITEM 411, STABILIZED CRUSHED AGGREGATE. THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE ASPHALT SURFACE COURSE AND THE EXISTING FIELD DRIVEWAYS. FIELD DRIVES WILL BE PLACED AFTER THE COMPLETION OF THE SURFACE COURSE AND SHALL HAVE AN AVERAGE 2" THICKNESS. ALL GRADING TOOLS, EQUIPMENT, MATERIALS, AND INCIDENTALS REQUIRED TO LAYOUT AND CONSTRUCT THE FIELD DRIVES WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 411, AGGREGATE BASE. AN ESTIMATED QUANTITY OF 3 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE I PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

251, PARTIAL DEPTH PAVEMENT REPAIR (441) 1,555 SY



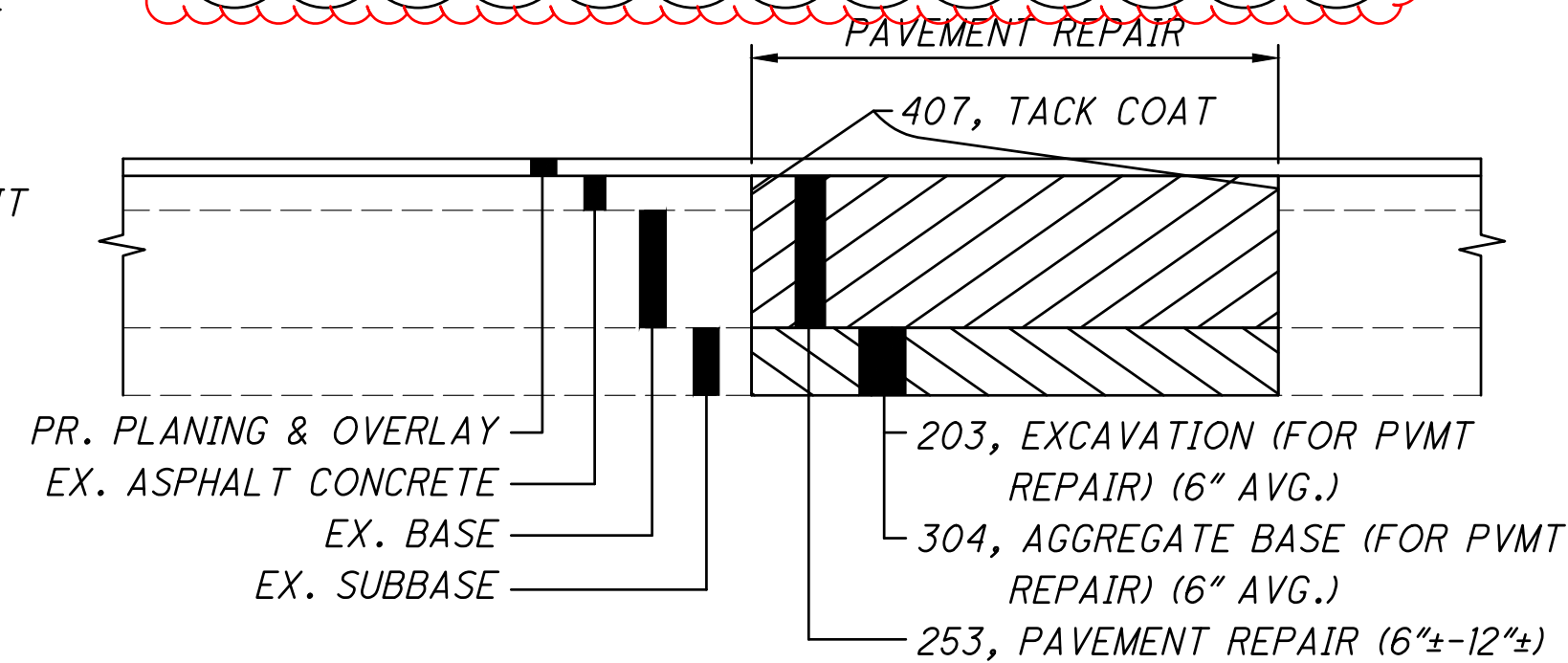
ITEM 253 - PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 6"-12"± 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE.

IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER.

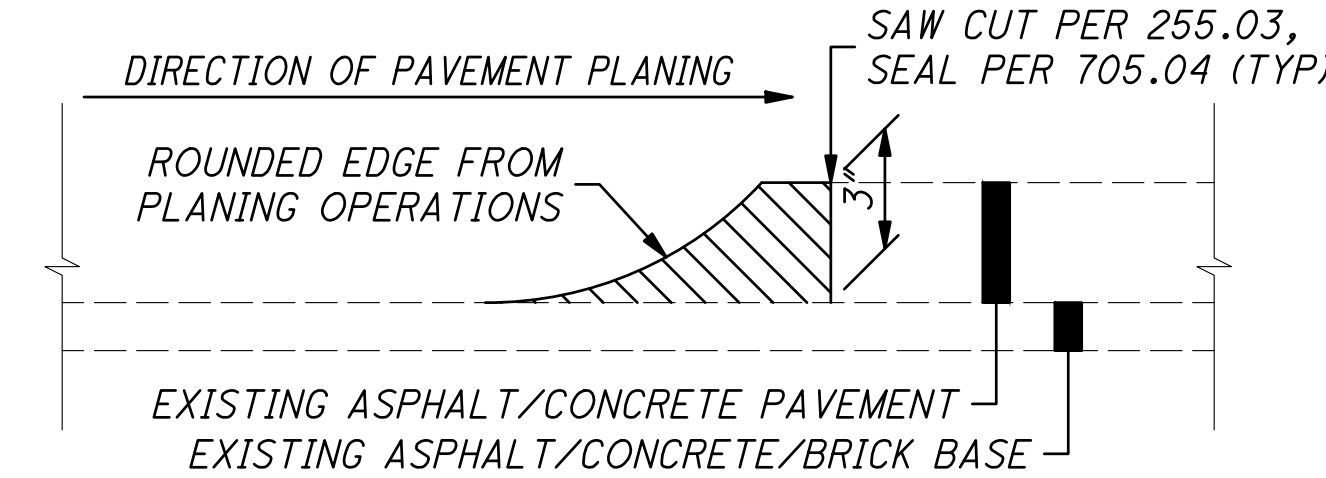
THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

253, PAVEMENT REPAIR 400 CY



ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN

WHEN PLANING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAWCUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE, AS DETAIL ON THIS SHEET. METHOD OF REMOVAL OF REMAINING ASPHALT CONCRETE IS LEFT TO THE DISCRETION OF THE ENGINEER. PAYMENT FOR ALL EQUIPMENT, LABOR, TOOLS, AND OTHER INCIDENTALS REQUIRED TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN.



ITEM 304 - AGGREGATE BASE (FOR PAVEMENT REPAIR)

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO BACKFILL AREAS WHICH WERE EXCAVATED UNDER ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

304, AGGREGATE BASE (FOR PAVEMENT REPAIR) 260 CY

ITEM 408 - PRIME COAT, AS PER PLAN

APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER.

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), AS PER PLAN (PG64-22)

703.05 DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED 'SR' OR 'SRH' ACCORDING TO THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM.

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), AS PER PLAN (PG70-22M)

703.05 DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED 'SR' OR 'SRH' ACCORDING TO THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

IN LOW SHOULDER AREAS EXCEEDING 1", AND ADJACENT TO THE SAFETY EDGE, OR AS DIRECTED BY THE ENGINEER, RECYCLED ASPHALT PAVEMENT (RAP) SHALL BE USED IN AREAS ADJACENT TO THE PAVED BERM. THE RAP SHALL HAVE A MINIMUM PG CONTENT OF 4.5% AND MEET THE FOLLOWING GRADATION. ONCE THE STOCKPILE MEETS THE GRADATION, THE PG CONTENT OF THE RAP SHALL BE DETERMINED PER 441.03. THE RAP ANALYSIS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL 2 WEEKS PRIOR TO USE. METHOD OF MEASUREMENT SHALL BE AS PER 617.06. PLACEMENT AND COMPACTION SHALL MEET THE REQUIREMENTS OF ITEM 617. ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 617 COMPACTED AGGREGATE, AS PER PLAN. MODIFIED GRADATION SHALL APPLY:

SIEVE	TOTAL PERCENT PASSING
1-1/2"	100
3/4"	50-100
NO. 4	35-70
NO. 30	9-33
NO. 200	0-13

changed measurement from SY to CY

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GENERAL NOTES

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BRECKSVILLE RD.**

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NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
LANE CLOSURES & RESTRICTIONS	≥ 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL SHALL HAVE NO OTHER CONSTRUCTION RELATED DUTIES. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 - WATER 50 MGAL

ITEM 614 - MAINTAINING TRAFFIC

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. THE CONTRACTOR SHALL INFORM THE VILLAGE OF RICHFIELD (330) 659-9201, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
2. A MINIMUM OF ONE TEN-FOOT BIDIRECTIONAL LANE SHALL BE MAINTAINED ON BRECKSVILLE ROAD UTILIZING FLAGGERS DURING MILLING AND PAVING OPERATIONS UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL OTHER WORK SHALL BE CONSTRUCTED WHILE MAINTAINING A MINIMUM OF ONE-TEN FOOT LANE IN EACH DIRECTION.
3. ~~SIGNS FURNISHED SHALL BE IN NEW OR LIKE NEW CONDITIONS. LIKE NEW SIGNS SHALL BE SUBJECT TO THE APPROVAL OF THE PROJECT ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES FOR PROVIDING AND MAINTAINING LIGHTS, SIGNS, AND BARRICADES FOR THE MAINTENANCE OF TRAFFIC AND SAFETY OF HIS/HER WORK AT THE LOCATIONS SHOWN ON THESE PLANS OR AS DIRECTED BY THE ENGINEER.~~
4. THE CONTRACTOR SHALL PLACE THE SIGNS; W8-1 [BUMP] PER OMUTCD 2C.28; W8-11 [UNEVEN LANES] PER OMUTCD 6F.45; AND W6-3 [TWO-WAY TRAFFIC] PER OMUTCD 6F.32. PAYMENT FOR THESE SIGNS SHALL BE INCIDENTAL TO THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.
5. LANE RESTRICTIONS OR REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS ARE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
6. ONLY DURING OFF-PEAK PERIODS (i.e. ANY PERIOD OTHER THAN 6-8AM AND 3-6PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.
7. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS, QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.
8. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH. EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.
9. EXISTING TRAFFIC CONTROL DEVICES (SIGNS AND/OR TRAFFIC SIGNALS), LOCATED WITHIN THE WORK AREA WHICH ARE REQUIRED FOR INTERIM OR PERMANENT TRAFFIC CONTROL, SHALL BE RELOCATED TO POINTS APPROVED BY THE ENGINEER. APPROPRIATE TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED IN COMPLIANCE WITH THE OMUTCD AT ALL TIMES WHILE TRAFFIC IS MAINTAINED.
10. THE CONTRACTOR SHALL DIVERT TRAFFIC FROM NORMAL CHANNELS BY THE USE OF PLASTIC DRUMS, FLASHING ARROW BOARDS COMPLYING WITH ODOT CMS 614 AND WORK ZONE SIGNS AND PAVEMENT MARKINGS.
11. THE PLACEMENT OF THE SIGNS AND THE DRUMS SHALL BE MADE SIMULTANEOUSLY. NO DRUMS SHALL BE PLACED WITHOUT PROPER SIGNAGE IN PLACE. CONFLICTING SIGNING SHALL BE COVERED.

ITEM 614 - MAINTAINING TRAFFIC (CONTINUED)

12. NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

13. LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PEDESTRIAN ACCESS

DURING TEMPORARY CLOSURE OR RELOCATION OF SIDEWALKS AND OTHER PEDESTRIAN FACILITIES, TEMPORARY PEDESTRIAN ACCESS FACILITIES SHALL BE PROVIDED. THESE FACILITIES SHALL BE DETECTABLE AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING FACILITY. PEDESTRIAN SIDEWALK CLOSURES, CROSSWALK CLOSURES AND PEDESTRIAN DETOURS OR BYPASSES SHALL BE INSTALLED IN ACCORDANCE WITH ODOT STANDARD CONSTRUCTION DRAWING MT-110.10.

ADVANCE NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE FIVE (5) CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$2000 PER DAY THAT THE TRAFFIC IS PLACES ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

INSTALLATION OF PAVEMENT MARKINGS

THE CONTRACTOR MAY REDUCE THE NUMBER OF THROUGH TRAFFIC LANES BY 50% AS DIRECTED BY THE ENGINEER, IN ORDER TO REMOVE PAVEMENT MARKINGS OR TO INSTALL WORK ZONE OR PERMANENT PAVEMENT MARKINGS. THE CONTRACTOR SHALL LIMIT THE AFOREMENTIONED CLOSURE TO BETWEEN THE HOURS OF 9:00 A.M. AND 3:30 P.M. UNLESS OTHERWISE APPROVED BY THE ENGINEER.

ALL WORK ZONE PAVEMENT MARKINGS AND SIGNS REQUIRED FOR A PARTICULAR LANE CLOSURE OR TRAFFIC PATTERN SHALL BE INSTALLED ON A SINGLE WORK DAY AND THE CORRESPONDING TRAFFIC PATTERN SHALL BE IMMEDIATELY IMPLEMENTED. IN ADDITION, THE REQUIREMENTS OF ODOT 614.11 SHALL APPLY.

DRIVEWAY ACCESS

THIS WORK SHALL BE IN CONJUNCTION WITH ITEM 614 - MAINTAINING TRAFFIC AND ALL COSTS INCURRED FOR THIS ITEM SHALL BE INCLUDED IN THE LUMP SUM BID FOR MAINTAINING TRAFFIC. NO ADDITIONAL PAYMENTS WILL BE MADE. ACCESS TO ALL PROPERTIES, INCLUDING RESIDENCES AND BUSINESSES, SHALL BE MADE AVAILABLE AT ALL TIMES DURING CONSTRUCTION.

THE CONTRACTOR SHALL MAKE AVAILABLE DURING CONSTRUCTION, STEEL PLATES, BRIDGES OR OTHER MEANS APPROVED BY THE ENGINEER TO BRIDGE ACROSS THE ROADWAY CONSTRUCTION, TO PROVIDE FULL TIME ACCESS (TWENTY-FOUR (24) HOURS, SEVEN (7) DAYS A WEEK) TO DRIVEWAYS THAT REQUIRE ACCESS. ALL DRIVEWAY CLOSURES AND/OR APRON WORK SHALL BE CONSTRUCTED IN ALTERNATING INTERVALS IN ORDER TO ELIMINATE ABUTTING PROPERTY OWNERS HAVING BOTH OF THEIR DRIVEWAYS CLOSED.

ADDENDUM		
NO.	DATE	DESCRIPTION
1	05/05/22	MOT NOTE REVISION

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SHEET NUMBER													PARTICIPATION		ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
6	7	8	13	14								01/MPO/ PV/RICH									
																			ROADWAY		
LS													LS	201	11000	LS		CLEARING AND GRUBBING	6		
25													25	202	98100	25	EACH	REMOVAL MISC.: BARRIER REFLECTOR	6		
260													260	203	10000	260	CY	EXCAVATION (FOR PAVEMENT REPAIR)			
18													18	209	15000	18	STA	RESHAPING UNDER GUARDRAIL	6		
				141									141	209	60200	141	STA	LINEAR GRADING	6		
				141									141	209	72000	141	STA	PREPARING SUBGRADE FOR SHOULDER PAVING			
2													2	SPECIAL	690E98000	2	EACH	VERTICAL CLEARANCE	6		
LS													LS	SPECIAL	690E98400	LS		PRE-CONSTRUCTION VIDEO	6		
																				EROSION CONTROL	
3720													3720	659	10000	3720	SY	SEEDING AND MULCHING	6		
0.50													0.50	659	20000	0.50	TON	COMMERCIAL FERTILIZER	6		
0.77													0.77	659	31000	0.77	ACRE	LIME	6		
10													10	659	35000	10	MGAL	WATER	6		
	20000												20000	832	30001	20000	EACH	EROSION CONTROL, AS PER PLAN	7		
																				DRAINAGE	
													4	611	98631	4	EACH	CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN	7		
	4												4	611	98644	4	EACH	CATCH BASIN GRATE, TYPE 3	7		
													6	611	99655	6	EACH	MANHOLE ADJUSTED TO GRADE, AS PER PLAN	7		
	5200												5200	SPECIAL	611E99820	5200	LB	MISCELLANEOUS METAL	7		
																				PAVEMENT	
													1555	251	01000	1555	SY	PARTIAL DEPTH PAVEMENT REPAIR (441)	8		
													400	253	02000	400	CY	PAVEMENT REPAIR	8		
													31103	254	01001	31103	SY	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (T-3")	8		
													260	304	20000	260	CY	AGGREGATE BASE (FOR PAVEMENT REPAIR)	8		
													4376	407	20000	4376	GAL	NON-TRACKING TACK COAT			
													1232	408	10001	1232	GAL	PRIME COAT, AS PER PLAN	8		
													3	411	10000	3	CY	STABILIZED CRUSHED AGGREGATE	8		
													39	441	50101	39	CY	1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), AS PER PLAN (PG64-22)	8		
													1092	441	50101	1092	CY	1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), AS PER PLAN (PG70-22M)	8		
													1533	441	50300	1533	CY	1.75" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448) (PG64-22)			
													282	617	10101	282	CY	COMPACTED AGGREGATE, AS PER PLAN	8		
													2843	875	10000	2843	LB	LONGITUDINAL JOINT ADHESIVE			
																				WATER WORK	
													2	638	10801	2	EACH	VALVE BOX ADJUSTED TO GRADE, AS PER PLAN	7		

GENERAL SUMMARY

SUM CR 0017 23.27
BRECKSVILLE RD.