

**UTILITIES**

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, THE OHIO UTILITIES PROTECTION SERVICE (OUPS), THE OHIO & GAS PROCEDURES UNDERGROUND PROTECTION SERVICE (OGPUPS), THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 4 HEAD-QUARTERS AND ALL NON REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION IN ALL AREAS.

OUPS 1-800-362-2764 (CONTACT LIMITED BASIS PARTICIPANTS DIRECTLY)  
 OGPUPS 1-800-925-0988  
 ODOT 330-786-2267 MICHELLE CHANEY  
 ODOT ITS - EMAIL: CEN.ITS.LAB@DOT.OHIO.GOV  
 TRAFFIC MONITORING SECTION ODOT, 1980 W. BROAD ST, COLUMBUS, OH 43223  
 DAN DIDDLE 614-560-9541

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

**WORK LIMITS**

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**PAVEMENT MARKING LANE WIDTHS**

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKINGS ON THIS PROJECT SHALL BE AS FOLLOWS:

ROUTE	S.L.M. TO S.L.M.	LANE WIDTH
SR 11	19.02 28.47	12'

**PAVEMENT MARKING DETAILS**

THE PAVEMENT MARKING DETAIL SHEETS WILL BE SUPPLIED TO THE CONTRACTOR AT THE PRE-CONSTRUCTION MEETING.

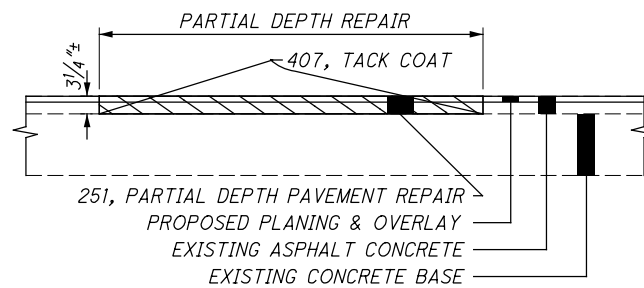
**PROFILE AND ALIGNMENT**

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)**

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE I PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED BEFORE THE MAINLINE PAVEMENT PLANING COMMENCES. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

251, PARTIAL DEPTH PAVEMENT REPAIR (441), 15,000 SQ. YD.



**ITEM 618 - RUMBLE STRIPS, (ASPHALT CONCRETE)**

FOR USE ALONG SR-11 WITHIN THE PROJECT LIMITS, FROM SLM 19.02 TO SLM 28.47. OFFSET "A" AND "B" SHALL FOLLOW THE OFFSET DIMENSION TABLE AS SHOWN ON STANDARD CONSTRUCTION DRAWING B.P-9.1 FOR THIS ITEM OF WORK.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:  
 ITEM 618, RUMBLE STRIPS (ASPHALT CONCRETE) 37.8 MILES

**ITEM 203 - EXCAVATION (FOR PAVEMENT REPAIR)**

THIS ITEM OF WORK SHALL CONSIST OF REMOVING AND DISPOSING OF ALL UNSUITABLE MATERIAL BY EXCAVATING THE EXISTING SUBGRADE AND SUBBASE TO AN AVERAGE DEPTH OF 6 INCHES OR AS DIRECTED BY THE ENGINEER. EXACT LIMITS OF REMOVAL SHALL BE DETERMINED BY THE ENGINEER. ALL EQUIPMENT, LABOR, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

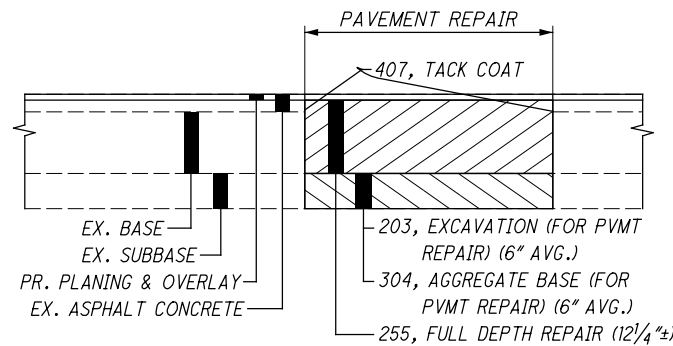
203, EXCAVATION (FOR PAVEMENT REPAIR), 833 CU YD

**ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT, CLASS QC1**

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 12"± FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT, CLASS QC1. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED BEFORE THE MAINLINE PAVEMENT PLANING. ALSO, THIS ITEM SHALL BE COMPLETED WITHIN 3 DAYS OF THE MAINLINE PAVEMENT PLANING. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

255, FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT, CLASS QC1, 5,000 SY  
 255, FULL DEPTH PAVEMENT SAWING, 26,250 FT

Adjusted note and material type



**ITEM 304 - AGGREGATE BASE (FOR PAVEMENT REPAIR)**

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO BACKFILL AREAS WHICH WERE EXCAVATED UNDER ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

304, AGGREGATE BASE (FOR PAVEMENT REPAIR), 833 CU YD

**ITEM 408 - PRIME COAT, AS PER PLAN**

APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER.

**ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN**

IN LOW SHOULDER AREAS EXCEEDING 1", AND ADJACENT TO THE SAFETY EDGE, OR AS DIRECTED BY THE ENGINEER, RECYCLED ASPHALT PAVEMENT (RAP) SHALL BE USED IN AREAS ADJACENT TO THE PAVED BERM. THE RAP SHALL HAVE A MINIMUM PG CONTENT OF 4.5% AND MEET THE FOLLOWING GRADATION. ONCE THE STOCKPILE MEETS THE GRADATION, THE PG CONTENT OF THE RAP SHALL BE DETERMINED PER 441.03. THE RAP ANALYSIS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL 2 WEEKS PRIOR TO USE. METHOD OF MEASUREMENT SHALL BE AS PER 617.06. PLACEMENT AND COMPACTION SHALL MEET THE REQUIREMENTS OF ITEM 617. ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 617 COMPACTED AGGREGATE, AS PER PLAN.

MODIFIED GRADATION SHALL APPLY:

SIEVE	TOTAL PERCENT PASSING
1-1/2"	100
3/4"	50-100
NO. 4	35-70
NO. 30	9-33
NO. 200	0-13

**ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446), AS PER PLAN (PG70-22M)**

703.05 DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED "SR" OR "SRH" ACCORDING TO THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM.

**ASPHALT GRINDINGS**

THE CONTRACTOR SHALL DELIVER A MINIMUM OF 3,000 TONS OF ASPHALT GRINDINGS TO THE BELOW LOCATIONS:

ODOT D4 OUTPOST (GUSTAVUS OUTPOST)  
 2979 KINSMAN ROAD, NORTH BLOOMFIELD, OHIO 44450  
 JOE PARTHEMER - 330.786.4988

THIS WORK SHALL BE COORDINATED BETWEEN THE PROJECT ENGINEER, THE CONTRACTOR, AND JOE PARTHEMER. PAYMENT FOR THIS WORK WILL BE INCIDENTAL TO:

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (T=1/2")

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CALCULATED  
 CMR  
 CHECKED  
 MJA

GENERAL NOTES

TRU-11-19.02

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SHEET NUM.											PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
3	4	5	7	8	9						01/NHS/P V	EXT	TOTAL				
<b>ROADWAY</b>																	
833											833	203	10000	833	CY	EXCAVATION	
<b>EROSION CONTROL</b>																	
											3,000	832	30000	3,000	EACH	EROSION CONTROL	
<b>PAVEMENT</b>																	
15,000											15,000	251	01000	15,000	SY	PARTIAL DEPTH PAVEMENT REPAIR (441)	
			437,074								437,074	254	01000	437,074	SY	PAVEMENT PLANING, ASPHALT CONCRETE (T=1 1/2")	
5,000											5,000	255	10010	5,000	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT CLASS QC1	
26,250											26,250	255	20000	26,250	FT	FULL DEPTH PAVEMENT SAWING	
833											833	304	20000	833	CY	AGGREGATE BASE	
																changed material type	
											39,337	407	20000	39,337	GAL	NON-TRACKING TACK COAT	
											19,691	408	10001	19,691	GAL	PRIME COAT, AS PER PLAN	
											12,281	441	00100	12,281	CY	ANTI-SEGREGATION EQUIPMENT	
											18,212	442	10001	18,212	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (446), AS PER PLAN (PG70-22M) (T=1 1/2)	
											2,735	617	10101	2,735	CY	COMPACTED AGGREGATE, AS PER PLAN	
											38	618	40600	38	MILE	RUMBLE STRIPS, (ASPHALT CONCRETE)	
											26,546	875	10000	26,546	LB	LONGITUDINAL JOINT ADHESIVE	
<b>TRAFFIC CONTROL</b>																	
											1,148	621	00100	1,148	EACH	RPM	
											870	621	54000	870	EACH	RAISED PAVEMENT MARKER REMOVED	
											40.8	646	10010	40.8	MILE	EDGE LINE, 6"	
											18.9	646	10110	18.9	MILE	LANE LINE, 6"	
											3,020	646	10310	3,020	FT	CHANNELIZING LINE, 12"	
											225	646	10400	225	FT	STOP LINE	
											8	646	20320	8	EACH	WRONG WAY ARROW	
											6,775	646	20504	6,775	FT	DOTTED LINE, 6"	
<b>STRUCTURE REPAIRS</b>																	
																FOR TRU-11-2383 ESTIMATED QUANTITIES	
																12	
																FOR TRU-11-2485 ESTIMATED QUANTITIES	
																12	
																FOR TRU-11-2691 ESTIMATED QUANTITIES	
																12	
																FOR TRU-11-2790 ESTIMATED QUANTITIES	
																12	
																FOR TRU-87-1513 ESTIMATED QUANTITIES	
																12	
<b>MAINTENANCE OF TRAFFIC</b>																	
	200										200	614	11110	200	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
	32										32	614	12460	32	EACH	WORK ZONE MARKING SIGN	
	20										20	614	13000	20	CY	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
		6									6	614	18601	6	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	
	18.9										18.9	614	20010	18.9	MILE	WORK ZONE LANE LINE, CLASS I, 6"	
	18.9										18.9	614	20560	18.9	MILE	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	
	40.8										40.8	614	22360	40.8	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	
	3,020										3,020	614	23010	3,020	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	
	3,020										3,020	614	23690	3,020	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
		24									24	808	18700	24	SNMT	DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY	
																5	
<b>INCIDENTALS</b>																	
											LS	614	11000	LS		MAINTAINING TRAFFIC	
											6	619	16020	6	MNTH	FIELD OFFICE, TYPE C	
											LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
											LS	624	10000	LS		MOBILIZATION	

GENERAL SUMMARY

TRU-11-19.02