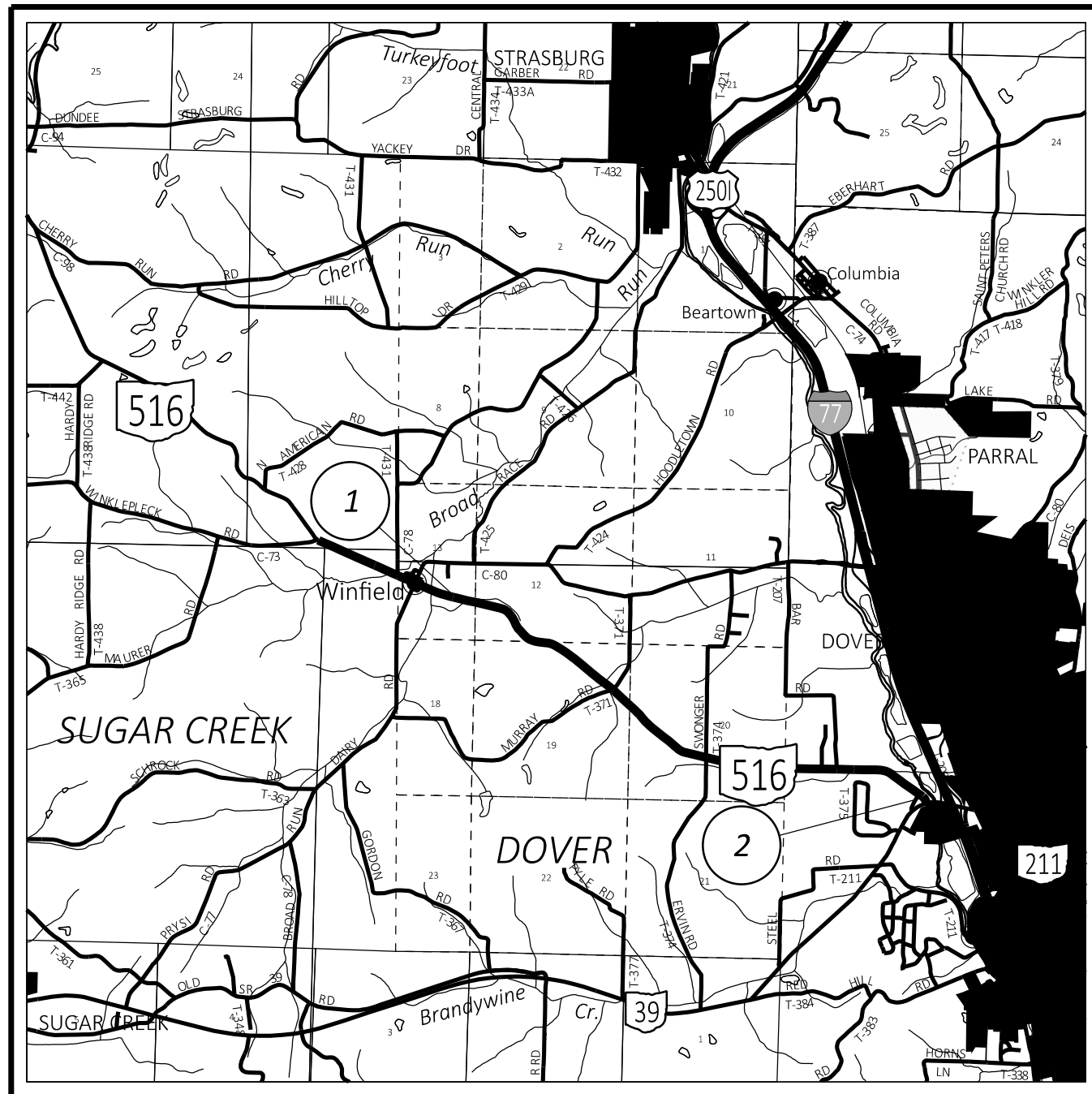


# STATE OF OHIO DEPARTMENT OF TRANSPORTATION

## TUS-516-3.70

### DOVER TOWNSHIP VILLAGE OF WINFIELD



**LOCATION MAP**

LATITUDE: N 40° 33' 35" LONGITUDE: W 81° 47' 40"



**INDEX OF SHEETS:**

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**FEDERAL PROJECT NUMBER**

E161563

**RAILROAD INVOLVEMENT**

NONE

**PROJECT DESCRIPTION**

441 - 2-LANE RESURFACING  
ASPHALT RESURFACING OF 4.02 MILES OF S.R. 516 IN TUSCARAWAS COUNTY.  
IN ADDITION TO RESURFACING OPERATIONS THIS WORK INCLUDES PAVEMENT  
REPAIRS, PAVEMENT MARKINGS, RPM'S, AND MINOR STRUCTURE REPAIR.

**EARTH DISTURBED AREAS**

PROJECT EARTH DISTURBED AREA: 0.00 ACRES  
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.00 ACRES  
NOTICE OF INTENT EARTH DISTURBED AREA: 0.00 ACRES  
\* NOI NOT REQUIRED  
\*ROUTINE MAINTENANCE PROJECT

**2023 SPECIFICATIONS**

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF  
TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN  
THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL  
SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN  
THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS  
IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY  
AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC  
WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

*Thomas D. Corey*  
Thomas D. Corey  
District 11 Deputy Director

*Jack Marchbanks*  
Jack Marchbanks, PhD  
Director, Department of Transportation

TITLE SHEET

**DESIGN EXCEPTIONS**

NONE

**ADA DESIGN WAIVERS**

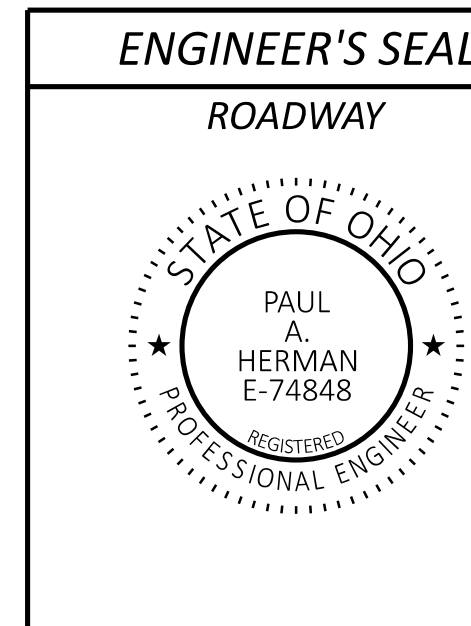
NONE

**UNDERGROUND UTILITIES**  
Contact Two Working Days  
Before You Dig

Before You Dig

**OHIO811, 8-1-1, or 1-800-362-2764**  
(Non members must be called directly)

PLAN PREPARED BY:  
ODOT DISTRICT 11  
ENGINEERING  
NEW PHILADELPHIA, OHIO



PART	COUNTY	ROUTE	FUNDING	PROJECT TERMINI		NET LENGTH MILES	CITY (VILLAGE)
				BEGIN	END		
1	TUS	S.R.516	01/STR/05	3.70	7.05	3.35	
2	TUS	S.R.516	02/S<2/05	7.05	7.72	0.67	

STANDARD CONSTRUCTION DRAWINGS							SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-3.1	1/21/22	MT-97.10	4/19/19	TC-41.20	10/18/13		800-2023	7/21/23		
BP-6.1	7/19/13	MT-97.12	1/20/17	TC-42.20	10/18/13		832	7/15/23		
		MT-99.20	4/19/19	TC-52.10	10/18/13		846	4/17/15		
DM-4.3	1/15/16	MT-101.90	7/17/20	TC-65.10	1/17/14					
DM-4.4	1/15/16	MT-105.10	1/17/20	TC-65.11	7/15/22					
				TC-71.10	4/26/23					

TUS-516-3.70

MODEL: Sheet\_SurvFt PAPER SIZE: 34x42 (in.) DATE: 9/12/2023 TIME: 1:02:23 PM USER: tcurry1 pwc:\ohiodot-pw-bentley.com\ohiodot-pw-02\Documents\01 Active Projects\District 11\Tuscarawas\102481\400-Engineering\Roadway\Sheets\102481\_GT001.dgn

DESIGN AGENCY	
DESIGNER	TLC
REVIEWER	PAH
PROJECT ID	102481
SHEET TOTAL	P.01   12

FUNCTIONAL CLASSIFICATION LEGEND	
01	INTERSTATE
02	OTHER FREEWAYS AND EXPRESSWAYS
03	PRINCIPAL ARTERIAL ROADS (RURAL) AND STREETS (URBAN)
04	MINOR ARTERIAL ROADS (RURAL) AND STREETS (URBAN)
05	MAJOR COLLECTOR ROADS (RURAL) AND STREETS (URBAN)
06	MINOR COLLECTOR ROADS (RURAL) AND STREETS (URBAN)
07	LOCAL ROADS (RURAL) AND STREETS (URBAN)
R	RURAL
U	URBAN

DESIGN DESIGNATION						
	YEAR	STUSSR00516**C				
LOG		0.389-3.839	3.963-4.227	4.227-7.065	7.065-7.669	7.669-7.719
CURRENT ADT	2024	1,600	1,600	1,600	2,700	2,700
DESIGN ADT	2044	1,700	1,600	1,600	2,700	2,800
DHV	2044	250	250	250	400	400
DD		52%	52%	52%	53%	53
T24		13%	13%	13%	6%	10
DESIGN SPEED		55	55	55	55	55
LEGAL SPEED		55	55	55	55	55
DESIGN FUNCTION CLASSIFICATION		R06	R06	R06	R06	R06
NHS		NO	NO	NO	NO	NO

DESIGN AGENCY



DESIGNER

TLC

REVIEWER

PAH MM-DD-YY

PROJECT ID

102481

SHEET TOTAL

P.02 | 12

**UTILITIES**

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

**PROFILE AND ALIGNMENT**

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE PROFILE AND ALIGNMENT OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

**SHIELD**

THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF THE PAVEMENT OR EDGELINE. THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS SET FORTH IN C&MS 107.10 CONCERNING THE PROTECTION AND RESTORATION OF ALL PUBLIC AND PRIVATE PROPERTY IMPACTED BY CONSTRUCTION OPERATIONS.

**EXTRA FOR WIDENING (PAVEMENT AREA)**

AN ADDITIONAL QUANTITY HAS BEEN ADDED TO THE PAVEMENT DATA SHEETS TO BE USED, AS DIRECTED BY THE ENGINEER, TO COVER AREAS THAT HAVE BEEN WIDENED EITHER ON CURVES, OR ON PREVIOUS MAINTENANCE ACTIVITIES BEYOND THE AVERAGE PAVEMENT WIDTH INDICATED IN THE PLANS.

**COORDINATION OF RESURFACING AND PLANING OPERATIONS**

ONCE THE PAVEMENT PLANING OPERATIONS HAVE COMMENCED, THE CONTRACTOR SHALL PLANE CONTINUOUSLY UNTIL ALL ELEMENTS OF WORK ASSOCIATED WITH THE PAVEMENT PLANING OPERATIONS ARE CONCLUDED FOR THAT SECTION OF ROADWAY. THE PAVEMENT PLANING OPERATIONS SHALL BE COMPLETED IN A TIMELY MANNER, OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR MUST BEGIN PAVING OPERATIONS TO COVER THE PLANED SURFACE NO LATER THAN 4 (FOUR) DAYS AFTER THE START OF THE PAVEMENT PLANING FOR THAT SECTION OF ROADWAY.

IF ASPHALT CONCRETE IS TO BE APPLIED DIRECTLY ONTO PORTLAND CEMENT, CONCRETE, OR BRICK PAVEMENT, THE CONTRACTOR SHALL TACK THE EXISTING PAVEMENT WITH RUBBERIZED ASPHALT EMULSION CONFORMING TO C&MS 702.13.

ALL GRINDINGS SHALL BECOME THE PROPERTY OF THE CONTRACTOR EXCEPT FOR WHAT IS REQUIRED TO BE USED FOR SHOULDER MATERIAL INDICATED IN ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN, OR STOCKPILED AT A SPECIFIED LOCATION INDICATED IN ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN.

**EXISTING PLANS**

THE FOLLOWING EXISTING PLANS ARE AVAILABLE FOR REFERENCE AT THE DISTRICT 11 OFFICE OF THE OHIO DEPARTMENT OF TRANSPORTATION, 2201 REISER AVE. S.E., NEW PHILADELPHIA, OHIO, 44663:

PID 87305

IN ADDITION, THE EXISTING PLANS CAN BE FOUND ON THE DEPARTMENT'S WEBSITE AT THE FOLLOWING ADDRESS:  
<https://ftp.dot.state.oh.us/pub/Contracts/Attach>

**WINDOW CONTRACT TABLE**

USE THE FOLLOWING TABLE AS REFERRED TO IN THE PROPOSAL:

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE	DISINCENTIVE \$ PER DAY	WORK WINDOW	
			START	END
1.5" SURFACE COURSE S.R. 516	75 DAYS	2,500	CONTRACT EXECUTION DATE: 5/1/25	PROJECT COMPLETION DATE: 9/31/25

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)**

THIS ITEM OF WORK SHALL ADHERE TO THE REQUIREMENTS DETAILED IN CMS 251 USING ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2 FOR THE PAVEMENT LIFTS.

ODOT SHALL PERFORM A FINAL FIELD REVIEW PRIOR TO CONSTRUCTION AND AT THAT TIME, THE ENGINEER WILL DETERMINE AND MARK THE SIZE AND LOCATION FOR EACH REPAIR AND PROVIDE THAT INFORMATION TO THE CONTRACTOR. FINAL PAYMENT FOR THESE LOCATIONS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ALL PARTIAL DEPTH REPAIRS SHALL BE COMPLETED PRIOR TO COMMENCING PAVING OPERATIONS.

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 251 PARTIAL DEPTH REPAIR (441).

ESTIMATED QUANTITIES  
 ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441) 75 CU YD

**ITEM 253 - PAVEMENT REPAIR**

ODOT SHALL PERFORM A FINAL FIELD REVIEW PRIOR TO CONSTRUCTION AND AT THAT TIME, THE ENGINEER WILL DETERMINE AND MARK THE SIZE AND LOCATION FOR EACH REPAIR FOR THE CONTRACTOR. FINAL PAYMENT FOR THESE LOCATIONS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

IF NECESSARY, AN AGGREGATE DRAIN SHALL BE INSTALLED IN ACCORDANCE WITH CMS 605.07.

ALL PAVEMENT REPAIRS ARE TO BE COMPLETED PRIOR TO THE PAVING OPERATIONS.

THIS ITEM OF WORK SHALL ADHERE TO THE REQUIREMENTS DETAILED IN CMS 253 AND CONFORM TO THE DETAILS SHOWN IN THE TYPICAL SECTION ABOVE. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 253 PAVEMENT REPAIR. THE ESTIMATED QUANTITIES FROM THIS SHEET ARE TO BE USED AS DIRECTED BY THE ENGINEER.

ESTIMATED QUANTITIES  
 ITEM 253 - PAVEMENT REPAIR 175 CU YD  
 ITEM 605 - AGGREGATE DRAINS 75 FT

**RAISED PAVEMENT MARKERS FOR CONCRETE BRIDGE DECKS AND APPROACH SLABS**

OMIT PLACING RAISED PAVEMENT MARKERS ACROSS THE CONCRETE BRIDGE DECK AND APPROACH SLABS. THERE IS NO DEDUCTION FOR THIS ACTION IN THE TRAFFIC CONTROL ESTIMATED QUANTITIES UNLESS NOTED IN THE PLANS.

**SURFACE COURSE COMPLETION REQUIREMENTS**

ANY GIVEN LENGTH OF WORK ON WHICH RESURFACING OPERATIONS HAVE BEEN STARTED IN A CONSTRUCTION SEASON SHALL HAVE THE SURFACE COURSE PLACED THAT SAME SEASON.

**ITEM 646 - EPOXY PAVEMENT MARKINGS**

THE CONTRACTOR SHALL REPLACE THE EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS WITH NEW PAVEMENT MARKINGS AT THE SAME LOCATIONS AS PER C&MS 641.06. SEE STANDARD DRAWING TC-71.10 FOR PAVEMENT MARKING DETAILS.

**ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (DRIVEWAYS), AS PER PLAN**

THE PAVING OPERATION FOR INTERSECTING PUBLIC ROADS (NON-MAINLINE), AND DRIVEWAYS SHALL CONSIST OF AN AVERAGE THICKNESS EQUAL TO THE ADJACENT SURFACE COURSE FOR AN APPROXIMATE DISTANCE FROM THE EDGE OF PAVEMENT OR PAVED SHOULDERS, WHICHEVER IS APPLICABLE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER AS FOLLOWS:

NORMAL OVERLAY OR UNEQUAL MILL/FILL (I.E. 1" MILL & 2" FILL)  
 PUBLIC ROADS: 20'  
 DRIVES: 10'

EQUAL MILL/FILL (I.E. 2" MILL & 2" FILL)  
 PUBLIC ROADS: 10' WITH 10' WIDE WEARING COURSE REMOVED  
 DRIVES: OMIT

FINE GRADED POLYMER OR THINLAY (WITH MILLING)  
 PUBLIC ROADS: 10' WITH 10' WIDE WEARING COURSE REMOVED (BUTT JOINT)  
 DRIVES: OMIT

FINE GRADED POLYMER OR THINLAY (WITHOUT MILLING)  
 PUBLIC ROADS: 10' WITH 10' WIDE WEARING COURSE REMOVED (BUTT JOINT)  
 DRIVES: 5'

UP-HILL DRIVEWAYS SHALL ONLY BE PAVED TO THE BEGINNING OF THE UPSLOPE OF THE DRIVEWAY, OR AS DIRECTED BY THE ENGINEER.

ALL GRADING, TACK COAT, PRIME COAT, TOOLS, EQUIPMENT, AND INCIDENTALS REQUIRED TO LAYOUT AND PAVE THE DRIVEWAYS AND INTERSECTING PUBLIC ROADS SHALL BE INCLUDED IN THE CU. YD. PRICE BID FOR ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (DRIVEWAYS), AS PER PLAN.

THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS SET FORTH IN C&MS 107.10 CONCERNING THE PROTECTION AND RESTORATION OF ALL PUBLIC AND PRIVATE PROPERTY IMPACTED BY CONSTRUCTION OPERATIONS.

ALL DRIVEWAYS SHALL BE PAVED WITHIN (5) WORKING DAYS AFTER PLACING THE SURFACE COURSE ON THE MAINLINE PAVEMENT.

**ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN**

GRADED SHOULDERS SHALL BE RESHAPED AS PER THE REQUIREMENTS OF ITEM 617, COMPACTED AGGREGATE. COMPACTED AGGREGATE MUST BE COMPACTED FLUSH WITH ROADWAY AND TAPERED TO MEET EXISTING SHOULDER SLOPE. GRINDINGS SHALL BE USED IN LIEU OF ITEM 617, COMPACTED AGGREGATE. IF THE AMOUNT OF GRINDINGS ARE NOT SUFFICIENT TO COVER THE COMPACTED AGGREGATE QUANTITY REQUIRED FOR THIS PLAN, THEN ADDITIONAL MATERIAL MEETING SPECIFICATION 617 SHALL BE PROCURED AND USED BY THE CONTRACTOR. ALL GRINDINGS SHALL ADHERE TO THE SPECIFICATIONS STATED IN C&MS 617 AND MEET GRADATIONS CONFORMING TO C&MS 703.18. THE COST FOR STORING THE GRINDINGS ON THE PROJECT AND PLACING THE GRINDINGS SHALL ALSO BE INCLUDED IN THIS ITEM.

**ITEM 408 - PRIME COAT, AS PER PLAN**

THIS ITEM OF WORK SHALL BE PERFORMED IN ACCORDANCE WITH C&MS "ITEM 408 - PRIME COAT", EXCEPT THE CONTRACTOR SHALL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER, AS PER PLAN.

**ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS**

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT, SHORT DURATION CLOSURE OF A MULTI-LANE DIVIDED HIGHWAY AS INDICATED ON THE PLAN INSERT SHEET OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE RESTRICTION OR AT THE POINT OF ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEO'S DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE -- 40 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

DESIGN AGENCY



DESIGNER	TLC
REVIEWER	
PROJECT ID	XXX MM-DD-YY
SHEET	102481
TOTAL	
P.03	12

**ITEM 614, MAINTAINING TRAFFIC (AT ALL TIMES)**

A MINIMUM OF ONE LANE(S) OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 502 STRUCTURE FOR MAINTAINING TRAFFIC, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, ITEM 615 ROADS FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614.

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED 14 CONSECUTIVE CALENDAR DAYS, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEET P.03. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$ 20,000 PER DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

**ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)**

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

- MEMORIAL DAY
- FOURTH OF JULY
- LABOR DAY
- ALIVE FESTIVAL
- AT WOOD LAKE

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

- WEEKDAY: TIME ALL LANES MUST BE OPENED TO TRAFFIC:
- SUNDAY: 12:00N FRIDAY THROUGH 6:00 AM MONDAY
- MONDAY: 12:00N FRIDAY THROUGH 6:00 AM TUESDAY
- TUESDAY: 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
- WEDNESDAY: 12:00N TUESDAY THROUGH 6:00 AM THURSDAY
- THURSDAY: 12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
- THURSDAY: 6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY (THANKSGIVING ONLY)
- FRIDAY: 12:00N THURSDAY THROUGH 6:00 AM MONDAY
- SATURDAY: 12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRAIN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

[NEWLY CONSTRUCTED LANE ADDITIONS, ONCE COMPLETED AND INITIALLY OPENED TO TRAFFIC, SHALL BE OPEN TO TRAFFIC DURING ALL SUBSEQUENT DESIGNATED HOLIDAYS AND SPECIAL EVENTS, AND RELATED PERIODS OF TIME, SPECIFIED ABOVE.]

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

S.R. 516 \$2,500/HOUR

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**NOTIFICATION OF TRAFFIC RESTRICTIONS**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTICE TO OFFICE OF COMMUNICATIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< = 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	> = 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

**ITEM 614 - WORKZONE MARKINGS AND WORK ZONE SIGNS**

THE CONTRACTOR SHALL INSTALL ITEM 614 - WORK ZONE CENTER LINE, CLASS II OR CLASS III, 642 PAINT PRIOR TO OPENING THE LANE TO TRAFFIC, OR WHEN THE EXISTING MARKINGS HAVE BEEN COVERED OR DAMAGED, AS PER CMS 614.11.

IN THE EVENT THE CONTRACTOR CANNOT INSTALL THE WORK ZONE CENTER LINE, CLASS III, DUE TO CONDITIONS BEYOND HIS CONTROL OR WHEN CLASS II PAVEMENT MARKINGS ARE USED, AN ESTIMATED QUANTITY OF "DO NOT PASS" (R4-1) AND "PASS WITH CARE" (R4-2) SIGNS HAVE BEEN PROVIDED BELOW.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

WORK ZONE PAVEMENT MARKINGS TABLE-TUS-516						
PART	LOCATION	614				FUNDING
		WORK ZONE CENTER LINE, CLASS II, 642 PAINT	WORK ZONE CENTER LINE, CLASS III, 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS III, 8" - 642 PAINT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
		MILE	MILE	FT.	FT.	
1	S.R. 516	10.05	10.05			
2	S.R. 516	2.01	2.01	1890	165	
SUB-TOTALS PARTS 1		10.05	10.05			01/STR/05
SUB-TOTALS PARTS 2		2.01	2.01	1890	165	02/S<2/05
SUB-TOTALS PART 1 & 2		12.06	12.06	1890	165	01/STR/05, 02/S<2/05
TOTALS (CARRIED TO GENERAL SUMMARY)		12.06	12.06	1890	165	01/STR/05, 02/S<2/05

**ITEM 614 - WORK ZONE MARKINGS AND WORK ZONE SIGNS (CONT'D)**

THE CONTRACTOR SHALL ERECT "NO EDGE LINES" (W8-H12a) SIGNS IN ADVANCE OF ANY SECTIONS OF ROADWAY LACKING CMS STANDARD EDGE LINE MARKINGS, AS PER CMS 614.04.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR WORK ZONE MARKING SIGNS PER THE REQUIREMENTS ABOVE AND ITEM 614 OF THE SPECIFICATIONS.

WORK ZONE MARKING SIGN TABLE -TUS-516					
PART	LOCATION	614			FUNDING
		WORK ZONE MARKING SIGN			
		NO EDGE LINES (W8-H12a)	DO NOT PASS (R4-1)	PASS WITH CARE (R4-2)	
		EACH	EACH	EACH	
1	S.R. 516	10	8	8	01/STR/05
2	S.R. 516	2	2	2	02/S<2/05
SUB-TOTALS PARTS 1		10	8	8	01/STR/05
SUB-TOTALS PARTS 2		2	2	2	02/S<2/05
SUB-TOTALS PARTS 1 & 2		12	10	10	01/STR/05, 02/S<2/05
TOTALS (CARRIED TO GENERAL SUMMARY)		32			

**ITEM 209 - PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN**

THIS ITEM OF WORK CONSISTS OF PREPARING THE SHOULDER TO ACCEPT THE SAFETY EDGE. THE PREPARATION SHALL BE CONSISTENT IN BOTH THICKNESS AND WIDTH.

PRIOR TO PAVING THE SAFETY EDGE, THE CONTRACTOR SHALL GRADE AN AREA 10 INCHES WIDE BEGINNING AT THE EDGE OF THE PAVED ROADWAY TO PROVIDE A LEVEL SURFACE FREE FROM VEGETATION FOR CONSTRUCTION OF THE SAFETY EDGE. THE CONTRACTOR SHALL EXCAVATE THE GRADED AREA TO A DEPTH NECESSARY TO CONSTRUCT THE SAFETY EDGE, AND COMPACT THE GRADED SHOULDER ACCORDING TO C&MS 617.05, OR AS DIRECTED BY THE ENGINEER.

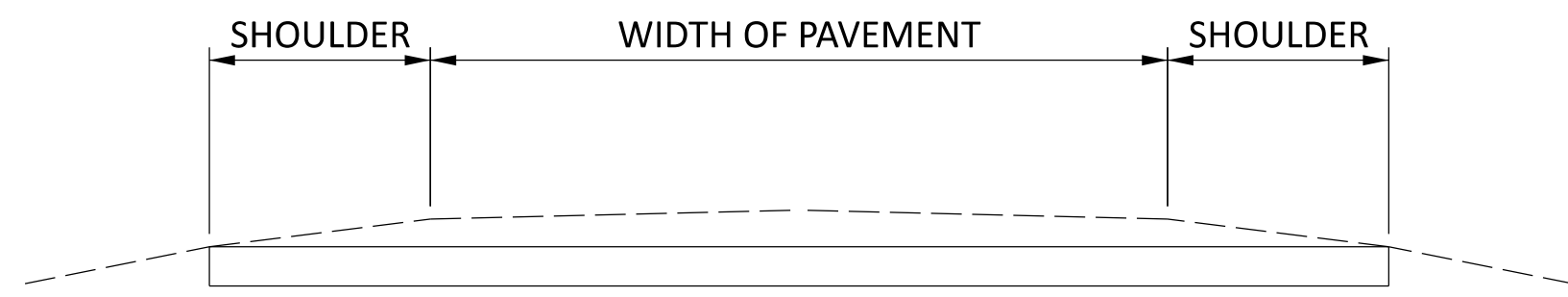
**ITEM 623 - MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN**

THIS ITEM OF WORK SHALL COMPLY WITH CMS 623 IN ADDITION TO THE FOLLOWING ITEMS. THE EXISTING MONUMENT BOXES ARE BURIED WITH AN UNKNOWN AMOUNT OF COVER AND WILL REQUIRE EXPOSING IN ORDER TO ADJUST THEM TO GRADE. THE CONTRACTOR SHALL PERFORM THIS WORK PRIOR TO PAVING AND BACK FILL ANY VOID LEFT BETWEEN THE MONUMENT BOX AND EXISTING PAVEMENT WITH ITEM 441 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (PG70-23). TWO WEEKS PRIOR TO ADJUSTING THE MONUMENT BOXES TO GRADE, THE CONTRACTOR SHALL CONTACT JON PENIX WITH THE DEPARTMENT AT 330-308-7866 TO LOCATE AND MARK THE LOCATION OF EACH BURIED MONUMENT BOX LISTED IN THE PLANS.

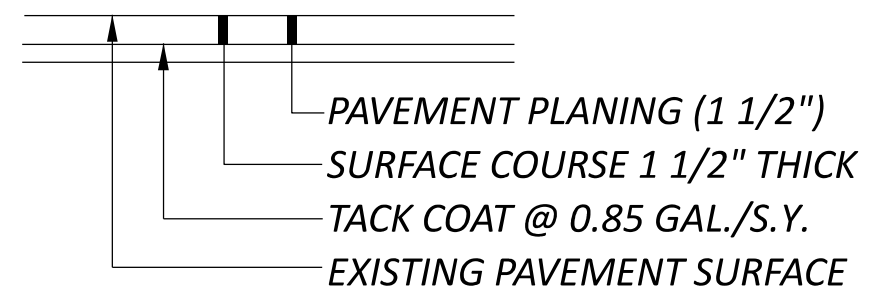
PAYMENT FOR THE ABOVE WORK SHALL BE PAID AT THE UNIT BID PRICE FOR EACH ITEM 623 MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN AND SHALL INCLUDE ALL LABOR, EQUIPMENT MATERIALS, AND INCIDENTALS REQUIRED TO COMPLETE THE WORK.







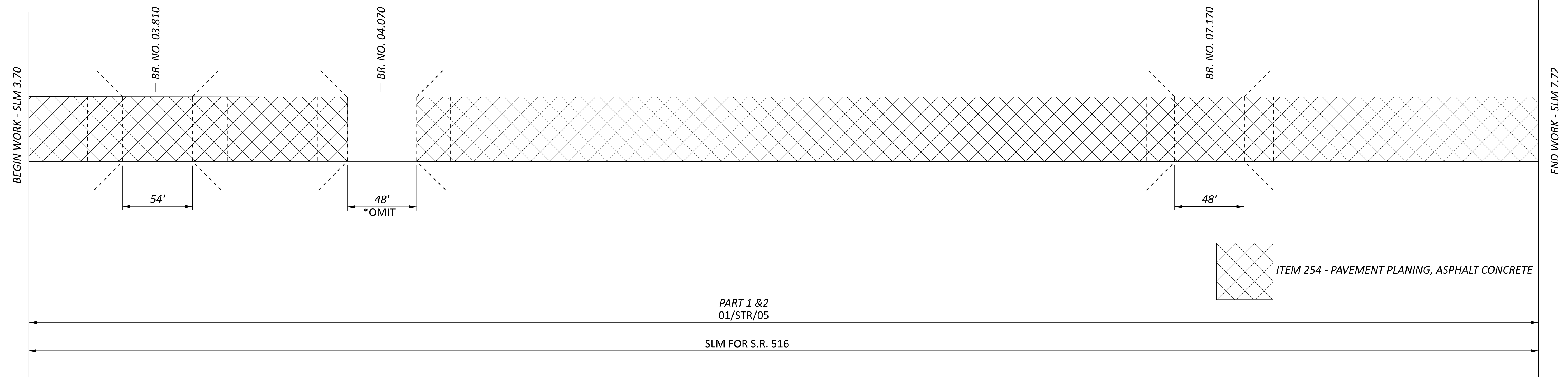
TYPICAL 1



PROPOSED WORK (TYPICAL 2)

BRIDGE TREATMENTS.

- BR. NO. TUS-516-03.810 (SFN 7906153): PROVIDE PROPOSED TREATMENT ON STRUCTURE AND APPROACH SLABS.
- BR. NO. TUS-516-04.070 (SFN 7906161): OMIT. PROVIDE PROPOSED TREATMENT ON APPROACH SLABS ONLY.
- BR. NO. TUS-516-07.170 (SFN 7906196): PROVIDE PROPOSED TREATMENT ON STRUCTURE AND APPROACH SLABS.



PAVEMENT DATA-TUS-516

PART	COUNTY	ROUTE	LOG POINT TO LOG POINT (STRAIGHT LINE MILEAGE)		MILE	DEDUCTIONS FOR BRIDGES	FEET (WITH DEDUCTIONS)	WIDTH OF PAVEMENT	NO. OF BUTT JOINTS OR FEATHER PER SCD BP-3.1	TYPICAL	EXISTING PAVEMENT TYPE	PAVEMENT AREA	407		441		FUNDING		
			FROM	TO									PAVEMENT PLANING	TACK COAT	IN.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), (PG70-22M)		IN.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (DRIVEWAYS), AS PER PLAN
													SQ. YD.	GAL.	CU. YD.	CU. YD.			
1	TUS	S.R.516	3.70	4.49	0.78	48	4,123	26		1	ASPHALT	11,911	11,911	1,012	1 1/2	496			
1	TUS	S.R.516	4.49	7.05	2.56		13517	24		1	ASPHALT	36045	36045	3064	1 1/2	1502			
EXTRA FOR DRIVES							1,600	10				1,778	1,778	151	1 1/2	74	1 1/2	74	
EXTRA FOR PUBLIC ROADS							320	20				711	711	60			1 1/2	30	
EXTRA FOR MAILBOX TURNOUTS							72 EA. x 20 SQ.YD.					1,440		122					
EXTRA FOR WIDENING							10%					4,973		423	1 1/2	207	1 1/2	207	
SUB-TOTALS: PART 1												50,445	4,832		2,279	311	01/STR/05		
2	TUS	S.R.516	7.05	7.59	0.54		2,851	24		1	ASPHALT	7,603	7,603	646	1 1/2	317			
2	TUS	S.R.516	7.59	7.67	0.08		422	47		1	ASPHALT	2,204	2,204	187	1 1/2	92			
2	TUS	S.R.516	7.67	7.72	0.05		264	94		1	ASPHALT	2,757	2,757	234	1 1/2	115			
EXTRA FOR DRIVES							100	10				111		9			1 1/2	5	
EXTRA FOR PUBLIC ROADS							110	20				244		21			1 1/2	10	
EXTRA FOR MAILBOX TURNOUTS							2 EA. x 20 SQ.YD.					40		3					
EXTRA FOR WIDENING							10%					1,256		107	1 1/2	52	1 1/2	52	
SUB-TOTALS: PART 2												12,564	1,207		576	67	02/S<2/05		
SUB-TOTALS: PARTS 1 & 2												63,009	6,039		2,855	378	01/STR/05, 02/S<2/05		

DESIGN AGENCY



DESIGNER  
TLC  
 REVIEWER  
XXX MM-DD-YY  
 PROJECT ID  
102481  
 SHEET TOTAL  
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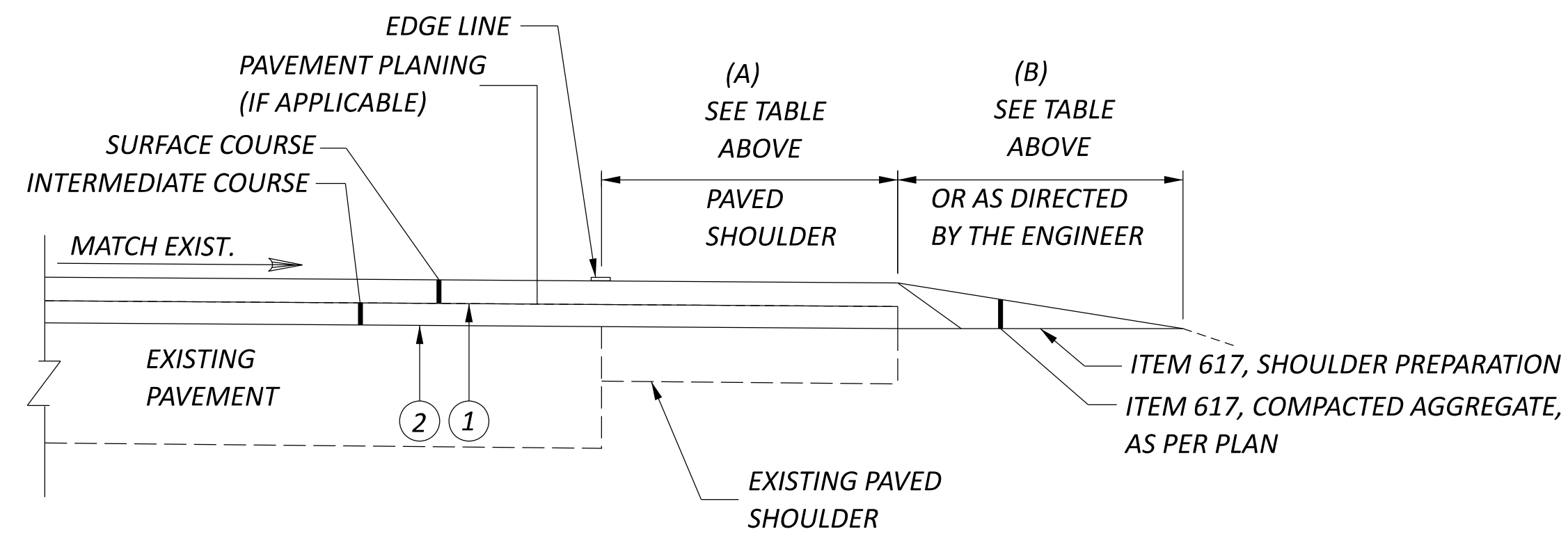
SHOULDER QUANTITIES-TUS-516

PART	COUNTY	ROUTE	LOG POINT TO LOG POINT (STRAIGHT LINE MILEAGE)		MILE	DEDUCTIONS FOR BRIDGES	FEET (WITH DEDUCTIONS)	NO. OF BUTT JOINTS OR FEATHER PER SCD BP-3.1	TYPICAL		PROPOSED WIDTH (FEET)				SHOULDER AREA	209	254	407	408	441		617		FUNDING	
			LT.	RT.					LEFT		RIGHT		MILE	SQ. YD.		GAL.	GAL.	IN.	CU. YD.	CU. YD.	SAFETY EDGE	IN.	CU. YD.		SQ. YD.
									A	B	A	B													
1	TUS	S.R. 516	3.70	7.05	3.34	48	17,640		1	2.0	2.0	2.0	2.0	7,840	6.68	7,840	666	3,136	1 1/2	327	50	1 1/2	327	7,840	
SUB-TOTALS: PART 1															6.68	7840	666	3,136		327	50		327	7,840	01/STR/05
2	TUS	S.R. 516	7.05	7.59	0.54		2,851		1	2.0	2.0	2.0	2.0	1,267	1.08	1267	108	507	1 1/2	53	8	1 1/2	53	1,267	
2	TUS	S.R. 516	7.59	7.67	0.08		422		1	2.0	2.0	2.0	2.0	188	0.16	188	16	75	1 1/2	8	1	1 1/2	8	188	
2	TUS	S.R. 516	7.67	7.72	0.05		264		1	15.0	15.0	15.0	15.0	880	0.10	880	75	352	1 1/2	37	0.74	1 1/2	37	880	
SUB-TOTALS: PART 2															1.34	2335	199	934		98	10		98	2,335	02/S<2/05
SUB-TOTALS: PART 1 & 2															8.02	10,175	865	4,070		425	60		425	10,175	01/STR/05, 02/S<2/05
TOTALS (CARRIED TO GENERAL SUMMARY)															8.02	10,175	865	4,070		485			425	10,175	

SHOULD THIS BE NOT SHOWN HERE AS I HAVE EXTENDED PAVE SHEET TO THIS LINE?

SHOULD THIS BE LINEAR GRADING? I HAD NOTES FROM SCOPE MEETING ALERTING OF THIS.

- ITEM 407
- ① TACK COAT FOR INTERMEDIATE COURSE
- ② TACK COAT



TYPICAL 1  
PAVED SHOULDER DETAIL

DESIGN AGENCY



DESIGNER  
TLC  
 REVIEWER  
XXX MM-DD-YY  
 PROJECT ID  
102481  
 SHEET TOTAL  
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EXTENDED PAVED SHOULDER QUANTITIES- TUS 516															
PART	COUNTY	ROUTE	LOG POINT TO LOG POINT (STRAIGHT LINE MILEAGE)		SIDE	TYPICAL	LENGTH (FIELD MEASUREMENTS)	PROPOSED WIDTH	PAVEMENT AREA	254	407	441		FUNDING	REMARKS
										PAVEMENT PLANING, ASPHALT CONCRETE,	TACK COAT	AVERAGE THICKNESS	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), AS PER PLAN, (PG70-22M)		
			1 1/2" SQ. YD.	0.085 SQ YD.						IN.	CU. YD.				
			FROM	TO			FT.	FT.	SQ. YD.	SQ. YD.	GAL.				
1	TUS	S.R. 516	7.41	7.71	LT.	1	1584	15	2640	2640	1	1 1/2	110		GARRET & SONS CONSTRUCTION
1	TUS	S.R. 516	7.64	7.71	RT.	1	370	15	617	617	1	1 1/2	26		DOVER RACE READY CYCLE
<b>SUB-TOTALS: PART 2</b>										<b>3257</b>	<b>2</b>		<b>136</b>		
<b>TOTALS (CARRIED TO GENERAL SUMMARY)</b>										<b>3,257</b>	<b>2</b>		<b>136</b>		<b>02/S&lt;2/05</b>

DESIGN AGENCY



DESIGNER  
TLC

REVIEWER

XXX MM-DD-YY

PROJECT ID

102481

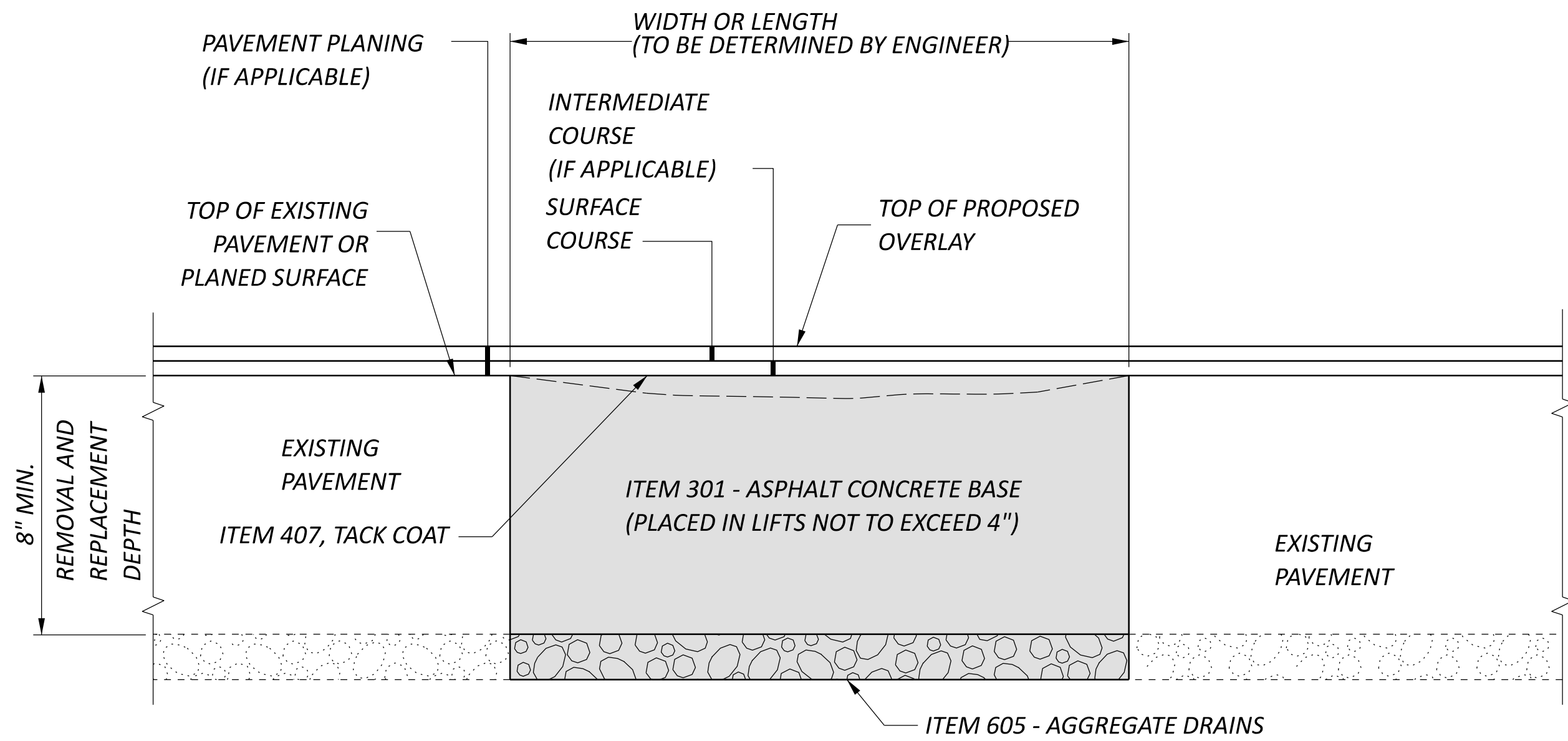
SHEET TOTAL

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TRAFFIC CONTROL SUBSUMMARY-TUS-516															
PART	SHEET NO.	COUNTY	ROUTE	LOG POINT TO LOG POINT (STRAIGHT LINE MILEAGE)		621				646					FUNDING
						RPM			RAISED PAVEMENT MARKER REMOVED	STOP LINE	LANE ARROW	CHANNELIZING LINE	EDGE LINE, 6" (WHITE)	CENTER LINE	
						SPACING	YELLOW/YELLOW	WHITE/RED							
FROM	TO	FT.	EACH	EACH	EACH	FT.	EACH	FT.	MILE	MILE					
1		TUS	S.R. 516	3.70	7.05	40	444		444				6.70	3.35	
<b>SUB-TOTALS PART 1</b>							<b>444</b>		<b>444</b>				<b>6.70</b>	<b>3.35</b>	<b>01/STR/05</b>
2		TUS	S.R. 516	7.05	7.59	40	73		73				1.08	0.54	
2		TUS	S.R. 516	7.59	7.67	40	12		12				0.16	0.08	
2		TUS	S.R. 516	7.67	7.72	40	8	9	17	55	15	635	0.10	0.05	
<b>SUB-TOTALS PART 2</b>							<b>102</b>		<b>102</b>	<b>55</b>	<b>15</b>	<b>635</b>	<b>1.34</b>	<b>0.67</b>	<b>02/S&lt;2/05</b>
<b>SUB-TOTALS PARTS 1 &amp; 2</b>							<b>546</b>		<b>546</b>	<b>55</b>	<b>15</b>	<b>635</b>	<b>8.04</b>	<b>4.02</b>	<b>01/STR/05, 02/S&lt;2/05</b>
<b>TOTALS (CARRIED TO GENERAL SUMMARY)</b>							<b>546</b>		<b>546</b>	<b>55</b>	<b>15</b>	<b>635</b>	<b>8.04</b>	<b>4.02</b>	





**PAVEMENT REPAIR TYPICAL**

**ITEM 253 - PAVEMENT REPAIR**

ODOT SHALL PERFORM A FINAL FIELD REVIEW PRIOR TO CONSTRUCTION AND AT THAT TIME, THE ENGINEER WILL DETERMINE AND MARK THE SIZE AND LOCATION FOR EACH REPAIR FOR THE CONTRACTOR. FINAL PAYMENT FOR THESE LOCATIONS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

IF NECESSARY, AN AGGREGATE DRAIN SHALL BE INSTALLED IN ACCORDANCE WITH CMS 605.07.

ALL PAVEMENT REPAIRS ARE TO BE COMPLETED AFTER PAVEMENT PLANING IF APPLICABLE AND PRIOR TO THE PAVING OPERATIONS.

THIS ITEM OF WORK SHALL ADHERE TO THE REQUIREMENTS DETAILED IN CMS 253 AND CONFORM TO THE DETAILS SHOWN IN THE TYPICAL SECTION ABOVE. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 253 PAVEMENT REPAIR. THE ESTIMATED QUANTITIES FROM THIS SHEET ARE TO BE USED AS DIRECTED BY THE ENGINEER.

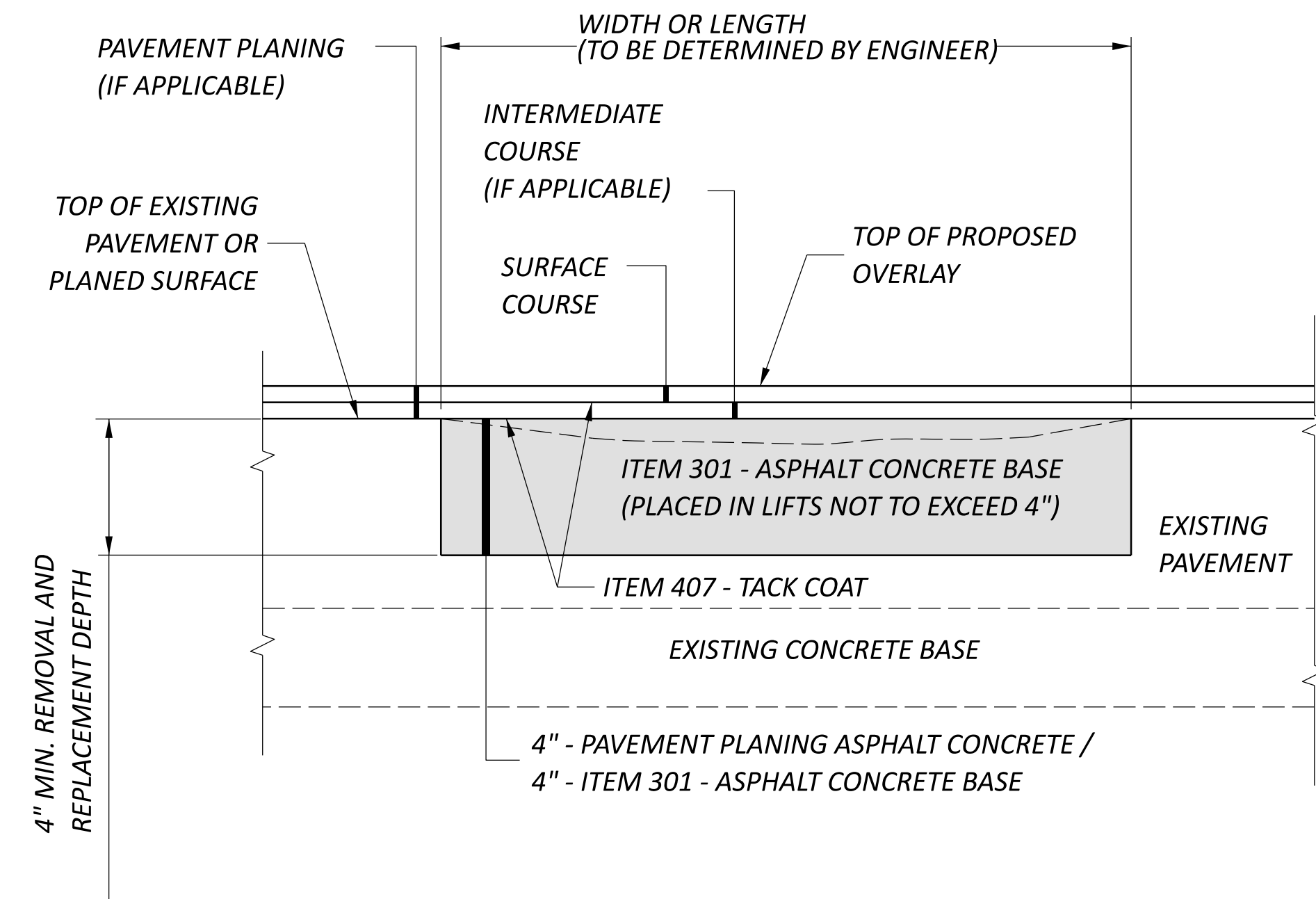
**ESTIMATED QUANTITIES**

(PART 1)  
ITEM - 253 - PAVEMENT REPAIR - 140 CU YD (01/STR/05)

(PART 2)  
ITEM - 253 - PAVEMENT REPAIR - 35 CU YD (02/S<2/05)  
TOTAL: 175 CU YD

(PART 1)  
ITEM - 605 - AGGREGATE DRAIN - 140 FT (01/STR/05)

(PART 2)  
ITEM - 605 - AGGREGATE DRAIN - 35 FT (02/S<2/05)  
TOTAL: 175 FT



**PARTIAL DEPTH PAVEMENT REPAIR TYPICAL**

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASHPALT CONCRETE BASE)**

THIS ITEM OF WORK SHALL ADHERE TO THE REQUIREMENTS DETAILED IN CMS 251 EXCEPT IN LIEU OF PLACING MULTIPLE SURFACE COURSE LIFTS FOR THE REPAIR, THE CONTRACTOR SHALL PLACE AND COMPACT ONE FOUR (4) INCH LIFT OF ITEM 301, ASPHALT CONCRETE BASE, PG64-22. ODOT SHALL PERFORM A FINAL FIELD REVIEW PRIOR TO CONSTRUCTION AND AT THAT TIME, THE ENGINEER WILL DETERMINE AND MARK THE SIZE AND LOCATION FOR EACH REPAIR FOR THE CONTRACTOR. FINAL PAYMENT FOR THESE LOCATIONS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ALL PARTIAL DEPTH REPAIRS SHALL BE COMPLETED AFTER PAVEMENT PLANING IF APPLICABLE AND PRIOR TO COMMENCING PAVING OPERATIONS.

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 251, PARTIAL DEPTH PAVEMENT REPAIR (ASHPALT CONCRETE BASE). THE ESTIMATED QUANTITIES FROM THIS SHEET ARE TO BE USED AS DIRECTED BY THE ENGINEER.

**ESTIMATED QUANTITIES**

(PART 1)  
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASHPALT CONCRETE BASE) - 60 CU YD (01/STR/05)

(PART 2)  
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASHPALT CONCRETE BASE) - 15 CU YD (02/S<2/05)  
TOTAL USE: 75 CU YD



MONUMENT BOX INVENTORY										
POINT NAME	PART	COUNTY	ROUTE	SLM	NORTHING	EASTING	LATITUDE	LONGITUDE	623	FUNDING
									MONUMENT BOX ADJUSTED TO GRADE	
									EACH	
	1	TUS	S.R. 516							
	1	TUS	S.R. 516							
	1	TUS	S.R. 516							
<b>SUB-TOTAL PART 1</b>									0	01/STR/05
	2	TUS	S.R. 516							
<b>SUB-TOTAL PART 2</b>									0	01/S<2/05
<b>TOTAL - (CARRIED TO GENERAL SUMMARY)</b>									0	

EASY ADDITION ONCE THIS DATA IS AVAILABLE



COUNTY - TUS  
 S.R. 516  
 SECTION - SLM 7.67  
 PART 2

**PART 2**  
**ADDITIONAL PAVEMENT MARKINGS (01/STR/05)**

- ① ITEM 646 - STOP LINE - 55 FT.
- ② ITEM 646 - LEFT LANE ARROW - 10 EACH
- ③ ITEM 646 - RIGHT THRU TURN ARROW - 5 EACH
- ④ ITEM 646 - CHANNELIZING LINE - 315 FT/315 FT.
- ⑤ ITEM 646 - LANE ARROW - 315 FT/315 FT

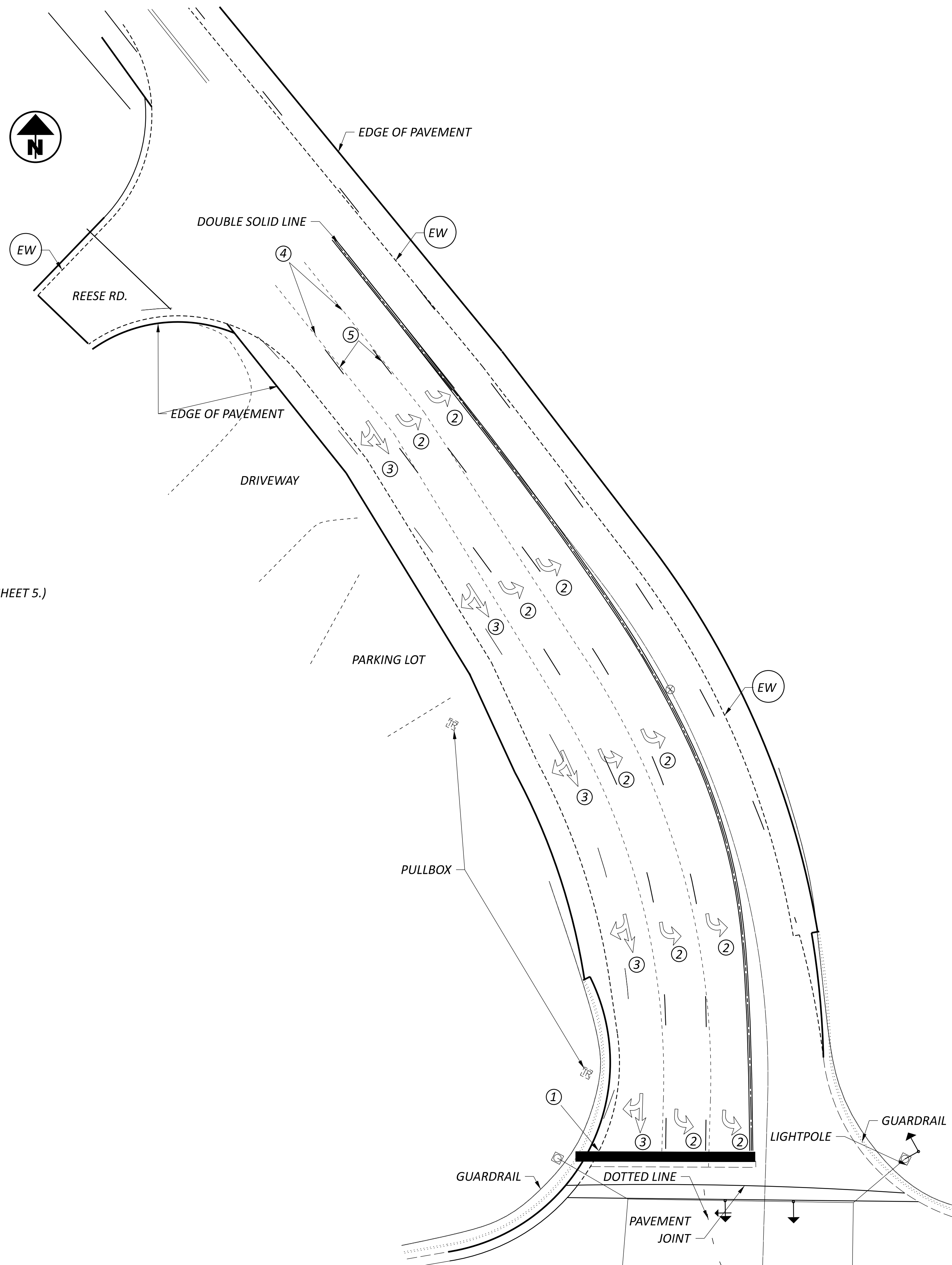
**NOTES:**

1. FOR PAVEMENT MARKING DETAILS NOT SHOWN, SEE STANDARD CONSTRUCTION DRAWING TC - 71.10.

2. ALL QUANTITIES CARRIED TO SHEET 5.

Ⓢ ITEM 646 - CENTER LINE (FOR INFO ONLY, LONG LINE AND RPM QUANTITY CARRIED TO SHEET 5.)

Ⓦ ITEM 646 - EDGE LINE (WHITE) (FOR INFO ONLY, QUANTITY CARRIED TO SHEET 5.)



TUS-516-3.70

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AUXILIARY PAVEMENT MARKING DETAIL SHEET

DESIGN AGENCY



DESIGNER  
 TLC

REVIEWER  
 PAH

PROJECT ID  
 102481

SHEET TOTAL  
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