

USE OF WEIGHTED CHANNELIZER:

THE WEIGHTED CHANNELIZER MAY BE USED IN DURING MAINLINE OVERLAY WORK ONLY. DRUMS MUST BE USED DURING WIDENING WORK. THE WEIGHTED CHANNELIZER SHALL BE PREDOMINANTLY ORANGE IN COLOR AND SHALL BE MADE OF LIGHTWEIGHT, FLEXIBLE, AND DEFORMABLE MATERIAL. THEY SHALL BE AT LEAST 42 INCHES IN HEIGHT WITH A WEIGHTED BASE. THEY MAY HAVE A HANDLE OR LIFTING DEVICE WHICH EXTENDS ABOVE THE 42" MINIMUM HEIGHT.

THE MARKINGS ON THE WEIGHTED CHANNELIZER SHALL BE HORIZONTAL, CIRCUMFERENTIAL, ALTERNATING ORANGE AND WHITE RETROREFLECTIVE STRIPES 6 INCHES WIDE. EACH WEIGHTED CHANNELIZER SHALL HAVE A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. ANY NON-RETROREFLECTIVE SPACES BETWEEN THE HORIZONTAL ORANGE AND WHITE STRIPES SHALL NOT EXCEED 2 INCHES WIDE. THE WEIGHTED CHANNELIZER SHALL HAVE A 4-INCH MINIMUM WIDTH, REGARDLESS OF ORIENTATION.

USE OF WEIGHTED CHANNELIZERS ON FREEWAYS AND MULTILANE HIGHWAYS SHALL BE LIMITED TO SHORT-TERM OPERATION FOR EITHER DAY OR NIGHT. UPON COMPLETION OF WORK, THE WEIGHTED CHANNELIZERS SHALL BE REMOVED. THE WEIGHTED CHANNELIZERS MAY AGAIN BE PLACED ON THE HIGHWAY WHEN THE WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT. ANY LANE CLOSURE USING CHANNELIZATION DEVICES, EXPECTED TO REMAIN FOR MORE THAN TWELVE HOURS, SHALL REQUIRE THE USE OF DRUMS OR BARRIERS.

WHEN USED AT NIGHT, WEIGHTED CHANNELIZERS SHALL ONLY BE PLACED IN THE TANGENT AREA. THE TANGENT AREA IS DEFINED AS THE AREA AFTER THE TRANSITION TAPER WHERE THE WORK TAKES PLACE. DRUMS SHALL BE USED IN THE TRANSITION TAPERS FOR NIGHT OPERATIONS. MAXIMUM SPACING OF THE WEIGHTED CHANNELIZER SHALL BE 40 FEET AT NIGHT.

STEPS SHOULD BE TAKEN TO ENSURE THAT THE WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND OR MOVING TRAFFIC. BALLASTS SHOULD NOT PRESENT A HAZARD IF THE WEIGHTED CHANNELIZERS ARE INADVERTENTLY STRUCK, NOR SHOULD THEY AFFECT THE VISIBILITY OF THE WEIGHTED CHANNELIZERS. ALL BALLASTS USED SHOULD BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

FLOODLIGHTING:

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK.

IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

DROPOFFS IN WORK ZONE:

THE DROPOFF ADJACENT TO THE TRAVELED LANE SHALL MEET THE CRITERIA OUTLINED IN STANDARD DRAWING MT-101.90. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR MATERIALS, LABOR OR EQUIPMENT NECESSARY TO MEET THE REQUIREMENTS OF MT-101.90.

LANE VALUE CONTRACT:

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE UNAUTHORIZED LANE USE TABLE FOR EACH UNIT OF TIME A LANE/SHOULDER/RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE UNAUTHORIZED LANE USE TABLE.

BELOW IS A SUMMARY OF MOT REQUIREMENTS FOR THIS PROJECT:

* AT LEAST ONE LANE IN EACH DIRECTION OF UNI-33 TRAFFIC SHALL BE MAINTAINED AS PER STD DWG MT-95.30. TYPICAL APPLICATION 35, MOBILE OPERATION ON A MULTI-LANE ROAD OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES SHALL ALSO APPLY. LANE AND CLOSURES SHALL OCCUR WHEN AUTHORIZED BY THE UNAUTHORIZED LANE TABLE BELOW.

LANE VALUE CONTRACT TABLE						
UNI-33						
Section (SLM)	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Fri	Sat	Sun	
Northwest Parkway (8.79) to US42 (20.37)	2	2 to 1	6AM-8AM & 3PM-6PM	No Restriction	No Restriction	\$200
US42 (20.37) to SR 161 (25.05)	2	2 to 1	5AM-10AM & 1PM-7PM	No Restriction	No Restriction	\$245
Single lane closure hours shall also apply to short term shoulder closures.						

SEE THE RAMP CLOSURE RESTRICTION CHART ON SHEET 15.

MAINTENANCE OF TRAFFIC FOR MARKING PAVEMENT REPAIRS:

PROVIDE LANE CLOSURES AS PER THE MAINTENANCE OF TRAFFIC NOTES IN THESE PLANS A MINIMUM OF 24 HOURS PRIOR TO PERFORMING PAVEMENT REPAIRS TO ALLOW THE ENGINEER TO IDENTIFY AND MARK THE AREAS OF THE PAVEMENT IN NEED OF REPAIRS.

PAYMENT FOR ALL LABOR, EQUIPMENT, LAW ENFORCEMENT OFFICERS AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

COORDINATION WITH ADJACENT PROJECTS:

THE CONTRACTOR SHALL COORDINATE WORK WITH ODOT AND THE CONTRACTORS ON THE ADJACENT PROJECTS. THE ADJACENT PROJECT IS PID NO. 80748 UNI-33-24.87 INTERCHANGE IMPROVEMENT PROJECT.

COORDINATION SHALL BE MADE TO PREVENT CONFLICTING ADVANCE WARNING SIGNS, CONFLICTING DETOUR ROUTES, OVERLAPPING/CONFLICTING LANE CLOSURES, AND TO ENSURE THAT A MINIMUM DISTANCE OF 2 MILES BETWEEN ADJACENT LANE CLOSURES IS MAINTAINED. THIS IS NOT AN EXHAUSTIVE LIST OF COORDINATION ITEMS THAT MAY NEED TO BE RESOLVED BETWEEN PROJECTS. THE DEPARTMENT RESERVES THE RIGHT TO DECIDE WHICH PROJECT'S ACTIVITIES TAKE PRECEDENCE. PROJECTS THAT HAVE ACTIVITIES DELAYED DUE TO CONFLICTS WILL CONSIDER THIS AN EXCUSABLE, NON-COMPENSABLE DELAY PER 108.06.B. ON PROJECTS THAT HAVE ACTIVITIES DELAYED DUE TO CONFLICTS WHERE THE CONTRACTOR FAILED TO MEET THE NOTIFICATION REQUIREMENTS, THE DELAYS SHALL NOT BE CONSIDERED EXCUSABLE OR COMPENSABLE.

ATTENDANCE AT DEPARTMENT ORDERED TRAFFIC COORDINATION MEETINGS BETWEEN ADJACENT PROJECTS SHALL BE CONSIDERED MANDATORY FOR EACH PROJECT'S SUPERINTENDENT AND INCIDENTAL TO THE LUMP SUM MAINTENANCE OF TRAFFIC PAYMENT ITEM.

ITEM 614, MAINTAINING TRAFFIC (LANE CLOSURE/REDUCTION REQUIRED):

A MINIMUM OF 1 LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, AND THE COMPLETED PAVEMENT. WORK ZONES SHALL BE LIMITED IN LENGTH TO THE AMOUNT OF WORK THAT CAN BE PERFORMED THAT DAY. WORK ZONES SHALL BE IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, APPLICABLE STANDARD DRAWINGS, AND PROCEDURES DETAILED IN THIS PLAN. CARE SHALL BE TAKEN TO FOLLOW MT-101.90 AND CONTRACTOR SHALL MAINTAIN W8-9 "LOW SHOULDER" SIGNS DURING PAVEMENT REPAIRS. USE OF W8-9 SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM 614 - MAINTAINING TRAFFIC.

ITEM 614 - WORK ZONE MARKING SIGN (NO EDGE LINES):

FURNISH, INSTALL, MAINTAIN, AND SUBSEQUENTLY REMOVE WORK ZONE MARKING SIGNS AND THEIR SUPPORTS WITHIN THE WORK LIMITS ACCORDING TO THE REQUIREMENTS OF CMS 614.04.

COUNTY	ROUTE	R4-1-18
		NO EDGE LINES EACH
UNI	33	28
TOTAL		28

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - WORK ZONE MARKING SIGN = 28 EACH

ITEM 614 - WORK ZONE LANE LINE, CLASS III, 642 PAINT:

WORK ZONE LANE LINE SHALL BE PLACED TO REFLECT THE PROPOSED LANE LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING.

COUNTY		MILE	APP	QUAN	UNIT
UNI	US 33	22.57	1	22.57	MILE
UNI	US 33	1.36	2	2.72	MILE
TOTAL				25.29	MILE

IN ADDITION, 0.52 x 3 (1.56 TOTAL) MILES OF WORK ZONE LANE LINE HAS BEEN PROVIDED TO RETURN STRIPING TO ORIGINAL IN BETWEEN PHASES OF THE STRUCTURE WORK (SEE SHEETS 16-30)

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - WORK ZONE LANE LINE, CLASS III, 642 PAINT
25.29 MILE + 1.56 MILE = 26.85 MILE

ITEM 614 - WORK ZONE EDGE LINE, CLASS III, 642 PAINT:

WORK ZONE EDGE LINE SHALL BE PLACED TO REFLECT THE PROPOSED EDGE LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

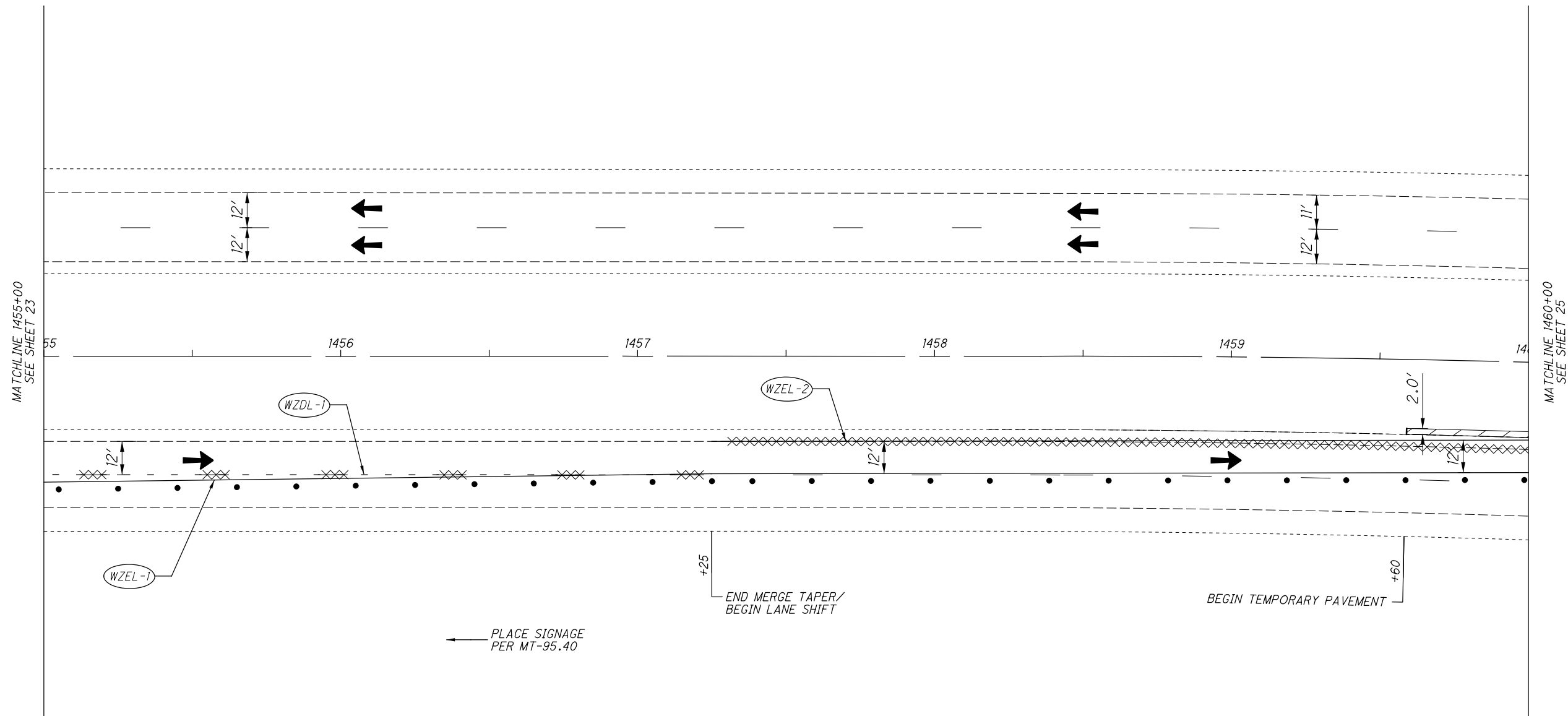
ITEM 614 - WORK ZONE EDGE LINE, CLASS III, 642 PAINT
= 51.99 MILE

SEE SHEET	SLM	REFERENCE NUMBER	PHASE	LOCATION		614	614	614	614	614	614	615	622	642		
				FROM	TO	WORKZONE IMPACT ATTENUATOR	BARRIER REFLECTOR, TYPE 1, ONE WAY	OBJECT MARKER, ONE WAY	WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT	WORK ZONE EDGE LINE, CLASS III, 642 PAINT	WORK ZONE DOTTED LINE, CLASS III, 642 PAINT	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	PORTABLE BARRIER	REMOVAL OF PAVEMENT MARKING		
						EACH	EACH	EACH	MILE	MILE	FT	SY	FT	FT		
16-19	UNI 33 EASTBOUND	WZEL-1	PHASE 1	1459+00.00	1469+86.00					0.21						
		WZEL-2		1459+00.00	1469+86.00					0.21						
		WZCH-1		1459+00.00	1469+86.00				1086							
		R-1		1459+00.00	1469+86.00										1086	
		R-2		1459+00.00	1469+86.00										1086	
		PB-1		1467+10.00	1468+46.00	1	9	3							136	
16-19	UNI 33 WESTBOUND	WZEL-3	PHASE 1	1463+40.00	1479+86.00					0.31						
		WZEL-4		1463+40.00	1479+86.00					0.31						
		WZCH-2		1463+40.00	1479+86.00				1646							
		R-3		1463+40.00	1479+86.00										1646	
		R-4		1463+40.00	1479+86.00										1646	
		PB-2		1467+60.00	1468+96.00	1	9	3							136	
20-28	UNI 33 EASTBOUND	WZEL-1	PHASE 2	1448+15.00	1468+46.00					0.38						
		WZEL-2		1457+25.00	1471+61.00					0.27						
		WZDL-1		1448+15.00	1457+25.00							910				
		R-1		1448+15.00	1457+25.00										910	
		R-2		1457+25.00	1471+61.00										1436	
		PB-1		1464+50.00	1468+46.00	1	26	10							396	
		TEMP PAVE		1459+60.00	1470+56.00								231			
20-28	UNI 33 WESTBOUND	WZEL-3	PHASE 2	1463+40.00	1479+86.00					0.31						
		WZEL-4		1467+60.00	1488+26.00					0.39						
		WZDL-2		1479+86.00	1488+26.00							840				
		R-3		1463+40.00	1479+86.00										1646	
		R-4		1479+86.00	1488+26.00										840	
		PB-2		1467+60.00	1470+56.00	1	20	7							296	
29-38	UNI 33 EASTBOUND	WZEL-1	PHASE 3	1447+80.00	1468+46.00					0.39						
		WZEL-2		1456+20.00	1472+66.00					0.31						
		WZDL-1		1447+80.00	1456+20.00							840				
		R-1		1447+80.00	1456+20.00										840	
		R-2		1456+20.00	1472+66.00										1646	
		PB-1		1463+90.00	1468+46.00	1	30	11							456	
29-38	UNI 33 WESTBOUND	WZEL-3	PHASE 3	1464+45.00	1478+81.00					0.27						
		WZEL-4		1467+60.00	1487+91.00					0.38						
		WZDL-2		1478+81.00	1487+91.00							910				
		R-3		1464+45.00	1478+81.00										1436	
		R-4		1478+81.00	1487+91.00										910	
		PB-2		1467+60.00	1472+06.00	1	30	11							446	
		TEMP PAVE		1465+85.00	1477+42.00								254			
TOTALS CARRIED TO GENERAL SUMMARY						6	124	45	2732	3.75	3500	485	1866	15128		

MAINTENANCE OF TRAFFIC SUBSUMMARY

UNI - 33 - 12.59

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NOTE:
 1. SEE STRUCTURE SHEETS 96-98 FOR SECTION VIEWS.
 2. DESIGN SPEED 70MPH.
 3. QUANTITIES SHOWN ON SHEET 16
 4. ALL CONFLICTING RAISED PAVEMENT
 MARKER REFLECTORS SHALL BE REMOVED.
 5. FOR ALL DETAILS NOT SHOW, SEE SCD 95.40

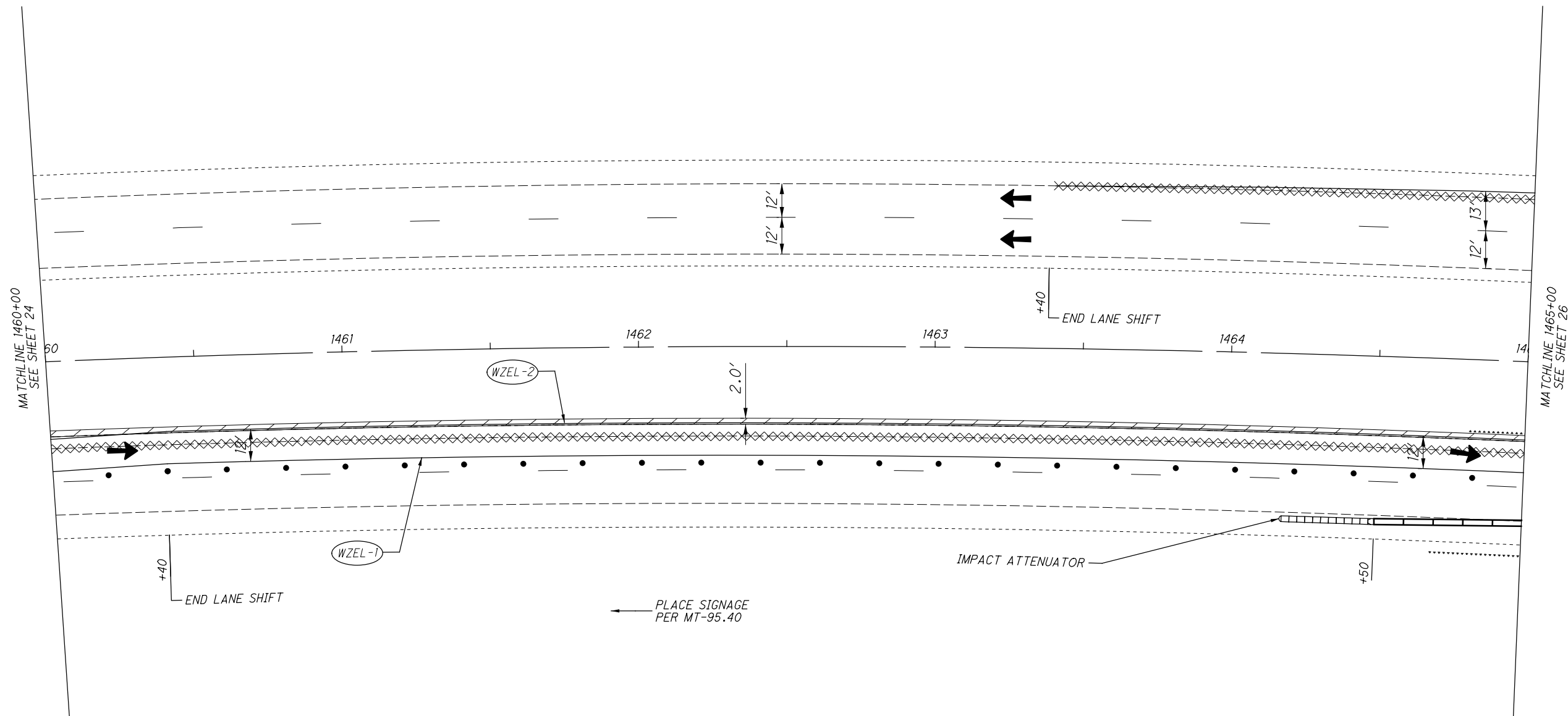
LEGEND	
	= WORK ZONE
	= TEMPORARY PAVEMENT
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

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**MAINTENANCE OF TRAFFIC PLAN
PHASE 2**

UNI-33-12.59



- NOTE:**
1. SEE STRUCTURE SHEETS 96-98 FOR SECTION VIEWS.
 2. DESIGN SPEED 70MPH.
 3. QUANTITIES SHOWN ON SHEET 16
 4. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.
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LEGEND	
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	= TEMPORARY PAVEMENT
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	= PAVEMENT MARKING REMOVED

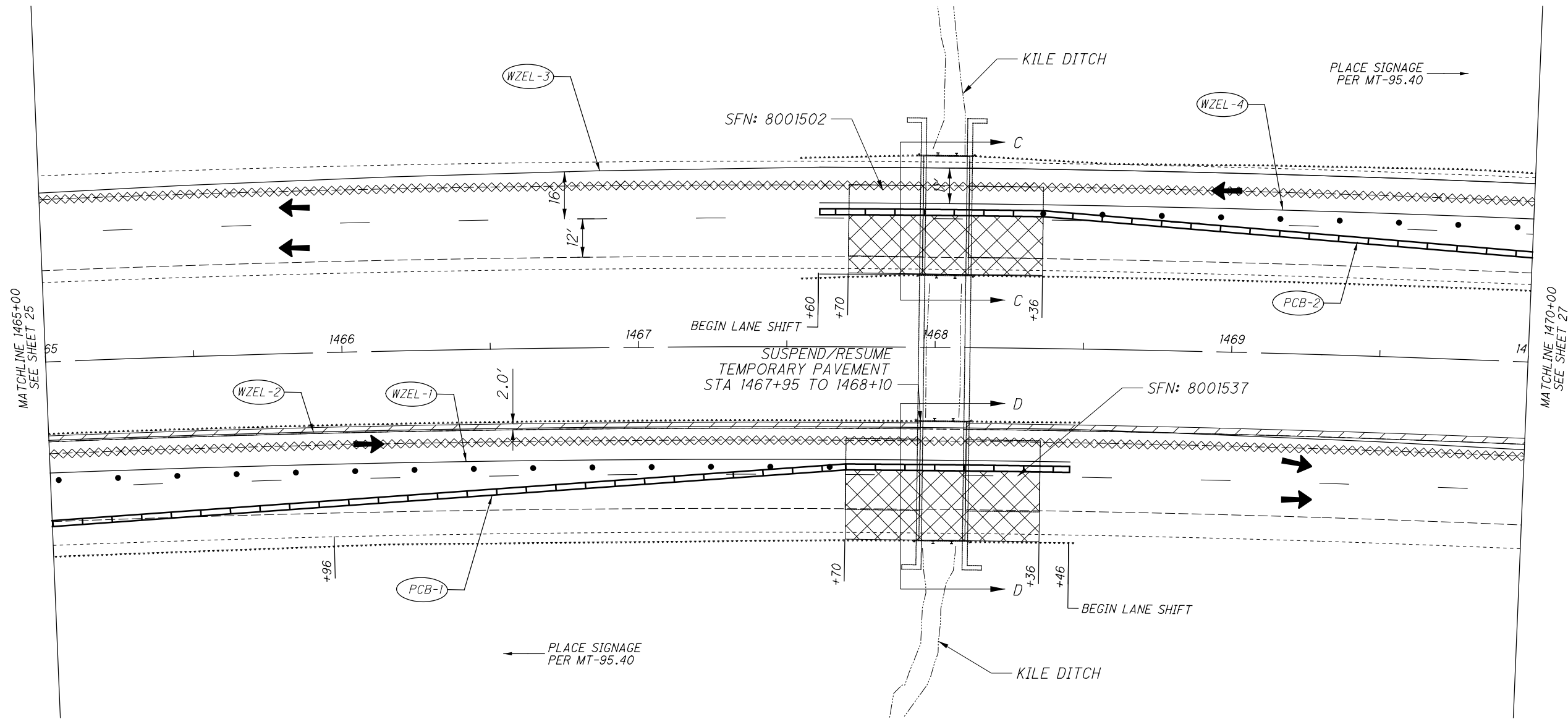


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**MAINTENANCE OF TRAFFIC PLAN
PHASE 2**

UNI-33-12.59

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TRAFFIC DIRECTION

FOR SECTION C-C & D-D, SEE SHEET 96

- NOTE:
1. SEE STRUCTURE SHEETS 96-98 FOR SECTION VIEWS.
 2. DESIGN SPEED 70MPH.
 3. QUANTITIES SHOWN ON SHEET 16
 4. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.
 5. FOR ALL DETAILS NOT SHOW, SEE SCD 95.40

LEGEND	
	= WORK ZONE
	= TEMPORARY PAVEMENT
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	= PAVEMENT MARKING REMOVED

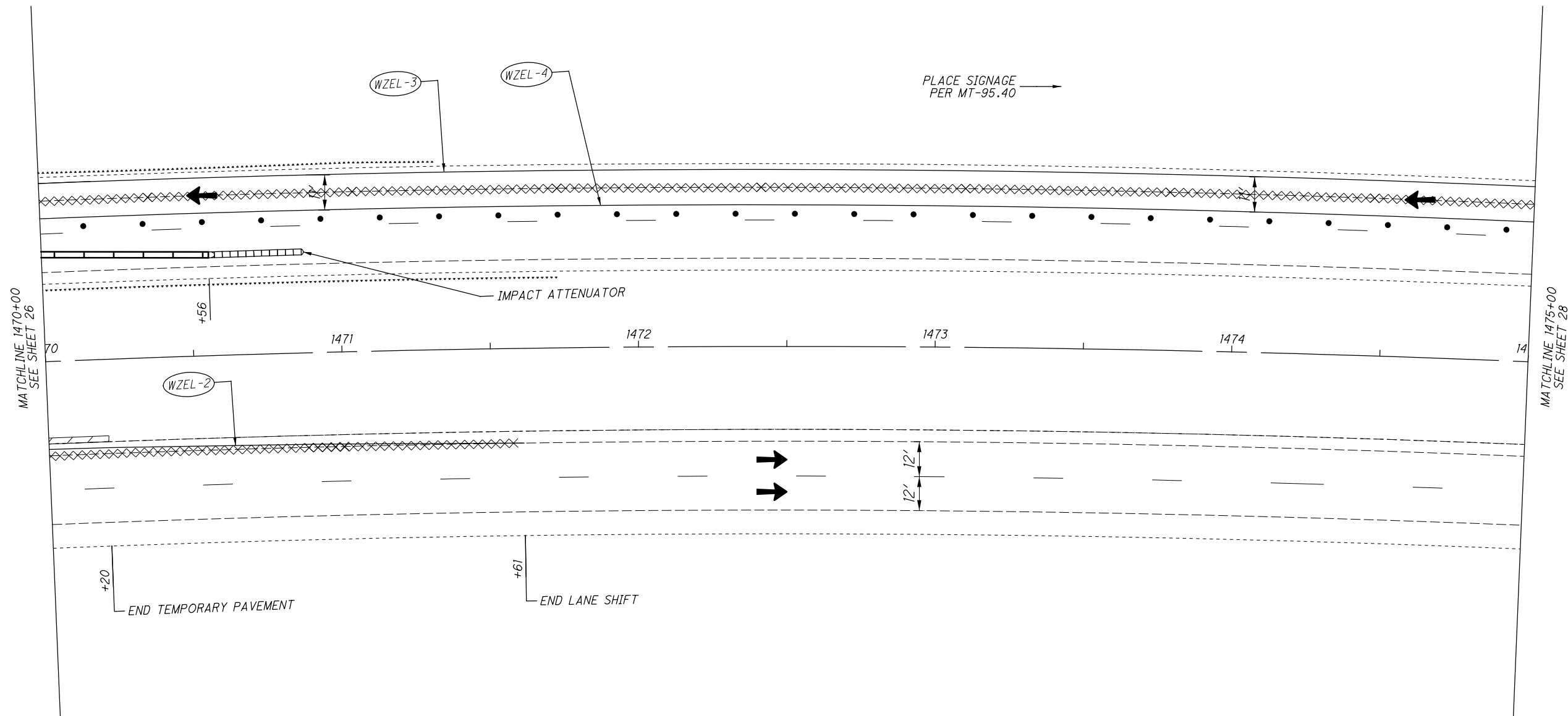


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**MAINTENANCE OF TRAFFIC PLAN
PHASE 2**

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 TRAFFIC DIRECTION

NOTE:
 1. SEE STRUCTURE SHEETS 96-98 FOR SECTION VIEWS.
 2. DESIGN SPEED 70MPH.
 3. QUANTITIES SHOWN ON SHEET 16
 4. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.
 5. FOR ALL DETAILS NOT SHOWN, SEE SCD 95.40

LEGEND	
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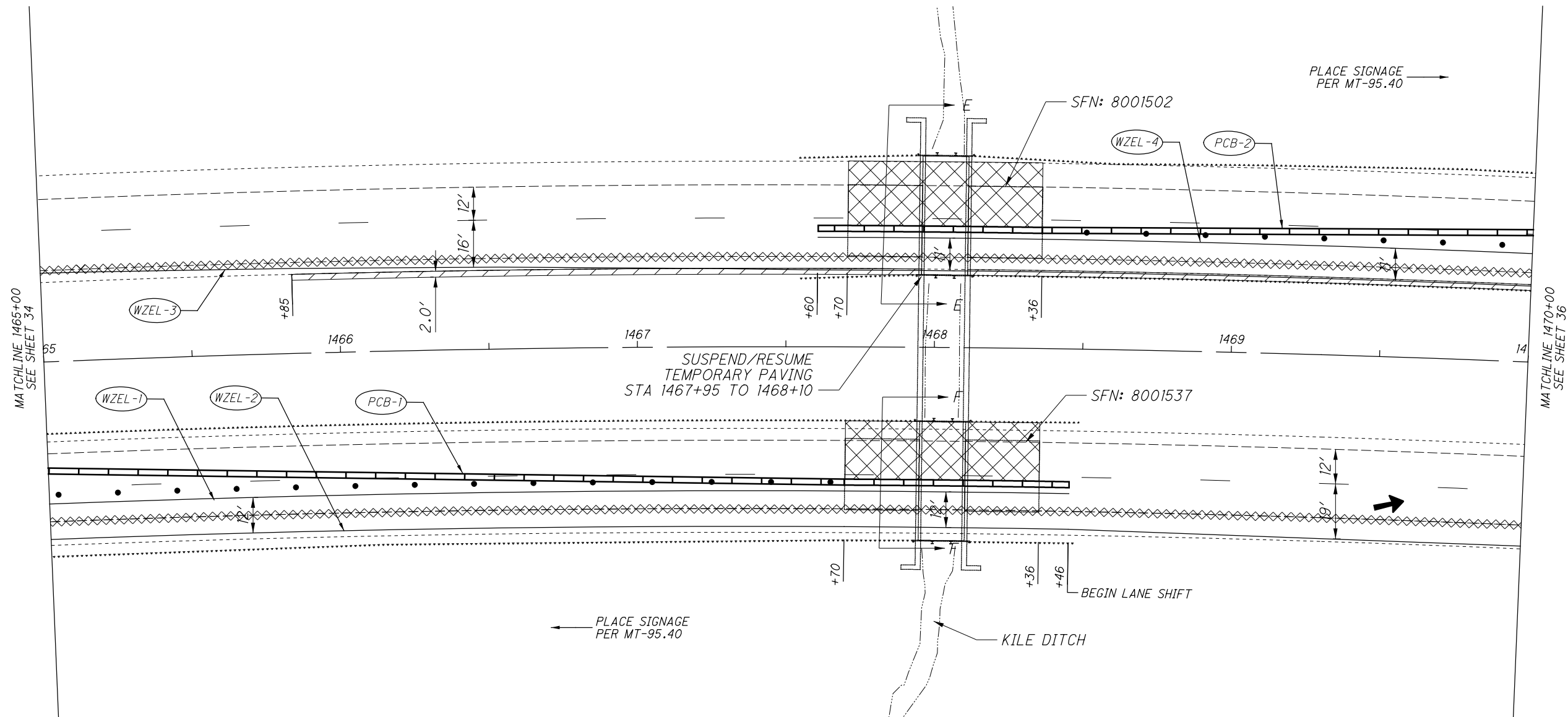


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**MAINTENANCE OF TRAFFIC PLAN
 PHASE 2**

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PLACE SIGNAGE PER MT-95.40 →

← PLACE SIGNAGE PER MT-95.40

SUSPEND/RESUME TEMPORARY PAVING STA 1467+95 TO 1468+10

BEGIN LANE SHIFT

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FOR SECTION E-E & F-F, SEE SHEET 97

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TRAFFIC DIRECTION

- NOTE:
1. SEE STRUCTURE SHEETS 96-98 FOR SECTION VIEWS.
 2. DESIGN SPEED 70MPH.
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LEGEND	
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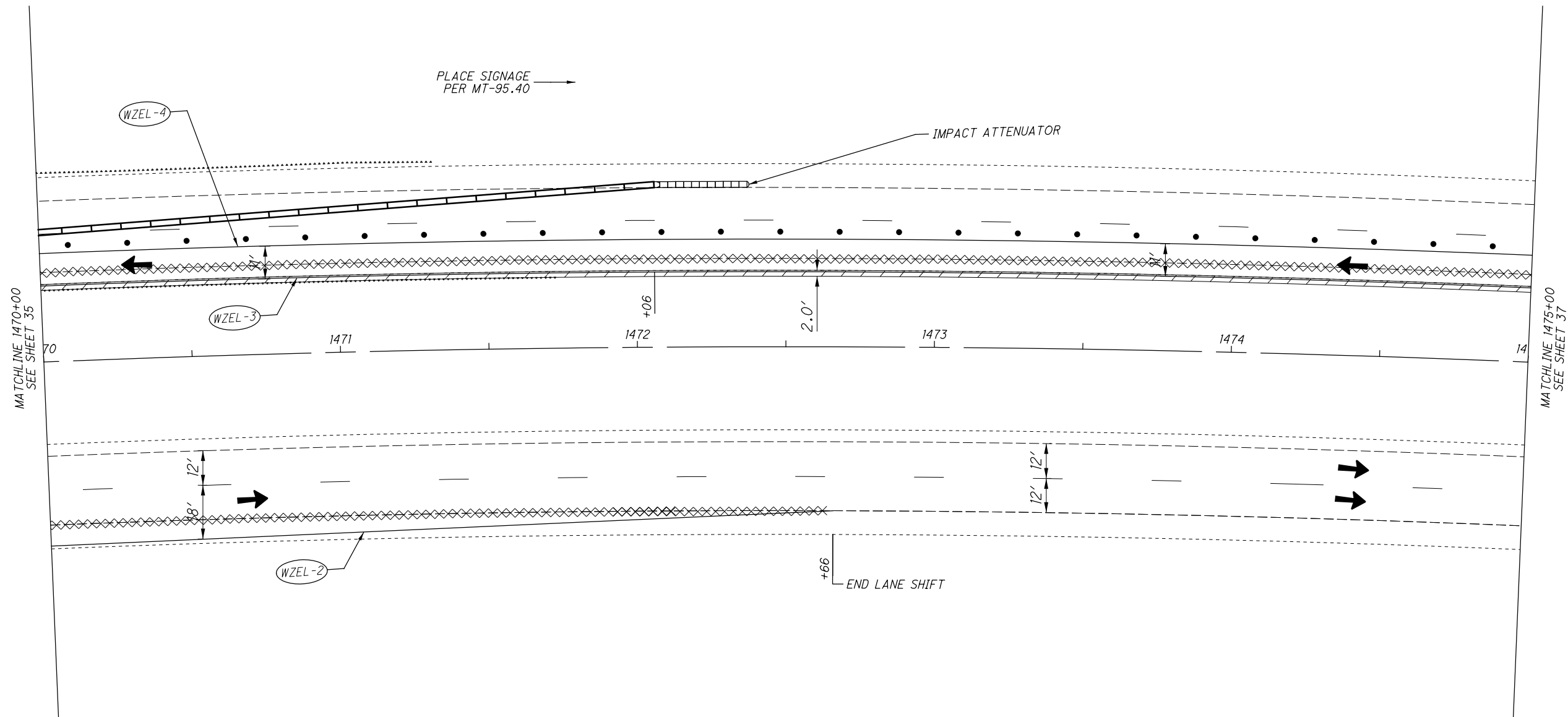
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SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN
PHASE 3

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TRAFFIC DIRECTION

- NOTE:
1. SEE STRUCTURE SHEETS 96-98 FOR SECTION VIEWS.
 2. DESIGN SPEED 70MPH.
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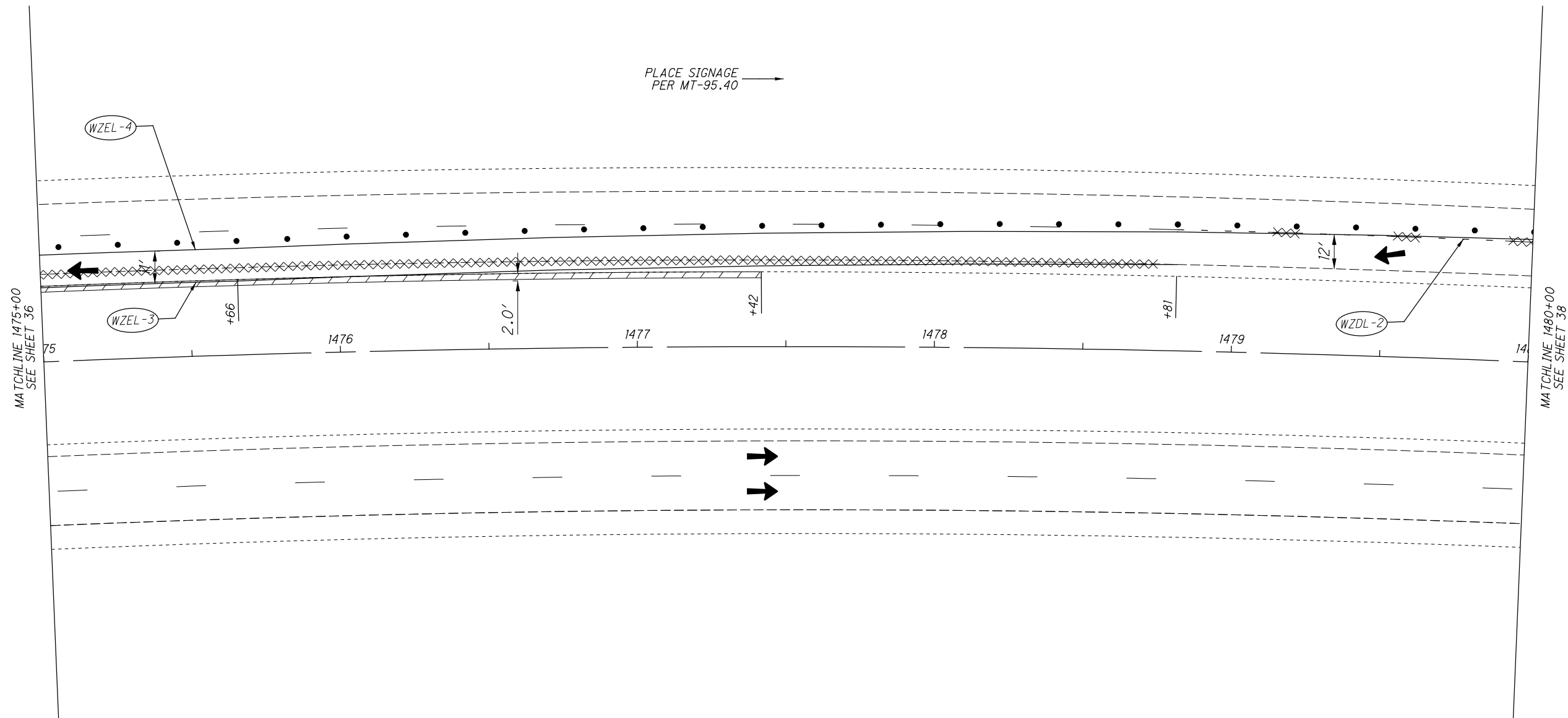
LEGEND	
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	= TEMPORARY PAVEMENT
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

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SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN
PHASE 3**

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 TRAFFIC DIRECTION

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CALCULATED
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 SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN
 PHASE 3**

UNI-33-12.59

