

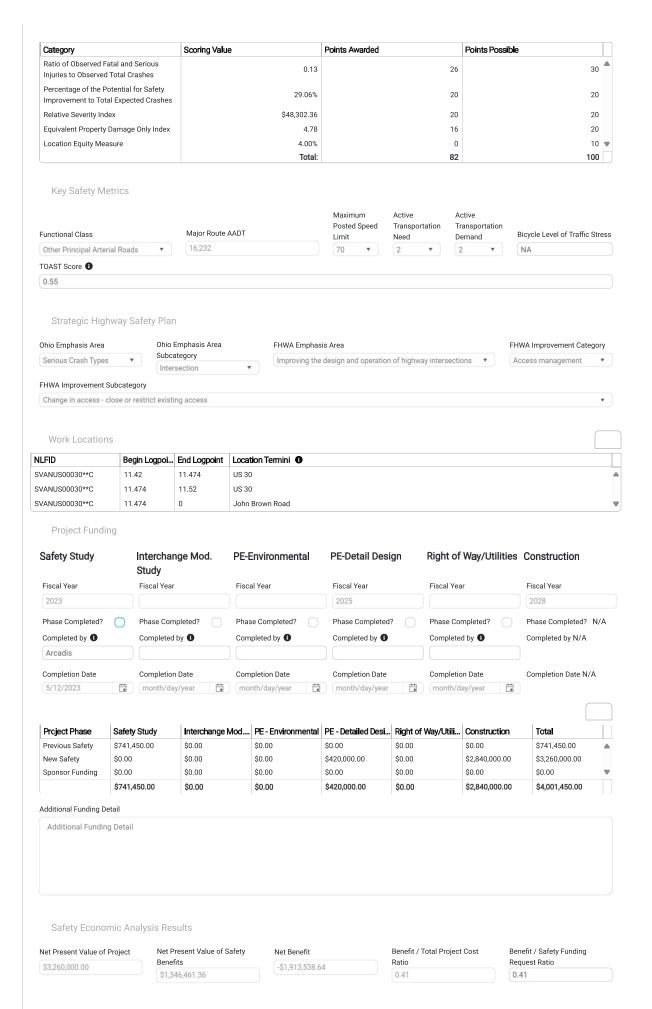
Review Application

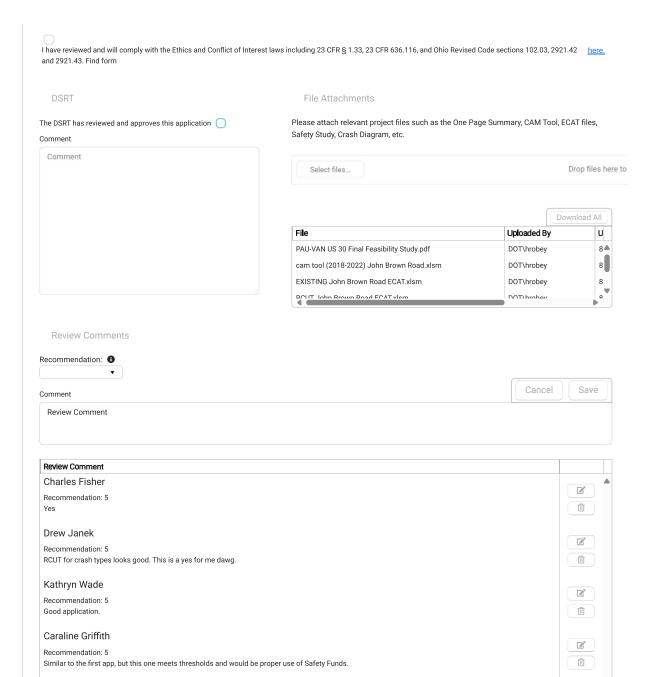
Search Project

Application Type:	Year:	Application Round	l:				
Formal	2023 😵	× 202308 8	× Search	1			
F202308D01-01							
F202308D01-02			Formal A	pplication	ì		
F202308D01-03	F202308D01-02						
F202308D01-04	Application Instructions ODOT's Safety Analysis Guidelines						
F202308D02-01	Application instructions ODO1's Safety Alialysis Guideline						
F202308D02-02	General Project Info	ormation					
F202308D02-03	Application Round	District Project S Application ODOT		ject Name AN-US 30 & John Bro	PID Applicant Na 120666 Hailey Rob	Bhana	
F202308D02-04		2			120000	(419)	
F202308D03-01	Contact Email						
F202308D03-02	Hailey.Robey@dot.ohio.go	V					
F202308D03-03	Location Information	on					
F202308D04-01	ODOT County	Route	Begin Logpoint	Begin Latitude	Begin Longitude		
F202308D04-02	District VAN WERT	Number	11.42	40.89119	84.61154	Google Map	
F202308D05-01	Road Name	US-30	Ford Laura int	End Latitude 🚯	Ford Languistada 🙆		
F202308D05-02	John Brown Road		End Logpoint 11.52	40.89071	End Longitude 1 84.60955	Google Map	
F202308D05-03							
F202308D05-04	Project Description						
F202308D05-05	Summary of Crash Patterns		Summary of Recommended Countermeasures Project		Project Priority Informat	ion	
F202308D06-01	There were 17 crashes at 2018-2022. Of these 17, rear end, 2 were fixed obj	8 were angle, 3 were	location and conducting a feasibility study, it is recommended to close the at-grade intersection This s		plan to close all of the	is project is part of the District's long range in to close all of the at-grades along US 30. is specific location has a history of high verity crashes and is a priority to close.	
F202308D06-02	was left turn, and 1 was angle crashes resulted in	sideswipe. One of the					
F202308D06-03	rate at this intersection is		di Noot.				
F202308D06-04							
F202308D07-01	Crash Data						
F202308D07-02	Crash Totals (average per year) Fatal & Serious Injury (KA) Visible Injury (B) Non-Visible (C) Property Damage Only (O) Total						
F202308D07-03	Existing Conditions: Predicted Crash Frequency	0.0988	Visible Injury (B) 0.1986	Non-Visible (C) 0.1189	Property Damage Only (0)	1.44 1	
F202308D08-01	Existing Conditions:	0.1268	0.2623	0.1545	1.4860	2.03	
F202308D08-02	Expected Crash Frequency Potential for Safety	0.0280	0.0637	0.0356	0.4648	0.59	
F202308D08-03	Improvement Proposed Conditions:						
F202308D09-01	Expected Crash Frequency Observed Crashes	0.0174	0.0326	0.0209	0.4277 2.0000	0.50 3.20 ▼	
F202308D09-02	Observed People I	1	1.3000	1.200	1.3000	1.23 ()	
F202308D09-03	Observed People I	Fatal Injury (K)	Serious Injury (A)	Visible Injury (B)	Non-Visible (C)	Total	
F202308D12-01	Observed People Injury Totals	0.2000	0.2000	0.6000	0.6000	1.60	
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Application Scoring

F202308D12-02





Brenton Bogard