



# Review Application

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## Formal Application

F202308D01-02

[Application Instructions](#)

[ODOT's Safety Analysis Guidelines](#)

### General Project Information

Application Round:  District Application #:  Project Sponsoring Agency:  Project Name:  PID:  Applicant Name:  Contact Phone: 

Contact Email:

### Location Information

ODOT District:  County:  Route Number:  Begin Logpoint:  Begin Latitude:  Begin Longitude:  [Google Map](#)

Road Name:  End Logpoint:  End Latitude:  End Longitude:  [Google Map](#)

### Project Description

#### Summary of Crash Patterns

There were 17 crashes at this location from 2018-2022. Of these 17, 8 were angle, 3 were rear end, 2 were fixed object, 2 were right turn, 1 was left turn, and 1 was sideswipe. One of the angle crashes resulted in a fatality. The injury rate at this intersection is 35.3%.

#### Summary of Recommended Countermeasures

After analyzing the crash patterns at this location and conducting a feasibility study, it is recommended to close the at-grade intersection of US 30 with John Brown Road and construct an RCUT.

#### Project Priority Information

This project is part of the District's long range plan to close all of the at-grades along US 30. This specific location has a history of high severity crashes and is a priority to close.

### Crash Data

#### Crash Totals (average per year)

	Fatal & Serious Injury (KA)	Visible Injury (B)	Non-Visible (C)	Property Damage Only (O)	Total
Existing Conditions: Predicted Crash Frequency	0.0988	0.1986	0.1189	1.0212	1.44
Existing Conditions: Expected Crash Frequency	0.1268	0.2623	0.1545	1.4860	2.03
Potential for Safety Improvement	0.0280	0.0637	0.0356	0.4648	0.59
Proposed Conditions: Expected Crash Frequency	0.0174	0.0326	0.0209	0.4277	0.50
Observed Crashes	0.4000	0.6000	0.2000	2.0000	3.20

#### Observed People Injury Totals

	Fatal Injury (K)	Serious Injury (A)	Visible Injury (B)	Non-Visible (C)	Total
Observed People Injury Totals	0.2000	0.2000	0.6000	0.6000	1.60

### Application Scoring

Category	Scoring Value	Points Awarded	Points Possible
Ratio of Observed Fatal and Serious Injuries to Observed Total Crashes	0.13	26	30 ▲
Percentage of the Potential for Safety Improvement to Total Expected Crashes	29.06%	20	20
Relative Severity Index	\$48,302.36	20	20
Equivalent Property Damage Only Index	4.78	16	20
Location Equity Measure	4.00%	0	10 ▼
<b>Total:</b>		<b>82</b>	<b>100</b>

### Key Safety Metrics

Functional Class: 
 Major Route AADT: 
 Maximum Posted Speed Limit: 
 Active Transportation Need: 
 Active Transportation Demand: 
 Bicycle Level of Traffic Stress:

TOAST Score ⓘ:

### Strategic Highway Safety Plan

Ohio Emphasis Area: 
 Ohio Emphasis Area Subcategory: 
 FHWA Emphasis Area: 
 FHWA Improvement Category:

FHWA Improvement Subcategory:

### Work Locations

NLFID	Begin Logpoi...	End Logpoint	Location Termini ⓘ
SVANUS00030**C	11.42	11.474	US 30
SVANUS00030**C	11.474	11.52	US 30
SVANUS00030**C	11.474	0	John Brown Road

### Project Funding

Safety Study	Interchange Mod. Study	PE-Environmental	PE-Detail Design	Right of Way/Utilities	Construction
Fiscal Year: <input type="text" value="2023"/>	Fiscal Year: <input type="text"/>	Fiscal Year: <input type="text"/>	Fiscal Year: <input type="text" value="2025"/>	Fiscal Year: <input type="text"/>	Fiscal Year: <input type="text" value="2028"/>
Phase Completed? <input checked="" type="checkbox"/>	Phase Completed? <input type="checkbox"/>	Phase Completed? <input type="checkbox"/>	Phase Completed? <input type="checkbox"/>	Phase Completed? <input type="checkbox"/>	Phase Completed? N/A
Completed by ⓘ: <input type="text" value="Arcadis"/>	Completed by ⓘ: <input type="text"/>	Completed by ⓘ: <input type="text"/>	Completed by ⓘ: <input type="text"/>	Completed by ⓘ: <input type="text"/>	Completed by N/A
Completion Date: <input type="text" value="5/12/2023"/>	Completion Date: <input type="text" value="month/day/year"/>	Completion Date: <input type="text" value="month/day/year"/>	Completion Date: <input type="text" value="month/day/year"/>	Completion Date: <input type="text" value="month/day/year"/>	Completion Date N/A

Project Phase	Safety Study	Interchange Mod....	PE - Environmental	PE - Detailed Desi...	Right of Way/Utili...	Construction	Total
Previous Safety	\$741,450.00	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00	\$741,450.00 ▲
New Safety	\$0.00	\$0.00	\$0.00	\$420,000.00	\$0.00	\$2,840,000.00	\$3,260,000.00
Sponsor Funding	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00	\$0.00 ▼
	\$741,450.00	\$0.00	\$0.00	\$420,000.00	\$0.00	\$2,840,000.00	\$4,001,450.00

### Additional Funding Detail

Additional Funding Detail

### Safety Economic Analysis Results

Net Present Value of Project: 
 Net Present Value of Safety Benefits: 
 Net Benefit: 
 Benefit / Total Project Cost Ratio: 
 Benefit / Safety Funding Request Ratio:

### Ethics



I have reviewed and will comply with the Ethics and Conflict of Interest laws including 23 CFR § 1.33, 23 CFR 636.116, and Ohio Revised Code sections 102.03, 2921.42 and 2921.43. Find form [here](#).

DSRT

The DSRT has reviewed and approves this application

Comment

Comment

File Attachments

Please attach relevant project files such as the One Page Summary, CAM Tool, ECAT files, Safety Study, Crash Diagram, etc.

Select files... Drop files here to

Download All

File	Uploaded By	U
PAU-VAN US 30 Final Feasibility Study.pdf	DOT\hrobey	8 ▲
cam tool (2018-2022) John Brown Road.xlsm	DOT\hrobey	8
EXISTING John Brown Road ECAT.xlsm	DOT\hrobey	8
RCUT John Brown Road ECAT.xlsm	DOT\hrobey	8 ▼

Review Comments

Recommendation:

Comment

Review Comment

Cancel Save

Review Comment	
<p>Charles Fisher</p> <p>Recommendation: 5 Yes</p>	 
<p>Drew Janek</p> <p>Recommendation: 5 RCUT for crash types looks good. This is a yes for me dawg.</p>	 
<p>Kathryn Wade</p> <p>Recommendation: 5 Good application.</p>	 
<p>Caraline Griffith</p> <p>Recommendation: 5 Similar to the first app, but this one meets thresholds and would be proper use of Safety Funds.</p>	 
<p>Brenton Bogard</p>	