

FED. RD. DIVISION	STATE	PROJECT
2	OHIO	STATE

SAN - 105 - 3.59

SEP 14 1962  
GROUND PHOTOLAB

# STATE OF OHIO

## DEPARTMENT OF HIGHWAYS

# SAN - 105 - 3.59

## SANDUSKY COUNTY

# VILLAGE OF WOODVILLE

**CONVENTIONAL SIGNS**

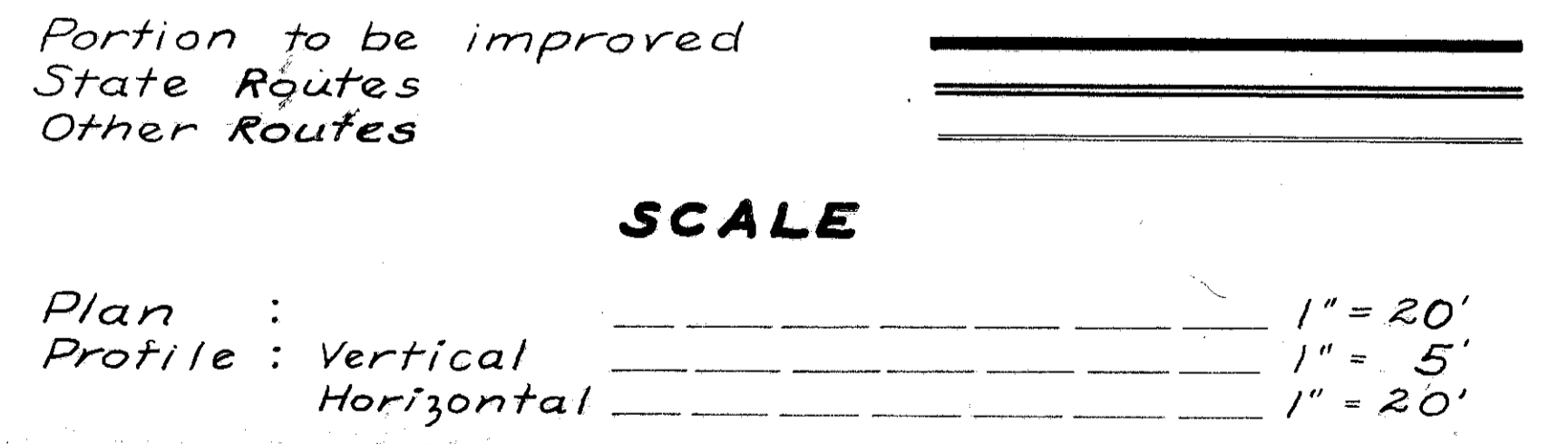
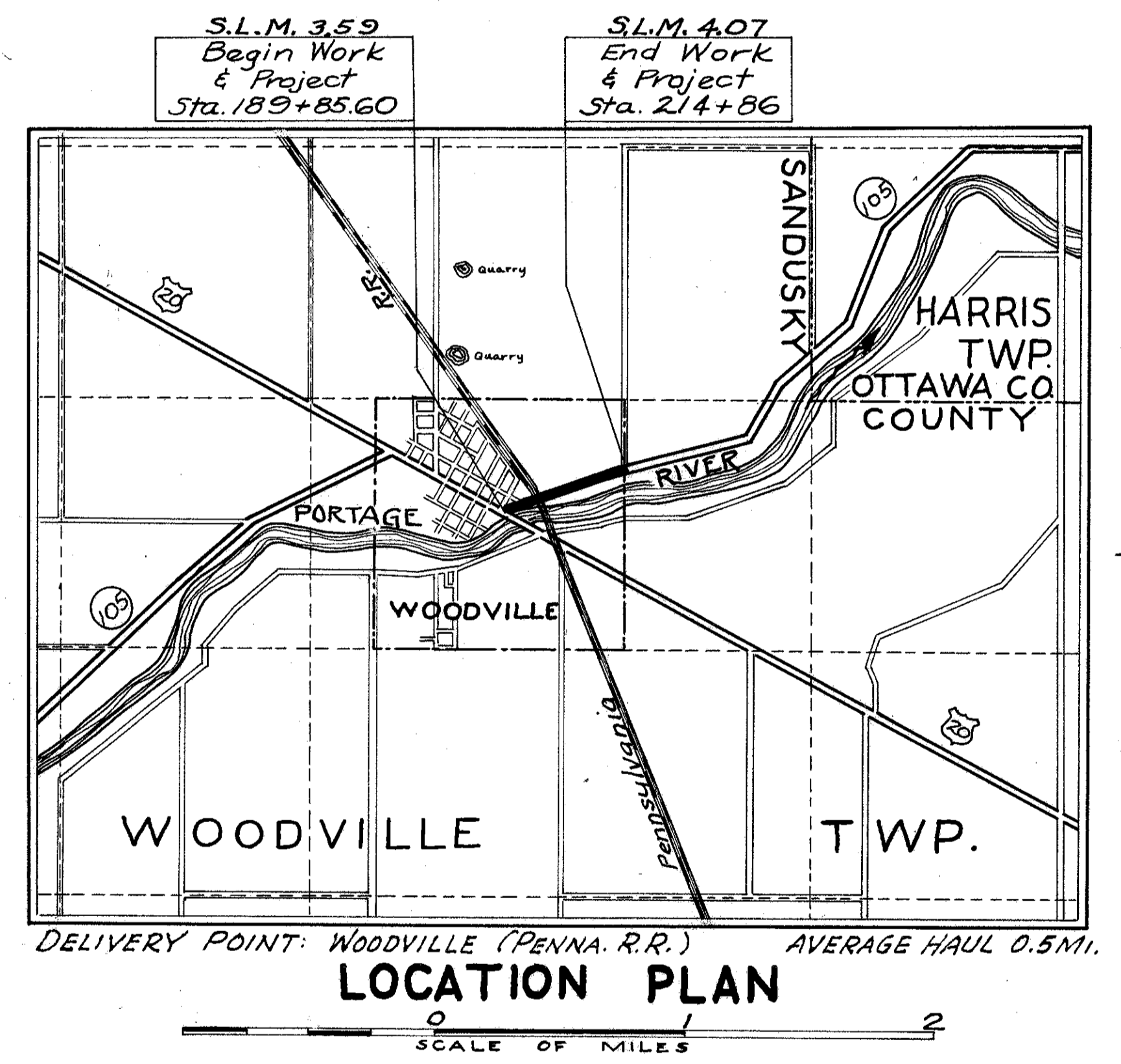
County Line	-----
Township Line	-----
Section Line	-----
Center Line	200 201 202
Corporation Line	-----
Fence Line	x x x x x
Railroad	=====
Power Poles	⊕ ⊕ ⊕ ⊕ ⊕ ⊕ ⊕ ⊕
Telephone Poles	⊕ ⊕ ⊕ ⊕ ⊕ ⊕ ⊕ ⊕
Trees or Stumps (existing)	⊗ ⊗ ⊗ ⊗ ⊗ ⊗ ⊗ ⊗
Trees or Stumps (to be removed)	⊗ ⊗ ⊗ ⊗ ⊗ ⊗ ⊗ ⊗
Drainage	Existing ----- New -----

**INDEX OF SHEETS**

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**LINE DATA**

Begin Work and Project	Sta. 189+85.60
End Work and Project	Sta. 214+86
Gross Length of Work and Project	2500.40 Lin. Ft.
Deduct for R.R. Crossing	22.37 Lin. Ft.
Net Length of Work and Project	2478.03 Lin. Ft. = 0.469



The standard specifications of the State of Ohio, Department of Highways, including changes and supplemental specifications listed in the proposal shall govern this improvement.

The right-of-way for this improvement will be provided by the State of Ohio.

I hereby approve these plans and declare that the making of this improvement will not require the closing of the highway to traffic and that provisions for the maintenance and safety of traffic will be as set forth on these plans and estimates.

Approved: Wm J. Sims  
Date: 6-3-59 Division Deputy Director

Approved: Gay E. Neeser  
Date: 6-16-59 Deputy Director of Planning & Programming

Approved: \_\_\_\_\_  
Date: \_\_\_\_\_ Engineer of Bridges

Approved: Wm J. Korman  
Date: 6-12-59 Engineer of Location & Design

Approved: C. W. McCauley  
Date: 6-12-59 Deputy Director of Design & Construction

Approved: J. Berry  
Date: 6-16-59 First Assistant Director

Approved: E. J. Rutter  
Date: 6-16-59 Director of Highways

**STANDARD DRAWINGS**

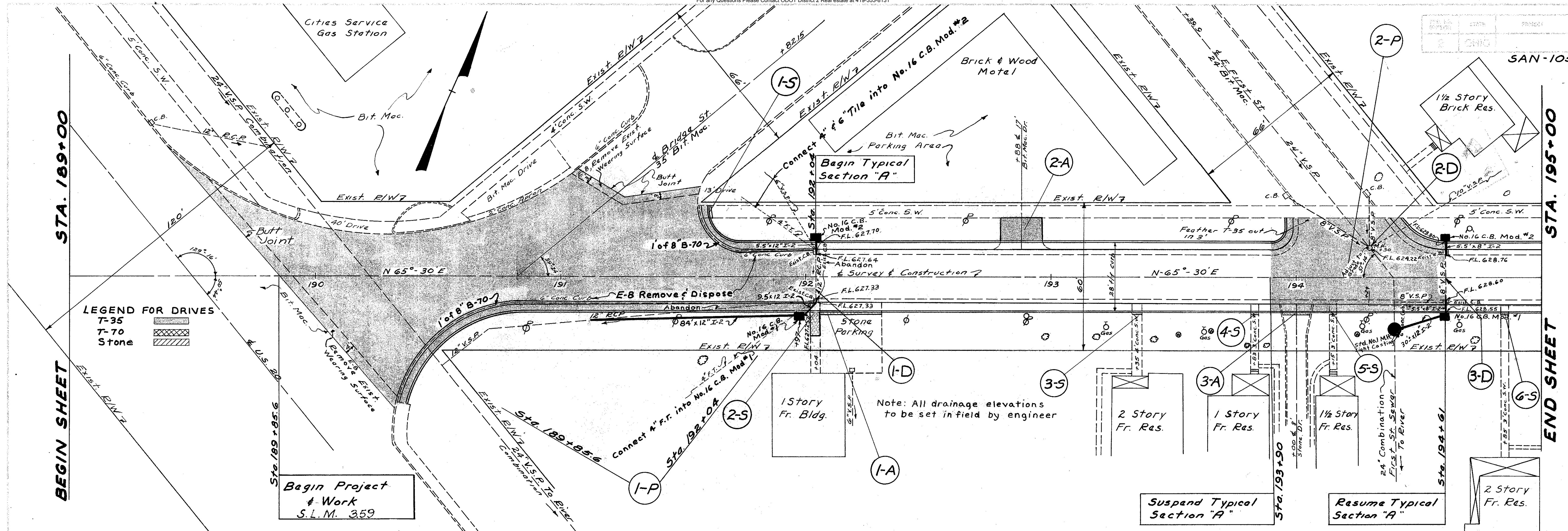
DR-1	1-3-55	T-35	1-2-56
G-707	6-1-56	I-8 M.H. No. 1A	1-26-59
I-1,2,3,4 & 5	4-24-58	L-3	4-1-50
I-8 C.B. No. 16, 3-2-59			
I-8 M.H. No. 1	1-26-59		
I-12	7-1-54		
L-3-A	4-1-50		

**SUPPLEMENTAL SPECIFICATIONS**


SEP 14 1962  
GROUND PHOTOLAB

File No.	SANDUSKY COUNTY	SAN-105-3.59
Date of Letting	_____	
Contract No.	_____	





**LEGEND FOR DRIVES**  
 T-35 [Symbol]  
 T-70 [Symbol]  
 Stone [Symbol]

**Begin Project & Work**  
 S.L.M. 359

B.M. U.S.G.S. Bronze Plate on  
 S.W. Wing Top of Coping, Portage  
 River Bridge Elev. 635.35

B.M. Sta. 194+72 20' Lt. Flange  
 Bolt on back of Fire Hydrant  
 Elev. 634.49

Note: All drainage elevations  
 to be set in field by engineer

Note: ± Grade back of Sta. 192+04  
 will be approx. 2 3/4' above existing ±.  
 Inside edge of combination curb &  
 gutter Rt & Lt. back of Sta. 192+04  
 will be approx. 2 1/2' above outside  
 edge of existing concrete gutter.

Proposed  
 Top of  
 Curb Elev.  
 Rt. & Lt.

640  
 635  
 630  
 625

Existing  
 Highest  
 Gutter  
 Elev.

634.10

632.53

630.87

631.30

631.84

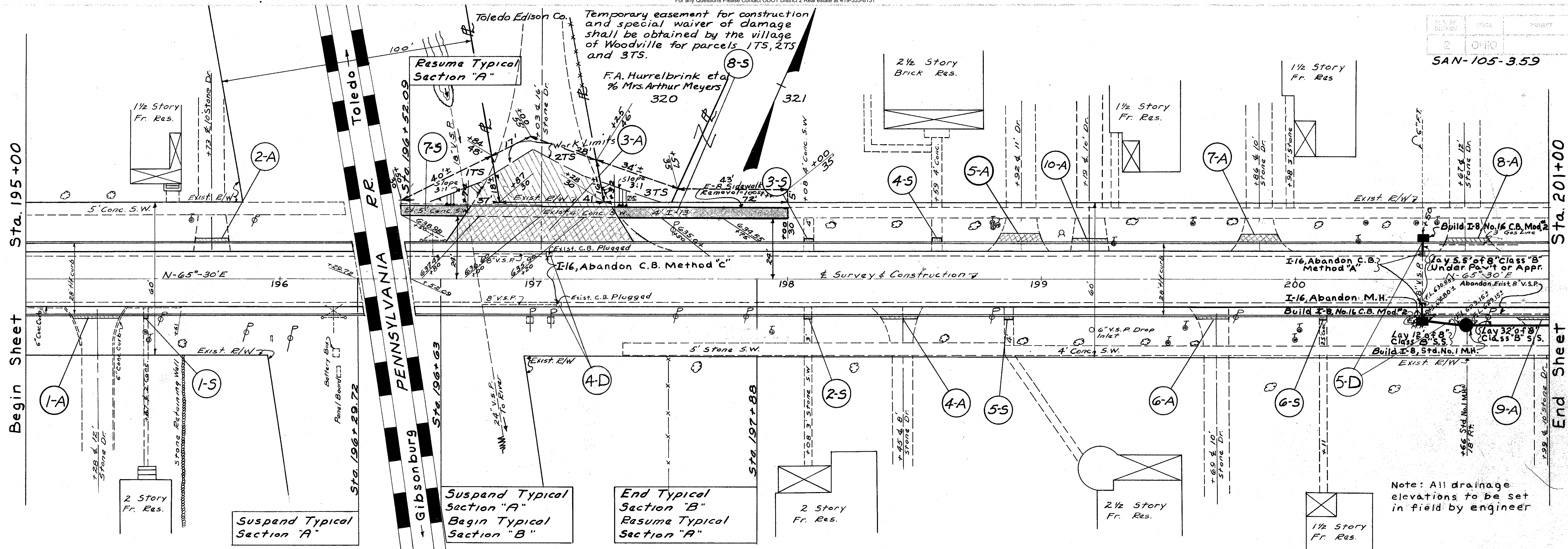
632.44

**I-D**  
 Sta. 191+11 ± Rt. to Sta. 191+95 Rt.  
 Locate existing 12" R.C.P. at Sta. 191+11 ± Rt.  
 Lay 84' ± of 12" I-2 Class "B" S.S.  
 Sta. 191+97 Rt.  
 I-B, Build No. 16 C.B. Mod. #1  
 Lay 9.5' of 12" I-2 Class "B" S.S. Under Pav't or Appr.  
 from existing C.B. to proposed No. 16 C.B. Mod. #1  
 I-16, Abandon Existing C.B. Method "A"  
 Sta. 192+04 Lt.  
 I-B, Build No. 16 C.B. Mod. #2  
 Lay 5.5' of 12" I-2 Class "B" S.S. Under Pav't or Appr.  
 I-16, Abandon Existing C.B. Method "A"

**2-D**  
 Sta. 194+30 Lt.  
 I-B, Adjust M.H. to Grade

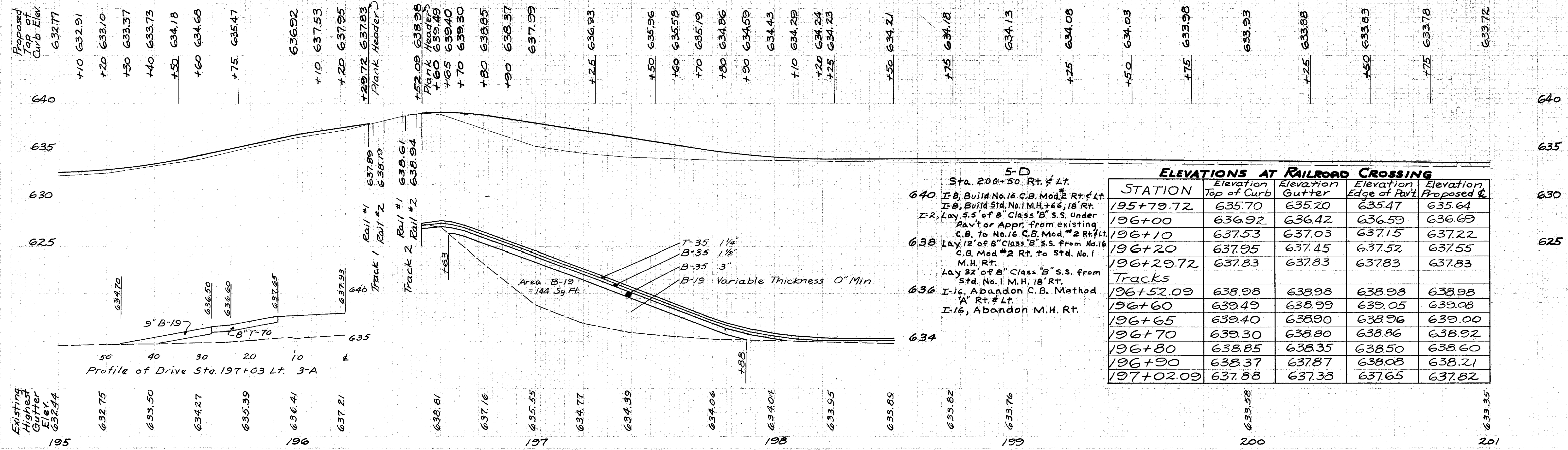
**3-D**  
 Sta. 194+61 Rt. & Lt.  
 I-B, Build No. 16 C.B. Mod. #2 Lt.  
 I-B, Build No. 16 C.B. Mod. #1 Rt.  
 Lay 5.5' of 8" I-2 Class "B" S.S. Under Pav't  
 or Appr. Rt. & Lt.  
 Locate 24" V.S.P. First St. Sewer  
 I-B, Build Std. No. 1 M.H. 21' Rt. and connect  
 First St. Sewer to M.H.  
 Lay 30' ± of 12" Class "B" S.S. from Std. No. 1 M.H.  
 to No. 16 C.B. Mod. #1  
 I-16, Abandon Existing C.B. Method "A" Rt. & Lt.





Note: All drainage elevations to be set in field by engineer

B.M. 196+98 - 16' Rt.  
Top S.W. corner conc. block  
around P.Pole Elev. 639.26



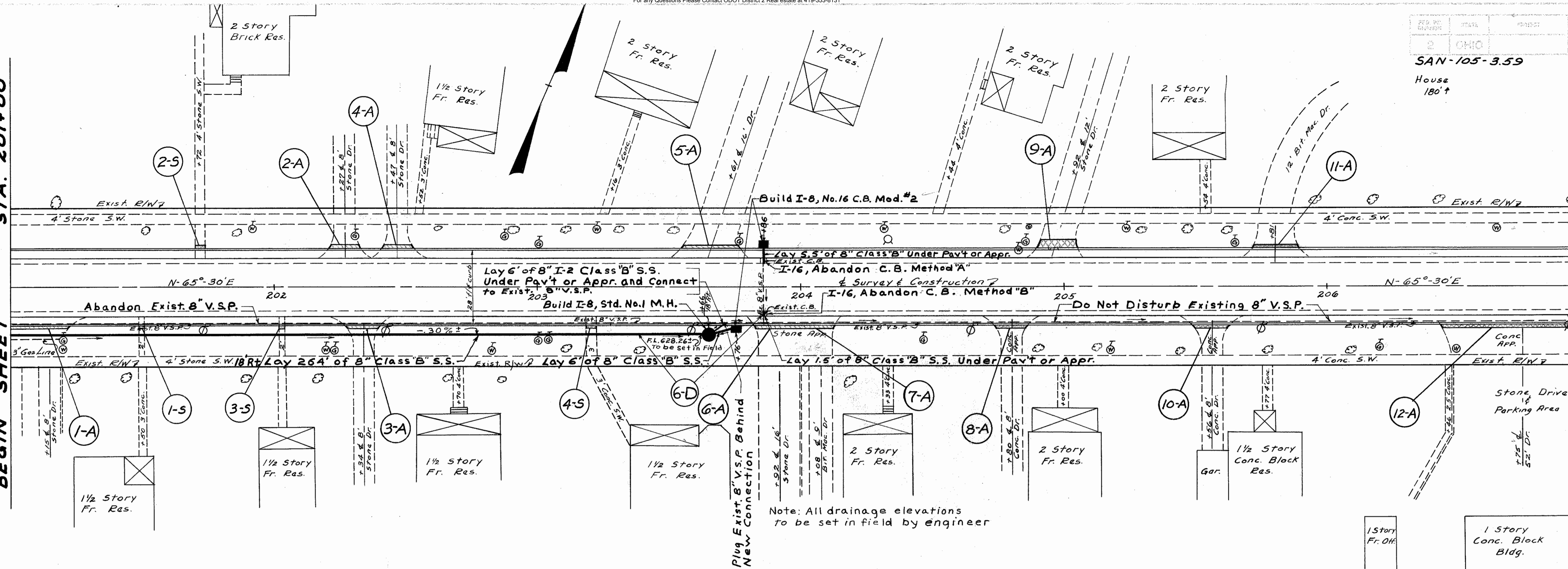


STA. 201+00

STA. 207+00

BEGIN SHEET

END SHEET

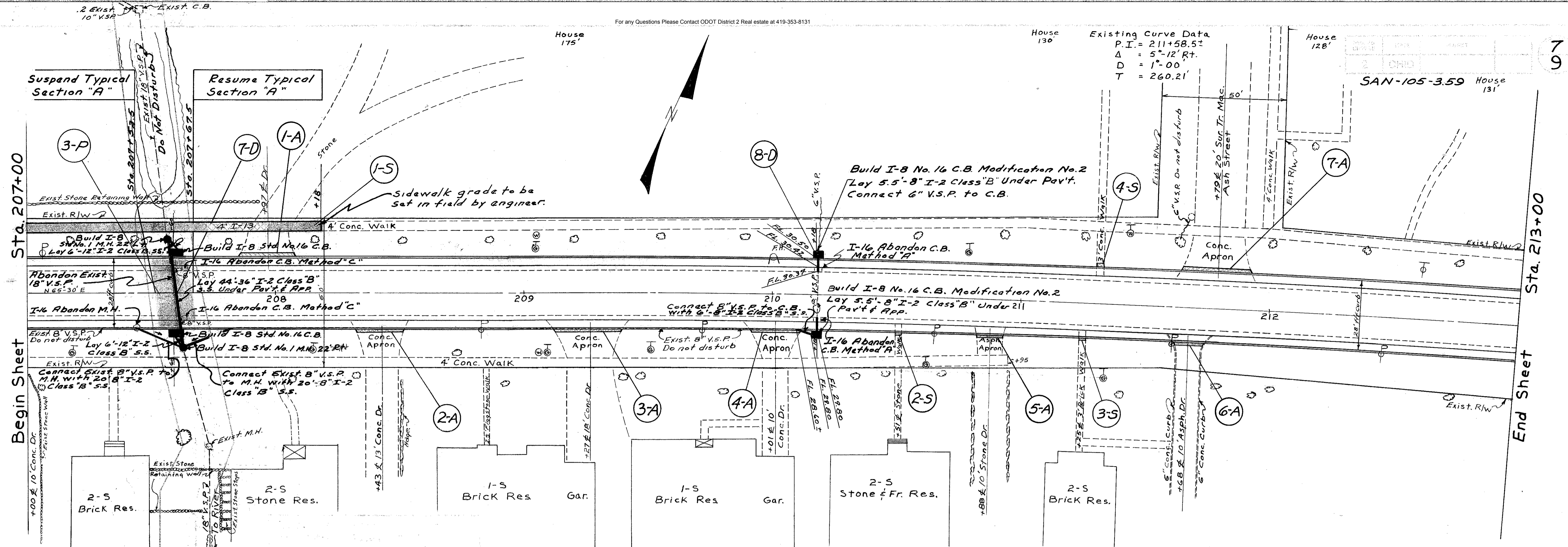


Note: All drainage elevations to be set in field by engineer

Station	Proposed Top of Curb Elev. Eft. Lt.	Structure / Note	Existing Highest Gutter Elev.
201	633.72		633.35
201+25	633.67		
201+50	633.62		
201+75	633.57		
202	633.51		633.13
202+25	633.46		
202+50	633.41		
202+75	633.36		
203	633.31		632.92
203+25	633.26		
203+50	633.21		
203+75	633.16		
204	633.10		632.83
204+25	633.05		
204+50	633.00		
204+75	632.95		
205	632.89		632.52
205+25	632.84		
205+50	632.79		
205+75	632.74		
206	632.68		632.28
206+25	632.61		
206+50	632.53		
206+75	632.46		
207	632.38		631.99

**6-D**  
 Sta. 203+86 Lt. & Rt.  
 I-8, Build No. 16 C.B. Mod. #2 Lt.  
 I-2, Lay 5.5' of 8" Class "B" S.S. Under Pav't or Appr. Lt.  
 I-2, Lay 1.5' of 8" Class "B" S.S. Under Pav't or Appr. Rt.  
 I-16, Abandon Existing C.B. Method "A" Lt.  
 I-16, Abandon Existing C.B. Method "B" Rt.  
 Sta. 203+76 Rt.  
 I-8, Build No. 16 C.B. Mod. #2  
 I-2, Lay 6' of 8" Class "B" S.S. from No. 16 C.B. Mod. #2 to Std. No. 1 M.H.  
 Sta. 203+66 Rt.  
 I-8, Build Std. No. 1 M.H. 18' Rt.  
 I-2, Lay 6' of 8" Class "B" S.S. Under Pav't or Appr. from Std. No. 1 M.H. to Existing 8" V.S.P. and Connect.  
 I-2, Lay 264' of Class "B" S.S. to Std. No. 1 M.H. 18' Rt.





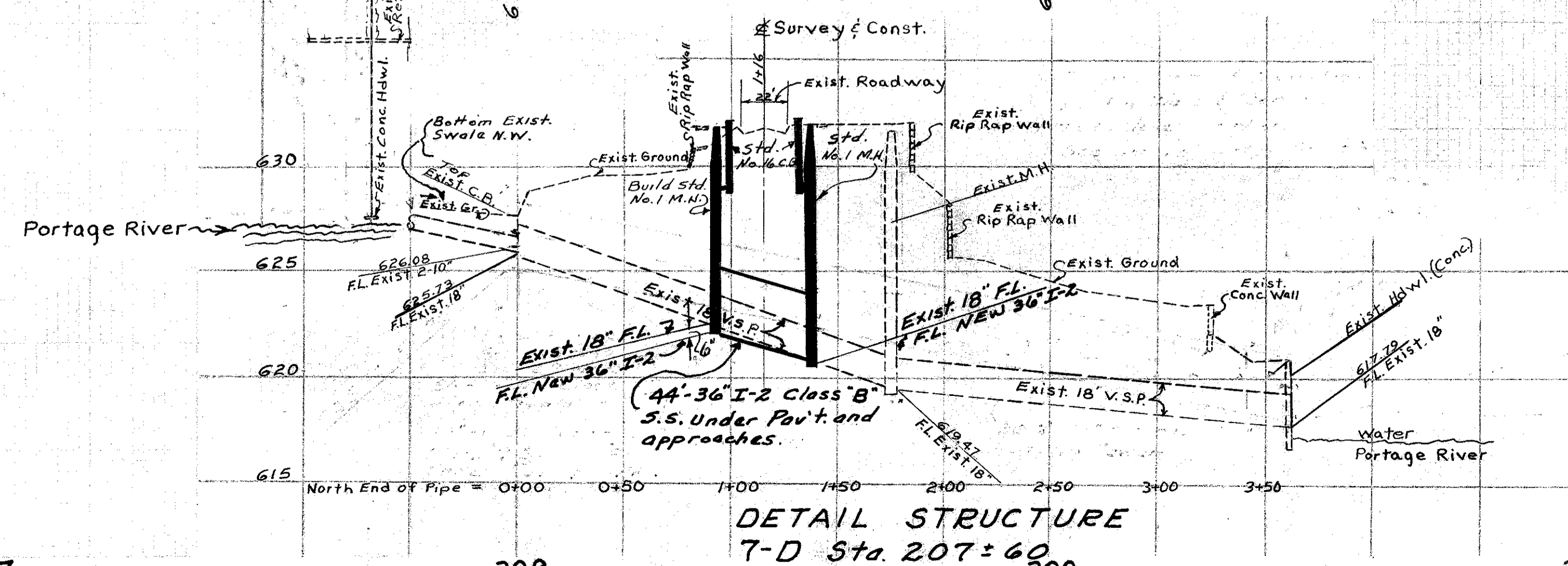
Existing Curve Data  
 P.I. = 211+58.5'  
 $\Delta$  = 5°-12' R.  
 D = 1°-00'  
 T = 260.21'

Sta. 207+00  
 Begin Sheet

Sta. 213+00  
 End Sheet

Station	Proposed Top of Curb Elev.	Existing Highest Gutter Elev.
207+00	632.36	631.99
207+10	632.35	
207+25	632.30	
207+60	632.27	
207+75	632.29	
208+00	632.35	631.88
208+25	632.38	
208+50	632.47	
208+75	632.54	
209+00	632.61	632.78
209+25	632.68	
209+50	632.76	
209+75	632.83	
210+00	632.90	632.46
210+25	632.97	
210+50	633.05	
210+75	633.12	
211+00	633.19	632.76
211+25	633.26	
211+50	633.34	
211+75	633.41	
212+00	633.48	633.00
212+25	633.55	
212+50	633.63	
212+75	633.68	
213+00	633.70	633.33

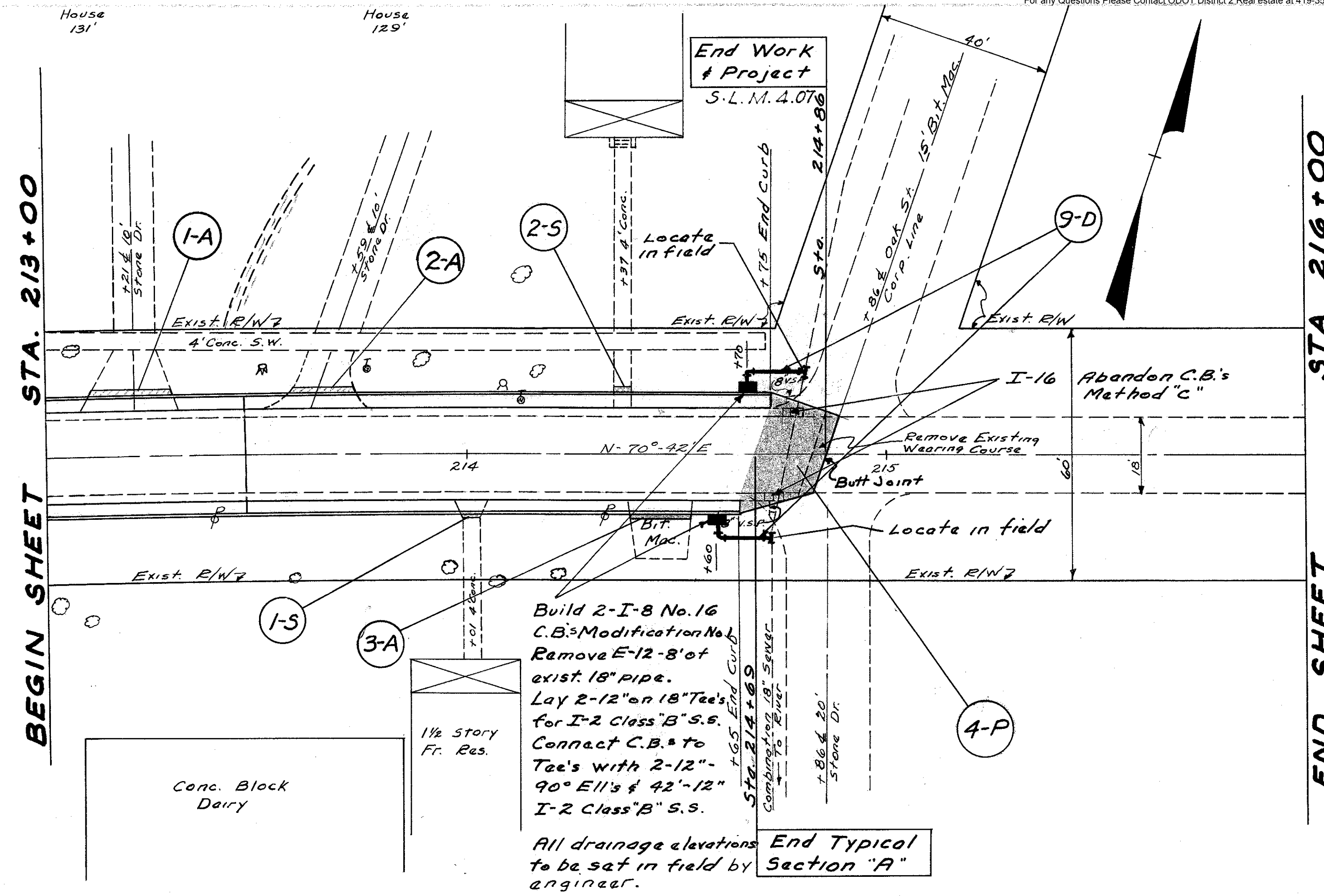
B.M. Sta. 210+49, 55' R.  
 S.W. Cor. 2nd Conc. Step  
 to Brick House, Elev. 634.31



Note: At Sta. 207+60 locate the existing 18" V.S.P. 22' R+L. Establish new 36" I-2 Storm Sewer grade as indicated on 7-D structure detail Sheet No. 7. It is the intent to lay the new 36" S.S. along the side of the existing 18" sewer, between the 2 new manholes in order that a continuous outlet for the existing 18" sewer North of the North manhole be provided during the construction of the new 36" storm sewer. One-way traffic shall be maintained at all times. Plug both ends of abandoned 18" V.S.P. At time of construction if any of the existing 18" V.S.P. to be abandoned, is found to be broken down, the portion broken shall be removed and these two ends of pipe shall be plugged. Payment for plugging of existing pipe and removal of broken pipe shall be included in the price bid per lin. ft. of 36" I-2 Class "B" S.S. under pav't and approaches.

The depth of new manholes as shown on Detail 7-D Sheet 7, and the depth of existing 18" V.S.P. Under pav't. to be replaced with new 36" I-2 S.S. are only approximate.





STA. 213+00  
BEGIN SHEET  
END SHEET  
STA. 216+00

**GENERAL NOTES**

**FIELD OFFICE:** The contractor shall provide a suitable field office in accordance with Section S-0.01 having a minimum floor area of 150 sq. feet. The Contractor shall have a telephone installed and maintained in this field office during the construction of this project.

**E-1 EMBANKMENT, METHOD "A":** An estimated 300 cu. yds. of E-1 Embankment, Method "A" have been estimated to supply embankment necessary back of proposed curb as shown on Typical Sections. Also embankment necessary to build new sidewalk on left from Sta. 206+96 to Sta. 208+18. Material required to supply embankment shall be obtained from the excavation necessary to construct the I-12 No. 2 Combination Curb and Gutter.

**HAND FINISHING:** Hand finishing will be permitted as per Sec. B-71.211 Portland Cement Concrete Base Course and T-71.211 Portland Cement Concrete Pavement on this project.

Where proposed curb grade is lower than existing ground line, the earth shall be sloped toward the top of the curb, for a distance of 3 feet, as shown on Typical Sections. Payment included in the price bid per lin. ft. of I-12 No. 2 Combination Curb and Gutter, Modified as per plan.

**SUPERELEVATION:** Curves are not to be superelevated since this project is a village street.

**CROWN AT RAILROAD CROSSING:** The crown shall be worked out of the pavement 50' each side of the railroad crossing by raising the edges of the pavement. The slope of the gutter shall be removed in this same distance to meet the rail elevation. See elevations, sheet No. 5.

**UTILITIES:** All work required to relocate or adjust, etc. all Water, Gas, Telephone and or Telegraph, Electric, Electric Street Light Poles or other services to the new grade and alignment will be done by and at the expense of the Utilities in question and the Village of Woodville.

**CURB ENDS:** Drop curb face from 6" to 0" in 3 feet at all curb ends.

**BUTT JOINT:** Where a butt joint is indicated on the plans, the existing Bituminous Concrete shall be dressed to a neat line down to a depth of 1/4" or more by using an air spade.

**UTILITY OWNERSHIP:**  
Water: Woodville Light & Water (Village owned)  
Electric: Woodville Light & Water (Village owned)  
Toledo Edison Co., Fremont, O.  
Gas: Ohio Fuel Gas Co., Fremont, O.  
Telephone: Lemoyne Telephone Co. Lemoyne, O.

**EXTRA 8" B-70 PLAIN PORTLAND CEMENT CONCRETE BASE COURSE:** An estimated 200 sq. yds. of extra 8" B-70 and 200 sq. yds. of E-8 Removal and Disposal of Existing Pavement have been added to replace existing pavement failures, as directed by the Engineer.

**L-9 SEEDING & FERTILIZER:** Quantities for seeding and fertilizer are calculated for the soil areas as shown on the Typical Sections.

12-12-12 Commercial fertilizer shall be applied to the above seeded area at the rate of 20 lbs. per 1000 sq. ft. Seeding shall be applied to the above area at the rate of 3 lbs. per 1000 sq. ft. using the following seed mixture:

**EXTRA I-13, 4" CONCRETE SIDEWALK:** An extra 200 sq. ft. of I-13, 4" Concrete Sidewalk and 200 sq. ft. of E-8 Removal & Disposal of Existing Sidewalk have been added to the plan quantities to allow for replacement of walks where the slope to curb is excessive or where the boulevard walk is badly broken. Also to replace walk broken in laying 8" I-2 from Sta. 200+50 to Sta. 203+86 on Rt.

**COMPACTION OF SUBGRADE:**

Compaction of subgrade for Curb and Gutter as required by Sec. E-1.09 is hereby waived on this project.

**EXISTING DRAINAGE OUTLETS:** At the following approximate stations on Rt; 191+00, 194+50, 203+80 and 214+75 the contractor shall locate, in the field, the existing drain pipes before starting drainage operations. Also see Note, Sheet 7

**TRAFFIC:** Traffic shall be maintained at all times. The length of one way traffic zones shall be kept to a minimum consistent with the specification requirements for Protection of Wearing Course. When placing the B-19 Crushed Aggregate Base Course, it shall be placed in 3" compacted layers for the full width and length as shown on Typical Section "B", and one way traffic maintained at all times.

**GRADING TOLERANCE:**

For areas in front of residences, for areas between curb and sidewalk and for other areas specifically indicated on the plans, the seed bed shall be prepared to provide a smooth surface. All stones larger than one inch in diameter shall be removed from the surface of the seed bed. Hand raking will be required in areas inaccessible to machines and hand raking may be required, if directed by the engineer in all the aforementioned areas if machines used do not provide results equivalent to results obtained by hand raking. Cost of this additional work shall be included in the unit price bid for Item L-9 Seeding and Protecting, As Per Plan.

\* To Table "P" Sheet No. 3  
\*\* To Table "S" Sheet No. 3

**B-35, BASE COURSE:**

Type "A" surface course composition may be used in the construction of the asphaltic concrete base course on this project.

Proposed Top of Curb Elev. (FT. Lr.)	633.77	633.68	633.64	633.58	633.51	633.24	633.19
	+25	+50	+75	+65	+75		
Existing Highest Gutter Elev.	633.33	633.15	632.88				
Station	213	214	+76	215	216		

B.M. Sta. 215+69 63' Lt.  
S.W. Cor. 1<sup>st</sup> Conc. Step  
to frame house Elev. 634.85