

GENERAL

UTILITIES (G102A)

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

CABLE ARMSTRONG UTILITIES 1215 CLAREMONT AVENUE ASHLAND, OH 44805 419.289.0161	COMMUNICATION BRIGHTSPEED 203 W. 9TH STREET LORAIN, OH 44052 440.244.8330
GAS COLUMBIA GAS OF OHIO 1021 NORTH MAIN STREET MANSFIELD, OH 44903 419.528.1134	GAS TC ENERGY 589 N STATE ROAD MEDINA, OH 44256 330.721.4163
ELECTRIC FIRELANDS ELECTRIC 1 ENERGY PLACE NEW LONDON, OH 44851 419.929.1571	GAS MARATHON PIPELINE 539 SOUTH MAIN STREET FINDLAY, OH 45840 419.884.0800
WATER AQUA OHIO, INC 870 THIRD STREET NW MASSILLON, OHIO 44647 330.832.7600	TRAFFIC ODOT DISTRICT THREE 906 CLARK AVENUE ASHLAND, OH 44805 419.207.2868
TRAFFIC ODOT OFFICE OF TRAFFIC OPERATIONS 1980 W BROAD STREET COLUMBUS, OH 43223 614.644.0270	ELECTRIC OHIO EDISON 2508 WEST PERKINS AVENUE SANDUSKY, OH 44870 419.627.6881
COUNTY RICHLAND COUNTY SANITARY ENGINEER 50 PARK AVENUE EAST MANSFIELD, OH 44902 419.774.3548	FIBER OPTIC SPRINT 11370 ENTERPRISE PARK DRIVE SHARONVILLE, OH 45241 513.612.4204
CABLE CHARTER COMMUNICATIONS 5520 WHIPPLE AVENUE NW NORTH CANTON, OH 44720 330.494.9200	GAS TC ENERGY P.O. BOX 85 LAKEVILLE, OH 44638 419.827.2620

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

EXISTING PLANS (G103)

EXISTING PLANS ENTITLED RIC-71-(6.39)(10.77), 2018 MAY BE INSPECTED IN THE ODOT DISTRICT THREE OFFICE IN ASHLAND.

WORK LIMITS (G106)

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROGRESSION OF WORK

WIDENING SHALL BE DONE PRIOR TO RESURFACING. GUARDRAIL SHALL BE REMOVED PRIOR TO ANY EMBANKMENT WORK AT THE GUARDRAIL RUN. GUARDRAIL WORK SHALL BE DONE AFTER WIDENING, RESURFACING, AND BERM WORK SO AS TO ESTABLISH PROPER GRADES FROM WHICH TO CONSTRUCT THE RAIL.

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

ROADWAY

ITEM 209 – LINEAR GRADING

THE CONTRACTOR IS REQUIRED TO PERFORM LINEAR GRADING ON THE GRADED SHOULDER. IT IS ANTICIPATED THAT THERE ARE AREAS WHERE THE GRADED SHOULDER IS AT A HIGHER ELEVATION THAN THE ADJACENT PROPOSED PAVEMENT. A 10:1 SLOPE SHALL BE ESTABLISHED, OR AS DIRECTED BY THE ENGINEER, WHEN PERFORMING ITEM 209 LINEAR GRADING. THE INTENT IS TO PROVIDE AN UNOBSTRUCTED AND POSITIVE FLOW OF STORM WATER FROM THE PAVEMENT TO THE DITCH. THE LINEAR GRADING SHALL BE PERFORMED AFTER THE INTERMEDIATE COURSE HAS BEEN COMPLETED AND BEFORE THE SURFACE COURSE IS PLACED. ALL LABOR AND EQUIPMENT NECESSARY TO PERFORM THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER MILE FOR ITEM 209 - LINEAR GRADING.

GUARDRAIL

CONNECTING GUARDRAIL TO EXISTING RAIL

IN LOCATIONS WHERE TYPE 5 GUARDRAIL, TERMINAL ASSEMBLIES, ETC. ARE TO BE CONNECTED TO EXISTING RAIL SOME MODIFICATIONS MAY BE REQUIRED, INCLUDING EXTRA POSTS, DRILLING HOLES AND POSSIBLY PARTIAL SECTIONS OF ADDITIONAL RAIL ELEMENTS. THE COST OF THIS ADDITIONAL WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR TYPE 5 GUARDRAIL. IF ADDITIONAL PORTIONS OF RAIL ELEMENT ARE USED THE LINEAL MEASUREMENT OF THIS ADDITIONAL PORTION SHALL BE ADDED FOR PAYMENT.

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL (R111)

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

LOCATIONS OF GUARDRAIL

THE GUARDRAIL PROTECTION PROVIDED IN THIS PLAN SHALL BE LOCATED IN THE FIELD TO ASSURE THAT THE INSTALLATION WILL AFFORD THE MAXIMUM PROTECTION FOR TRAFFIC. THIS LOCATION SHALL BE POSITIONED AS FAR AS POSSIBLE FROM THE EDGE OF PAVEMENT WHILE MAINTAINING PROPER GRADE IN FRONT OF GUARDRAIL AS PER STANDARD DRAWINGS AND PLAN DETAILS.

SUGGESTED SEQUENCE OF GUARDRAIL WORK

1. GUARDRAIL WORK IS TO BEGIN AFTER THE LINEAR GRADING IS COMPLETED AND THE 617 MATERIAL IS PLACED.
2. REMOVE THE GUARDRAIL.
3. PERFORM THE RESHAPING UNDER GUARDRAIL INCLUDING COMPLETING THE EMBANKMENT, AS PER PLAN IF APPLICABLE.
4. REBUILD/CONSTRUCT THE GUARDRAIL RUN.
5. INSTALL BARRIER REFLECTORS.

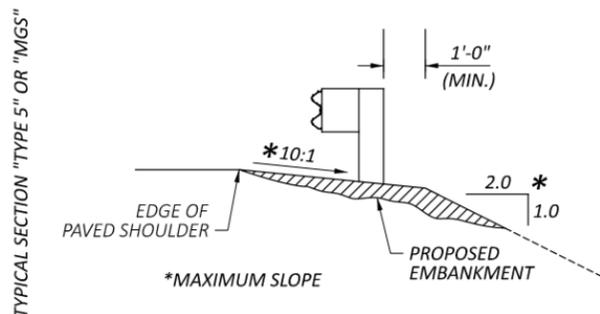
ITEM 203 – EMBANKMENT, AS PER PLAN

AT SPECIFIED LOCATIONS AND LOCATIONS AS DIRECTED BY THE ENGINEER, EMBANKMENT SHALL BE PLACED AS TO PROVIDE A SUITABLE AREA TO CONSTRUCT GUARDRAIL AND TO PROVIDE STRUCTURAL INTEGRITY OF THE ROADWAY SHOULDER.

AREAS WHERE EMBANKMENT MATERIAL IS TO BE PLACED SHALL BE SCALPED. THE REQUIREMENTS FOR BENCHING SHALL BE WAIVED. THE DEPTH OF LAYERS IN WHICH THE EMBANKMENT IS PLACED SHALL BE LIMITED TO EIGHT (8) INCHES IN THICKNESS. THE METHOD OF COMPACTION AND EQUIPMENT USED SHALL BE PER C&MS 203.07 OR 98% MAXIMUM DRY DENSITY.

AFTER THE EMBANKMENT HAS BEEN PLACED, THE AREAS SHALL BE FERTILIZED, SEEDED, MULCHED, AND WATERED AS PER ITEM 659. THE COST SHALL BE INCLUDED IN THIS ITEM FOR PAYMENT.

THE METHOD OF MEASUREMENT FOR EMBANKMENT MATERIAL SHALL BE BY THE NUMBER OF CUBIC YARDS MEASURED BY LOOSE VOLUME IN THE CARRIER AT THE WORK SITE, IN LIEU OF THE REQUIREMENTS OF 203.09. PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT UNIT BID PRICE PER CUBIC YARD FOR ITEM 203 - EMBANKMENT, AS PER PLAN AND SHALL INCLUDE ALL WORK DESCRIBED ABOVE.



ITEM 606 – ANCHOR ASSEMBLY, MGS TYPE B (R112A)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE MASH 2016 GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING'S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

REFER TO THE MANUFACTURER'S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND, THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE. THE FACE OF THE TYPE B IMPACT HEAD SHALL BE COVERED WITH TYPE J, ASTM D4956 TYPE XI REFLECTIVE SHEETING, PER CMS 730.193.

THE FACE OF THE TYPE B IMPACT HEAD SHALL BE COVERED WITH REROUNDABLE RETROREFLECTIVE SHEETING, PER CMS 730.191.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE B, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING REFLECTIVE SHEETING AND ALL RELATED HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM 606 – ANCHOR ASSEMBLY, MGS TYPE E (R113A)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE MASH 2016 GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING'S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE FACE OF THE TYPE E IMPACT HEAD SHALL BE COVERED WITH REROUNDABLE RETROREFLECTIVE SHEETING, PER CMS 730.191.

REFER TO THE MANUFACTURER'S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE E, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING ALL RELATED TRANSITIONS, REFLECTIVE SHEETING, HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM SPECIAL 606 – CABLE BARRIER ITEM SPECIAL 606 – CABLE BARRIER, ANCHOR ASSEMBLY (R127)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY ONE OF THE HIGH TENSION FOUR CABLE GUARDRAIL SYSTEMS AS LISTED ON THE OFFICE OF ROADWAY ENGINEERING'S WEB PAGE. PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, CABLE BARRIER, AND ITEM 606 CABLE BARRIER, ANCHOR ASSEMBLY AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL HIGH TENSION CABLE GUARDRAIL SYSTEM NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER. THE LENGTH OF THE TENSIONED CABLE NECESSARY TO INSTALL A FUNCTIONAL ANCHOR SYSTEM SHALL BE INCLUDED IN ITEM 606, CABLE BARRIER, ANCHOR ASSEMBLY.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

SYSTEMS SHALL HAVE A MAXIMUM DEFLECTION OF 8 FEET AND THE MAXIMUM LONGITUDINAL DISTANCE BETWEEN POSTS SHALL BE 15 FEET.

DELINEATE THE CABLE BARRIER USING TYPE 6 BARRIER REFLECTORS PER ITEM 626 OR USING FLEXIBLE POSTS PER ITEM 620 AS CALLED FOR IN THE PLANS OR DIRECTED BY THE ENGINEER.

ANCHOR TERMINAL STRUTS SHALL BE COVERED COMPLETELY ON BOTH SIDES WITH YELLOW REROUNDABLE RETROREFLECTIVE SHEETING, PER CMS 730.191.

TRANSITIONS TO W-BEAM GUARDRAIL ARE NOT ALLOWED.

REFER TO MANUFACTURER FOR MAXIMUM OFFSET FROM BREAK POINT.

TORPEDO OR BULLET SPLICES ARE NOT ALLOWED. ALL CABLE SPLICES SHALL BE A SWAGED OR OPEN BODY DESIGN THAT ALLOWS FOR ANNUAL INSPECTION BETWEEN THE WEDGE AND STRANDS OF CABLE.

PAVEMENT

ITEM 897 – PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (VARIES 0.0” – 0.5”)

THE INTENT OF ITEM 897 PLANING IS TO SMOOTH THE COMPLETED ASPHALT REPAIRS IN THE SECTION OF RIC-71-15.46 TO 20.64. PLANING IS TO BE PERFORMED WITHIN 14 DAYS AFTER COMPLETION OF THE PAVEMENT REPAIR. THESE AREAS MAY VARY IN DEPTH, AS DIRECTED BY THE ENGINEER.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE ABOVE WORK. PAYMENT FOR THE PLANING WILL BE MADE FOR A MAXIMUM OF THE SQUARE YARDS OF THE PAVEMENT REPAIR AREA REGARDLESS OF ANY OTHER INCIDENTAL PLANING OUTSIDE OF THE REPAIR AREA TO ENSURE SMOOTHNESS, AT THE DISCRETION OF THE ENGINEER. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SY OF ITEM 897 – PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A

ITEM 897 – PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (VARIES 0.0” – 0.5”) 6050 SY

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE

PLANING IS TO BE PERFORMED AS DIRECTED AND IN AREAS DESIGNATED BY THE ENGINEER. REMOVAL OF EXISTING PAVEMENT SURFACE MAY BE REQUIRED TO ELIMINATE ADVERSE SURFACE DISTORTION, WHICH IN THE JUDGEMENT OF THE ENGINEER, CANNOT BE SATISFACTORILY CORRECTED IN THE PAVING COURSES. THESE AREAS MAY VARY IN DEPTH, AS DIRECTED BY THE ENGINEER. THESE AREAS MAY INCLUDE MATERIAL DISPLACED BY RUTTING OR SHOVING ASPHALT SURFACE PATCHES, CONCRETE PATCHES, TRANSVERSE BUMPS, JOINTS AT STRUCTURES, ADJOINING PAVEMENTS, RAILROADS, ETC.

ITEM 408 - PRIME COAT, AS PER PLAN

THE CONTRACTOR SHALL APPLY ONE COAT OF MC-70 (AS PER SECTION 702) AT A RATE OF 0.40 GAL/SY TO THE COMPLETED AGGREGATE SHOULDER (ITEM 617) AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF PAVEMENT OR EDGE LINE. THE CONTRACTOR SHALL APPLY THE PRIME COAT TO THE AGGREGATE SHOULDER WITHIN 7 (SEVEN) CALENDAR DAYS OF THE BERM MATERIAL BEING LAYED ON THE SHOULDER. PRIOR TO APPLYING THE PRIME COAT LAYER, IT IS ALSO THE CONTRACTOR'S RESPONSIBILITY TO CORRECT ANY DAMAGES TO AGGREGATE SHOULDER THAT HAS OCCURRED DURING THE PERIOD OF TIME BETWEEN THE LAYING OF THE BERM MATERIAL AND THE SPRAYING OF PRIME COAT. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

ITEM 254 - PATCHING PLANED SURFACE

AN ESTIMATED QUANTITY OF ITEM 254 - PATCHING PLANED SURFACE HAS BEEN SET UP TO BE USED AS DIRECTED BY THE ENGINEER AS DESCRIBED IN CMS 254.04. THE LIMIT OF THE PATCHING DEPTH IS 0 TO 2 IN.

ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. PAVEMENT REPAIR SHALL BE PERFORMED BEFORE PAVEMENT PLANING AND BEFORE PLACEMENT OF THE OVERLAY COURSE(S). THE DEPTH OF REMOVAL SHALL BE AS DIRECTED BY THE ENGINEER WITH A MAXIMUM DEPTH OF 6”. THE MINIMUM WIDTH OF REPAIR SHALL BE 4FT. REPLACEMENT MATERIAL SHALL BE ITEM 301 ASPHALT CONCRETE BASE (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE.

LONGITUDINAL IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PARALLEL TO THE CENTERLINE THAN THE MEASUREMENT PERPENDICULAR TO THE CENTERLINE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN IS TO BE A MAXIMUM OF 6” DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

RIC-71-10.76 TO 15.46

ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN (LONGITUDINAL) 1500 CY

ITEM 253 – PAVEMENT REPAIR, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. PAVEMENT REPAIR SHALL BE PERFORMED BEFORE PAVEMENT PLANING AND BEFORE PLACEMENT OF THE OVERLAY COURSE(S). THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MINIMUM DEPTH OF 6” AND A MAXIMUM DEPTH OF 16” AND A MINIMUM WIDTH OF 4FT. FOR FULL DEPTH REPAIRS WHERE CONCRETE IS UNDERLYING ASPHALT BUT CONCRETE IS NOT BEING REPLACED AS PART OF THE REPAIR, REMOVE ALL ASPHALT TO THE TOP OF CONCRETE THEN COMPLETE FLEXIBLE REPAIR ON TOP OF EXISTING CONCRETE. REPLACEMENT MATERIAL SHALL BE ITEM 301 ASPHALT CONCRETE BASE (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. THE MAXIMUM PAVEMENT LIFT THICKNESS IS 6”.

LONGITUDINAL IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PARALLEL TO THE CENTERLINE THAN THE MEASUREMENT PERPENDICULAR TO THE CENTERLINE.

THE REQUIREMENTS OF SCD MT-101.90 (DROP-OFFS IN WORK ZONES) APPLY. UNLESS SPECIFIED AND ACCOUNTED FOR OTHERWISE IN THESE PLANS, FILL REPAIR AREAS WITH EITHER THE FINAL MATERIAL OR A TEMPORARY MATERIAL AT THE DISCRETION AND APPROVAL OF THE ENGINEER WHEN WORK IS NOT ACTIVE IN THE AREA OF THE REPAIR.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 253 PAVEMENT REPAIR, AS PER PLAN IS TO BE GREATER THAN 6” DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

RIC-71-10.76 TO 15.46

ITEM 253 – PAVEMENT REPAIR, AS PER PLAN (LONGITUDINAL) 100 CY

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE

THE INTENT OF THE PLANING IS TO MILL 1.75 INCHES AT THE CENTER OF PAVEMENT AT NON-CURBED AREAS UNLESS OTHERWISE SHOWN. THE PAVEMENT SLOPE SHALL BE 0.010 MINIMUM AND 0.016 PREFERRED, CONTINUOUS BETWEEN THE CROWN AND THE PROPOSED EDGELINE/SHOULDER. THE MILLING DEPTH SHALL BE CONTROLLED FROM THE CENTER OF PAVEMENT IN CONFORMANCE WITH THE ABOVE GUIDELINES.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER. IF THERE IS NO INFORMATION IN THE PLANS TO CHANGE THE SUPERELEVATION, THE INTENT IS TO MAINTAIN THE EXISTING SUPERELEVATION.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. FOR EACH CALENDAR DAY BEYOND THE 14 DAYS THAT THE ROADWAY REMAINS EXPOSED TO THE PLANED SURFACE, THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE OF \$5,000 PER DAY.

DRAINAGE SLOTS SHALL BE CUT INTO THE SHOULDER(S) AT THE LOW POINT OF EACH PLANED SECTION TO PREVENT TRAPPED WATER PUDDLES AND REFILLED DURING RESURFACING. CUTTING AND FILLING DRAINAGE SLOTS SHALL BE INCLUDED IN PAYMENT WITH ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE.

THE AMOUNT OF GRINDINGS RESULTING FROM THIS WORK MAY PRODUCE UNEXPECTED VOLUMES OF GRINDINGS DUE TO THE EXISTING TRANSVERSE SLOPE OF THE PAVEMENT.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE.

ITEM SPECIAL, INTELLIGENT COMPACTION (IC)

THIS ITEM CONSISTS OF PROVIDING AN INTELLIGENT COMPACTION (IC) SYSTEM TO MONITOR COMPACTION DURING PLACEMENT OF ASPHALT CONCRETE. INSTRUMENT ALL ROLLERS INVOLVED IN THE COMPACTION OF THE ASPHALT CONCRETE MAT. CONFORM TO THE SPECIFICATIONS FOUND IN THE SPECIAL PROVISION. NOTIFY THE ODOT OFFICE OF PAVEMENT ENGINEERING (OPE) AT LEAST TWO WEEKS PRIOR TO THE START OF IC DATA COLLECTION. INCLUDE THE COST OF ALL LABOR, EQUIPMENT, SOFTWARE, AND INCIDENTALS NECESSARY TO INSTALL THE EQUIPMENT AND ANALYZE THE DATA IN THE LUMP SUM BID FOR ITEM SPECIAL, INTELLIGENT COMPACTION (IC).

ITEM 618 – RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE), AS PER PLAN

PLACE THE RUMBLE STRIP ON THE INSIDE SHOULDER AS PER STANDARD CONSTRUCTION DRAWING BP-9.1. PLACE THE RUMBLE STRIP ON THE OUTSIDE SHOULDER 3 FEET ON CENTER MEASURED FROM THE CENTER OF THE EDGE LINE. ALL OTHER ASPECTS OF THE OUTSIDE RUMBLE STRIP ARE TO REMAIN AS PER DETAILED ON STANDARD CONSTRUCTION DRAWING BP-9.1.

ALL WORK NEEDED TO COMPLETE THIS WORK WILL BE INCLUDED IN THE CONTRACT BID PRICE PER MILE FOR ITEM 618 – RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE), AS PER PLAN AND WILL INCLUDE ALL MATERIAL, LABOR, EQUIPMENT, AND INCIDENTALS NEEDED.

ITEM SPECIAL, PAVER MOUNTED THERMAL PROFILING (PMTP)

THIS ITEM CONSISTS OF PROVIDING A PAVER MOUNTED THERMAL PROFILING (PMTP) SYSTEM TO IDENTIFY THE PRESENCE OF ANY THERMAL SEGREGATION OF AN UNCOMPACTED MAT OF HOT MIX ASPHALT. CONFORM TO THE SPECIFICATIONS FOUND IN THE SPECIAL PROVISION FOR METHODS AND PROCEDURES FOR DETERMINING THE THERMAL PROFILE USING A PMTP SYSTEM. NOTIFY THE ODOT OFFICE OF PAVEMENT ENGINEERING (OPE) AT LEAST TWO WEEKS PRIOR TO THE START OF PMTP DATA COLLECTION.

OPE WILL CONDUCT DENSITY GAUGE TESTING ON SOME OR ALL OF THERMAL PROFILE DATA SUBLOTS CATEGORIZED AS HAVING SEVERE THERMAL SEGREGATION, AS DEFINED IN THE SPECIAL PROVISION. THE RESULTS OF THIS TESTING ARE FOR RESEARCH PURPOSES AND WILL NOT RESULT IN ANY ADDITIONAL ADJUSTMENT TO THE ITEM UNIT BID PRICE. PROVIDE TRAFFIC CONTROL TO ALLOW OPE TO CONDUCT TESTING. THE DEPARTMENT WILL NOT REQUIRE MORE THAN FOUR DAYS OF ADDITIONAL LANE CLOSURES TO PERFORM DENSITY GAUGE TESTING. INCLUDE THE COST OF ALL LABOR, EQUIPMENT, SOFTWARE, AND INCIDENTALS NECESSARY TO INSTALL THE EQUIPMENT AND ANALYZE THE DATA IN THE LUMP SUM BID FOR ITEM SPECIAL, PAVER MOUNTED THERMAL PROFILING (PMTP).

ITEM 442 – ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A, (447) (PG70-22M), AS PER PLAN

FOLLOW ALL REQUIREMENTS OF THE SPECIFICATIONS WITH THE ADDITION OF THE FOLLOWING: PROVIDE LOCATION REFERENCE COORDINATES FOR EACH CORE TAKEN FOR MAT AND JOINT DENSITY ACCEPTANCE. UTILIZE A GLOBAL NAVIGATION SATELLITE SYSTEM (GNSS) RECEIVER WITH AN ACCURACY OF ± 2 INCHES (50 MM) IN THE X AND Y DIRECTIONS. USE A GNSS WITH THE ABILITY TO OBTAIN POSITIONAL CORRECTIONS USING THE ODOT REAL TIME NETWORK (RTN) OR A BASE-ROVER REAL-TIME KINEMATIC (RTK) SYSTEM. TIE DATA TO THE MOST CURRENT VERSION OF THE NATIONAL SPATIAL REFERENCE SYSTEM (NSRS) THROUGH OHIO COUNTY COORDINATE SYSTEM (OCCS) COORDINATES. USE THE RTN ONLY WHEN COVERAGE IS AVAILABLE THROUGHOUT THE PROJECT LIMITS.

ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE THE DEPTH OF REMOVAL SHALL BE AS DIRECTED BY THE ENGINEER WITH A MAXIMUM DEPTH OF 3.0”. THE MINIMUM WIDTH OF REPAIR SHALL BE 4FT. REPLACEMENT MATERIAL SHALL BE ITEM 442 ASPHALT CONCRETE INTERMEDIATE COURSE, 12.5MM, TYPE A (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE.

LONGITUDINAL IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PARALLEL TO THE CENTERLINE THAN THE MEASUREMENT PERPENDICULAR TO THE CENTERLINE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN IS TO BE A MAXIMUM OF 6” DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

RIC-71-15.46 TO 20.64

ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN (LONGITUDINAL) 1500 CY

ITEM 253 – PAVEMENT REPAIR, MISC.: FULL DEPTH PATCHING WITHOUT RESURFACING

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MINIMUM DEPTH OF 6” AND A MAXIMUM DEPTH OF 16” AND A MINIMUM WIDTH OF 4FT. FOR FULL DEPTH REPAIRS WHERE CONCRETE IS UNDERLYING ASPHALT BUT CONCRETE IS NOT BEING REPLACED AS PART OF THE REPAIR, REMOVE ALL ASPHALT TO THE TOP OF CONCRETE THEN COMPLETE FLEXIBLE REPAIR ON TOP OF EXISTING CONCRETE. REPLACEMENT MATERIAL SHALL BE ITEM 301 ASPHALT CONCRETE BASE (449) FOR AN ASSUMED THICKNESS OF 13.5” BELOW 2.5” OF ITEM 442 ASPHALT CONCRETE INTERMEDIATE COURSE, 12.5MM, TYPE A (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. THE MAXIMUM PAVEMENT LIFT THICKNESS IS 6” FOR ITEM 301.

LONGITUDINAL IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PARALLEL TO THE CENTERLINE THAN THE MEASUREMENT PERPENDICULAR TO THE CENTERLINE

THE REQUIREMENTS OF SCD MT-101.90 (DROP-OFFS IN WORK ZONES) APPLY. UNLESS SPECIFIED AND ACCOUNTED FOR OTHERWISE IN THESE PLANS, FILL REPAIR AREAS WITH EITHER THE FINAL MATERIAL OR A TEMPORARY MATERIAL AT THE DISCRETION AND APPROVAL OF THE ENGINEER WHEN WORK IS NOT ACTIVE IN THE AREA OF THE REPAIR.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 253 PAVEMENT REPAIR, AS PER PLAN IS TO BE GREATER THAN 6” DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

RIC-71-15.46 TO 20.64

ITEM 253 – PAVEMENT REPAIR, MISC.: FULL DEPTH PATCHING WITHOUT RESURFACING 50 CY