

OHIO DEPARTMENT OF TRANSPORTATION - DISTRICT THREE

Project Scope

PID **122940** Project Name **RIC US 0030 16.42 Brdg Reconstr.**

Project Overview

Scope Project Mgr.	Joseph Clark	Initial Scope Meeting	1/16/2025
Design Project Mgr.	Nick Foster	Scope Version	Original
In-House Designer(s)	Nick Foster	Letting Type	ODOT Let
Environmental Mgr.	Donald E Rostofer	Contract Type	Standard Build
Design Responsibility	ODOT District 3	PDP Path	Path 1
Design Team	Consultant	File Date	3/10/2025
Primary Work Cat.	Bridge Preservation	Federal Aid Number	N/A
Project Termini	RIC US 30 16.42 bridge	County	RIC
		City/Village	None
		Environmental Doc Type	C1

Existing 1968 (Original), 1998 (Rehab w/ Overlay), 2003 (Heat Straightening), 2006 (Minor Repairs), 2011 (Minor Plans Repairs/Sealing), 2020 (Minor Repairs).

Project Description
 RIC US 30 16.42 bridge hit in a crash and requires repairs. Replacing a portion of the damaged beam, heat-straightening the damaged beam at either side of the replacement, removing temporary struts, installing replacement crossframe assemblies, repairing concrete using epoxy-injection, and painting exposed steel.

 Emergency repair work was done on PID 122937.

Purpose & Need
 This project is in response to a bridge strike that took place November 13, 2024, at 5:13PM.

Action Items	Jared F. - Add Detour Route, Traffic Disincentives	Complete?	<input checked="" type="checkbox"/>
	Joseph C. - Coordinate with ODOT Ric. Forces for a more permanent shoulder closure		<input type="checkbox"/>
	Jared F. - Get DDD approval for US 30 closure (MOTEC not needed since less than 72 hrs)		<input type="checkbox"/>
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Bridge

Bridge 1	Bridge Location	RIC-42-1642	Preliminary Cost Estimate	\$825,000.00
	Treatment Types	NA: 210 - Steel Repair		
	Existing Bridge Information	Continuous steel beam with reinforced concrete deck and substructure. 4 spans for a total length of 289.50 feet. Bridge has been struck four times previously by over-height loads; PIDs: 79352 (2010), 97639 (2014), 110382 (2019), and 114029 (2023).		
	<i>Comment</i>			
	Alignment	Use Existing	Curb Present	No
	Profile	Use Existing	R/W Req'd	No
	Floodplain Coord.	Select...	Survey Req'd	Yes
	OHWB Determ.	Select...	Soil Borings Req'd	No
	MOT Type	Detour	Utility Relocation Req'd	No
			Hydraulic Analysis Req'd	No
	Existing	Structure Type Study Req'd	No	
General Appraisal*	7	Driveway Accomodations Req'd	No	
Sufficiency Rating	096.0	Addendum Sheet for Structure	No	
Year Built	1977	Eligible for National Historic Register	No	
Structure Type*	Steel continuous/Stringer/Multi-beam or Girder	Proposed		
Structure File No.*	7001517	→	SAME	
Feature Intersected	OVER RIC-030 -1638	→	SAME	
Design Loading	HS20	→	SAME	
Number of Spans	4	→	SAME	
Out↔Out Width*	46	ft →	SAME	
Bridge Railing Type	32" Defl Type Para (NJ Shape)	→	SAME	
Curb↔Curb Width	42.5	ft →	SAME	
Overall Length	289.5	ft →	SAME	
Approach Slab Len	25	ft →	SAME	
Vertical Clearance	14'-3" Posted; 14'-6" Actual	ft →	SAME	
Horiz. Clearance		ft →	ft	
Wearing Surf Type	Monolithic	→	SAME	
Wearing Surf Thick	1	in →	SAME	

Proposed Bridge Work (What & Why)

Due to analysis and design requirements outside the availability of the Bridge Department, these plans will be prepared by a Task Order Consultant.

'SEE MAP FOR LOCATION REFERENCES, STATIONING AND DIAGRAM OF DAMAGED AREAS

Beam Repair:

- Remove approximately 54' of Beam 1 in Span 3 from the bolted splice 14'-6" south of the centerline of Pier 3, to 20'-6" north of the centerline of Pier 2 (Station 0+54.5 to 1+08.5). Develop a plan to perform removal by sections, maintaining one lane of traffic on US30 WB at all times.
- Provide a pay item for the contractor to install temporary support to ensure structural stability until the replacement beam section is completely installed. Support from the top of deck is preferred.
- Heat straighten approx. 12'-6" each side of the removed portion of the beam per SS 849.

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- Replace the removed portion of the beam with a functionally equivalent beam section, connected using the existing splice and a designed field drilled bolted splice connection. The shape selected shall not reduce the vertical clearance over traffic. As needed, carefully remove, and subsequently replace, portions of the deck, retaining existing reinforcement, to facilitate beam splicing.

- In the retained portions of Beam 1, repair section loss due to tearing of crossframe connections by removing a segment of web including the damaged weld and replacing with a field welded steel coupon with drilled holes at the corners.

- Field paint all areas of new steel, as well areas where the paint coating is damaged by deformation, heat straightening, welding, or any other aspect of the repair work.

- Perform epoxy injection to ensure bearing connection throughout areas of top flange separation and spalling, including:

Beam 1: All of Span 3 and 24' of Span 4.

Beam 2: Portions of Span 3

Pavement: County forces have filled pavement gouges with cold patch. No work under this PID.

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Maintenance of Traffic (MOT)

MOT Item 1 of 1	MOT Type	Detour	Feature	Bridge	Coordination Needed	Yes
	CRS	RIC-30-1642	Duration	See Below (Days)	Municipality	See Below
	Disincentive	Road Closure	\$50 / minute	(Amt.)	Work Zone Speed Zone	No
	MOT Exception	Yes	DDD Approval Needed	(Desc.)	PLCS	Yes
	Conflict	Description		Route	Dates to Avoid	LEO No. of Hours	16
	See Below					

Comments

Detour US 30 WB traffic: I-71 SB to SR 39 WB to SR 13 NB, include signing along I-71 SB prior to US 30 exit and I-71 NB prior to SR 39 exit, one weekend closure will be permitted from 8pm on Friday to 5am on Monday, coordination needed with City of Mansfield, in addition to holidays the weekend closure shall not occur during the following events: major events at Mid-Ohio Sports Car Course, Inkcarceration Festival, or Richland County Fair

At least one lane of US 30 traffic shall be maintained at all times outside of the permitted weekend closure. Any lane closures shall follow the PLCS.

Detour Reed Rd to US 30 WB Ramp: Reed Rd NB to Crider Rd WB to Laver Rd SB, ramp may be closed as needed for duration of work, coordination needed with Madison Township and Richland County

Maintain one lane of traffic in each direction of Reed Road at all times (flaggers or signalized closure).

PN 127/Lane Value Contract: US 30 \$10,000 per day, Reed Rd \$1,000 per day, Reed Rd to US 30 WB Ramp \$300 per day

Forecast Information

Segment ID	2025 AADT	2045 AADT	DHV-30	K%	D%	T24%	TD%
1949543	20,000	23,000	2,300	10.0	50.8	31	23

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Environmental

Environ. Category		Code	Responsibility															
C1		I	In-House	RIC-30-16.42														
		T	OES Task Order															
	Environmental PM	C	Consultant Services															
	ROSTOFER, DONALD E	-	Not Applicable															
Cultural	Section 106 - Scoping Request Form (*)			...	I	...	...	...	...	...	...	...	...	...	...	...	...	...
	Phase 1 Hist./Arch. Survey Rpt. (If Auth.)			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Phase 1 Arch. Survey Report (If Auth.)			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
Forms	Determination Request Form			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Individual Section 4(f) Eval.			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Section 6(f) Documentation			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
Ecology	Ecological Exempt Form (*)			...	I	...	...	...	...	...	...	...	...	...	...	...	...	...
	Level 1 Ecological Survey Reports			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	UNIONID Mussel Survey Report			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Sole Source Aquifer Coordination			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
Waterway Permits	Farmland Conversion Impact Rating Form			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Permit Determination Request Package			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Concp. Stream/Wetland Mitg. Rpts.			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Section 404/401 Applications			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	USACE Pre-Constr. Notification (PCN) Applications			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Ohio EPA Isol. Wetland Permit Pre-Act. Notif. (PAN)			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Coastguard Section 9 Application			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	ACOE Section 10 Permit			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Floodplain Permit Application			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Floodplain Coordination			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...
Site	Coastal Waterway Permit			...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Regulated Mat. Review (RMR) (*)			...	I	...	...	...	...	...	...	...	...	...	...	...	...
	Phase 1 Env. Site Assess. Rpt. (If Auth.)			...	...	...	...	...	...	...	...	...	...	...	...	...	...
Air	Asbestos Survey/Inspection			...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Ozone Analysis			...	...	...	...	...	...	...	...	...	...	...	...	...	...
	MSAT Analysis			...	...	...	...	...	...	...	...	...	...	...	...	...	...
Noise	PM 2.5 Analysis			...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Traffic Noise Analysis Report			...	...	...	...	...	...	...	...	...	...	...	...	...	...
Public	Noise Barrier Public Involvement Summary			...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Public Involvement Plan			...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Public Meeting Activities			...	...	...	...	...	...	...	...	...	...	...	...	...	...
	Public Announce. (webpage, article, news release)			...	I	...	...	...	...	...	...	...	...	...	...	...	...
Underserved Population Outreach			...	...	...	...	...	...	...	...	...	...	...	...	...	...	...

Any Known Env. Concerns (ex. historic properties on Nat. Reg., wetlands, underground storage tanks, stream reloc.)

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Survey

Survey Site 1 of 1	CRS	RIC-30-1642	Asset Type	Bridge	Surveyor	ODOT District 3	
	Mapping			Count		Responsibility	
		Type 'A' Control Monument		0	Monuments	Select...	
		Type 'B' Control Monument		0	Monuments	Select...	
		Mon. Recovery for Existing CL and R/W		0	Miles	Select...	
		Monument Recovery for Property Lines		0	Owners	Select...	
		Stake R/W for Acquisition/Utilities/Tree Clearing		0	Owners	Select...	
		Pin New R/W Following Construction		0	Owners	Select...	
		Base Mapping (and Field Verify)		1	Tenths of a Mile	In-House	
		Est. Prop. Lines, Tax ID, Owners on Map		0	Owners	Select...	
		Property Owner Notification		0	Owners	Select...	
	Soil Boring Staking		0	Borings	Select...		
	Topo	Intersects	Road (Grade Sep.)	1	Tenths of a Mile	In-House	
		Topo for	Bridge Rehab Over a Road	290	46 FT	In-House	

Comments District 3 Survey has performed topographic scan to generate 3D point cloud to produce cross-sections and validate extent of deflection. This information is stored on ProjectWise at: pw:\ohiodot-pw.bentley.com:ohiodot-pw-02\Documents\01 Active Projects\District 03\Richland\122940\300-Survey\Basemaps\

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Project Schedule

Project Schedule	Date		Date	
	01/14/2025	Field Review	02/16/2026	Plan Package Received in C.O.
	01/16/2025	In-House Scope Meeting	05/21/2026	Sale
	02/03/2025	Initial Project Scope Complete	06/01/2026	Award
	08/04/2025	Authorized Design Consultant	07/01/2026	Begin Construction
	10/06/2025	Stage 2 Plans - Submitted	10/31/2026	End Construction
	10/27/2025	Stage 2 Plans - Complete		
	12/01/2025	Stage 3 Plans - Submitted		
	01/05/2026	Environmental Document Approved		
	01/05/2026	Stage 3 Plans - Complete		
	01/26/2026	Final Tracings - Submitted		
	02/02/2026	Final Tracings - Complete		
	02/04/2026	District R/W Certification		

Comments *Special request letting may be needed pending material lead time estimates*

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Funding

Split / Priority			Funding Source		Funding Source		Cost
	Name	Plan Split Code	Source 1		Source 2		
			%	Fund	%	Fund	
1 / 1		01/NHS/13	100	4SR7			\$825,000.00
Descr:		Bridge Rehabilitation. 100% State rehabilitation funds.					

Funding Summary

Project Phase	Funding Source / Description	Percent		Fiscal Year / Quarter	Phase Estimate
		Fed	State		
Preliminary Engineering	4SR7*		100	2025/Q3	\$100,000.00
Detailed Design	4SR7*		100	2025/Q3	\$25,000.00
Right of Way Services					\$0.00
Right of Way Acquisition					\$0.00
Utilities Reimbursement					\$0.00
Construction Contract	4SR7		100	2025/Q4	\$825,000.00
Construction Engineering	LABR		100	2025/Q4	\$82,500.00
Total					\$1,032,500.00

*Work to be performed by Task Order Consultant

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Field Review

Attendees

Joseph Clark, Kent Kapustar, Andy Heining, Jeff Labaki

Date

1/14/2025

Notes

Notes section containing a large empty area for text entry.

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Narrative

Document the decision process here. Why were certain treatments chosen? What was left out and why?

Following the bridge hit, it was determined that immediate heat-straightening would be cost prohibitive due to seasonal temperatures. This led to the two-project solution, with stabilization using temporary struts under PID: 122937, with full repairs to be carried out under this project.

During scoping for this PID, we consulted with Ben Neal at Ruhlin, who we reached out to because of the company's expertise and experience with the bridge during the emergency stabilization. Prompted by his suggestions, Bridge made the decision to pursue replacement of 54' of the most deformed portion of the beam, with heat straightening on the remaining deformed portions. This was decided for two major reasons: 1. Heat straightening over this length of span will be heavily time consuming and disruptive of traffic, and 2. This beam has been heat-straightened multiple times prior. While this change will require increased cost and time on the plan development side, we expect it will significantly improve the longevity of the beam, lower traffic impacts, and likely be cost-effective overall.

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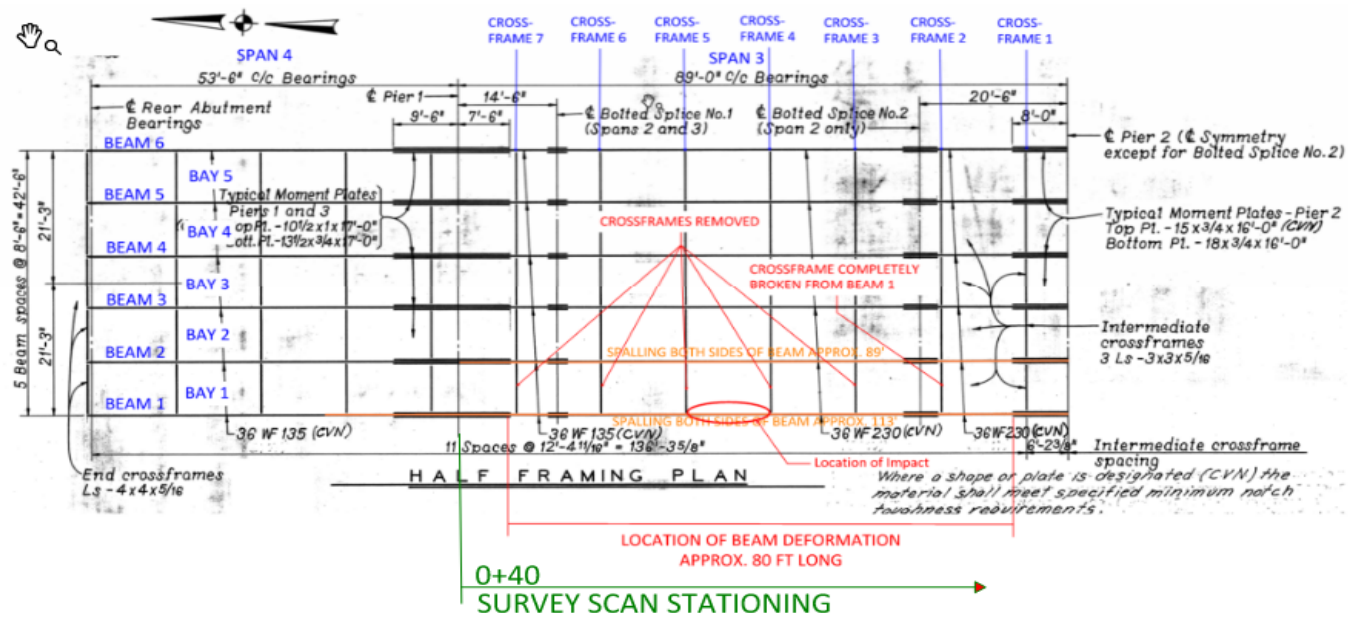
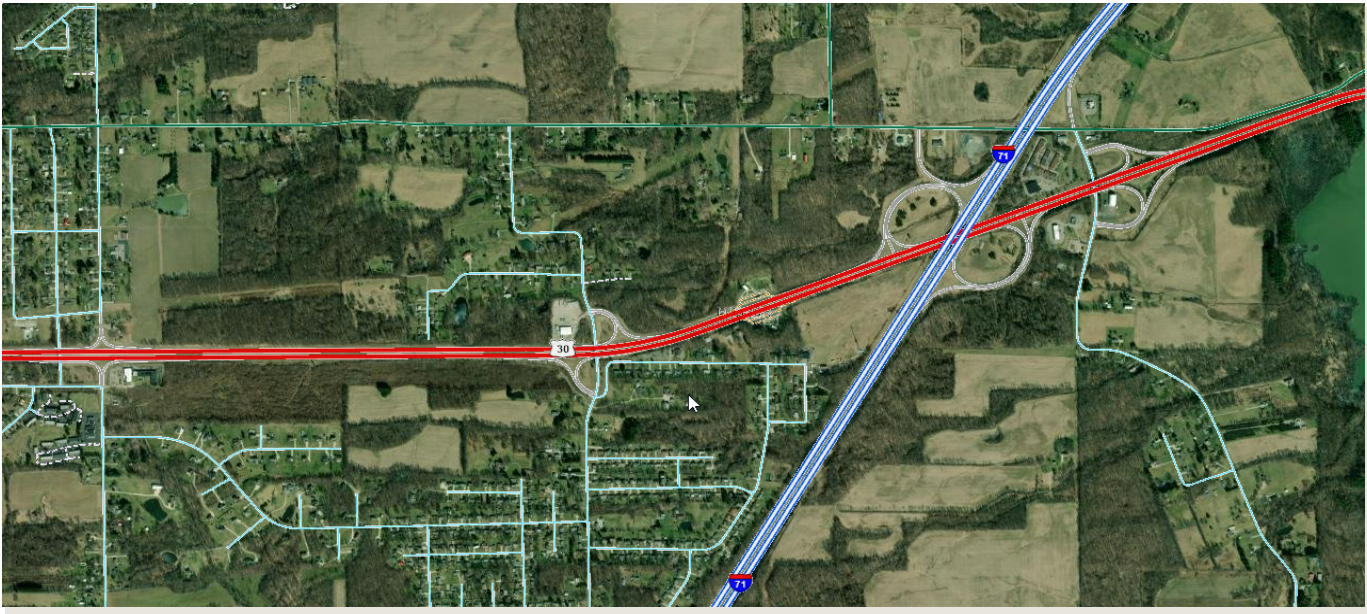
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Map

See the project overview map below.



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Signatures and Attendance				Approval		Field Visit	Scope Meeting	
		Signature	Date	Approve	Disapprove			
ELLIS Coordinator	Heidi Mertler	<i>Heidi Mertler</i>	2/14/2025	X		-	-	-
Environmental PM	Donald E Rostofer					-	-	-
Bridge Engineer	Kent Kapustar	Kent A. Kapustar	02/14/2025	X		-	-	-
Planning Engineer	Scott Ockunzzi	<i>Scott R. Ockunzzi</i>	02/18/2025	X		-	-	-
Design PM	Nick Foster	Nick Foster	02/19/2025	X		-	-	-
Roadway Engineer	Charlie Laughrey	Charlie Laughrey	2/18/25	x		-	-	-
Traffic Engineer	Julie Cichello	Julie Cichello	2/14/2025	X		-	-	-
Survey Op. Mgr.	Scott Hawkins	Scott Hawkins	02/14/2025	x		-	-	-
Utility Coordinator	John Schafrath	John Schafrath	2-18-25	X		-	-	-
Real Estate Admin.	Brad Corder	<i>Bradley S. Corder</i>	02/14/2025	X		-	-	-
Design Engineer	Kenny Knapp	<i>Kenny Knapp</i>	02/14/2025	X		-	-	-
Constr. Area Eng.	Jeffrey J Labaki	Jeff Labaki	2/18/25	X		-	-	-
Constr. Engineer	Mike Fair	Mike Fair	02/14/2025	X		-	-	-
Cap. Prog. Admin.	Matt Walter	Matt Walter	02/14/2025	X		-	-	-
Rdwy. Serv. Mgr.	Kimberly Conklin					-	-	-
Hwy. Mgt. Admin.	Eric Sheppard	Eric Sheppard	02/17/2025	X		-	-	-
County Manager	Jason Sutherland	Jason Sutherland	2/14/2025	X		-	-	-
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