

**PAVING UNDER GUARDRAIL**

THIS OPERATION SHALL INCLUDE PREPARATION OF THE GRADED SHOULDER USING 209, LINEAR GRADING AS PER PLAN, AND PAVING UNDER THE GUARDRAIL USING 441 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448), UNDER GUARDRAIL, AS PER PLAN.

ITEM 209, LINEAR GRADING AS PER PLAN, SHALL CONSIST OF EXCAVATING TOPSOIL, AND PLACING GRANULAR MATERIAL.

ALL COLLECTED DEBRIS AND TOPSOIL, INCLUDING RHIZOMES, ROOTS AND OTHER VEGETATIVE PLANT MATERIAL SHALL BE REMOVED AND DISPOSED OF AS SPECIFIED IN 105.17.

THE REMOVED MATERIAL SHALL BE REPLACED WITH COMPACTABLE GRANULAR MATERIAL CONFORMING TO 703.16 PLACED TO GRADE AS DETAILED ON THE TYPICAL SECTION OR AS APPROVED BY THE ENGINEER.

ALL EQUIPMENT, MATERIALS AND LABOR REQUIRED TO PERFORM THE WORK OUTLINED ABOVE SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 209, LINEAR GRADING, AS PER PLAN.

PAVING UNDER GUARDRAIL SHALL CONSIST OF PLACING ITEM 441 TO THE DEPTH SPECIFIED USING ONE OF THE FOLLOWING METHODS:

- METHOD A:
1. SET GUARDRAIL POSTS
  2. PLACE ITEM 441

- METHOD B:
1. PLACE ITEM 441
  2. BORE ASPHALT AT POST LOCATIONS (MAY BE OMITTED IF STEEL POSTS ARE USED)
  3. SET GUARDRAIL POSTS
  4. PATCH AROUND POSTS. THE MATERIALS USED FOR PATCHING SHALL BE AN ASPHALT CONCRETE APPROVED BY THE ENGINEER. PATCHED AREAS SHALL BE COMPACTED USING EITHER HAND OR MECHANICAL METHODS. FINISHED SURFACES SHALL BE SMOOTH AND SLOPED TO DRAIN AWAY FROM THE POSTS.

ALL EQUIPMENT, MATERIALS AND LABOR REQUIRED TO PERFORM THE WORK OUTLINED ABOVE, WITH THE EXCEPTION OF SETTING GUARDRAIL POSTS, SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 441, ASPHALT CONCRETE, INTERMEDIATE COURSE, TYPE 1, (448), UNDER GUARDRAIL, AS PER PLAN.

ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK:

ITEM 209, LINEAR GRADING, AS PER PLAN 0.45 MILES

ITEM 441, ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1 (448), (UNDER GUARDRAIL), AS PER PLAN. 147 CY

**PART-WIDTH CONSTRUCTION**

BECAUSE OF THE NECESSITY TO BUILD THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXERCISE CARE TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN THE BASE COURSES. LAP LONGITUDINAL JOINTS AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

**ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN**

RECYCLED ASPHALT PAVEMENT (RAP) SHALL BE USED IN AREAS ADJACENT TO THE PAVED BERM. THE RAP SHALL HAVE A MINIMUM PG CONTENT OF 4.5% AND MEET THE FOLLOWING GRADATION. ONCE THE STOCKPILE MEETS THE GRADATION, THE PG CONTENT OF THE RAP SHALL BE DETERMINED PER 441.03. THE RAP ANALYSIS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL 2 WEEKS PRIOR TO USE. METHOD OF MEASUREMENT SHALL BE AS PER 617.06. PLACEMENT AND COMPACTION SHALL MEET THE REQUIREMENTS OF ITEM 617. ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 617 COMPACTED AGGREGATE, AS PER PLAN.

MODIFIED GRADATION SHALL APPLY:

SIEVE	TOTAL PERCENT PASSING
1-1/2"	100
3/4"	50-100
NO. 4	35-70
NO. 30	9-33
NO. 200	0-13

AN ESTIMATED QUANTITY OF 217 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK.

**ITEM 304 AGGREGATE BASE, AS PER PLAN A**

THIS ITEM SHALL BE USED ON THE CITY STREETS.

ONLY VIRGIN CRUSHED LIMESTONE SHALL BE PERMITTED FOR THIS ITEM. GRANULATED SLAG (GS) SHALL NOT BE PERMITTED. ALL OTHER REQUIREMENTS OF SECTIONS 304 AND 703.17 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS SHALL STILL BE APPLICABLE.

**ITEM 304 - AGGREGATE BASE, AS PER PLAN B**

GRANULATED SLAG (GS) SHALL NOT BE PERMITTED FOR THIS ITEM. ALL OTHER REQUIREMENTS OF SECTIONS 304 AND 703.17 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS SHALL STILL BE APPLICABLE.

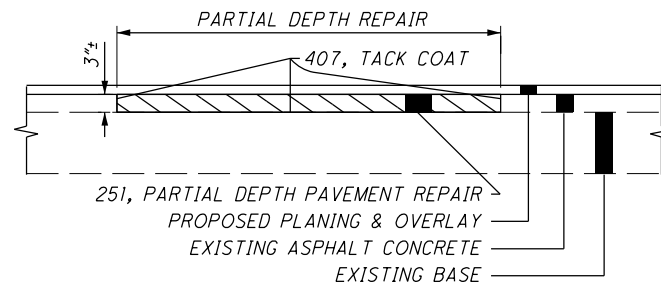
**ITEM 304 - AGGREGATE BASE, AS PER PLAN C**

THIS ITEM SHALL BE USED ON THE TEMPORARY GRAVEL PARKING LOT AT YALE ST. AND EAST SOUTH ST. LOCATION AND INCLUDES GRADING THE SITE, PLACEMENT OF THE GRAVEL, AND REMOVAL OF THE GRAVEL AFTER THE COMPLETION OF YALE ST. ALL OTHER REQUIREMENTS OF SECTIONS 304 AND 703.17 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS SHALL STILL BE APPLICABLE.

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR**

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE 1 PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING. ALSO, THIS ITEM SHALL COMMENCE WITHIN 7 DAYS OF THE COMPLETION OF MAINLINE PAVEMENT PLANING. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

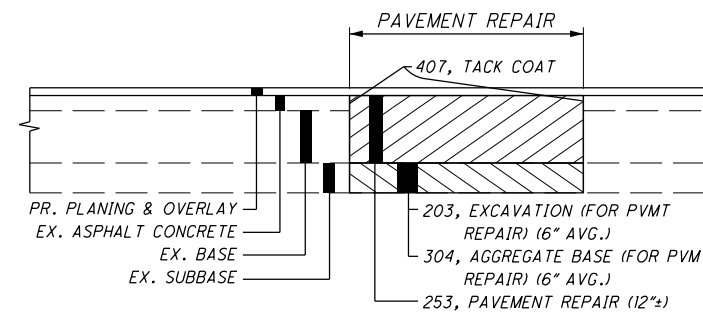
251, PARTIAL DEPTH PAVEMENT REPAIR, 650 SQ. YD.



**ITEM 253 - PAVEMENT REPAIR**

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 12"± 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING. ALSO, THIS ITEM SHALL COMMENCE WITHIN 7 DAYS OF THE COMPLETION OF MAINLINE PAVEMENT PLANING. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

253, PAVEMENT REPAIR, 650 SQ YD.



**ITEM 451 - 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI**

THIS ITEM IS FOR USE ON E. THORNTON STREET AND E. BARTGES STREET WHERE INDICATED ON THE PLANS.

WHERE THE PROPOSED PAVEMENT BUTTS WITH EXISTING PAVEMENT EITHER LONGITUDINALLY OR TRANSVERSELY, THE JOINTS BETWEEN THE EXISTING AND PROPOSED PAVEMENTS SHALL BE IN ACCORDANCE WITH ODOT STANDARD CONSTRUCTION DRAWINGS BP-2.1 AND BP-2.5.

THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI WILL BE MEASURED BY THE SQUARE YARD OF FINISHED SURFACE COMPLETE IN PLACE. THE ACCEPTED QUANTITIES OF THIS ITEM WILL BE PAID FOR AT THE CONTRACT PRICES DESIGNATED. THE CONTRACT PRICE SHALL INCLUDE ALL LABOR, MATERIAL EQUIPMENT AND INCIDENTAL ITEMS NEEDED FOR EXCAVATION, BACKFILL, REINFORCING STEEL, EXPANSION JOINT MATERIAL, CONTRACTION JOINTS AND OTHER RELATED MISCELLANEOUS ITEMS WILL NOT BE PAID FOR SEPARATELY, BUT THE COST THEREOF SHALL BE INCLUDED IN THE COST OF THE ITEM OF WHICH THEY ARE A PART. PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT PRICE FOR: THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI.

**ITEM 451 - 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI, AS PER PLAN "A"**

THIS ITEM IS FOR USE AT THE INTERSECTIONS TO RECEIVE DECORATIVE CONCRETE PAVEMENT TREATMENT. THE INTERSECTIONS TO RECEIVE THIS TREATMENT ARE AS FOLLOWS: S. MAIN STREET AT MILLER AVENUE, S. MAIN STREET AT E. THORNTON STREET, E. THORNTON STREET AT RAMP W-5 AND S. BROADWAY STREET

THIS WORK IS TO PROVIDE COLORED REINFORCED CONCRETE PAVEMENT WITHIN THE BOUNDED BY THE DECORATIVE CONCRETE HEADERS. THE COLOR SHALL BE "COAL GRAY".

THIS WORK SHALL CONFORM TO ODOT CMS ITEM 451 UNLESS OTHERWISE NOTED HEREIN.

THE INTERSECTION PAVEMENT SHALL BE BOUNDED BY A DECORATIVE CONCRETE HEADER AS SHOWN ON DETAIL SHEET 801

THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI, AS PER PLAN "A" WILL BE MEASURED BY THE SQUARE YARD OF FINISHED SURFACE COMPLETE IN PLACE. THE ACCEPTED QUANTITIES OF THIS ITEM WILL BE PAID FOR AT THE CONTRACT PRICES DESIGNATED. THE CONTRACT PRICE SHALL INCLUDE ALL LABOR, MATERIAL EQUIPMENT AND INCIDENTAL ITEMS NEEDED FOR EXCAVATION, BACKFILL, REINFORCING STEEL, EXPANSION JOINT MATERIAL, CONTRACTION JOINTS AND OTHER RELATED MISCELLANEOUS ITEMS WILL NOT BE PAID FOR SEPARATELY, BUT THE COST THEREOF SHALL BE INCLUDED IN THE COST OF THE ITEM OF WHICH THEY ARE A PART. PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT PRICE FOR: THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI, AS PER PLAN "A" (SQUARE YARD). THIS ITEM WILL BE PAID BY 100% CITY OF AKRON COSTS.

**ITEM 202 - REMOVAL MISC.: BILLBOARD REMOVAL**

REMOVE AND DISPOSE OF THE BILLBOARD IN ITS ENTIRETY INCLUDING THE FOUNDATION, AS PER 202.02. THE TWO BOLLARDS IN FRONT OF THE BILLBOARDS POST ARE TO BE REMOVED AND INCIDENTAL TO THE COST OF THE REMOVAL OF THE BILLBOARD. ALL NECESSARY ELECTRICAL WIRE DISCONNECTIONS ARE INCIDENTAL TO THE REMOVAL OF THE BILLBOARD. ALL LABOR, MATERIAL, EQUIPMENT AND INCIDENTAL ITEMS FOR THE COMPLETE REMOVAL OF THE BILLBOARD SHALL BE IN THE UNIT PRICE BID, LUMP SUM, OF ITEM 202 - REMOVAL MISC.: BILLBOARD REMOVED.

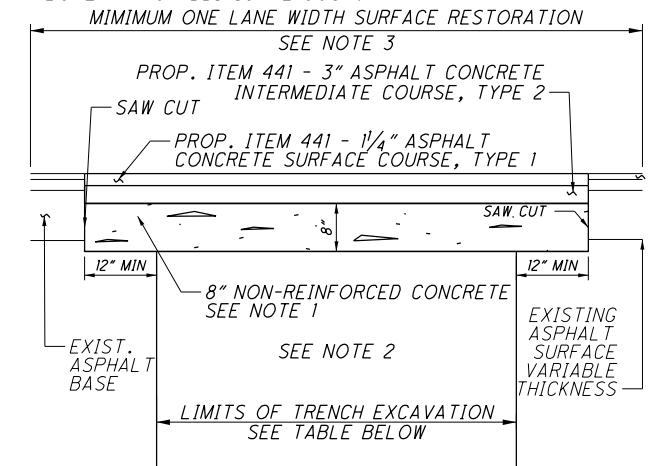
**PAVEMENT RESTORATION FOR PIPE, WATERLINE, AND STRUCTURE INSTALLATIONS AND/OR REMOVALS**

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION AND/OR REMOVAL OF PIPES AND STRUCTURES.

QUANTITIES FOR ITEM 202 - PAVEMENT REMOVED; ITEM 407 - TACK COAT; ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG 64-22; ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446); ITEM 452 - 8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS HAVE BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING THE INSTALLATION AND/OR REMOVAL OF PIPES/STRUCTURES UNDER ITEM 611 AND WATERLINES UNDER ITEM 638 AND THE REMOVAL OF PIPES UNDER ITEM 202.

THE QUANTITIES BASED ON AN ITEM 407 - TACK COAT APPLICATION RATE OF 0.075 GALLONS PER SQUARE YARD, ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG 64-22 THICKNESS OF 1.25 INCHES, AN ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446) THICKNESS OF 3 INCHES, ITEM 452 - 8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS THICKNESS OF 8 INCHES. THE QUANTITIES ARE BASED ON A PAVEMENT WIDTH THAT INCLUDES THE TRENCH WIDTH PLUS SEE NOTE 4.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.



- NOTES:
1. THE TOP OF THE NEW CONCRETE BASE SHALL NOT BE CONSTRUCTED HIGHER THAN THE EXISTING ASPHALT BASE.
  2. BACKFILL MEETING THE REQUIREMENTS OF 611.
  3. LIMITS MAY BEGIN AT FACE OF CURB OR EDGE OF CURB AND GUTTER.
  4. WHEN A TRENCH CROSSES THE ROADWAY PERPENDICULARLY, THE PAVEMENT BASE SHALL EXTEND A MINIMUM OF 12" OUTSIDE OF THE TRENCH LIMITS AND THE ASPHALT SURFACE SHALL EXTEND A MINIMUM OF 12" OUTSIDE OF THE PAVEMENT BASE LIMITS.

TRENCH DIMENSIONS		
MATERIAL	SIDE TRENCH WIDTH (SW)	WIDTH (W)
Long Span	2'	Span+2(12)
Concrete	0.165 X Span	1.33 X Span
Metal & Plastic	0.5 + 0.125 X Span	1.25 X Span + 1

REV. BY	DATE	DESCRIPTION
KMK	4/21/16	ADDED BILLBOARD REMOVAL

**ITEM 622 - PORTABLE BARRIER PLACEMENT (I-76 / I-77)**

DURING THE PLACEMENT OF THE PORTABLE BARRIER, TRAFFIC WILL BE PROHIBITED FROM OCCUPYING THE TRAVEL LANE ADJACENT TO THE BARRIER. THE BARRIER WILL BE PLACED AT NIGHT PER THE WORK HOUR RESTRICTION NOTE AND IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE CHART. THE CLOSURE OF THE ADJACENT LANE WILL BE PER THE STANDARD DRAWING MT-95.30. THE CONTRACTOR WILL SUBMIT A PLAN TO THE ENGINEER FOR APPROVAL SEVEN (7) DAYS IN ADVANCE OF THE PLANNED LANE CLOSURE. WORK WILL NOT BEGIN UNTIL APPROVAL OF THE PLANS HAS BEEN GRANTED. ALL COSTS INVOLVED IN PLACING THE PORTABLE CONCRETE BARRIER WILL BE INCLUDED IN THE CONTRACT PRICE BID FOR ITEM 622 PORTABLE CONCRETE BARRIER.

**614 WORK ZONE PAVEMENT MARKINGS, AS PER PLAN**

IN ADDITION TO THE REQUIREMENTS OF 614.11, ALL CLASS 1 EDGE LINES, LANE LINES, CENTER LINES AND DOTTED LINES SHALL BE 6 INCHES WIDE AND CHANNELIZING MARKINGS SHALL BE 8 INCHES WIDE. THE APPLICATION RATES FOR THE 6 INCH LINES SHALL BE 1.5 TIMES THE RATES SPECIFIED FOR 4 INCH LINES IN TABLE 614.11-1.

**ITS MESSAGE BOARDS (I-76 / I-77)**

THE EXISTING ITS MESSAGE BOARDS IN THE VICINITY OF THE PROJECT WILL BE UTILIZED TO PROVIDE SUPPLEMENTAL INFORMATION TO THE TRAVELING PUBLIC. THE CONTRACTOR WILL NOTIFY THE PROJECT ENGINEER ONE (1) WEEK IN ADVANCE OF ANY PHASE CHANGE. THE PROJECT ENGINEER WILL COORDINATE WITH EITHER LISA BOSE AT 330-786-4817 OR BRENT KOVACS AT 330-786-2208 TO GET THE ITS MESSAGE BOARDS ADJUSTED.

**ITEM 614 - BUSINESS ENTRANCE (M4-H15) SIGN, AS PER PLAN (LOCAL)**

THE BUSINESS ENTRANCE (M4-H15) SIGN SHOULD BE PROVIDED AT EACH TEMPORARILY RELOCATED COMMERCIAL DRIVEWAY FOR WHICH THE RELOCATION IS NOT OBVIOUS TO THE MOTORIST. THE PROJECT ENGINEER SHALL DETERMINE WHETHER OR NOT THE DRIVEWAY RELOCATION IS, OR IS NOT, OBVIOUS AND WHETHER OR NOT A SIGN SHOULD BE PROVIDED. ONLY ONE SIGN PER BUSINESS SHALL BE PERMITTED. THE SIGN SHALL BE 36 INCH X 48 INCH IN SIZE WITH TYPE G OR TYPE H ORANGE RETROREFLECTIVE SHEETING. THE SIGN LEGEND SHALL BE PLACED ON BOTH SIDES OF THE SIGN (BACK TO BACK). THE SIGN SHALL HAVE THE STANDARD M4-H15 LEGEND WITH THE WORD "BUSINESS" ON THE TOP LINE, EXCEPT UNDER UNUSUAL CIRCUMSTANCES WHERE IT MAY NOT BE INTUITIVE THAT A DRIVEWAY SERVES A SPECIFIC BUSINESS. IN SUCH UNUSUAL CASES, THE ACTUAL BUSINESS NAME MAY BE SUBSTITUTED FOR THE WORD "BUSINESS".

THE SIGN SHALL BE MOUNTED ON TWO NO. 3 POSTS OR ON TEMPORARY POSTS IN ACCORDANCE WITH SCD MT-105.10 AND IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION. THE SIGN SHALL BE CLEARLY VISIBLE AND SHALL CLEARLY IDENTIFY THE LOCATION OF THE DRIVEWAY. THE SIGN SHOULD BE POSITIONED AT 90 DEGREES TO THE DIRECTION(S) OF TRAFFIC. THE SIGN MAY NEED TO BE MOVED FOR EACH PHASE OF THE MAINTENANCE OF TRAFFIC OPERATIONS.

PAYMENT FOR ALL COSTS ASSOCIATED WITH MANUFACTURING, MOUNTING, RELOCATING, AND REMOVING THE SIGN, INCLUDING ALL LABOR, MATERIALS AND EQUIPMENT SHALL BE INCLUDED IN THE CONTRACT PRICE PER EACH FOR ITEM 614-BUSINESS ENTRANCE SIGN.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS ITEM.

ITEM 614 BUSINESS ENTRANCE SIGN, AS PER PLAN 20 EACH

**ITEM 614 - COVERING OF SIGNS (I-76 / I-77)**

THE CONTRACTOR WILL COVER ANY EXISTING AND/OR PROPOSED SIGN INSTALLATION WHICH IS IN CONFLICT WITH THE MAINTENANCE OF TRAFFIC PLANS. THE SIGNS SHALL BE COVERED IN SUCH A MANNER AS TO AVOID DAMAGING THE SIGN WHEN THE COVER IS REMOVED. THE COVER SHALL BE TOTALLY OPAQUE AND COVERS THE ENTIRE SIGN FACE. THE USE OF ADHESIVE TAPE APPLIED DIRECTLY TO THE SIGN FACE IS STRICTLY PROHIBITED. THE CONTRACTOR WILL PROVIDE ALL OF THE "CLOSED" PLAQUES NECESSARY. THE OVERLAY MAY BE RIVETED TO THE SIGN. THE CONTRACTOR SHALL PROVIDE ALL OF THE PLAQUES, SIGNS, AND SIGN PANELS NECESSARY.

UNLESS SEPARATELY ITEMIZED IN THE PLAN, THE LUMP SUM PRICE BID FOR ITEM 614-MAINTAINING TRAFFIC SHALL INCLUDE ALL COSTS NECESSARY TO COVER AND/OR MODIFY CONFLICTING SIGN INSTALLATIONS.

**ITEM 625 - LIGHT POLE MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION (I-76 / I-77)**

CONTRACTOR SHALL REMOVE AND SALVAGE THE EXISTING MEDIAN LIGHT POLE AND LUMINAIRE. THE SALVAGED ITEMS SHALL BE STORED BY THE CONTRACTOR FOR THE DURATION OF THE NEED FOR THE CROSSOVER THROUGH THE MEDIAN. THE FOUNDATION SHALL BE REMOVED TO A DEPTH OF 12" BELOW PROPOSED GUTTER AND PER REQUIREMENTS OF ODOT CMS 202. THE CONTRACTOR SHALL CONSTRUCT A NEW 24" X 10' DEEP FOUNDATION, INCLUDING NEW PULL BOX, ANCHOR BOLTS AND MOUNTING PLATE, ADJACENT TO THE EXISTING FOUNDATION TO REMAIN BELOW GRADE. THE FOUNDATION AND PULL BOX SHALL BE CONSTRUCTED PER SCD HL-20.13. THE SALVAGED POLE AND LUMINAIRE SHALL BE RE-INSTALLED ON THE NEW FOUNDATION AND CONNECTED TO THE EXISTING LIGHTING CIRCUITRY. ALL MATERIALS AND LABOR REQUIRED FOR THE COMPLETE INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH OF ITEM 625 LIGHT POLE MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION. CONTRACTOR SHALL MAINTAIN EXISTING LIGHTING CIRCUIT, UNTIL NEW CIRCUIT IS INSTALLED.

**CONSTRUCTION ACCESS POINTS**

THE CONTRACTOR SHALL USE THE DESIGNED CONSTRUCTION ACCESS POINTS SHOWN ON SHEETS 196, 201, 206, AND 211, FOR PHASE 1, STAGE 1 AND STAGE 2 OF BOTH WOLF LEDGES AND GRANT STREET BRIDGE CONSTRUCTION. FOR ACCESS INTO THE CONSTRUCTION ZONE DURING THESE PHASES, THREE LANES SHALL BE MAINTAINED MONDAY - FRIDAY, 6 AM TO 8 AM AND 3 PM TO 6 PM. ONE LANE CAN BE CLOSED ANY TIME ON SATURDAY AND SUNDAY.

**CONSTRUCTION RESTRICTIONS**

THE FOLLOWING DISCUSSION OF CONSTRUCTION SEQUENCING RESTRICTIONS IS PROVIDED FOR THE CONTRACTOR'S BENEFIT WHEN PLANNING HIS/HER CONSTRUCTION WORK TASK SEQUENCING. ITEMS UNDER PERMITTED SEQUENCING ARE NON-BINDING AND COULD BE MODIFIED BY THE CONTRACTOR IF AN ALTERNATE MAINTENANCE OF TRAFFIC METHOD IS SELECTED, ANY APPROVED ALTERNATE MUST INCLUDE THESE RESTRICTIONS.

**RESTRICTIONS**

1. THE LOCAL STREET CONSTRUCTION MUST BE COMPLETED TO THE POINT THAT BROADWAY TRAFFIC CAN BE PLACED ONTO THE WIDENED/RELOCATED MAIN STREET BEFORE PHASE 2 CAN BEGIN.
2. EXCEPT FOR THE 2 WEEK DETOUR FOR THE CONSTRUCTION OF THE TIE-IN FOR RAMP W-5, WB EXIT TO DOWNTOWN (RAMP W-5A AND W-5) SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.
3. RAMP W-5 MUST BE COMPLETED AND OPEN BEFORE BEGINNING PHASE 3.
4. EXISTING RAMP W-9 SHALL NOT BE CLOSED UNTIL THE NEW RAMP W-9 IS COMPLETE AND OPEN.
5. WOLF LEDGES PARKWAY MUST BE COMPLETED AND OPEN TO TRAFFIC BEFORE CONSTRUCTION WHICH RESTRICTS LANES CAN BEGIN ON GRANT STREET.
6. PHASE 2 MAY BEGIN ONCE STEEL IS SET AND FALSWORK IN PLACE FOR THE GRANT STREET STRUCTURE REPLACEMENT (SUM-76-1085). THE INTENT IS TO HAVE THE GRANT STREET STRUCTURE COMPLETE TO THE POINT OF NOT REQUIRING ANY ZONES ON I-76 BEFORE PHASE 2 BEGINS.
7. BUILDING DEMOLITION WORK, INCLUDING RESTORATION OF THE SITES, SHALL BE COMPLETED BY SEPTEMBER 30, 2016.
8. WORK ON WOLF LEDGES AND GRANT STREET THAT REQUIRES AND TRAFFIC CONTROL ON I-76 SHALL NOT BEGIN BEFORE AUGUST 1, 2016.
9. THERE ARE ADJACENT BRIDGES, WITHIN CLOSE PROXIMITY TO WOLF LEDGES AND GRANT STREET THAT WILL ALSO BE UNDER CONSTRUCTION DURING THE FALL OF 2016. THE CONTRACTOR SHALL COORDINATE WITH THE ADJACENT CONTRACTORS TO MAKE SURE THE I-76 CONSTRUCTION ZONES FOR THIS PROJECT MATCH IN AND DO NOT CONFLICT WITH THE ZONES FOR THE ADJACENT PROJECTS.
10. THE WOLF LEDGES AND GRANT STREET STRUCTURES AND APPROACH ROADWAY WORK SHALL BE COMPLETE AND OPEN TO TRAFFIC BY OCTOBER 31, 2017.
11. PHASE 2 SHALL BE COMPLETED BY AUGUST 31, 2018.
12. PHASE 3 SHALL BE COMPLETED BY AUGUST 31, 2019.

DATES PROVIDED ABOVE SHALL BE CONSIDERED INTERIM COMPLETION DATES AND SHALL BE SUBJECT TO DISINCENTIVE PENALTY IN THE AMOUNT OF \$1,500 PER DAY THAT THE WORK IS NOT COMPLETE BEYOND INTERIM COMPLETION DATE.

**ITEM 630 - SIGNING MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER (I-76 / I-77)**

CONTRACTOR SHALL REMOVE AND SALVAGE THE EXISTING BARRIER-MOUNTED MILE MARKER PER REQUIREMENTS OF ODOT CMS 202. THE CONTRACTOR SHALL STORE THE EXISTING SIGNS FOR THE DURATION OF THE NEED FOR THE CROSSOVER THROUGH THE MEDIAN. THE EXISTING MOUNTING AND POST SHALL BE REMOVED AND REPLACED WITH NEW MOUNTING AND POST PER DETAILS ON SHEET 1135. ALL MATERIALS AND LABOR REQUIRED FOR THE COMPLETE INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH OF ITEM 630 SIGNING MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER.

**MAINTENANCE OF TRAFFIC AND ACCESS TO PROPERTIES**

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC AND ACCESS TO PROPERTIES.

ITEM 410, TRAFFIC COMPACTED SURFACE, TYPE A OR B 500 CY

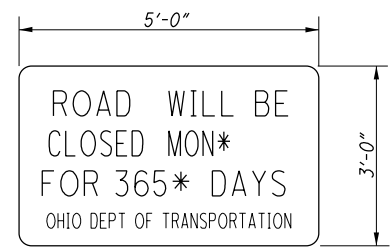
ITEM 410, TRAFFIC COMPACTED SURFACE, TYPE C 500 CY

ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 100 CY

DRIVES TO REMAIN OPEN AT ALL TIMES UNLESS NOTED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES USING PARTIAL WIDTH CONSTRUCTION.

**ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)**

NOTICE OF CLOSURE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD OR RAMP CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.



\* CONTRACTOR SHALL USE ACTUAL CLOSURE DATE AND DURATION.

THE COST OF THE NOTICE OF CLOSURE SIGN IS CONSIDERED TO BE INCIDENTAL TO AND INCLUDED IN ITEM 614 - MAINTAINING TRAFFIC.

KMK	4/25/16	CONSTRUCTION RESTRICTIONS NOTE (6) REVISED
KMK	4/18/16	COMPLETION DATE FOR GRANT AND WOLF
MEP	2/12/16	A.C. FOR MOT QUANTITY REVISION
<b>REV. BY</b>	<b>DATE</b>	<b>DESCRIPTION</b>
DATE COMPLETED		

**LIMITATION OF STREET CLOSURES**

THE CONTRACTOR SHALL COMPLETE ALL CONSTRUCTION AND SAFETY ITEMS AND HAVE THE DESIGNATED SECTIONS OPEN TO UNRESTRICTED TRAFFIC WITH IN THE CALENDER DAYS SPECIFIED.

**DESCRIPTION OF LOCATION COMPLETION DAYS**

PHASE 1, STAGE 1A  
BROADWAY STREET AND MILLER AVENUE CLOSURE: 10 CALENDAR DAYS

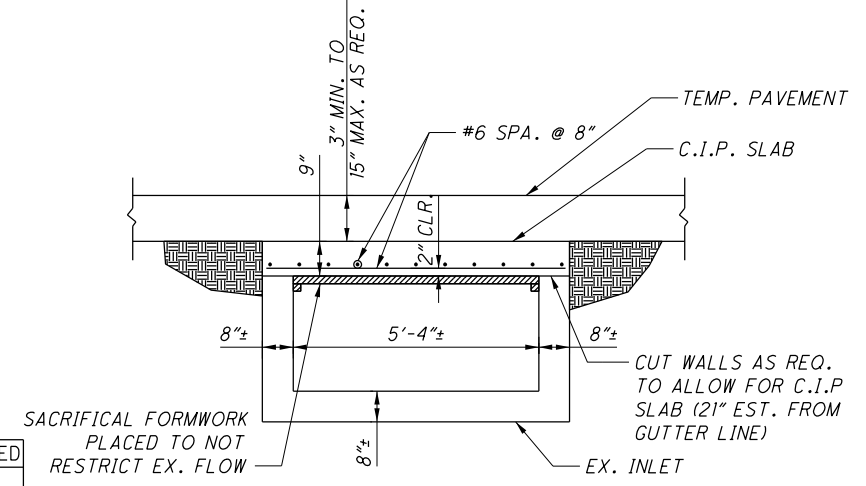
PHASE 1, STAGE 1B  
BROADWAY STREET AND MILLER AVENUE CLOSURE: 10 CALENDAR DAYS

PHASE 1, STAGE 1C  
YALE STREET CLOSURE: 21 CALENDER DAYS

PHASE 2  
BROADWAY STREET AND THORNTON INTERSECTION AVENUE CLOSURE: 14 CALENDAR DAYS. NO CLOSURE OF THE BROADWAY/THORNTON INTERSECTION MAY TAKE PLACE UNTIL RAMP W-5 (WB TO NB) BRIDGE IS COMPLETE AND PAVEMENT COMPLETED TO STA 575+75+/- SO THAT THIS MOVEMENT CAN BE OPENED TO TRAFFIC AFTER THE 14 DAY INTERSECTION CLOSURE. THE EXISTING RAMP E MUST REMAIN OPEN PRIOR TO THIS CLOSURE.

**ITEM 611 - INLET MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET (I-76 / I-77)**

CONTRACTOR SHALL SAWCUT THE EXISTING BARRIER MEDIAN INLET TO A DEPTH OF 21" BELOW PROPOSED GUTTER AND PER REQUIREMENTS OF ODOT CMS 202. THE CONTRACTOR SHALL PROTECT THE EXISTING PIPE INVERT WITH A CONCRETE SLAB AND BACKFILL WITH PROPOSED SHOULDER PAVEMENT SECTION FOR THE DURATION OF THE NEED FOR THE CROSSOVER THROUGH THE MEDIAN. THE SLAB SHALL BE AS PER DETAIL SHOWN. CONCRETE SHALL BE CMS ITEM 511, CLASS OC1 MISC. WITH 4 KSI DESIGN STRENGTH. REINFORCING SHALL BE PER CMS ITEM 509 NO SEPARATE PAYMENT WILL BE MADE FOR TIME, CONCRETE AND REBAR AND OTHER MISCELLANEOUS ITEMS REQUIRED TO COMPLETE THE WORK AS IT IS INCIDENTAL OT THIS ITEM. THE EXISTING INVERT AND ONE SEGMENT OF EXISTING PIPE SHALL BE REMOVED AND REPLACED PER SCD I-2.4. WHEN THE TIME COMES TO REPLACE THE MEDIAN BARRIER WITH NEW BARRIER THE EXISTING MEDIAN INLET MUST BE REMOVED AND THE NEW MEDIAN INLET RECONSTRUCTED IN IT'S ORIGINAL LOCATION AS APPROVED BY THE ENGINEER. ALL MATERIALS, LABOR, AND EQUIPMENT REQUIRED FOR THE COMPLETE INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH OF ITEM 611 INLET MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET.



**DETAIL FOR ITEM 611 - INLET MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET (I-76 / I-77)**

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PLAN SHEET NO.	REFERENCE NO.	STATION		SIDE	PARTICIPATION (01/IMS/PV)	202	202	202	202	202	202	202	202	202	202	202	202	202												
		STRUCTURE REMOVED	PAVEMENT REMOVED			PAVEMENT REMOVED, AS PER PLAN	WEARING COURSE REMOVED	WALK REMOVED	STEPS REMOVED	CONCRETE BARRIER REMOVED	TRAFFIC ISLAND REMOVED	CURB REMOVED	CURB AND GUTTER REMOVED	GUARDRAIL REMOVED	REMOVAL MISC.: WALL REMOVED	REMOVAL MISC.: UTILITY POLE FOUNDATION	REMOVAL MISC.: BILLBOARD REMOVED	LS	SY	SY	SY	SF	FT	FT	SY	FT	FT	FT	FT	EACH
		FROM	TO			LS	SY	SY	SY	SF	FT	FT	SY	FT	FT	FT	FT	FT												
		EX. BACHTEL ST.																												
786	R504	19+44.11	20+82.64	LT	01/IMS/PV		774																							
786	R505	18+64.14	21+65.53	LT	01/IMS/PV					2099																				
786	R506	18+64.14	21+65.53	LT	01/IMS/PV									333																
786	R507	18+64.08	21+51.33	LT/RT	01/IMS/PV		815																							
786	R508	20+53.46	21+18.44	RT	01/IMS/PV		142																							
786	R509	18+64.08	21+34.04	RT	01/IMS/PV					1573																				
786	R510	18+64.09	20+63.92	RT	01/IMS/PV									279																
		EX. YALE ST.																												
787	R511	10+03.01	13+09.99	LT	01/IMS/PV					1542																				
787	R512	10+04.67	13+21.57	LT/RT	01/IMS/PV		833																							
787	R513	10+04.67	13+10.25	LT	01/IMS/PV									306																
787	R514	10+01.09	13+11.04	RT	01/IMS/PV					2537																				
787	R515	10+01.09	20+90.10	RT	01/IMS/PV									309																
(THIS SHEET)							2564			7751				1227																
SUBTOTAL (CARRIED FROM SHEET 313 )							34915	1452	11999	1452		2350		4608		2568														
SUBTOTAL (CARRIED FROM SHEET 314 )							34750	2166	1857	6271		2160		3031		2870														
SUBTOTAL (CARRIED FROM SHEET 315 )							24121			14145		1060		3098		1413														
SUBTOTAL (CARRIED FROM SHEET 316 )						LS	12222			5780	3		44	2925		153														
SUBTOTAL (CARRIED FROM SHEET 317 )							7597			19068	3			2795				6	LS											
SUBTOTAL (CARRIED FROM SHEET 318 )							5561	4181		27269				3937			87	6												
SUBTOTAL (CARRIED FROM SHEET 319 )							4207	1321		14875				3362			21													
SUBTOTAL (CARRIED FROM SHEET 320 )							8127			13068		381		2253	1699	61														
SUBTOTAL (CARRIED FROM SHEET 321 )							10771			15397				2351		55														
SUBTOTAL (CARRIED FROM SHEET 322 )							7167			13194				2620																
SUBTOTAL (CARRIED FROM SHEET 323 )							5253			16735				2161		58														
SUBTOTAL (CARRIED FROM SHEET 324 )							4379	1549		7488	7		9	2765																
TOTALS CARRIED TO GENERAL SUMMARY						LS	161634	10669	13856	162493	13	5951	53	37133	1699	7120	166	12	LS											

KMK	4/21/16	ADDED BILLBOARD REMOVAL
JEM	4/7/16	BP GAS STATION PARCEL/ACCESS REVISION
REV. BY	DATE	DESCRIPTION
DATE COMPLETED		

CALCULATED BL W CHECKED KMK	<b>ROADWAY REMOVAL SUBSUMMARY</b>	<b>SUM - 76 - 10.00</b>
--------------------------------------	-----------------------------------	-------------------------

325  
1822

**CROSS REFERENCES**

FOR SUBSUMMARIES AND CALCULATIONS, SEE SHEETS 304-369  
 FOR RAMP W-5 PROFILE, SEE SHEET 418  
 FOR RAMP W-5 CROSS SECTIONS, SEE SHEETS 422 - 430  
 FOR SUPERELEVATION TABLES, SEE SHEETS 661 - 667  
 FOR RAMP W-5 DRAINAGE PLANS, SEE SHEETS 861 - 865  
 FOR REMOVAL PLAN, SEE SHEET 733  
 FOR DRAINAGE PLAN AND PROFILE, SEE SHEETS 852 - 963  
 FOR WATER WORK PLAN AND PROFILE, SEE SHEETS 1000- 1033  
 FOR GRADING DETAILS, SEE SHEETS 793 - 794  
 FOR BRIDGE SUM-76-1051 DETAILS, SEE SHEETS 1422- 1464  
 FOR WALL 8 PLANS AND DETAILS, SEE SHEETS 1670- 1673

**B CONST. RAMP W-5**

AA P.I. Sta. 581+90.39  
 $\Delta = 70^{\circ} 50' 03''$  (L.T.)  
 $Dc = 16^{\circ} 30' 00''$   
 $R = 347.25'$   
 $T = 246.93'$   
 $L = 429.30'$   
 $E = 78.85'$   
 $e_{max} = 0.060$   
 P.C. Sta. 579+43.46  
 C.S. Sta. 583+72.75  
 DESIGN SPEED = 35 mph

BEGIN TYPE 6 CURB  
 (MATCH EXISTING)  
 +40.78, 26.23' RT.  
 E. VORIS ST.

END TYPE 6 CURB  
 (MATCH EXISTING)  
 +52.81, 20.08' LT.  
 E. VORIS ST.

CALCULATED  
 MJT  
 CHECKED  
 KMK

0 20 40  
 HORIZONTAL  
 SCALE IN FEET

REV. BY	DATE	DESCRIPTION
KMK	4/21/16	"BLOW-UP" DETAIL ADDED
		DATE COMPLETED

**PLAN - RAMP W-5  
 STA. 575+50 TO STA. 580+50**

**SUM-76-10.00**

417  
 1822

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"BLOW-UP" DETAIL  
 CAUTION DURING  
 CONSTRUCTION

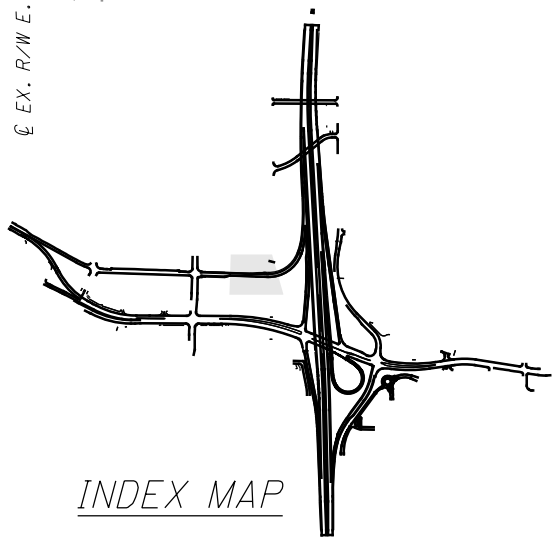
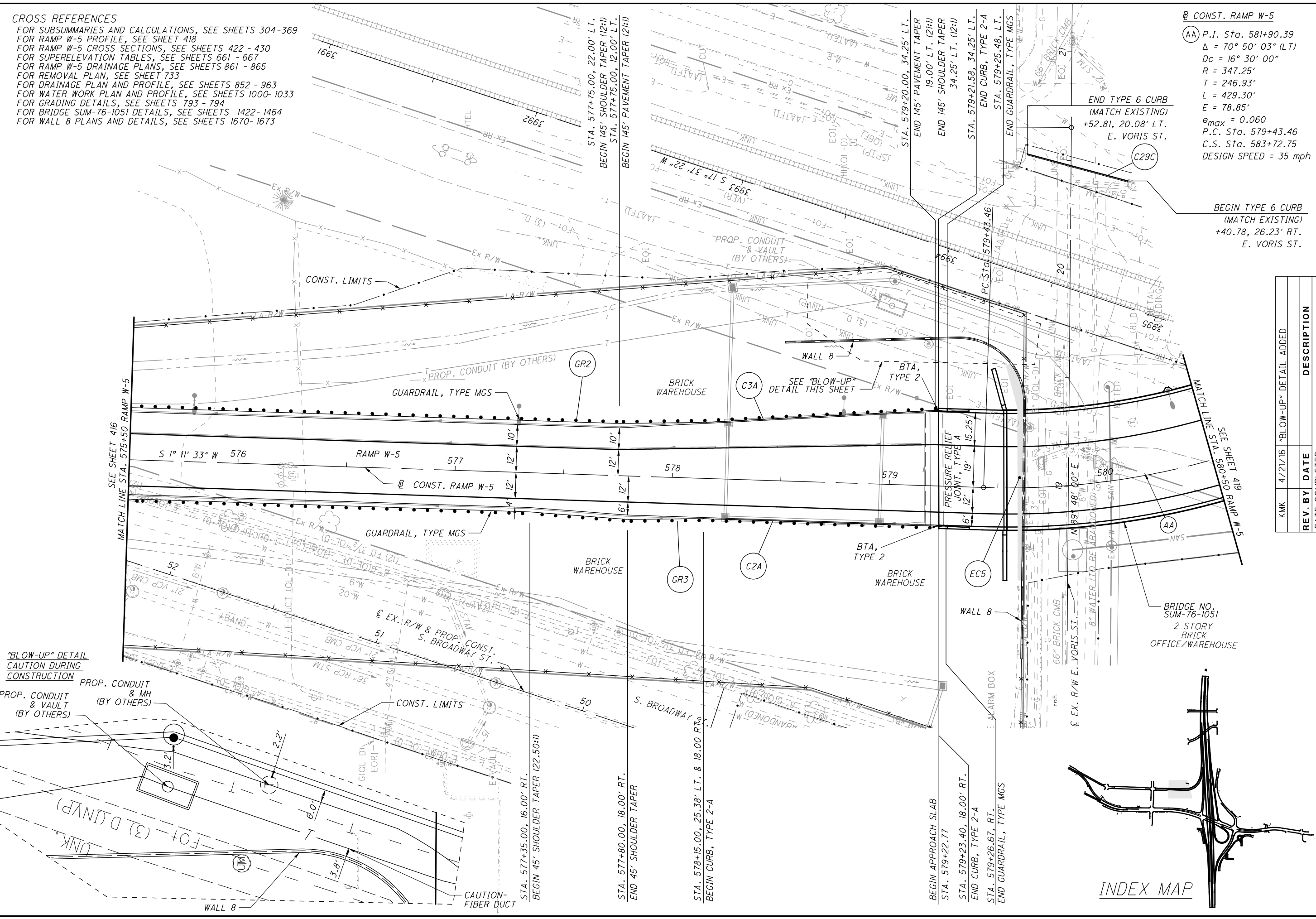
PROP. CONDUIT  
 & VAULT  
 (BY OTHERS)

PROP. CONDUIT  
 & MH  
 (BY OTHERS)

UNK  
 FOT - (3) D (GNVP)

SEE SHEET 416  
 MATCH LINE STA. 575+50 RAMP W-5

SEE SHEET 419  
 RAMP W-5  
 MATCH LINE STA. 580+50



INDEX MAP

CONST. RAMP W-5

AA P.I. Sta. 581+90.39  
 $\Delta = 70^\circ 50' 03''$  (LT)  
 $Dc = 16^\circ 30' 00''$   
 $R = 347.25'$   
 $T = 246.93'$   
 $L = 429.30'$   
 $E = 78.85'$   
 $e_{max} = 0.060$   
 P.C. Sta. 579+43.46  
 C.S. Sta. 583+72.75  
 DESIGN SPEED = 35 mph

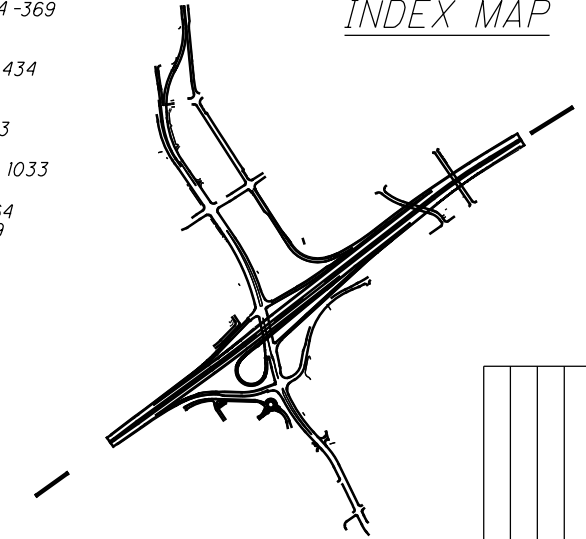
BB P.I. STA. 584+29.07  
 $\Delta = 14^\circ 03' 45''$  (LT)  
 $Dc1 = 16^\circ 30' 00''$   
 $R1 = 347.25'$   
 $Dc2 = 2^\circ 15' 00''$   
 $R2 = 2,546.48'$   
 $Ls = 150.00'$   
 $p = 2.33'$   
 $\Delta1 = 1^\circ 41' 07''$   
 $\Delta2 = 12^\circ 22' 38''$   
 $T1 = 56.32'$   
 $T2 = 94.35'$   
 $e1 = 0.060$   
 $e2 = 0.038$   
 C.S. Sta. 583+72.75  
 S.C. Sta. 585+22.75

"BLOW-UP" DETAIL  
CAUTION DURING  
CONSTRUCTION

CROSS REFERENCES

FOR SUBSUMMARIES AND CALCULATIONS, SEE SHEETS 304-369  
 FOR RAMP W-5 PROFILE, SEE SHEET 421  
 FOR RAMP W-5A CROSS SECTIONS, SEE SHEETS 422 - 430  
 FOR RAMP W-5A PLAN AND PROFILES, SEE SHEETS 432 - 434  
 FOR SUPERELEVATION TABLES, SEE SHEETS 661 - 667  
 FOR REMOVAL PLAN, SEE SHEET 734  
 FOR DRAINAGE PLAN AND PROFILE, SEE SHEETS 852 - 963  
 FOR RAMP W-5 DRAINAGE PLANS, SEE SHEETS 861 - 865  
 FOR WATER WORK PLAN AND PROFILE, SEE SHEETS 1000-1033  
 FOR GRADING DETAILS, SEE SHEETS 793 - 794  
 FOR BRIDGE SUM-76-1051 DETAILS, SEE SHEETS 1422-1464  
 FOR WALL 7 PLANS AND DETAILS, SEE SHEETS 1655-1669

INDEX MAP



CALCULATED	MJT	CHECKED	KMK
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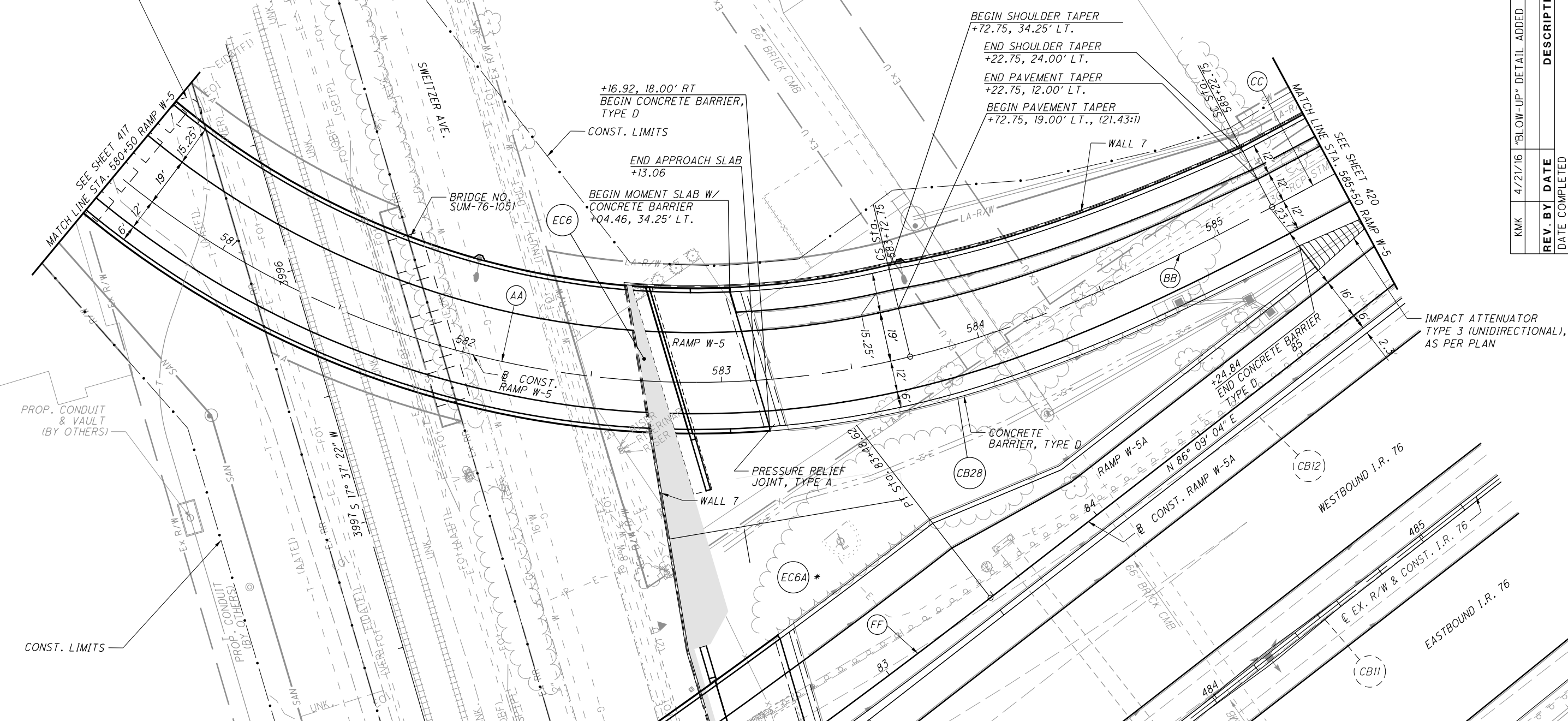
REV. BY	DATE	DESCRIPTION
KMK	4/21/16	"BLOW-UP" DETAIL ADDED

PLAN - RAMP W-5  
 STA. 580+50 TO STA 585+50

SUM-76-10.00

419  
 1822

SEE "BLOW-UP"  
DETAIL THIS SHEET



\* EC6A IS PROVIDED FOR USE  
 IN THE INFIELD BETWEEN RAMPS  
 W-5 & W-5A AND WALL 7.

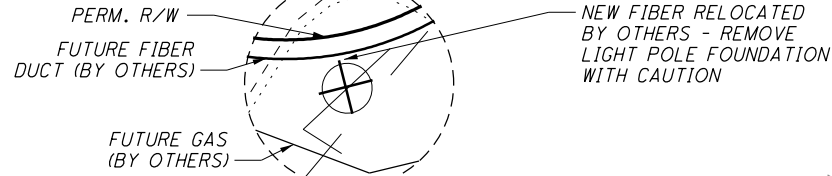
I:\Projects\Main\_Roadway\Roadway\Sheets\77269GP019.dgn 4/22/2016 3:40:44 PM kyle\_koppes

PROP. R/W & CONST. RUSSELL AVE.

(R) P.I. Sta. 10+84.71  
 $\Delta = 10^\circ 17' 59''$  (LT)  
 $D_c = 11^\circ 15' 00''$   
 $R = 509.30'$   
 $T = 45.90'$   
 $L = 91.55'$   
 $E = 2.06'$   
 $e_{max} = NC$   
 P.C. Sta. 10+38.81  
 P.T. Sta. 11+30.36

REV. BY	DATE	DESCRIPTION
KMK	4/21/16	"BLOW-UP" DETAIL ADDED
JEM	4/7/16	ADDED LABEL FOR TREES TO BE REMOVED BY OTHERS
DATE COMPLETED		

"BLOW-UP" DETAIL  
 CAUTION DURING  
 CONSTRUCTION



CROSS REFERENCES

FOR SUBSUMMARIES AND CALCULATIONS, SEE SHEETS 304-369  
 FOR RAMP W-8 PLAN AND PROFILES, SEE SHEETS 467 - 471  
 FOR RUSSELL AVE. PROFILE, SEE SHEET 598  
 FOR REMOVAL PLAN, SEE SHEET 775  
 FOR DRAINAGE PLAN AND PROFILE, SEE SHEETS 852 - 963  
 FOR SANITARY PLAN AND PROFILE, SEE SHEETS 978 - 992  
 FOR WATER WORK PLAN AND PROFILE, SEE SHEETS 1000- 1033  
 FOR GRADING DETAILS, SEE SHEETS 793 - 794  
 FOR WALL #1 PLANS, SEE SHEETS 1621 - 1624

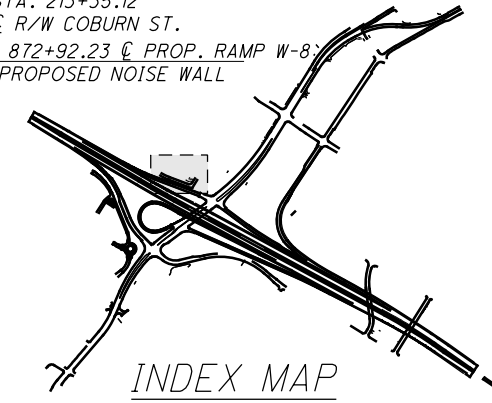
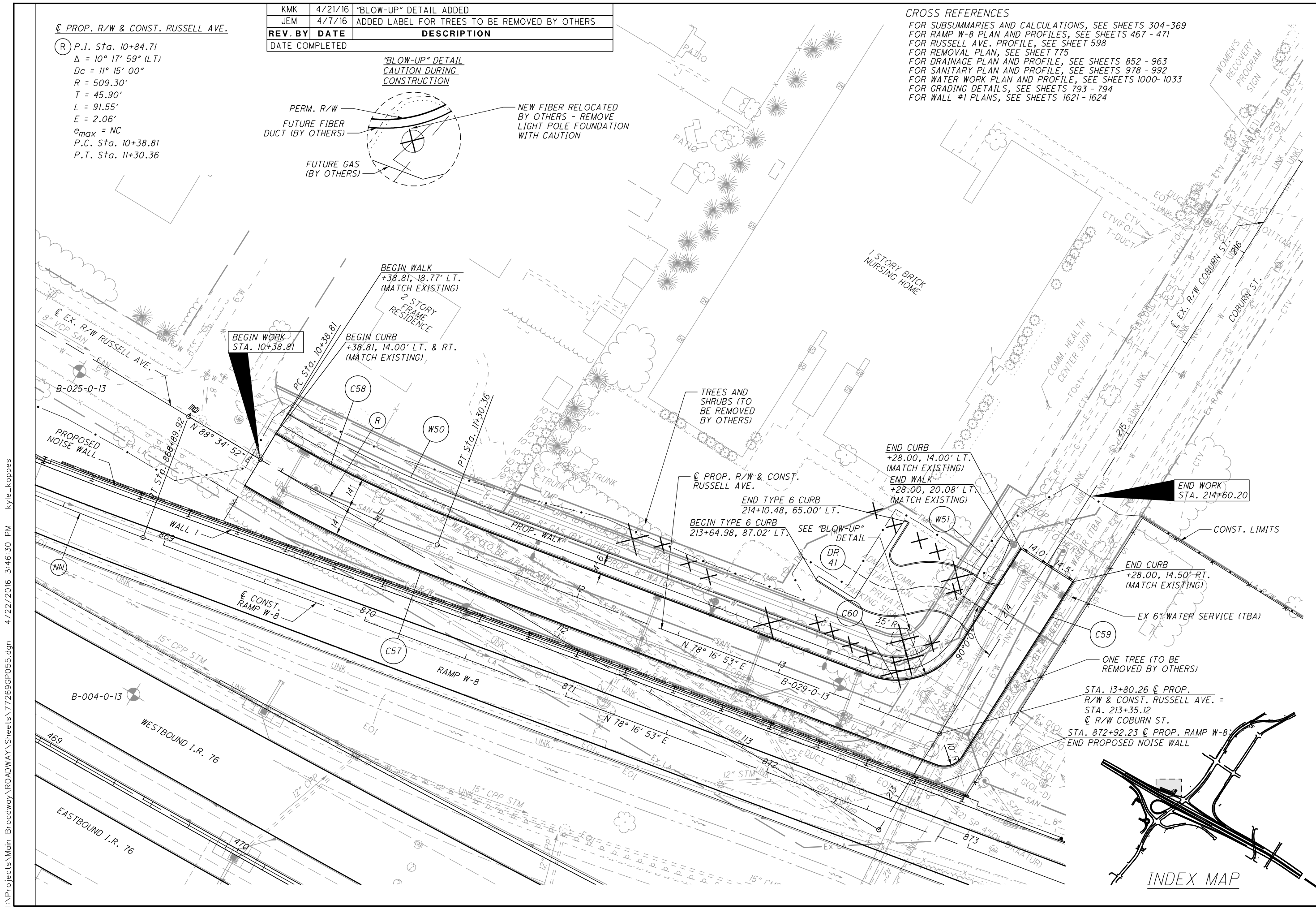
CALCULATED MJT CHECKED KMK

HORIZONTAL SCALE IN FEET

PLAN - RUSSELL AVE.  
 STA. 10+00 TO STA 16+00

SUM-76-10.00

597  
 1822

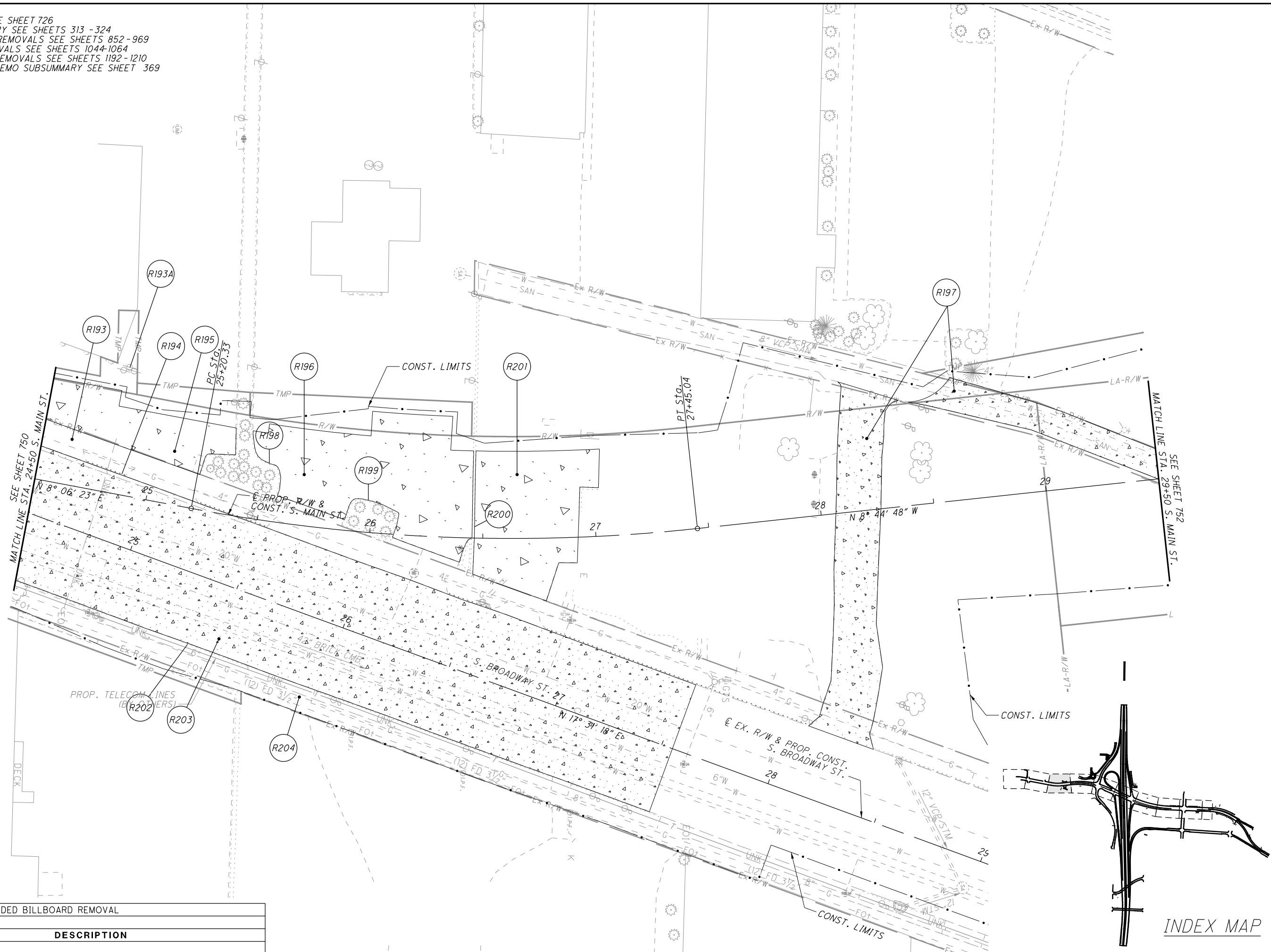


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FOR LEGEND SEE SHEET 726  
 FOR SUBSUMMARY SEE SHEETS 313 - 324  
 FOR DRAINAGE REMOVALS SEE SHEETS 852 - 969  
 FOR SIGN REMOVALS SEE SHEETS 1044 - 1064  
 FOR LIGHTING REMOVALS SEE SHEETS 1192 - 1210  
 FOR BUILDING DEMO SUBSUMMARY SEE SHEET 369

CALCULATED  
 MJT  
 CHECKED  
 KMK

0 20 40  
 HORIZONTAL  
 SCALE IN FEET



**REMOVAL PLAN - S. MAIN ST.**  
**STA. 24+50 TO STA. 29+50**

**SUM - 76 - 10.00**

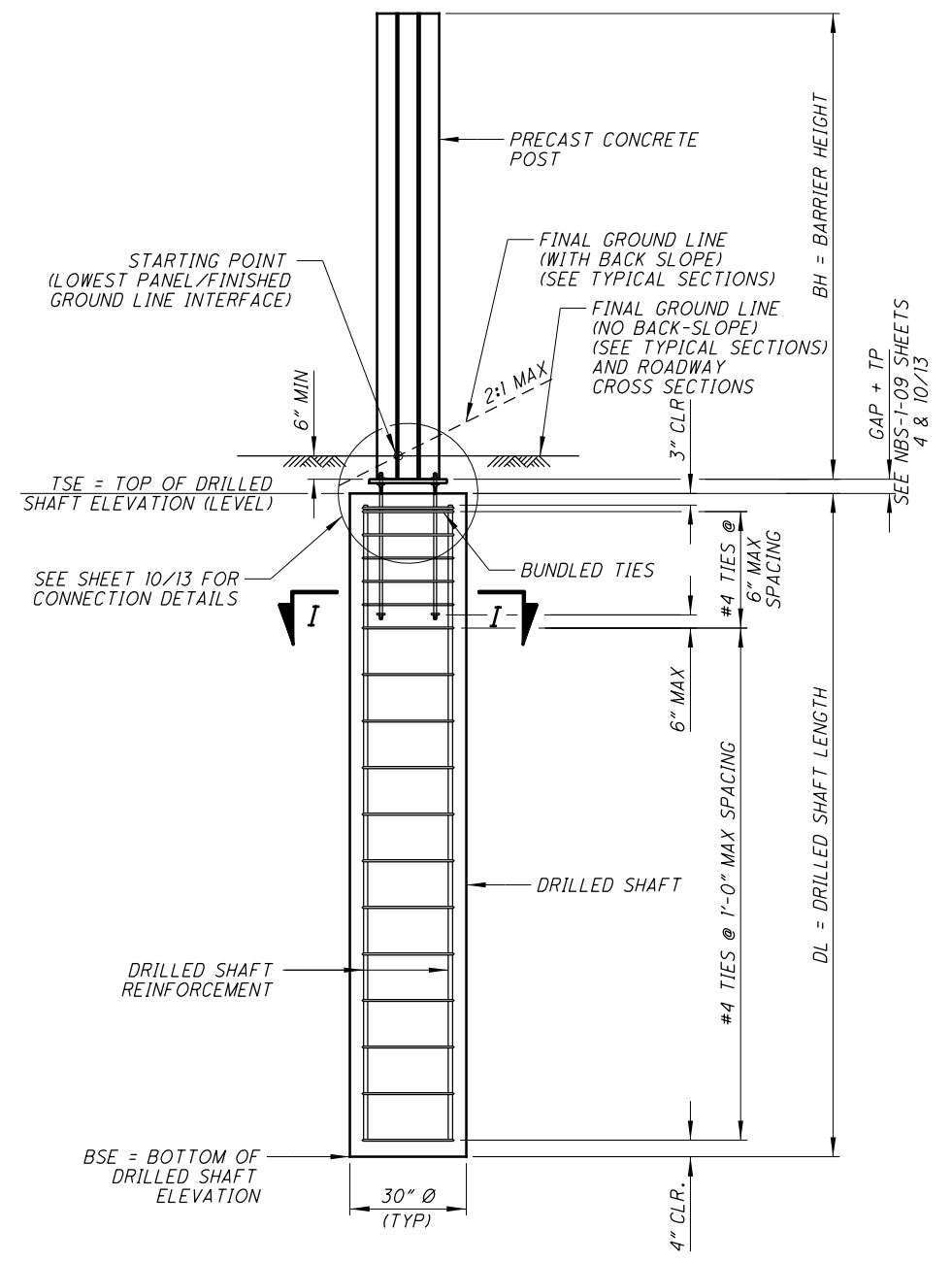
751  
 1822

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REV. BY	DATE	DESCRIPTION
KMK	4/21/16	ADDED BILLBOARD REMOVAL

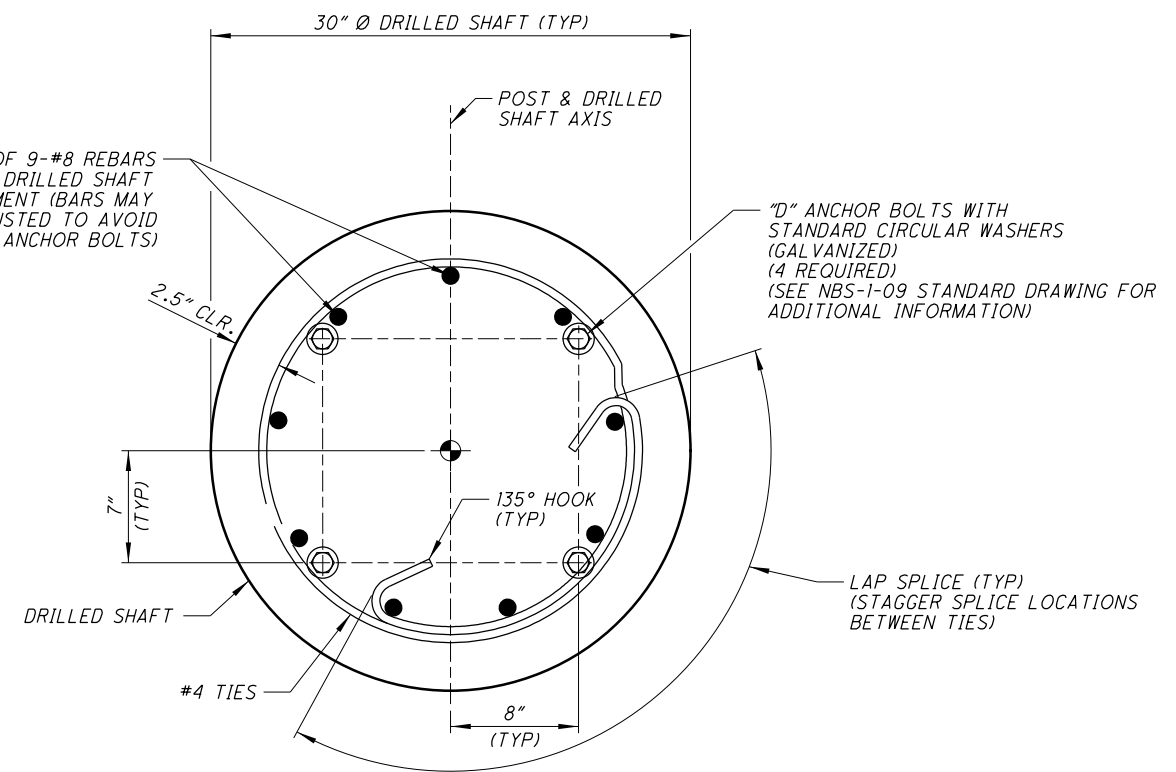
INDEX MAP

CALCULATED  
JSP  
CHECKED  
DEB



TYPICAL DRILLED SHAFT ELEVATION

PROVIDE A MINIMUM OF 9-#8 REBARS (EQUALLY SPACED) AS DRILLED SHAFT VERTICAL REINFORCEMENT (BARS MAY BE SLIGHTLY ADJUSTED TO AVOID INTERFERENCE WITH THE ANCHOR BOLTS)



SECTION I-I: TYPICAL DRILLED SHAFT

**LEGEND:**

● = CENTER OF DRILLED SHAFT

**NOTES:**

1. FOR GENERAL NOTES REFER TO SHEETS 1-3/13 OF NBS-1-09.
2. FOR DRILLED SHAFT LENGTH, SEE SHEET (839/1822)

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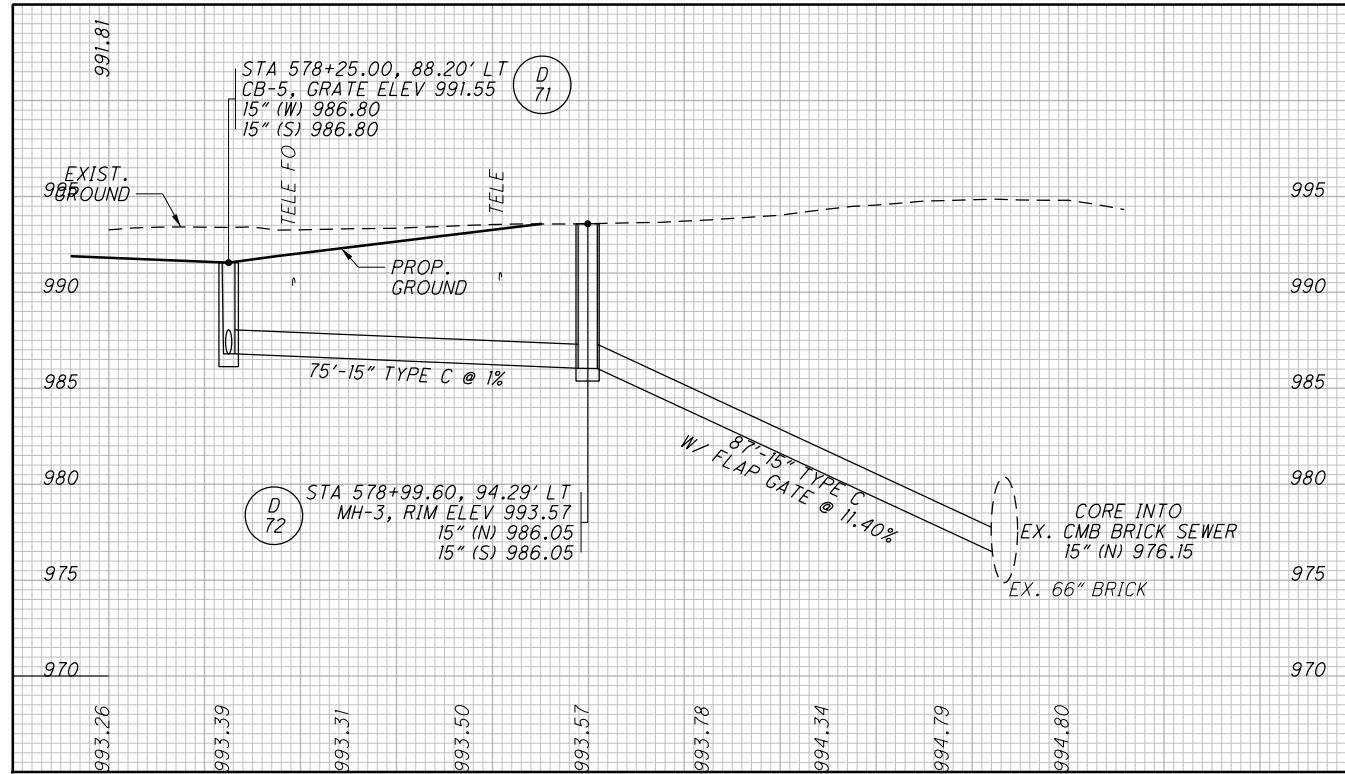
REV. BY	DATE	DESCRIPTION
KMK	4/19/16	3" TO 2.5" CLEARANCE REVISION ON DRILLED SHAFTS

NOISE WALL DETAILS

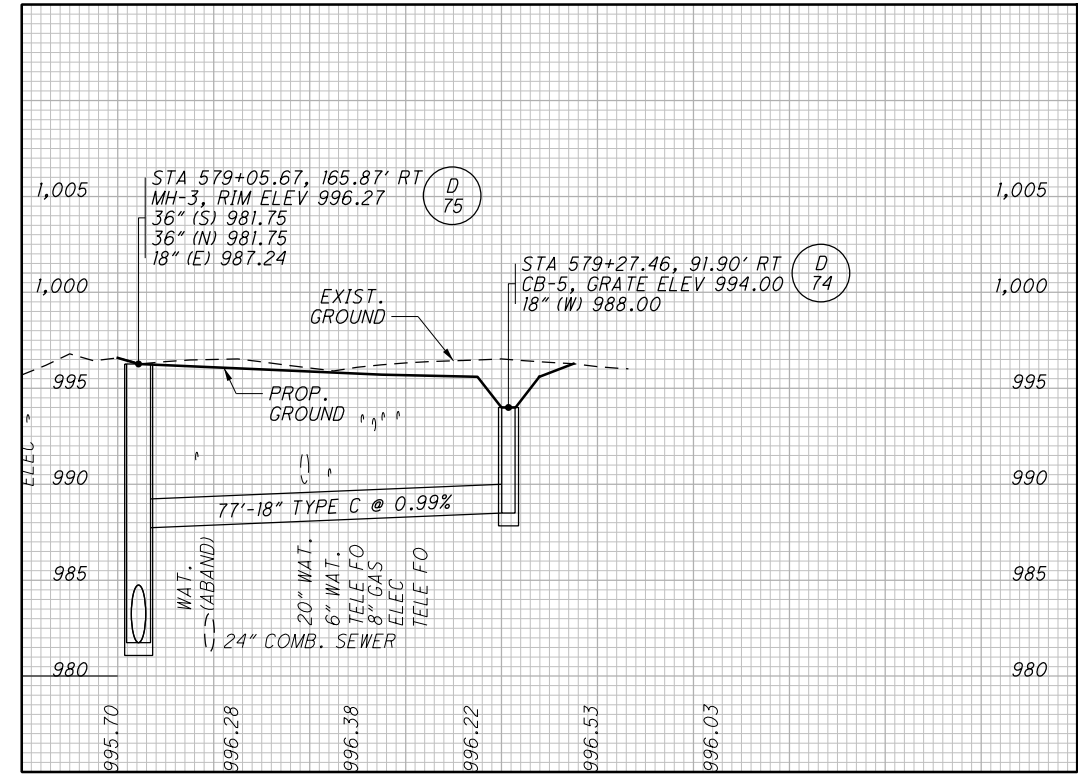
SUM - 76 - 10.00

806  
1822

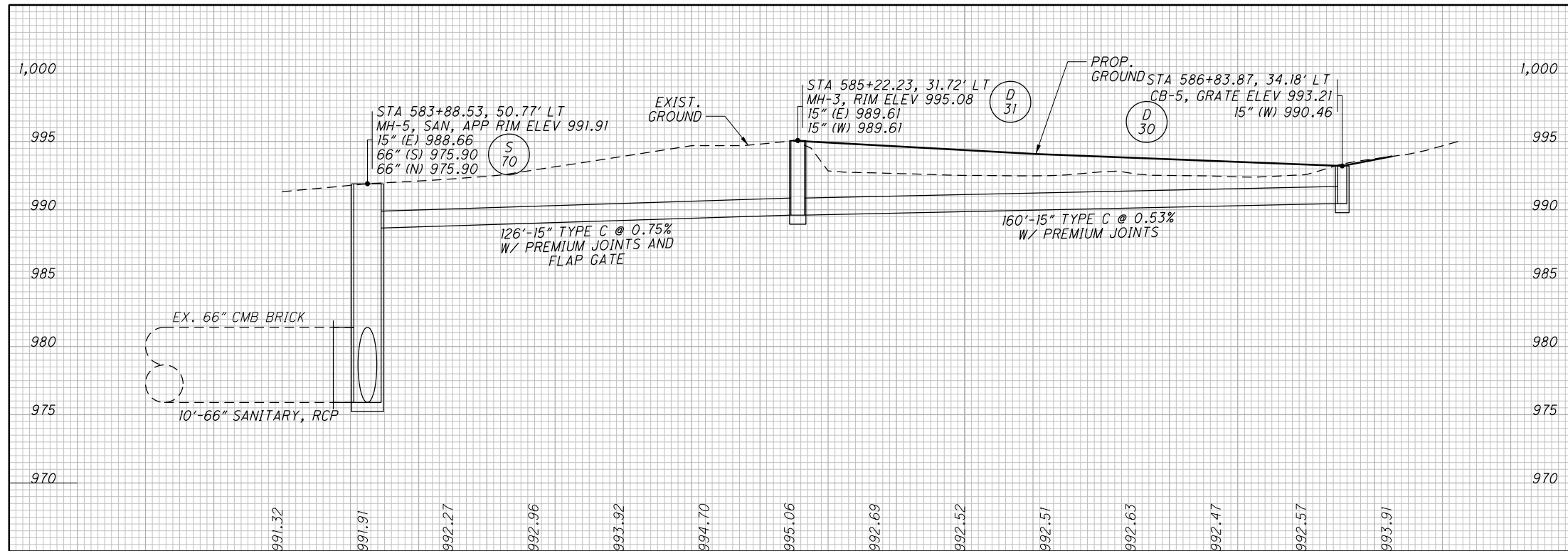
PROFILE (F) - RAMP W-5, STA. 578+25.00 TO STA. 578+99.60, LEFT SIDE



PROFILE (G) - RAMP W-5, STA. STA 579+05.67, 165.87' RT TO STA. STA 579+27.46, 91.90' RT, RIGHT SIDE



PROFILE (H) - RAMP W-5, STA. 583+88.53 TO STA. 586+94.00, LEFT SIDE



STORM SEWER PROFILES

SUM - 76 - 10.00

REV. BY	DATE	DESCRIPTION

KMK 4/18/16 INVERTS DROPPED 1.5' ON D71 AND D72 (NO QUANTITY CHANGES)

959  
1822



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Table with columns: SHEET NUMBER (1182-1190), PARTICIPATION 01/IMS/PV, ITEM, ITEM EXT., GRAND TOTAL, UNIT, DESCRIPTION, SEE SHEET NO. (1182-1183), and ALTERNATE BID ITEMS. Includes a summary table at the bottom right with columns: REV BY, DATE, DESCRIPTION.

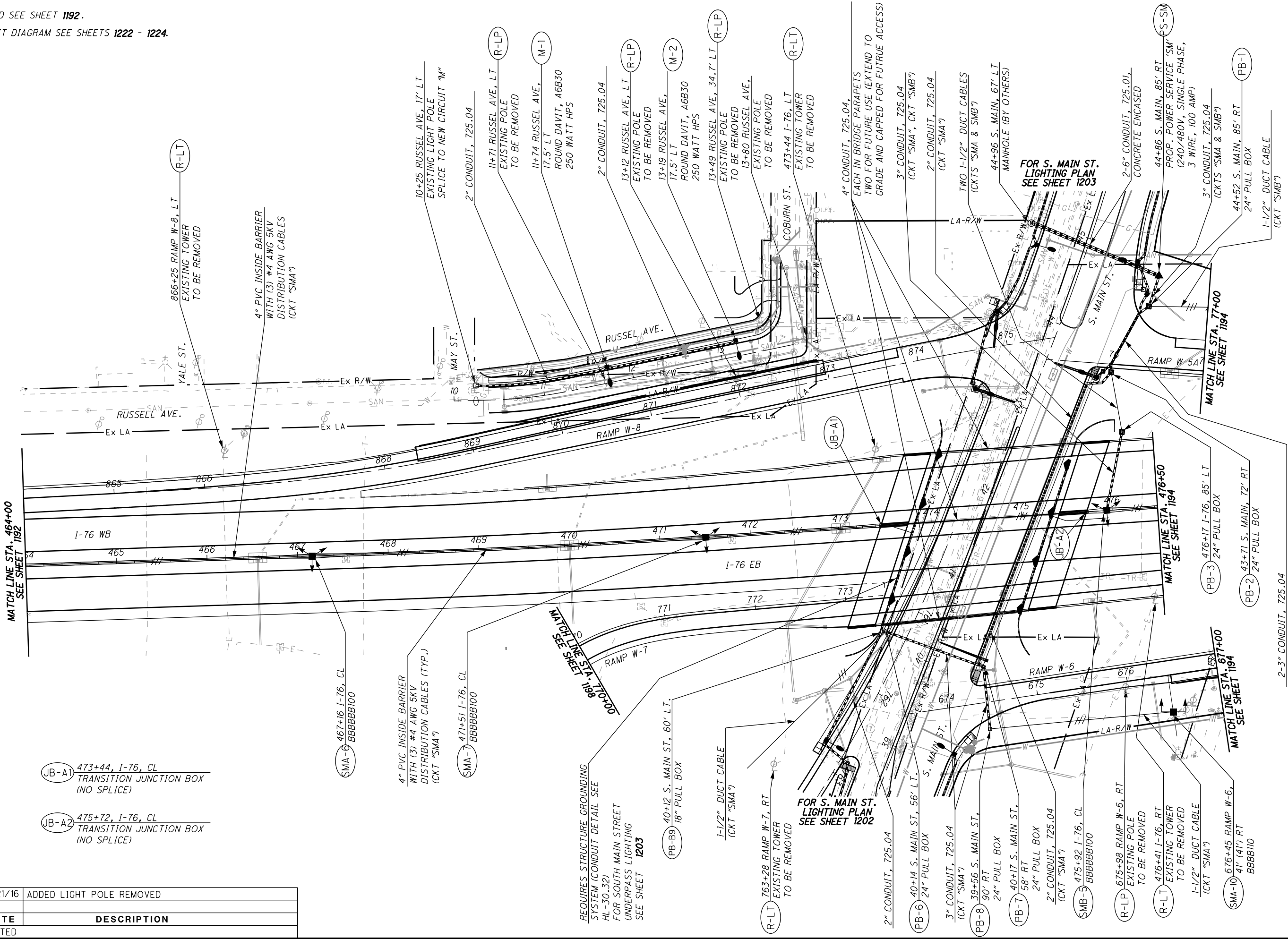
CALCULATED  
CX  
CHECKED  
RJS

LIGHTING GENERAL SUMMARY

SUM - 76 - 10.00  
1184  
1822



FOR LEGEND SEE SHEET 1192.  
 FOR CIRCUIT DIAGRAM SEE SHEETS 1222 - 1224.



- (JB-A1) 473+44, I-76, CL  
TRANSITION JUNCTION BOX  
(NO SPLICE)
- (JB-A2) 475+72, I-76, CL  
TRANSITION JUNCTION BOX  
(NO SPLICE)
- (SMA-6) 467+16 I-76, CL  
BBBBBB100
- (SMA-7) 471+51 I-76, CL  
BBBBBB100
- (PB-B9) 40+12 S. MAIN ST, 60' LT.  
18" PULL BOX
- (PB-6) 40+14 S. MAIN ST, 56' LT.  
24" PULL BOX
- (PB-8) 39+56 S. MAIN ST,  
90' RT  
24" PULL BOX
- (PB-7) 40+17 S. MAIN ST,  
58' RT  
24" PULL BOX
- (SMB-9) 475+92 I-76, CL  
BBBBBB100
- (R-LP) 675+98 RAMP W-6, RT  
EXISTING POLE  
TO BE REMOVED
- (R-LT) 476+41 I-76, RT  
EXISTING TOWER  
TO BE REMOVED
- (SMA-10) 676+45 RAMP W-6,  
41' (41') RT  
BBBB110
- (PB-3) 476+17 I-76, 85' LT  
24" PULL BOX
- (PB-2) 43+71 S. MAIN, 72' RT  
24" PULL BOX
- (PB-1) 44+52 S. MAIN, 85' RT  
24" PULL BOX
- (R-LT) 866+25 RAMP W-8, LT  
EXISTING TOWER  
TO BE REMOVED
- (R-LP) 11+71 RUSSEL AVE, LT  
EXISTING POLE  
TO BE REMOVED
- (M-1) 11+74 RUSSEL AVE,  
17.5' LT  
ROUND DAVIT, A6B30  
250 WATT HPS
- (R-LP) 13+12 RUSSEL AVE, LT  
EXISTING POLE  
TO BE REMOVED
- (M-2) 13+19 RUSSEL AVE,  
17.5' LT  
ROUND DAVIT, A6B30  
250 WATT HPS
- (R-LP) 13+49 RUSSEL AVE, 34.7' LT  
EXISTING POLE  
TO BE REMOVED
- (R-LT) 473+44 I-76, LT  
EXISTING TOWER  
TO BE REMOVED
- (R-LT) 44+96 S. MAIN, 67' LT  
MANHOLE (BY OTHERS)
- (PS-SM) 44+86 S. MAIN, 85' RT  
PROP. POWER SERVICE "SM"  
(240/480V, SINGLE PHASE,  
3 WIRE, 100 AMP)

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REV. BY	DATE	DESCRIPTION
KMK	4/21/16	ADDED LIGHT POLE REMOVED

**SUM-76-10.00**

1193  
1822

**LIGHTING PLAN I-76**

**STA. 464+00 TO STA. 476+50**

CALCULATED 0  
 CX 25  
 CHECKED 100  
 RJS HORIZONTAL SCALE IN FEET

↑

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ITEM ODOT	EXT.	PARTICIPATION	TOTAL	UNIT	DESCRIPTION	MSE WALL 1	WALL 2	MSE WALL 3	MSE WALL 4	MSE WALL 5	MSE WALL 6	MSE WALL 7	MSE WALL 8	WALL 9	WALL 10	WALL 11	MODULAR BLOCK WALL	REF. SHEET
		02/IMS/BR																
517	75120	416	416	FT	RAILING (CONCRETE PARAPET WITH TWIN STEEL TUBE RAILING)									416				
517	76300	124	124	FT	RAILING, MISC.: REATTACH EXISTING HANDRAIL		124											(1626/1822)
518	21200	298	298	CY	POROUS BACKFILL WITH FILTER FABRIC									26	272			
518	40000	730	730	FT	6" PERFORATED CORRUGATED PLASTIC PIPE									416	314			
518	40010	61	61	FT	6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS									50	11			
524	94703	445	445	FT	DRILLED SHAFTS, 36" DIAMETER, ABOVE BEDROCK, AS PER PLAN									445				(1676/1822)
524	94803	405	405	FT	DRILLED SHAFTS, 42" DIAMETER, ABOVE BEDROCK, AS PER PLAN									405				(1676/1822)
524	94903	465	465	FT	DRILLED SHAFTS, 48" DIAMETER ABOVE BEDROCK, AS PER PLAN									465				(1676/1822)
524	94915	745	745	FT	DRILLED SHAFTS, 60" DIAMETER, ABOVE BEDROCK, AS PER PLAN									745				(1676/1822)
524	94919	124	124	FT	DRILLED SHAFTS, 60" DIAMETER, INTO BEDROCK, AS PER PLAN									124				(1676/1822)
SPECIAL	53013000	6,720	6,720	SF	FORM LINER									6720				(1616/1822)
601	37500	380	380	FT	PAVED GUTTER, TYPE 1-2			79	96	60	42		103					
607	39920	559	559	FT	VANDAL PROTECTION FENCE, 10' CURVED, COATED FABRIC					394		165						
SPECIAL	61050010	309	309	SF	RETAINING WALL, MISC.: MODULAR CONCRETE BLOCK WALL												309	(845/1822)
SPECIAL	61060000	LS	LS		RETAINING WALL, MISC.: TEMPORARY WIRE FACED MSE WALL			LS	LS		LS	LS						(1617/1822) & (1618/1822)
840	20000	80,889	80,889	SF	MECHANICALLY STABILIZED EARTH WALL	6488		4336	4977	9997	13849	34024	4900			2318		
840	21000	27,900	27,900	CY	WALL EXCAVATION	1625		2926	5448	2175	4627	8915	1558			626		
840	22000	8,079	8,079	SY	FOUNDATION PREPARATION	750		798	804	1088		3720	641			278		
840	22001	1,735	1,735	SY	FOUNDATION PREPARATION, AS PER PLAN						1735							(1649/1822)
840	23000	60,848	60,848	CY	SELECT GRANULAR BACKFILL	2582		3854	4322	4909	10891	27628	5536			1126		
840	25010	8,222	8,222	FT	6" DRAINAGE PIPE, PERFORATED	920		586	578	1150	1340	2821	462			365		
840	25020	471	471	FT	6" DRAINAGE PIPE, NON-PERFORATED	25		52	45	60	133	77	10			69		
840	26000	4,166	4,166	FT	CONCRETE COPING	450		295	300	593	653	1448	246			181		
840	26050	80,889	80,889	SF	AESTHETIC SURFACE TREATMENT	6488		4336	4977	9997	13849	34024	4900			2318		
840	27000	13	13	DAY	ON-SITE ASSISTANCE	1		2	2	1	2	3	1			1		
840	28000	LS	LS		SGB INSPECTION AND COMPACTION TESTING	LS		LS	LS	LS	LS	LS	LS			LS		

REV.	BY	DATE	DESCRIPTION
DEB		4/21/16	FORM LINER QUANTITY REVISION