

PAVING UNDER GUARDRAIL

THIS OPERATION SHALL INCLUDE PREPARATION OF THE GRADED SHOULDER USING 209, LINEAR GRADING AS PER PLAN, AND PAVING UNDER THE GUARDRAIL USING 441 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448), UNDER GUARDRAIL, AS PER PLAN.

ITEM 209, LINEAR GRADING AS PER PLAN, SHALL CONSIST OF EXCAVATING TOPSOIL, AND PLACING GRANULAR MATERIAL.

ALL COLLECTED DEBRIS AND TOPSOIL, INCLUDING RHIZOMES, ROOTS AND OTHER VEGETATIVE PLANT MATERIAL SHALL BE REMOVED AND DISPOSED OF AS SPECIFIED IN 105.17.

THE REMOVED MATERIAL SHALL BE REPLACED WITH COMPACTABLE GRANULAR MATERIAL CONFORMING TO 703.16 PLACED TO GRADE AS DETAILED ON THE TYPICAL SECTION OR AS APPROVED BY THE ENGINEER.

ALL EQUIPMENT, MATERIALS AND LABOR REQUIRED TO PERFORM THE WORK OUTLINED ABOVE SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 209, LINEAR GRADING, AS PER PLAN.

PAVING UNDER GUARDRAIL SHALL CONSIST OF PLACING ITEM 441 TO THE DEPTH SPECIFIED USING ONE OF THE FOLLOWING METHODS:

- METHOD A:
1. SET GUARDRAIL POSTS
 2. PLACE ITEM 441

- METHOD B:
1. PLACE ITEM 441
 2. BORE ASPHALT AT POST LOCATIONS (MAY BE OMITTED IF STEEL POSTS ARE USED)
 3. SET GUARDRAIL POSTS
 4. PATCH AROUND POSTS. THE MATERIALS USED FOR PATCHING SHALL BE AN ASPHALT CONCRETE APPROVED BY THE ENGINEER. PATCHED AREAS SHALL BE COMPACTED USING EITHER HAND OR MECHANICAL METHODS. FINISHED SURFACES SHALL BE SMOOTH AND SLOPED TO DRAIN AWAY FROM THE POSTS.

ALL EQUIPMENT, MATERIALS AND LABOR REQUIRED TO PERFORM THE WORK OUTLINED ABOVE, WITH THE EXCEPTION OF SETTING GUARDRAIL POSTS, SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 441, ASPHALT CONCRETE, INTERMEDIATE COURSE, TYPE 1, (448), UNDER GUARDRAIL, AS PER PLAN.

ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK:

ITEM 209, LINEAR GRADING, AS PER PLAN 0.45 MILES

ITEM 441, ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1 (448), (UNDER GUARDRAIL), AS PER PLAN. 147 CY

PART-WIDTH CONSTRUCTION

BECAUSE OF THE NECESSITY TO BUILD THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXERCISE CARE TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN THE BASE COURSES. LAP LONGITUDINAL JOINTS AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

RECYCLED ASPHALT PAVEMENT (RAP) SHALL BE USED IN AREAS ADJACENT TO THE PAVED BERM. THE RAP SHALL HAVE A MINIMUM PG CONTENT OF 4.5% AND MEET THE FOLLOWING GRADATION. ONCE THE STOCKPILE MEETS THE GRADATION, THE PG CONTENT OF THE RAP SHALL BE DETERMINED PER 441.03. THE RAP ANALYSIS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL 2 WEEKS PRIOR TO USE. METHOD OF MEASUREMENT SHALL BE AS PER 617.06. PLACEMENT AND COMPACTION SHALL MEET THE REQUIREMENTS OF ITEM 617. ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 617 COMPACTED AGGREGATE, AS PER PLAN.

MODIFIED GRADATION SHALL APPLY:

SIEVE	TOTAL PERCENT PASSING
1-1/2"	100
3/4"	50-100
NO. 4	35-70
NO. 30	9-33
NO. 200	0-13

AN ESTIMATED QUANTITY OF 217 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK.

ITEM 304 AGGREGATE BASE, AS PER PLAN A

THIS ITEM SHALL BE USED ON THE CITY STREETS.

ONLY VIRGIN CRUSHED LIMESTONE SHALL BE PERMITTED FOR THIS ITEM. GRANULATED SLAG (GS) SHALL NOT BE PERMITTED. ALL OTHER REQUIREMENTS OF SECTIONS 304 AND 703.17 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS SHALL STILL BE APPLICABLE.

ITEM 304 - AGGREGATE BASE, AS PER PLAN B

GRANULATED SLAG (GS) SHALL NOT BE PERMITTED FOR THIS ITEM. ALL OTHER REQUIREMENTS OF SECTIONS 304 AND 703.17 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS SHALL STILL BE APPLICABLE.

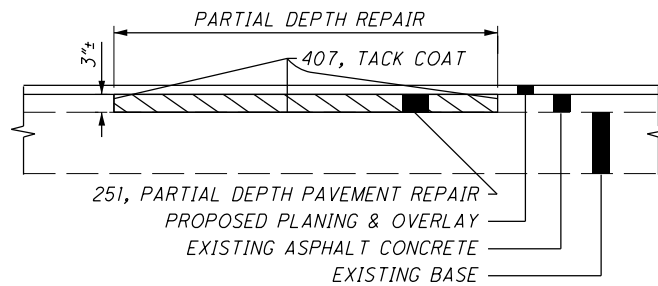
ITEM 304 - AGGREGATE BASE, AS PER PLAN C

THIS ITEM SHALL BE USED ON THE TEMPORARY GRAVEL PARKING LOT AT YALE ST. AND EAST SOUTH ST. LOCATION AND INCLUDES GRADING THE SITE, PLACEMENT OF THE GRAVEL, AND REMOVAL OF THE GRAVEL AFTER THE COMPLETION OF YALE ST. ALL OTHER REQUIREMENTS OF SECTIONS 304 AND 703.17 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS SHALL STILL BE APPLICABLE.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE 1 PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING. ALSO, THIS ITEM SHALL COMMENCE WITHIN 7 DAYS OF THE COMPLETION OF MAINLINE PAVEMENT PLANING. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

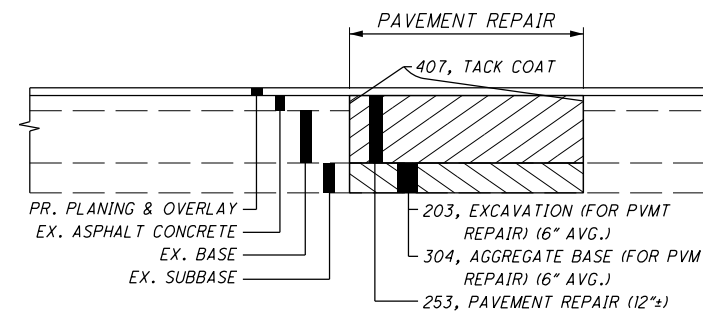
251, PARTIAL DEPTH PAVEMENT REPAIR, 650 SQ. YD.



ITEM 253 - PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 12"+ 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING. ALSO, THIS ITEM SHALL COMMENCE WITHIN 7 DAYS OF THE COMPLETION OF MAINLINE PAVEMENT PLANING. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

253, PAVEMENT REPAIR, 650 SQ YD.



ITEM 451 - 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI

THIS ITEM IS FOR USE ON E. THORNTON STREET AND E. BARTGES STREET WHERE INDICATED ON THE PLANS.

WHERE THE PROPOSED PAVEMENT BUTTS WITH EXISTING PAVEMENT EITHER LONGITUDINALLY OR TRANSVERSELY, THE JOINTS BETWEEN THE EXISTING AND PROPOSED PAVEMENTS SHALL BE IN ACCORDANCE WITH ODOT STANDARD CONSTRUCTION DRAWINGS BP-2.1 AND BP-2.5.

THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI WILL BE MEASURED BY THE SQUARE YARD OF FINISHED SURFACE COMPLETE IN PLACE. THE ACCEPTED QUANTITIES OF THIS ITEM WILL BE PAID FOR AT THE CONTRACT PRICES DESIGNATED. THE CONTRACT PRICE SHALL INCLUDE ALL LABOR, MATERIAL EQUIPMENT AND INCIDENTAL ITEMS NEEDED FOR EXCAVATION, BACKFILL, REINFORCING STEEL, EXPANSION JOINT MATERIAL, CONTRACTION JOINTS AND OTHER RELATED MISCELLANEOUS ITEMS WILL NOT BE PAID FOR SEPARATELY, BUT THE COST THEREOF SHALL BE INCLUDED IN THE COST OF THE ITEM OF WHICH THEY ARE A PART. PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT PRICE FOR: THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI.

ITEM 451 - 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI, AS PER PLAN "A"

THIS ITEM IS FOR USE AT THE INTERSECTIONS TO RECEIVE DECORATIVE CONCRETE PAVEMENT TREATMENT. THE INTERSECTIONS TO RECEIVE THIS TREATMENT ARE AS FOLLOWS: S. MAIN STREET AT MILLER AVENUE, S. MAIN STREET AT E. THORNTON STREET, E. THORNTON STREET AT RAMP W-5 AND S. BROADWAY STREET

THIS WORK IS TO PROVIDE COLORED REINFORCED CONCRETE PAVEMENT WITHIN THE BOUNDED BY THE DECORATIVE CONCRETE HEADERS. THE COLOR SHALL BE "COAL GRAY".

THIS WORK SHALL CONFORM TO ODOT CMS ITEM 451 UNLESS OTHERWISE NOTED HEREIN.

THE INTERSECTION PAVEMENT SHALL BE BOUNDED BY A DECORATIVE CONCRETE HEADER AS SHOWN ON DETAIL SHEET 801

THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI, AS PER PLAN "A" WILL BE MEASURED BY THE SQUARE YARD OF FINISHED SURFACE COMPLETE IN PLACE. THE ACCEPTED QUANTITIES OF THIS ITEM WILL BE PAID FOR AT THE CONTRACT PRICES DESIGNATED. THE CONTRACT PRICE SHALL INCLUDE ALL LABOR, MATERIAL EQUIPMENT AND INCIDENTAL ITEMS NEEDED FOR EXCAVATION, BACKFILL, REINFORCING STEEL, EXPANSION JOINT MATERIAL, CONTRACTION JOINTS AND OTHER RELATED MISCELLANEOUS ITEMS WILL NOT BE PAID FOR SEPARATELY, BUT THE COST THEREOF SHALL BE INCLUDED IN THE COST OF THE ITEM OF WHICH THEY ARE A PART. PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT PRICE FOR: THIS ITEM 451 9" REINFORCED CONCRETE PAVEMENT, CLASS QCI, AS PER PLAN "A" (SQUARE YARD). THIS ITEM WILL BE PAID BY 100% CITY OF AKRON COSTS.

ITEM 202 - REMOVAL MISC.: BILLBOARD REMOVAL

REMOVE AND DISPOSE OF THE BILLBOARD IN ITS ENTIRETY INCLUDING THE FOUNDATION, AS PER 202.02. THE TWO BOLLARDS IN FRONT OF THE BILLBOARDS POST ARE TO BE REMOVED AND INCIDENTAL TO THE COST OF THE REMOVAL OF THE BILLBOARD. ALL NECESSARY ELECTRICAL WIRE DISCONNECTIONS ARE INCIDENTAL TO THE REMOVAL OF THE BILLBOARD. ALL LABOR, MATERIAL, EQUIPMENT AND INCIDENTAL ITEMS FOR THE COMPLETE REMOVAL OF THE BILLBOARD SHALL BE IN THE UNIT PRICE BID, LUMP SUM, OF ITEM 202 - REMOVAL MISC.: BILLBOARD REMOVED.

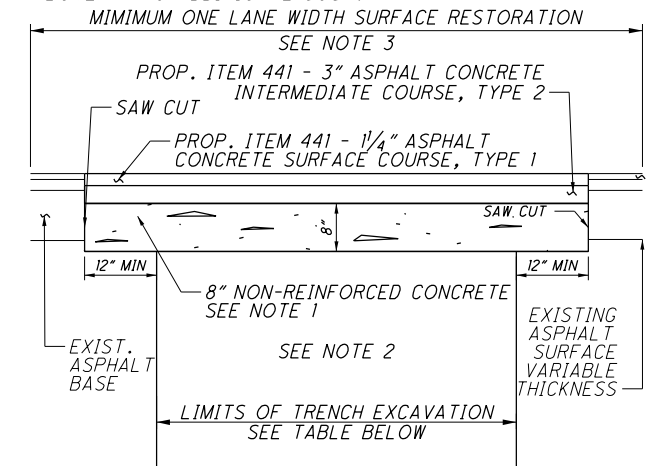
PAVEMENT RESTORATION FOR PIPE, WATERLINE, AND STRUCTURE INSTALLATIONS AND/OR REMOVALS

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION AND/OR REMOVAL OF PIPES AND STRUCTURES.

QUANTITIES FOR ITEM 202 - PAVEMENT REMOVED; ITEM 407 - TACK COAT; ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG 64-22; ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446); ITEM 452 - 8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS HAVE BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING THE INSTALLATION AND/OR REMOVAL OF PIPES/STRUCTURES UNDER ITEM 611 AND WATERLINES UNDER ITEM 638 AND THE REMOVAL OF PIPES UNDER ITEM 202.

THE QUANTITIES BASED ON AN ITEM 407 - TACK COAT APPLICATION RATE OF 0.075 GALLONS PER SQUARE YARD, ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG 64-22 THICKNESS OF 1.25 INCHES, AN ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446) THICKNESS OF 3 INCHES, ITEM 452 - 8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS THICKNESS OF 8 INCHES. THE QUANTITIES ARE BASED ON A PAVEMENT WIDTH THAT INCLUDES THE TRENCH WIDTH PLUS SEE NOTE 4.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.



- NOTES:
1. THE TOP OF THE NEW CONCRETE BASE SHALL NOT BE CONSTRUCTED HIGHER THAN THE EXISTING ASPHALT BASE.
 2. BACKFILL MEETING THE REQUIREMENTS OF 611.
 3. LIMITS MAY BEGIN AT FACE OF CURB OR EDGE OF CURB AND GUTTER.
 4. WHEN A TRENCH CROSSES THE ROADWAY PERPENDICULARLY, THE PAVEMENT BASE SHALL EXTEND A MINIMUM OF 12" OUTSIDE OF THE TRENCH LIMITS AND THE ASPHALT SURFACE SHALL EXTEND A MINIMUM OF 12" OUTSIDE OF THE PAVEMENT BASE LIMITS.

TRENCH DIMENSIONS

MATERIAL	SIDE TRENCH WIDTH (SW)	WIDTH (W)
Long Span	2'	Span+2(12)
Concrete	0.165 X Span	1.33 X Span
Metal & Plastic	0.5 + 0.125 X Span	1.25 X Span + 1

REV. BY	DATE	DESCRIPTION
KMK	4/21/16	ADDED BILLBOARD REMOVAL
		DATE COMPLETED

CALCULATED
PHF
CHECKED
WJC

GENERAL NOTES

SUM - 76 - 10.00

43
1822

ITEM 622 - PORTABLE BARRIER PLACEMENT (I-76 / I-77)

DURING THE PLACEMENT OF THE PORTABLE BARRIER, TRAFFIC WILL BE PROHIBITED FROM OCCUPYING THE TRAVEL LANE ADJACENT TO THE BARRIER. THE BARRIER WILL BE PLACED AT NIGHT PER THE WORK HOUR RESTRICTION NOTE AND IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE CHART. THE CLOSURE OF THE ADJACENT LANE WILL BE PER THE STANDARD DRAWING MT-95.30. THE CONTRACTOR WILL SUBMIT A PLAN TO THE ENGINEER FOR APPROVAL SEVEN (7) DAYS IN ADVANCE OF THE PLANNED LANE CLOSURE. WORK WILL NOT BEGIN UNTIL APPROVAL OF THE PLANS HAS BEEN GRANTED. ALL COSTS INVOLVED IN PLACING THE PORTABLE CONCRETE BARRIER WILL BE INCLUDED IN THE CONTRACT PRICE BID FOR ITEM 622 PORTABLE CONCRETE BARRIER.

614 WORK ZONE PAVEMENT MARKINGS, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF 614.11, ALL CLASS 1 EDGE LINES, LANE LINES, CENTER LINES AND DOTTED LINES SHALL BE 6 INCHES WIDE AND CHANNELIZING MARKINGS SHALL BE 8 INCHES WIDE. THE APPLICATION RATES FOR THE 6 INCH LINES SHALL BE 1.5 TIMES THE RATES SPECIFIED FOR 4 INCH LINES IN TABLE 614.11-1.

ITS MESSAGE BOARDS (I-76 / I-77)

THE EXISTING ITS MESSAGE BOARDS IN THE VICINITY OF THE PROJECT WILL BE UTILIZED TO PROVIDE SUPPLEMENTAL INFORMATION TO THE TRAVELING PUBLIC. THE CONTRACTOR WILL NOTIFY THE PROJECT ENGINEER ONE (1) WEEK IN ADVANCE OF ANY PHASE CHANGE. THE PROJECT ENGINEER WILL COORDINATE WITH EITHER LISA BOSE AT 330-786-4817 OR BRENT KOVACS AT 330-786-2208 TO GET THE ITS MESSAGE BOARDS ADJUSTED.

ITEM 614 - BUSINESS ENTRANCE (M4-H15) SIGN, AS PER PLAN (LOCAL)

THE BUSINESS ENTRANCE (M4-H15) SIGN SHOULD BE PROVIDED AT EACH TEMPORARILY RELOCATED COMMERCIAL DRIVEWAY FOR WHICH THE RELOCATION IS NOT OBVIOUS TO THE MOTORIST. THE PROJECT ENGINEER SHALL DETERMINE WHETHER OR NOT THE DRIVEWAY RELOCATION IS, OR IS NOT, OBVIOUS AND WHETHER OR NOT A SIGN SHOULD BE PROVIDED. ONLY ONE SIGN PER BUSINESS SHALL BE PERMITTED. THE SIGN SHALL BE 36 INCH X 48 INCH IN SIZE WITH TYPE G OR TYPE H ORANGE RETROREFLECTIVE SHEETING. THE SIGN LEGEND SHALL BE PLACED ON BOTH SIDES OF THE SIGN (BACK TO BACK). THE SIGN SHALL HAVE THE STANDARD M4-H15 LEGEND WITH THE WORD "BUSINESS" ON THE TOP LINE, EXCEPT UNDER UNUSUAL CIRCUMSTANCES WHERE IT MAY NOT BE INTUITIVE THAT A DRIVEWAY SERVES A SPECIFIC BUSINESS. IN SUCH UNUSUAL CASES, THE ACTUAL BUSINESS NAME MAY BE SUBSTITUTED FOR THE WORD "BUSINESS".

THE SIGN SHALL BE MOUNTED ON TWO NO. 3 POSTS OR ON TEMPORARY POSTS IN ACCORDANCE WITH SCD MT-105.10 AND IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION. THE SIGN SHALL BE CLEARLY VISIBLE AND SHALL CLEARLY IDENTIFY THE LOCATION OF THE DRIVEWAY. THE SIGN SHOULD BE POSITIONED AT 90 DEGREES TO THE DIRECTION(S) OF TRAFFIC. THE SIGN MAY NEED TO BE MOVED FOR EACH PHASE OF THE MAINTENANCE OF TRAFFIC OPERATIONS.

PAYMENT FOR ALL COSTS ASSOCIATED WITH MANUFACTURING, MOUNTING, RELOCATING, AND REMOVING THE SIGN, INCLUDING ALL LABOR, MATERIALS AND EQUIPMENT SHALL BE INCLUDED IN THE CONTRACT PRICE PER EACH FOR ITEM 614-BUSINESS ENTRANCE SIGN.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS ITEM.

ITEM 614 BUSINESS ENTRANCE SIGN, AS PER PLAN 20 EACH

ITEM 614 - COVERING OF SIGNS (I-76 / I-77)

THE CONTRACTOR WILL COVER ANY EXISTING AND/OR PROPOSED SIGN INSTALLATION WHICH IS IN CONFLICT WITH THE MAINTENANCE OF TRAFFIC PLANS. THE SIGNS SHALL BE COVERED IN SUCH A MANNER AS TO AVOID DAMAGING THE SIGN WHEN THE COVER IS REMOVED. THE COVER SHALL BE TOTALLY OPAQUE AND COVERS THE ENTIRE SIGN FACE. THE USE OF ADHESIVE TAPE APPLIED DIRECTLY TO THE SIGN FACE IS STRICTLY PROHIBITED. THE CONTRACTOR WILL PROVIDE ALL OF THE "CLOSED" PLAQUES NECESSARY. THE OVERLAY MAY BE RIVETED TO THE SIGN. THE CONTRACTOR SHALL PROVIDE ALL OF THE PLAQUES, SIGNS, AND SIGN PANELS NECESSARY.

UNLESS SEPARATELY ITEMIZED IN THE PLAN, THE LUMP SUM PRICE BID FOR ITEM 614-MAINTAINING TRAFFIC SHALL INCLUDE ALL COSTS NECESSARY TO COVER AND/OR MODIFY CONFLICTING SIGN INSTALLATIONS.

ITEM 625 - LIGHT POLE MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION (I-76 / I-77)

CONTRACTOR SHALL REMOVE AND SALVAGE THE EXISTING MEDIAN LIGHT POLE AND LUMINAIRE. THE SALVAGED ITEMS SHALL BE STORED BY THE CONTRACTOR FOR THE DURATION OF THE NEED FOR THE CROSSOVER THROUGH THE MEDIAN. THE FOUNDATION SHALL BE REMOVED TO A DEPTH OF 12" BELOW PROPOSED GUTTER AND PER REQUIREMENTS OF ODOT CMS 202. THE CONTRACTOR SHALL CONSTRUCT A NEW 24" X 10' DEEP FOUNDATION, INCLUDING NEW PULL BOX, ANCHOR BOLTS AND MOUNTING PLATE, ADJACENT TO THE EXISTING FOUNDATION TO REMAIN BELOW GRADE. THE FOUNDATION AND PULL BOX SHALL BE CONSTRUCTED PER SCD HL-20.13. THE SALVAGED POLE AND LUMINAIRE SHALL BE RE-INSTALLED ON THE NEW FOUNDATION AND CONNECTED TO THE EXISTING LIGHTING CIRCUITRY. ALL MATERIALS AND LABOR REQUIRED FOR THE COMPLETE INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH OF ITEM 625 LIGHT POLE MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION. CONTRACTOR SHALL MAINTAIN EXISTING LIGHTING CIRCUIT, UNTIL NEW CIRCUIT IS INSTALLED.

CONSTRUCTION ACCESS POINTS

THE CONTRACTOR SHALL USE THE DESIGNED CONSTRUCTION ACCESS POINTS SHOWN ON SHEETS 196, 201, 206, AND 211, FOR PHASE 1, STAGE 1 AND STAGE 2 OF BOTH WOLF LEDGES AND GRANT STREET BRIDGE CONSTRUCTION. FOR ACCESS INTO THE CONSTRUCTION ZONE DURING THESE PHASES, THREE LANES SHALL BE MAINTAINED MONDAY - FRIDAY, 6 AM TO 8 AM AND 3 PM TO 6 PM. ONE LANE CAN BE CLOSED ANY TIME ON SATURDAY AND SUNDAY.

CONSTRUCTION RESTRICTIONS

THE FOLLOWING DISCUSSION OF CONSTRUCTION SEQUENCING RESTRICTIONS IS PROVIDED FOR THE CONTRACTOR'S BENEFIT WHEN PLANNING HIS/HER CONSTRUCTION WORK TASK SEQUENCING. ITEMS UNDER PERMITTED SEQUENCING ARE NON-BINDING AND COULD BE MODIFIED BY THE CONTRACTOR IF AN ALTERNATE MAINTENANCE OF TRAFFIC METHOD IS SELECTED, ANY APPROVED ALTERNATE MUST INCLUDE THESE RESTRICTIONS.

- RESTRICTIONS
1. THE LOCAL STREET CONSTRUCTION MUST BE COMPLETED TO THE POINT THAT BROADWAY TRAFFIC CAN BE PLACED ONTO THE WIDENED/RELOCATED MAIN STREET BEFORE PHASE 2 CAN BEGIN.
 2. EXCEPT FOR THE 2 WEEK DETOUR FOR THE CONSTRUCTION OF THE TIE-IN FOR RAMP W-5, WB EXIT TO DOWNTOWN (RAMP W-5A AND W-5) SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.
 3. RAMP W-5 MUST BE COMPLETED AND OPEN BEFORE BEGINNING PHASE 3.
 4. EXISTING RAMP W-9 SHALL NOT BE CLOSED UNTIL THE NEW RAMP W-9 IS COMPLETE AND OPEN.
 5. WOLF LEDGES PARKWAY MUST BE COMPLETED AND OPEN TO TRAFFIC BEFORE CONSTRUCTION WHICH RESTRICTS LANES CAN BEGIN ON GRANT STREET.
 6. PHASE 2 MAY BEGIN ONCE STEEL IS SET AND FALSWORK IN PLACE FOR THE GRANT STREET STRUCTURE REPLACEMENT (SUM-76-1085). THE INTENT IS TO HAVE THE GRANT STREET STRUCTURE COMPLETE TO THE POINT OF NOT REQUIRING ANY ZONES ON I-76 BEFORE PHASE 2 BEGINS.
 7. BUILDING DEMOLITION WORK, INCLUDING RESTORATION OF THE SITES, SHALL BE COMPLETED BY SEPTEMBER 30, 2016.
 8. WORK ON WOLF LEDGES AND GRANT STREET THAT REQUIRES AND TRAFFIC CONTROL ON I-76 SHALL NOT BEGIN BEFORE AUGUST 1, 2016.
 9. THERE ARE ADJACENT BRIDGES, WITHIN CLOSE PROXIMITY TO WOLF LEDGES AND GRANT STREET THAT WILL ALSO BE UNDER CONSTRUCTION DURING THE FALL OF 2016. THE CONTRACTOR SHALL COORDINATE WITH THE ADJACENT CONTRACTORS TO MAKE SURE THE I-76 CONSTRUCTION ZONES FOR THIS PROJECT MATCH IN AND DO NOT CONFLICT WITH THE ZONES FOR THE ADJACENT PROJECTS.
 10. THE WOLF LEDGES AND GRANT STREET STRUCTURES AND APPROACH ROADWAY WORK SHALL BE COMPLETE AND OPEN TO TRAFFIC BY OCTOBER 31, 2017.
 11. PHASE 2 SHALL BE COMPLETED BY AUGUST 31, 2018.
 12. PHASE 3 SHALL BE COMPLETED BY AUGUST 31, 2019.

DATES PROVIDED ABOVE SHALL BE CONSIDERED INTERIM COMPLETION DATES AND SHALL BE SUBJECT TO DISINCENTIVE PENALTY IN THE AMOUNT OF \$1,500 PER DAY THAT THE WORK IS NOT COMPLETE BEYOND INTERIM COMPLETION DATE.

ITEM 630 - SIGNING MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER (I-76 / I-77)

CONTRACTOR SHALL REMOVE AND SALVAGE THE EXISTING BARRIER-MOUNTED MILE MARKER PER REQUIREMENTS OF ODOT CMS 202. THE CONTRACTOR SHALL STORE THE EXISTING SIGNS FOR THE DURATION OF THE NEED FOR THE CROSSOVER THROUGH THE MEDIAN. THE EXISTING MOUNTING AND POST SHALL BE REMOVED AND REPLACED WITH NEW MOUNTING AND POST PER DETAILS ON SHEET 1135. ALL MATERIALS AND LABOR REQUIRED FOR THE COMPLETE INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH OF ITEM 630 SIGNING MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER.

MAINTENANCE OF TRAFFIC AND ACCESS TO PROPERTIES

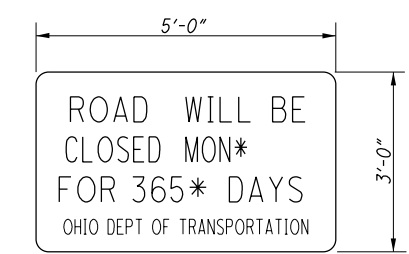
THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC AND ACCESS TO PROPERTIES.

ITEM 410, TRAFFIC COMPACTED SURFACE, TYPE A OR B	500 CY
ITEM 410, TRAFFIC COMPACTED SURFACE, TYPE C	500 CY
ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	100 CY

DRIVES TO REMAIN OPEN AT ALL TIMES UNLESS NOTED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES USING PARTIAL WIDTH CONSTRUCTION.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD OR RAMP CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.



* CONTRACTOR SHALL USE ACTUAL CLOSURE DATE AND DURATION.

THE COST OF THE NOTICE OF CLOSURE SIGN IS CONSIDERED TO BE INCIDENTAL TO AND INCLUDED IN ITEM 614 - MAINTAINING TRAFFIC.

KMK	4/25/16	CONSTRUCTION RESTRICTIONS NOTE (6) REVISED
KMK	4/18/16	COMPLETION DATE FOR GRANT AND WOLF
MEP	2/12/16	A.C. FOR MOT QUANTITY REVISION
REV. BY	DATE	DESCRIPTION
DATE COMPLETED		

LIMITATION OF STREET CLOSURES

THE CONTRACTOR SHALL COMPLETE ALL CONSTRUCTION AND SAFETY ITEMS AND HAVE THE DESIGNATED SECTIONS OPEN TO UNRESTRICTED TRAFFIC WITH IN THE CALENDER DAYS SPECIFIED.

DESCRIPTION OF LOCATION COMPLETION DAYS

PHASE 1, STAGE 1A
BROADWAY STREET AND MILLER AVENUE CLOSURE: 10 CALENDAR DAYS

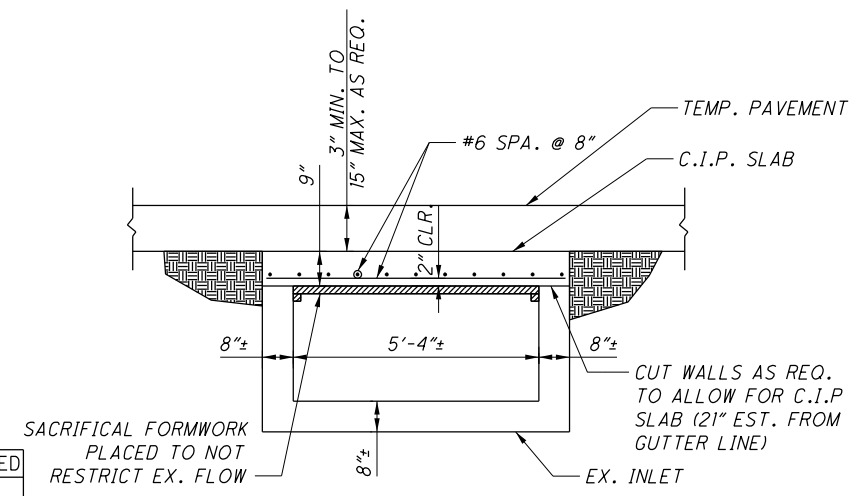
PHASE 1, STAGE 1B
BROADWAY STREET AND MILLER AVENUE CLOSURE: 10 CALENDAR DAYS

PHASE 1, STAGE 1C
YALE STREET CLOSURE: 21 CALENDER DAYS

PHASE 2
BROADWAY STREET AND THORNTON INTERSECTION AVENUE CLOSURE: 14 CALENDAR DAYS. NO CLOSURE OF THE BROADWAY/THORNTON INTERSECTION MAY TAKE PLACE UNTIL RAMP W-5 (WB TO NB) BRIDGE IS COMPLETE AND PAVEMENT COMPLETED TO STA 575+75+/- SO THAT THIS MOVEMENT CAN BE OPENED TO TRAFFIC AFTER THE 14 DAY INTERSECTION CLOSURE. THE EXISTING RAMP E MUST REMAIN OPEN PRIOR TO THIS CLOSURE.

ITEM 611 - INLET MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET (I-76 / I-77)

CONTRACTOR SHALL SAWCUT THE EXISTING BARRIER MEDIAN INLET TO A DEPTH OF 21" BELOW PROPOSED GUTTER AND PER REQUIREMENTS OF ODOT CMS 202. THE CONTRACTOR SHALL PROTECT THE EXISTING PIPE INVERT WITH A CONCRETE SLAB AND BACKFILL WITH PROPOSED SHOULDER PAVEMENT SECTION FOR THE DURATION OF THE NEED FOR THE CROSSOVER THROUGH THE MEDIAN. THE SLAB SHALL BE AS PER DETAIL SHOWN. CONCRETE SHALL BE CMS ITEM 511, CLASS OC1 MISC. WITH 4 KSI DESIGN STRENGTH. REINFORCING SHALL BE PER CMS ITEM 509 NO SEPARATE PAYMENT WILL BE MADE FOR TIME, CONCRETE AND REBAR AND OTHER MISCELLANEOUS ITEMS REQUIRED TO COMPLETE THE WORK AS IT IS INCIDENTAL OT THIS ITEM. THE EXISTING INVERT AND ONE SEGMENT OF EXISTING PIPE SHALL BE REMOVED AND REPLACED PER SCD I-2.4. WHEN THE TIME COMES TO REPLACE THE MEDIAN BARRIER WITH NEW BARRIER THE EXISTING MEDIAN INLET MUST BE REMOVED AND THE NEW MEDIAN INLET RECONSTRUCTED IN IT'S ORIGINAL LOCATION AS APPROVED BY THE ENGINEER. ALL MATERIALS, LABOR, AND EQUIPMENT REQUIRED FOR THE COMPLETE INSTALLATION SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH OF ITEM 611 INLET MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET.



DETAIL FOR ITEM 611 - INLET MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET (I-76 / I-77)

I:\Projects\Main_Broadway\MOT\Sheets\77269MN005.dgn 4/25/2016 11:26:22 AM kyle_koppes

REFERENCE NO.	SHEET NO.	LOCATION		PARTICIPATION (100% CITY OR PROJECT)	202	611	614	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622	622	625	630	630	630				
		FROM	TO		CONCRETE BARRIER REMOVED	INLET, MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET	WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL)	WORK ZONE IMPACT ATTENUATOR (BIDIRECTIONAL)	WORK ZONE RAISED PAVEMENT MARKER	BARRIER REFLECTOR, TYPE B	MAINTAINING TRAFFIC, MISC.: TEMPORARY DRAINAGE OUTLET FOR MOT	WORK ZONE LANE LINE, CLASS I	WORK ZONE LANE LINE, CLASS I, AS PER PLAN	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, AS PER PLAN	WORK ZONE CHANNELIZING LINE, CLASS I	WORK ZONE CHANNELIZING LINE, CLASS I, AS PER PLAN	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT, AS PER PLAN	WORK ZONE STOP LINE, CLASS I	WORK ZONE ARROW, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	CONCRETE BARRIER, SINGLE SLOPE, TYPE BI	BARRIER TRANSITION	CONCRETE BARRIER, END ANCHORAGE, REINFORCED, TYPE BI	PORTABLE BARRIER, 50", AS PER PLAN	PORTABLE BARRIER, 50", BRIDGE MOUNTED, AS PER PLAN	LIGHT POLE, MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION	SIGN ERECTED, TEMPORARY OVERLAY	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION, AS PER PLAN	SIGNING, MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER	
					FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	SY	FT	EACH	EACH	FT	FT	EACH	SF	EACH	EACH			
		E. SOUTH ST.																															
EW-2	127	50+10.50	51+77	01/IMS/PV																													
EW-3	127	37+50	39+52	01/IMS/PV																													
A-4	128	46+95	46+95	01/IMS/PV																													
EY-6	127-128	37+50	48+82	01/IMS/PV																													
EW-4	128	47+02	47+56	01/IMS/PV																													
S-4	128	46+97	47+10	01/IMS/PV																													
		E. VORIS ST.																															
S-4A	128	17+70		01/IMS/PV																													
		S. MAIN ST.																															
EY-7	127-129	27+46	55+54	01/IMS/PV																													
EY-8	128+138	45+80	82+44	01/IMS/PV																													
EY-8A	128	44+47	45+63	01/IMS/PV																													
EY-9	128	47+05	47+53	01/IMS/PV																													
EY-10	128-129	49+27	55+91	01/IMS/PV																													
	128	44+50		01/IMS/PV																													
S-5	129	55+26		01/IMS/PV																													
EW-5	129+132	50+32	55+57	01/IMS/PV																													
		E. THORNTON ST.																															
S-6	129	55+91	56+02	01/IMS/PV																													
S-7	129	55+57	55+68	01/IMS/PV																													
S-8	129	55+64	55+75	01/IMS/PV																													
S-9	129	56+08	56+18	01/IMS/PV																													
LL-7	128-129	47+41	55+44	01/IMS/PV																													
LL-8	129-130	56+20	163+32	01/IMS/PV																													
C-1	129+134	56+08	22+00	01/IMS/PV																													
C-2	129+133	114+26	110+25	01/IMS/PV																													
C-3	129	14+93	18+64	01/IMS/PV																													
EW-6	128+133	110+15	55+57	01/IMS/PV																													
EW-7	129-131	13+90	166+30	01/IMS/PV																													
S-10	129	56+20		01/IMS/PV																													
EW-8	129	55+54	27+00 (E. Thornton St.)	01/IMS/PV																													
EY-11	129-130	55+86	162+46	01/IMS/PV																													
EY-12	129-130	56+13	166+99	01/IMS/PV																													
	130	64+95		01/IMS/PV																													
EY-13		NOT	USED																														
EY-14		NOT	USED																														
PB-1	130-131	164+46	170+16	01/IMS/PV																													
A-5	130	8+92 (E. Bartges)		01/IMS/PV																													
A-6	130	9+29 (E. Bartges)		01/IMS/PV																													
A-7	131	20+60 (E. Bartges)		01/IMS/PV																													
A-8	131	20+94 (E. Bartges)		01/IMS/PV																													
S-11	131	9+33 (E. Bartges)		01/IMS/PV																													
S-12	131	168+29		01/IMS/PV																													
S-13	131	20+60 (E. Bartges)		01/IMS/PV																													
PHASE I / STAGE IC (S. MAIN ST. & S. BROADWAY ST.) SUBTOTALS							1			11		0.28		0.20	1.94												194	5	622		550	80.0	
PHASE I SUBTOTALS (CARRIED TO SHEET 80)							1			11		0.28		0.20	1.94													194	5	622		550	80.0

KMK	4/21/16	TEMP. OVERLAY QUANTITY REVISION
MEP	3/30/16	TEMP. OVERLAY QUANTITY REVISION
REV. BY	DATE	DESCRIPTION

Maintenance of Traffic Subsummary

SUM -76 -10.00

CALCULATED AP CHECKED RT

74
1822

REFERENCE NO.	SHEET NO.	LOCATION	PARTICIPATION (100% CITY OR PROJECT)	202	611	614	614	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622	622	625	630	630	630				
				CONCRETE BARRIER REMOVED	INLET, MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET	WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL)	WORK ZONE IMPACT ATTENUATOR (BIDIRECTIONAL)	WORK ZONE RAISED PAVEMENT MARKER	BARRIER REFLECTOR, TYPE B	MAINTAINING TRAFFIC, MISC.: TEMPORARY DRAINAGE OUTLET FOR MOT	WORK ZONE LANE LINE, CLASS I	WORK ZONE LANE LINE, CLASS I, AS PER PLAN	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, AS PER PLAN	WORK ZONE CHANNELIZING LINE, CLASS I	WORK ZONE CHANNELIZING LINE, CLASS I, AS PER PLAN	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT, AS PER PLAN	WORK ZONE STOP LINE, CLASS I	WORK ZONE ARROW, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	CONCRETE BARRIER, SINGLE SLOPE, TYPE BI	BARRIER TRANSITION	CONCRETE BARRIER, END ANCHORAGE, REINFORCED, TYPE BI	PORTABLE BARRIER, 50", AS PER PLAN	PORTABLE BARRIER, 50", BRIDGE MOUNTED, AS PER PLAN	LIGHT POLE, MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION	SIGN ERECTED, TEMPORARY OVERLAY	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION, AS PER PLAN	SIGNING, MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER		
		FROM	TO	FT	EACH	EACH	EACH	EACH	EACH	EACH	MILE	MILE	MILE	MILE	MILE	FT	FT	FT	FT	FT	EACH	SY	FT	EACH	EACH	FT	FT	EACH	SF	EACH	EACH		
S. MAIN ST. & S. BROADWAY ST.																																	
C-3	148	120+05 (E. Miller St.)	121+17 (E. Miller St.)											0.02																			
C-4	148	20+54	24+01											0.07																			
S-1	148	119+08																							12								
S-2	148	120+05																							12								
EW-3	148-149	122+17 (E. Miller St.)	33+00											0.28																			
EY-1	148-149	24+01	37+01											0.25																			
EY-2	148-149	24+01	26+50											0.05																			
EW-4	148-149,161	20+10	972+25											0.25																			
EY-3	150	972+26	973+51											0.02																			
S-4	150	37+10	37+10																					12									
EW-5	150	10+44	13+13											0.05																			
EY-4	150-152	37+26	55+10											0.34																			
LL-3	151-152	43+30	54+90								0.22																						
EW-6	150	37+19	40+28											0.06																			
EY-5	150	38+95	40+30											0.03																			
EW-7	151-152	43+70	55+10											0.22																			
A-1	152	52+75																															
A-2	152	55+10																															
S-8	152	55+26																															
LL-4	152-154	56+19	162+50									0.12																					
		153	64+95																														
EY-6	152-155	55+40	166+40											0.21																			
EY-7	152-155	55+80	176+10											0.38																			
PB-1	153-154	152+00	160+50																														
A-3	154	8+92																															
A-4	154	9+29																															
A-5	154	20+57																															
A-6	154	20+82																															
S-5	154	157+75																															
A-7	154	158+90																															
A-8	154	158+90																															
S-6	155	176+10																															
S-7		NOT	USED																														
EW-8	156+152	110+50	113+60											0.06																			
C-6	156+152	110+50	113+60																														
EW-9	156+152	112+20	113+60											0.03																			
EW-10	157-158	119+50	Wolf Ledges Ramp											0.16																			
C-7	157-158	119+50	124+00											0.09																			
PHASE 1 / STAGE 2A SUBTOTALS														17	0.34	0.18	2.39	310														48.0	
S. MAIN ST., S. BROADWAY ST. & E. THORNTON ST.															0.03																		
LL-1	163	10+50	12+00																														
EY-1	163	972+10	973+50																														
EW-1	163	972+50	36+50 (S. Main St.)											0.02																			
S-1	163	973+50	973+50																														
EY-2	163	36+50	37+00											0.01																			
PHASE 1 / STAGE 2B SUBTOTALS															0.03			0.06															
PHASE 1 SUBTOTALS (CARRIED TO SHEET 80)															17	0.37	0.18	2.45	310														48.0

KMK	4/21/16	TEMP. OVERLAY QUANTITY REVISION
MEP	3/30/16	TEMP. OVERLAY QUANTITY REVISION
REV. BY	DATE	DESCRIPTION
		DATE COMPLETED

Maintenance of Traffic Subsummary

SUM -76 -10.00

CALCULATED
AP
CHECKED
RT

REFERENCE NO.	SHEET NO.	LOCATION	PARTICIPATION (100% CITY OR PROJECT)	202	611	614	614	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622	622	625	630	630	630		
				CONCRETE BARRIER REMOVED	INLET, MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET	WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL)	WORK ZONE IMPACT ATTENUATOR (BIDIRECTIONAL)	WORK ZONE RAISED PAVEMENT MARKER	BARRIER REFLECTOR, TYPE B	MAINTAINING TRAFFIC, MISC.: TEMPORARY DRAINAGE OUTLET FOR MOT	WORK ZONE LANE LINE, CLASS I	WORK ZONE LANE LINE, CLASS I, AS PER PLAN	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, AS PER PLAN	WORK ZONE CHANNELIZING LINE, CLASS I	WORK ZONE CHANNELIZING LINE, CLASS I, AS PER PLAN	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT, AS PER PLAN	WORK ZONE STOP LINE, CLASS I	WORK ZONE ARROW, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	CONCRETE BARRIER, SINGLE SLOPE, TYPE BI	BARRIER TRANSITION	CONCRETE BARRIER, END ANCHORAGE, REINFORCED, TYPE BI	PORTABLE BARRIER, 50", AS PER PLAN	PORTABLE BARRIER, 50", BRIDGE MOUNTED, AS PER PLAN	LIGHT POLE, MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION	SIGN ERECTED, TEMPORARY OVERLAY	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION, AS PER PLAN	SIGNING, MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER
		FROM	TO	FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	SY	FT	EACH	EACH	FT	FT	EACH	SF	EACH	EACH		
GRANT ST.																															
CH-4	206-208	496+50	517+70																												
CH-5	206-208	496+50	517+70																												
CH-6	206-208	496+50	517+70																												
EW-2	206-208	496+50	514+70												0.40																
EY-2	206-208	496+50	514+70												0.40																
PB-1	205-206	495+30	501+00																												
PB-1A	206-207	501+00	509+70																												
PB-2	206	496+50	498+50																												
PB-2A	206-208	498+50	513+14																												
PB-3	206	498+34	499+53																												
PB-4	206	498+74	500+29																												
PHASE 1 / STAGE 1 SUBTOTALS						3			69						0.80		6360														
GRANT ST.																															
CH-1	210-212	493+44	506+66																												
CH-2	210-212	493+44	506+66																												
CH-3	210-212	493+44	506+66																												
EW-1	210-212	493+44	506+66												0.25																
EY-1	210-212	493+44	506+66												0.25																
CH-4	210-212	493+18	511+55																												
CH-5	210-212	493+18	511+55																												
CH-6	210-212	493+18	511+55																												
EY-2	210-212	493+18	511+55												0.35																
EY-2	210-212	493+18	511+55												0.35																
PB-1	210-212	493+18	508+44																												
PB-2	211	497+74	500+53																												
PB-3	211	498+77	502+75																												
CH-1	213-217	484+50	514+70																												
CH-2	213-217	483+60	514+70																												
CH-3	213-217	483+60	514+70																												
CH-4	213-217	485+09	514+60																												
CH-5	213-217	485+09	514+60																												
CH-6	213-217	485+09	514+60																												
EW-1	214-217	482+50	514+70												0.61																
EY-1	214-217	483+60	514+70												0.59																
PB-1	213-215	483+07	498+50																												
PB-1A	215-217	498+50	513+10																												
EY-2	213-216	485+09	514+60												0.56																
EW-2	213-216	485+09	514+60												0.56																
PB-2	214-215	495+61	501+00																												
PB-2A	215-216	501+00	509+70																												
PB-3	215	498+34	499+53																												
PB-4	215	498+74	500+29																												
PHASE 1 / STAGE 3 SUBTOTALS						4			141						3.52		27570														
PHASE 1 SUBTOTALS (THIS SHEET)						7			210						4.32		33930														
SUBTOTALS (CARRIED FROM SHEET 73)														0.79		0.04	1.50	1461													
SUBTOTALS (CARRIED FROM SHEET 74)						1			11				0.28		0.20	1.94															
SUBTOTALS (CARRIED FROM SHEET 75)													0.76		0.30	1.23		547													
SUBTOTALS (CARRIED FROM SHEET 76)													0.37		0.18	2.45		310													
SUBTOTALS (CARRIED FROM SHEET 77)													0.16		0.30	0.68		334													
SUBTOTALS (CARRIED FROM SHEET 78)													1.45		0.62	1.47		595													
SUBTOTALS (CARRIED FROM SHEET 79)						3			105				0.13			0.14	3.30	37060													
PHASE 1 TOTALS (CARRIED TO SHEET 87)						1450			393				3.94		1.64	9.41	7.62	3247	70990												

CALCULATED	AP	CHECKED	RT
MAINTENANCE OF TRAFFIC SUBSUMMARY			
SUM - 76 - 10.00			
80			
1822			

KMK	4/21/16	TEMP. OVERLAY QUANTITY REVISION
MEP	3/30/16	TEMP. OVERLAY QUANTITY REVISION
REV. BY	DATE	DESCRIPTION

80.0

48.0

128.0

REFERENCE NO.	SHEET NO.	LOCATION	PARTICIPATION (100% CITY OR PROJECT)	202	611	614	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622	622	625	630	630	630								
				CONCRETE BARRIER REMOVED	INLET, MISC.: REMOVE, PROTECT AND RECONSTRUCT BARRIER MEDIAN INLET	WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL)	WORK ZONE IMPACT ATTENUATOR (BIDIRECTIONAL)	WORK ZONE RAISED PAVEMENT MARKER	BARRIER REFLECTOR, TYPE B	MAINTAINING TRAFFIC, MISC.: TEMPORARY DRAINAGE OUTLET FOR MOT	WORK ZONE LANE LINE, CLASS I	WORK ZONE LANE LINE, CLASS I, AS PER PLAN	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, AS PER PLAN	WORK ZONE CHANNELIZING LINE, CLASS I	WORK ZONE CHANNELIZING LINE, CLASS I, AS PER PLAN	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT	WORK ZONE DOTTED LINE, CLASS I, 642 PAINT, AS PER PLAN	WORK ZONE STOP LINE, CLASS I	WORK ZONE ARROW, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	CONCRETE BARRIER, SINGLE SLOPE, TYPE B1	BARRIER TRANSITION	CONCRETE BARRIER, END ANCHORAGE, REINFORCED, TYPE B1	PORTABLE BARRIER, 50", AS PER PLAN	PORTABLE BARRIER, 50", BRIDGE MOUNTED, AS PER PLAN	LIGHT POLE, MISC.: REMOVE, SALVAGE AND RE-ERECT MEDIAN LIGHT POLE WITH RECONSTRUCTION OF MEDIAN FOUNDATION	SIGN ERECTED, TEMPORARY OVERLAY	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION, AS PER PLAN	SIGNING, MISC.: REMOVE, SALVAGE AND RE-ERECT CONCRETE MEDIAN BARRIER-MOUNTED MILE MARKER					
		FROM	TO	FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	SY	FT	EACH	EACH	FT	FT	EACH	SF	EACH	EACH								
		I.R. 76																																		
LL-8	275-276	48+00	54+80									0.13																								
A-8	276	54+10																	1																	
CH-1B	276	53+65	54+85												120																					
A-9	276	54+75																																		
S-5	276	54+80																																		
S-6	276	56+15																																		
A-10	276	56+25																																		
LL-9	276	56+15	161+71																																	
LL-10	276-278	56+15	266+80									0.11																								
LL-11	276-277	56+15	262+21									0.20																								
A-11	276	56+90																																		
CH-5A	276-277	56+15	58+15																																	
EW-6		NOT USED																																		
EY-8	276-277	56+15	161+50														0.10																			
EY-9	276-277	56+15	161+50														0.10																			
EW-7	277-278	59+00	9+14														0.17																			
CH-3	277	361+00	363+50																																	
S-7	278	158+49																																		
CH-5	278-279	159+54	162+54																																	
A-12	278	158+73																																		
CH-6	278	158+49	159+24																																	
CH-4	277	161+50	163+50														0.01																			
EY-1	280-289	434+10	515+50																																	
DW-1	280	438+60	444+00																																	
EY-2	280-290	442+94	523+00																																	
D-1	280	447+01																																		
L-1	280	445+89																																		
M-1	280	446+25																																		
PB-1	280-281	444+00	449+10																																	
PB-2	280-281	444+40	449+50																																	
PB-3	282	456+50	466+10																																	
PB-4	282	456+90	466+50																																	
PB-5	284	477+50	479+10																																	
PB-6	284	477+90	479+50																																	
PB-7	289	499+50	514+10																																	
PB-8	289	499+90	514+50																																	
L-2	288	511+35																																		
D-2	289	512+50	512+50																																	
	280	444+50	449+00																																	
	289	509+00	517+00																																	
PHASE 4 SUBTOTALS						2	8					0.55				3.44		820		540		126	5		1250	2	4	6460		2		1				
PHASE 4 SUBTOTALS (THIS SHEET)						2	8					0.55				3.44		820		540		126	5		1250	2	4	6460		2		1				
PHASE 4 SUBTOTALS (CARRIED FROM SHEET 86)												0.93				1.03		1189																		
PHASE 4 TOTALS						2	8					1.48			4.47		2009		540							1250	2	4	6460		2		1			
PHASE 1 TOTALS (CARRIED FROM SHEET 80)												1450			3.94		1.64	9.41	7.62	3247	70990		852	23	14578			18198			128.0					
PHASE 2 TOTALS (CARRIED FROM SHEET 84)														3	0.12	2.63	0.24	7.59	452	5563							15576	530		232.5	1					
PHASE 3 TOTALS (CARRIED FROM SHEET 86)																0.53	3.40	0.16	0.76	7.91	236	12535		5270	36		13284	225		9.0						
TOTALS CARRIED TO GENERAL SUMMARY						1450	2	25					1194	942	3	6.07	6.03	1.80	14.88	23.12	5944	89088	540	6905	1136	35	16086	1250	2	4	53518	755	2	395.0	1	1

REV. BY	DATE	DESCRIPTION
KMS	4/21/16	TEMP OVERLAY QUANTITY REVISED
KMS	4/7/16	IMPACT ATTENUATOR (UNI/B1) QUANTITY REVISION/STA. ADJUST.
MEP	3/30/16	PCB AND TEMP. OVERLAY QUANTITY REVISION

CROSS REFERENCES

FOR SUBSUMMARIES AND CALCULATIONS, SEE SHEETS 304-369
 FOR RAMP W-5 PROFILE, SEE SHEET 418
 FOR RAMP W-5 CROSS SECTIONS, SEE SHEETS 422 - 430
 FOR SUPERELEVATION TABLES, SEE SHEETS 661 - 667
 FOR RAMP W-5 DRAINAGE PLANS, SEE SHEETS 861 - 865
 FOR REMOVAL PLAN, SEE SHEET 733
 FOR DRAINAGE PLAN AND PROFILE, SEE SHEETS 852 - 963
 FOR WATER WORK PLAN AND PROFILE, SEE SHEETS 1000- 1033
 FOR GRADING DETAILS, SEE SHEETS 793 - 794
 FOR BRIDGE SUM-76-1051 DETAILS, SEE SHEETS 1422- 1464
 FOR WALL 8 PLANS AND DETAILS, SEE SHEETS 1670- 1673

B CONST. RAMP W-5

AA P.I. Sta. 581+90.39
 $\Delta = 70^\circ 50' 03''$ (L.T.)
 $Dc = 16^\circ 30' 00''$
 $R = 347.25'$
 $T = 246.93'$
 $L = 429.30'$
 $E = 78.85'$
 $e_{max} = 0.060$
 P.C. Sta. 579+43.46
 C.S. Sta. 583+72.75
 DESIGN SPEED = 35 mph

BEGIN TYPE 6 CURB
 (MATCH EXISTING)
 +40.78, 26.23' RT.
 E. VORIS ST.

END TYPE 6 CURB
 (MATCH EXISTING)
 +52.81, 20.08' LT.
 E. VORIS ST.

CALCULATED
 MJT
 CHECKED
 KMK

0 20 40
 HORIZONTAL
 SCALE IN FEET

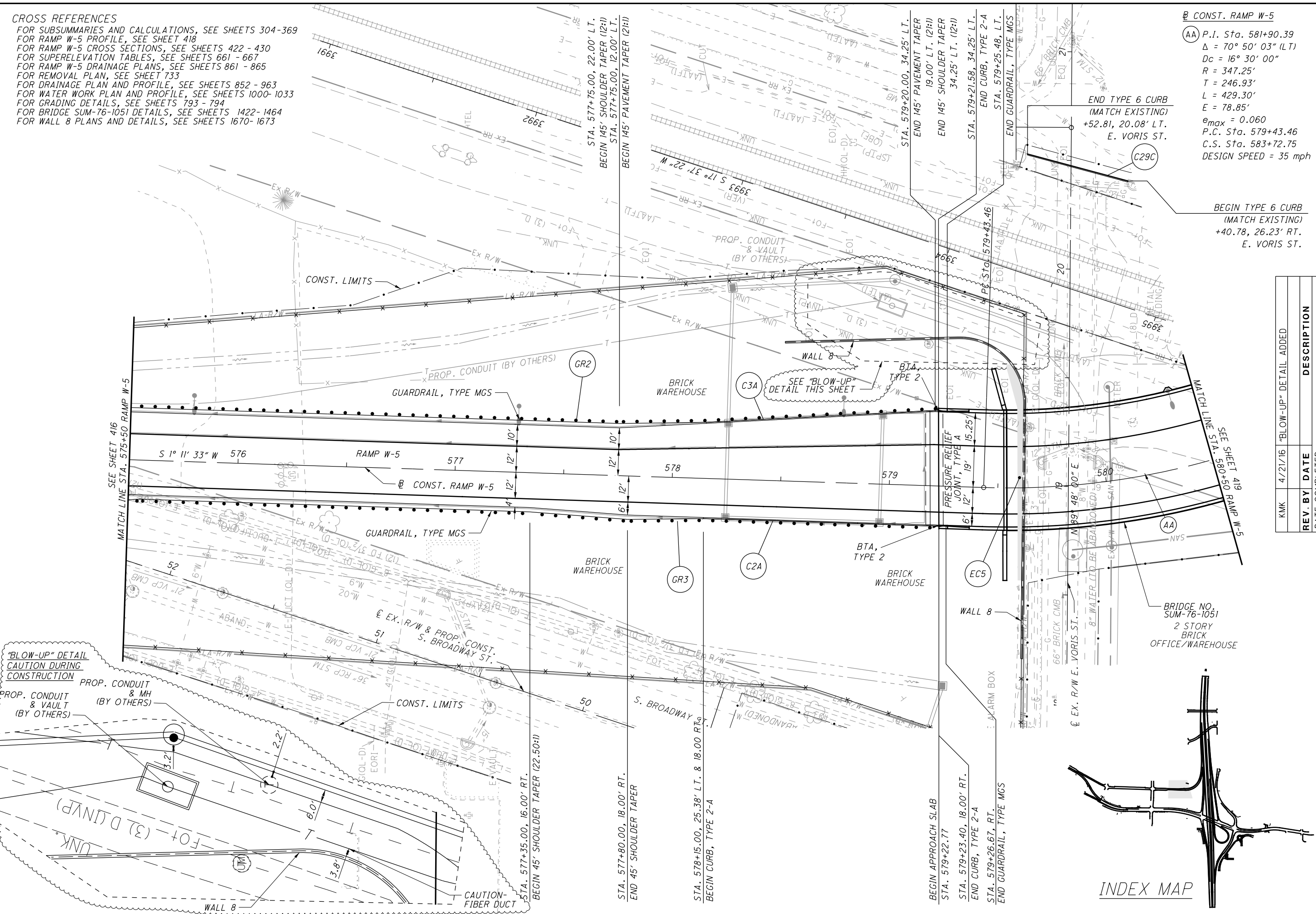
REV. BY	DATE	DESCRIPTION
KMK	4/21/16	"BLOW-UP" DETAIL ADDED
		DATE COMPLETED

**PLAN - RAMP W-5
 STA. 575+50 TO STA. 580+50**

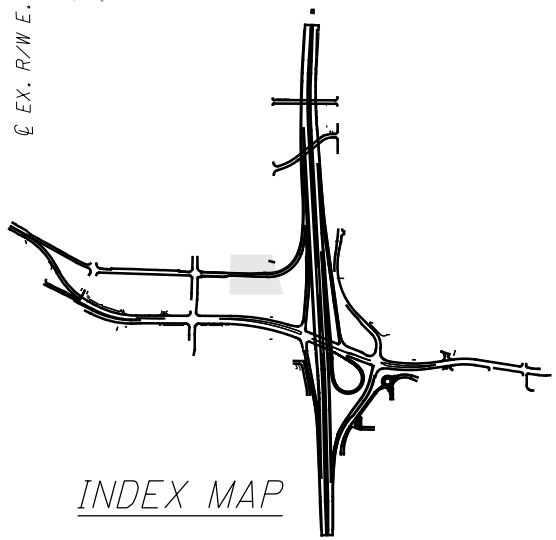
SUM-76-10.00

417
 1822

I:\Projects\Main Broadway\ROADWAY\Sheets\77269GP018.dgn 4/22/2016 3:42:11 PM kyle_koppes



"BLOW-UP" DETAIL
 CAUTION DURING
 CONSTRUCTION
 PROP. CONDUIT
 & VAULT
 (BY OTHERS)



CONST. RAMP W-5

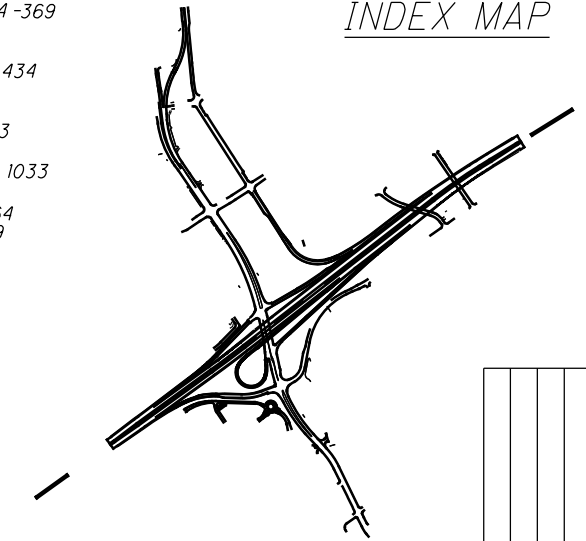
AA P.I. Sta. 581+90.39
 $\Delta = 70^\circ 50' 03''$ (LT)
 $Dc = 16^\circ 30' 00''$
 $R = 347.25'$
 $T = 246.93'$
 $L = 429.30'$
 $E = 78.85'$
 $e_{max} = 0.060$
 P.C. Sta. 579+43.46
 C.S. Sta. 583+72.75
 DESIGN SPEED = 35 mph

BB P.I. STA. 584+29.07
 $\Delta = 14^\circ 03' 45''$ (LT)
 $Dc1 = 16^\circ 30' 00''$
 $R1 = 347.25'$
 $Dc2 = 2^\circ 15' 00''$
 $R2 = 2,546.48'$
 $Ls = 150.00'$
 $p = 2.33'$
 $\Delta1 = 1^\circ 41' 07''$
 $\Delta2 = 12^\circ 22' 38''$
 $T1 = 56.32'$
 $T2 = 94.35'$
 $e1 = 0.060$
 $e2 = 0.038$
 C.S. Sta. 583+72.75
 S.C. Sta. 585+22.75

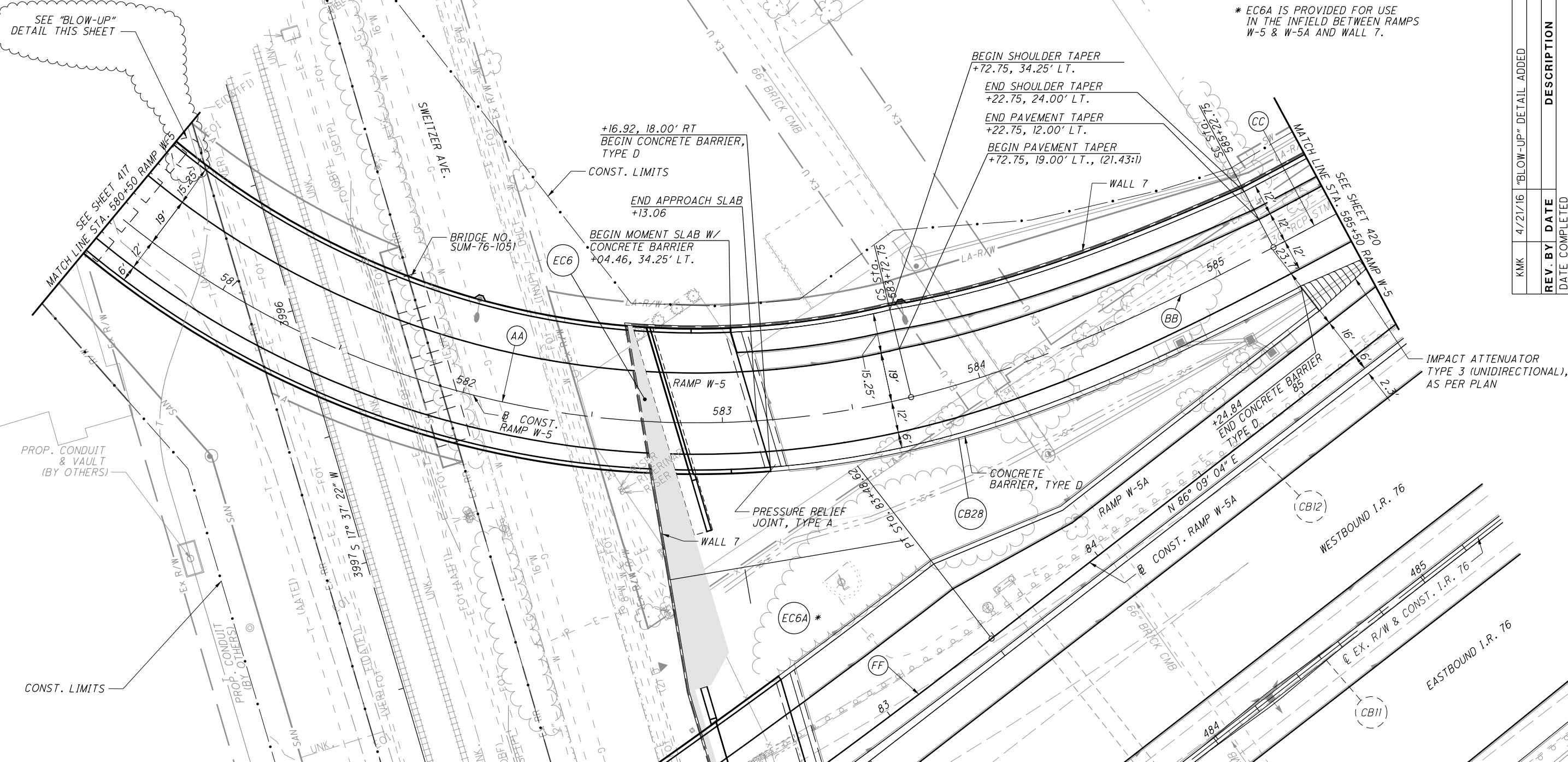
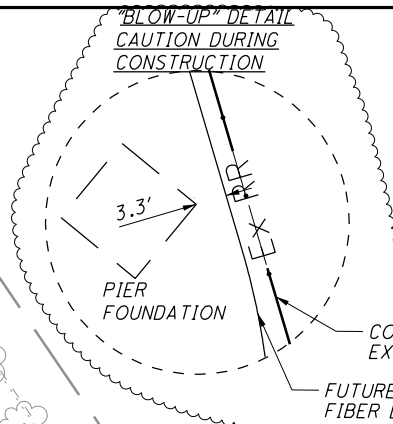
CROSS REFERENCES

FOR SUBSUMMARIES AND CALCULATIONS, SEE SHEETS 304-369
 FOR RAMP W-5 PROFILE, SEE SHEET 421
 FOR RAMP W-5A CROSS SECTIONS, SEE SHEETS 422 - 430
 FOR RAMP W-5A PLAN AND PROFILES, SEE SHEETS 432 - 434
 FOR SUPERELEVATION TABLES, SEE SHEETS 661 - 667
 FOR REMOVAL PLAN, SEE SHEET 734
 FOR DRAINAGE PLAN AND PROFILE, SEE SHEETS 852 - 963
 FOR RAMP W-5 DRAINAGE PLANS, SEE SHEETS 861 - 865
 FOR WATER WORK PLAN AND PROFILE, SEE SHEETS 1000-1033
 FOR GRADING DETAILS, SEE SHEETS 793 - 794
 FOR BRIDGE SUM-76-1051 DETAILS, SEE SHEETS 1422-1464
 FOR WALL 7 PLANS AND DETAILS, SEE SHEETS 1655-1669

INDEX MAP



SEE "BLOW-UP" DETAIL THIS SHEET



* EC6A IS PROVIDED FOR USE IN THE INFIELD BETWEEN RAMPS W-5 & W-5A AND WALL 7.

REV. BY	DATE	DESCRIPTION
KMK	4/21/16	"BLOW-UP" DETAIL ADDED

PLAN - RAMP W-5
 STA. 580+50 TO STA 585+50

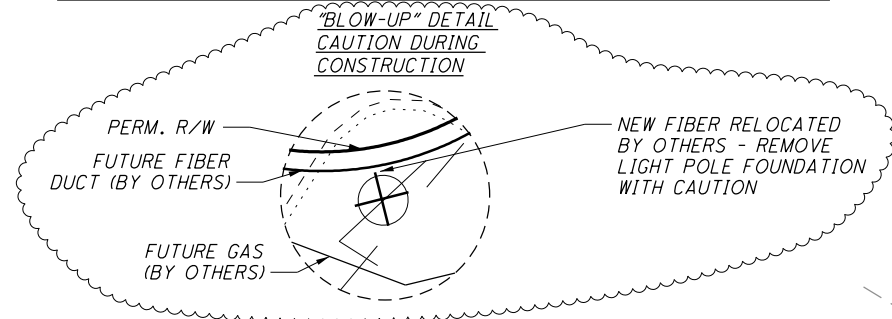
SUM-76-10.00

I:\Projects\Main_Roadway\Roadway\Sheets\77269GP019.dgn 4/22/2016 3:39:52 PM kyle_koppes

PROP. R/W & CONST. RUSSELL AVE.

(R) P.I. Sta. 10+84.71
 $\Delta = 10^\circ 17' 59''$ (LT)
 $D_c = 11^\circ 15' 00''$
 $R = 509.30'$
 $T = 45.90'$
 $L = 91.55'$
 $E = 2.06'$
 $e_{max} = NC$
 P.C. Sta. 10+38.81
 P.T. Sta. 11+30.36

REV. BY	DATE	DESCRIPTION
KMK	4/21/16	"BLOW-UP" DETAIL ADDED
JEM	4/7/16	ADDED LABEL FOR TREES TO BE REMOVED BY OTHERS
DATE COMPLETED		



CROSS REFERENCES

FOR SUBSUMMARIES AND CALCULATIONS, SEE SHEETS 304-369
 FOR RAMP W-8 PLAN AND PROFILES, SEE SHEETS 467 - 471
 FOR RUSSELL AVE. PROFILE, SEE SHEET 598
 FOR REMOVAL PLAN, SEE SHEET 775
 FOR DRAINAGE PLAN AND PROFILE, SEE SHEETS 852 - 963
 FOR SANITARY PLAN AND PROFILE, SEE SHEETS 978 - 992
 FOR WATER WORK PLAN AND PROFILE, SEE SHEETS 1000-1033
 FOR GRADING DETAILS, SEE SHEETS 793 - 794
 FOR WALL #1 PLANS, SEE SHEETS 1621 - 1624

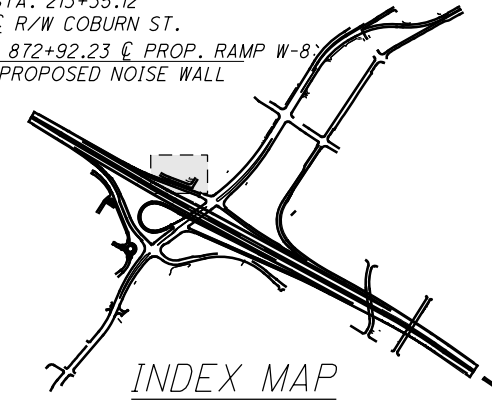
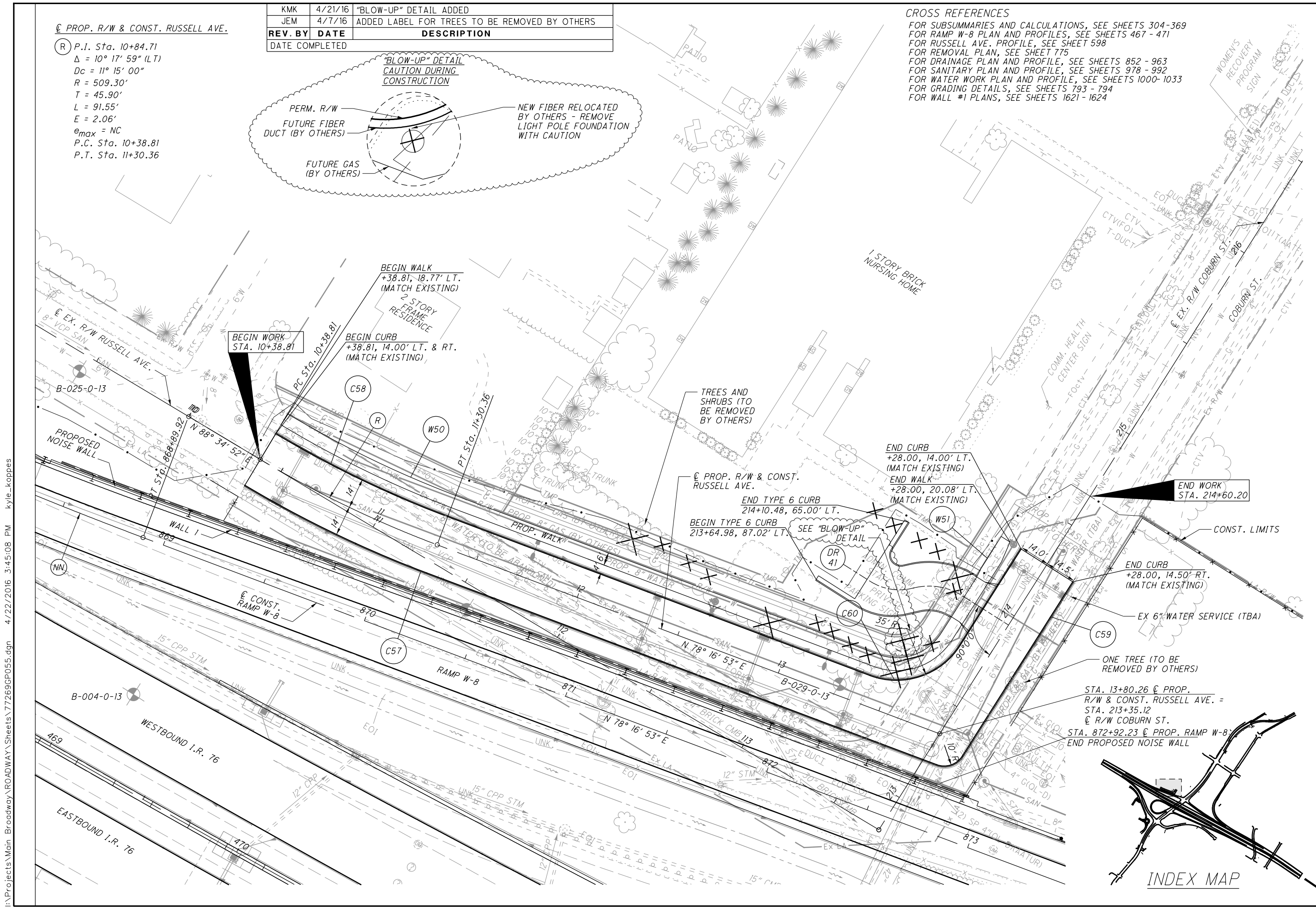


CALCULATED MJT CHECKED KMK

PLAN - RUSSELL AVE.
STA. 10+00 TO STA 16+00

SUM-76-10.00


597
1822




INDEX MAP

I:\Projects\Main_Broadway\ROADWAY\Sheets\77269GP055.dgn 4/22/2016 3:45:08 PM kyle_koppes

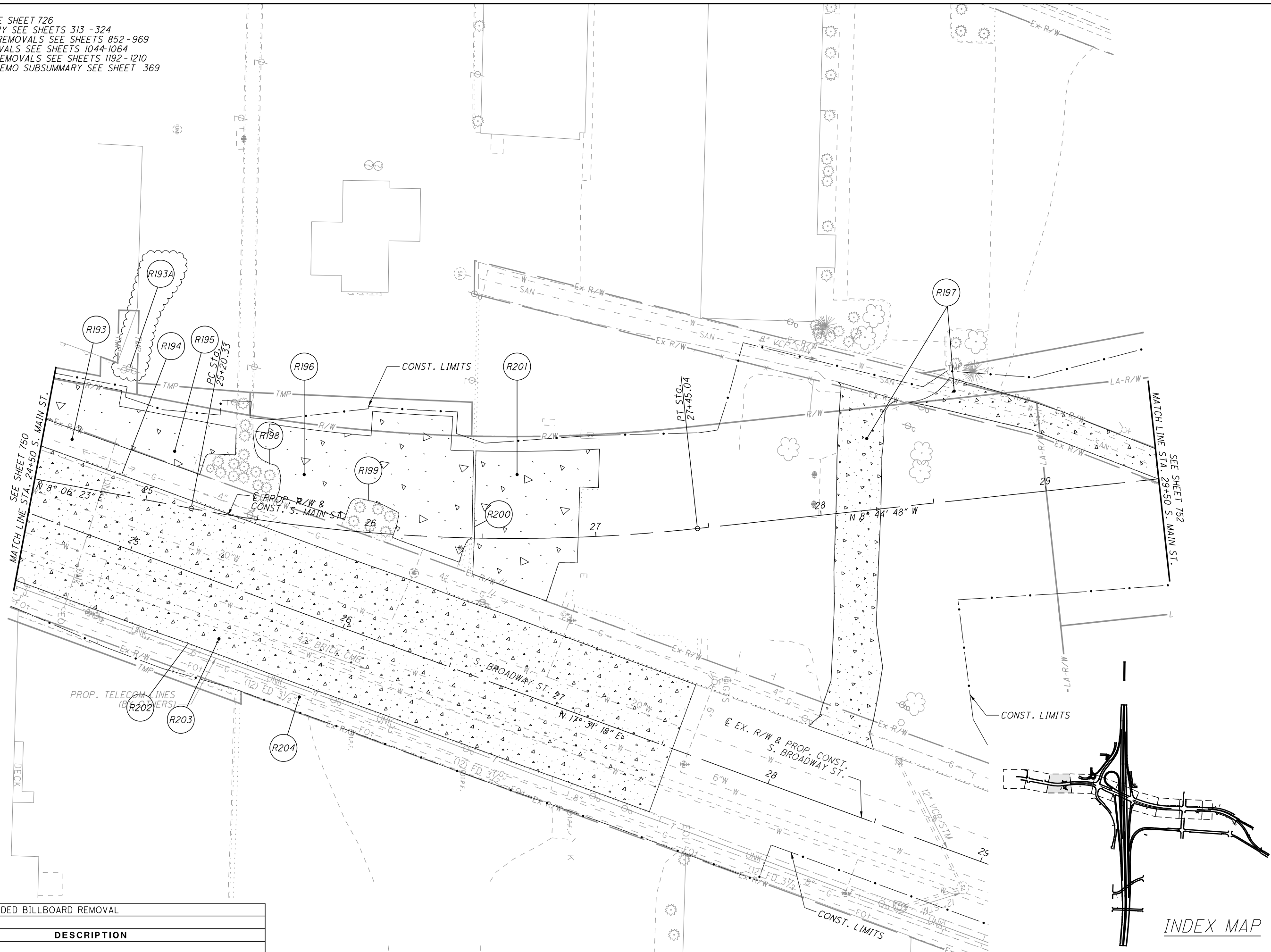
FOR LEGEND SEE SHEET 726
 FOR SUBSUMMARY SEE SHEETS 313 - 324
 FOR DRAINAGE REMOVALS SEE SHEETS 852 - 969
 FOR SIGN REMOVALS SEE SHEETS 1044 - 1064
 FOR LIGHTING REMOVALS SEE SHEETS 1192 - 1210
 FOR BUILDING DEMO SUBSUMMARY SEE SHEET 369





 HORIZONTAL SCALE IN FEET

CALCULATED	
MJT	
CHECKED	
KMK	



**REMOVAL PLAN - S. MAIN ST.
 STA. 24+50 TO STA. 29+50**

SUM - 76 - 10.00

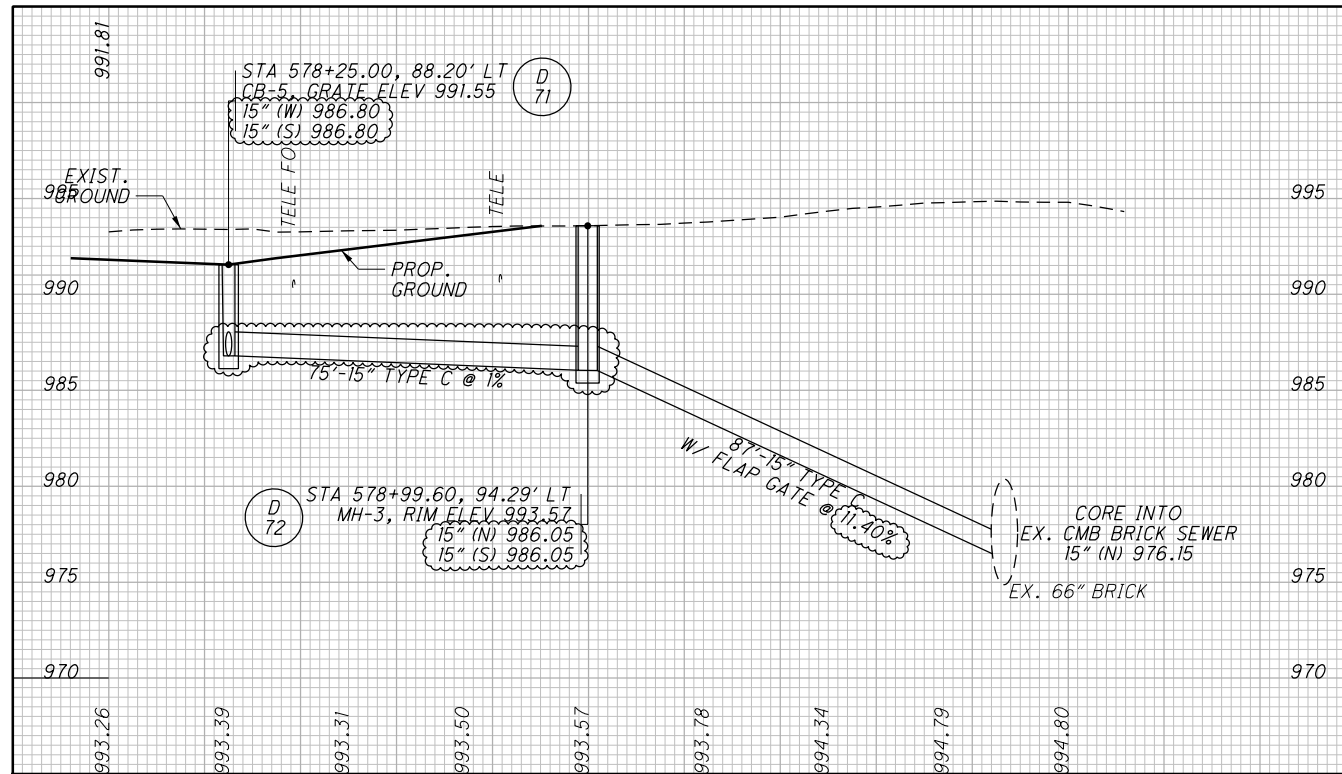
751
1822

I:\Projects\Main_Broadway\Roadway\Sheets\77269GM109.dgn 4/21/2016 2:59:49 PM kyle_koppes

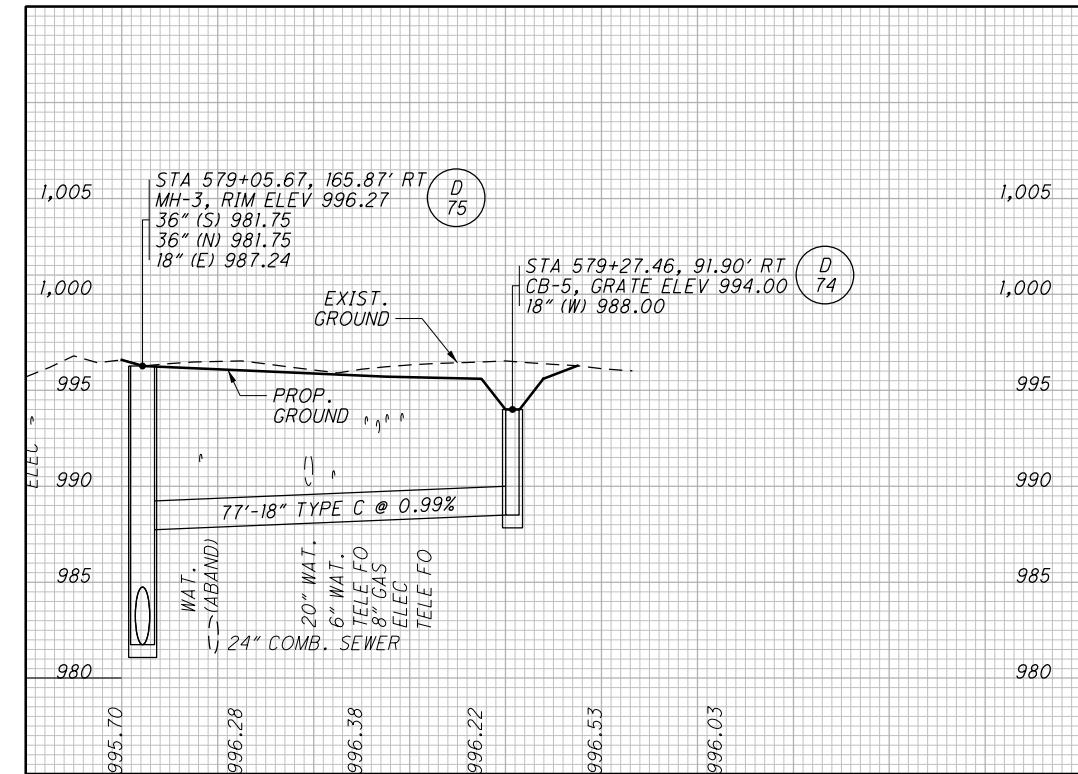
KMK	4/21/16	ADDED BILLBOARD REMOVAL
REV. BY	DATE	DESCRIPTION

INDEX MAP

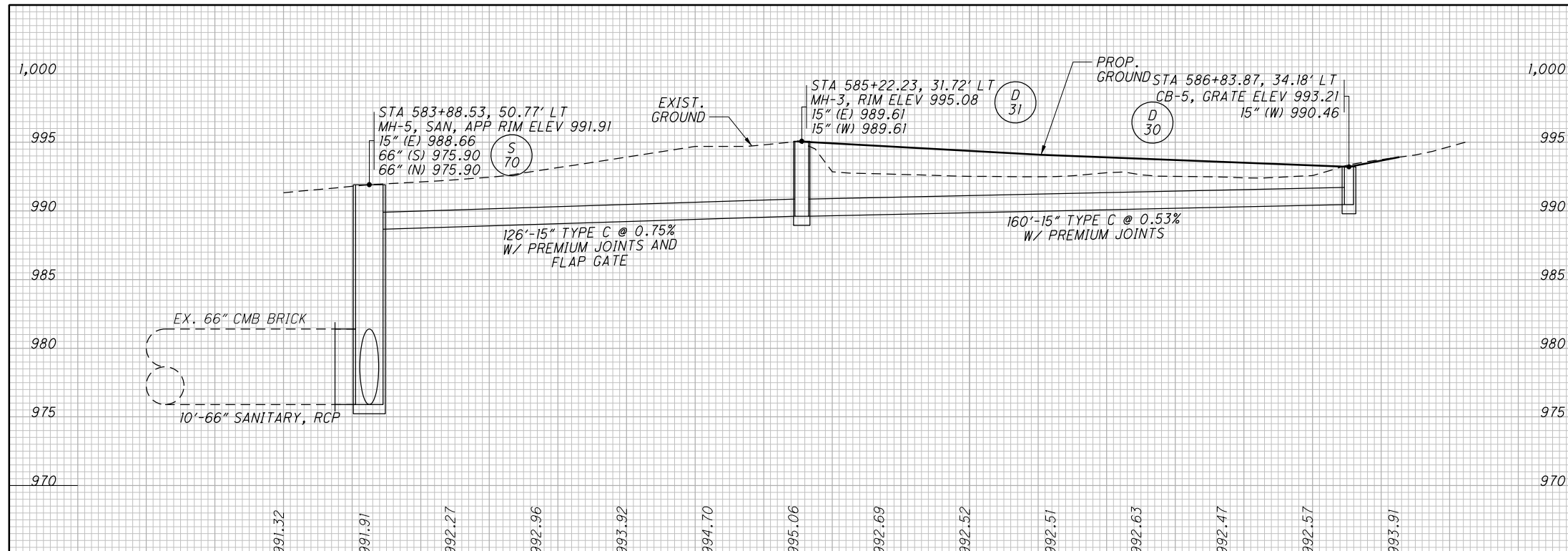
PROFILE (F) - RAMP W-5, STA. 578+25.00 TO STA. 578+99.60, LEFT SIDE



PROFILE (G) - RAMP W-5, STA. STA 579+05.67, 165.87' RT TO STA. STA 579+27.46, 91.90' RT, RIGHT SIDE



PROFILE (H) - RAMP W-5, STA. 583+88.53 TO STA. 586+94.00, LEFT SIDE



CALCULATED	AMP	CHECKED
		KMK

STORM SEWER PROFILES

REV. BY	DATE	DESCRIPTION
DATE COMPLETED		

SUM - 76 - 10.00

959
1822

KMK 4/18/16 INVERTS DROPPED 1.5' ON D71 AND D72 (NO QUANTITY CHANGES)

I:\Projects\Main_Broadway\STRUCTURES\Wall_01\Sheets\77269WIEQ002.dgn 4/25/2016 10:14:52 AM kyle_koppes

ITEM ODOT	EXT.	PARTICIPATION	TOTAL	UNIT	DESCRIPTION	MSE WALL 1	WALL 2	MSE WALL 3	MSE WALL 4	MSE WALL 5	MSE WALL 6	MSE WALL 7	MSE WALL 8	WALL 9	WALL 10	WALL 11	MODULAR BLOCK WALL	REF. SHEET	
		02/IMS/BR																	
517	75120	416	416	FT	RAILING (CONCRETE PARAPET WITH TWIN STEEL TUBE RAILING)									416					
517	76300	124	124	FT	RAILING, MISC.: REATTACH EXISTING HANDRAIL		124												(1626/1822)
518	21200	298	298	CY	POROUS BACKFILL WITH FILTER FABRIC									26	272				
518	40000	730	730	FT	6" PERFORATED CORRUGATED PLASTIC PIPE									416	314				
518	40010	61	61	FT	6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS									50	11				
524	94703	445	445	FT	DRILLED SHAFTS, 36" DIAMETER, ABOVE BEDROCK, AS PER PLAN									445					(1676/1822)
524	94803	405	405	FT	DRILLED SHAFTS, 42" DIAMETER, ABOVE BEDROCK, AS PER PLAN									405					(1676/1822)
524	94903	465	465	FT	DRILLED SHAFTS, 48" DIAMETER ABOVE BEDROCK, AS PER PLAN									465					(1676/1822)
524	94915	745	745	FT	DRILLED SHAFTS, 60" DIAMETER, ABOVE BEDROCK, AS PER PLAN									745					(1676/1822)
524	94919	124	124	FT	DRILLED SHAFTS, 60" DIAMETER, INTO BEDROCK, AS PER PLAN									124					(1676/1822)
SPECIAL	53013000	6,720	6,720	SF	FORM LINER									6720					(1616/1822)
601	37500	380	380	FT	PAVED GUTTER, TYPE 1-2			79	96	60	42		103						
607	39920	559	559	FT	VANDAL PROTECTION FENCE, 10' CURVED, COATED FABRIC					394		165							
SPECIAL	61050010	309	309	SF	RETAINING WALL, MISC.: MODULAR CONCRETE BLOCK WALL												309		(845/1822)
SPECIAL	61060000	LS	LS		RETAINING WALL, MISC.: TEMPORARY WIRE FACED MSE WALL			LS	LS		LS	LS							(1617/1822) & (1618/1822)
840	20000	80,889	80,889	SF	MECHANICALLY STABILIZED EARTH WALL	6488		4336	4977	9997	13849	34024	4900						
840	21000	27,900	27,900	CY	WALL EXCAVATION	1625		2926	5448	2175	4627	8915	1558						
840	22000	8,079	8,079	SY	FOUNDATION PREPARATION	750		798	804	1088		3720	641						
840	22001	1,735	1,735	SY	FOUNDATION PREPARATION, AS PER PLAN						1735								(1649/1822)
840	23000	60,848	60,848	CY	SELECT GRANULAR BACKFILL	2582		3854	4322	4909	10891	27628	5536						
840	25010	8,222	8,222	FT	6" DRAINAGE PIPE, PERFORATED	920		586	578	1150	1340	2821	462						
840	25020	471	471	FT	6" DRAINAGE PIPE, NON-PERFORATED	25		52	45	60	133	77	10						
840	26000	4,166	4,166	FT	CONCRETE COPING	450		295	300	593	653	1448	246						
840	26050	80,889	80,889	SF	AESTHETIC SURFACE TREATMENT	6488		4336	4977	9997	13849	34024	4900						
840	27000	13	13	DAY	ON-SITE ASSISTANCE	1		2	2	1	2	3	1						
840	28000	LS	LS		SGB INSPECTION AND COMPACTION TESTING	LS		LS	LS	LS	LS	LS	LS						

DEB	4/21/16	FORM LINER	QUANTITY	REVISION
REV. BY	DATE	DESCRIPTION		
DATE COMPLETED				