

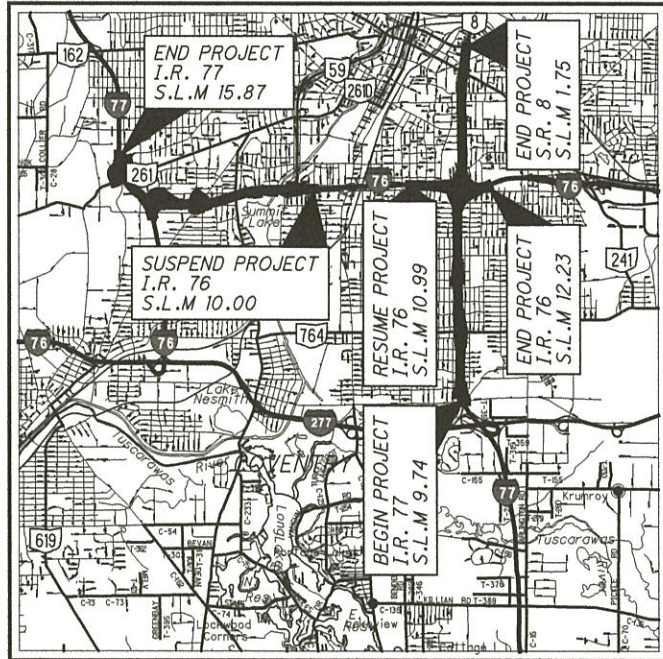
Released for Construction
 Thomas J Powell, PE
 05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

STATE OF OHIO
 DEPARTMENT OF TRANSPORTATION

SUM-76-8.24
SUM-77-9.74
SUM-8-0.00

CITY OF AKRON
 SUMMIT COUNTY



LOCATION MAP

LATITUDE: 41° 03' 43" LONGITUDE: 81° 30' 17"



PORTION TO BE IMPROVED	-----
INTERSTATE HIGHWAY	-----
FEDERAL ROUTES	-----
STATE ROUTES	-----
COUNTY & TOWNSHIP ROADS	-----
OTHER ROADS	-----

DESIGN DESIGNATION

ROADWAY PLANS: SEE BU-34

NHS PROJECT ----- YES

DESIGN EXCEPTIONS

N/A

INDEX OF SHEETS:

TITLE SHEET	1
MOT NOTES	2 - 8
DETOUR PLANS	9 - 15
MAINTENANCE OF TRAFFIC	16 - 43

PROJECT DESCRIPTION

THE AKRON CENTRAL INTERCHANGE PROJECT (PID 102329) INCLUDES RECONSTRUCTION OF IR-76 EB/WB PAVEMENT FROM SLM 11.05 (IR-76) TO SLM (IR-76) FOR APPROX. 1.16 MILES OF MAINLINE WORK. THIS PROJECT ALSO INCLUDES REPLACEMENT OF TWO FREEWAY STRUCTURES (RAMP N AND RAMP O) ON NEW ALIGNMENTS, WIDENING OF TWO EXISTING BRIDGES (IR-76 EB OVER BROWN STREET AND IR-76 WB OVER INMAN STREET), CONSTRUCTION OF A NEW PEDESTRIAN/MULTI-USE OVERPASS SPANNING SR-8, AND CONSTRUCTION OF NOISE BARRIERS AT THE PERIMETER OF THE INTERCHANGE. PORTIONS OF SR-8, LANE O AND LANE S ARE TO BE RESURFACED. THE EXISTING IR-76 WB TO INMAN STREET AND IR-77 SB TO LOVERS LANE WILL BE REMOVED PERMANENTLY, AS WELL AS THE EXISTING LAFOLLETTE STREET BRIDGE OVER SR-8 (SUM-77-1184).

BUILDABLE UNIT 3A DESCRIPTION

THIS BUILDABLE UNIT SHOWS CONSTRUCTION OF THE OUTSIDE PORTION OF IR 76/77 FROM SOUTH OF VERNON ODOM BOULEVARD TO SOUTH OF EAST AVENUE.

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

UNDERGROUND UTILITIES

Contact Two Working Days
 Before You Dig



OHIO811, 8-1-1, or 1-800-362-2764
 (Non-members must be called directly)

ENGINEERS SEAL:



SIGNED: [Signature]
 DATE: 5/20/21

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
MT-95.40	1/17/20	MT-101.80	1/17/20	800-2020	7/17/20
MT-95.50	7/21/17	MT-101.90	7/17/20	821	4/20/12
MT-95.70	1/17/20	MT-102.10	1/17/20	829	1/20/17
MT-95.72	1/17/20	MT-103.10	1/19/18	831	10/21/16
MT-95.73	1/17/20	MT-105.10	1/17/20	839	1/17/20
MT-98.10	1/17/20				
MT-98.11	1/17/20	DM-1.3	7/18/14		
MT-98.21	1/17/20				
MT-98.29	1/17/20				
MT-99.30	1/17/20				
MT-99.50	1/17/20				
MT-99.60	7/15/16				
MT-100.00	1/15/16				
MT-101.60	1/17/20				
MT-101.70	1/17/20				
MT-101.75	1/17/20				



BU-3A
MOT
PHASE 2A
I-77/76
VERNON ODOM BLVD TO
WEST OF EAST AVE
RFC PLANS
May 20, 2021

FEDERAL PROJECT NO. **E180(428)**
 CONSTRUCTION PROJECT NO. **21-3000**
 RAILROAD INVOLVEMENT **NONE**
 PID NO. **102329**
 SUM-76/77/8-8.24/9.74/0.00
 1/43

USER: SDATES: 20210520 10:09:05 AM: apilipchuk
 PROJECT: 20210520 10:09:05 AM: apilipchuk
 SHEET: 5/20/2021 10:09:05 AM: apilipchuk
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ITEM 614, MAINTAINING TRAFFIC

A MINIMUM OF TWO LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 502 STRUCTURE FOR MAINTAINING TRAFFIC, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, ITEM 615 ROADS FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614.

A MINIMUM OF TWO LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$3,000 PER HOUR FOR EACH HOUR THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING
EASTER	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN FOR TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED

ITEM 614, MAINTAINING TRAFFIC (CONT'D)

SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 - MAINTENANCE OF TRAFFIC (RESTRICTIONS)

ALL EXISTING LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT AS PERMITTED BY THE PERMITTED LANE CLOSURE TIMES NOTE AND THE MOTEC/PIAC APPROVED CLOSURES NOTE, BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, AND ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC CLASS A.

THE FOLLOWING CRITERIA SHALL BE USED IN DEVELOPMENT OF MAINTENANCE OF TRAFFIC PLANS FOR ALL ROADWAYS WITHIN THE PROJECT LIMITS:

- MINIMUM LANE WIDTH: 11 FEET*
- MINIMUM BARRIER/CURB OFFSET: 2 FOOT*
- MINIMUM EDGE OF PAVEMENT OFFSET: 2 FEET

*THE MINIMUM BARRIER/CURB OFFSET MAY BE REDUCED TO 1 FOOT AND LANE WIDTH REDUCED TO 10 FEET WHEN NECESSARY IN SPOT LOCATIONS. SPOT LOCATIONS INCLUDE ON BRIDGE DECKS, ON APPROACH SLABS, AND BETWEEN BRIDGE PIERS ONLY. STANDARD TAPER RATES SHALL APPLY IN THE SHOULDER TRANSITION FROM 2 FOOT TO 1 FOOT; AND VICEVERSA.

BEFORE WORK BEGINS, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THE NAME AND TELEPHONE NUMBER OF THE WORKSITE TRAFFIC SUPERVISOR(S) WHO CAN BE CONTACTED 24 HOURS A DAY BY THE OHIO DEPT. OF TRANSPORTATION AND ALL INTERESTED POLICE AGENCIES.

NOTIFICATION OF TRAFFIC RESTRICTIONS

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THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTICE OF OFFICE OF COMMUNICATION TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

DETOUR NOTIFICATION CITY OF AKRON/SUMMIT COUNTY

THE CONTRACTOR SHALL ADVISE THE ODOT DISTRICT OFFICE (330-786-3148) AND COUNTY/CITY EIGHTEEN (18) DAYS IN ADVANCE OF WHEN THE DETOUR ROUTE SHOULD BE IN EFFECT. ALL WORK ZONE DEVICES REQUIRED SHALL BE FURNISHED, ERECTED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. PAYMENT FOR ALL WORK ASSOCIATED WITH THE DETOUR SHALL BE INCLUDED UNDER THE LUMP SUM BID FOR ITEM 614, DETOUR SIGNING.

WORK ZONE EGRESS WARNING SYSTEM

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN AN APPROVED WORK ZONE EGRESS WARNING SYSTEM (WZEWS) AS PER SUPPLEMENTAL SPECIFICATION 829.

THE PROBABLE INITIAL LOCATIONS OF THE WZEWS DEVICES ARE SHOWN IN THE PLAN. IT IS EXPECTED THAT THESE LOCATIONS WILL VARY BASED ON PLANNED OR UNPLANNED PHASE AND TRAFFIC PATTERN CHANGES. PLACEMENT, OPERATION, AND MAINTENANCE AND ALL ACTIVATION OF THE DEVICES BY THE CONTRACTOR SHALL BE DIRECTED BY THE ENGINEER.

ITS MESSAGE BOARDS

THE EXISTING ITS MESSAGE BOARDS IN THE VICINITY OF THE PROJECT WILL BE UTILIZED WHEN AVAILABLE TO PROVIDE SUPPLEMENTAL INFORMATION TO THE TRAVELING PUBLIC. THE CONTRACTOR WILL NOTIFY THE PROJECT ENGINEER ONE [1] WEEK IN ADVANCE OF ANY PHASE CHANGE ON SUM-76/77/8-8.42/9.47/0.00. THE PROJECT ENGINEER WILL COORDINATE WITH THE PROJECT ENGINEER TO GET THE ITS MESSAGE BOARDS ADJUSTED.

ITEM 614 WORK ZONE PAVEMENT MARKINGS, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF 614.11, ALL CLASS 1 EDGE LINES, LANE LINES, CENTER LINES AND DOTTED LINES SHALL BE 6 INCHES WIDE AND CHANNELIZING MARKINGS SHALL BE 12 INCHES WIDE. THE APPLICATION RATES FOR THE 6 INCH LINES SHALL BE 1.5 TIMES THE RATES SPECIFIED FOR 4 INCH LINES IN TABLE 614.11-1.

ITEM 614 WORK ZONE PAVEMENT MARKINGS, SPRAY THERMOPLASTIC, AS PER PLAN

THE CONTRACTOR SHALL PLACE THE WORK ZONE PAVEMENT MARKINGS, SPRAY THERMOPLASTIC, AS PER PLAN PER ODOT SPECIFICATION 614.11 AND ODOT SPECIFICATION 648 WITH THE EXCEPTION ODOT SPECIFICATION 648.05 SHALL BE MODIFIED TO ALLOW PLACEMENT OF THE MATERIAL AT A TEMPERATURE OF NOT LESS THAN 35 DEGREES FAHRENHEIT.

DELINEATION OF TEMPORARY AND PERMANENT GUARDRAIL

BARRIER REFLECTORS SHALL BE INSTALLED ON ALL TEMPORARY GUARDRAIL USED FOR TRAFFIC CONTROL AND ON ALL PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. BARRIER REFLECTORS SHALL CONFORM TO C&MS 626.

OBJECT MARKERS SHALL BE INSTALLED ON ALL TEMPORARY AND PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. GUARDRAIL-MOUNTING OF OBJECT MARKERS SHALL BE MADE BY INSTALLING THE OBJECT MARKERS ON THE EXTENSION BLOCKS RATHER THAN DIRECTLY ONTO THE GUARDRAIL ITSELF. OBJECT MARKERS SHALL CONFORM TO C&MS 614.03 AND THE SPACING SHALL BE APPROXIMATELY 50 FEET.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER.

COOPERATION BETWEEN CONTRACTORS

THE CONTRACTOR SHALL BE ADVISED THAT PROJECTS SUM-77/277/US224 VARIOUS (PID 106002), SUM-8-1.75 (PID 91710), D04 ITS MAINT FY 2021 (PID 113833), AND SUM-77-15.87 (PID 88937) MAY BE ONGOING IN AN AREA IMMEDIATELY ADJACENT TO AND WITHIN THE PROJECT LIMITS OF THIS PROJECT. THE CONTRACTOR SHALL SCHEDULE HIS WORK SO AS TO CAUSE A MINIMUM OF DELAY OR CONFLICT WITH THE OTHER PROJECTS. IN ACCORDANCE WITH 105.08, THE CONTRACTOR SHALL ARRANGE WITH THE OTHER CONTRACTORS APPROVAL OF THE ENGINEER. THE CONTRACTOR SHALL RECEIVE DAILY APPROVALS FROM THE ENGINEER PRIOR TO COMMENCING ANY OPERATIONS. ANY CONFLICT BETWEEN CONTRACTORS INVOLVING WORK SCHEDULES, WORK AREA, OR COOPERATION SHALL BE RESOLVED BY THE ENGINEER.

MAINTENANCE OF TRAFFIC NOTES

SUM-76/77/8-8.24/9.74/0.00

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BRIDGE PAINTING EQUIPMENT ON SHOULDERS

IF BRIDGE PAINTING EQUIPMENT IS TO REMAIN ON THE SHOULDERS WHEN THE CONTRACTOR IS NOT WORKING, IT SHALL BE PLACED BEHIND PORTABLE CONCRETE BARRIER (PCB) AND A WORK ZONE IMPACT ATTENUATOR (WZIA) SHALL PROTECT THE LEADING BLUNT END OF THE PCB (SEE OMTCD, FIGURE 6H-5 "SHOULDER CLOSURE ON FREEWAY" (TYPICAL APPLICATION 5)).

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

ITEM 615 - PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF 615

ON THIS PROJECT THE CLASS A PAVEMENT FOR MAINTAINING TRAFFIC SHALL BE 18 FEET WIDE AND THE ROADWAY WIDTH SHALL NOT BE LESS THAN 22 FEET OUT TO OUT OF SHOULDERS. THE EXISTING PAVEMENT/SHOULDER SHALL BE SAWCUT AS PER 203.04(E).

UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL REMOVE THE PAVEMENT FOR MAINTAINING TRAFFIC INCLUDING ANY TEMPORARY DRAINAGE FACILITIES. THE AFFECTED EXISTING EARTH MEDIAN AND PAVED SHOULDERS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AS DIRECTED BY THE ENGINEER AND AS PER 615.08.

PAVEMENT FOR MAINTAINING TRAFFIC SHALL NOT BE OPENED TO TRAFFIC UNTIL ALL WORK ZONE TRAFFIC CONTROL DEVICES, SIGNS, PAVEMENT MARKINGS AND PORTABLE CONCRETE BARRIERS HAVE BEEN ERECTED AND APPROVED BY THE ENGINEER.

ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 1

ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 2

ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 3

ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 4

THIS ITEM SHALL BE UTILIZED FOR THE PAVEMENT REPAIRS NEEDED DURING THIS CONSTRUCTION PROCESS. ALL AREAS TO BE REPAIRED SHALL BE LOCATED BY THE ENGINEER. IT IS LIKELY THAT REPAIRS WILL BE NEEDED PRIOR TO EACH PHASE SWITCH. GREAT CARE SHALL BE TAKEN TO MAINTAIN THE EXISTING PAVEMENT CROSS SLOPE AS WELL AS ALL LONGITUDINAL SLOPES. THE TYPE OF REPAIR SHALL BE DETERMINED BY THE PROJECT ENGINEER. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS REQUIRED FOR MAINTENANCE OF TRAFFIC FOR PAVEMENT REPAIRS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

TYPE 1 - IS TO BE USED WHEN YOU NEED TO MILL & FILL AN AREA OF VARYING LENGTH AND HAVE AN AVERAGE WIDTH OF NOT LESS THAN 2 FEET.

TYPE 2 - IS TO BE USED FOR FIXING THE LONGITUDINAL JOINT ISSUES OF VARYING LENGTH AND HAVE A CONSISTENT WIDTH OF 2 FEET.

TYPE 3 - IS TO BE USED FOR DEEPER REPAIRS (POTHOLE) OF VARYING LENGTH AND WILL HAVE AN AVERAGE WIDTH OF NOT LESS THAN 4 FEET.

TYPE 4 - IS TO BE USED FOR COMPOSITE PAVEMENT REPAIRS OF VARYING LENGTH AND WILL HAVE AN AVERAGE WIDTH OF NOT LESS THAN 3 FEET.

WET REFLECTIVE (WR) WORK ZONE PAVEMENT MARKINGS

ALL CLASS I WORK ZONE PAVEMENT MARKINGS ON INTERSTATES AND MULTILANE HIGHWAYS EXPECTED TO BE IN PLACE LONGER THAN 14 DAYS SHOULD BE SPECIFIED AS WET REFLECTIVE BY REFERENCING EITHER WORK ZONE MARKING MATERIAL TYPE 807 (WET REFLECTIVE LIQUID APPLIED BINDER) OR 873 (WET REFLECTIVE REMOVABLE TAPE).

TO AVOID SCARRING ON THE FINAL SURFACE COURSE, WET REFLECTIVE REMOVABLE TAPE SHOULD BE SELECTED WHEN REMOVAL OF THE TEMPORARY MARKING FROM THE FINAL SURFACE COURSE IS NECESSARY PRIOR TO INSTALLATION OF THE PERMANENT MARKING.

IF THE WORK ZONE MARKING ON THE FINAL SURFACE COURSE IS IN THE LOCATION OF THE FINAL PERMANENT PAVEMENT MARKING AND THE FINAL PERMANENT PAVEMENT MARKING WILL BE A RECESSED MARKING, THE WORK ZONE MARKING DOES NOT NEED TO BE REMOVED PRIOR TO INSTALLATION OF THE PERMANENT MARKING.

ITEM 614 MAINTAINING TRAFFIC MISC.: BRIDGE DECK PAVEMENT PATCHING

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05/24/2021

THIS WORK WILL BE AS DIRECTED BY THE ENGINEER AND WILL INCLUDE ALL ASSOCIATED MOT COSTS WITH THE ACTIVITY. THE FIXED AMOUNT SHOWN IN THE PROPOSAL IS INCLUDED (AS ANY OTHER BID ITEMS) IN THE TOTAL BID AMOUNT. THIS FIXED AMOUNT IS THE DEPARTMENT'S ESTIMATE OF THE TOTAL COST OF BRIDGE DECK AND PAVEMENT PATCHING WORK REQUIRED TO BE PERFORMED WITHIN THE WORK LIMITS AS DIRECTED BY THE ENGINEER. C&MS TABLE 104.02-2 DOES NOT APPLY TO REDUCTIONS IN THIS CONTRACT ITEM. FORCE ACCOUNT RECORDS SHALL BE KEPT TO TRACK AND ULTIMATELY DETERMINE THE AMOUNT OF THE PAY ITEM USED. THE WORK ITEM SHALL INCLUDE ALL WORK, AS DIRECTED BY THE ENGINEER, NEEDED TO RE-ESTABLISH A REASONABLY SAFE AND PASSABLE CONDITION OF THE DECK AND/OR PAVEMENT FOR THE DURATION OF THE REQUIRED UPCOMING MOT PHASES. THE CONTRACTOR SHALL MEET WITH THE ENGINEER TO ESTABLISH THE WORK AFTER EXECUTION OF THE CONTRACT. THE CONTRACTOR'S PROPOSED PHASING AND PHASING DURATIONS WILL ASSIST THE ENGINEER IN DETERMINING THE EXTENT OF THE WORK. THIS WORK IS ONLY INTENDED TO ESTABLISH A SAFE AND DRIVABLE CONDITION FOR THE DURATION OF THE PROJECT. THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITIES OF 614.02B.

ITEM 622 PORTABLE BARRIER PLACEMENT

DURING THE PLACEMENT OF THE PORTABLE BARRIER, TRAFFIC WILL BE PROHIBITED FROM OCCUPYING THE TRAVEL LANE ADJACENT TO THE BARRIER. THE BARRIER WILL BE PLACED AT NIGHT PER THE WORK HOUR RESTRICTION NOTE AND IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE MAP. THE CLOSURE OF THE ADJACENT LANE WILL BE PER THE STANDARD DRAWING MT-95.30.

THE CONTRACTOR WILL SUBMIT A PLAN TO THE ENGINEER FOR APPROVAL SEVEN (7) DAYS IN ADVANCE OF THE PLANNED LANE CLOSURE. WORK WILL NOT BEGIN UNTIL APPROVAL OF THE PLANS HAS BEEN GRANTED.

SPECIAL - WORK ZONE GUARDRAIL

THIS WORK AND MATERIALS SHALL COMPLY WITH ITEM 606 FOR PERMANENT GUARDRAIL, EXCEPT THAT USED TYPE 5 RAILS AND POSTS MAY BE USED IF IN GOOD CONDITION AND APPROVED BY THE ENGINEER. FOR EXISTING GUARDRAIL RUNS WHICH REQUIRE AN EXTENSION, THE EXISTING END TERMINAL ASSEMBLY SHALL BE REMOVED AND RESET TO THE NEW LOCATIONS, AS SHOWN ON THE PLANS. NEW RUNS, FOR BRIDGE PARAPET PROTECTION SHALL HAVE THE APPROPRIATE BRIDGE TERMINAL ASSEMBLIES INSTALLED. UPON COMPLETION OF THE PHASE WHICH REQUIRES THE TEMPORARY GUARDRAIL, ALL WORK ZONE GUARDRAIL SHALL BE REMOVED AND THE POST HOLES BACKFILLED (UNLESS PERMANENT GRADING TO BE PERFORMED LATER WOULD REPAIR THE HOLES), ALL TERMINAL ASSEMBLIES REMOVED, AND END TERMINAL ASSEMBLIES RESET TO THEIR ORIGINAL LOCATION.

DRUM REQUIREMENTS

IN ADDITION TO THE REQUIREMENTS OF THE PLANS, SPECIFICATION AND PROPOSAL, DRUMS FURNISHED BY THE CONTRACTOR SHALL BE NEW AND UNUSED AT THE TIME OF ARRIVAL ON THE PROJECT. ANY DRUMS BROUGHT ON THE PROJECT, WHICH HAVE PREVIOUSLY BEEN USED ELSEWHERE, WILL NOT BE ACCEPTED.

ISSUE RECORD:		
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ITEM 622, PORTABLE BARRIER, 50", AS PER PLAN

THIS WORK SHALL CONSIST OF FURNISHING, MAINTAINING, AND SUBSEQUENTLY REMOVING A 50-INCH PORTABLE BARRIER AT THE LOCATIONS SHOWN ON THE PLANS. FOR DETAILS, SEE SCD RM-4.1.

PORTABLE STEEL BARRIER IS AN APPROVED ALTERNATIVE TO PORTABLE CONCRETE BARRIER. FOR INFORMATION ON APPROVED VENDORS, SEE THE APPROVED PRODUCTS LIST MAINTAINED BY ROADWAY ENGINEERING.

PORTABLE BARRIER, 32 INCHES HIGH WITH AN 18-INCH MINIMUM HEIGHT GLARE SCREEN MAY BE USED AT THE OPTION OF THE CONTRACTOR. THE GLARE SCREEN SHALL BE CONSTRUCTED USING ONE OF THE SCREENS PROVIDED ON THE APPROVED LIST, AVAILABLE ON THE OFFICE OF ROADWAY ENGINEERING WEBSITE.

PADDLE OR INTERMITTENT TYPE GLARE SCREENS SHALL BE DESIGNED USING A 20 DEGREE CUT-OFF ANGLE BASED ON TANGENT ALIGNMENT. THAT SPACING SHALL BE USED THROUGHOUT THE BARRIER LENGTH WITHOUT REGARD TO BARRIER CURVATURE.

THE GLARE SCREEN SYSTEM SHALL BE SECURELY FASTENED TO THE 32-INCH PORTABLE BARRIER USING THE HARDWARE AND PROCEDURES SPECIFIED BY THE MANUFACTURER.

FOR DIRECTIONS ON HOW TO INSTALL THE GLARE SCREEN AND THE BARRIER, SEE THE MANUFACTURER'S INSTRUCTIONS.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL AND ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND CONCRETE PERMANENT BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE UNDER EITHER OF THE FOLLOWING CONDITIONS: ALONG TAPERS AND TRANSITION AREAS AND ALONG CURVES (OUTSIDE ONLY) WITH DEGREE OF CURVATURE GREATER THAN OR EQUAL TO 3 DEGREES.

THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.

DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT-101.70.

TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT-101.70.

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SUM-76 / 77 / 8-
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DATE DATE
FILE FILE

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL OR BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR HAZARDS OVER 24" AND LESS THAN 36" WIDE (UNIDIRECTIONAL OR BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS. THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

ITEM 611 - 12", SLOTTED DRAIN, TYPE 2

THIS ITEM SHALL CONSIST OF 12 INCH DIAMETER SLOTTED DRAIN ALUMINUM COATED STEEL CONDUIT 707.01 WITH 6 INCH TRAPEZOIDAL GALVANIZED SOLID BAR GRATE AS APPROVED BY THE ENGINEER.

ITEM 614 - WORK ZONE RAISED PAVEMENT MARKER, AS PER PLAN

WORK ZONE RAISED PAVEMENT MARKERS, AS PER PLAN, AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614 OR C&MS 621 AS SPECIFIED HEREIN.

RAISED PAVEMENT MARKERS IN USE DURING THE SNOW-PLOWING SEASON SHALL CONFORM TO 621, EXCEPT FOR RAISED PAVEMENT MARKERS ON PERMANENT CONCRETE SURFACES. SEE ITEM 614 - WORK ZONE RAISED PAVEMENT MARKERS ON PERMANENT CONCRETE SURFACES NOTE BELOW.

RAISED PAVEMENT MARKERS IN USE DURING THE NON-SNOW-PLOW SEASON SHALL CONFORM TO EITHER 614 OR TO 621.

THE SNOW-PLOWING SEASON SHALL RUN FROM OCTOBER 15 THROUGH APRIL 1.

IF PROJECT DELAYS, NOT THE FAULT OF ODOT, CAUSE THE WORK TO EXTEND INTO THE SNOW-PLOWING SEASON, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING WORK ZONE RAISED PAVEMENT MARKERS (WZRPMS) CONFORMING TO C&MS 614, WITH RAISED PAVEMENT MARKERS CONFORMING TO 621, AS DETERMINED BY THE ENGINEER, AT THE CONTRACTOR'S EXPENSE.

THIS ITEM SHALL INCLUDE PURCHASE, INSTALLATION AND REMOVAL OF ITEM 614 WORK ZONE RAISED PAVEMENT MARKER, AS PER PLAN, INCLUDING FILLING OF ANY DEPRESSIONS CREATED IN THE PAVEMENT AS PER C&MS 621.08.

RESURFACING OF THE TRANSITION AREAS SHALL BE PERFORMED AT THE TIME THAT THE SURFACE COURSE IS BEING APPLIED TO THE ENTIRE PROJECT. PRIOR TO APPLICATION OF THE SURFACE COURSE ON THE PROJECT, THE EXISTING PAVEMENT WITHIN THE TRANSITION AREA SHALL BE REMOVED TO A DEPTH NECESSARY TO REACH THE LEVEL OF THE INTERMEDIATE COURSE OF THE PAVEMENT, AS DETERMINED BY THE ENGINEER.

ITEM 614 - WORK ZONE RAISED PAVEMENT MARKERS ON PERMANENT CONCRETE SURFACES

RAISED PAVEMENT MARKERS IN WORK ZONES, INSTALLED ON PERMANENT CONCRETE SURFACES, SHALL BE ITEM 614 WORK ZONE RAISED PAVEMENT MARKERS. WZRPMS ARE INTENDED FOR USE ONLY DURING THE NON-SNOW-PLOWING SEASON. WZRPMS SHALL NOT BE PROVIDED DURING THE SNOW-PLOWING SEASON.

THE SNOW-PLOWING SEASON SHALL RUN FROM OCTOBER 15 THROUGH APRIL 1.

WHERE A TEMPORARY ALIGNMENT WILL REMAIN IN USE THROUGH THE WINTER, THE WZRPMS SHALL BE REMOVED PRIOR TO THE BEGINNING OF THE SNOW-PLOWING SEASON AND REPLACED APPROXIMATELY APRIL 1, OR AS OTHERWISE DETERMINED BY THE ENGINEER.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

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Powell, PE

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE ODOT INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE ODOT, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE ODOT, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:

- ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
- AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
- AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

- THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
- THE ACTIVE WORK AREA Laterally CLOSEST TO THE OPEN TRAVELED LANE; OR

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT'D)

-OTHER LOCATION AS APPROVED BY THE ENGINEER. THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

CONCRETE MEDIAN BARRIER REPLACEMENT

REMOVING, GRADING AND INSTALLING THE REPLACEMENT BARRIER IN A CONTINUOUS OPERATION SHALL BE LIMITED TO 250 LINEAR FEET AND SHALL AT ALL TIMES BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE ENGINEER SHALL BE SATISFIED THAT ALL INSTALLATIONS WILL AFFORD MAXIMUM PROTECTION FOR TRAFFIC.

EARTHWORK FOR MAINTAINING TRAFFIC

WHEN UNDERCUTS ARE NECESSARY FOR MAINLINE PAVEMENT OR EMBANKMENT CONSTRUCTION, EVALUATE THE NEED FOR TEMPORARY ROAD UNDERCUTS IF WITHIN A CLOSE PROXIMITY TO THE MAINLINE UNDERCUTS. A GEOTECHNICAL EVALUATION SHOULD BE CONSIDERED TO DETERMINE IF THE EXISTING SOIL CONDITIONS ARE ADEQUATE TO SUPPORT THE TEMPORARY ROAD. ADDITIONAL SOIL BORINGS ALONG THE TEMPORARY ROAD ARE NOT NORMALLY REQUIRED.

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SUM-76 / 77 / 8 -
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43

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 05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

Table - Lane / Ramp Closure Matrix

Phase	Roadway/Ramp	Approximate Workzone Limits	Exist # Mainline Thru Lanes	Min. # MOT Mainline Thru Lanes	Required System Ramp Closure	Required Concurrent Local Ramp Closure	Maximum Lane/Ramp Closure Duration(s)	Concurrent 2-lane Operation Not Permissible	Concurrent Ramp Closure Not Permissible	Required Temporary Improvements
1A, 1B	8NB/SB	77 to Perkins St	3	PLCC	N/A	N/A	N/A	N/A	N/A	N/A
2A	77NB/77SB (construct outside improvements)	Vernon Odom Blvd to west of East Ave	6	4	76EB to 77NB 76EB to 76EB (at 77) 77SB to 76WB 277WB to 76EB 76EB to 76EB (at 277) 76WB/77NB to 76WB (permissible)	Kenmore Blvd to 76EB 22 nd St to 76WB (on ramps) 261 to 77SB	150-Day	77NB from Waterloo Rd to 8 77SB from Waterloo Rd to 8	77NB to 76WB 76EB to 77SB	Restripe 77SB to maintain 4 lanes south of Cole Ave to 277 (also applies to inside improvements)
2B	77NB/77SB (construct inside improvements)	Vernon Odom Blvd to west of East Ave	6	0	8SB to 76WB/77NB 77NB to 76WB/77NB	N/A		77NB from Waterloo Rd to 8 77SB from Waterloo Rd to 8	77NB to 76EB 76EB to 77SB	Restripe 76WB Ramp (at 77) to create 2 lane exit (merge to 1 lane on ramp)
5A	8SB to 76EB	Length of Ramps	N/A	N/A	Lane S	N/A	Lane S: 45-Day	N/A	N/A	N/A
5A	Ramp R (76EB to 8 NB)	Ramp	1	1	Yes	N/A	Ramp R: 75-Day	77NB from Waterloo Rd to 8	N/A	N/A
	Ramp N (76WB to 77 SB)	Ramp	1	1	Yes	N/A	Ramp N: 60-Day	76WB west of East Ave to Princeton St	N/A	N/A
4A, 4B, 5A	76EB	west of East Ave to Princeton St	3	2	76EB to 76EB (at 77) 76EB to 76EB (at 277) 76EB to 77SB	East Ave On-ramp (EB) South St On-ramp	640-Day	77NB from Waterloo Rd to 8 76 WB from East Ave to Princeton St	77NB to 76WB 77NB to 76EB	N/A
		Sumner St to Inman St bridge	2	1						
5A, 5B	77SB Ramp M (76EB to 77SB)	Waterloo Rd to 8	3	2	76EB to 77SB 77SB to 277WB 76EB to 76EB (at 77) 76EB to 76EB (at 277)	Wilbeth Rd On-ramp (SB) East Ave On-ramp (EB)	640-Day Wilbeth (210-Day) 77SB-277WB (210-Day*)	77NB from Waterloo Rd to 8	8SB to 76WB	(*) 76WB/77NB: Restripe to maintain 4 lanes from Main St to 76WB Off-ramp; Exit 2 lanes at 76WB Off-ramp (inside merge vicinity of Morse St overpass); Reduce 76EB-77NB ramp to 1 lane
4A, 4B	76EB	Allyn St to Ramp M	4							
3A	76WB	west of East Ave to Princeton St	3	2	77NB to 76WB 8SB to 76WB (inside work area) 59SB to 76WB (outside work area)	East Ave Off-ramp (outside work area)	640-Day 59SB to 76WB: 320-Day	76EB from East Ave to Princeton St	76EB to 77NB 77SB to 277WB	N/A
3A, 3B	76WB	77NB Ramp T (77NB to 76EB) Lane O (77NB to 76WB)	3	2	77NB to 76EB 77NB to 76WB	Waterloo Rd, Wilbeth Rd & Archwood Ave On-ramps (NB)	640-Day 77NB-76EB (210-Day) Service Ramps (210-Day)	77SB from Waterloo Rd to 8	76EB to 77NB 76EB to 76EB (at 77)	N/A
		Brown St to Sumner St	4							
5A, 6B	76WB	west of 8 (approx. STA 530+75) to Inman St	2	1	N/A	N/A	60-Day	77SB from Waterloo Rd to 8	76WB to 77SB (new flyover shall be open before lane reduction is permitted)	N/A
3A, 3B, 4A	76WB	East of Inman St	3	PLCC	N/A	N/A	N/A	N/A	N/A	N/A

MAINTENANCE OF TRAFFIC NOTES

SUM-76 / 77 / 8 -
8.24 / 9.74 / 0.00

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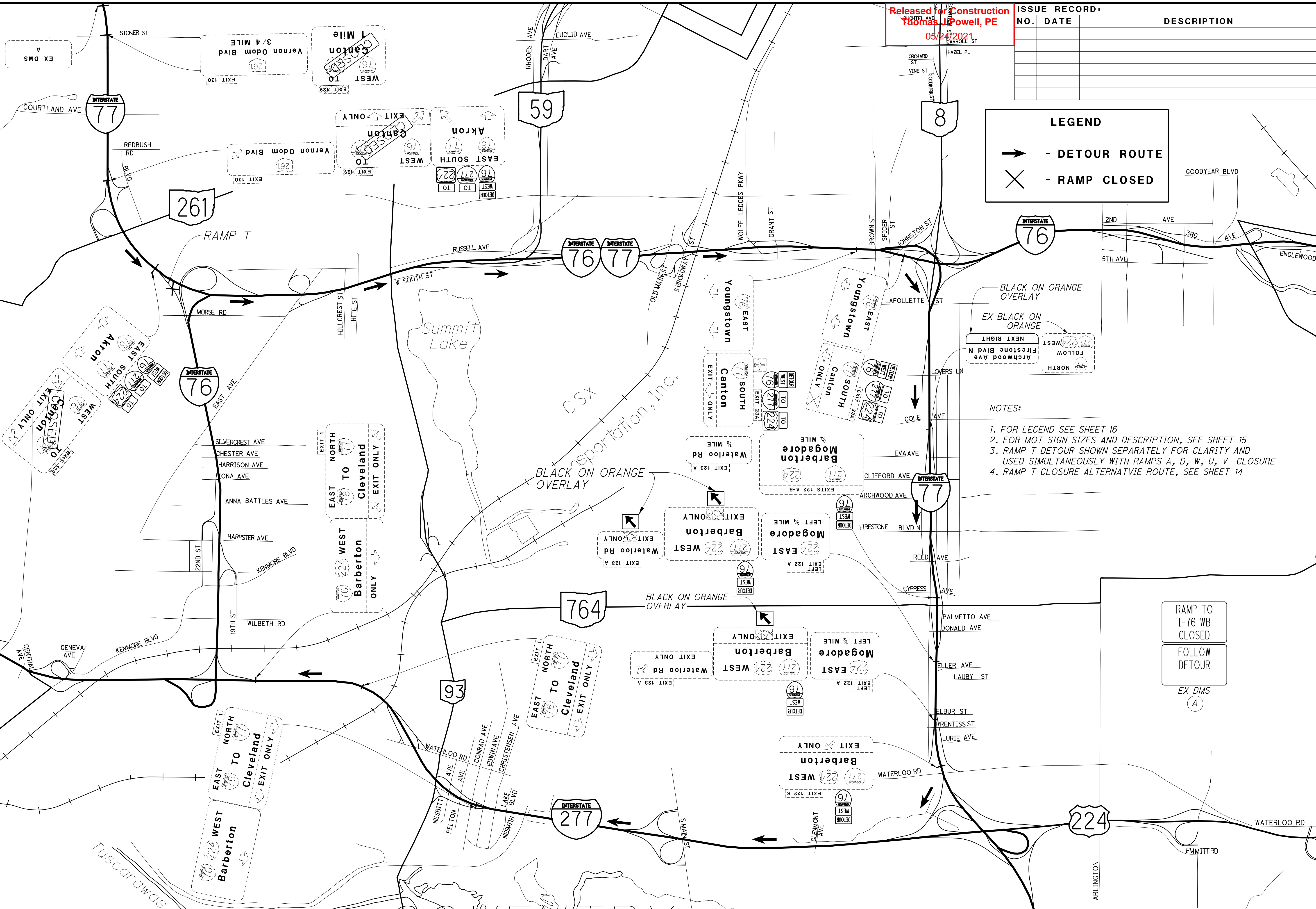
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LEGEND

- DETOUR ROUTE
 - RAMP CLOSED

CALCULATED VJ
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0 500 1000
 HORIZONTAL SCALE IN FEET



- NOTES:**
1. FOR LEGEND SEE SHEET 16
 2. FOR MOT SIGN SIZES AND DESCRIPTION, SEE SHEET 15
 3. RAMP T DETOUR SHOWN SEPARATELY FOR CLARITY AND USED SIMULTANEOUSLY WITH RAMP A, D, W, U, V CLOSURE
 4. RAMP T CLOSURE ALTERNATIVE ROUTE, SEE SHEET 14

RAMP TO I-76 WB CLOSED
 FOLLOW DETOUR
 EX DMS A

RAMP T CLOSURE (PHASE 2A) DETOUR MAP

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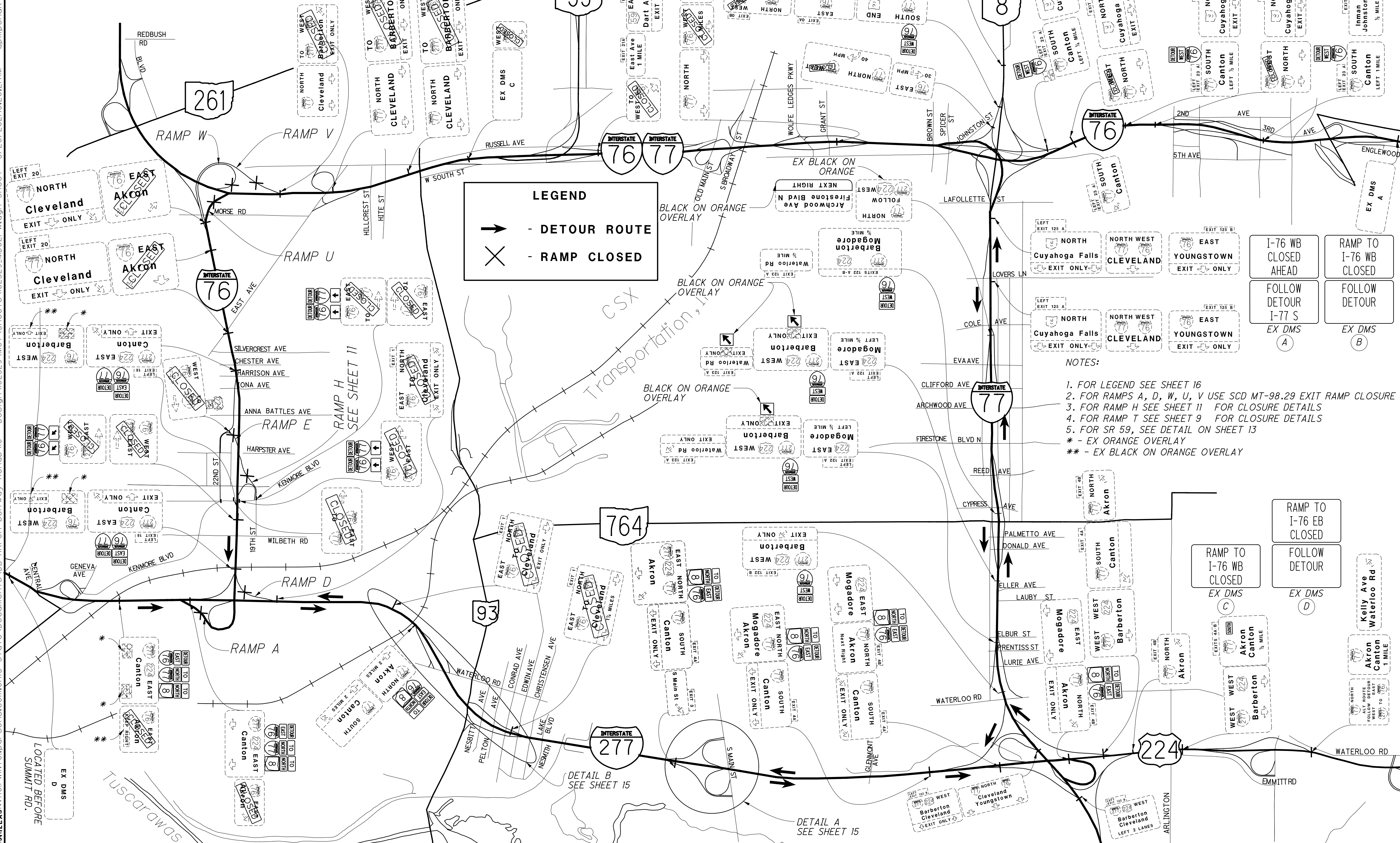
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OVERLAY
BLACK ON
ORANGE

CALCULATED VJ
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 1000
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 SCALE IN FEET



LEGEND

→ - DETOUR ROUTE
 X - RAMP CLOSED

I-76 WB
 CLOSED
 AHEAD
 FOLLOW
 DETOUR
 I-77 S
 EX DMS
 (A)

RAMP TO
 I-76 WB
 CLOSED
 FOLLOW
 DETOUR
 EX DMS
 (B)

- NOTES:**
- FOR LEGEND SEE SHEET 16
 - FOR RAMPS A, D, W, U, V USE SCD MT-98.29 EXIT RAMP CLOSURE
 - FOR RAMP H SEE SHEET 11 FOR CLOSURE DETAILS
 - FOR RAMP T SEE SHEET 9 FOR CLOSURE DETAILS
 - FOR SR 59, SEE DETAIL ON SHEET 13
- * - EX ORANGE OVERLAY
 ** - EX BLACK ON ORANGE OVERLAY

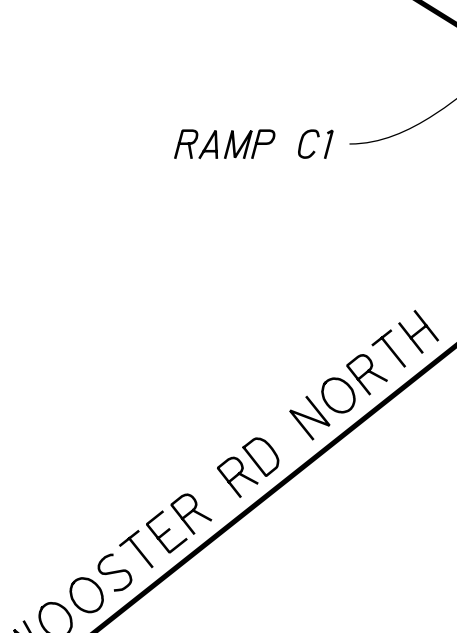
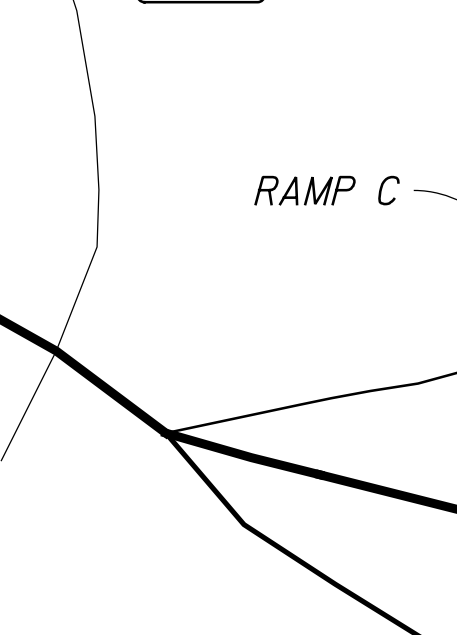
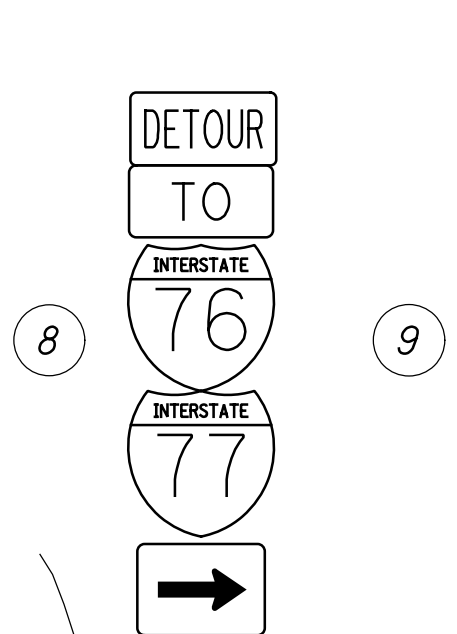
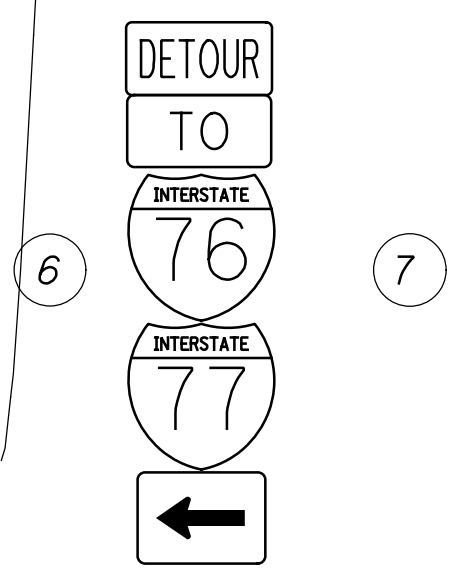
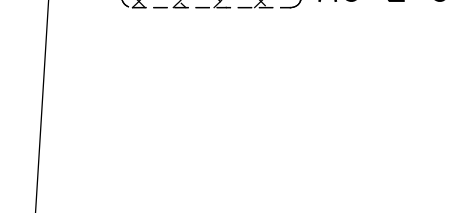
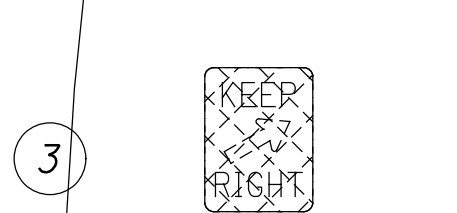
RAMP TO
 I-76 EB
 CLOSED
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RAMP TO
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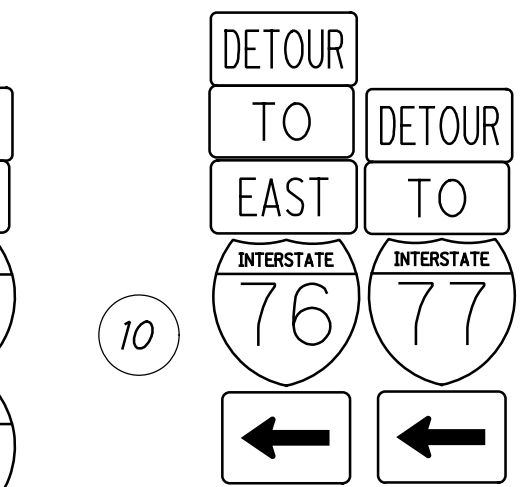
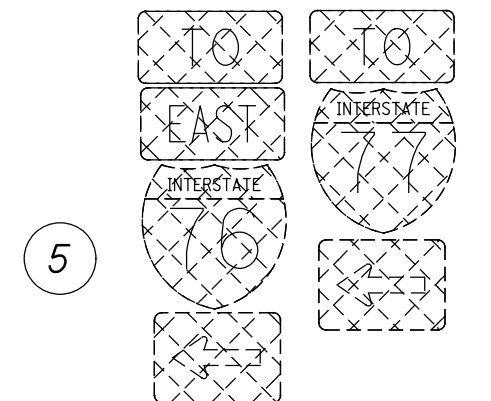
RAMPS A, D, H, U, W, V CLOSURE (PHASE 2A)
DETOUR MAP
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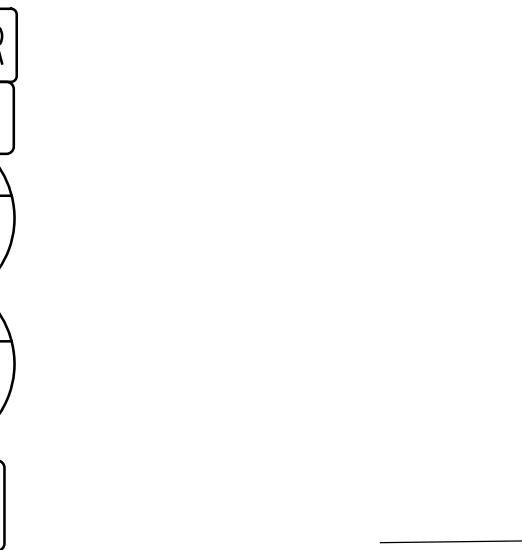
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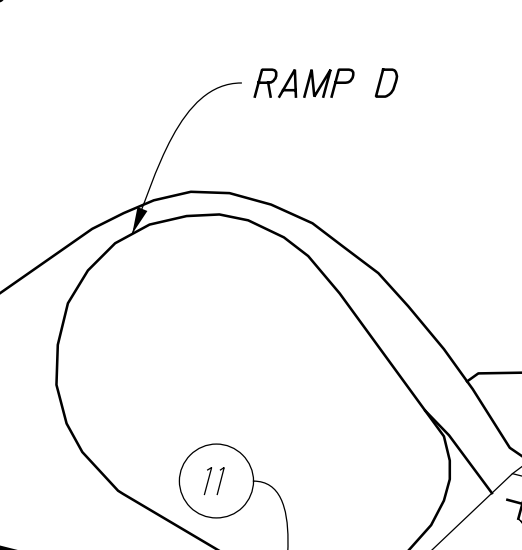
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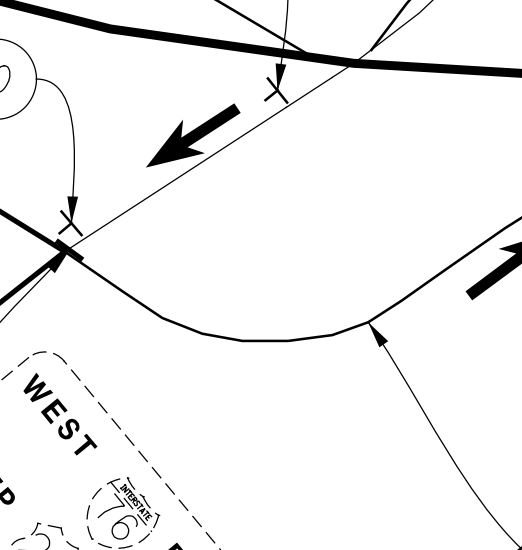
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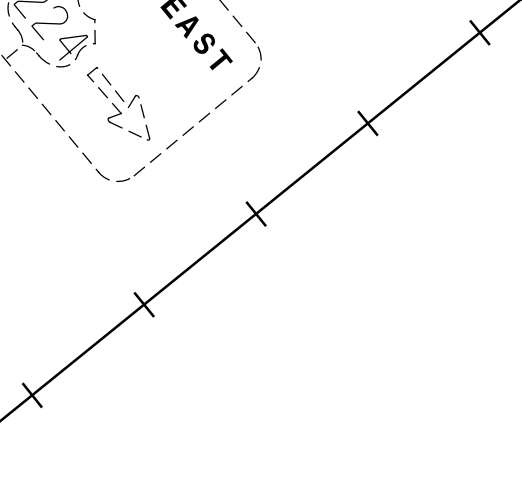
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9



10



LEGEND

→ - DETOUR ROUTE

✕ - RAMP CLOSED

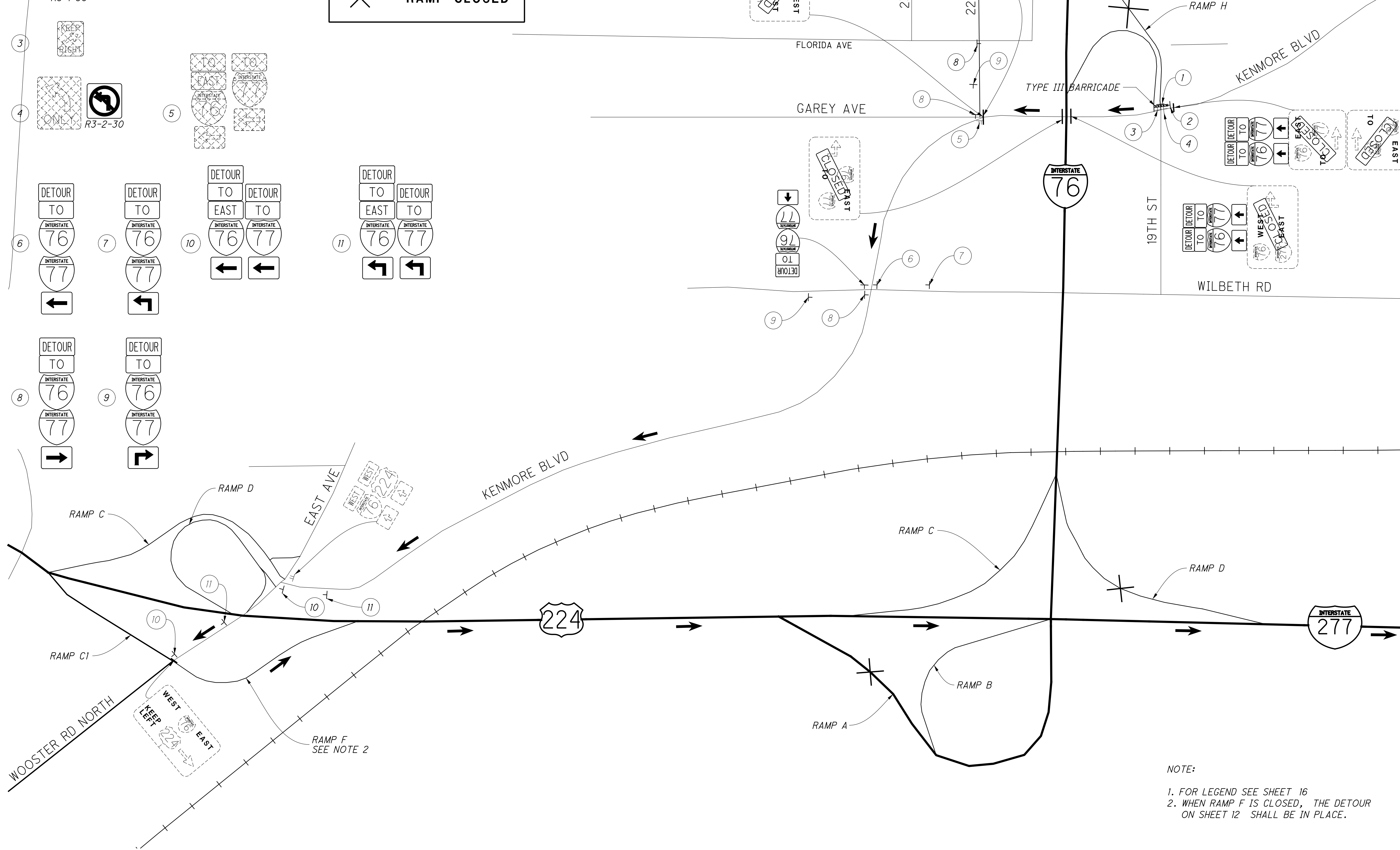
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 HORIZONTAL SCALE IN FEET

CALCULATED VJ
 CHECKED AVP



**RAMP H CLOSURE (PHASE 2A)
 DETOUR MAP**

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

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 43

NOTE:

1. FOR LEGEND SEE SHEET 16

2. WHEN RAMP F IS CLOSED, THE DETOUR ON SHEET 12 SHALL BE IN PLACE.

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NOTES:
 1. FOR LEGEND SEE SHEET 16
 2. FOR RAMPS A, D, W, U, V CLOSURE, SEE SHEET 10

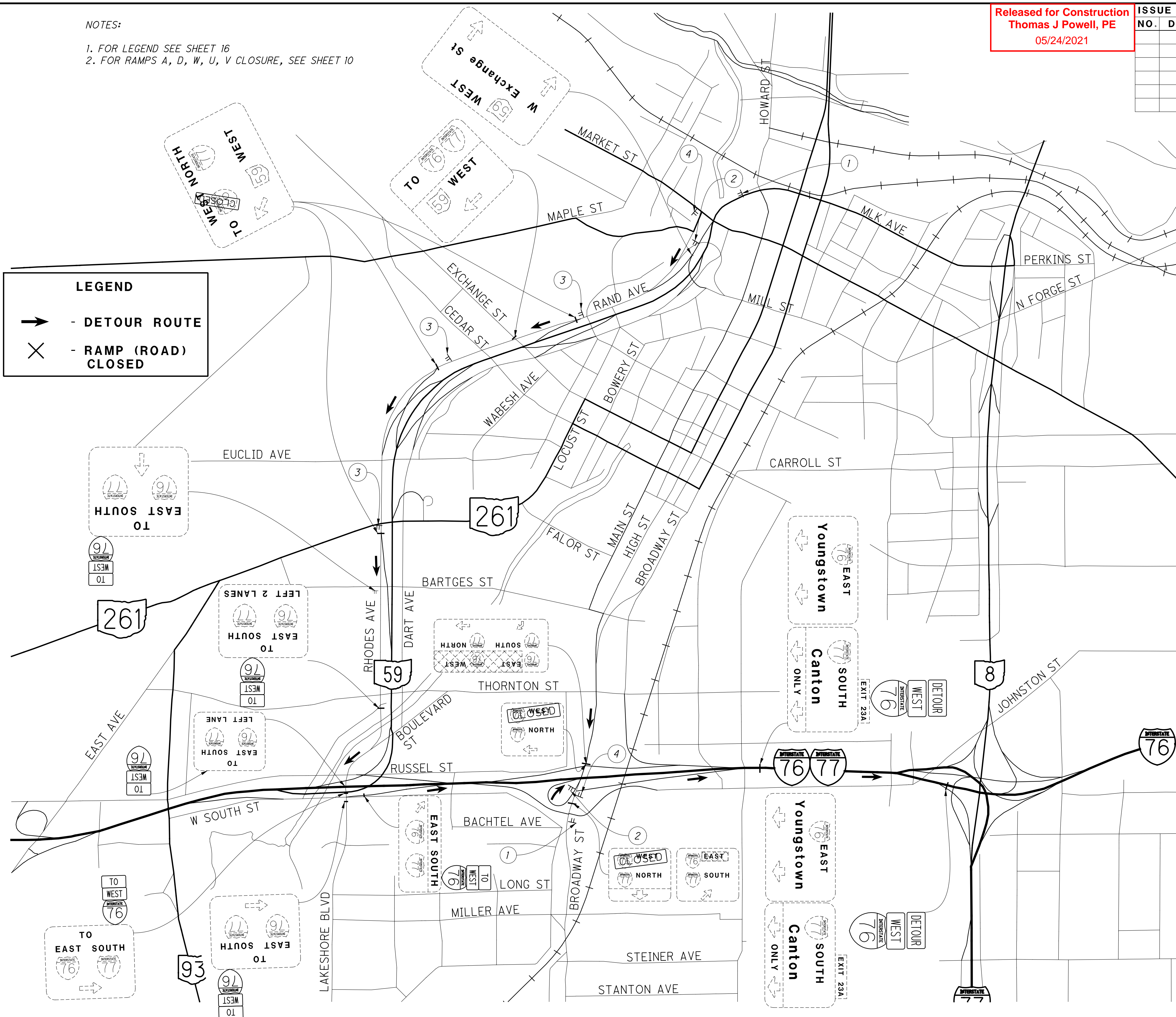
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 05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED VJ CHECKED AVP
 0 750 1500
 375
 HORIZONTAL SCALE IN FEET

LEGEND

- DETOUR ROUTE
- RAMP (ROAD) CLOSED



①		W20-2-36
②		
③		
④*		

* - INSTALL SIGNS USING 100 FT SEPARATION

USER: STIMESTAMPS DATE: 05/24/2021 10:21:10 AM FILE: \\NAVAOP\INTO\parsons.com\Ohio State\Documents\DB-Akron Beltway Rehab\0 - Design\02329\M01\Sheets\02329-MD021-Detail.dgn Sheet 5/20/2021 10:21:10 AM aplipchuk

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NO.	DATE	DESCRIPTION

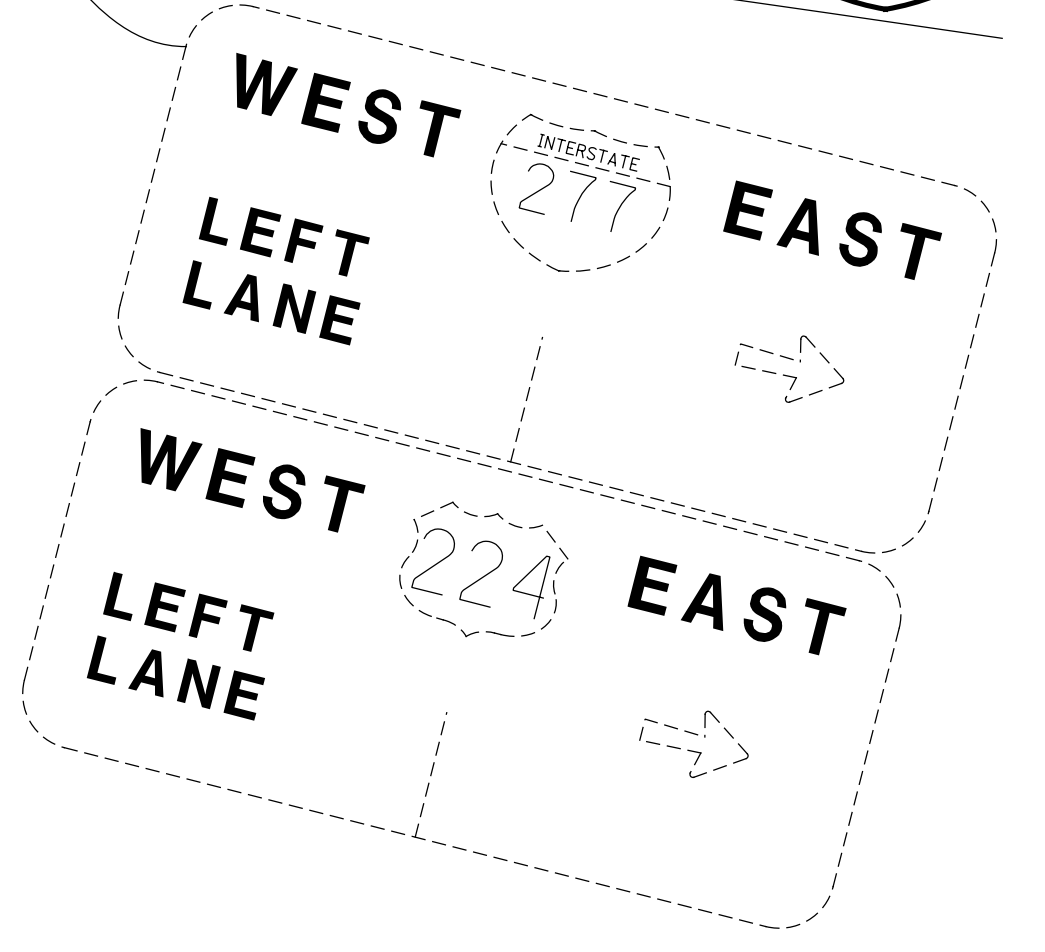
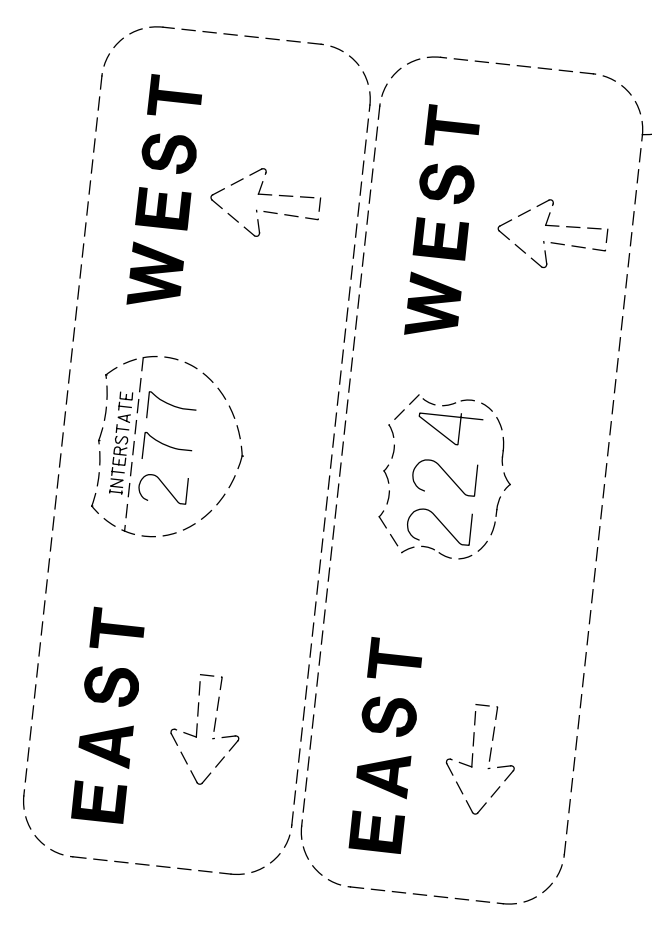
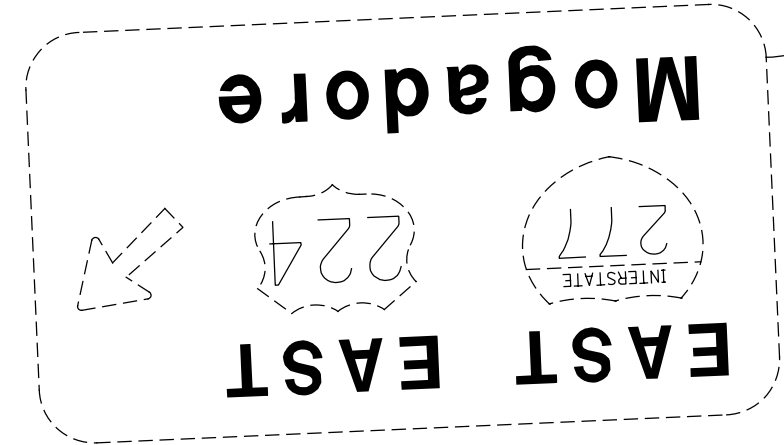
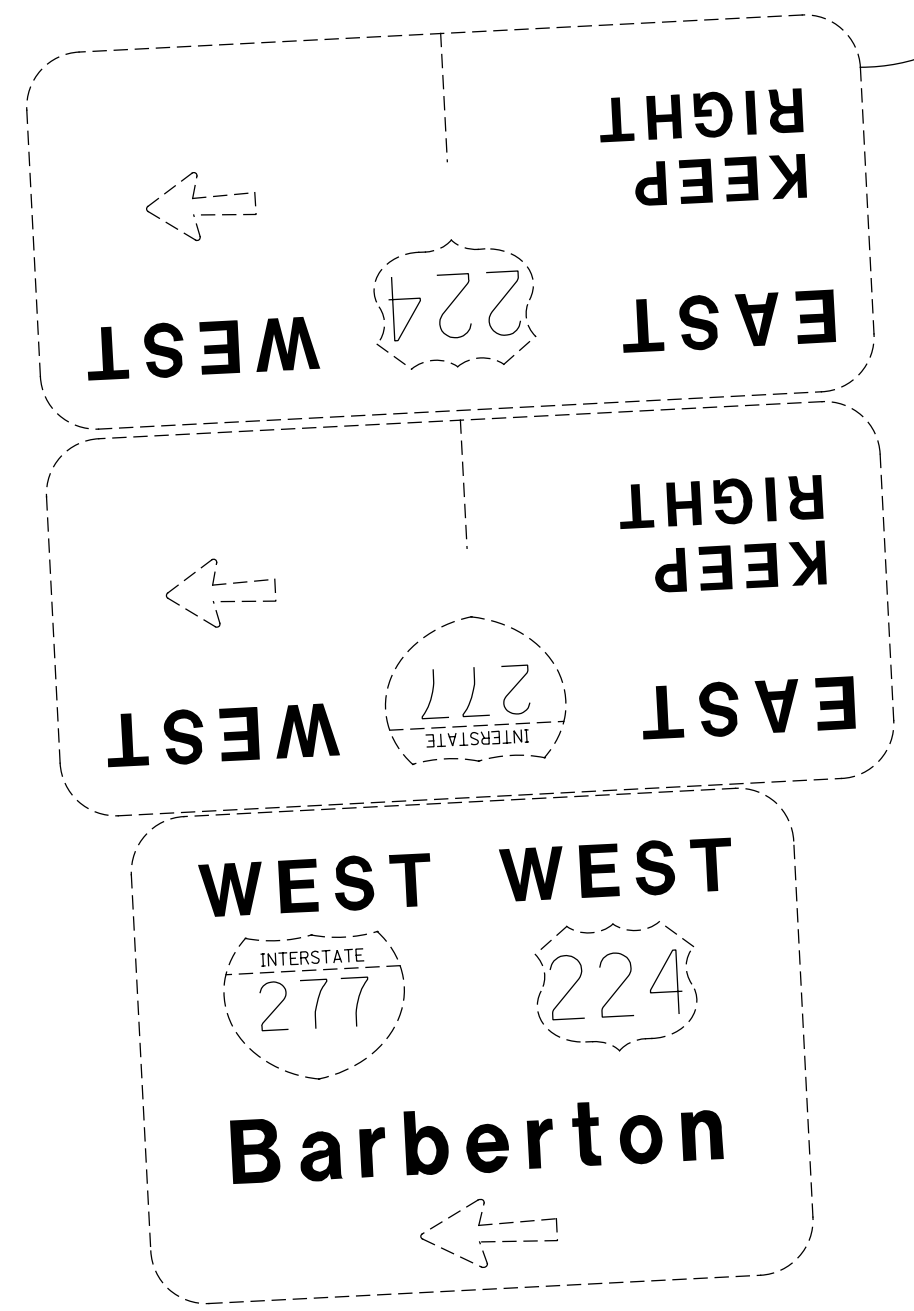
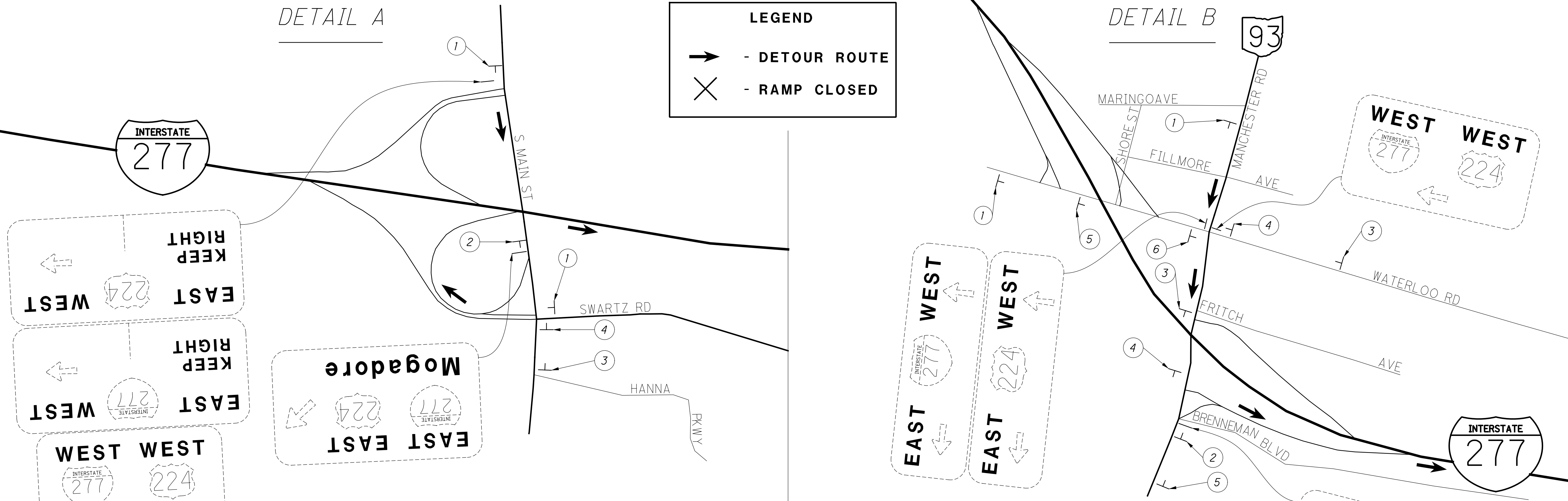
CALCULATED VJ
 CHECKED AVP

DETAIL A

LEGEND

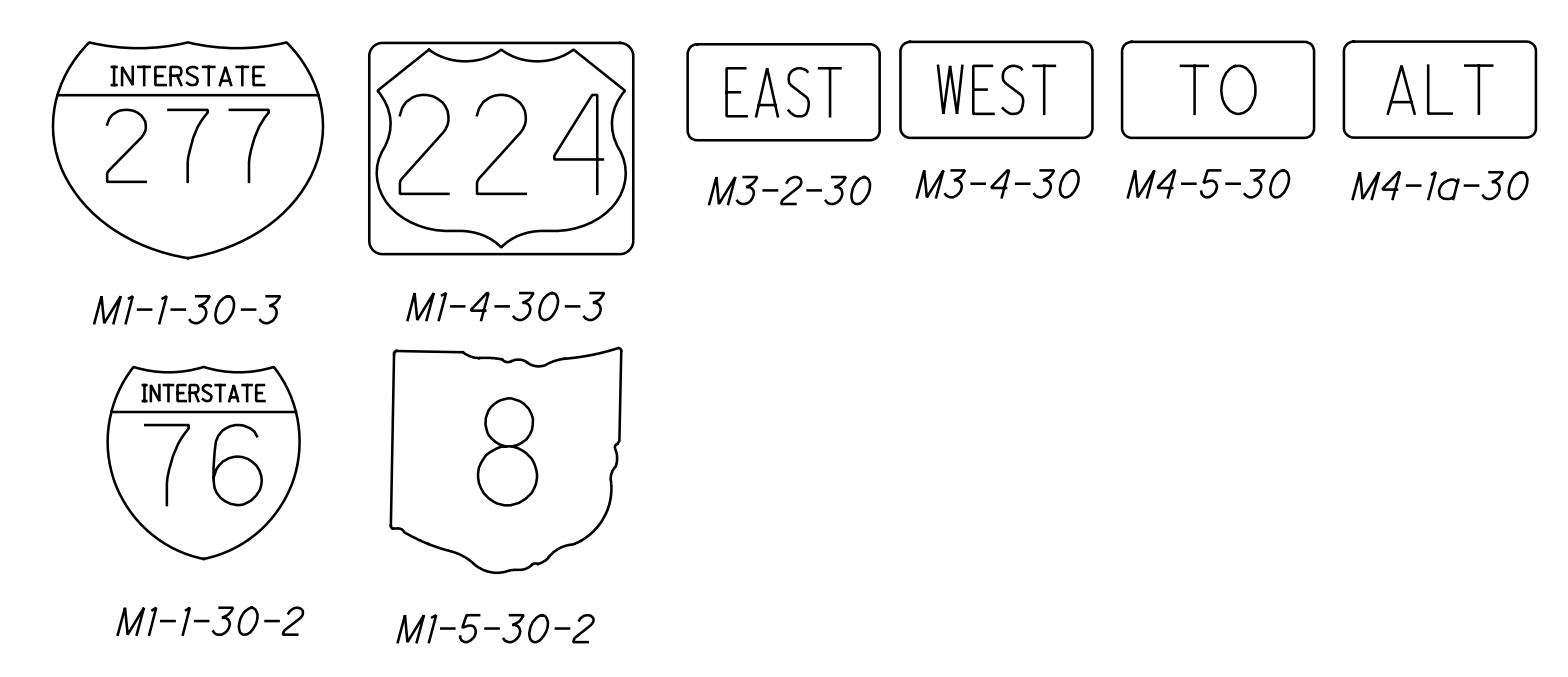
- ➔ - DETOUR ROUTE
- ✕ - RAMP CLOSED

DETAIL B



1	2	3	4	5	6
DETOUR NORTH	DETOUR NORTH	DETOUR NORTH	DETOUR NORTH	DETOUR NORTH	DETOUR NORTH
INTERSTATE 77	INTERSTATE 77	INTERSTATE 77	INTERSTATE 77	INTERSTATE 77	INTERSTATE 77
↑	↗	↖	←	↘	→
M4-8-30 M3-1-30 M1-1-30-2 M6-3-30	M4-8-30 M3-1-30 M1-1-30-2 M6-2-30	M4-8-30 M3-1-30 M1-1-30-2 M5-1-30	M4-8-30 M3-1-30 M1-1-30-2 M6-1-30	M4-8-30 M3-1-30 M1-1-30-2 M5-1-30	M4-8-30 M3-1-30 M1-1-30-2 M6-1-30

DETOUR KEY SIGNS



NOTE:
 ALL ROUTE SHIELDS ON OVERHEAD SIGNS
 SHALL BE 36" x 36" MINIMUM.

DETAIL A & B (PHASE 2A)
 DETOUR MAP

SUM-76/77/8-
 8.24/9.74/0.00

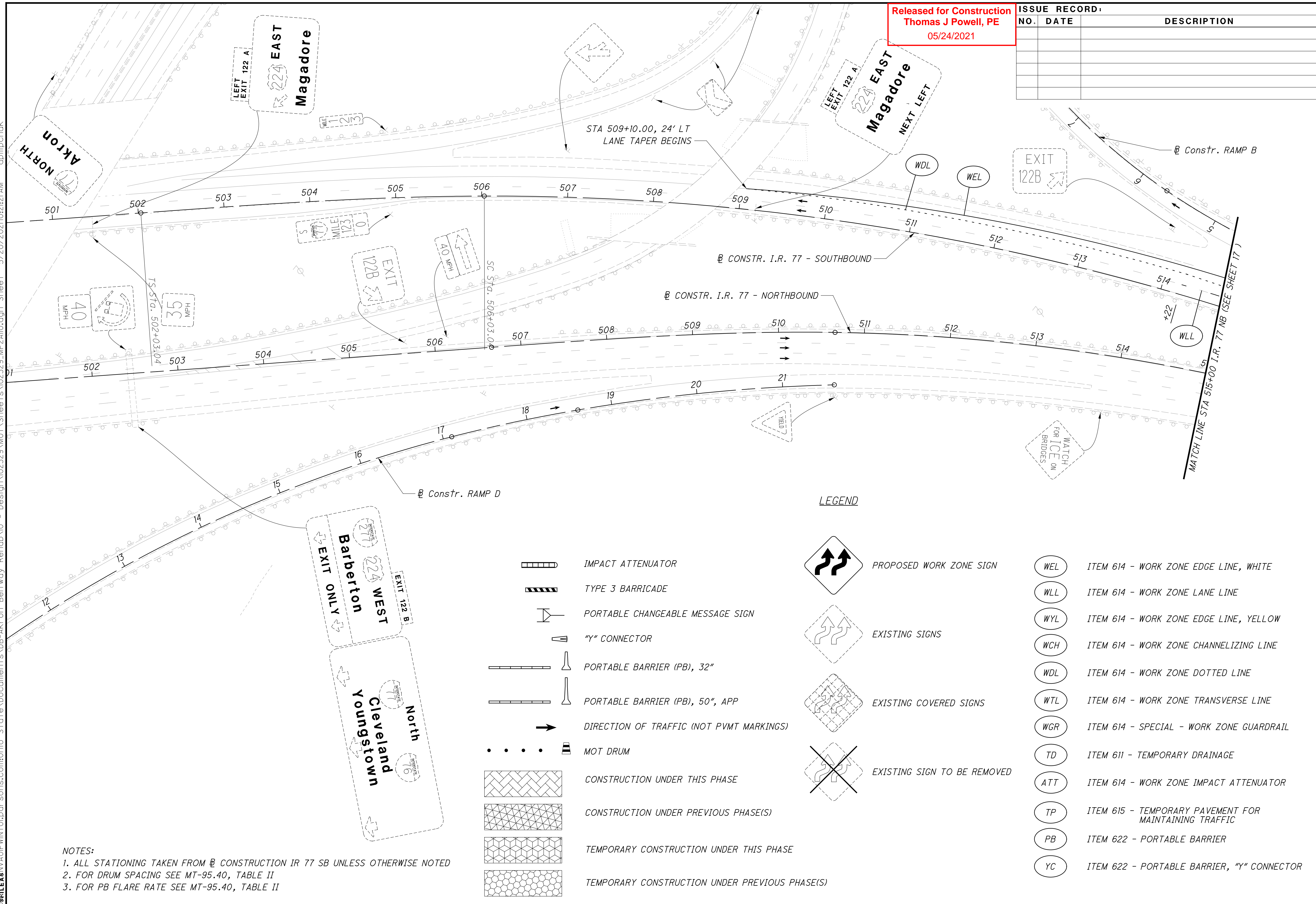
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ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED: FGW
 CHECKED: AVP

0 50 100
 HORIZONTAL SCALE IN FEET



LEGEND

- IMPACT ATTENUATOR
- TYPE 3 BARRICADE
- PORTABLE CHANGEABLE MESSAGE SIGN
- "Y" CONNECTOR
- PORTABLE BARRIER (PB), 32"
- PORTABLE BARRIER (PB), 50", APP
- DIRECTION OF TRAFFIC (NOT PVMT MARKINGS)
- MOT DRUM
- CONSTRUCTION UNDER THIS PHASE
- CONSTRUCTION UNDER PREVIOUS PHASE(S)
- TEMPORARY CONSTRUCTION UNDER THIS PHASE
- TEMPORARY CONSTRUCTION UNDER PREVIOUS PHASE(S)
- PROPOSED WORK ZONE SIGN
- EXISTING SIGNS
- EXISTING COVERED SIGNS
- EXISTING SIGN TO BE REMOVED
- WEL ITEM 614 - WORK ZONE EDGE LINE, WHITE
- WLL ITEM 614 - WORK ZONE LANE LINE
- WYL ITEM 614 - WORK ZONE EDGE LINE, YELLOW
- WCH ITEM 614 - WORK ZONE CHANNELIZING LINE
- WDL ITEM 614 - WORK ZONE DOTTED LINE
- WTL ITEM 614 - WORK ZONE TRANSVERSE LINE
- WGR ITEM 614 - SPECIAL - WORK ZONE GUARDRAIL
- TD ITEM 611 - TEMPORARY DRAINAGE
- ATT ITEM 614 - WORK ZONE IMPACT ATTENUATOR
- TP ITEM 615 - TEMPORARY PAVEMENT FOR MAINTAINING TRAFFIC
- PB ITEM 622 - PORTABLE BARRIER
- YC ITEM 622 - PORTABLE BARRIER, "Y" CONNECTOR

NOTES:
 1. ALL STATIONING TAKEN FROM CONSTR. IR 77 SB UNLESS OTHERWISE NOTED
 2. FOR DRUM SPACING SEE MT-95.40, TABLE II
 3. FOR PB FLARE RATE SEE MT-95.40, TABLE II

MOT IR 77 PHASE 2A
STA 502+00 TO STA 515+00

SUM-76/77/8-
8.24/9.74/0.00

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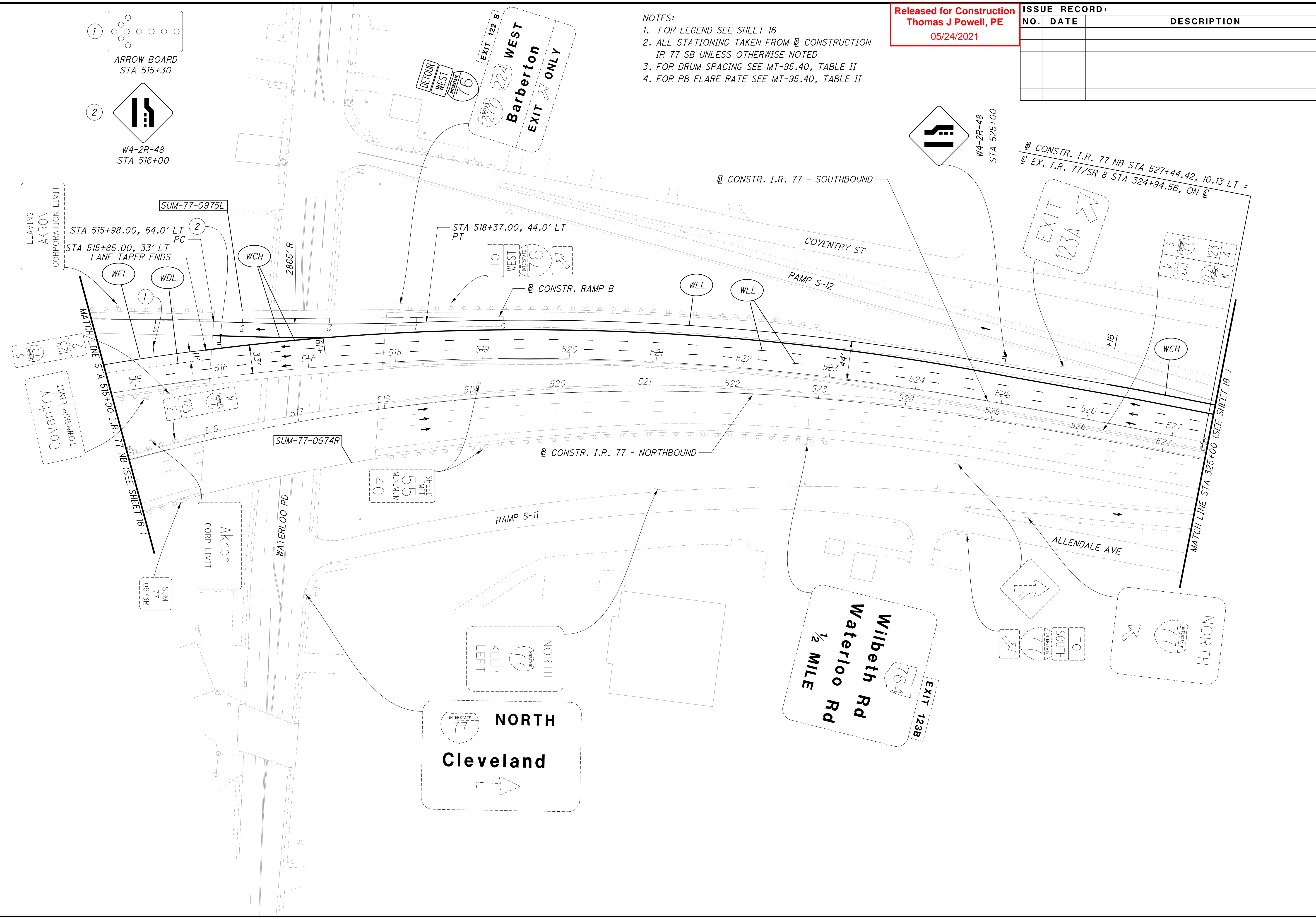
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 05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM @ CONSTRUCTION IR 77 SB UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II



MOT IR 77 PHASE 2A
 STA 515+00 TO STA 325+00

SUM-76/77/8-
 8.24/9.74/0.00

SUSER: STIMESTAMPS: 5/20/2021 10:21:43 AM
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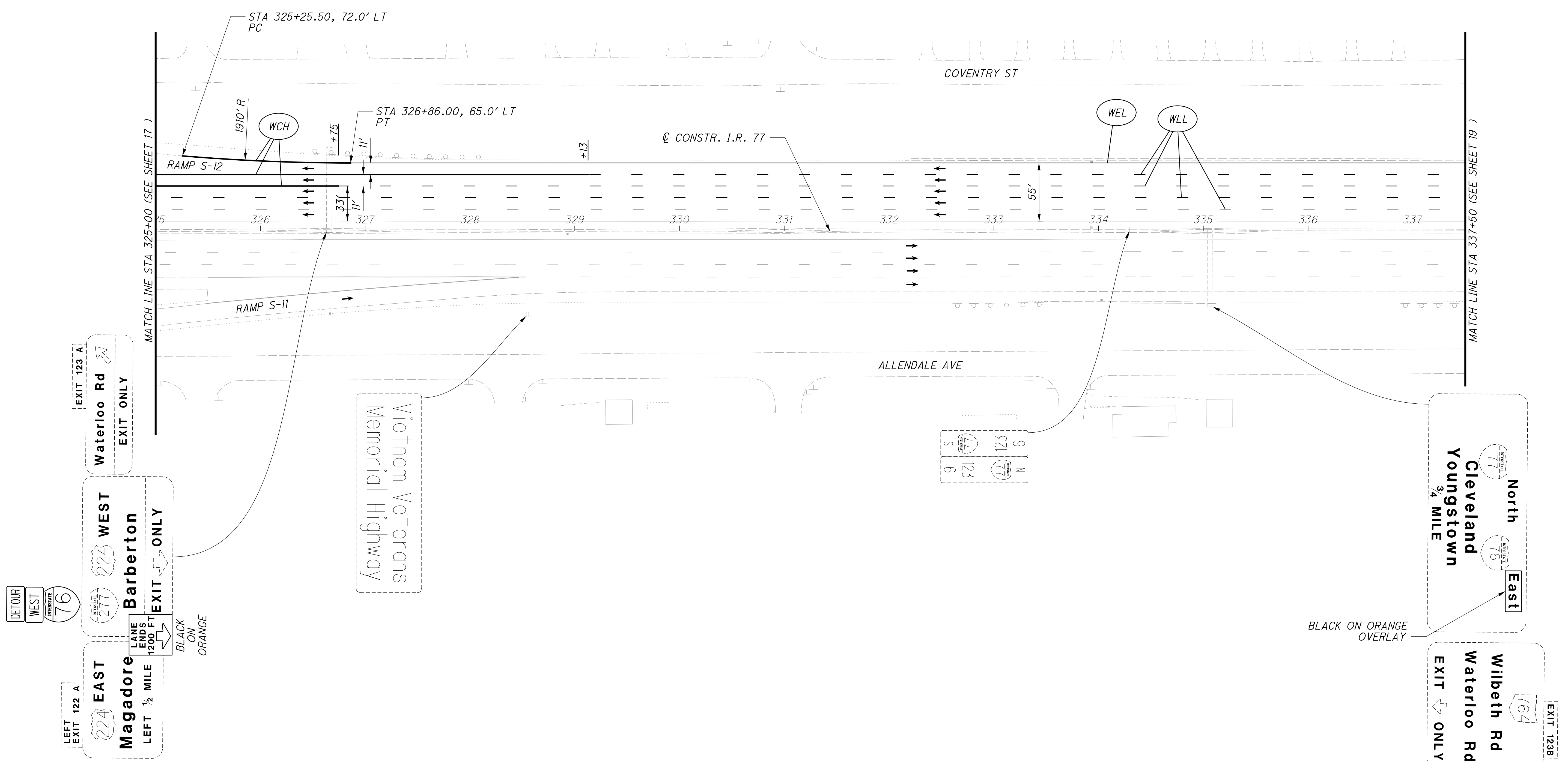
ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED: FGW
 CHECKED: AVP

0 50 100
 HORIZONTAL SCALE IN FEET

18
 43

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM \varnothing CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II



MOT IR 77 PHASE 2A
 STA 325+00 TO STA 337+50

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

18
 43

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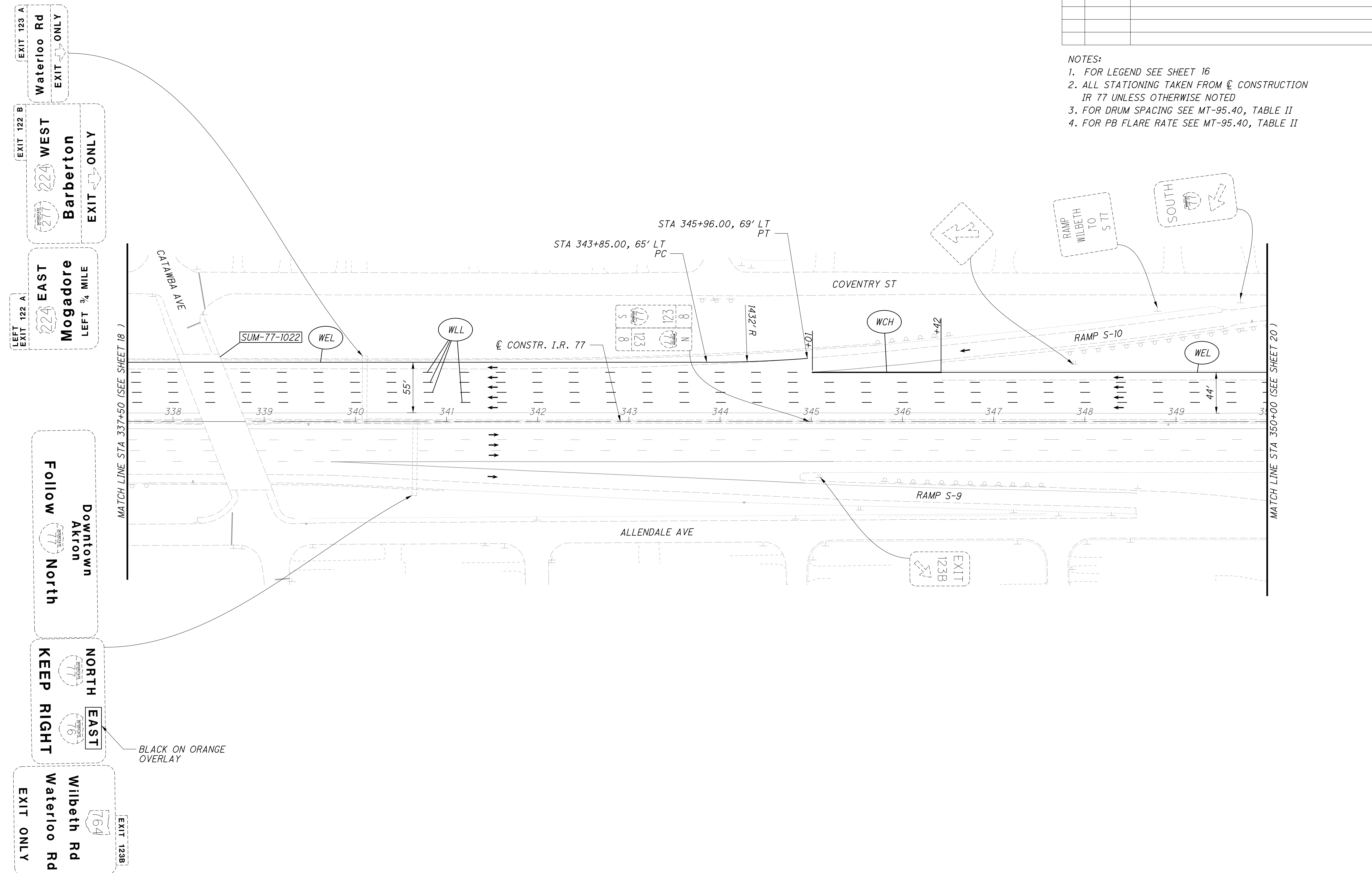
Released for Construction
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ISSUE RECORD:		
NO.	DATE	DESCRIPTION

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM ϕ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



EXIT 123B
 Wilbeth Rd
 Waterloo Rd
 EXIT ONLY

EXIT 123A
 EAST Mogadore
 LEFT 3/4 MILE

EXIT 122 WEST
 Barberton
 EXIT ONLY

EXIT 123 A
 Waterloo Rd
 EXIT ONLY

Follow Downtown Akron North
 NORTH KEEP RIGHT EAST

BLACK ON ORANGE OVERLAY

MOT IR 77 PHASE 2A
 STA 337+50 TO STA 350+00

SUM-76 / 77 / 8 -
 8.24 / 9.74 / 0.00

19
 43

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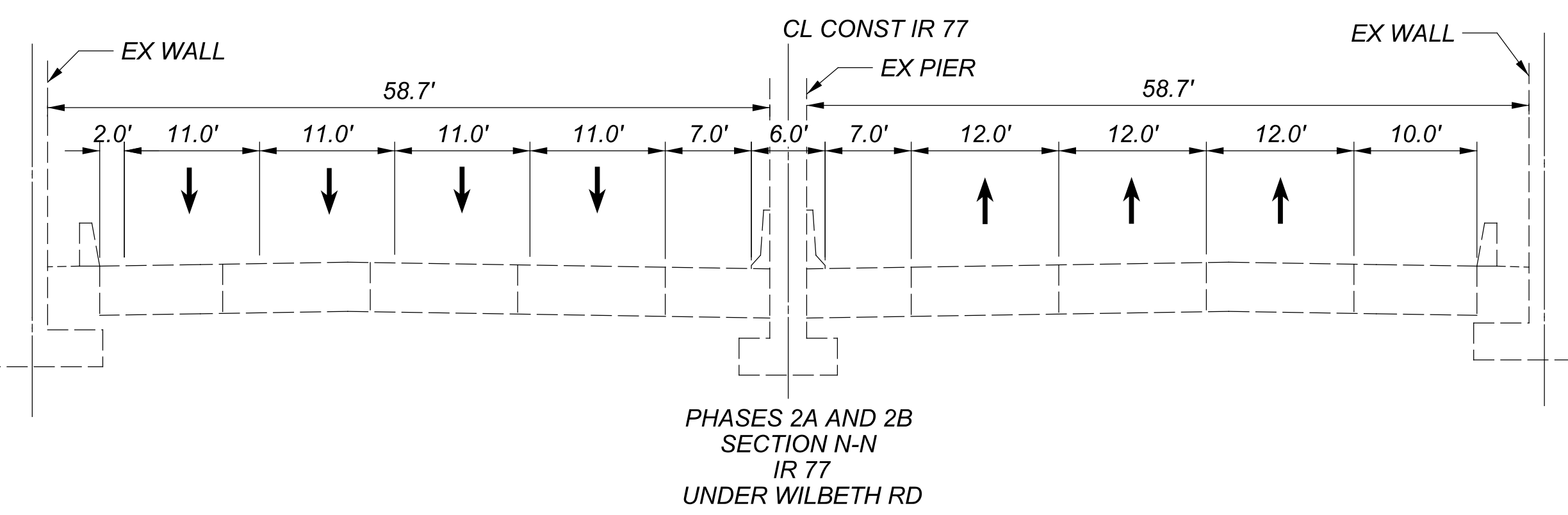
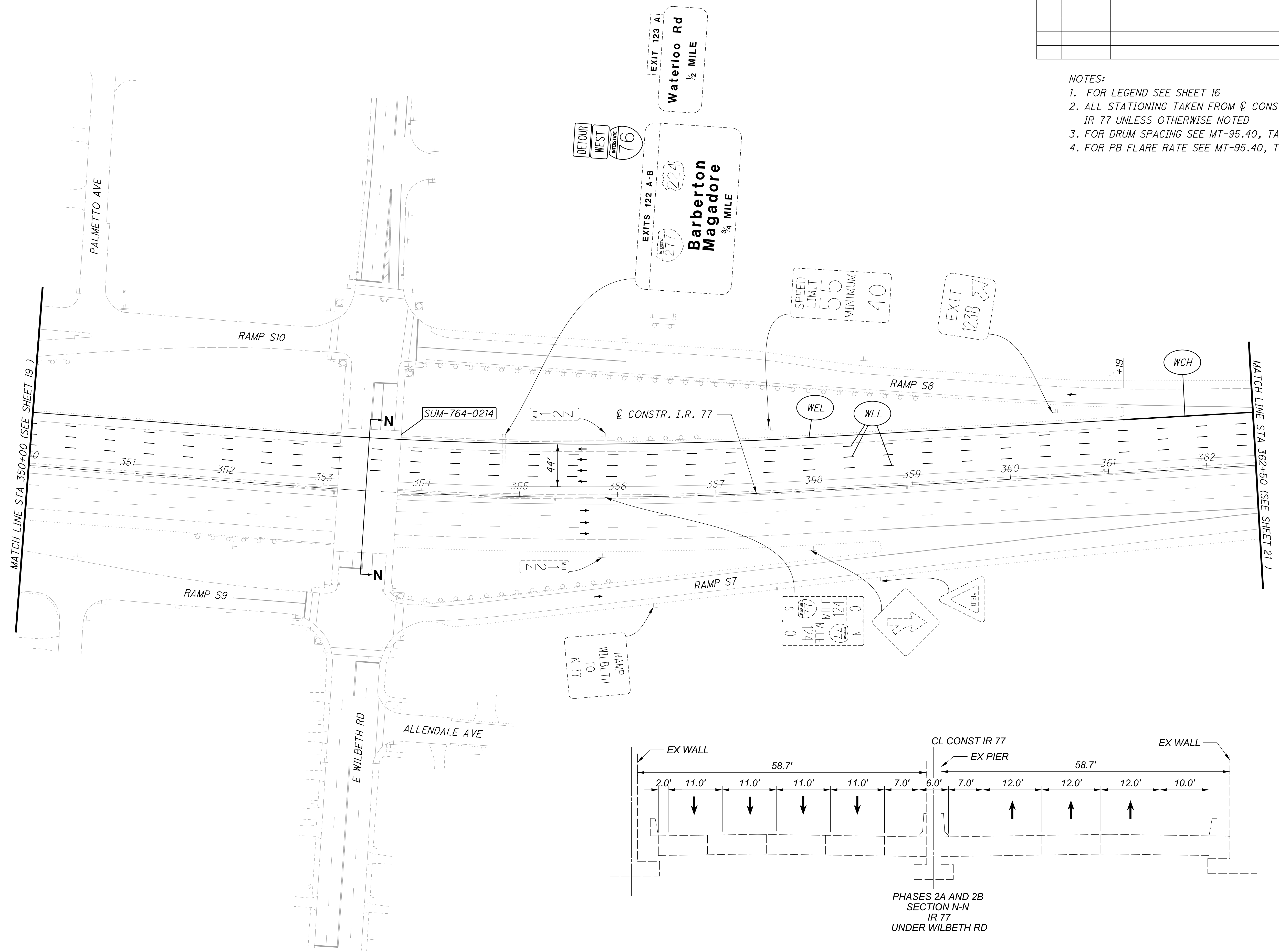
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NO.	DATE	DESCRIPTION

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM ϕ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



MOT IR 77 PHASE 2A
 STA 350+00 TO STA 362+50

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

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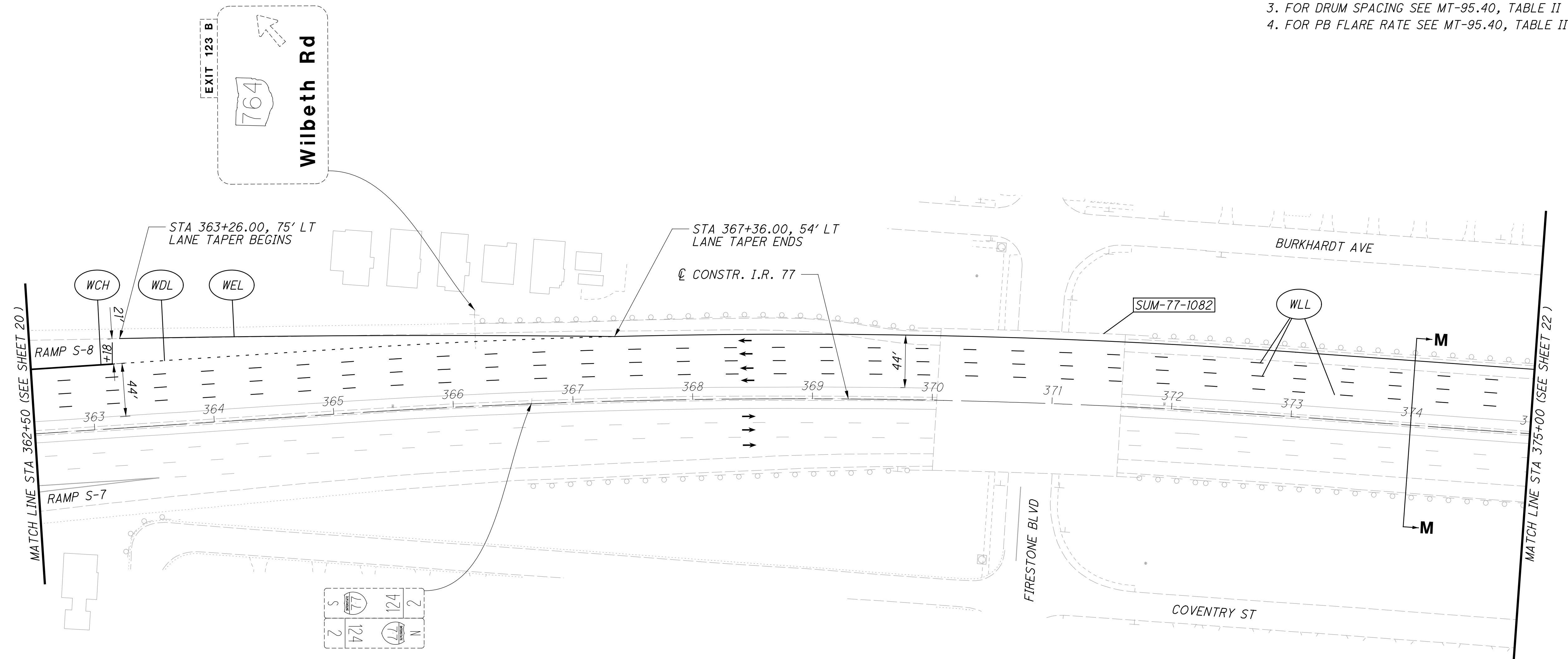
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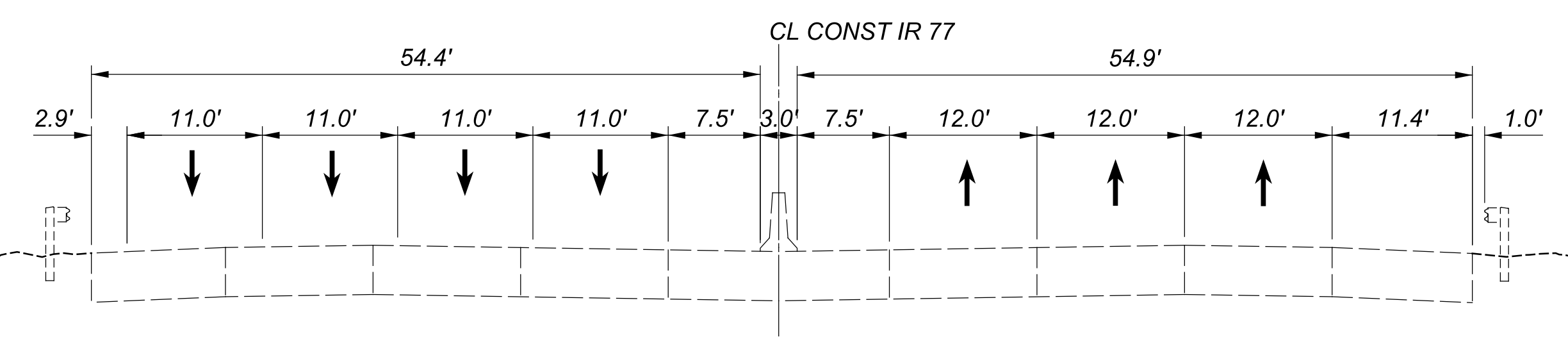
- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM \varnothing CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



S	124	2
2	124	N



PHASES 2A AND 2B
 SECTION M-M
 IR 77 STA 374+00

MOT IR 77 PHASE 2A
 STA 362+50 TO STA 375+00

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

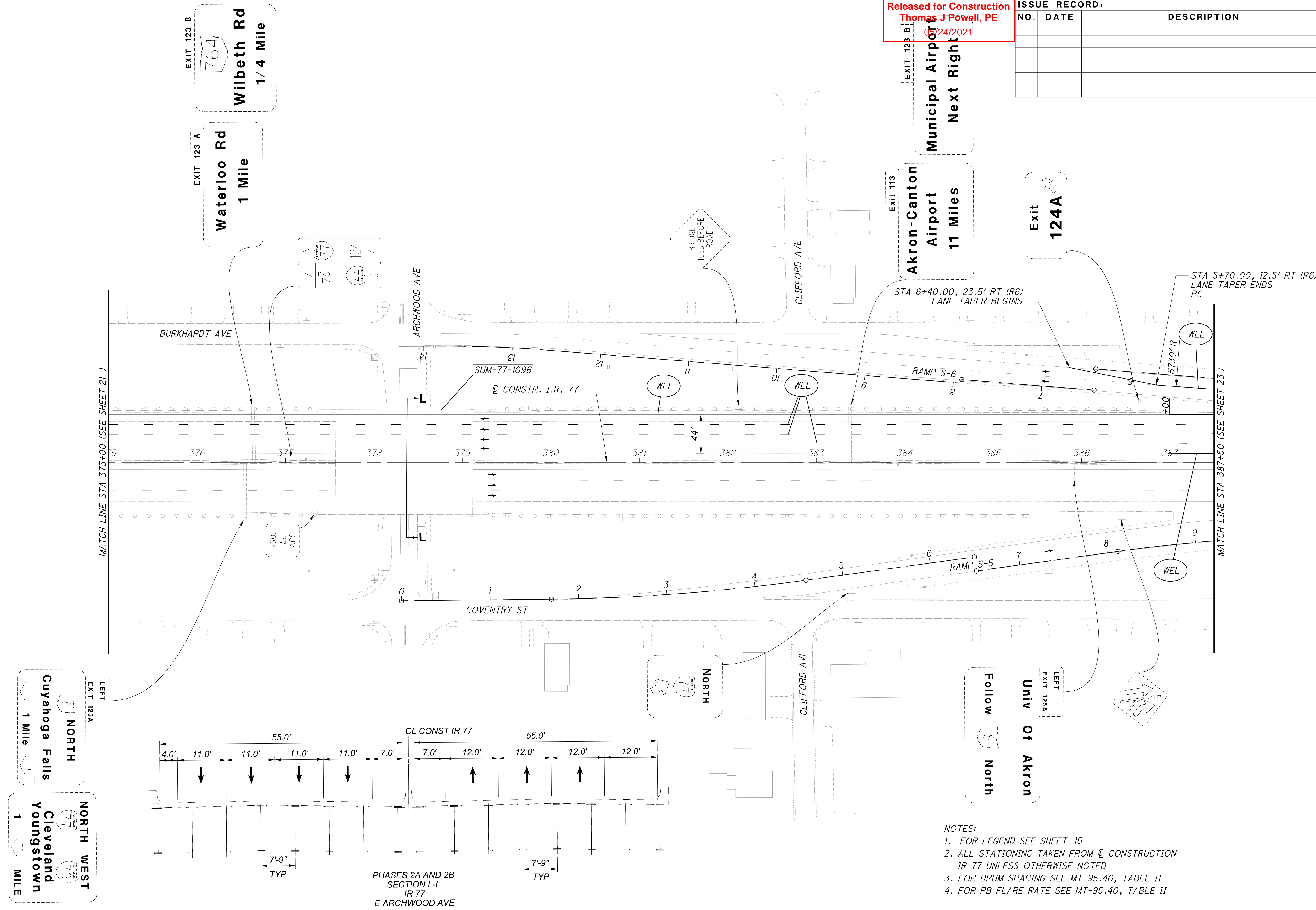
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ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED: FGW
 CHECKED: AVP

0 50 100
 HORIZONTAL SCALE IN FEET

22
 43



- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM @ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

MOT IR 77 PHASE 2A
STA 375+00 TO STA 387+50

SUM-76 / 77 / 8-
8.24 / 9.74 / 0.00

LEFT EXIT 125A
 1 MILE
 NORTH
 Cuyahoga Falls

LEFT EXIT 125A
 1 MILE
 NORTH WEST
 Cleveland
 Youngstown

EXIT 123 B
 764
 Wilbeth Rd
 1/4 Mile

EXIT 123 A
 Waterloo Rd
 1 Mile

EXIT 113
 Akron-Canton Airport
 11 Miles

Exit 124A

Follow North
 Univ Of Akron
 LEFT EXIT 125A

MATCH LINE STA 375+00 (SEE SHEET 21)

MATCH LINE STA 387+50 (SEE SHEET 23)

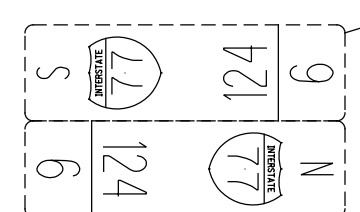
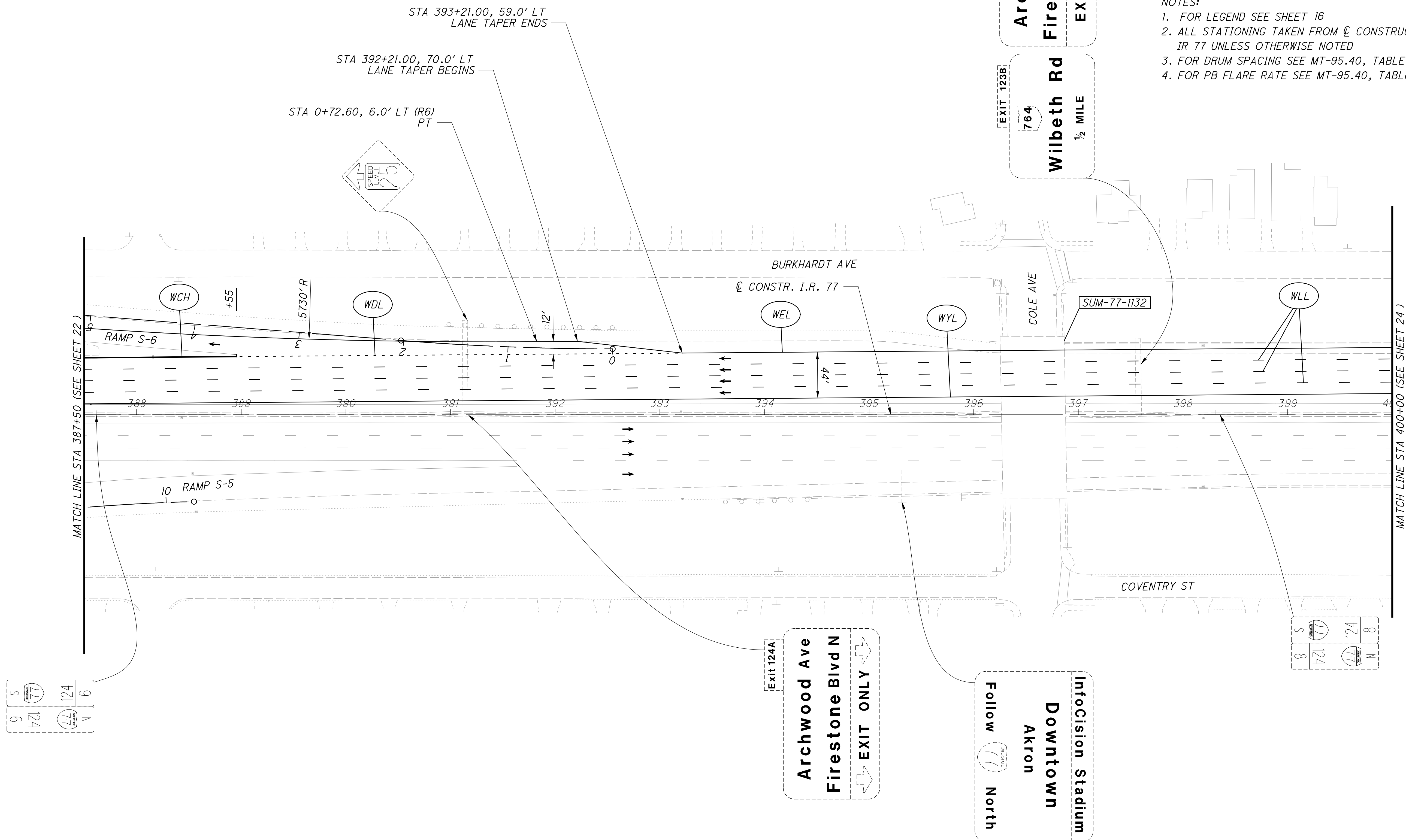
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NO.	DATE	DESCRIPTION

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM @ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



Exit 124A
Archwood Ave
Firestone Blvd N
 EXIT ONLY

Follow
 Akron
 North
InfoCision Stadium
Downtown

Exit 123B
 764
Wilbeth Rd
 1/2 MILE

Exit 124A
Archwood Ave
Firestone Blvd
 EXIT ONLY

MOT IR 77 PHASE 2A
STA 387+50 TO STA 400+00

SUM-76 / 77 / 8-
8.24 / 9.74 / 0.00

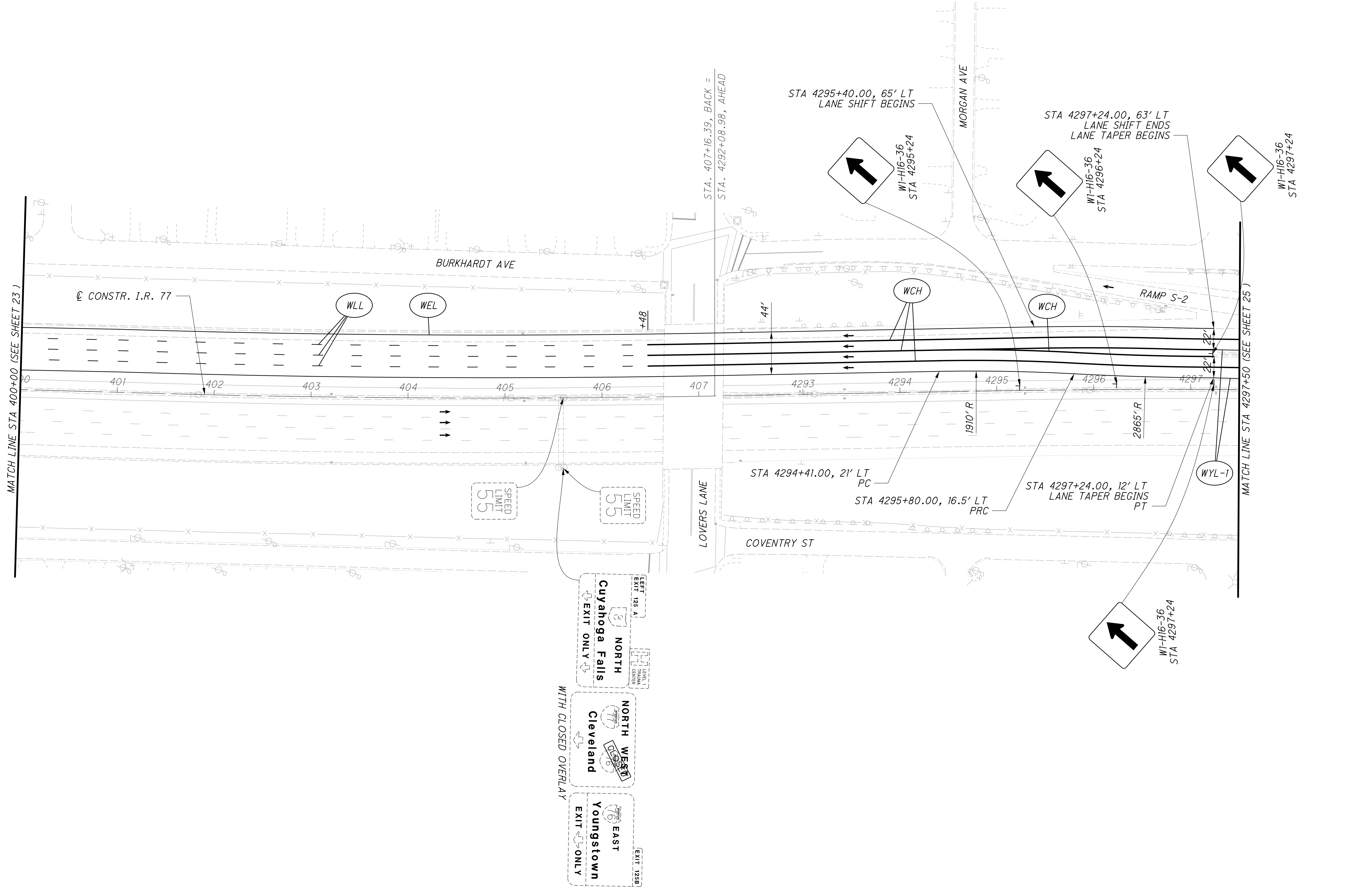
- NOTES:
 1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM \varnothing CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

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CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



**SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00**

**MOT IR 77 PHASE 2A
 STA 400+00 TO STA 4297+50**

24
 43

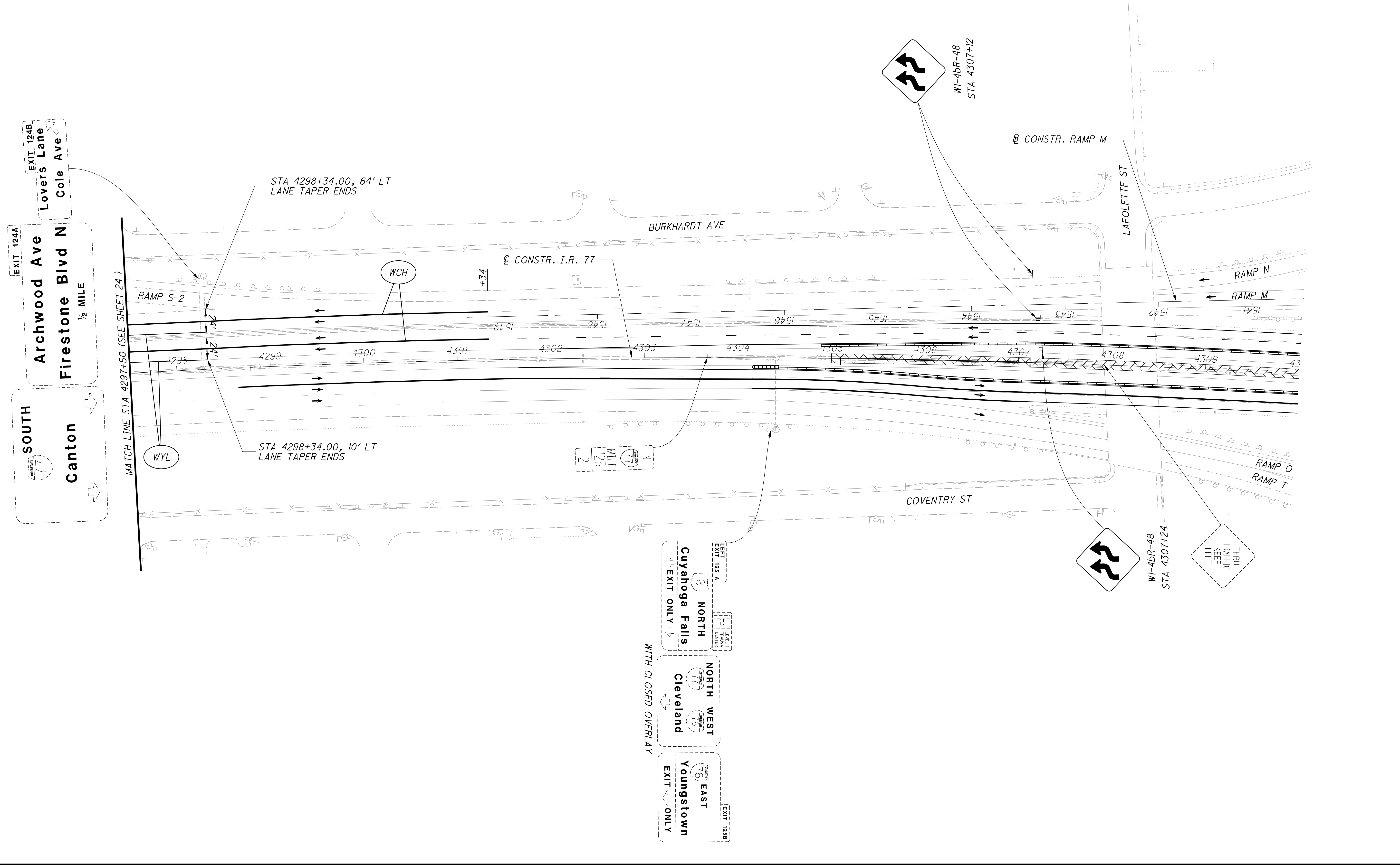
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 2. ALL STATIONING TAKEN FROM \varnothing CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

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CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



SUM-76 / 77 / 8-
8.24 / 9.74 / 0.00

MOT IR 77 PHASE 2A
STA 4297+50 TO STA 4310+00

25
 43

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NO.	DATE	DESCRIPTION

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM C CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

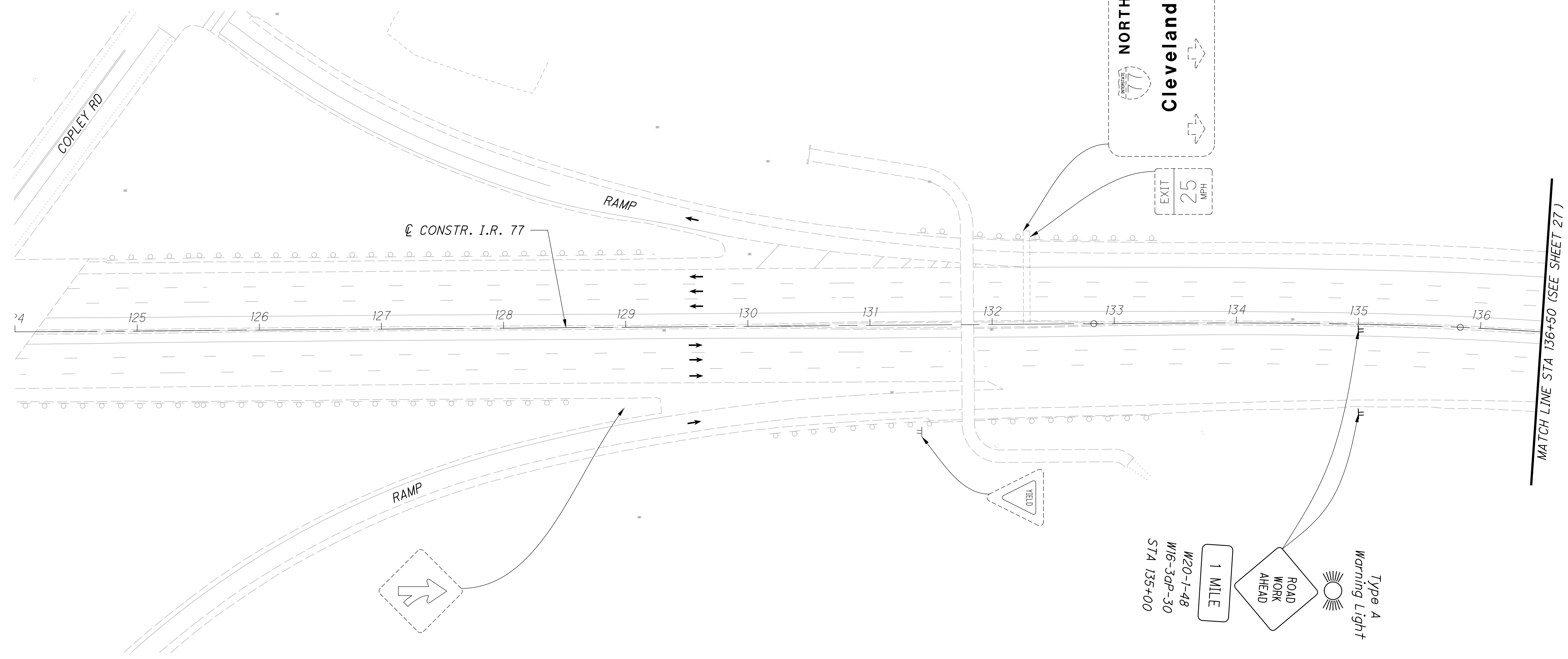
0 50 100
 HORIZONTAL
 SCALE IN FEET

EXTRA ADVANCE WARNING SIGNS (IR 77 SB SIDE)

Type A
 Warning Light

ROAD
 WORK
 AHEAD
 W20-1-48
 STA. 82+00

WATCH
 FOR STOPPED
 TRAFFIC
 W3-H4b-48
 STA. 123+60



MOT IR 77 PHASE 2A STEP 1 & 2
 STA 124+00 TO STA 136+50

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

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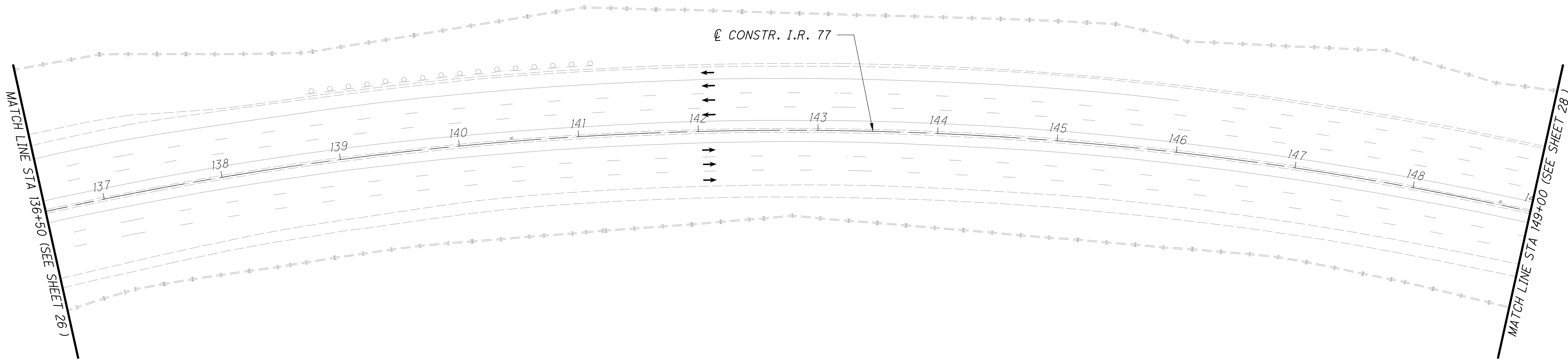
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 05/24/2021

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NO.	DATE	DESCRIPTION

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM \odot CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

HORIZONTAL
 SCALE IN FEET



SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

MOT IR 77 PHASE 2A STEP 1 & 2
 STA 136+50 TO STA 149+00

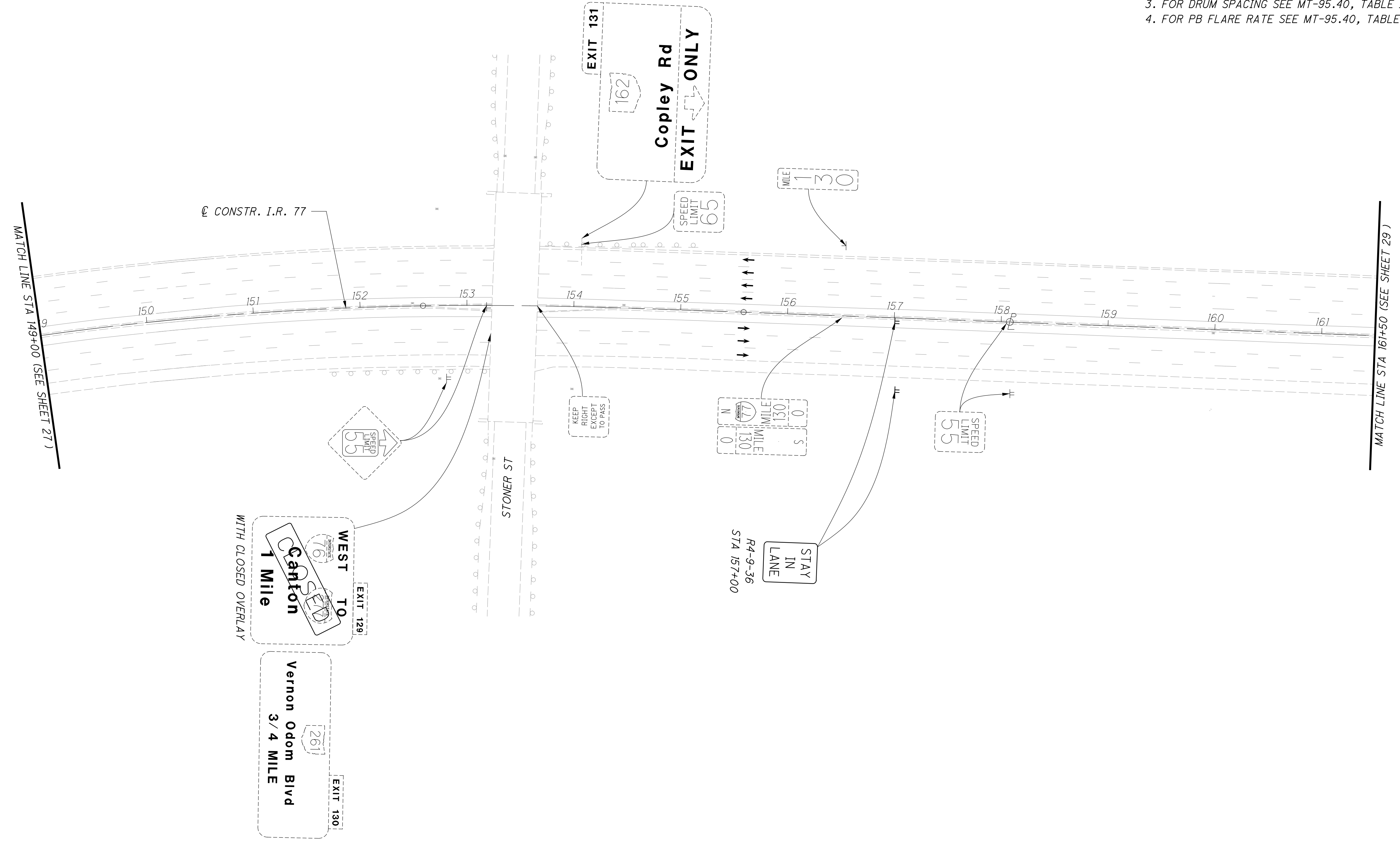
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ISSUE RECORD:		
NO.	DATE	DESCRIPTION

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM ϕ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



MOT IR 77 PHASE 2A STEP 1 & 2
 STA 149+00 TO STA 161+50

SUM-76 77 8-
 8.24 / 9.74 / 0.00

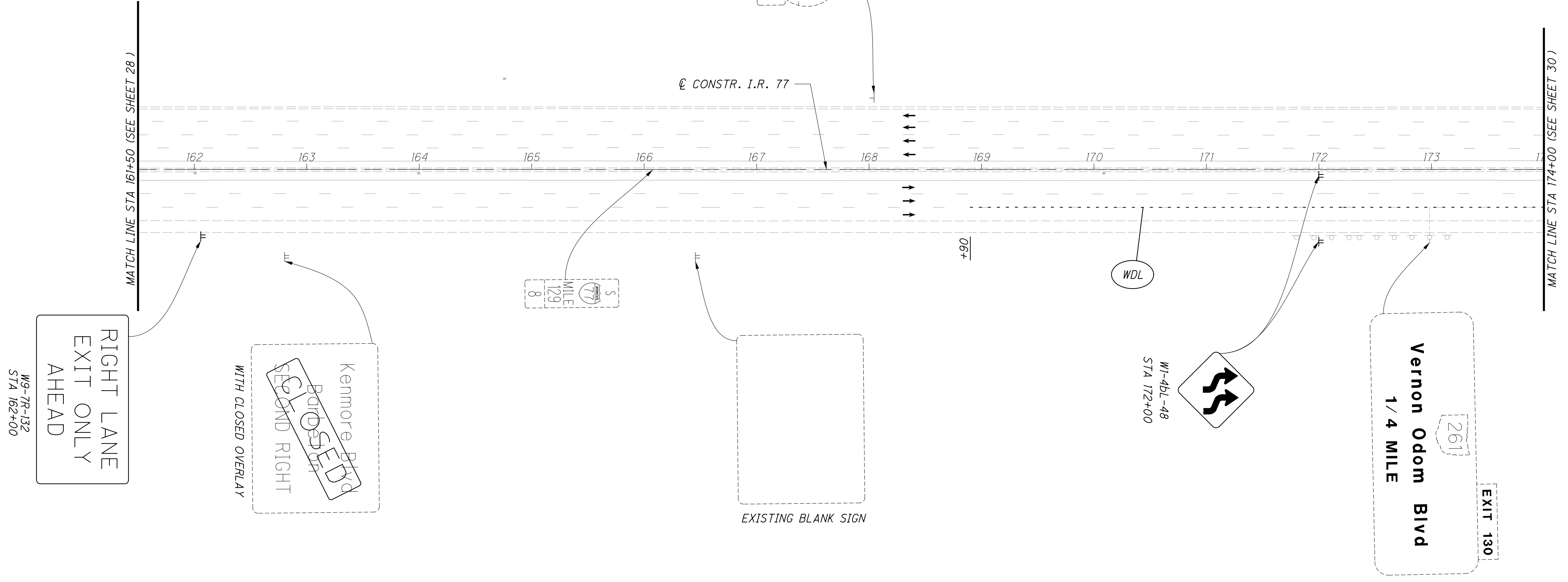
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 05/24/2021

ISSUE RECORD:		
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- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM \odot CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



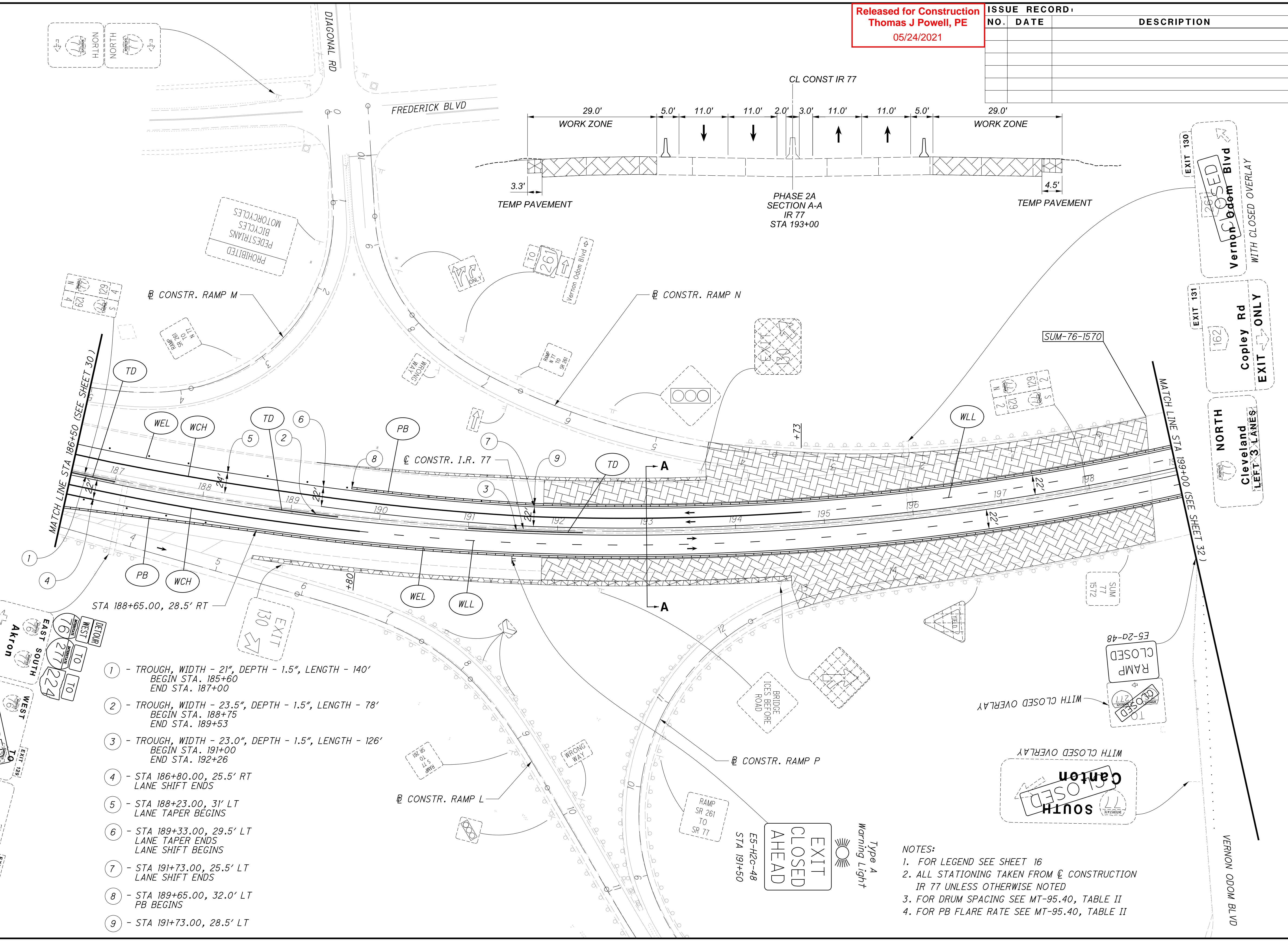
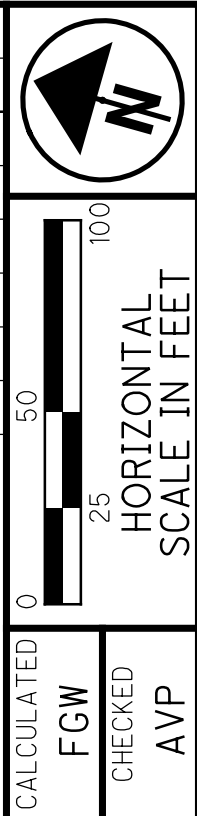
MOT IR 77 PHASE 2A STEP 1 & 2
 STA 161+50 TO STA 174+00

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

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ISSUE RECORD:		
NO.	DATE	DESCRIPTION



- ① - TROUGH, WIDTH - 21", DEPTH - 1.5", LENGTH - 140'
 BEGIN STA. 185+60
 END STA. 187+00
- ② - TROUGH, WIDTH - 23.5", DEPTH - 1.5", LENGTH - 78'
 BEGIN STA. 188+75
 END STA. 189+53
- ③ - TROUGH, WIDTH - 23.0", DEPTH - 1.5", LENGTH - 126'
 BEGIN STA. 191+00
 END STA. 192+26
- ④ - STA 186+80.00, 25.5' RT
 LANE SHIFT ENDS
- ⑤ - STA 188+23.00, 31' LT
 LANE TAPER BEGINS
- ⑥ - STA 189+33.00, 29.5' LT
 LANE TAPER ENDS
 LANE SHIFT BEGINS
- ⑦ - STA 191+73.00, 25.5' LT
 LANE SHIFT ENDS
- ⑧ - STA 189+65.00, 32.0' LT
 PB BEGINS
- ⑨ - STA 191+73.00, 28.5' LT

NOTES:
 1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM \varnothing CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

MOT IR 77 PHASE 2A STEP 1
STA 186+50 TO STA 199+00

SUM-76 / 77 / 8 -
8.24 / 9.74 / 0.00

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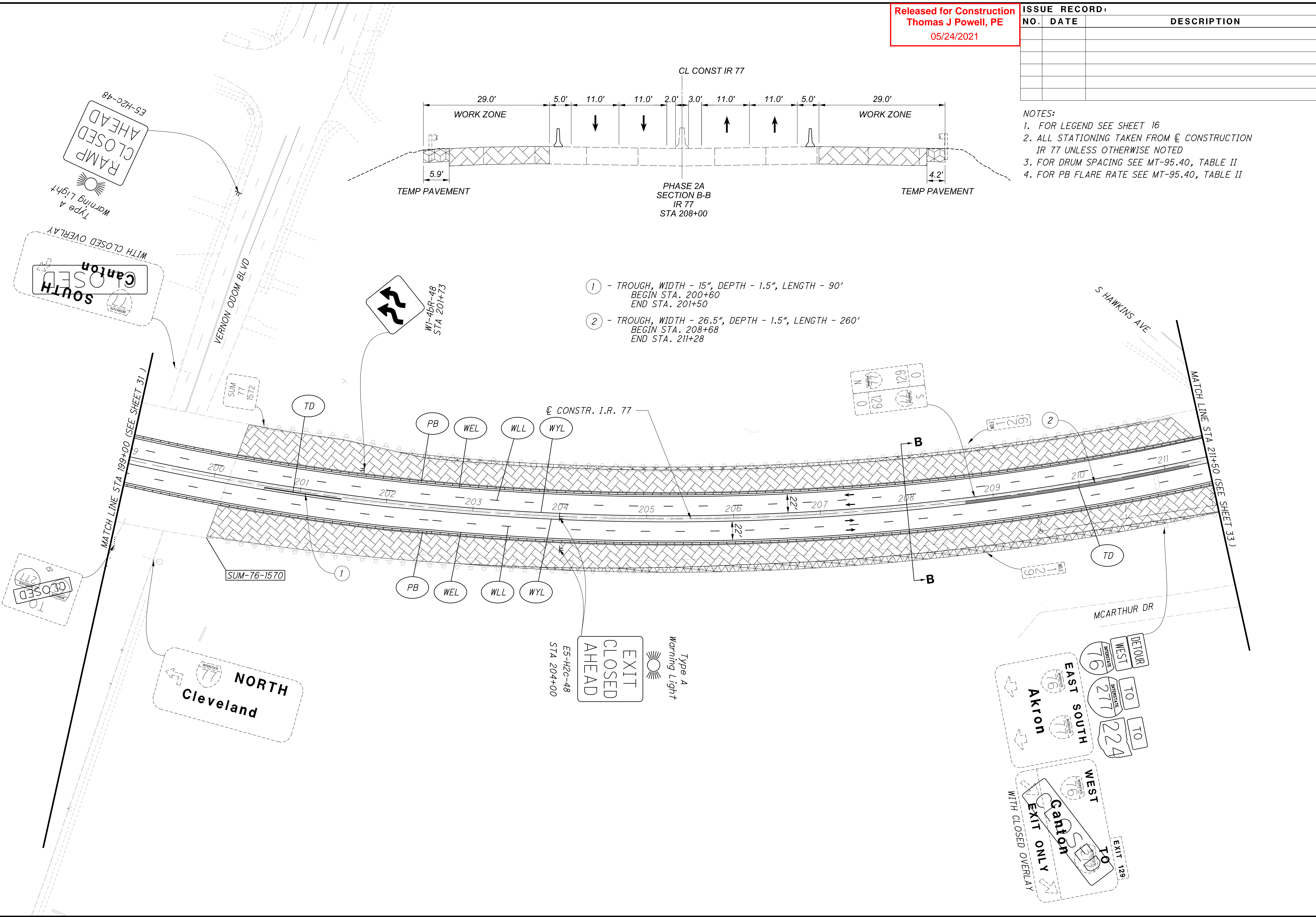
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 05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

- NOTES:
- FOR LEGEND SEE SHEET 16
 - ALL STATIONING TAKEN FROM $\text{\textcircled{C}}$ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 - FOR DRUM SPACING SEE MT-95.40, TABLE II
 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

HORIZONTAL
 SCALE IN FEET



IR 77 PHASE 2A STEP 1
 STA 199+00 TO STA 211+50

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

32
 43

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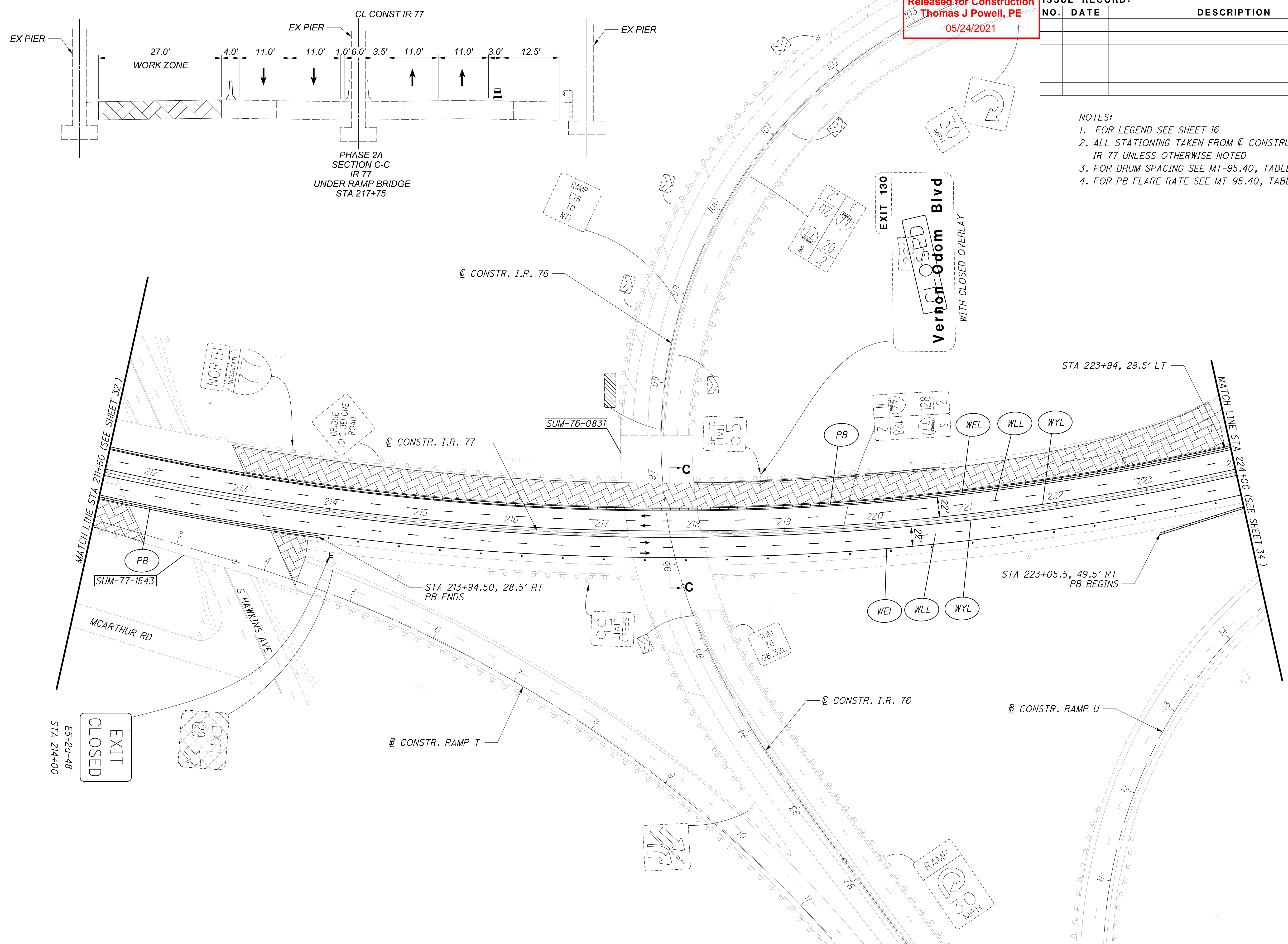
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05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

- NOTES:
1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM @ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

0 50 100
25
HORIZONTAL
SCALE IN FEET

CALCULATED
FGW
CHECKED
AVP



MOT IR 76 / IR 77 PHASE 2A STEP 1
STA 211+50 TO STA 224+00

SUM-76 / 77 / 8-
8.24 / 9.74 / 0.00

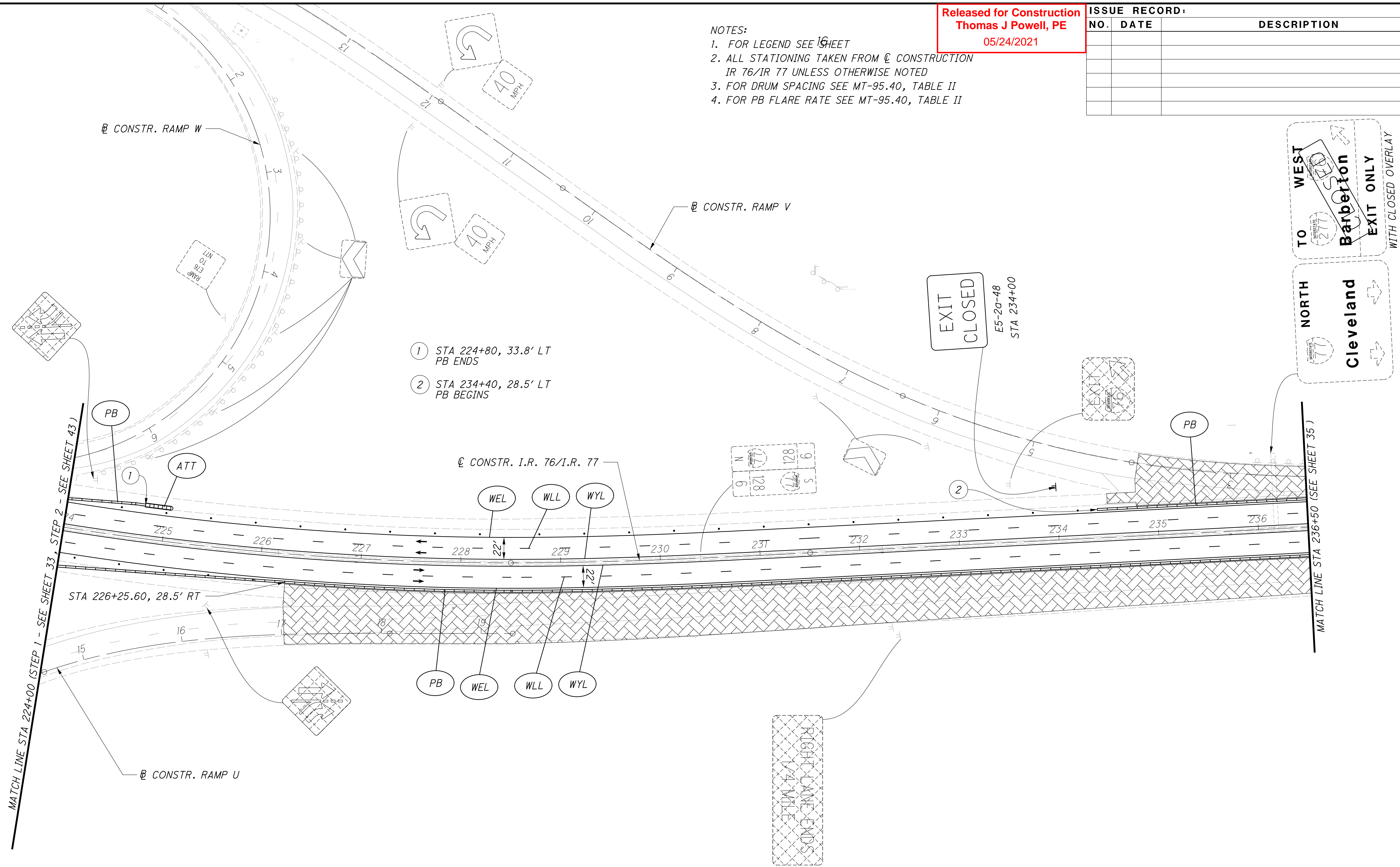
33
43

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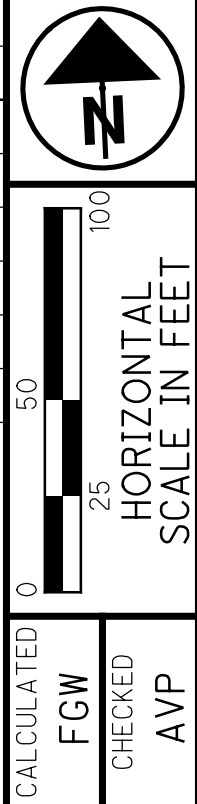
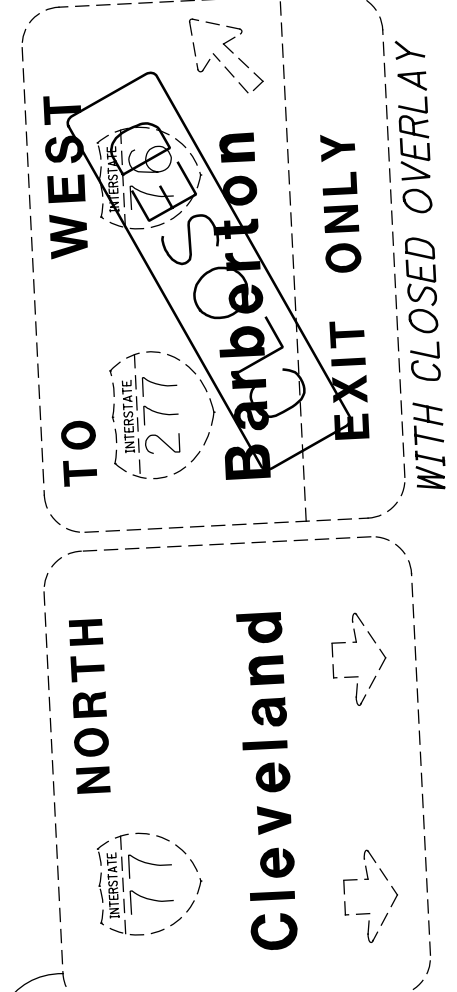
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 05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

- NOTES:
1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM $\text{\textcircled{C}}$ CONSTRUCTION IR 76/IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II



- 1 STA 224+80, 33.8' LT
PB ENDS
- 2 STA 234+40, 28.5' LT
PB BEGINS



CALCULATED
 FGW
 CHECKED
 AVP

MOT IR 76 / IR 77 PHASE 2A STEP 1 & 2
STA 224+00 TO STA 236+50

SUM-76 / 77 / 8-
8.24 / 9.74 / 0.00

34
 43

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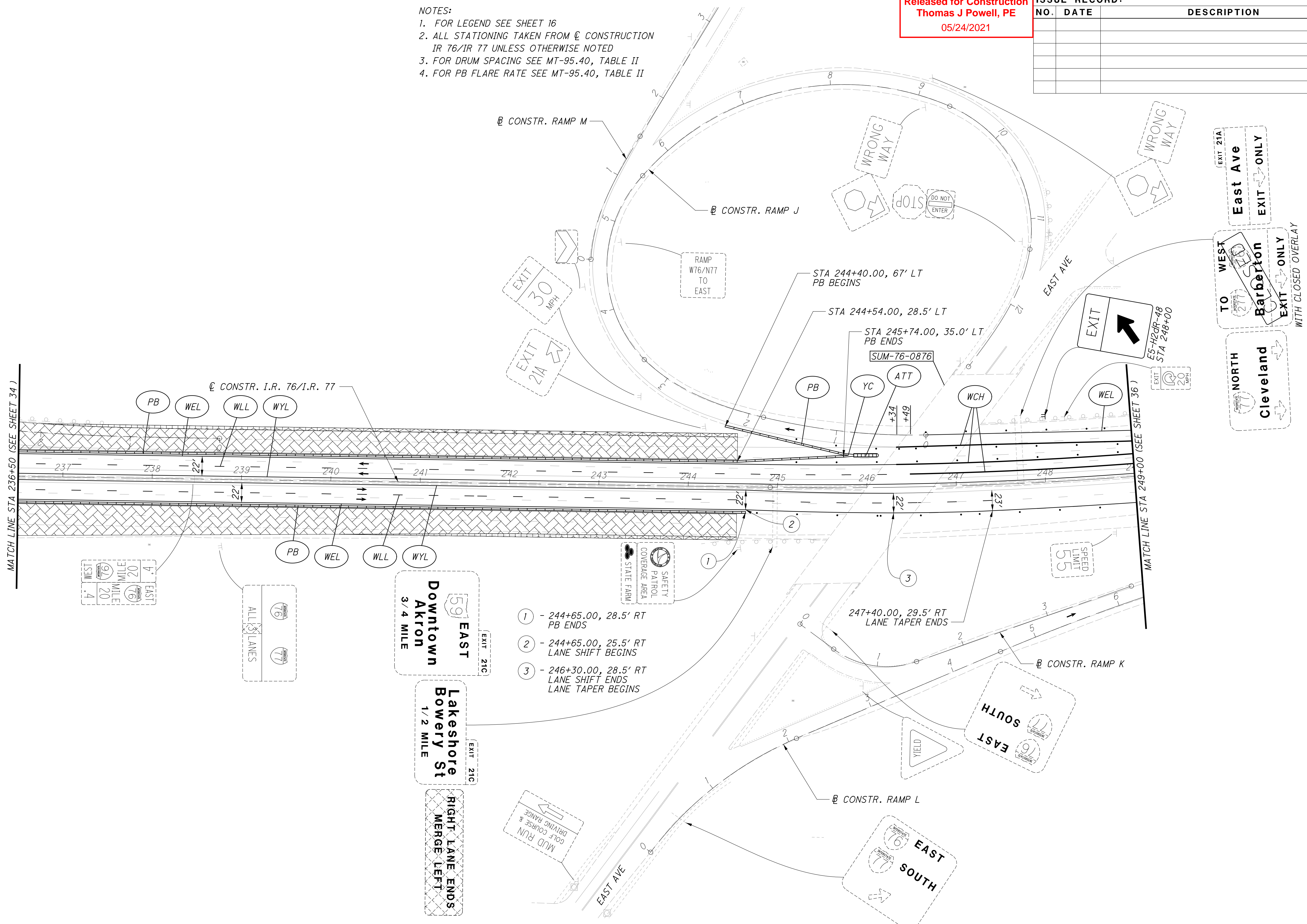
- NOTES:
1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM \varnothing CONSTRUCTION IR 76/IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

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ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 25
 HORIZONTAL
 SCALE IN FEET



- ① - 244+65.00, 28.5' RT PB ENDS
- ② - 244+65.00, 25.5' RT LANE SHIFT BEGINS
- ③ - 246+30.00, 28.5' RT LANE SHIFT ENDS LANE TAPER BEGINS

MOT IR 76 / IR 77 PHASE 2A STEP 1 & 2
 STA 236+50 TO STA 249+00

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

35
 43

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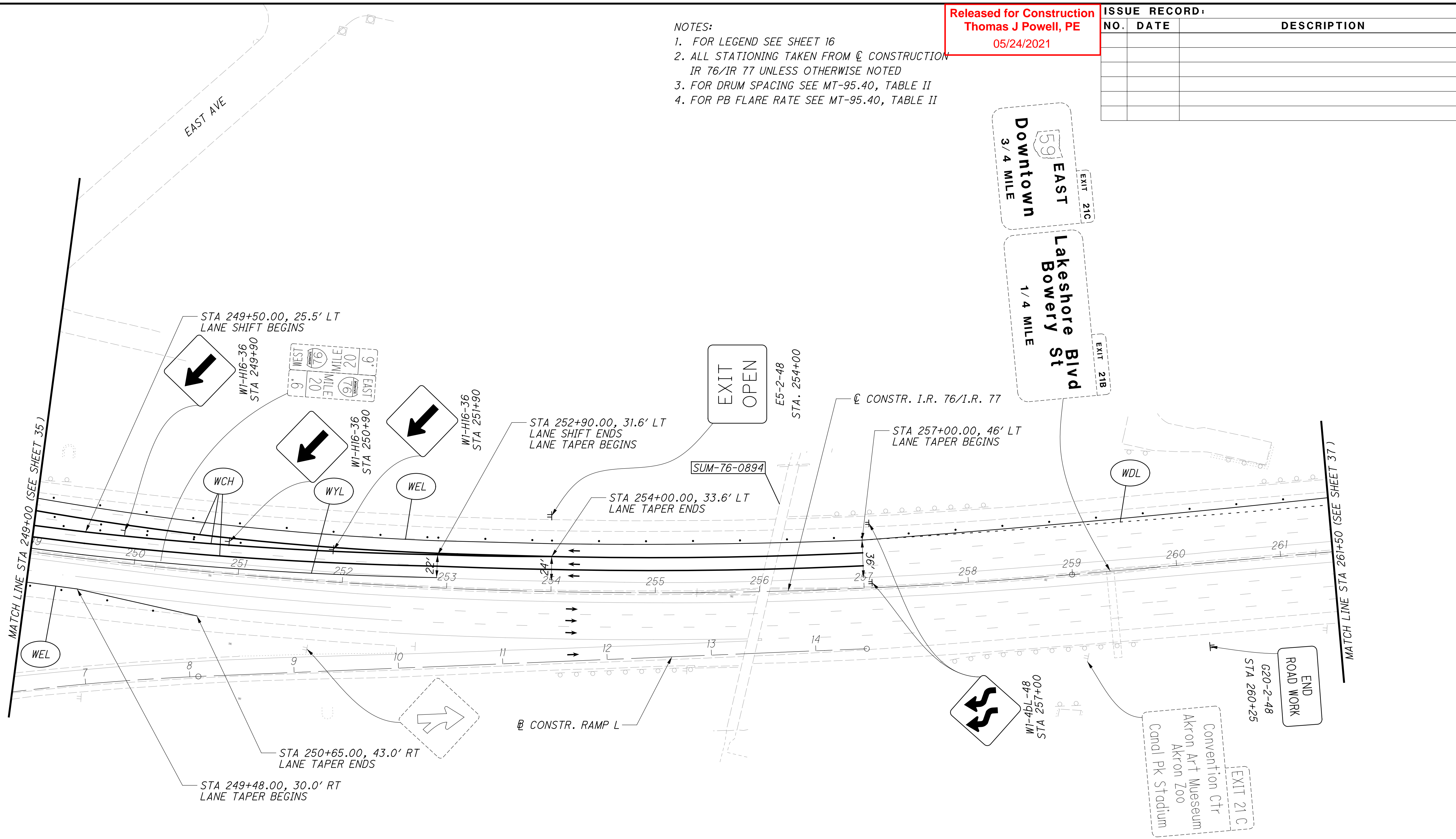
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05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

- NOTES:
1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM $\text{\textcircled{C}}$ CONSTRUCTION IR 76/IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

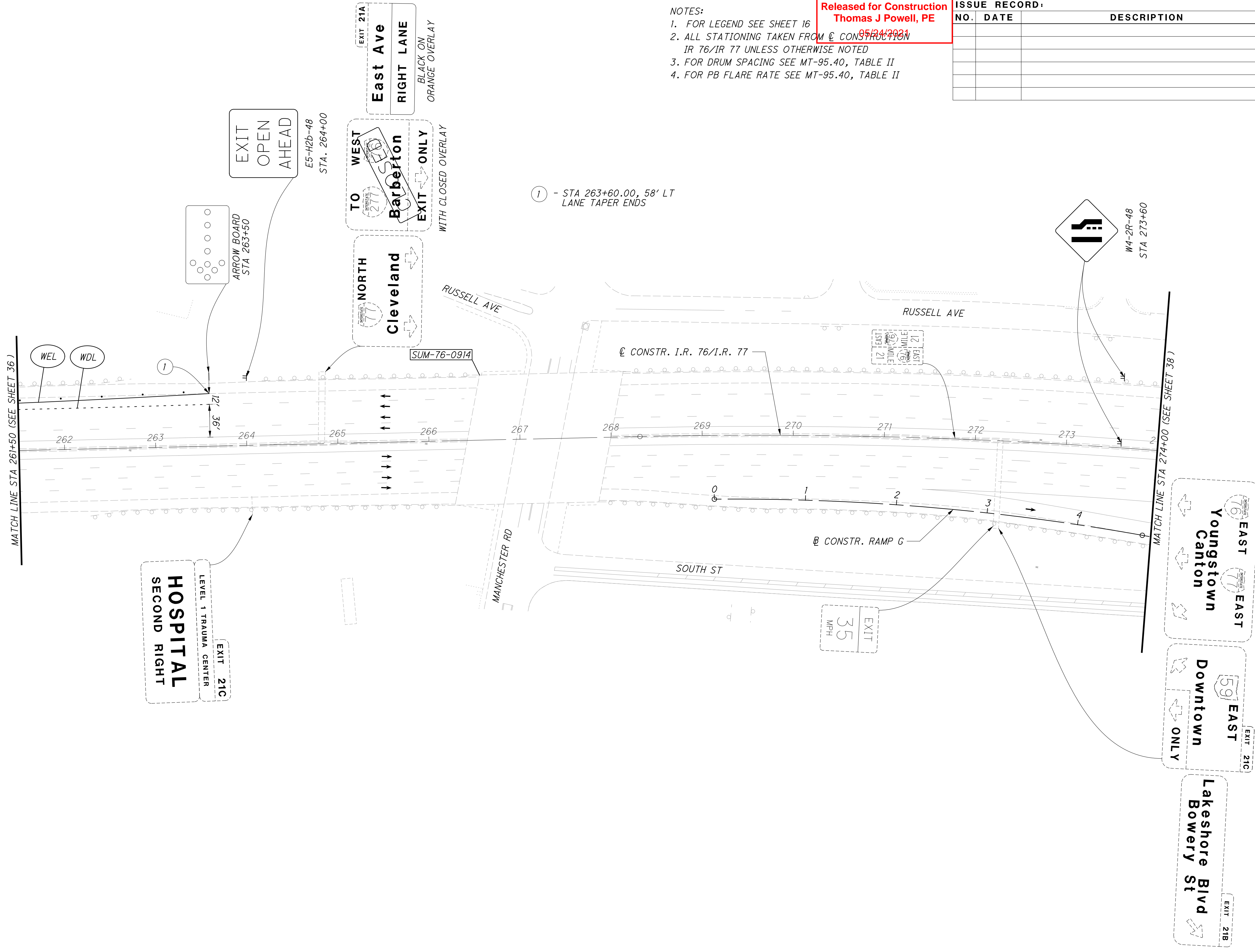
CALCULATED
FGW
CHECKED
AVP

0 50 100
25
HORIZONTAL
SCALE IN FEET



MOT IR 76 / IR 77 PHASE 2A STEP 1 & 2
STA. 249+00 TO STA 261+50

SUM-76 / 77 / 8-
8.24 / 9.74 / 0.00



NOTES:
 1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM $\text{\textcircled{C}}$ CONSTRUCTION IR 76/IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

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ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET

① - STA 263+60.00, 58' LT
 LANE TAPER ENDS

MOT IR 76 / IR 77 PHASE 2A STEP 1 & 2
 STA 261+50 TO STA 274+00

SUM-76/77 8-
 8.24/9.74/0.00

37
 43

SUSER STIMESTAMPS
 SDATES
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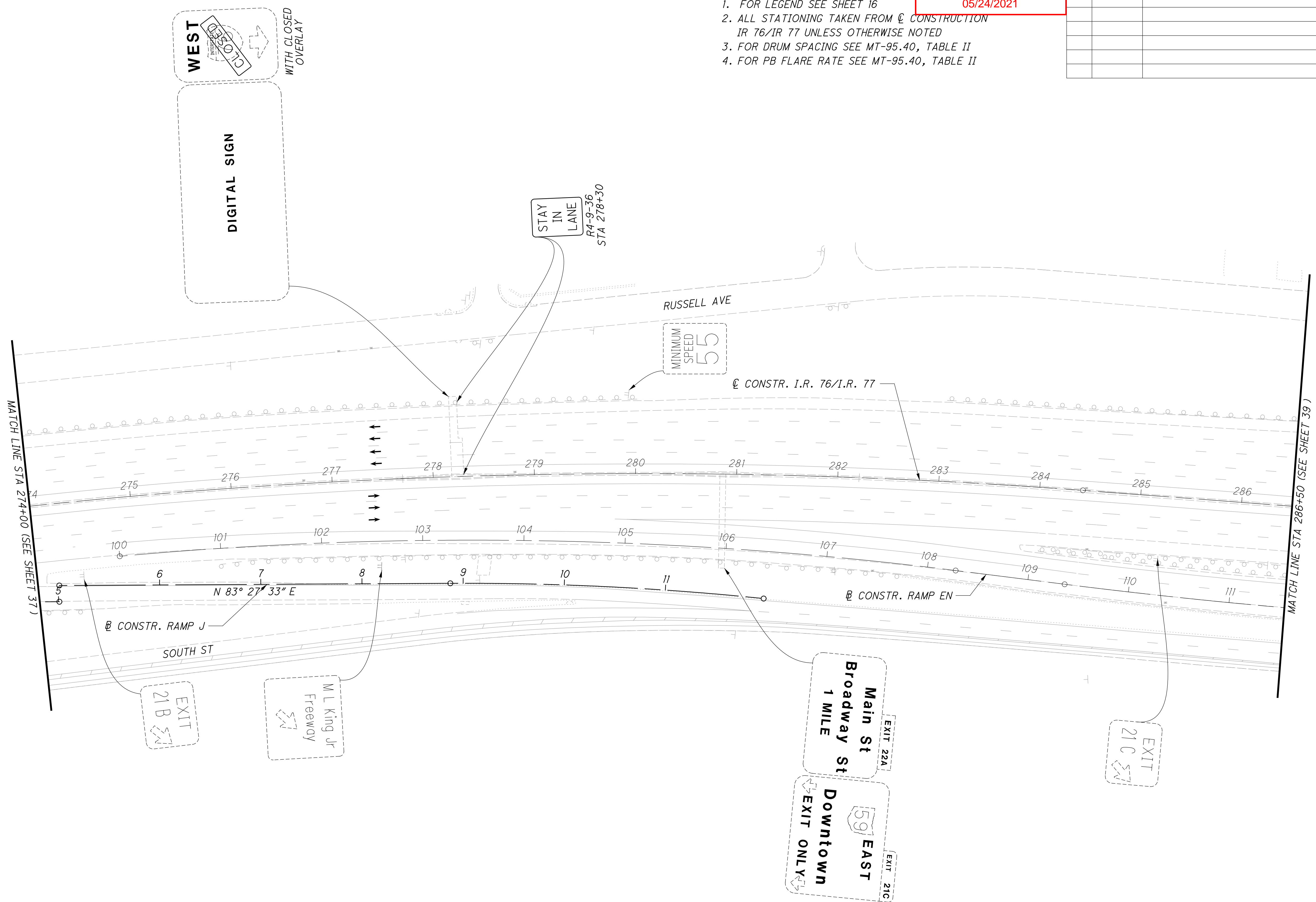
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 05/24/2021

ISSUE RECORD:		
NO.	DATE	DESCRIPTION

- NOTES:
1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM @ CONSTRUCTION IR 76/IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



MOT IR 76 / IR 77 PHASE 2A STEP 1 & 2
 STA 274+00 TO STA 286+50

SUM-76/77 8-
 8.24/9.74/0.00

SUSER SPATES STIMESTAMPS
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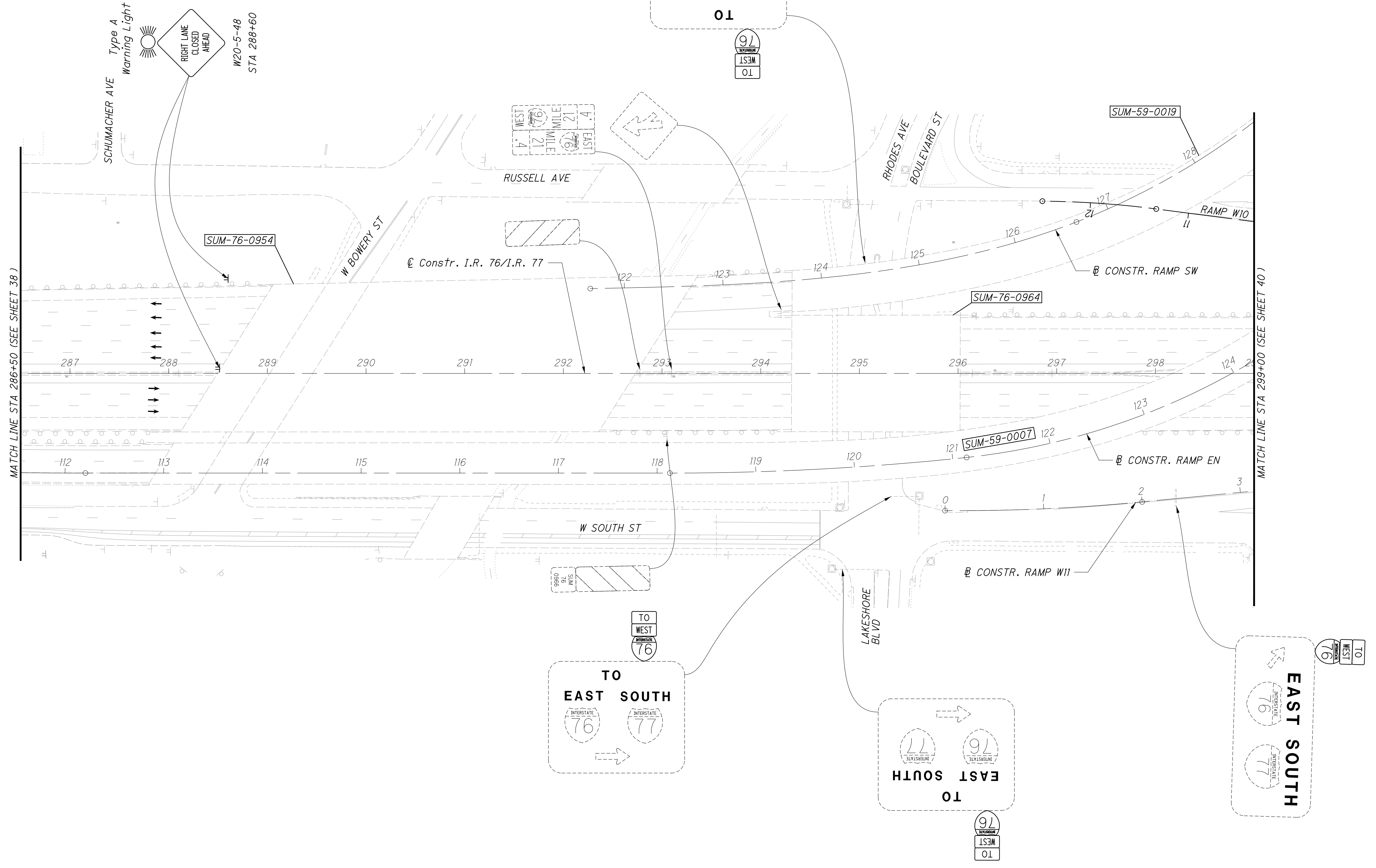
- NOTES:
 1. FOR LEGEND SEE SHEET 16
 2. ALL STATIONING TAKEN FROM C CONSTRUCTION IR 76/IR 77 UNLESS OTHERWISE NOTED
 3. FOR DRUM SPACING SEE MT-95.40, TABLE II
 4. FOR PB FLARE RATE SEE MT-95.40, TABLE II

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ISSUE RECORD:		
NO.	DATE	DESCRIPTION

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET



MATCH LINE STA 286+50 (SEE SHEET 38)

MATCH LINE STA 299+00 (SEE SHEET 40)

TO EAST SOUTH
 LEFT LANE

76 INTERSTATE
 77 INTERSTATE

TO WEST
 76 INTERSTATE

TO EAST SOUTH

76 INTERSTATE
 77 INTERSTATE

TO EAST SOUTH

76 INTERSTATE
 77 INTERSTATE

TO WEST
 76 INTERSTATE

EAST SOUTH

76 INTERSTATE
 77 INTERSTATE

MOT IR 76 / IR 77 PHASE 2A STEP 1 & 2
 STA 286+50 TO 299+00

SUM-76/77/8-
 8.24/9.74/0.00

39
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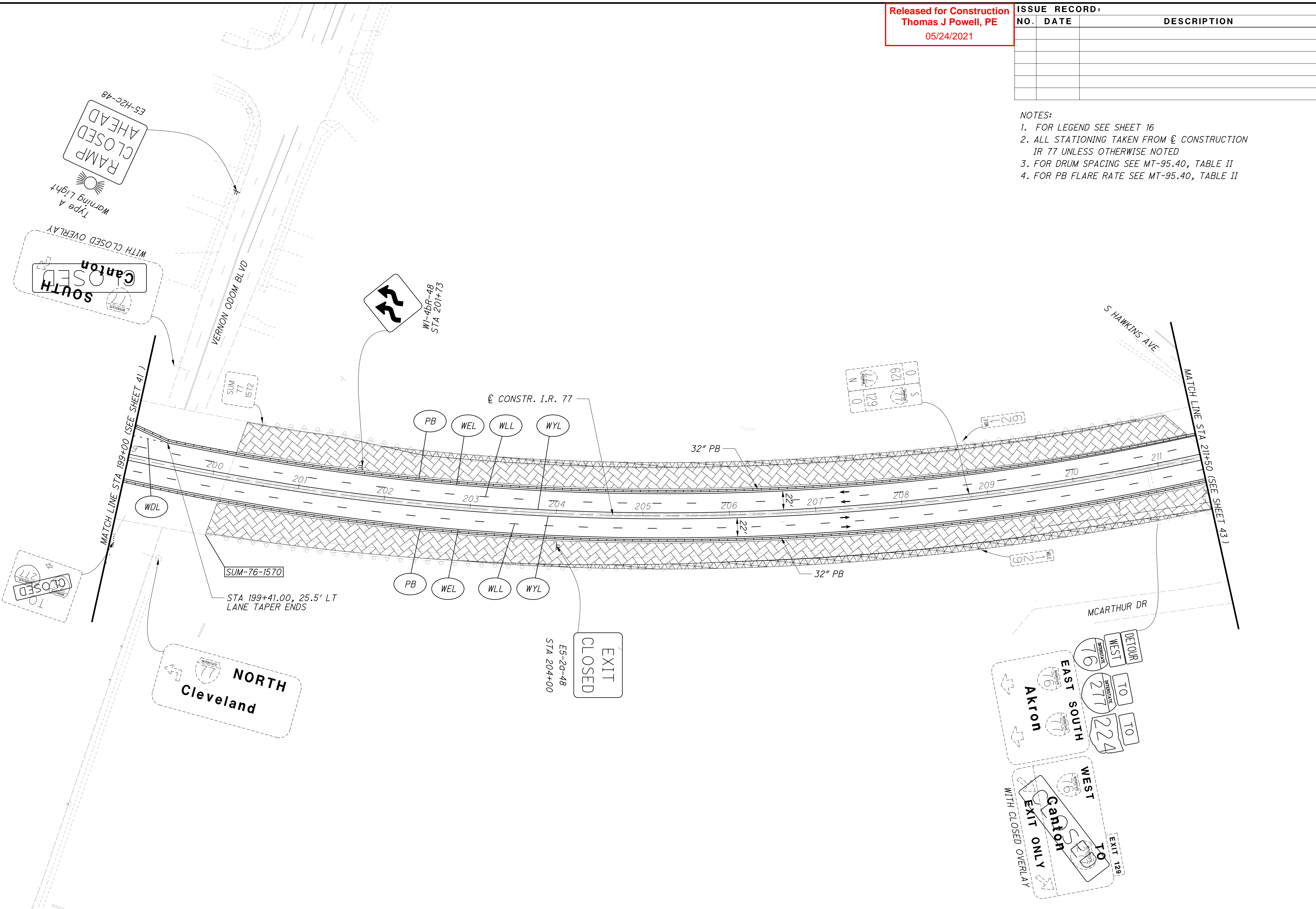
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 - ALL STATIONING TAKEN FROM ϕ CONSTRUCTION IR 77 UNLESS OTHERWISE NOTED
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 - FOR PB FLARE RATE SEE MT-95.40, TABLE II

CALCULATED
 FGW
 CHECKED
 AVP

0 50 100
 HORIZONTAL
 SCALE IN FEET

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MOT IR 76 / IR 77 PHASE 2A STEP 2
 STA 199+00 TO STA 211+50

SUM-76 / 77 / 8-
 8.24 / 9.74 / 0.00

42
 43

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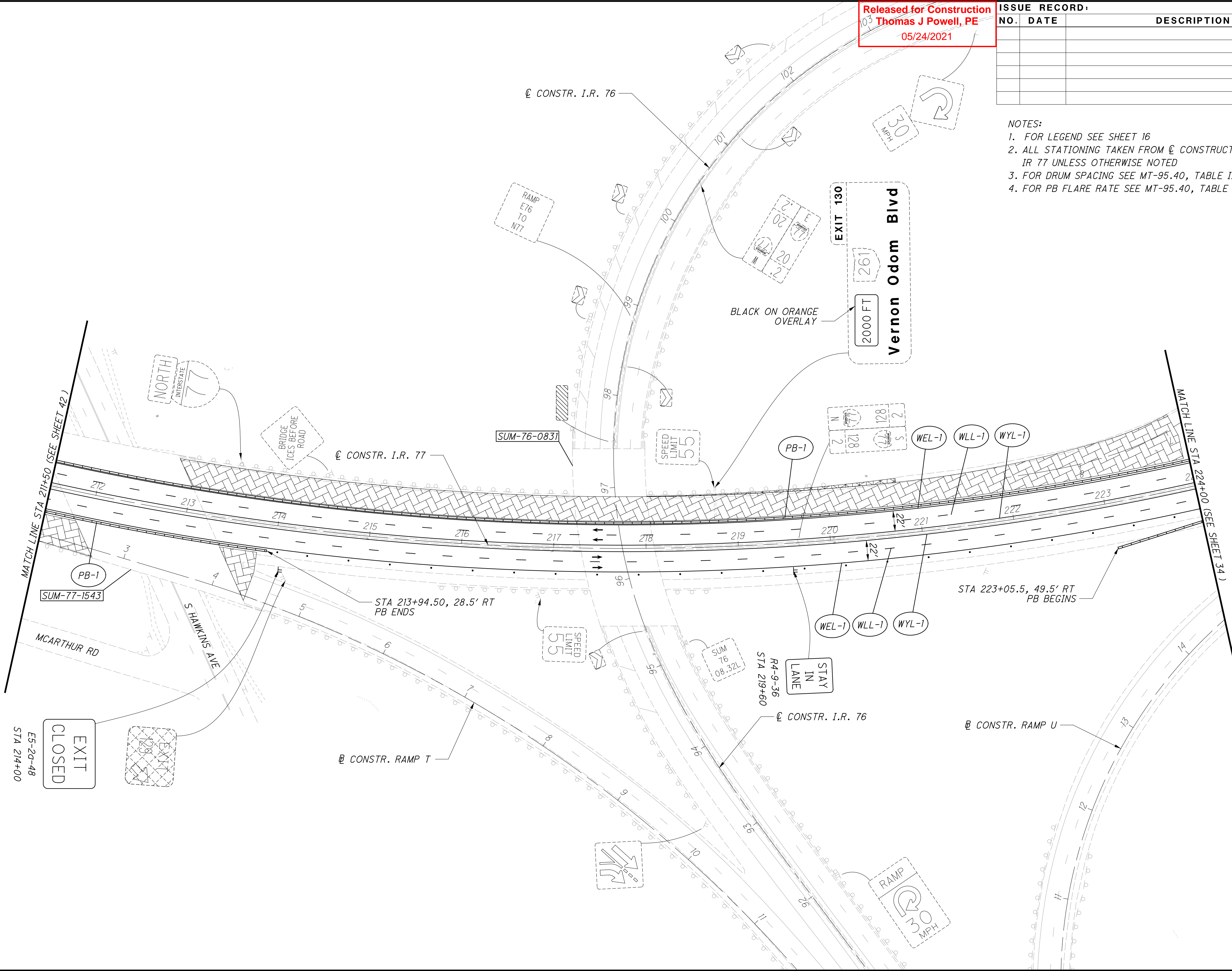
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- NOTES:
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CALCULATED
FGW
CHECKED
AVP

0 50 100
25
HORIZONTAL
SCALE IN FEET



MOT IR 76 / IR 77 PHASE 2A STEP 2
STA 211+50 TO STA 224+00

SUM-76 / 77 / 8-
8.24 / 9.74 / 0.00

43
43