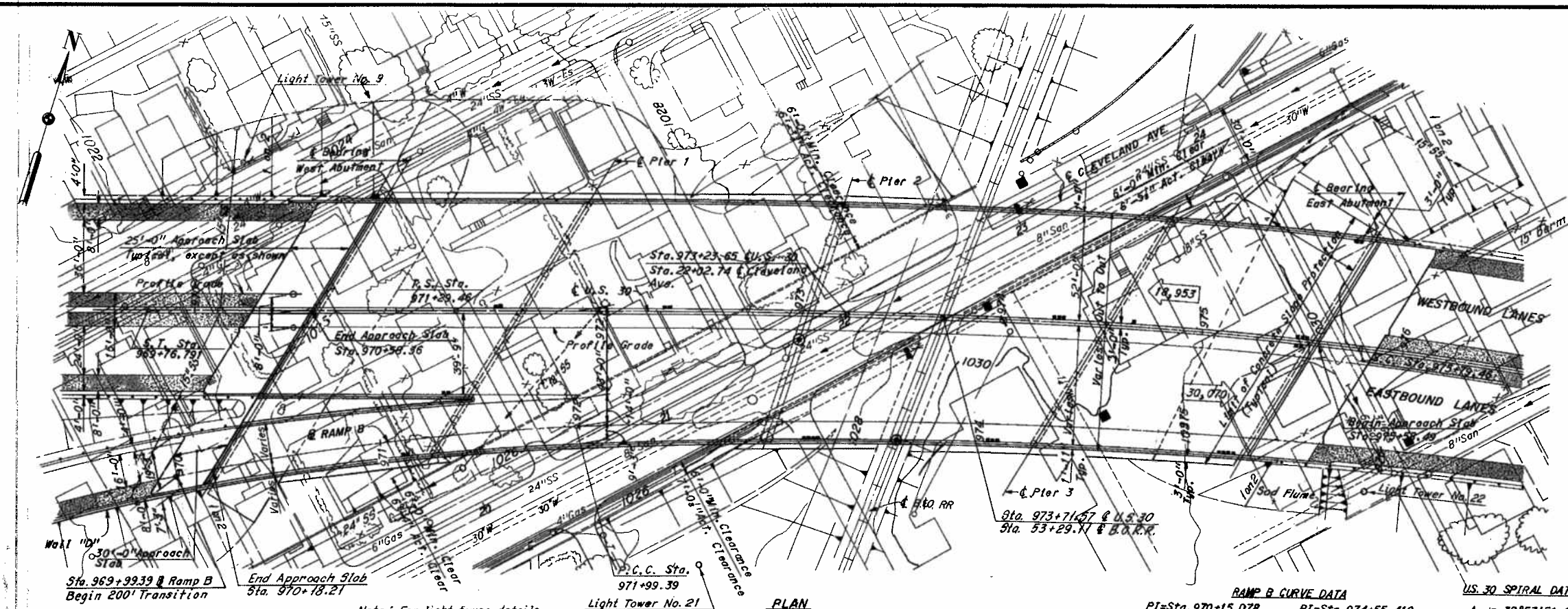


FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS
2	OHIO		

205
299

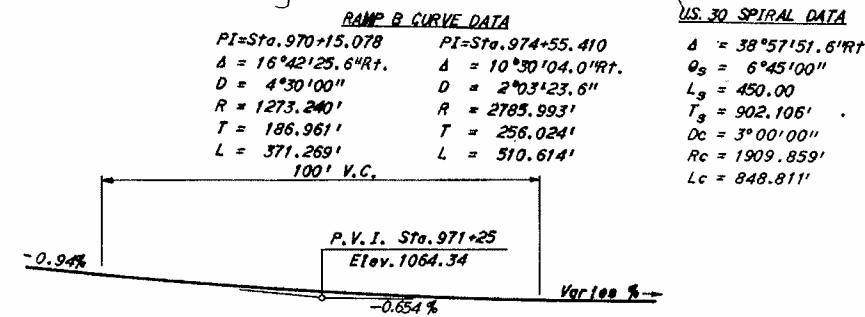
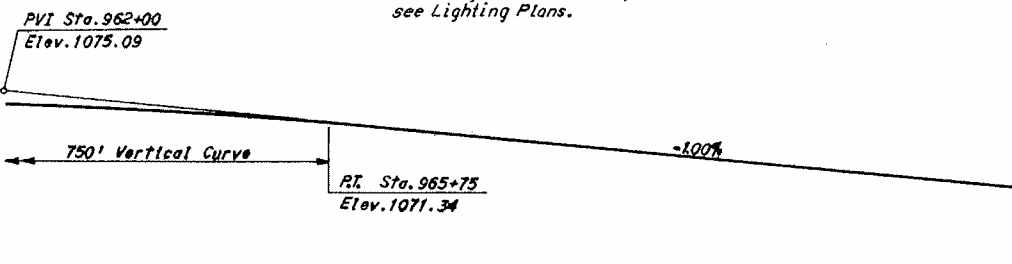
STARK COUNTY
STA-30-15.04
STA-77-9.22



Notes:
 The following items are not included in the bridge plans. See roadway plans for details.
 1. Relocation or removal of existing utilities.
 2. Approach grading, pavements and slabs.
 3. Guardrails, except barrier railing on bridge median.
 18,953 Denotes one way 1995 average daily traffic.
 Earthwork limits shown are schematic. Actual slopes shall conform to plan cross-sections.
 Ⓞ Denotes point of minimum vertical clearance.
 ■ Denotes Standard scupper
 All piles shall be 12" cast-in-place concrete with an estimated average piling length of 70'-0" for the East and West Abutments, 50'-0" for Piers 1, 2, and 3. These estimates are based on boring data and are approximate only. The contractor shall assume full responsibility for the length of pile selected for driving.
 For additional information see Sheet 2120

PROPOSED STRUCTURE	
Type:	Continuous Welded Girders with Reinforced Concrete Slab.
Spans:	101'-6", 141'-6", 149'-0", 103'-0" = 495'-0"
Center to Center:	End Bearings along U.S. 30.
Center to Center:	West Abutment to Pier 1 along Ramp B to 112'-3 1/2"
Roadway:	Varies face to face of parapets including 3'-0" Median Barrier on U.S. 30.
Loading:	H20-44, Case I and the Alternate Military Load.
Skew:	62°01'21" E Cleveland Avenue to a line tangent to U.S. 30 @ Sta. 971+29.46.
Wearing Surface:	2 1/2" Asphalt Concrete
Alignment:	From tangent to a 450' spiral to a 3°00'00" curve (Right)
Approach Slab:	AS-1-72 (30 and 25' long) (Modified)
1995 ADT:	Eastbound Lanes = 30,070; Westbound Lanes 18,953 (49,023)

MICROFILMED
APR 16 1985



RAMP B CURVE DATA		U.S. 30 SPIRAL DATA	
PI=Sta. 970+15.078	PI=Sta. 974+55.410	Δ = 38°57'51.6" Rt.	Q _s = 6°45'00"
Δ = 16°42'25.6" Rt.	Δ = 10°30'04.0" Rt.	Q _s = 450.00	L _s = 902.106'
D = 4°30'00"	D = 2°03'23.6"	Dc = 3°00'00"	Rc = 1909.859'
R = 1273.240'	R = 2785.993'	Lc = 848.811'	
T = 186.961'	T = 256.024'		
L = 371.269'	L = 510.614'		

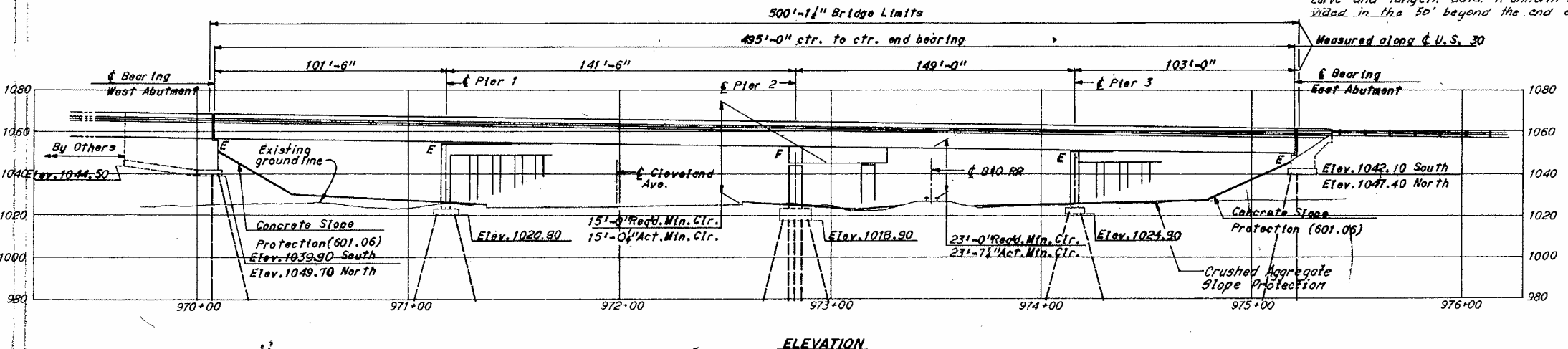
PROFILE GRADE - U.S. 30

PROFILE GRADE - RAMP B

The Vertical Curve and Tangent Grade Data does not apply to elevations on the bridge or in the transition areas. Bridge elevations are 1' above the elevations obtained from the curve and tangent data. A uniform transition shall be provided in the 50' beyond the end of the bridge.

UTILITY LEGEND

- G --- Gas
- W --- Water
- SS --- Storm Sewer
- San --- Sanitary Sewer
- T --- Telephone
- E --- Power Lines
- 4W-EAKW { 4 Wire, Primary 4 Kilo-volt
- 3W-Es { 3 Wire, Secondary
- Manhole
- Catch Basin
- ⊕ Pole, Telephone



ELEVATION

H.M.T. BRIDGE NO. 3 Revised 12-5-75 By EPS
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS KANSAS CITY CLEVELAND NEW YORK
SITE PLAN
 U.S. 30 AND RAMP B OVER CLEVELAND AVE. AND B. & O. RR.
 BR. NO. STA-30-1507 STA. 970+58.36
 SCALE: 1" = 30'-0" STA. 975+58.49
RELOCATED U.S. 30
 CANTON STARK COUNTY OHIO
 DRAWN BY: JKH TRACED: JKH CHECKED: JKH REVIEWED: JKH DATE: 5-22-70 DATE: 9-18-70 DATE: 11-24-71 DATE: 12-14-77
 1648

BRIDGE # 3 STA-30-1505