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20-4-2

OHIO DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

FHWA REGION	STATE	FEDERAL PROJECT
5	OHIO	

1/12

PLAN NO. 154

*D+R
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#08-92*

PART	COUNTY	ROUTE	SECTIONS	PROJECT TERMINI		NET LENGTH MILES	TOWNSHIP	CITY	VILLAGE
				BEGIN	END				
1	DEL	US-36	(9.04 - 10.74)	9.04	11.25	2.21		DELAWARE	
2	DEL	US-42	(8.63 - 8.88)	8.63	9.36	.73		DELAWARE	
3	DEL	US-42	(9.36)	9.36	9.56	.20			
4	DEL	SR745	(0.00 - 0.23)	0.00	0.38	.38		DUBLIN	
5	DEL	SR745	(0.38) (2.57)	0.38	7.03	4.93			
6	DEL	SR745	(0.38 - 1.51)	0.85	1.82	.97			SHAWNEE HILLS

The Standard 1991 Specifications of the State of Ohio, Department of Transportation, including changes and Supplemental Specifications listed in the plans and proposal shall govern these improvements.

I hereby approve these plans and declare that the making of these improvements will require the closing of the highways to traffic on Parts No. None and that detours will be provided by State forces. The closing to traffic of the highways will not be required on Parts No. 1-2-3-4-5-6 and provisions for the maintenance and safety of traffic will be as indicated in the proposal.

Approved Date 9/4/91

Michael C. Fly
District Deputy Director of Transportation

JEM Approved Date 9-23-91

B.D. Stanhilanis
Engineer of Bridges

Approved Date _____

Engineer of Maintenance

Approved Date 11-19-91

Alexander H. Hynds
DEPUTY DIRECTOR, OPERATIONS

Approved Date _____

Assistant Deputy Director, Program Development

Approved Date _____

Chief Engineer, Construction

Approved Date _____

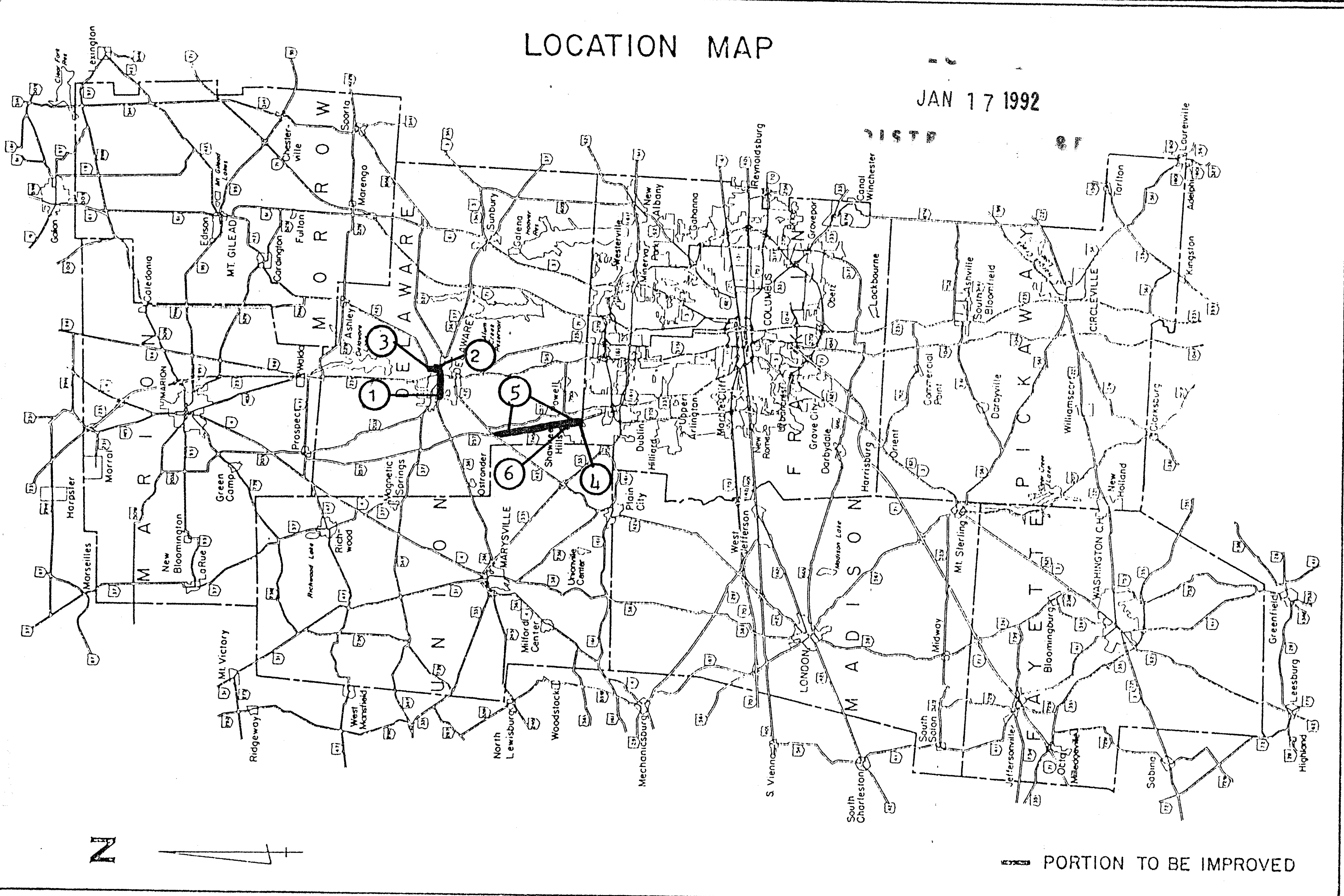
Chief Engineer, Design

Approved Date _____

Assistant Director, Department of Transportation

Approved Date 11-19-91

Jerry Wray
Director, Department of Transportation



DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:

DIVISION ADMINISTRATOR DATE

STANDARD DRAWINGS		SUPPLEMENTAL SPECIFICATIONS	
BP-5	10-01-87		
MT-97.10	04-29-88		
MT-97.11	10-04-89		
MT-99.10	11-14-86		
MT-99.20	04-29-88		

ASPHALT CONCRETE

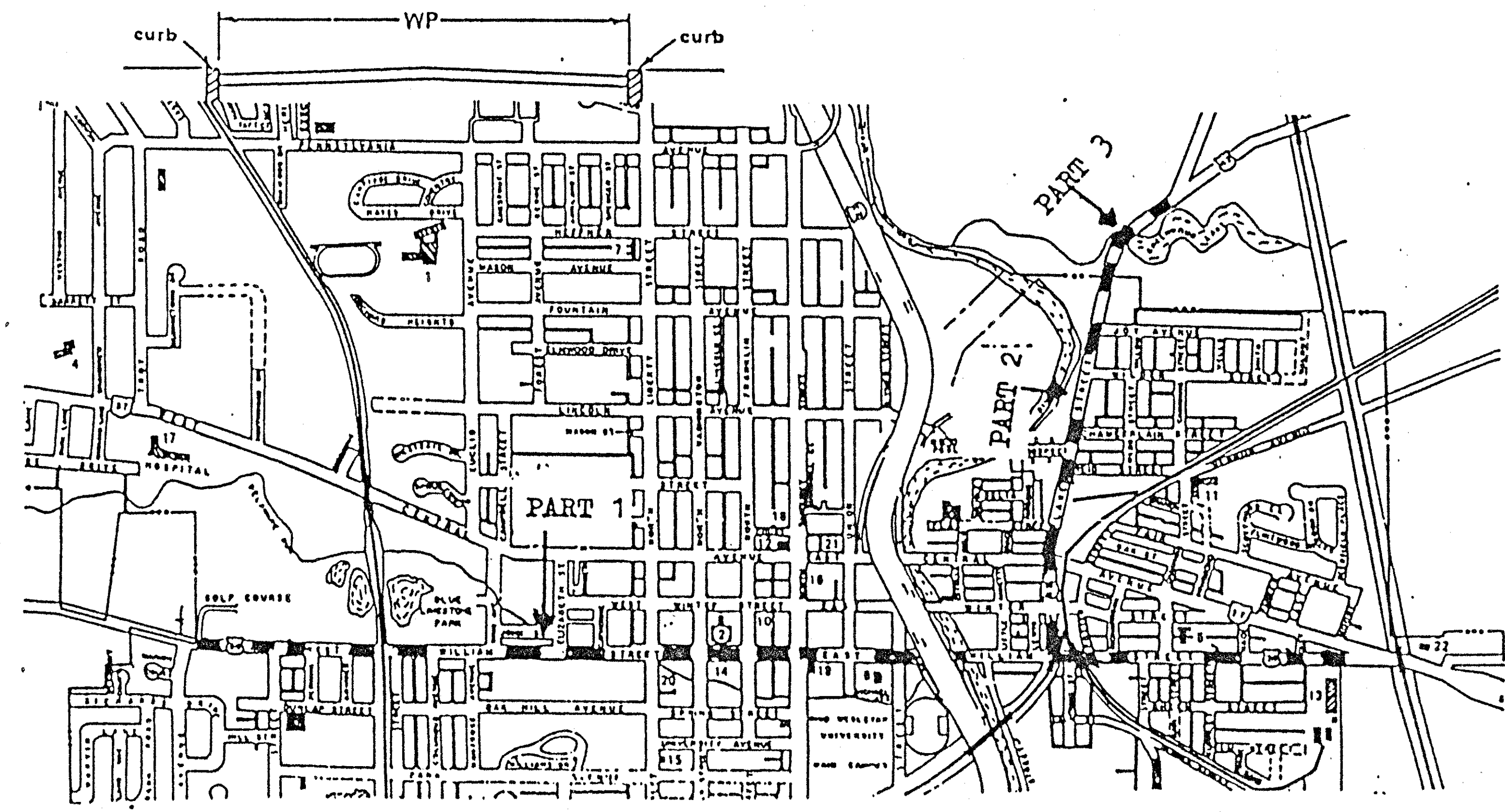
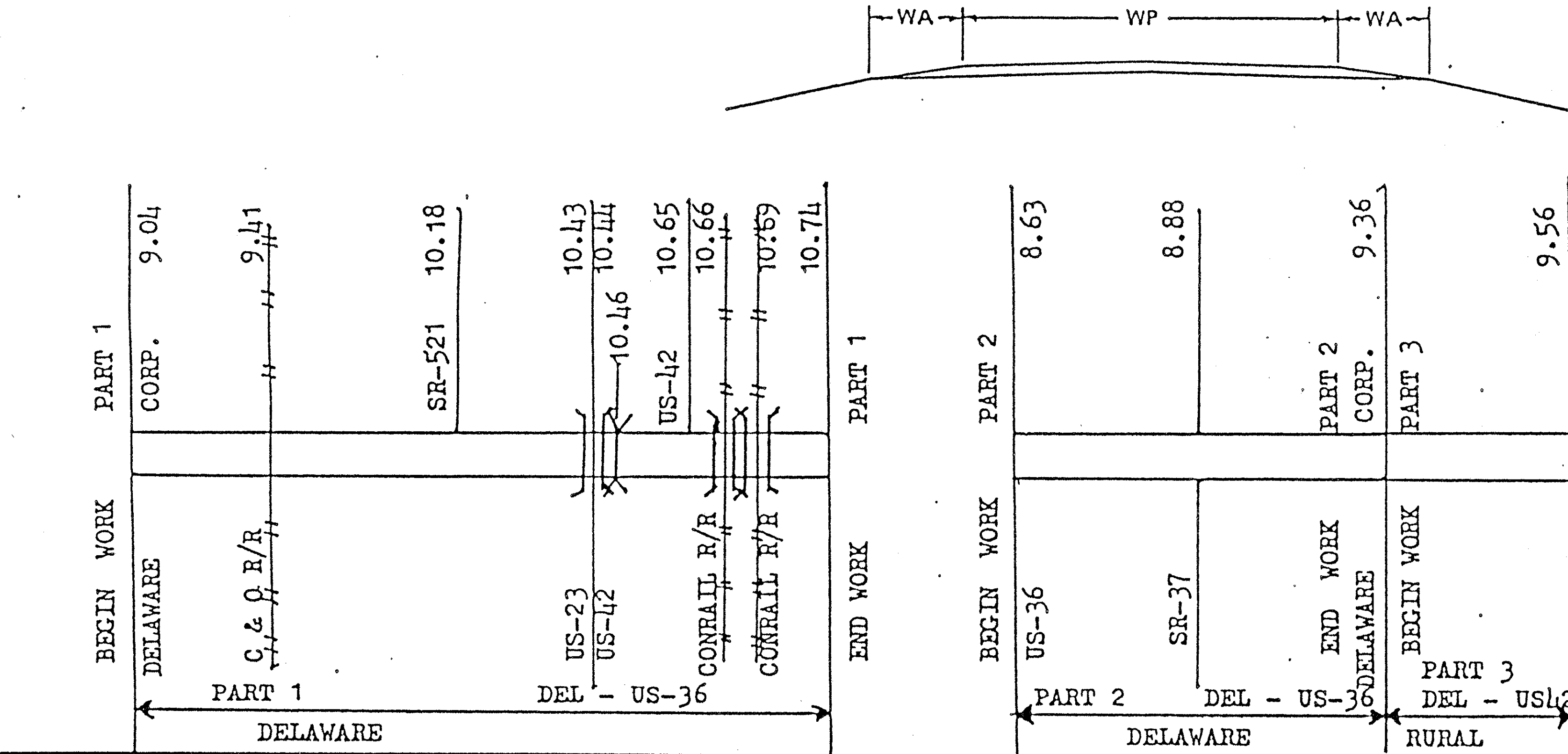
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- ① SEE NOTE ON SHEET 3
- ② SEE NOTE ON SHEET 4

TYPICAL 1

TYPICAL 2



PAVEMENT DATA

PART	ROUTE	LOG POINT TO LOG POINT	LENGTH		WP FEET	TYPICAL	EXISTING TYPE PAVEMENT	PAVEMENT AREA SQ. YDS.	PROPOSED PAVEMENT						RAISED PAVEMENT MARKER REMOVED FOR STORAGE	202 EACH	254 PAVEMENT PLANING BITUMIN. (2) SQ. YD.	614 TEMP. CENTER LINE CLASS II MILE	614 TEMP. LANE LINE CLASS II MILE	604 MANHOLE ADJUSTED TO GRADE EACH	814 VALVE BOX ADJUSTED TO GRADE EACH
			MILES	LIN. FT.					407		ASPHALT CONCRETE										
									TACK COAT @ .10... gal./s.y. GALS.	COVER AGGR. @ lbs./s.y. TONS	ITEM ... THICK INCHES	ITEM ... THICK INCHES	ITEM ... THICK INCHES	ITEM ... THICK INCHES							
1	US-36	9.04 - 9.41	.37	1954	30	2	404	6513	651				1 1/2	271		6513	.74				
		9.41 - 9.87	.46	2429	38	2	404	10256	1026				1 1/2	427		10256	.92				
		9.87 - 10.08	.21	1109	48	2	404	5915	592				1 1/2	246		5915	.42	.84			
		10.08 - 10.44	.36	1901	66	2	404	13941	1394				1 1/2	581		13941	.72	1.44			
		10.44 - 10.65	.21	1109	49	2	404	6038	604				1 1/2	252		6038	.42	.84			
		10.65 - 10.74 10.74 - 11.25	.09 .51	475 2693	30 30	2 2	404 404	1583 8977	158 898				1 1/2 3 1/4	66 187		1583 83	.18 .51			11	5
1	US-36	Extra Area From Sheet 5					6502	650				1 1/2	271		5242						
	TOTAL PART 1		2.23	11670			59725	5973					2301		49571	3.91	3.12	11	5		
2	US-42	8.63 - 8.72	.09	475	32	2	404	1689	169				1 1/2	70		1689	.18				
		8.72 - 8.88	.16	845	38	2	404	3568	357				1 1/2	149		3568	.32				
		8.88 - 9.36	.48	2534	34	2	404	9573	957				1 1/2	399		9573	.96				
2	US-42	Extra Area From Sheet 5					2540	254				1 1/2	106		2540						
	TOTAL PART 2		.73	3854			17370	1737					724		17370	1.46					
3	US-42	9.36 - 9.56	.20	1056	24	1	404	2816	282			39	1	78	14	156	.40				
3	US-42	Extra Area From Sheet 5					300	30				4		8							
	DOT 1920 TOTAL PART 3		.20	1056			3116	312				43		86	14	156	.40				

ASPHALT CONCRETE

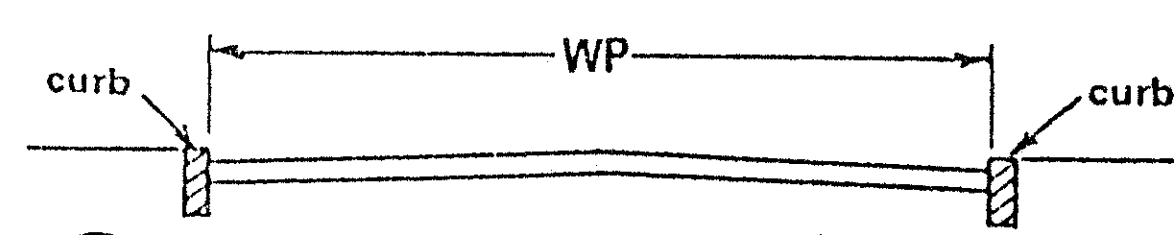
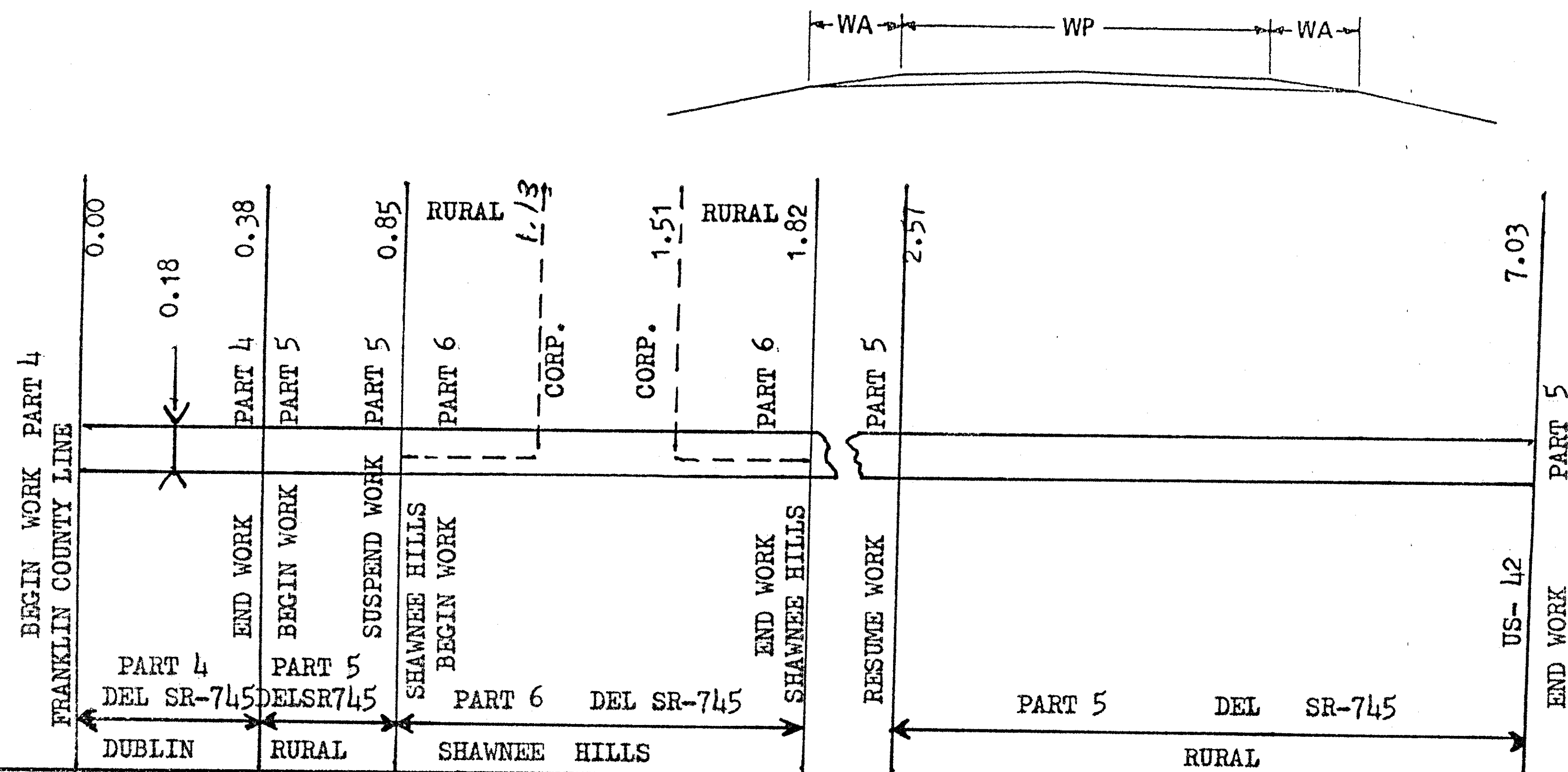
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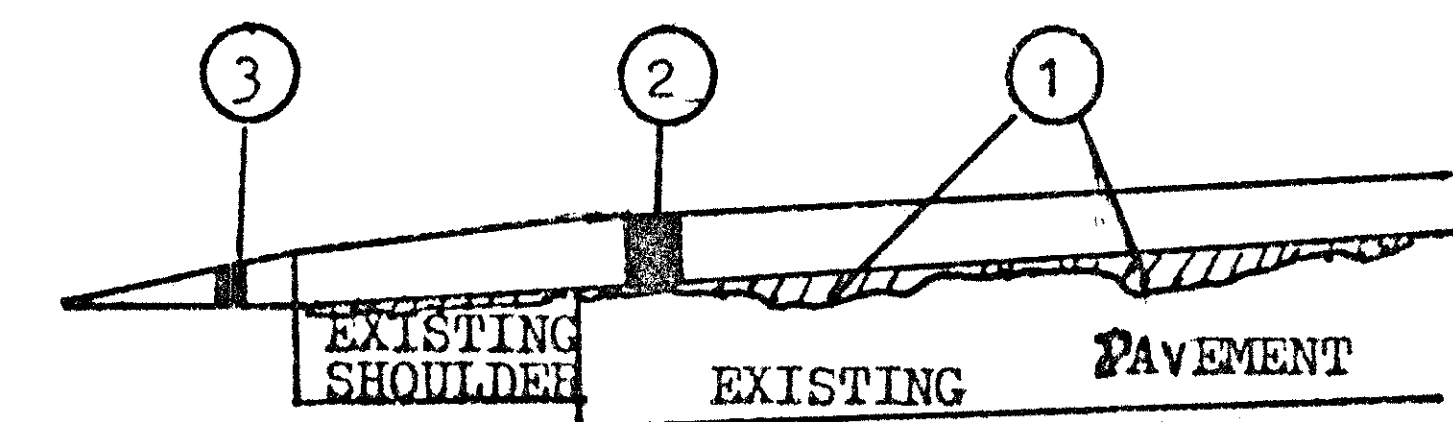
TYPICAL 1

TYPICAL 2

2 SEE NOTE ON SHEET 4



1 ITEM 403 - ASPHALT CONCRETE AC-20, SPOT LEVELING:
One-half inch ($\frac{1}{2}$ " has been used to compute the quantities for this item. This material is to be used, as directed by the Engineer, to fill ruts, and imperfections; and to level and re-establish the original alignment and profile of the pavement, prior to placement of the surface course.
This may result in a portion of the original surface still being exposed before the overlay is placed.



- 1 - 403 ASPHALT CONCRETE AC-20, SPOT LEVELING COURSE OF MIN
- 2 - 404 ASPHALT CONCRETE AC-20, SURFACE COURSE
- 3 - 617 AGGREGATE BACKUP

PAVEMENT DATA

PART	ROUTE	LOG POINT TO LOG POINT	LENGTH		WP FEET	TYPICAL	EXISTING TYPE PAVEMENT	PAVEMENT AREA SQ. YDS.	PROPOSED PAVEMENT						RAISED PAVEMENT MARKER REMOVED FOR STORAGE	PAVEMENT PLANING BLENDED	TEMP CENTER LINE CLASS II	WORK ZONE MARKING SIGN
			MILES	LIN. FT.					407		ASPHALT CONCRETE							
									TACK COAT @ .10 gal./s.y. GALS.	COVER AGGR. @ lbs./s.y. TONS	ITEM 403		ITEM 404					
											THICK INCHES	CU. YDS.	THICK INCHES	CU. YDS.				
4	SR-745	0.00 - 0.08	.08	422	32	1	404	1500	150									
		0.08 - 0.17	.09	475	35	1	404	1847	185									
		0.17 - 0.27	.10	528	40	1	404	2347	235									
		0.27 - 0.38	.11	581	20	1	404	1291	129									
4	SR-745	Extra Area From Sheet 5						366	37									
		TOTAL PART 4	.38	2006				7351	736									
5	SR-745	0.38 - 0.85	.47	2482	20	1		5516	552									
		2.57 - 7.03	4.46	23549	20	1		52331	5233									
5	SR-745	Extra Area From Sheet 5						2476	248									
		TOTAL PART 5	4.93	26031				60323	6033									
6	SR-745	0.85 - 1.13	.28	1478	10	1		1642	164									
		1.51 - 1.82	.31	1637	10	1		1819	182									
6	SR-745	Extra Area From Sheet 5						460	46									
		TOTAL PART 6 -RURAL	(.59)	(3115)				(3921)	(392)									

ASPHALT CONCRETE

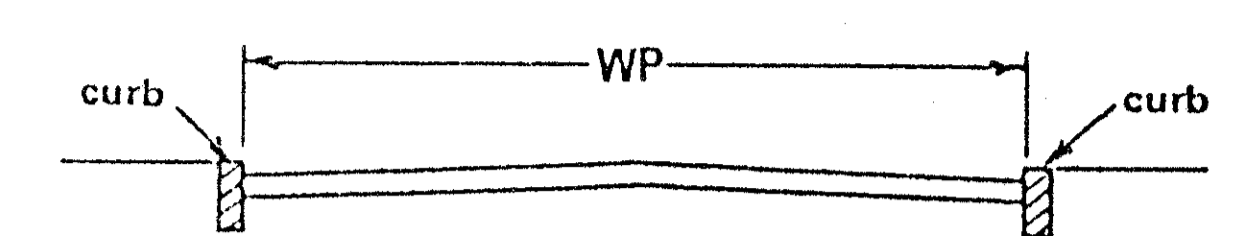
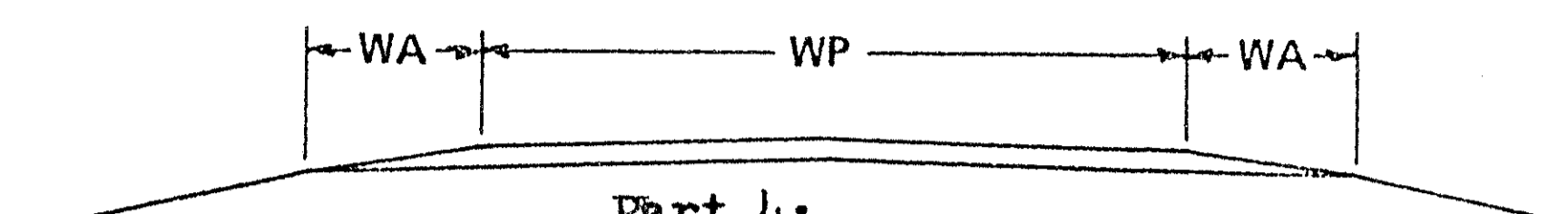
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TYPICAL 1

TYPICAL 2

① SEE NOTE ON SHEET 3



ITEM 254 - PAVEMENT PLANING BITUMINOUS:

The existing wearing course shall be removed to a depth equal to the depth of the proposed new pavement (1 1/2"). Or to the concrete pavement if the depth is less than 1 1/2". It is not the intent of this plan to adjust the castings in the pavement planing areas in Parts 1 and 2. The depth of planing close to the castings shall be as directed, to achieve a smooth riding finished pavement. The Contractor shall be totally (100%) responsible for any and all damage that may result from the planing operation; including castings (Manhole, Catch Basin, Inlet, etc.).

- Part 4:**
S.L.M. 0.00 : 32' x 50' ÷ 9 = 178 Sq.Yd.
- Part 5:**
S.L.M. 2.57 : 23' x 50' ÷ 9 = 128 Sq.Yd.
S.L.M. 7.03 : 22' x 50' ÷ 9 = 122 Sq.Yd.
- Part 6:**
S.L.M. 1.82 : 25' x 50' ÷ 9 = 140 Sq.Yd.

ITEM 254 - PATCHING PLANED SURFACE:

The following estimated quantity is carried to the General Summary to be used as directed by the Engineer.

The Planed Area shall not be exposed to traffic for more than five (5) days prior to resurfacing.

- Part 1:**
S.L.M. 9.04 - 10.74 : Full Width - As Noted
S.L.M. 11.25 : 30' x 25' ÷ 9 = 83 Sq.Yd.
- Part 2:**
S.L.M. 8.63 - 9.36 : Full Width - As Noted
- Part 3:**
S.L.M. 9.56 : 28' x 50' ÷ 9 = 156 Sq.Yd.

- Part 1:**
500 Sq.Yd.
- Part 2:**
150 Sq.Yd.

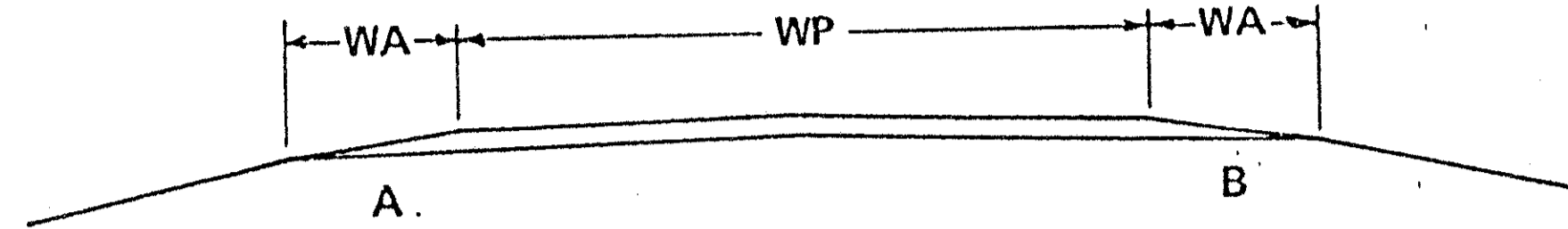
PAVEMENT DATA

PART	ROUTE	LOG POINT TO LOG POINT	LENGTH		WP FEET	TYPICAL	EXISTING TYPE PAVEMENT	PAVEMENT AREA SQ. YDS.	PROPOSED PAVEMENT						202 RAISED PAVEMENT MARKER REMOVED FOR STORAGE EACH	254 PAVEMENT PLANING BITUMINOUS ② SQ. YD.	614 TEMP. CENTER LINE CLASS II MILE	614 WORK ZONE MARKING SIGN EACH
			MILES	LIN. FT.					407		ASPHALT CONCRETE							
									TACK COAT @ . . . 10 . . . GAL./S.Y.	COVER AGGR. @ LBS./S.Y.	ITEM THICK INCHES	ITEM 403 THICK INCHES ①	ITEM 404 THICK INCHES	ITEM 404 THICK INCHES				
			CU. YDS.	MIN.					CU. YDS.	CU. YDS.	CU. YDS.							
		VILLAGE																
6	SR-745	0.85 - 1.13	.28	1478	10	1	404	1642	164			23	1	46			.28	
		1.13 - 1.51	.38	2006	20	1	404	4458	446			62	1	124			.76	
		1.51 - 1.82	.31	1637	10	1	404	1819	182			25	1	51		70	.31	
6	SR-745 Extra Area From Sheet 5							2562	256			36	1	71				
		TOTAL PART 6 - VILLAGE	(.97)	(5121)				(10481)	(1048)			(146)		(292)		(70)	(1.35)	
		TOTAL PART 6	.97	5121				14402	1440			200		402		140	1.94	2

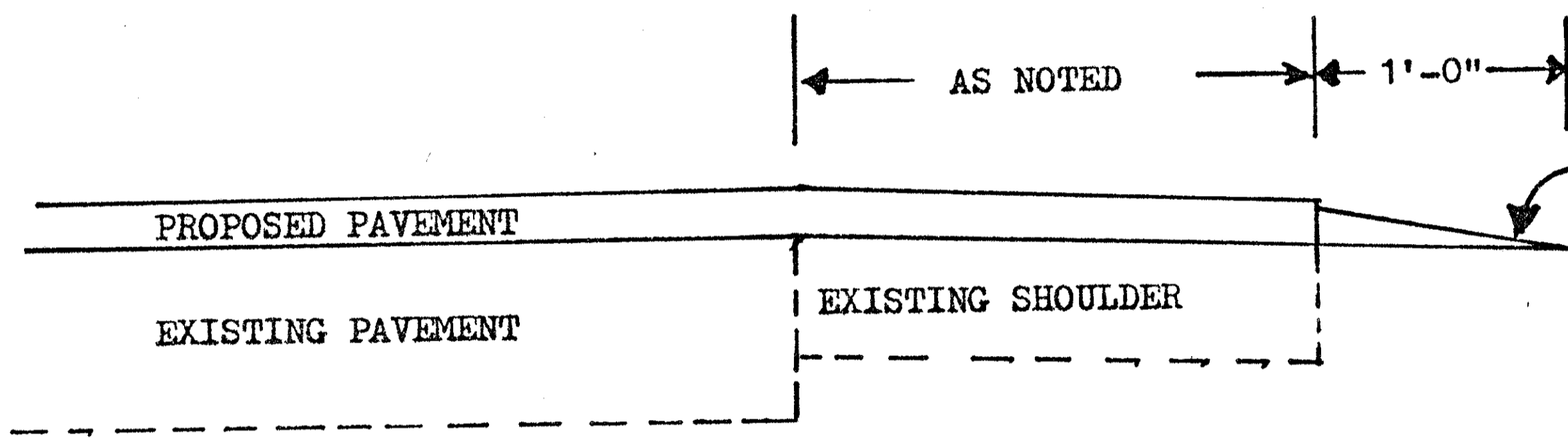
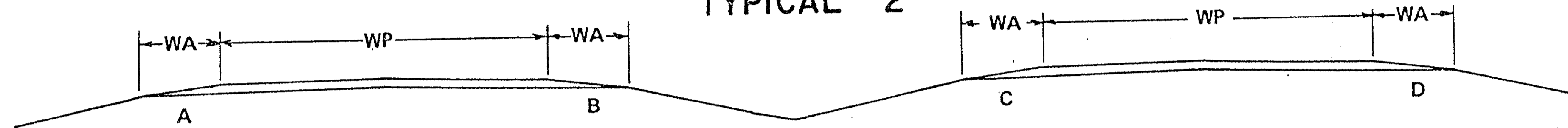
SHOULDER TREATMENT

1 SEE NOTE ON SHEET 3

TYPICAL 1



TYPICAL 2

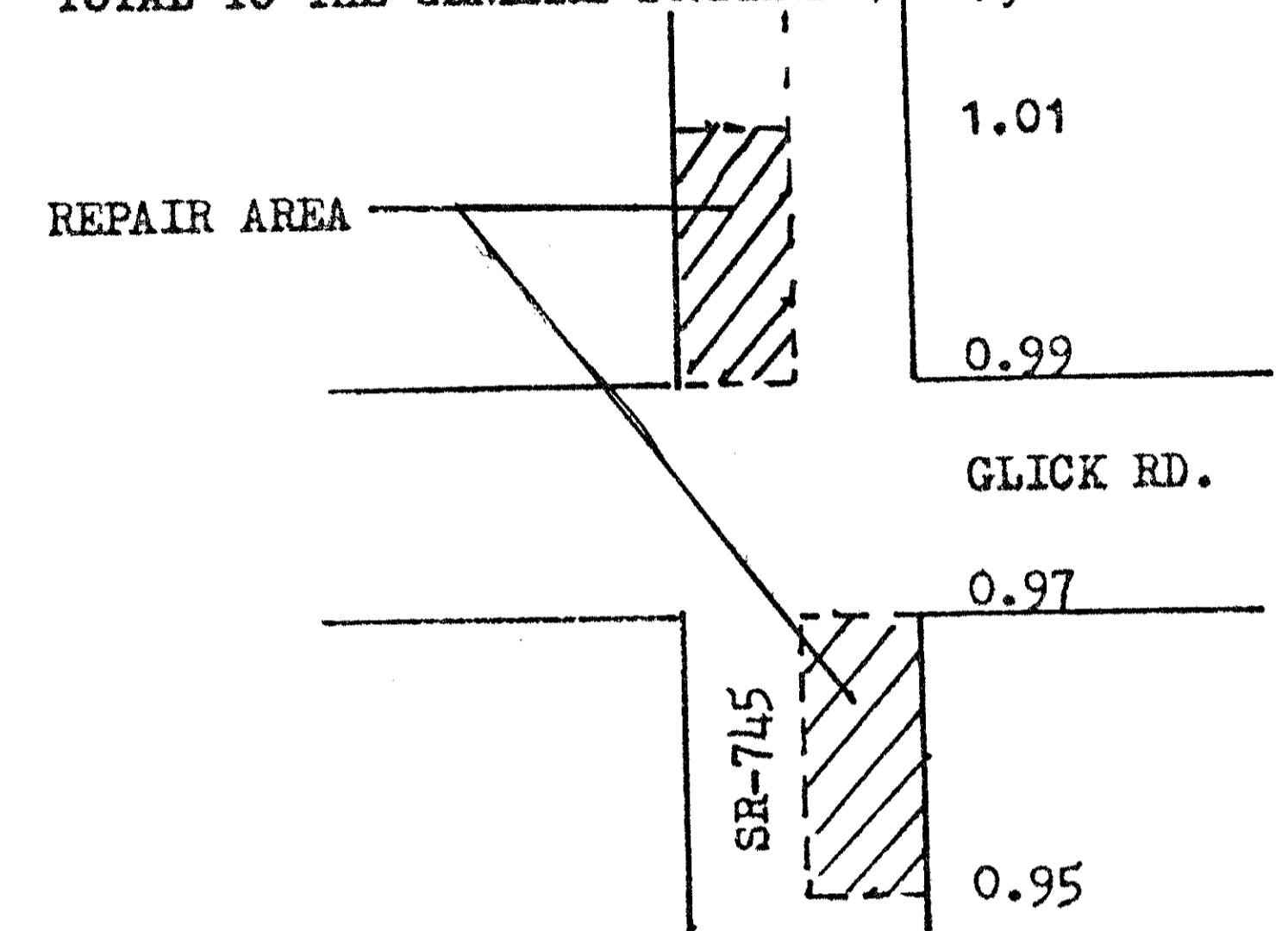


* ITEM 617 - COMPACTED AGGREGATE, TYPE A:
A thickness of 3" has been used to calculate the quantities of this material, to allow for low berms etc.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR:

The use of this material shall be where and as directed by the Engineer; and the depth of the repairs shall be approximately 3". The quantities shown in the plan are for estimating purposes only. Payment for Item 251 - Partial Depth Pavement Repair shall be for the actual quantity of the item used, as measured in the field by the Engineer.

SR-745 - 0.95 to 0.97 - Right - $125' \times 12' \div 9 = 167 \text{ Sq. Yd.}$
 SR-745 - 0.99 to 1.01 - Left - $125' \times 13' \div 9 = 181 \text{ Sq. Yd.}$
 Additional to be used as directed = 152 Sq. Yd.
TOTAL TO THE GENERAL SUMMARY (PART 6) = **500 Sq. Yd.**



SHOULDER DATA

PART	ROUTE	LOG POINT TO LOG POINT	LENGTH		TYPICAL	EXISTING TYPE - WIDTH (ft.)								AREA SQ. YDS.	407 TACK		ASPHALT CONCRETE			409 SEAL		617		NOTES	
			MILES	LIN. FT.		A		B		C		D			Bit. Matl. @ .10 gal./s.y. Gals.	Cover Aggr. @ . lbs./s.y. Tons	403		404		Bit. Matl. @ gal./s.y. Gals.	Aggr. @ c.y./s.y. Cu. Yds.	Sq. Yds.		Compacted Aggregate TYPE A AVG. THICK INCHES = 3" Cu. Yds.
						TYPE	WIDTH	TYPE	WIDTH	TYPE	WIDTH	TYPE	WIDTH				AVG THICK INCHES	①	AVG THICK INCHES						
			0"	MIN.		Cu. Yds.	Cu. Yds.																		
6	SR-745	RURAL	.12	634	1	404	1 1/2								1	3	1			70	6				
		0.85 - 0.97			1	404	2 1/2									3	7	1			94	8			
6	SR-745		.31	1637	1	404	2 1/2							6	13	1			182	15					
		1.51 - 1.82														(10)	(23)				(346)	(29)			
	Total	Part 6 - Rural	(.59)	(3116)																					
6	SR-745	VILLAGE	.12	634	1		404	1 1/2							1	3	1			70	6				
		0.85 - 0.97			1		404	2 1/2								3	7	1			94	8			
			.38	2006	1	404	2 1/2	404	2 1/2					15	31	1			446	37					
		1.13 - 1.51			1		404	2 1/2							6	13	1			182	15				
6	SR-745		.31	1637	1		404	2 1/2						6	13	1			182	15					
		1.51 - 1.82														(25)	(54)				(792)	(66)			
	Total	Part 6 - Village	(.97)	(5122)																					
	TOTAL	PART 6	.97	5122										35	77				1138	95					

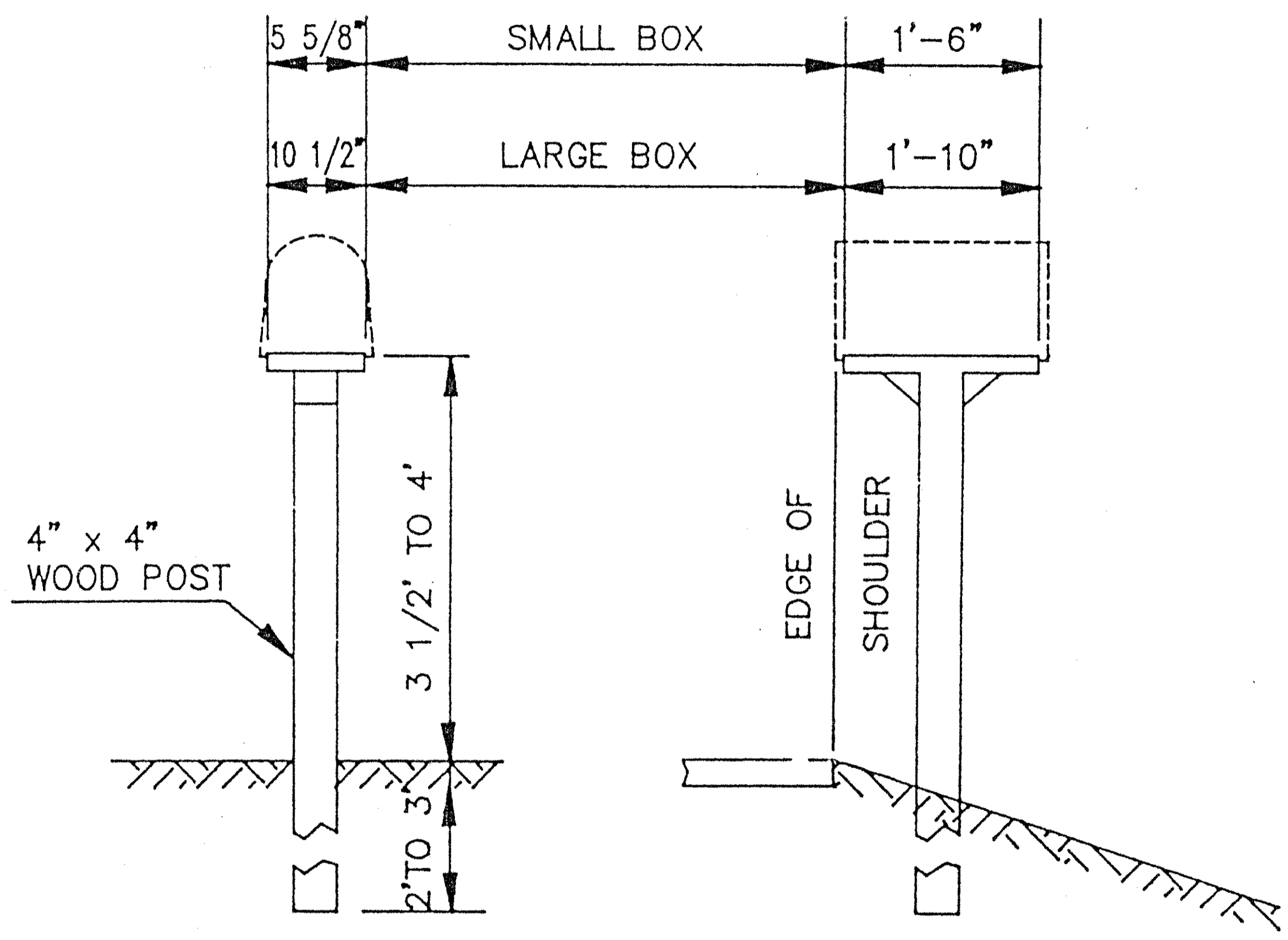
ITEM SPECIAL – MAILBOX SUPPORTS

PLAN NO.
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MAILBOX SUPPORT DATA						
PART	SLM	SIDE	ADDRESS	NAME	SIZE	DESCRIPTION
5	2.51	R	8975		2'x2'-8"	Brick
5	2.55	R	8945			On Tree Stump
5	2.91	L			6" x 6"	Wood - Square
5	3.16	L	8415		6" x 6"	Wood - Square
5	3.62	R			4"	Metal - Round
5	4.39	L	7433	Scott	8"	Wood - Round
5	4.41	L	7415	Dozer	6"	Wood - Round
5	4.44	L	7403		8"	Wood - Round
5	4.89	R	7021		8" x 16"	Concrete Block
5	5.30	L	6680		6" x 6"	Wood - Square
5	5.40	L	6600		6" x 6"	Wood - Square
5	5.44	R	6535		6"	Wood - Round
5	5.47	L	6525		6"	Wood - Round
5	5.60	L	6400		5" 5"	Wood - Round Wood - Square
5	6.09	L	5979		5"	Wood - Square
5	6.41	L	5677		6" x 6"	Wood - Square

THIS ITEM SHALL CONSIST OF REPLACING AND RESETTING DESIGNATED MAILBOX SUPPORTS WITH PRESSURE TREATED FOUR INCH (NOMINAL) TIMBER POSTS MEETING AASHTO M 133-81 AWPA P 8. MAILBOX SUPPORTS SHALL BE CONSTRUCTED AS PER THE DRAWING ON THIS SHEET. ALL MATERIAL, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE REMOVAL AND INSTALLATION OF THE EXISTING MAILBOX ON THE NEW TIMBER POSTS AND RESETTING THE MAILBOX POSTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM SPECIAL – MAILBOX SUPPORTS. THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE PURPOSE.

ITEM SPECIAL – MAILBOX SUPPORT PART 5
17 Each



TYPICAL MAILBOX LOCATION AND MOUNTING HEIGHT

PAVEMENT MARKING SUB-SUMMARY

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PART	ROUTE	S.L.M.	FROM	S.L.M.	TO	612 CENTER LINE MILES			PARTICIPATION	REMARKS
						TOTAL	DASHED	SOLID		
3	DEL-US-42	9.36		9.56		.20		.40		
5	DEL-SR-745	0.38		0.85		.47	.47	.31		
6	DEL-SR-745	0.85		1.82		.97	.13	1.81		
5	DEL-SR-745	2.57		7.03		4.46	1.61	6.83		
CENTER LINE TOTAL						6.10	2.21	9.35		

PART	ROUTE	S.L.M.	FROM	S.L.M.	TO	612 LANE LINE MILES			PARTICIPATION	REMARKS
						TOTAL	DASHED	SOLID		
LANE LINE TOTAL										

PART	ROUTE	S.L.M.	FROM	S.L.M.	TO	612 EDGE LINE MILES				PART	LANE WIDTH	NO. OF LANES	TOTAL PAVE. WIDTH
						TOTAL MILES	WHITE HIGHWAY MILES	WHITE RAMP MILES	YELLOW				
3	DEL-US-42	9.36		9.56		0.40	0.40				12	2	24
5	DEL-SR-745	0.38		0.85		0.94	0.94				10	2	20
6	DEL-SR-745	0.85		1.82		1.94	1.94				10	2	20
5	DEL-SR-745	2.57		7.03		8.92	8.92				10	2	20
EDGE LINE TOTAL						12.20	12.20						

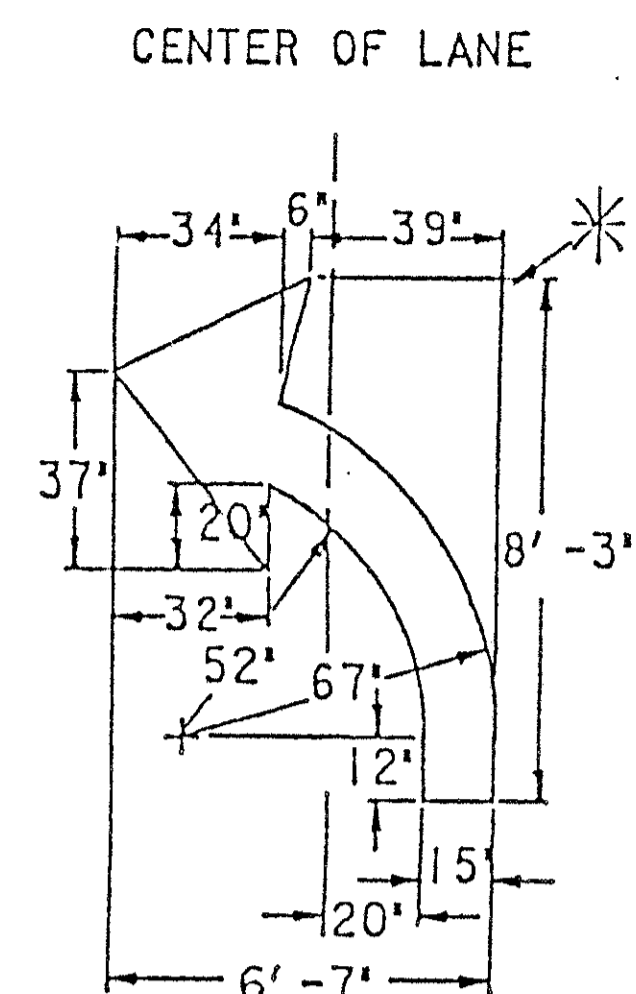
PART	ROUTE	S. L. M.		TRANSVERSE LINES		STOP LINE	CROSS-WALK LINE	WORD ON PAVEMENT			LANE ARROWS				RAILROAD SYMBOL	DOTTED LINE		REMARKS
		FROM	TO	WHITE	YELLOW			ONLY	SCHOOL	TURN LEFT	TURN RIGHT	THRU	COMB.	WHITE		YELLOW		
		LIN. FT.	LIN. FT.	LIN. FT.	LIN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	LIN. FT.	LIN. FT.					
6	DEL-SR-745	.89																
6	DEL-SR-745	.90																
5	DEL-SR-745	.97	Right			11												
5	DEL-SR-745	.98	Left			11												
5	DEL-SR-745	3.97	Right			11												
5	DEL-SR-745	3.99	Left			11												
5	DEL-SR-745	7.03	Right			11												
AUXILIARY MARKING TOTALS						55					2							

WORD AND SYMBOL MARKING DETAILS

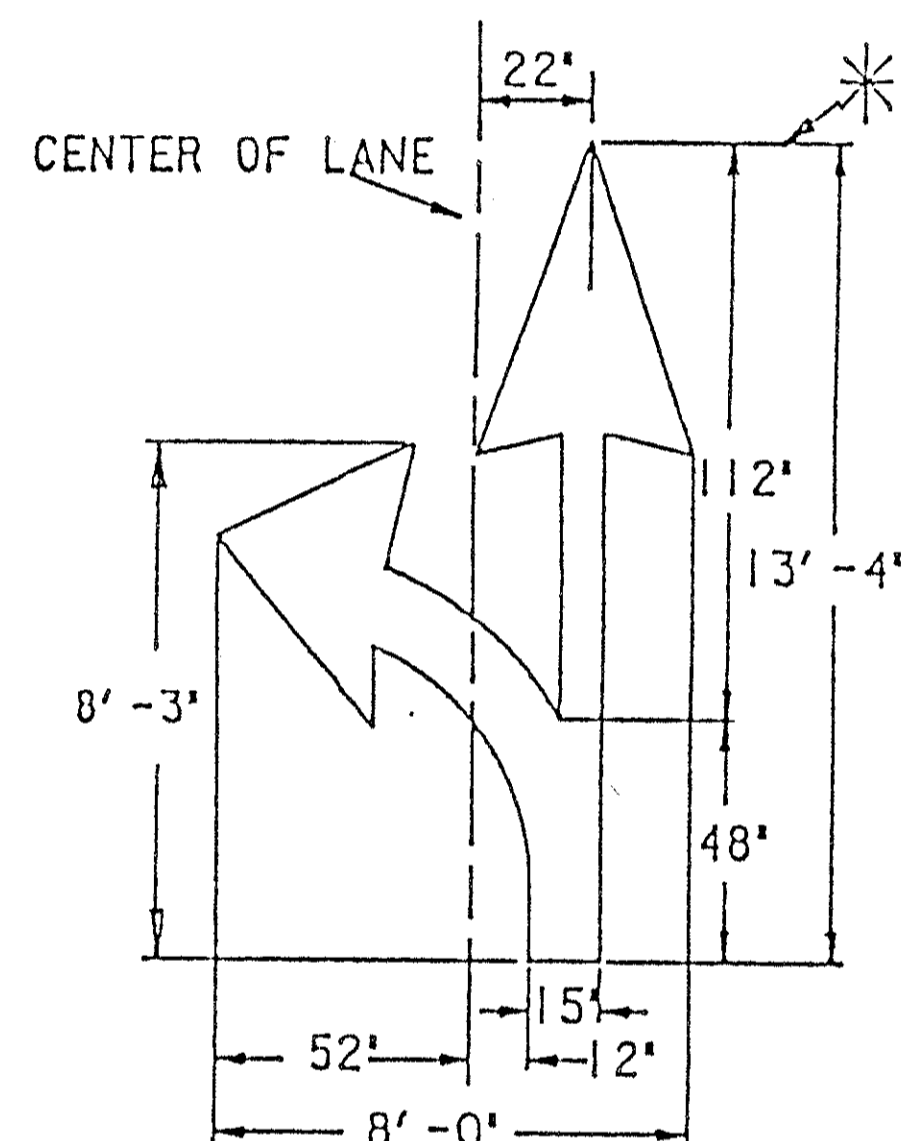
FED RD DIVISION	STATE	PROJECT
5	OHIO	

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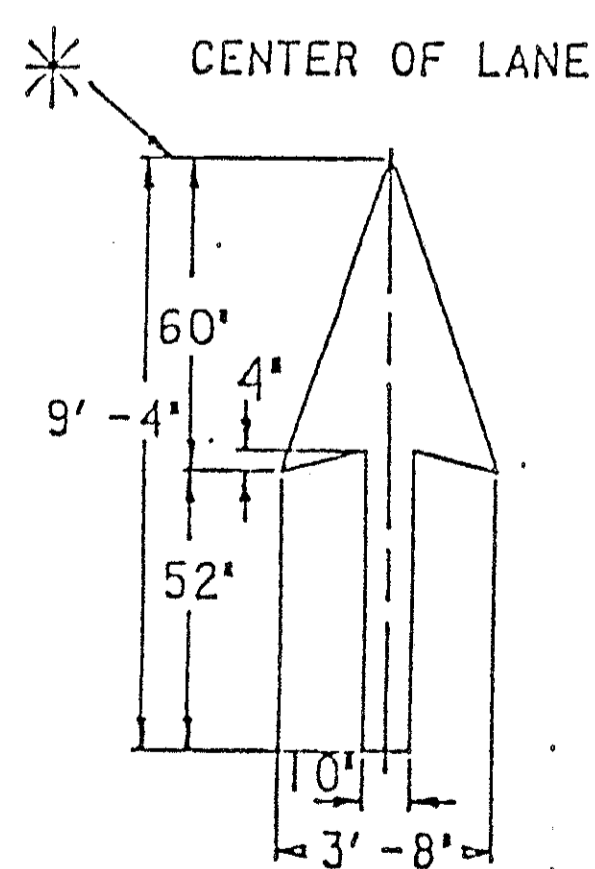
PLAN NO. 154



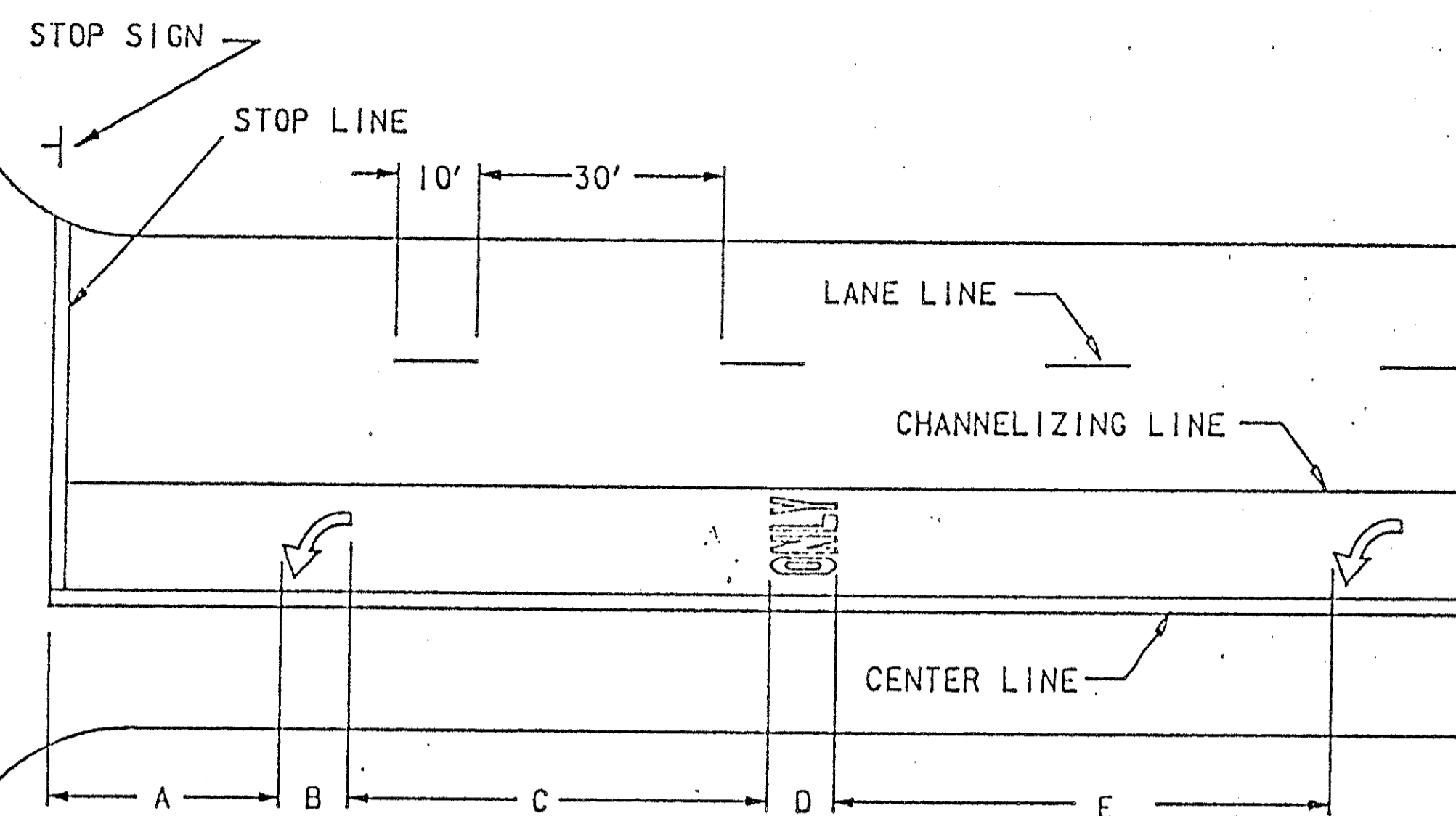
TURN ARROW
(LEFT ARROW SHOWN
RIGHT ARROW OPPOSITE)



COMBINED ARROW
THREE HEAD DIRECTIONAL ARROWS
CAN BE ACHIEVED BY THE COMBINATION
OF TURN ARROWS.



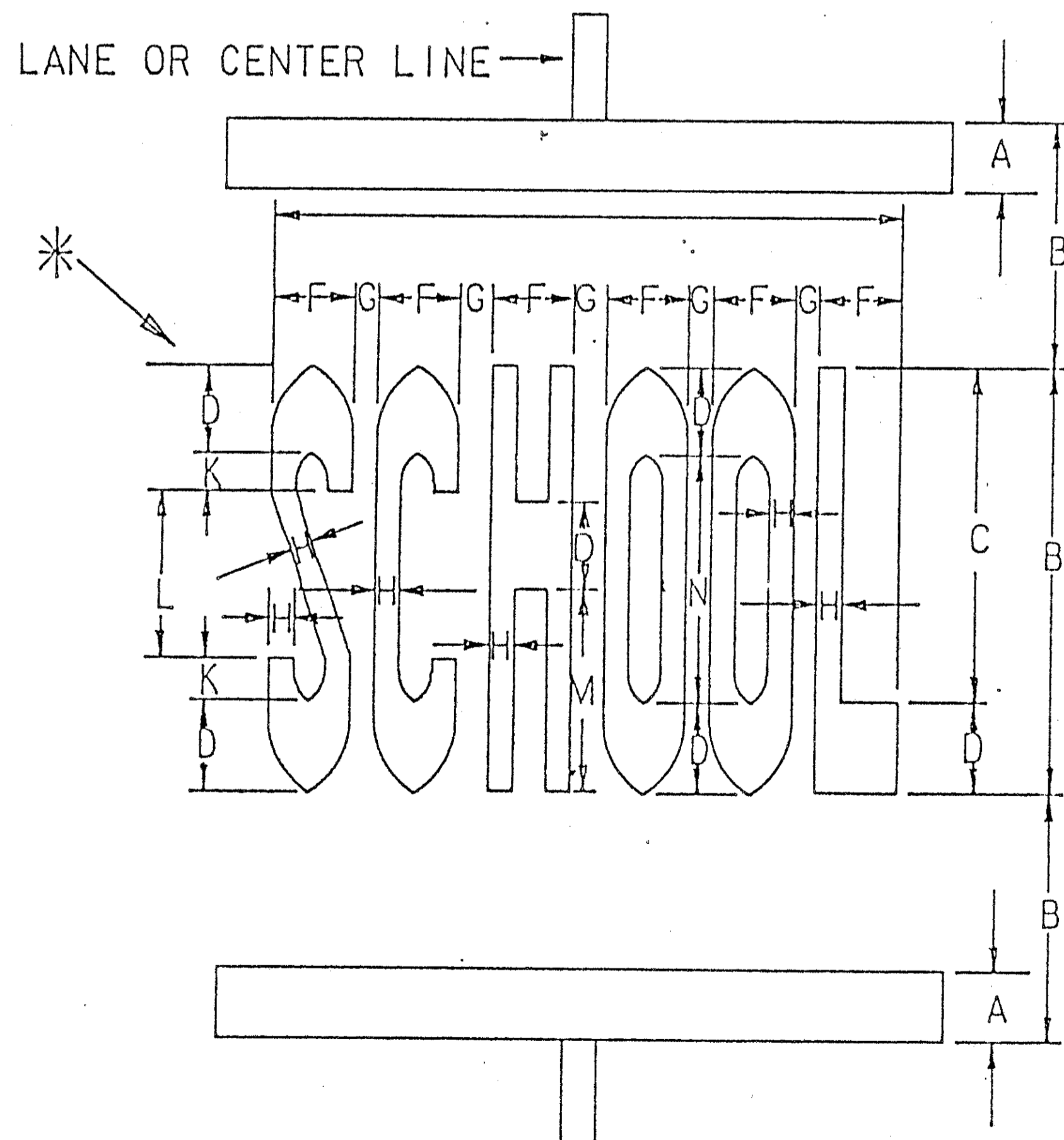
THROUGH ARROW



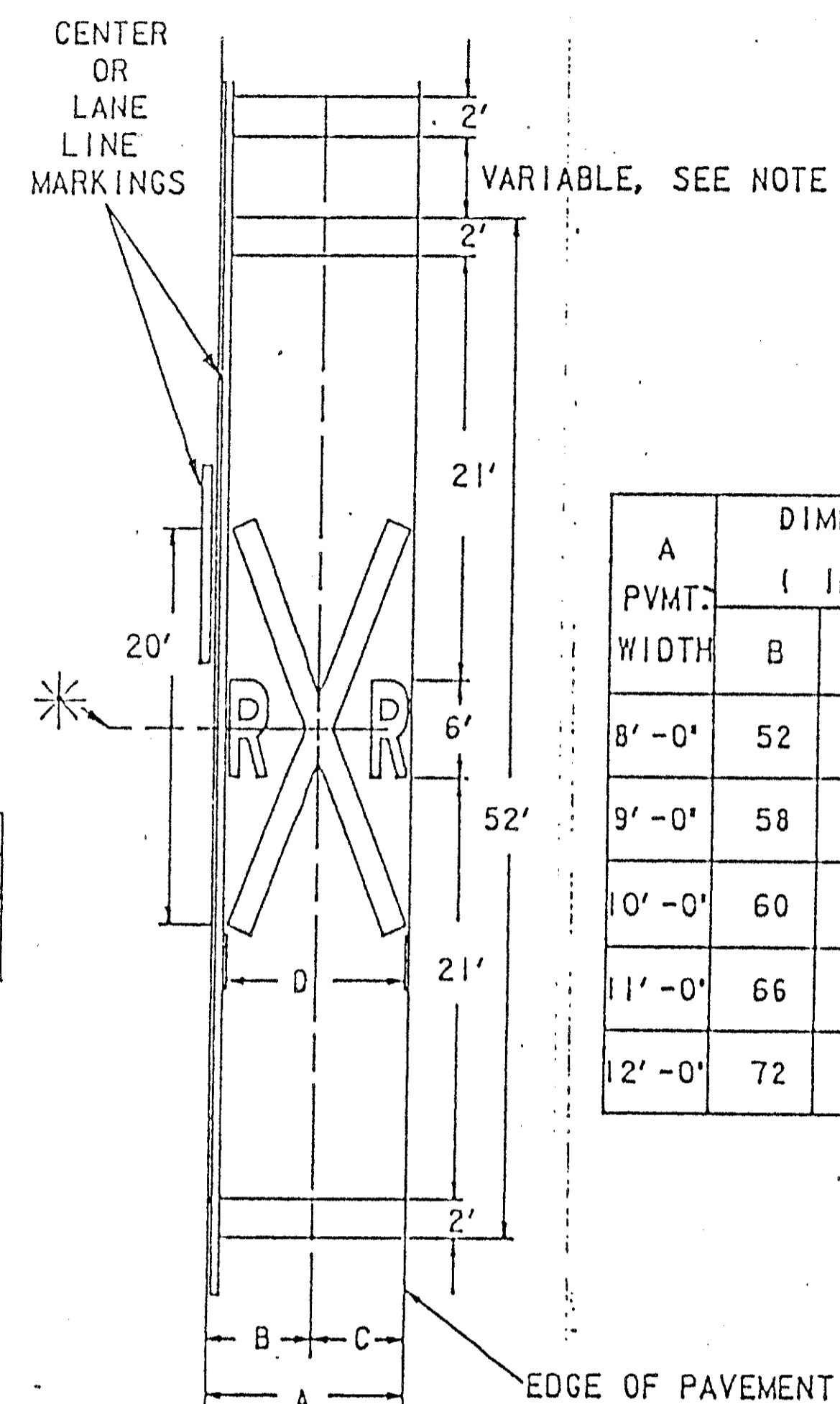
NOTE:
STOP BAR LOCATED MIN. 40' FROM
AT LEAST ONE SIGNAL HEAD WHICH APPLIES
TO THAT APPROACH

TYPE	DIMENSIONS (FEET)				
	A	B	C	D	E
RURAL	30 MIN.	8.25	32-80	8	32-60
URBAN	10 MIN.	8.25	32-80	6	24-60

TYPE	DIMENSIONS (INCHES)												
	A	B	C	D	E	F	G	H	J	K	L	M	N
RURAL	16.96	76	20	150	20	6	6	8	9	38	45	56	
URBAN	16.72	57	15	148	18	8	4.50	9	6.75	28.50	33.75	42	



TYPE	DIMENSIONS (INCHES)												
	A	B	C	D	E	F	G	H	J	K	L	M	
RURAL	96	94	20	6	2	6	20	56	45	76	32	36	
URBAN	72	90	18	8	2	4.50	15	42	33.75	57	24	27	



A PVMT WIDTH	DIMENSIONS (INCHES)		
	B	C	D
8'-0"	52	44	84
9'-0"	58	50	96
10'-0"	60	60	96
11'-0"	66	66	96
12'-0"	72	72	120

NOTES

- PAVEMENT MARKING FOR WORDS, SYMBOLS, ARROWS AND TRANSVERSE LINES FOR WORD 'SCHOOL' OR THE RAILROAD SYMBOL SHALL BE WHITE REFLECTIVE MATERIAL.
- TWO TRANSVERSE LINES SHALL BE INCLUDED IN THE PAYMENT FOR EACH WORD 'SCHOOL'. THREE TRANSVERSE LINES SHALL BE INCLUDED IN THE PAYMENT FOR EACH RAILROAD SYMBOL.
- ON MULTI-LANE APPROACHES, THE TRANSVERSE LINES USED WITH THE RAILROAD SYMBOLS SHALL EXTEND ACROSS ALL APPROACH LANES AND SYMBOLS SHALL BE PLACED IN EACH APPROACH LANE.
- FOR THE RAILROAD SYMBOL, NO PORTION OF ONE TRANSVERSE LINE SHALL BE CLOSER THAN 30 FEET FROM THE NEAREST RAILROAD TRACK RAIL AND THE LINE MAY BE EITHER PARALLEL TO THE TRACK OR PERPENDICULAR TO THE CENTERLINE OF THE ROADWAY. THE OTHER TWO TRANSVERSE LINES AND THE RAILROAD SYMBOL SHALL BE LOCATED FROM THE STATION REFERENCE POINT SHOWN IN THE PLANS.
- ON MULTI-LANE APPROACHES, THE TRANSVERSE LINES USED WITH THE WORD 'SCHOOL' SHALL EXTEND ACROSS ALL APPROACH LANES WITH A SINGLE WORD 'SCHOOL' CENTERED ACROSS THE APPROACH LANES. ON TWO LANE ROADWAYS, THE TRANSVERSE LINES SHALL EXTEND ACROSS THE ROADWAY WITH THE WORD 'SCHOOL' CENTERED ACROSS THE ROADWAY. CENTER OR LANE LINES SHALL NOT PASS THROUGH THE 'SCHOOL' MARKING.
- THE STOP LINE SHOULD BE PLACED WHERE CROSS-CORNER VISION IS MAXIMUM, IN NO CASE MORE THAN 30 FEET OR LESS THAN 4 FEET FROM NEAREST EDGE OF THE INTERSECTING ROADWAY. FOR NORMAL INTERSECTIONS A MAXIMUM DISTANCE OF 10 FEET IS RECOMMENDED.
- IF A MARKED CROSSWALK IS PRESENT THE STOP LINE SHOULD BE PLACED 4 FEET IN ADVANCE OF AND PARALLEL TO THE NEAREST CROSSWALK.
- ANY OF THE FOLLOWING STANDARDS FOR LETTER (EXCEPT 'R'), NUMERAL OR SYMBOL DIMENSIONING MAY BE USED:

- STANDARD DIMENSIONS SHOWN ON THIS DETAIL (NOMINAL)
- STANDARD DIMENSIONS IN ACCORDANCE WITH THE 1977 METRIC EDITION STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING WITH ERRATA.
- STANDARD DIMENSIONS CONFORMING TO REQUIREMENTS OF SECTION 3B-17 OR AS SHOWN IN FIGURES 3-17, 3-18, 7-2, 7-3, 8-2 OR 9-6 OF THE 1978 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

** THE LETTER 'R' DIMENSIONS SHALL BE AS SHOWN ON THIS SHEET AND IN THE NMUTCD SECTION 8-2.

* INDICATES STATION REFERENCE POINT

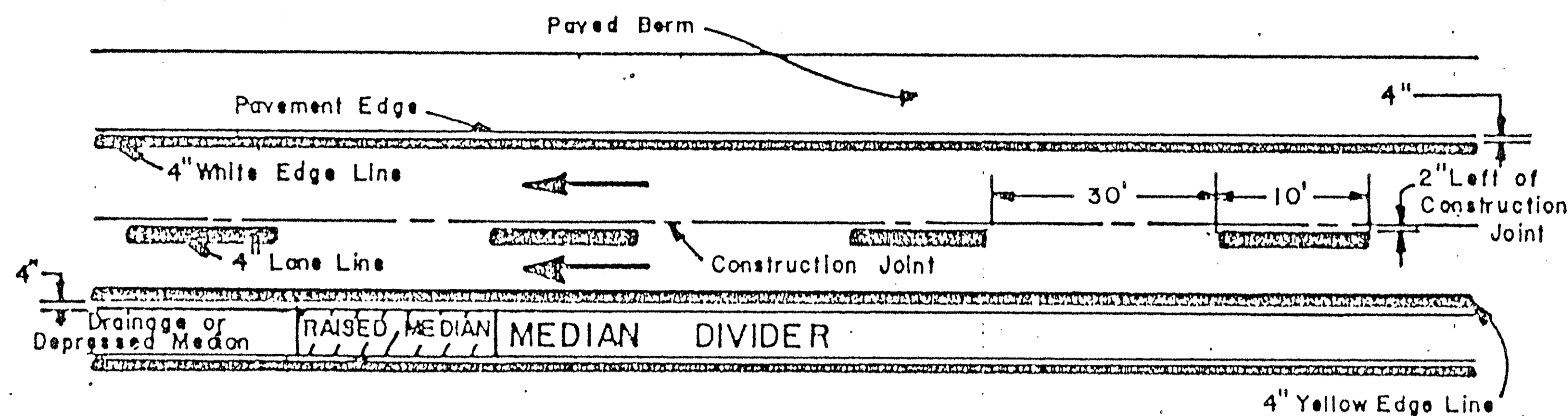
PAVEMENT MARKING TYPICAL DETAILS

FED. RD. DIV.	STATE	PROJECT	
5	OHIO		

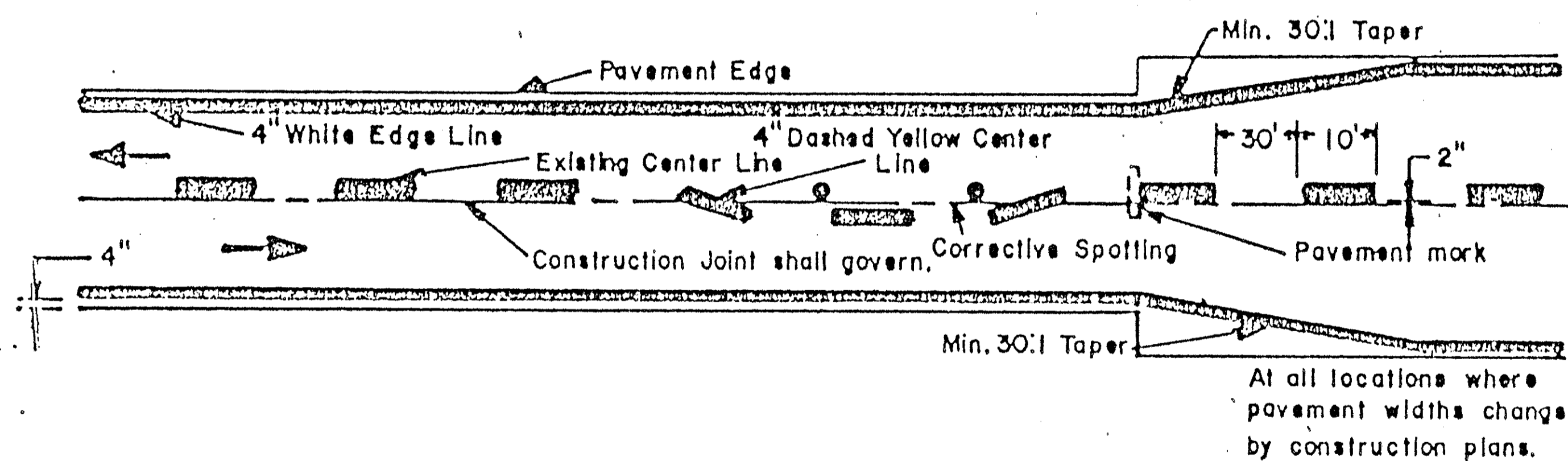
11
12

PLAN NO. 154

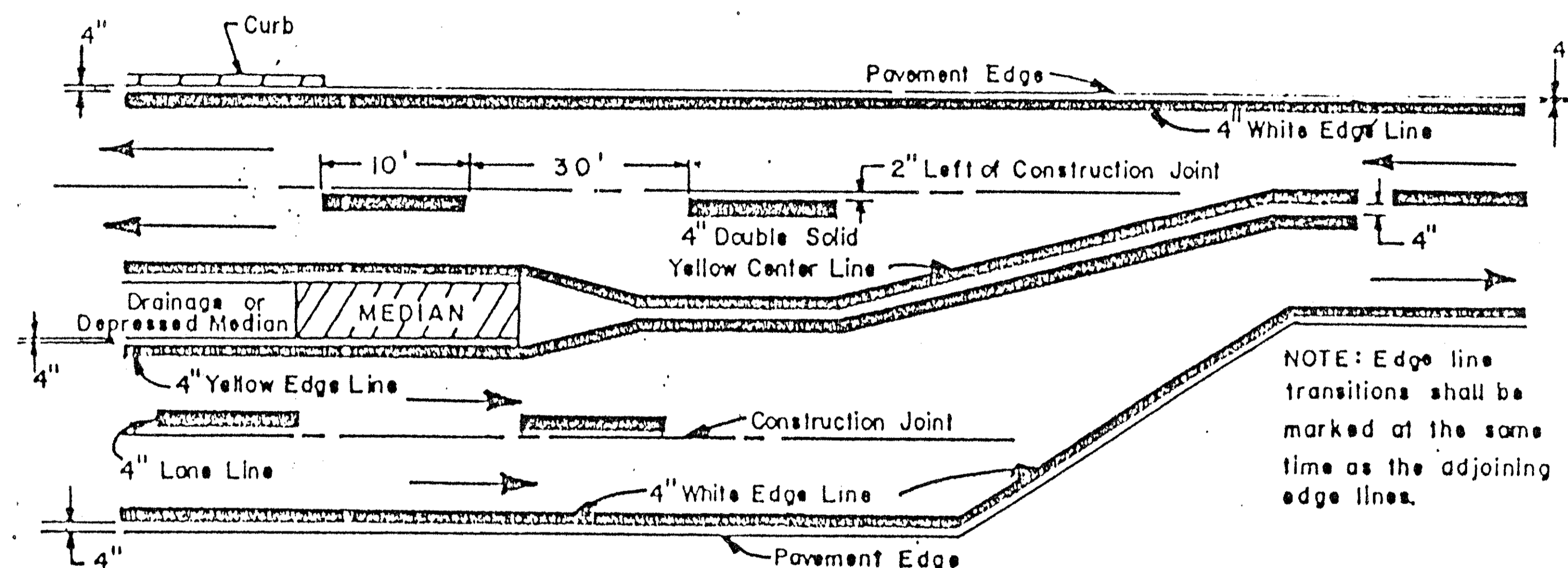
FREEWAY & EXPRESSWAY MAINLINE MARKINGS



TWO LANE MARKINGS



MULTILANE DIVIDED & UNDIVIDED HIGHWAY MARKINGS



NOTES:

1. THE DISTANCE FROM THE PAVEMENT EDGE TO THE NEAR-SIDE EDGE OF THE EDGELINE MAY BE INCREASED WITH THE APPROVAL OF THE ENGINEER IN ORDER TO MAINTAIN UNIFORM LANE WIDTH.
2. SEE TC 72.20 FOR ENTRANCE AND EXIT RAMP MARKINGS.
3. The cycle length for dashed lines shall be 40 feet plus or minus 6 inches. The minimum length of dash shall be sufficiently long to maintain a 3:1 ratio between length of gap and length of dash.

DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING TYPICAL DETAILS	DATE 11/80
JDL: CDR	

GENERAL SUMMARY

ITEM	PART 1	PART 2	PART 3	TOTAL PARTS 2 & 3	PART 4	PART 5	PART 6	TOTAL PARTS 4-5-6	ITEM NO.	EXTENSION	GRAND TOTAL	UNIT	DESCRIPTION
407	5973	1737	359	2096	755	6672	1713	9140	407	10000	17209	GALLON	Tack Coat
251							500	500	251	01000	500	Sq. Yd.	Partial Depth Pavement Repair
403			50	50	106	926	235	1267	403	25000	1317	Cu. Yd.	Asphalt Concrete, AC-20, Spot Leveling
404	2301	724	99	823	209	1853	479	2541	404	20000	5665	Cu. Yd.	Asphalt Concrete, AC-20
253	10				8	25		33	253	02000	43	Cu. Yd.	Pavement Repair
202			14	14	31	30		61	202	54100	75	Each	Raised Pavement Marker Removed For Storage
254	49571	17370	156	17526	178	250	140	568	254	01000	67665	Sq. Yd.	Pavement Planing, Bituminous
254	500	150		150					254	01600	650	Sq. Yd.	Patching Planed Surface
645							2	2	645	01290	2	Each	Lane Arrow, Type A, Inlaid
645						55		55	645	00490	55	Lin. Ft.	Stop Line, Type A, Inlaid
614	3.12								614	20400	3.12	Mile	Temporary Lane Line, Class II
642			.40	.40		9.86	1.94	11.80	642	00102	12.20	Mile	Edge Line, Type 2
642			.20	.20		4.93	.97	5.90	642	00302	6.10	Mile	Center Line, Type 2
614	3.91	1.46	.40	1.86	.76	9.86	1.94	12.56	614	21400	18.33	Mile	Temporary Center Line, Class II
624	Lump	Lump	Lump	Lump	Lump	Lump	Lump	Lump	624	10000	Lump	Lump	Mobilization
614					2	12	2	16	614	12460	16	Each	Work Zone Marking Sign
617			.20	20	64	482	95	641	617	10100	661	Cu. Yd.	Compacted Aggregate, Type A
614	Lump	Lump	Lump	Lump	Lump	Lump	Lump	Lump	614	11000	Lump	Lump	Maintaining Traffic
604	11								604	34500	11	Each	Manhole Adjusted To Grade
814	5								814	10800	5	Each	VALVE BOX Adjusted To Grade
SPECIAL									SPECIAL	69050000	17	EACH	MAILBOX SUPPORT

TRAFFIC:
Traffic shall be maintained at all times. The length of restricted traffic zones shall be kept to a minimum consistent with the specification requirements for protection of completed courses.

RAILROAD CROSSINGS:
The new surface course shall be feathered or butt jointed to meet the rail grades as specified.

ALIGNMENT AND PROFILE:
The work proposed by this project is for the resurfacing of the existing pavement. The alignment of the existing pavement will not be changed, and the profile of the proposed surface will be similar to that of the existing pavement except that it will be raised an amount equal to the thickness of the resurfacing course or courses specified in these plans.

INTERMEDIATE COURSE, SPOT LEVELING AND PATCHING:
This material shall be placed in a separate operation where and as directed by the engineer.

TACK COAT:
The tack coat operation shall be as determined at a pre-construction conference as per 407.05, and application rates shall not exceed 0.10 gal. per sq. yd.

COVER AGGREGATE:
Cover aggregate shall conform to 703.06.

ITEM 642 EDGE LINE- PLACEMENT:
See Sheet 9, Pavement Marking Sub-Summary, for the width of each lane and the total width of pavement, edge line to edge line.