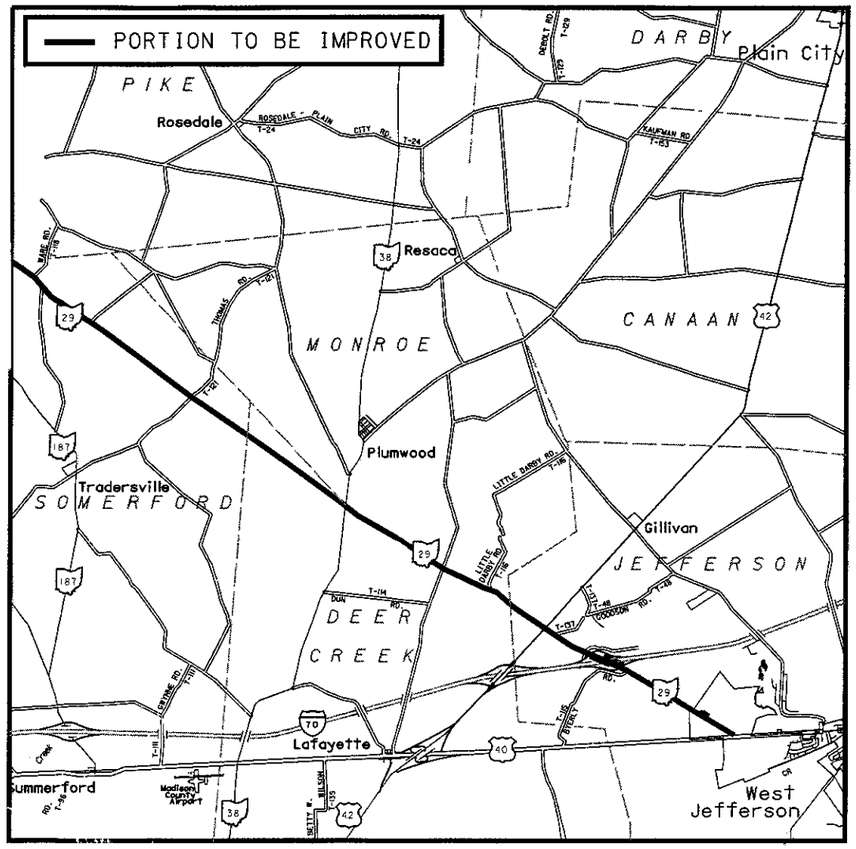
MAD-29-0.00
010025
DIST.06

PID# 20896
01/24/01

OHIO DEPARTMENT OF TRANSPORTATION



LAT: 39° 59' 20" N LONG: 83° 24' 45" W

PART	COUNTY	ROUTE	SECTIONS	PROJECT TERMINII		NET LENGTH MI	VILLAGE
				BEGIN	END		
1	MAD	29	(0.00 - 12.19)	0.00	12.98	12.98	WEST JEFFERSON

INDEX OF SHEETS:

PROJECT DESCRIPTION

RESURFACING OF 12.98 MI OF SR-29 FROM THE CHAMPAIGN COUNTY LINE TO THE INTERSECTION OF US-40.

1997 SPECIFICATIONS

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN the PROPOSAL SHALL GOVERN THIS PROJECT.

STANDARD DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	
BP-3.1M	10/28/94					SS-806	09/09/97
TC-65.10M	11/01/95					SS-863	10/12/99
TC-65.11M	11/01/95					SS-905	04/01/98
TC-65.12M	11/01/95					SS-906	01/06/99
TC-71.10M	09/01/93					SS-908	01/06/99
MT-97.10M	04/25/94						
MT-97.11M	01/30/95						
MT-99.20M	01/30/95						
MT-105.10M	04/25/94						
MT-105.11M	04/25/94						
RM-1.1M	04/29/99						

ENGINEERS SEAL

STATE OF OHIO
DAVID P. POLING
56872
REGISTERED PROFESSIONAL ENGINEER

SIGNED David P. Poling
DATE 6-28-00

UNDERGROUND UTILITIES
TWO WORKING DAYS
BEFORE YOU DIG
CALL 1-800-362-2764 (TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY

PLAN PREPARED BY:
O.D.O.T.
DISTRICT SIX
IN-HOUSE DESIGN

PLANS CERTIFIED BY:
NAME Robert Kusilla DATE 6/16/00
DISTRICT 6
OHIO DEPT. OF TRANSPORTATION

Approved Jack R. Marchbanks
Date 6.16.00 District Deputy Director of Transportation

Approved Gordon Proctor
Date 10/12/00 Director, Department of Transportation

FEDERAL PROJECT NO. TE-21-G000(500)
PID NO. 20896
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
MAD-29-0.00
1/13

bkingell
16-JUN-2000 2:45PM
i:\Projects\mad\029\0000.00\DGN\gt101.dgn

SHEET NUMBER											ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
3\4	5	6	7	8\9	10\11											
															ROADWAY	
					897						202	54100	897	EACH	RAISED PAVEMENT MARKER REMOVED FOR STORAGE	
5											604	38500	5	EACH	MONUMENT ASSEMBLY	
															PAVEMENT	
250											251	01002	250	CY	PARTIAL DEPTH PAVEMENT REPAIR	
	5259	1579	1,025	240							254	01000	8,103	SY	PAVEMENT PLANING, BITUMINOUS	
	14497	82	382	62							407	10000	15,023	GAL	TACK COAT	
	4553										407	14000	4,553	GAL	TACK COAT FOR INTERMEDIATE COURSE	
692	2520			16							448	46020	3,228	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22	
	6709	40	184	14							448	47020	6,947	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22	
	1636	77	34								617	10100	1,747	CY	COMPACTED AGGREGATE, TYPE A	
3.00											617	25000	3.00	MGAL	WATER	
															TRAFFIC CONTROL	
					942						621	00200	942	EACH	RAISED PAVEMENT MARKER, INSTALLATION ONLY	
					942						621	00300	942	EACH	PRISMATIC RETROREFLECTOR	
62											626	00100	62	EACH	BARRIER REFLECTOR, TYPE A	
					13.13						642	00302	13.13	MILE	CENTER LINE, TYPE 2	
					25.96						642	00102	25.96	MILE	EDGE LINE, TYPE 2	
					66						644	00500	66	LF	STOP LINE	
					120						644	00400	120	LF	CHANNELIZING LINE	
					340						644	00700	340	LF	TRANSVERSE LINE	
					2						644	01300	2	EACH	LANE ARROW	
					1						644	01410	1	EACH	WORD ON PAVEMENT, 96"	
															STRUCTURE	
				352							SPECIAL	51631200	352	LF	SAWING AND SEALING BITUMINOUS CONCRETE JOINTS	
				28							SPECIAL	51912200	28	SY	PATCHING CONCRETE BRIDGE DECK - TYPE A (SEE PROPOSAL NOTE)	
															MAINTENANCE OF TRAFFIC	
72											614	12460	72	EACH	WORK ZONE MARKING SIGN	
18.95											614	21400	18.95	MILE	TEMPORARY CENTER LINE, CLASS II	
											614	11000	LUMP	SUM	MAINTAINING TRAFFIC	
LUMP											623	10001	LUMP	SUM	CONSTRUCTION LAYOUT STAKES, AS PER PLAN	
											624	10000	LUMP	SUM	MOBILIZATION	
LUMP											806	16001	3	MNTH	FIELD OFFICE, TYPE A, AS PER PLAN	

CALCULATED

CHECKED

GENERAL SUMMARY

MAD-29-0.00

CONSTRUCTION INITIATION:
THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS AT 740-363-1251, EXT. 469 OR BY FAX AT 740-369-7437 AND THE DISTRICT TRAFFIC MANAGEMENT ENGINEER AT 740-363-1251, EXT. 323, FOURTEEN (14) DAYS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT TRAFFIC MANAGEMENT ENGINEER OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION PROJECT. THE PROJECT ENGINEER WILL PROVIDE CLARIFICATION FOR ANY QUESTIONS ABOUT THIS NOTIFICATION REQUIREMENT.

COORDINATION WITH O.D.O.T.'S CENTRAL OHIO TRAFFIC MANAGEMENT PROGRAM (COTMP):

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES ON A WEEKLY BASIS. WHEN DETOURS ARE PLANNED, THIS NOTIFICATION SHALL BE AT THE PRE CONSTRUCTION MEETING OR 30 DAYS IN ADVANCE ONCE CONSTRUCTION HAS BEGUN. LANE AND RAMP CLOSURES FOR 2 OR MORE WEEKS SHALL BE REPORTED 2 WEEKS IN ADVANCE OF CLOSURE. LANE AND RAMP CLOSURES OF LESS THAN 2 WEEKS DURATION AND MORE THAN 2 DAYS SHALL BE REPORTED AT LEAST 3 WORKING DAYS IN ADVANCE. FOR SHORT TERM LANE OR RAMP CLOSURES (2 DAYS OR LESS) NOTIFICATION SHALL BE MADE AT LEAST 1 WORKING DAY IN ADVANCE. INFORMATION SHALL INCLUDE BUT NOT BE LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT TRAFFIC AT PRESENT AND IN THE NEXT 30 DAYS. THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL WHO WILL BE RESPONSIBLE FOR PREPARING THIS REPORT AT THE PRE - CONSTRUCTION MEETING. ANY UNFORESEEN IMPACTS TO TRAFFIC SHALL BE REPORTED TO THE PROJECT ENGINEER AS SOON AS POSSIBLE. THE PROJECT ENGINEER SHALL PROVIDE THIS INFORMATION TO COTMP. ALL CONSTRUCTION ACTIVITIES THAT INTERFERE WITH TRAFFIC SHALL BE REPORTED TO COTMP. THIS INFORMATION SHALL BE PROVIDED TO COTMP AT (740) 363-1251 (EXT. 323), OR BY FAX AT (740) 363-6831.CONSTRUCTION INITIATION:
THE

GENERAL:
THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 101.18) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

UNDERGROUND UTILITIES:
IT IS THE CONTRACTOR'S RESPONSIBILITY TO HAVE ANY UNDERGROUND UTILITIES MARKED.
OHIO UTILITY PROTECTION SERVICE 1-800-362-2764
NON-MEMBERS MUST BE CALLED DIRECTLY.

ALIGNMENT AND PROFILE:
THE WORK PROPOSED BY THIS PROJECT IS FOR THE RESURFACING OF THE EXISTING PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED, AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT EXCEPT THAT IT MAY BE RAISED AN AMOUNT EQUAL TO THE THICKNESS OF THE RESURFACING COURSE OR COURSES SPECIFIED IN THESE PLANS.

CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:
THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

CONTINGENCY QUANTITIES:
THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR:
THIS ITEM IS A CONTINGENCY QUANTITY AND SHALL BE USED WHERE DIRECTED BY THE ENGINEER. REPAIRS SHALL BE LOCATED BY THE ENGINEER AND PERFORMED PRIOR TO THE PLACEMENT OF THE 448 ASPHALT CONCRETE SURFACE COURSE TYPE 1. REPAIR AREAS SHALL BE A MINIMUM OF 3 FEET IN WIDTH AND A VARIABLE LENGTH, WHICH SHALL BE DETERMINED BY THE ENGINEER. THE DEPTH OF REPAIRS SHALL BE APPROXIMATELY 6 INCHES IN DEPTH. THE FOLLOWING QUANTITY HAS BEEN PROVIDED:
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR: = 250 CU. YD.

ITEM 254 - PAVEMENT PLANING, BITUMINOUS:
THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE THAT MAY RESULT FROM THE PLANING OPERATION, INCLUDING CASTINGS AND LOOP DETECTORS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED, TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT.

NO PLANED PAVEMENT SHALL BE LEFT EXPOSED TO TRAFFIC FOR MORE THAN 5 CONSECUTIVE DAYS PRIOR TO THE PLACEMENT OF ITEM 448 - ASPHALT CONCRETE SURFACE COURSE TYPE 1. FAILURE TO COMPLY SHALL SUBJECT THE CONTRACTOR TO LIQUIDATED DAMAGES AS PER SECTION 108.07 OF CMS

DISPOSAL OF DEBRIS:
ALL ASPHALT GRINDINGS, EXCESS ASPHALTIC MATERIAL OR ANY OTHER DEBRIS GENERATED DURING THE RESURFACING SHALL BE REMOVED FROM WITHIN 1,000 FEET OF THE SCENIC RIVER (LITTLE DARBY) AND DISPOSED OF AT AN APPROPRIATE FACILITY ABOVE THE 100 YEAR FLOOD ELEVATION OF THAT RIVER.

ITEM 407 - TACK COAT:
THE TACK COAT OPERATION SHALL BE AS DETERMINED AT A PRE-CONSTRUCTION CONFERENCE AS PER 407.05 AND APPLICATION RATES SHALL NOT EXCEED 0.075 GALLON PER SQ. YARD. A COVER AGGREGATE SHALL BE USED IF HEAVY TRACKING OF THE TACK COAT ON TO THE EXISTING PAVEMENT SHOULD ACCURE DURING THE PAVING OPERATIONS. THE COST OF THE COVER AGGREGATE SHALL BE INCLUDED IN THE COST OF THIS ITEM.

ITEM 448 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1 PG 64-22:
THIS MATERIAL IS TO BE USED, WHERE SPECIFIED IN THE PLANS, TO FILL RUTS AND IMPERFECTIONS, AND TO LEVEL AND RE-ESTABLISH THE ORIGINAL ALIGNMENT AND PROFILE OF THE PAVEMENT, PRIOR TO THE PLACEMENT OF THE SURFACE COURSE. THIS MAY RESULT IN A PORTION OF THE ORIGINAL SURFACE BEING EXPOSED BEFORE THE OVERLAY IS PLACED.
ITEM 448 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1:
6,916 X 10% = 692 CY

ITEM 604 - MONUMENT ASSEMBLY:
THIS WORK SHALL CONSIST OF FURNISHING AND PLACING CENTERLINE MONUMENTS AT THE FOLLOWING INTERSECTIONS:

1.) S.R. 29 AND COUNTY LINE OF MADISON -CHAMPAIGN COUNTY'S
2.) S.R. 29 AND TWP. RD. 121 (THOMAS RD.)
3.) S.R. 29 AND S.R.38
4.) S.R. 29 AND CO. RD. 5 (LAFAYETTE - PLAIN CITY RD.)
5.) S.R. 29 AND S.R. 42

A REGISTERED SURVEYOR FROM DISTRICT 6 SURVEY DEPARTMENT SHALL BE RESPONSIBLE FOR REFERENCING AND VERIFYING THE LOCATIONS OF THE CENTERLINE MONUMENTS. THE CONTRACTOR SHALL NOTIFY THE SURVEY DEPARTMENT (614-363-1251) 48 HOURS PRIOR TO START OF MONUMENT WORK. PAYMENT FOR THIS ITEM SHALL INCLUDE ALL NECESSARY LABOR, MISCELLANEOUS HARDWARE, AND EQUIPMENT REQUIRED FOR PLACEMENT. PAYMENT WILL BE AT CONTRACT BID PRICE PER EACH. THE FOLLOWING QUANTITY HAS BEEN PROVIDED:
ITEM 604 - MONUMENT ASSEMBLY: = 5 EACH

ITEM 614 - WORK ZONE MARKING SIGN:
THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED:

OW-167-36	"NO EDGE LINES"	= 24 EACH
R-33-30	"DO NOT PASS"	= 21 EACH
R-34-30	"PASS WITH CARE"	= 27 EACH
ITEM 614 - WORK ZONE MARKING SIGN		= 72 EACH

ITEM 614 - TEMPORARY CENTER LINE CLASS II:
THE FOLLOWING QUANTITY HAS BEEN PROVIDED

SLM 0.00 - 12.98 = 12.98 MI (SURFACE COURSE)	= 12.98 MI
SLM 0.00 - 5.97 = 12.98 MI (INTERMED. COURSE)	= 5.97 MI
ITEM 614 - TEMPORARY CENTER LINE, CLASS II	= 18.95 MI

CALCULATED | CHECKED

GENERAL NOTES

MAD-29-0.00

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ITEM 614 - MAINTAINING TRAFFIC:
ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING AND COMPLETED PAVEMENT. WORK ZONES SHALL BE LIMITED IN LENGTH TO THE AMOUNT OF WORK THAT CAN BE PERFORMED THAT DAY.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN, WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL, TO THE DISTRICT SIX MAINTENANCE OF TRAFFIC COORDINATOR, THE CONTRACTOR'S MAINTENANCE OF TRAFFIC PLAN WITH CONSTRUCTION PHASING DESCRIPTIONS, PRIOR TO BEGINNING WORK.

ITEM 617 WATER:
THIS ITEM SHALL BE USED AS DIRECTED BY THE ENGINEER. THIS IS AN ESTIMATED QUANTITY OF 3 M/GAL
ITEM 617 WATER: = 3 M/GAL

ITEM 623 - CONSTRUCTION LAYOUT STAKES, AS PER PLAN:
THIS ITEM SHALL CONSIST OF STATIONING USING 3 FT LATH STAKES. THE STAKES SHALL BE SPACED AT 100 FT INTERVALS AND SHALL EXTEND THROUGHOUT THE LENGTH OF THE PROJECT AND THROUGHOUT THE LENGTH OF ALL RAMPS. PLACEMENT OF THE STAKES SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED OR MISSING STAKES.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH PRICE SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

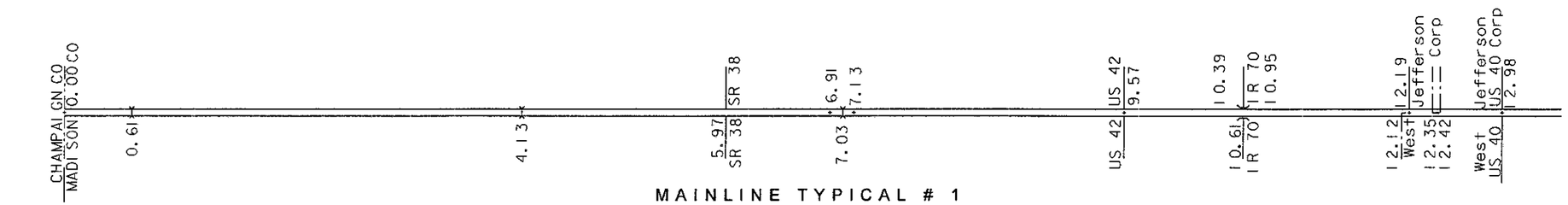
ITEM 626 BARRIER REFLECTOR TYPE A:
THIS QUANTITY SHALL BE USED TO REPLACE THE BARRIER REFLECTORS, WHICH ARE MISSING FROM THE EXISTING GUARDRAIL AND AS DIRECTED BY THE ENGINEER.
ITEM 626 BARRIER REFLECTOR TYPE A: = 62 EACH

ITEM 806 - FIELD OFFICE, TYPE A, AS PER PLAN:
UNDER THIS ITEM, THE CONTRACTOR SHALL PROVIDE A FIELD OFFICE MEETING ALL REQUIREMENTS OF ITEM 806 - FIELD OFFICE, TYPE A, WITH THE FOLLOWING MODIFICATION. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF TWO SEPARATE PHONE LINES FOR THE FIELD OFFICE ON THIS PROJECT. THE FOLLOWING QUANTITY HAS BEEN PROVIDED:
ITEM 806 - FIELD OFFICE, TYPE A, AS PER PLAN = 3 MONTHS

CALCULATED | CHECKED

GENERAL NOTES

MAD-29-0.00



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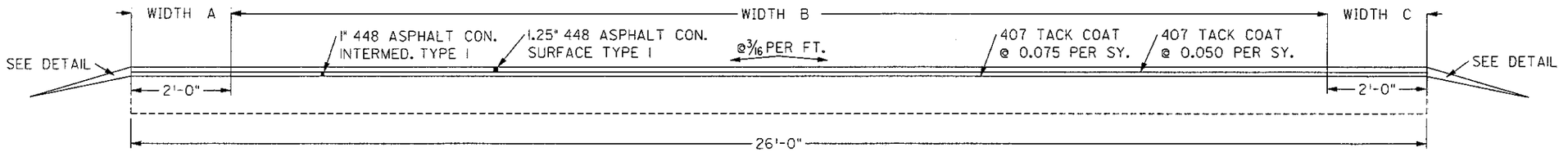

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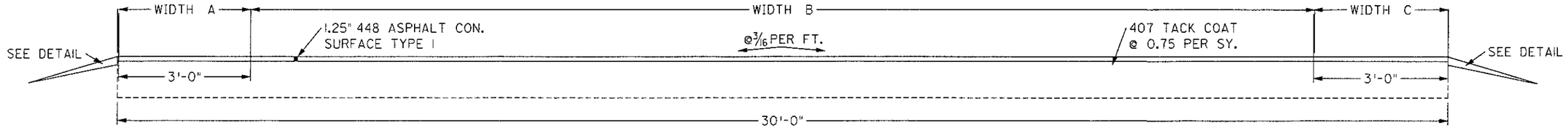

LOCATION							PAVEMENT PLANING										SURFACE PAVEMENT											REMARKS
							AVERAGE PAVEMENT WIDTHS							254 PAVEMENT PLANING, BITUM.		407 TACK COAT 0.075 GAL/SQ Y	448 AC INT COURSE TYPE 1 PG 64-22		AVERAGE PAVEMENT WIDTHS							407 TACK COAT 0.050 GAL/SQ Y	448 AC SURF COURSE TYPE 1 PG 64-22		617 COMPACT AGG TYPE A		
PART	COUNTY	ROUTE	LOG BEG	LOG END	LENGTH		TYPICAL	A	B	C	D	E	F	AREA	AVG DEP		AREA	AVG THK		A	B	C	D	E	F	AREA		AVG THK		AVG THK		
			MI	MI	MI	LF		FT	FT	FT	FT	FT	FT	SY	IN	SY	SY	IN	CY	FT	FT	FT	FT	FT	FT		SY	IN	CY	IN	CY	
1	MAD	29	0.000	5.970	5.970	31522	1	2.0	22.0	2.0				91062			6830	1.00	2530	2.0	22.0	2.0				91062	4553	1.25	3162	2.00	778	MAINLINE PAVEMENT
1	MAD	29	5.983	6.910	0.927	4895	3	2.0	22.0	2.0				14140			1061			2.0	22.0	2.0				14140		1.25	491	2.00	120	MAINLINE PAVEMENT
1	MAD	29	6.910	7.130	0.220	1162	2	3.0	24.0	3.0				3872			290			3.0	24.0	3.0				3872		1.25	134	2.00	28	MAINLINE PAVEMENT
1	MAD	29	7.130	9.570	2.440	12883	3	2.0	22.0	2.0				37218			2791			2.0	22.0	2.0				37218		1.25	1292	2.00	318	MAINLINE PAVEMENT
1	MAD	29	9.583	10.390	0.807	4261	3	2.0	22.0	2.0				12309			923			2.0	22.0	2.0				12309		1.25	427	2.00	106	MAINLINE PAVEMENT
1	MAD	29	10.390	10.950	0.560	2957	2	3.0	24.0	3.0				9856			739			3.0	24.0	3.0				9856		1.25	342	2.00	74	MAINLINE PAVEMENT
1	MAD	29	10.950	12.720	1.770	9346	3	2.0	22.0	2.0				26998			2025			2.0	22.0	2.0				26998		1.25	937	2.00	230	MAINLINE PAVEMENT
1	MAD	29	0.610	0.633	0.023	126	1	2.0	22.0	2.0				364			-27	1.00	-10	2.0	22.0	2.0				364		1.25	-13	2.00	-4	DEDUCT FOR BRIDGE 00.61
1	MAD	29	10.610	10.712	0.102	541	2	3.0	24.0	3.0				1803			-135			3.0	24.0	3.0				1803		1.25	-63	2.00	-14	DEDUCT FOR BRIDGE 10.61
1	MAD	29	0.000	0.019	0.038	200		2.0	22.0	2.0					0-2.25	578																MAINLINE BUTT JOINT
1	MAD	29	0.572	0.610	0.038	200	7\8	2.0	22.0	2.0					0-2.25	578																MAINLINE BUTT JOINT STRUCTURE 00.61
1	MAD	29	0.633	0.671	0.038	200	7\8	2.0	22.0	2.0					0-2.25	578																MAINLINE BUTT JOINT STRUCTURE 00.61
1	MAD	29	4.092	4.175	0.083	443	7\9	2.0	22.0	2.0					0-2.25	1280																MAINLINE TAPER FOR STRUCTURE 04.13
1	MAD	29	5.932	5.970	0.038	200	5	2.0	22.0	2.0					0-2.25	578																MAINLINE BUTT JOINT SR 38
1	MAD	29	5.983	6.002	0.019	100	5	2.0	22.0	2.0					0-1.25	289																MAINLINE BUTT JOINT SR 38
1	MAD	29	7.011	7.063	0.052	277	7\9	2.0	22.0	2.0					0-1.25	800																MAINLINE TAPER FOR STRUCTURE 07.03
1	MAD	29	9.551	9.570	0.019	100	5	2.0	22.0	2.0					0-1.25	289																MAINLINE BUTT JOINT US 42
1	MAD	29	9.583	9.602	0.019	100	5	2.0	22.0	2.0					0-1.25	289																MAINLINE BUTT JOINT US 42
TOTALS																5259	14497		2520								4553		6709		1636	

CALCULATED | CHECKED

PAVEMENT SUBSUMMARY AND DETAILS

MAD-29-0.00

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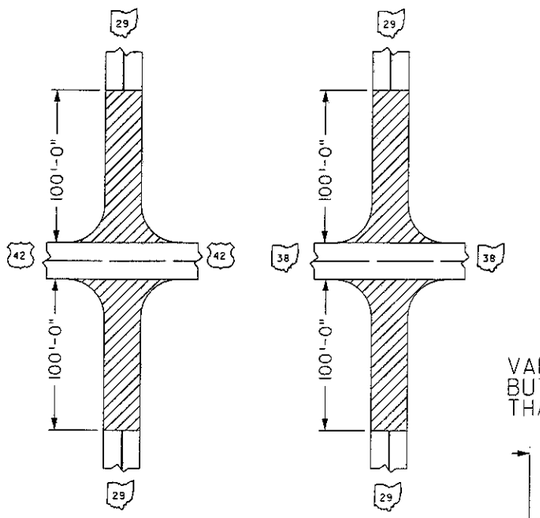
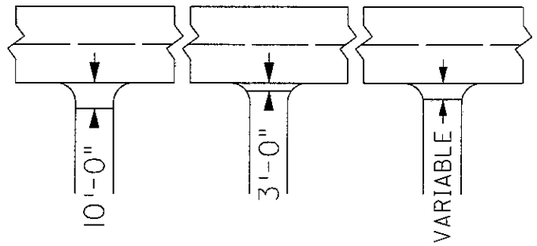
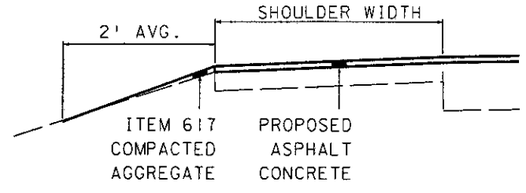
INTERSECTION PLANING & PAVING DETAILS # 5



DRIVEWAY TREATMENTS # 6



SHOULDER TREATMENT



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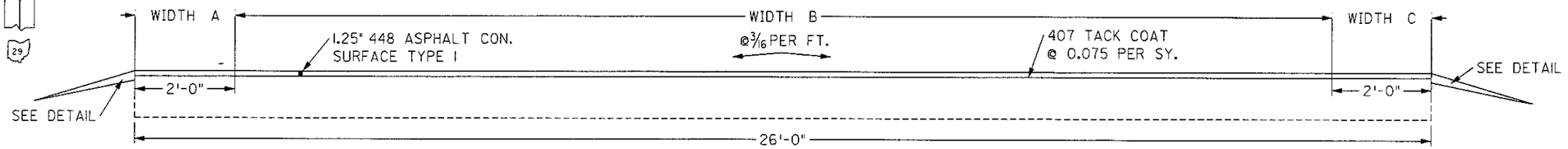

LOCATION							PAVEMENT PLANING									SURFACE PAVEMENT										REMARKS
							AVERAGE PAVEMENT WIDTHS							254	407	448	AVERAGE PAVEMENT WIDTHS							407	448		617		
PART	COUNTY	ROUTE	LOG BEG	LOG END	LENGTH	TYPICAL	A	B	C	D	E	F	AREA	PAVEMENT PLANING, BITUM. AVG THK	TACK COAT 0.075 GAL/SQ Y	AC INT COURSE TYPE 1 PG64-22 AVG THK	A	B	C	D	E	F	AREA	TACK COAT 0.050 GAL/SQ Y	AC SURF COURSE TYPE 1 PG64-22 AVG THK		COMPACT AGG TYPE A AVG THK		
			MI	MI	MI	LF	FT	FT	FT	FT	FT	FT	SY	IN	SY	IN	FT	FT	FT	FT	FT	FT	SY	SY	IN	CY	IN	CY	
1	MAD	29	10.591	10.610	0.019	100	7\8	3.0	24.0	3.0					0-1.25	333													MAINLINE BUTT JOINT STRUCTURE 10.61
1	MAD	29	10.712	10.731	0.019	100	7\8	3.0	24.0	3.0					0-1.25	333													MAINLINE BUTT JOINT STRUCTURE 10.61
1	MAD	29	VAR	LOC		25	5					30.0		83	0-1.25	913	68						30.0		83		1.25	33	2.00	11	COUNTY ROAD INTERSECTIONS
1	MAD	29	VAR	LOC		3	6					15.0		190			14						15.0		190		1.25	7			ASPHALT DRIVEWAYS
1	MAD	29	VAR	LOC		10	6					15.0																	2.00	66	GRAVEL DRIVEWAYS
TOTALS													273		1579	82										40		77		

CALCULATED | CHECKED

PAVEMENT SUBSUMMARY AND DETAILS

MAD-29-0.00

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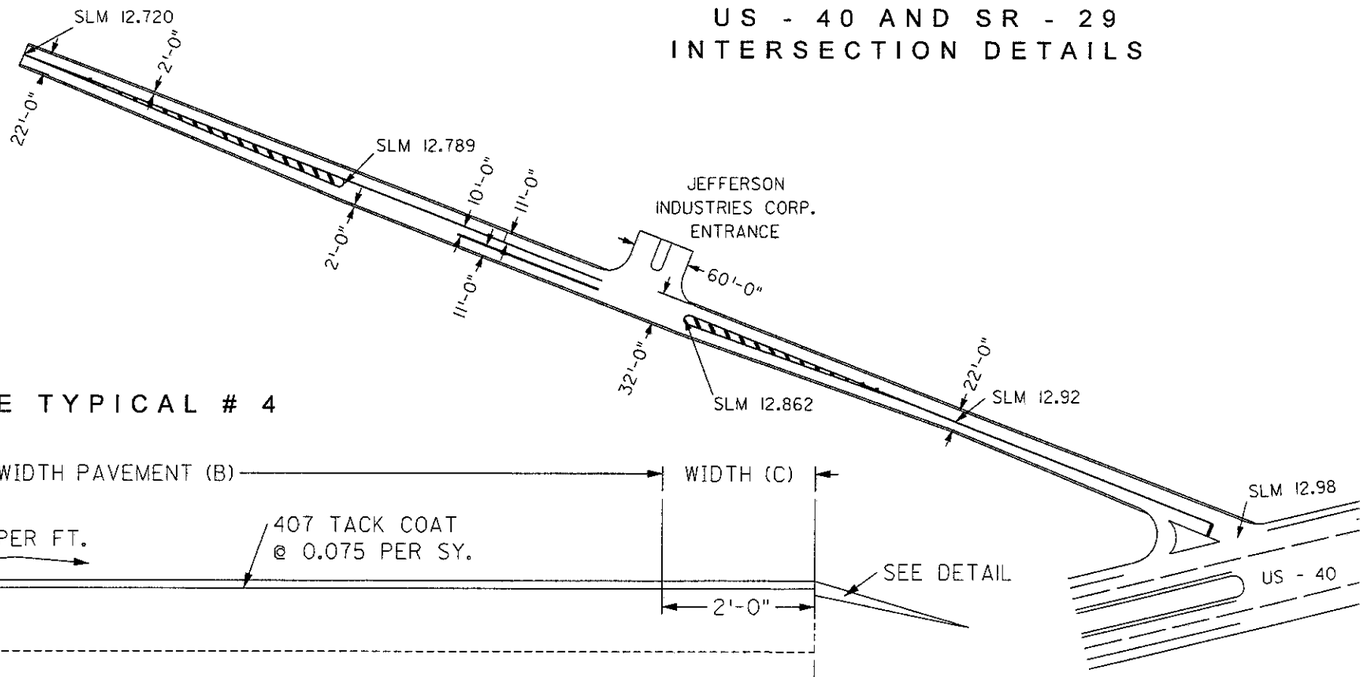
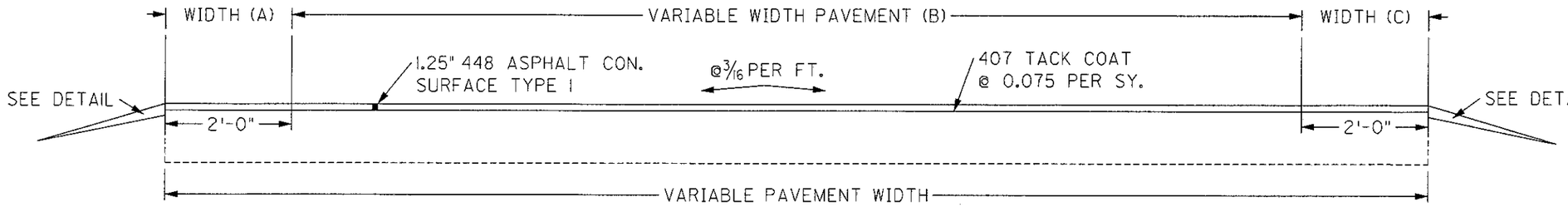
US - 40 AND SR - 29
INTERSECTION DETAILS



MAINLINE TYPICAL # 4



LOCATION							PAVEMENT PLANING										SURFACE PAVEMENT											REMARKS
							AVERAGE PAVEMENT WIDTHS							254		407	448	AVERAGE PAVEMENT WIDTHS							407	448		617		
PART	COUNTY	ROUTE	LOG BEG	LOG END	LENGTH	TYPICAL	A	B	C	D	E	F	AREA	PAVEMENT PLANING, BITUM. AVG THK		TACK COAT 0.075 GAL/SQ Y	AC INT. COURSE TYPE 1 PG64-22 AVG THK		A	B	C	D	E	F	AREA	TACK COAT 0.050 GAL/SQ Y	AC SURF COURSE TYPE 1 PG64-22 AVG THK		COMPACT AGG TYPE A AVG THK		
			MI	MI	MI	LF	FT	FT	FT	FT	FT	FT	SY	IN	CY	SY	IN	CY	FT	FT	FT	FT	FT	FT		SY	IN	CY	IN	CY	
1	MAD	29	12.720	12.788	0.068	359	4	2.0	27.0	2.0				1235			93			2.0	27.0	2.0						1.25	43	2.00	8	MAINLINE PAVEMENT TAPER / WIDENING
1	MAD	29	12.789	12.862	0.073	385	4	2.0	22.0	2.0				1113			83			2.0	22.0	2.0						1.25	39	2.00	10	MAINLINE PAVEMENT
1	MAD	29	12.789	12.862	0.073	385	4		10.0					428			32				10.0							1.25	15			CENTER LEFT TURN LANE
1	MAD	29	12.862	12.920	0.058	306	4	2.0	27.0	2.0				1055			79			2.0	27.0	2.0						1.25	37	2.00	8	MAINLINE PAVEMENT TAPER / WIDENING
1	MAD	29	12.920	12.980	0.060	317	4	2.0	27.0	2.0				1091			82			2.0	27.0	2.0						1.25	38	2.00	8	MAINLINE PAVEMENT TAPER / WIDENING
1	MAD	29	12.968	12.980	0.012	63	4		24.0					169	0-1.25	169	13				24.0							1.25	6			WEST US - 40 SLIP RAMP
1	MAD	29	12.920	12.980	0.060	317	4	2.0	27.0	2.0				1091	0-1.25	689					24.0											MAINLINE BUTT JOINT PLANING US 40
1	MAD	29	12.844			25	4		60.0					167	0-1.25	167					60.0							1.25	6			JEFFERSON INDUSTRIES CORP ENTRANCE
TOTALS														6349		1025	382												184		34	

PAVEMENT SUBSUMMARY AND DETAILS

MAD-29-0.00

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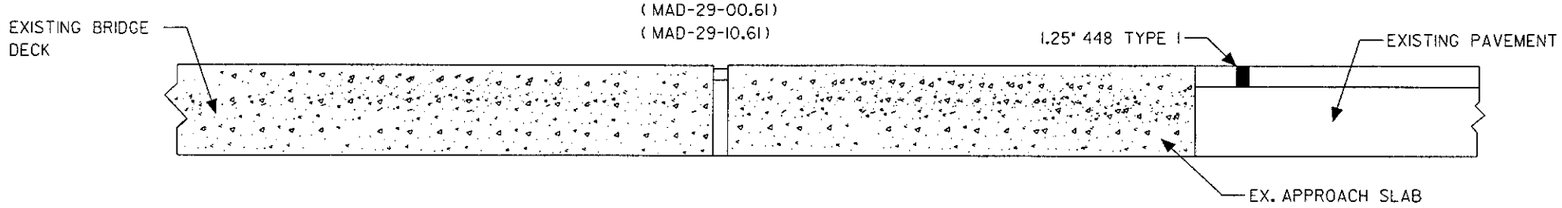
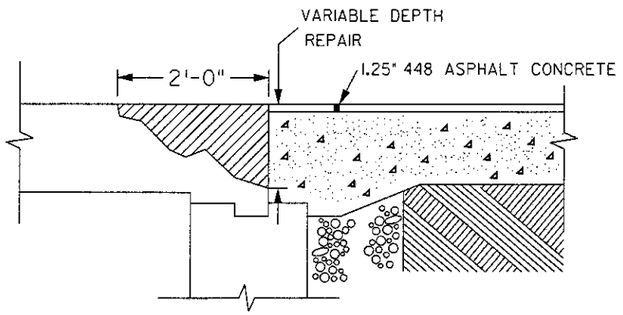
STRUCTURE PAVEMENT JOINT DETAIL # 8

(MAD-29-00.61)
(MAD-29-10.61)



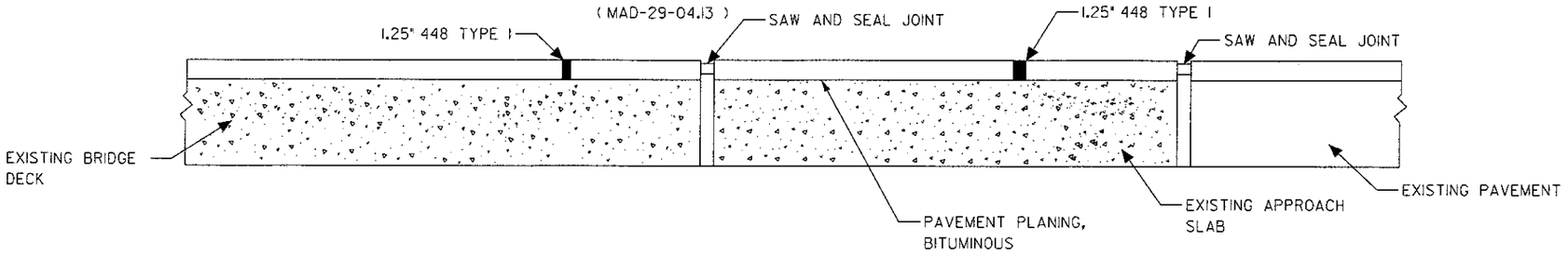
LIMITS OF TYPE (A) PATCHING

(MAD-29-00.61)



STRUCTURE EXPANSION JOINT DETAIL # 9

(MAD-29-07.03)
(MAD-29-04.13)



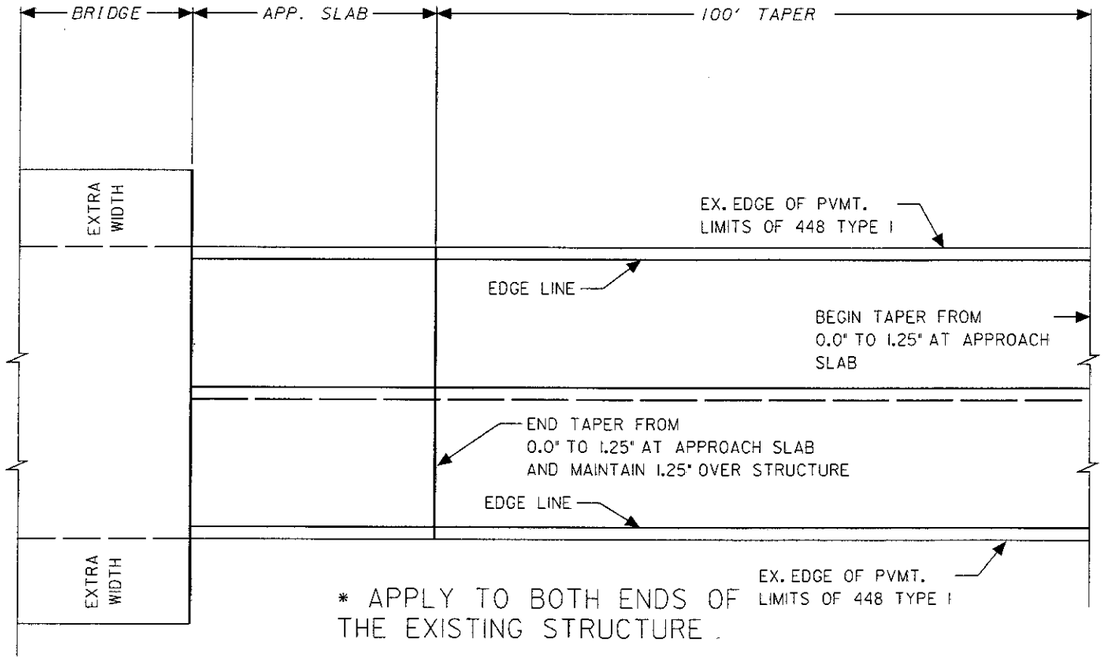
* THESE ASPHALT QUANTITIES ARE FOR THE 1.25" OF ASPHALT CONCRETE USED TO RESURFACE THE EXTRA WIDTH OF THE EXISTING STRUCTURES AND 1" OF INTERMEDIATE COURSE OVER THE ENTIRE DECK SURFACEOF THE STRUCTURE

* ITEM 254 - PAVEMENT PLANING , BITUMINOUS SHALL BE USED IN LIEU OF ITEM 202 - WEARING COURSE REMOVED AS CALLED FOR IN STANDARD DRAWING BP-3.IM.

BRIDGE TREATMENT QUANTITIES

STRUCTURE	DETAIL	254 PAVEMENT PLANING BITUINOUS	407 TACK COAT FOR SURFACE COURSE (0.075 PER SY)	448 ASPHALT CONCRETE SURFACE COURSE 1 PG 64-22 1.25"DEPTH		SPECIAL PATCHING CONCRETE BRIDGE DECK TYPE A	448 ASPHALT CONCRETE SURFACE COURSE 1 PG 64-22 1"DEPTH
		SY	GAL	CY		SY	CY
MAD-29-0.61	7\8					28	
MAD-29-4.13	7\9	86	22	6			6
MAD-29-7.03	7\9	154	40	8			10
TOTALS		240	62	14		28	16

BRIDGE TREATMENT PLAN VIEW AND BRIDGE PAVING TAPER DETAILS # 7



* APPLY TO BOTH ENDS OF THE EXISTING STRUCTURE .

CALCULATED
CHECKED

STRUCTURE DETAILS

MAD-29-0.00

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ITEM SPECIAL-SAWING AND SEALING BITUMINOUS CONCRETE JOINTS

1) DESCRIPTION:

THIS WORK SHALL CONSIST OF CUTTING AND SEALING TRANSVERSE JOINTS IN THE NEW BITUMINOUS CONCRETE OVERLAY OF BRIDGES. BITUMINOUS CONCRETE JOINTS SHALL BE CONSTRUCTED DIRECTLY OVER, AND IN LINE WITH, THE EXISTING UNDERLYING TRANSVERSE ABUTMENT AND APPROACH SLAB JOINTS.

2) MATERIALS:

THE JOINT SEALANT SHALL MEET THE REQUIREMENTS OF ITEM 705.04, JOINT SEALANTS, HOT-POURED, FOR CONCRETE AND ASPHALT PAVEMENTS. ACCEPTABLE ALTERNATE MATERIALS ARE:

A SILICONE SEALANT MEETING FEDERAL SPECIFICATIONS TT-S-001543A CLASS A (ONE-PART SILICONE SEALANTS) AND TT-S-00230C CLASS A (ONE-COMPONENT SEALANTS), SUCH AS THOSE MANUFACTURED BY GENERAL ELECTRIC, SILICONE PRODUCTS DIVISION, 4015 EXECUTIVE PARK DRIVE, CINCINNATI, OHIO 45242 (513-243-1953)OR DOW CORNING, 400 TECHNE CENTER, SUITE 103, MILFORD, OHIO 45150 (513-831-3586); OR SOF-SEAL, A COLD-APPLIED, LOW-MODULUS, TWO-COMPONENT POLY-MERIC COMPOUND HORIZONTAL SEALANT AS MANUFACTURED BY W.R.MEADOWS, INC., P.O. BOX 543,ELGIN,ILLINOIS 60121 (800-342-5976).

3) CONSTRUCTION DETAILS:

A) GENERAL: THE CONTRACTOR SHALL CONDUCT HIS OPERATION SO THAT THE CUTTING, CLEANING AND SEALING OF TRANSVERSE JOINTS IS A CONTINUOUS OPERATION THAT WILL BE PERFORMED AS SOON AS PRACTICAL AFTER THE PAVING, BUT NO LATER THAN FOUR (4) DAYS AFTER PLACEMENT OF THE ASPHALT CONCRETE SURFACE COURSE. TRAFFIC SHALL NOT BE ALLOWED TO KNEAD TOGETHER OR DAMAGE JOINT CUT PRIOR TO SEALING.

B) CUTTING OF TRANSVERSE JOINTS: THE CONTRACTOR SHALL SAW OR ROUT TRANSVERSE JOINTS TO THE DIMENSIONS SHOWN IN THE DETAILS ON THIS SHEET. THE CUT JOINTS SHALL LIE DIRECTLY ABOVE EACH TRANSVERSE JOINT.

THE BLADE OR BLADES SHALL BE OF SUCH SIZE THAT THE FULL WIDTH AND DEPTH OF THE CUT CAN BE MADE WITH ONE PASS. DRY OR WET CUTTING WILL BE ALLOWED. JOINTS SHALL EXTEND THE FULL WIDTH OF THE BRIDGE.

C) CLEANING JOINTS: DRY SAWED JOINTS SHALL BE THOROUGHLY CLEANED WITH A SUFFICIENT AMOUNT OF COMPRESSED AIR TO REMOVE ANY DIRT, DUST, OR DELETERIOUS MATTER. WET SAWED JOINTS SHALL BE WASHED CLEAN OF ALL CUTTINGS BY FLUSHING WITH A JET OF WATER AND WITH OTHER TOOLS AS NECESSARY. AFTER FLUSHING, THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR. WHEN THE SURFACES ARE THOROUGHLY CLEAN AND DRY, AND JUST PRIOR TO PLACING THE JOINT SEALER, COMPRESSED AIR HAVING A PRESSURE OF AT LEAST 90 PSI SHALL BE USED TO BLOW OUT THE JOINT AND REMOVE ALL TRACES OF DUST.

IN THE EVENT FRESHLY CUT JOINTS BECOME CONTAMINATED BEFORE THEY ARE SEALED, THEY SHALL BE RE-CLEANED OF ALL FOREIGN MATERIAL BY HIGH PRESSURE WATER JET.

D) SEALING JOINTS: THE JOINT SHALL BE THOROUGHLY DRY WHEN THE SEALANT IS PLACED. AFTER CLEANING AND DRYING, A BOND-BREAKER MATERIAL SHALL BE APPLIED TO THE BOTTOM OF THE GROOVE.

HOT-POURED JOINT SEALANT MATERIAL SHALL BE HEATED IN A KETTLE OR MELTER CONSTRUCTED AS A DOUBLE BOILER, WITH THE SPACE BETWEEN THE INNER AND OUTER SHELLS FILLED WITH OIL OR OTHER HEAT TRANSFER MEDIUM. POSITIVE TEMPERATURE CONTROL AND MECHANICAL AGITATION SHALL BE PROVIDED. HEATING MUST BE IN STRICT ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION. JOINT SEALER MATERIAL SHALL NEVER BE KEPT HEATED AT THE POURING TEMPERATURE FOR MORE THAN FOUR (4) HOURS AND SHALL NEVER BE REHEATED. SEALER LEFT IN THE APPLICATOR AT THE END OF A DAY'S WORK SHALL NOT BE USED.

HOT-POURED SEALANT SHALL BE APPLIED IMMEDIATELY THROUGH A NOZZLE, WHICH MUST PROJECT INTO THE SAWED JOINT, FILLING FROM THE BOTTOM UP. THE SEALANT SHALL COMPLETELY FILL THE JOINT IN SUCH A MANNER THAT, AFTER COOLING, THE LEVEL OF THE SEALANT WILL NOT BE HIGHER THAN 1/8" BELOW THE PAVEMENT SURFACE. ANY DEPRESSION IN THE COOLED SEAL GREATER THAN 1/4" SHALL BE BROUGHT UP TO THE SPECIFIED LIMIT BY FURTHER ADDITION OF HOT-POURED SEALANT. CARE SHALL BE TAKEN IN THE SEALING OF THE JOINTS SO THAT THE FINAL APPEARANCE WILL PRESENT A NEAT FINE LINE.

THE COLD APPLIED SEALANT MATERIALS (POLYURETHANE, SILICONE, AND POLYMERIC COMPOUNDS) SHALL BE INSTALLED AS PER MANUFACTURERS' RECOMMENDATIONS, EXCEPT AS MODIFIED BY THIS DRAWING. THE SEALANT SHALL BE INSTALLED WHEN THE AMBIENT TEMPERATURE IS 40 DEGREES F OR HIGHER. TRAFFIC SHALL NOT BE ALLOWED ON THE JOINT FOR ONE HOUR AFTER APPLICATION OF THE SEALANT.

4) METHOD OF MEASUREMENT:

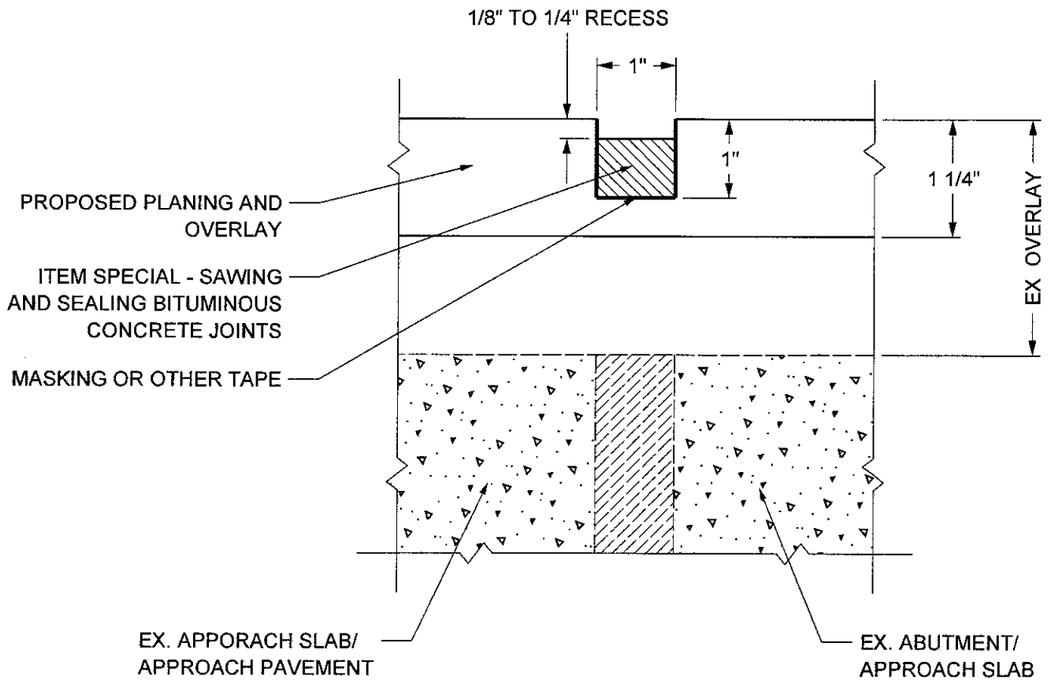
THE QUANTITY TO BE PAID FOR UNDER THIS ITEM WILL BE THE NUMBER OF LINEAR FEET OF JOINTS SAWED AND SEALED AS PER THE ABOVE REQUIREMENTS.

5) BASIS OF PAYMENT:

THE UNIT PRICE PER LINEAR FOOT FOR ITEM SPECIAL - "SAWING AND SEALING BITUMINOUS CONCRETE JOINTS" SHALL INCLUDE THE COST OF ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK, INCLUDING THE FURNISHING AND PLACING OF THE JOINT SEALER MATERIAL.

6) QUANTITY PROVIDED:

MAD-29-04.13
NUMBER OF JOINTS = 4 (BOTH ABUTMENTS AND APPROACH SLABS)
44' X 4 JOINTS = 176 LF
MAD-29-07.03
NUMBER OF JOINTS = 4
44' X 4 JOINTS = 176 LF
ITEM SPECIAL - "SAWING AND SEALING BITUMINOUS CONCRETE JOINTS" = 352 LF



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SAWING AND SEALING BITUMINOUS CONCRETE JOINTS

MAD-29-0.00

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PAVEMENT MARKING SUBSUMMARY

CALCULATED | CHECKED

LOCATION									LONG LINES					AUXILIARY MARKINGS													
									642					644							644						
PART	COUNTY	ROUTE	LOG BEG	LOG END	LENGTH		SIDE	DESCRIPTION	CENTER LINE	EQUIV SOLID LINE	EDGE LINE		LANE LINE	TRANSVERSE LINE		ISLAND (BULL-NOSE)	CHANL LINE	STOP LINE	CROSS WALK LINE	RAILROAD SYMBOL	LT	LT/ STR	STR	RT STR	RT	WORD ON PAVE 96"
											WHT	YEL		WHT	YEL											
			MI	MI	MI	LF			MI	MI	MI	MI	MI	LF	LF	SF	LF	LF	LF	EACH	EACH	EACH	EACH	EACH	EACH	EACH
1	MAD	29	0.00	12.97	12.97	68482		MAINLINE PAVEMENT	12.97	9.61	25.94															
1	MAD	29	9.57					INTERSECTION US 42										28								
1	MAD	29	12.72	12.92	0.20	1056		JEFF. IND. CORP. ENTRANCE	0.16								120				2					1 \ ONLY
1	MAD	29	12.98					INTERSECTION US 40			0.01	0.01						38								
TOTALS									13.13	9.61	25.95	0.01					120	66			2					1

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MATERIALS SUPPLIED BY THE DEPARTMENT:
ALL MATERIALS ARE TO BE CONTRACTOR FURNISHED, EXCEPT THAT THE DEPARTMENT SHALL SUPPLY RPM MATERIALS IN THE QUANTITIES SHOWN HEREIN TO THE CONTRACTOR. PAY ITEMS FOR THE DEPARTMENT SUPPLIED MATERIALS SHALL BE INDICATED AS "INSTALLATION ONLY". THE QUANTITY AND TYPE OF DEPARTMENT SUPPLIED MATERIALS ARE SHOWN ON THIS SHEET.

THE CONTRACTOR SHALL PICK UP THE DEPARTMENT SUPPLIED RPM MATERIALS AT THE OPI WAREHOUSE, 315 PHILLIPI ROAD, COLUMBUS, OHIO 43228. FOR SOME PROJECTS HAVING QUANTITIES OF LESS THAN 20 RPMS, THE CONTRACTOR MAY PICK UP RPM MATERIALS AT THE DISTRICT OFFICES. QUANTITIES OVER 20 RPMS WILL BE PICKED UP AT THE RECYCLER'S WAREHOUSE OR AS ARRANGED WITH THE DISTRICT. THE CONTRACTOR SHALL PICK UP DEPARTMENT SUPPLIED RPM MATERIALS AT THE SPECIFIED LOCATION(S) FOR TRANSPORT TO THE WORK SITE OR TO THE CONTRACTOR'S STORAGE FACILITY. THE RECYCLED RAISED PAVEMENT MARKER (RPM) AUTHORIZATION FORM IS TO BE SIGNED BY THE DISTRICT CONSTRUCTION ENGINEER PRIOR TO PICK UP OF THE RPMS. THE CONTRACTOR SHALL NOTIFY THE DISTRICT AND/OR THE PARTIES LISTED ON THE AUTHORIZATION FORM IN WRITING AT LEAST FIVE (5) CALENDAR DAYS PRIOR TO PICK UP OF THE DEPARTMENT SUPPLIED MATERIALS. THE CONTRACTOR SHALL STORE THE RPMS WITHOUT DAMAGE OR CONTAMINATION WITH FOREIGN MATTER. A DEDUCTION IN THE AMOUNT OF THE ACTUAL COST TO THE DEPARTMENT SHALL BE MADE FOR MATERIALS DAMAGED BY THE CONTRACTOR OR FOR CASTINGS RECEIVED BY THE CONTRACTOR WHICH WERE NOT INSTALLED AND WERE NOT RETURNED TO THE DEPARTMENT.

RETURN OF NON-PERFORMED RAISED PAVEMENT MARKER MATERIALS SUPPLIED BY THE DEPARTMENT:
RAISED PAVEMENT MARKER MATERIALS SUPPLIED BY THE DEPARTMENT, THAT ARE NON-PERFORMED SHALL BE CAREFULLY REPACKED OR PACKED IN THE BOXES IN THE SAME STYLE AND QUANTITY AS ORIGINALLY RECEIVED FROM THE DEPARTMENT. CASTING STYLES SHALL NOT BE MIXED WITHIN ANY ONE CONTAINER. THE CONTRACTOR SHALL CLEARLY MARK ON THE OUTSIDE OF EACH CONTAINER, THE COLOR OF THE PRISMATIC RETRO-REFLECTOR, THE STYLE OF CASTING. BOXES SHALL BE PLACED ON SKIDS OR PALLETS IN THE SAME STYLE (LOW PROFILE OR CONVENTIONAL, REFLECTORISED OR NON REFLECTORISED) AND NO MORE THAN 420 RPMS (OR 21 BOXES) ON ONE SKID.

ONLY USE THE BOXES SUPPLIED BY THE RAISED PAVEMENT MARKER RECYCLER. BOXES MUST BE MARKED WITH THE RECYCLER'S PART OR CATALOG NUMBER AND THE PROJECT NUMBER. THE RECYCLER'S CATALOG OR PART NUMBERS MAY BE OBTAINED FROM THE OFFICE OF TRAFFIC ENGINEERING IN COLUMBUS, OHIO OR FROM THE RECYCLER. BOXES NOT MARKED WITH THE PROPER RECYCLER'S CATALOG OR PART NUMBERS, AND THE DEPARTMENT'S PROJECT NUMBER WILL NOT BE ACCEPTED AT THE RECYCLER'S WAREHOUSE.

NON PERFORMED MATERIALS WILL BE RETURNED TO THE LOCATION AS SPECIFIED BY THE DISTRICT CONSTRUCTION ENGINEER WITHIN 30 DAYS OF THE COMPLETION OF THE PROJECT.

THE ABOVE WORK INCLUDING ALL LABOR, EQUIPMENT AND MATERIAL NEEDED TO PERFORM THE WORK, SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE PAY ITEM.

IF THE DEPARTMENT HAS TO REPACKAGE THE RPMS CORRECTLY, THE CONTRACTOR WILL BE ASSESSED THE ACTUAL COST FOR REPACKAGING THE MATERIALS BY THE DEPARTMENT'S FORCES.

LOADING OF MATERIALS SUPPLIED BY THE DEPARTMENT AT THE RECYCLER'S WAREHOUSE:

TRUCKS SHALL HAVE A LOADING HEIGHT OF 48 INCHES AND BE ABLE TO BACK UP FLUSH TO THE LOADING DOCK.
TRUCKS SHALL NOT HAVE ANY OBSTRUCTIONS OR PROTRUSIONS THAT PREVENT THE LOADING BY A STANDARD FORKLIFT OR LIFT TRUCK.

SEMI TRUCKS OR 20 FOOT COMMERCIAL TRUCKS ARE THE MOST APPROPRIATE TRUCKS FOR LOADS IN EXCESS OF 4 PALLETS (ONE PALLET = 21 BOXES = 2100 LBS).

STAKE BODY TRUCKS ARE APPROPRIATE TO LOAD LESS THAN 4 PALLETS, PROVIDED THE TRUCK IS RATED FOR THE LOAD AND THE LOAD CAN BE SAFELY SECURED FOR TRANSPORT BY CHAINING OR STRAPPING DOWN AS NEEDED.

PICKUP TRUCKS ARE APPROPRIATE FOR LOADS OF APPROXIMATELY ONE PALLET, PROVIDED THE PICKUP TRUCK IS RATED FOR THE LOAD AND THE LOAD CAN BE SAFELY SECURED FOR TRANSPORT.

THE RECYCLERS WAREHOUSE WILL NOT LOAD DUMP TRUCKS, TILT BED TRUCKS, AND NON COMMERCIAL MOVING VANS.
THE WAREHOUSE SUPERVISOR WILL REFUSE TO LOAD ANY TRUCK THAT IS UNSAFE TO LOAD OR UNSUITABLE FOR THE LOAD BEING PLACED ON THE TRUCK.

DETAIL		DETAIL	
1	TYPICAL CENTER LINE	7	ONE LANE BRIDGE
2	ACCELERATION LANE	8	STOP APPROACH
3	DECELERATION LANE	9	TWO WAY LEFT TURN LANE
4	4 LANE DIVIDED TO 2 LANE TRANSITION	10	APPROACH W/LEFT TURN LANE
5	4 LANE UNIDIVIDED TO 2 LANE TRANSITION	11	HORIZONTAL CURVE W/RADIUS LESS THAN 1250'
6	MULTILANE DIVIDED-CONTROLLED ACCESS	12	HORIZONTAL CURVE W/RADIUS LESS THAN 820'

LOCATION						REFLECTOR TYPE												TOTAL			REMARKS
						ONE-WAY				TWO-WAY								202	621	621	
						WHITE			YELLOW	WHITE WHITE	WHITE RED			YELLOW RED	YELLOW YELLOW			RAISED PVMT MARKER RMVD FOR STORAGE	RAISED PVMT MARKER INSTALL ONLY	PRIS-MATIC RETRO-REFLECT	
PART	COUNTY	ROUTE	LOG BEG	LOG END	DETAIL	RIGHT EDGE LINE		LANE LINE	LEFT EDGE LINE	RIGHT EDGE LINE	RIGHT EDGE LINE	CHANNELIZING LINE		LANE LINE	LEFT EDGE LINE	CENTER LINE					
			MI	MI		40'	80'	120'	80'	40'	80'	40'	80'	80'	80'	20'	40'	80'	EACH	EACH	EACH	
1	MAD	29	0.00	0.37	1													24	897	24	24	CENTER LINE 29
1	MAD	29	0.37	0.43	11												8			8	8	CENTER LINE 29
1	MAD	29	0.43	6.19	1													380		380	380	CENTER LINE 29
1	MAD	29	6.19	6.22	11												4			4	4	CENTER LINE 29
1	MAD	29	6.22	12.72	1													429		429	429	CENTER LINE 29
1	MAD	29	9.57		8	32												12		44	44	INTERSECTION SR.42
1	MAD	29	12.72	12.92	10							3						24		27	27	JEFFERSON IND. CORP. ENTRANCE
1	MAD	29	12.98		8	18			2									6		26	26	INTERSECTION US.40
TOTALS																			897	942	942	

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PAVEMENT SUBSUMMARY AND DETAILS

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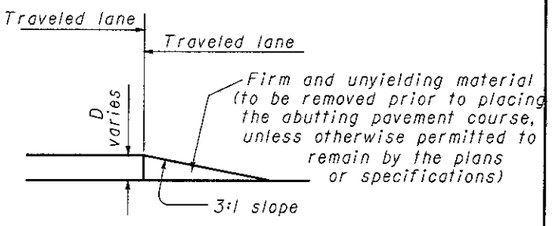
GENERAL NOTES

1. It is intended that this drawing be used for treatment of drop-offs that develop during construction operations, and that are not otherwise provided for in the construction plans. Where the plans do not provide specific items for labor, equipment, or materials to implement the drop-off treatments specified hereon, they shall be included for payment in the lump sum bid for Item 614 - Maintaining Traffic.
2. While the need for certain advisory signing is noted hereon, it is not intended that this be indicative of all signing that may be required to advise or warn motorists, and all requirements of the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) must be fulfilled.
3. In urban or otherwise heavily developed areas where pedestrians and/or bicyclists may be present in significant numbers, additional signing and protective measures other than those shown hereon may be required.
4. The drop-off treatment selected for use at any given location shall be as appropriate for the prevailing conditions at the site.
5. Where concrete barrier is specified, it shall be in accordance with Standard Construction Drawing MC-9.2 and Item 622.
6. When drums are specified for a dropoff condition, a minimum number of four drums shall be used. Spacing shall be as indicated in the plans or as specified in the OMUTCD.
7. When OW-15I (Low Shoulder) signs or OW-17I (Uneven Lanes) and OWP-17I signs are required, they shall be placed 750' in advance of the condition, on all intersecting entrance ramps within the limits of the condition and immediately beyond all intersecting roadways within the limits of the condition. When the dropoff condition extends more than one-half mile, additional signs should be erected at intervals of one mile or less.
8. For locations, such as at ramps, lane shifts, lane closures, etc., where traffic is required to negotiate any difference in elevation between pavements, a 3:1 slope treatment similar to the Optional Wedge Treatment shall be provided.
9. Portable concrete barrier shall be placed on the same level as the traffic surface and shall not encroach on lane width(s) designated as the minimum required for traffic use. Where drums are used, and their presence would reduce traveled lane widths to less than 10', drums may be placed on the opposite level from that of traffic provided the dropoff depth does not exceed 5" and approval is granted by the Project Engineer.
10. Pavement Repairs (or similar work):
    a. Lengths greater than 60 feet - utilize appropriate treatment from Condition I.
    b. Lengths of 60 feet or less - repairs shall be effected in accordance with 255.08. Drums may be used as a separator adjacent to the traveled lane.

OPTIONAL WEDGE TREATMENT
(MILLING OR RESURFACING)

1. This treatment may be used when permitted for Condition I only.
2. OW-17I and OWP-17I signs required.

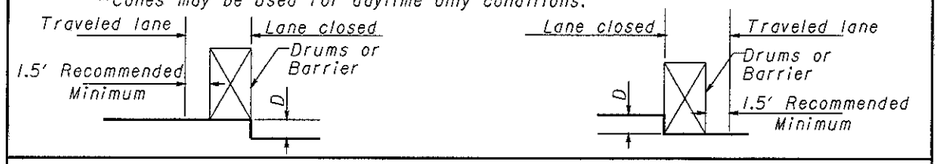


CONDITION I
DROPOFFS BETWEEN TRAVELED LANES

1. These treatments are to be used for resurfacing, pavement planing, excavation, etc. between or within traveled lanes.

D (In.)	Treatment
≤1½	Erect OW-17I and OWP-17I signs.
>1½-3	1) Lane closure utilizing drums* as shown below OR 2) Optional Wedge Treatment
>3-5	Lane closure utilizing drums as shown below.
>5	Lane closure utilizing portable concrete barrier as shown below.

*Cones may be used for daytime only conditions.

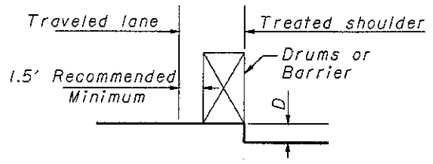


CONDITION II
DROPOFFS WITHIN GRADED SHOULDER AREA

1. The treatments indicated below are for use in conjunction with resurfacing, planing, or excavations within the graded shoulder area.
2. The graded shoulder area is that flat or gradually sloping area between the edge of a normally traveled lane and the more steeply sloping ditch foreslope or embankment slope. Its surface may be soil or turf, and/or it may be inclusive of a "treated" area (improved with aggregates, asphaltic materials, or concrete). For the purposes herein, its maximum width shall be considered to be twelve (12) feet.

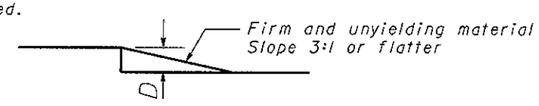
D (In.)	Treatment
≤1½	1) If edgelines are present, no treatment necessary OR 2) Erect OW-17I and OWP-17I signs.
>1½-5	1) If min. lane width* requirements can be met, maintain lanes utilizing drums as shown below OR 2) If min. lane width* requirements cannot be met, close adjacent lane utilizing drums OR 3) Optional Shoulder Treatment.
>5-12 Daylight only	If min. lane width* requirements can be met, maintain lanes utilizing drums as shown below.
>5-24	1) If min. lane width* requirements can be met, maintain lanes utilizing portable concrete barrier as shown below. OR 2) If min. lane width* requirements cannot be met, close adjacent lane utilizing drums.
>24	Lane closure utilizing portable concrete barrier as shown below.

* Minimum lane widths shall be 10' unless otherwise specified in the plans.



OPTIONAL SHOULDER TREATMENT

1. This treatment may not be used within a bituminous shoulder where a hot longitudnal joint per 401.15 is required.
2. OW-15I signs required.

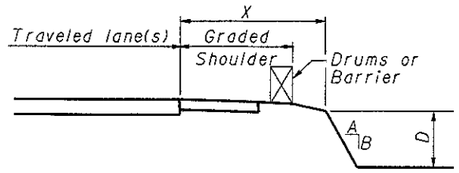


CONDITION III
DROPOFFS BEYOND GRADED SHOULDER OR BACK OF CURB

1. See Note 2 under Condition II.
2. Use Chart A or B below, as applicable.

CHART A

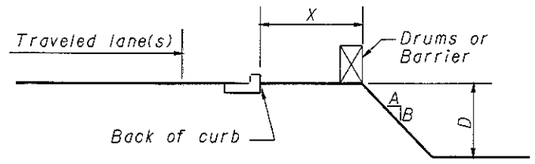
USE FOR:
1. Uncurbed Facilities.
2. Curbed Facilities, where:
   a. Curbs are less than 6" in height.
   b. Curbs are 6" or greater in height and the legal speed is greater than 40 mph.



X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
0-4	Any	Any	(a)	(a)
4-30	Any	3:1 or Flatter	None	None
4-12	≤3	Steeper than 3:1	None	None
4-12	>3-≤12	Steeper than 3:1	Drums	Drums
4-12	>12	Steeper than 3:1	Drums	Barrier
>12-20	≤12	Steeper than 3:1	None	None
>12-20	>12-≤24	Steeper than 3:1	Drums	Drums
>12-20	>24	Steeper than 3:1	Drums	Barrier
>20-30	≤24	Steeper than 3:1	None	Drums
>20-30	>24	Steeper than 3:1	Drums	Barrier
>30	Any	Any	None	None
(a) Use treatment specified under Condition II.				

CHART B

USE FOR: Curbed facilities, where the curb is 6" or greater in height and the legal speed is 40 mph or less.



X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
0-10	<12	Any	None	Drums
0-10	>12	Any	Drums	Drums
>10	Any	Any	None	None

CALCULATED
CHECKED

DROPOFFS IN WORK ZONES

MAD-29-0.00

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13

16-JUN-2000 2:45PM bkinsell

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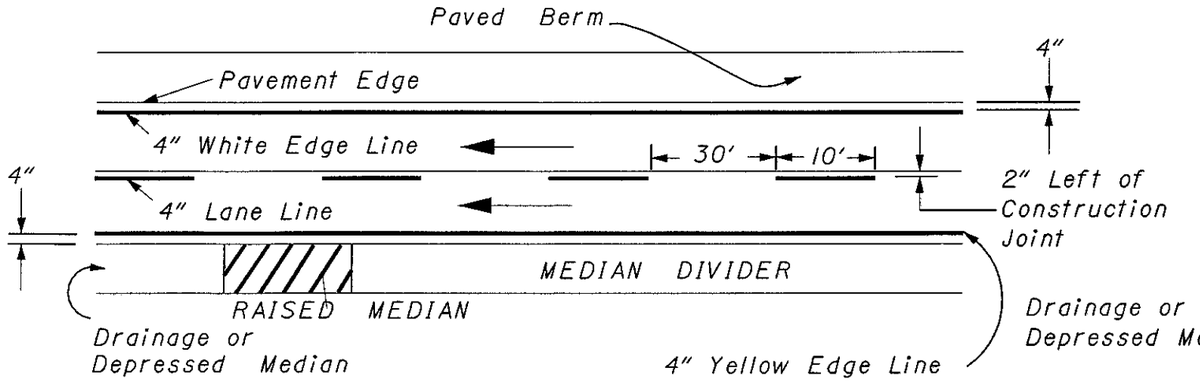
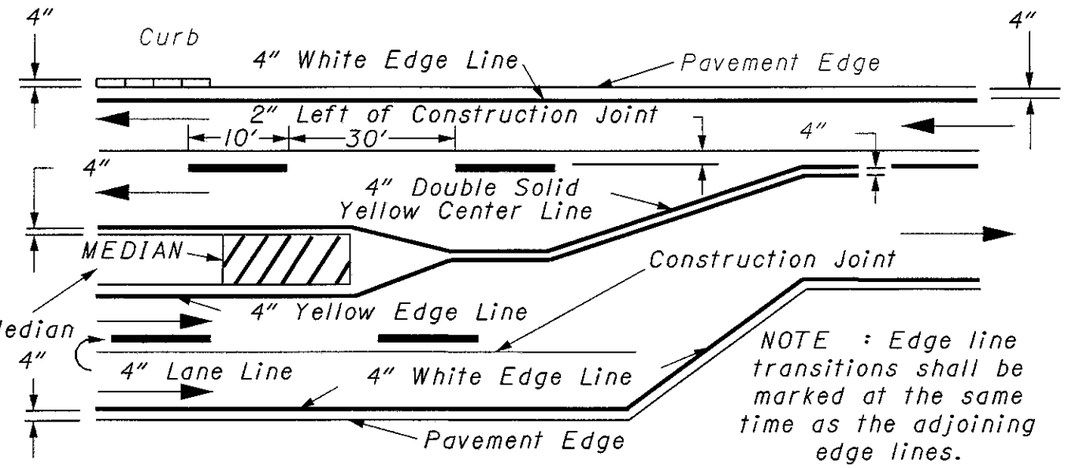
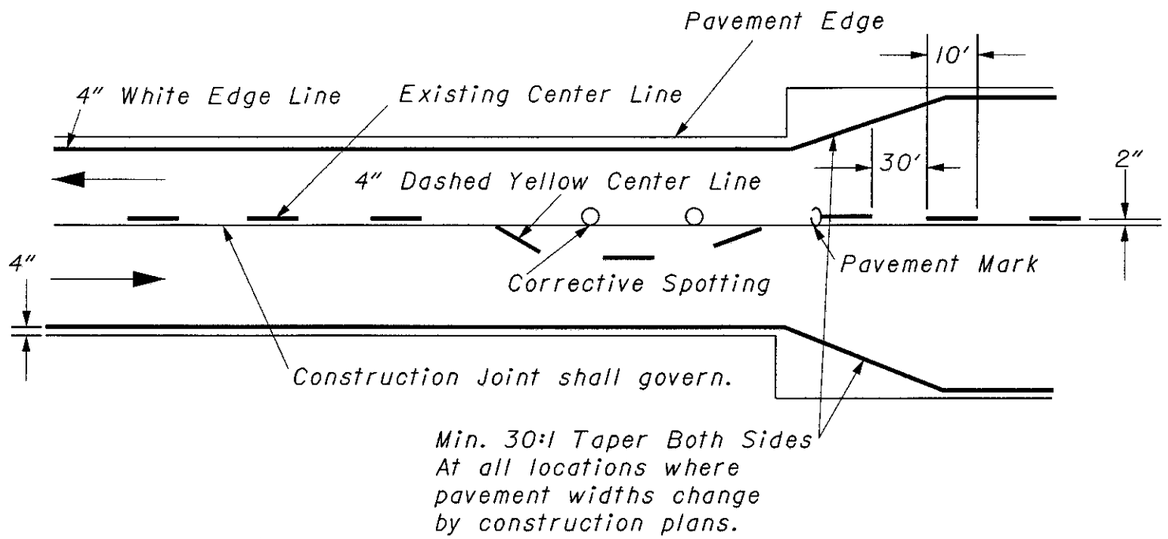
FREEWAY & EXPRESSWAY MAINLINE MARKINGS



MULTILANE DIVIDED & UNDIVIDED HIGHWAY MARKINGS



TWO LANE MARKINGS



NOTES :

1. The distance from the pavement edge to the nearside edge of the edgeline may be increased with the approval of the engineer in order to maintain uniform lane width.

2. See TC-72.20M for entrance and exit ramp markings.

3. The cycle length for dashed lines shall be 10 feet plus or minus 6". The minimum length of dash shall be sufficiently long to maintain a 3:1 ratio between length of gap and length of dash.

CALCULATED | CHECKED

PLAN NO.

PAVEMENT MARKING TYPICAL DETAILS

MAD-29-0.00

13
13