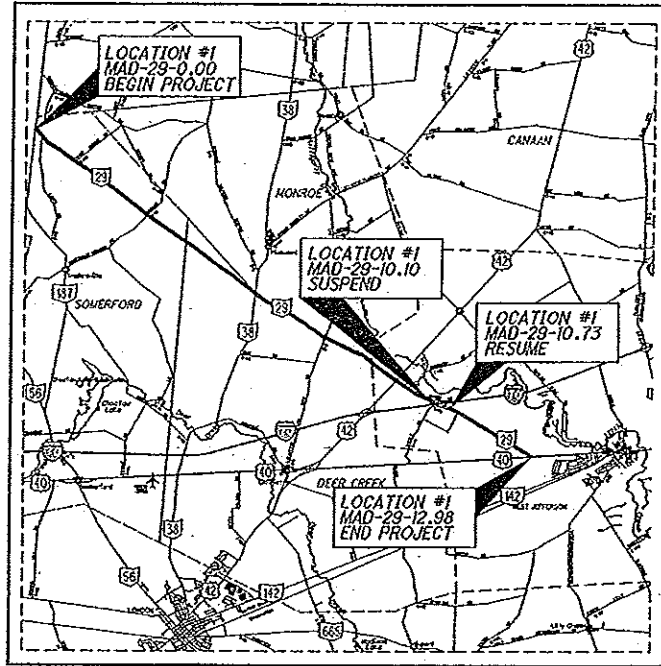


STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

MAD-29-0.00

VILLAGE OF WEST JEFFERSON

SOMERFORD TOWNSHIP, DEER CREEK TOWNSHIP,
JEFFERSON TOWNSHIP, WEST JEFFERSON TOWNSHIP
MADISON COUNTY



LOCATION MAP

LAT: N 39° 59' 21" / LONG: W 83° 24' 45"
LAT: / LONG = CENTER OF PRIMARY ROUTE
SCALE IN MILES 1" EQUALS 2 MILES



PORTIONS TO BE IMPROVED
INTERSTATE & DIVIDED HIGHWAY
UNDIVIDED STATE & FEDERAL ROUTES
OTHER ROADS

LIMITED ACCESS

CURRENT ADT (2010)	3,759
DESIGN YEAR ADT (2020)	4,582
DESIGN HOURLY VOLUME (2020)	191
DIRECTIONAL DISTRIBUTION	50%
TRUCKS (24 HOUR B & C)	12%
DESIGN SPEED	60
LEGAL SPEED	55 MPH
LANE ADTT	249

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EARTH DISTURBANCE AREA

PROJECT EARTH DISTURBED AREA	N/A*
EST. CONTRACTOR EARTH DISTURBED AREA :	N/A*
NOTICE OF INTENT EARTH DISTURBED AREA	N/A*

DESIGN FUNCTIONAL CLASSIFICATION

MAD-29-0.00 RURAL MAJOR COLLECTOR

DESIGN EXCEPTIONS

NONE REQUIRED

PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF PAVEMENT REPAIRS AND RESURFACING OF SR-29 FOR A TOTAL OF 12.31 MILES WITHIN MADISON COUNTY.

2010 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS PROJECT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

UNDERGROUND UTILITIES	
CONTACT BOTH SERVICES CALL TWO WORKING DAYS BEFORE YOU DIG	
CALL 1-800-362-2764 (TOLL FREE)	CALL 1-800-362-2764 (TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE NON-MEMBERS MUST BE CALLED DIRECTLY	
OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE CALL: 1-800-925-0988	

PLANS CERTIFIED BY:
NAME: *Richard Kivell* DATE: 11/15/2011
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT SIX PRODUCTION

NAME: *Steve Fellenger* DATE: 12-27-11
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT DEPUTY DIRECTOR OF TRANSPORTATION

NAME: *Jerry Whayff* DATE: 2-12-11
OHIO DEPARTMENT OF TRANSPORTATION
DIRECTOR, DEPARTMENT OF TRANSPORTATION

STANDARD DRAWINGS

SUPPLEMENTAL SPECIFICATIONS

STANDARD DRAWINGS	SUPPLEMENTAL SPECIFICATIONS	PLANS PREPARED BY:
BP-3.1 10/19/2007	800-2010 1/20/2012	STEVEN D FELLENGER #71781 REGISTERED PROFESSIONAL ENGINEER
BP-4.1 7/16/2004	832 5/5/2009	
RM-1.1 7/15/2011		NAME: <i>Steve Fellenger</i> DATE: 11/15/2011
MT-35.10 4/20/2001		
MT-95.31 7/17/2009		SPECIAL PROVISIONS
MT-95.32 7/17/2009		
MT-97.12 10/15/2010		PLANS PREPARED BY: O.D.O.T. DISTRICT SIX IN HOUSE DESIGN
MT-98.10 7/17/2009		
MT-101.90 10/21/2011		
MT-105.10 1/16/2009		
TC-41.20 1/19/2001		
TC-52.10 1/19/2007		
TC-52.20 1/19/2007		
TC-65.10 1/21/2005		
TC-65.11 1/21/2005		
TC-71.10 1/21/2011		
TC-73.10 1/21/2011		
TC-82.10 1/21/2011		

MAD - SR-29-0.00
120292 PID - 85993
Dist 6 5/10/2012

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Contract Proposal Available
@ www.contracts.dot.state.oh.us/home

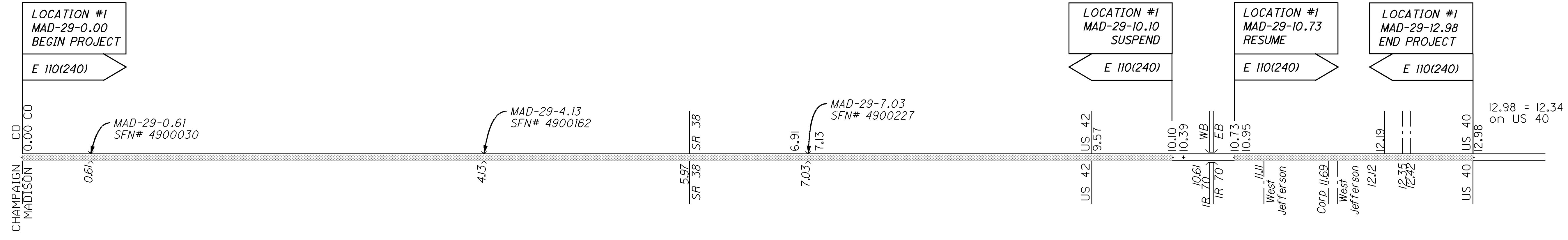
FEDERAL PROJECT NO. E 110(240)

PID NO. 85993

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT NONE

MAD - 29 - 0.00



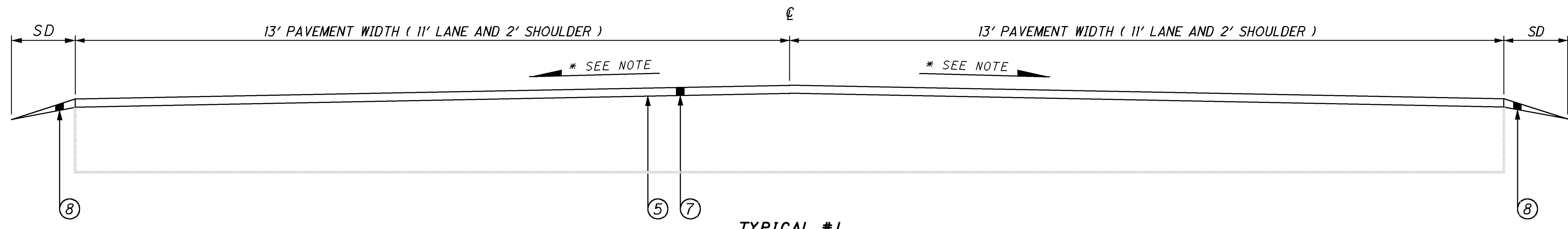
LOCATION #1
MAD-29 SLM 0.00 TO SLM 12.98

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	DATE

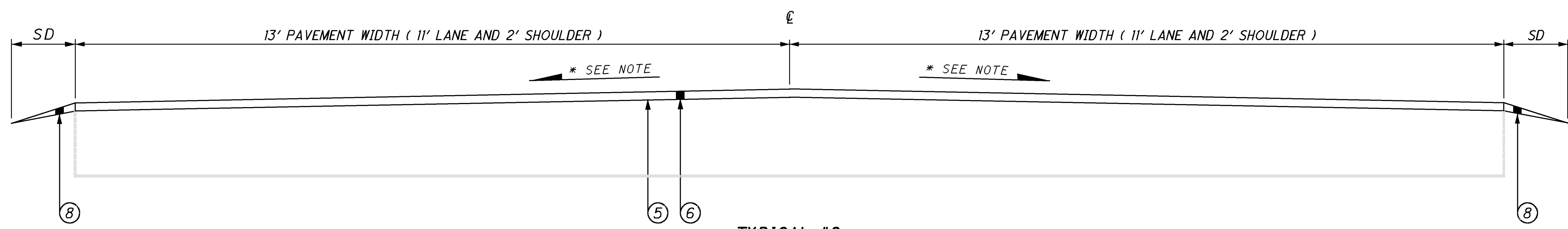
STRAIGHT LINE DIAGRAM

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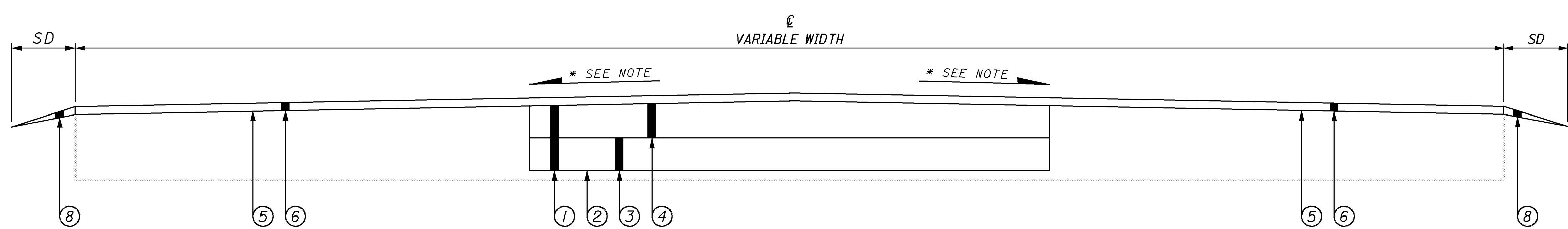
TYPICAL NOTES
SD = SEE SHOULDER DETAIL ON PLAN SHEET 4/46
* SLOPE = MAINTAIN EXISTING PAVEMENT CROSS SLOPE
FOR ADDITIONAL INFORMATION ON PARTIAL DEPTH PAVEMENT REPAIRS, PAVEMENT REPAIR,
AND COMPACTED AGGREGATE SEE PLAN SHEETS 6/46 AND 7/46



TYPICAL #1
MAD SR-29 SLM 0.00 TO SLM 9.09



TYPICAL #2
MAD SR-29 SLM 9.09 TO SLM 10.425
MAD SR-29 SLM 10.425 TO SLM 10.917 (3' SHOULDERS & 12' LANES)
MAD SR-29 SLM 10.917 TO SLM 12.98



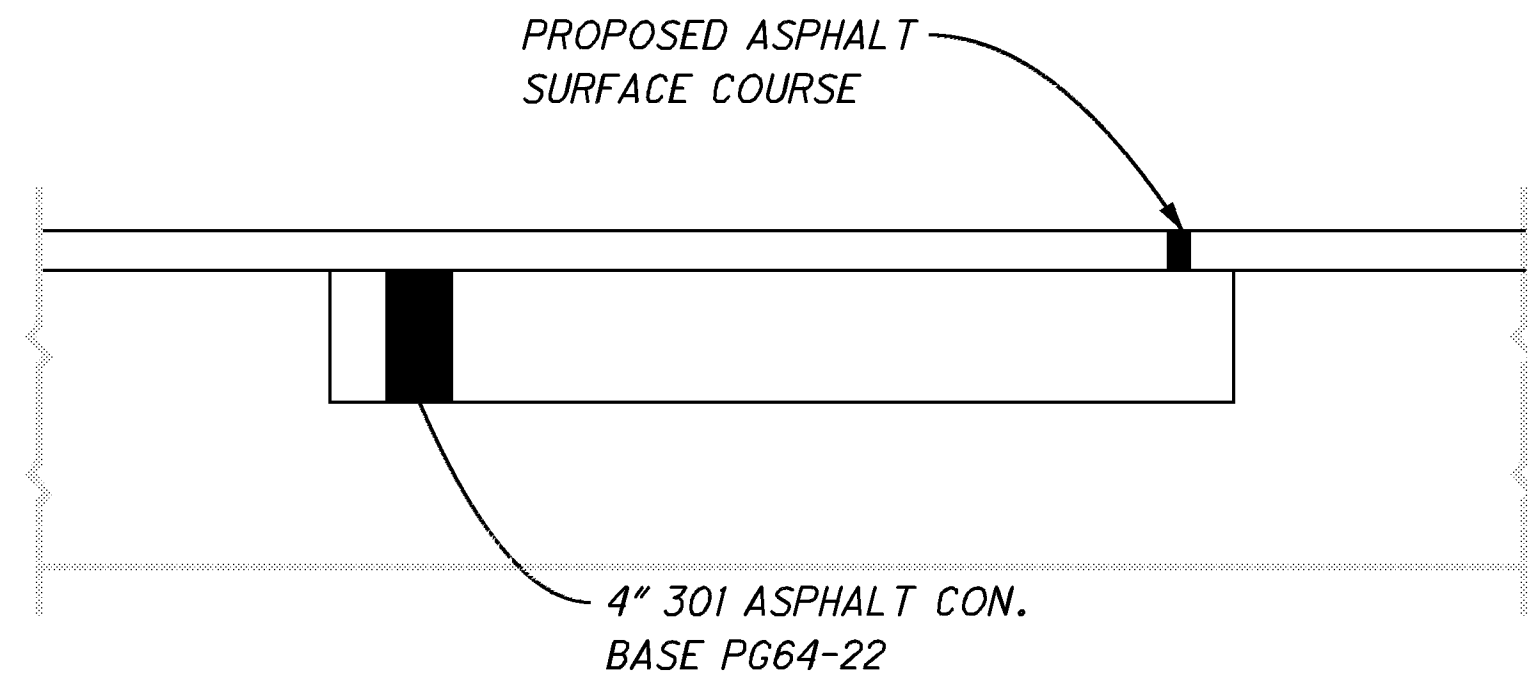
TYPICAL #3
INTERSECTION SR-29 & US40 ISLAND REMOVAL AND REPLACEMENT

TYPICAL LEGEND

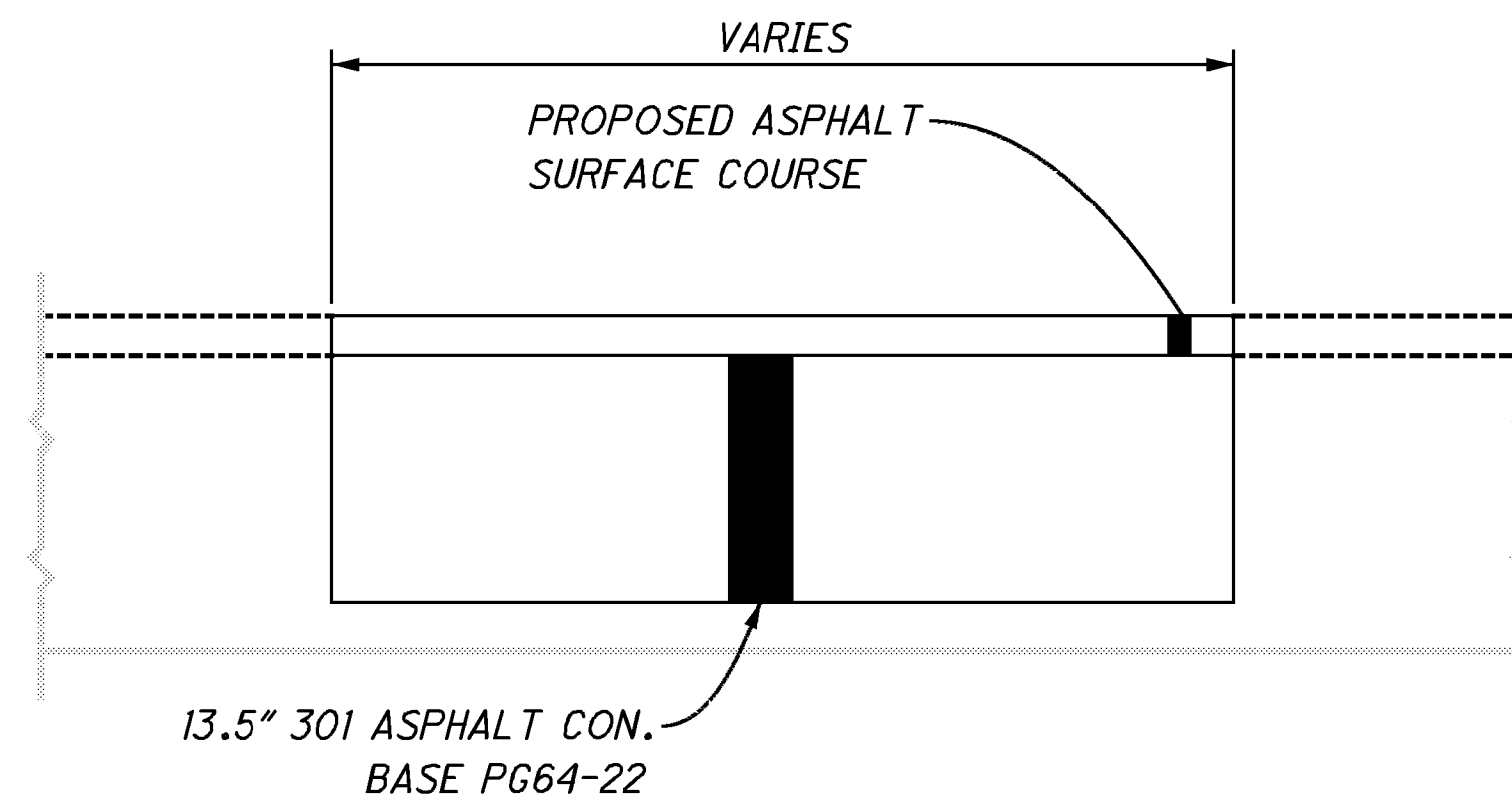
- | | |
|--|--|
| ① ITEM 203 - EXCAVATION 12" DEPTH | ⑤ ITEM 407 - TACK COAT @ 0.075 PER SY. YD. |
| ② ITEM 204 - SUBGRADE COMPACTION | ⑥ ITEM 442 - 1.5" ASPHALT CONCRETE SURFACE COURSE, 12.5mm, TYPE A, (448) |
| ③ ITEM 304 - 6" AGGREGATE BASE | ⑦ ITEM 448 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 |
| ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22 | ⑧ ITEM 617 - COMPACTED AGGREGATE |

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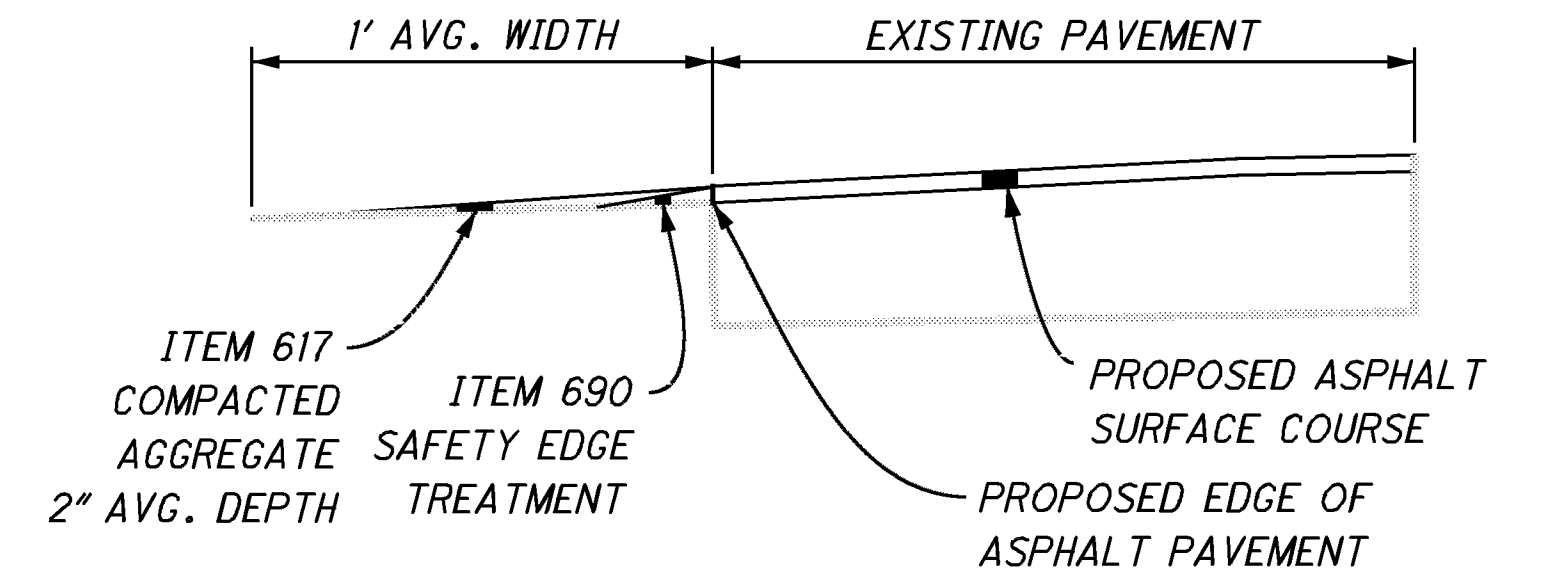
PARTIAL DEPTH
PAVEMENT REPAIR
A.P.P. DETAIL



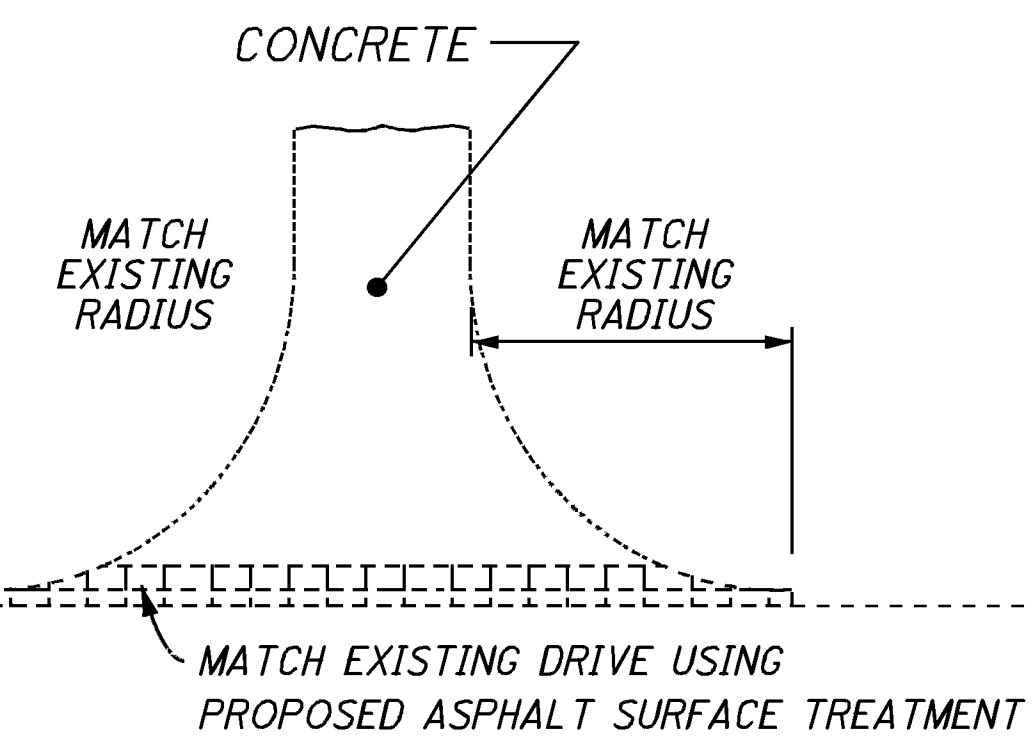
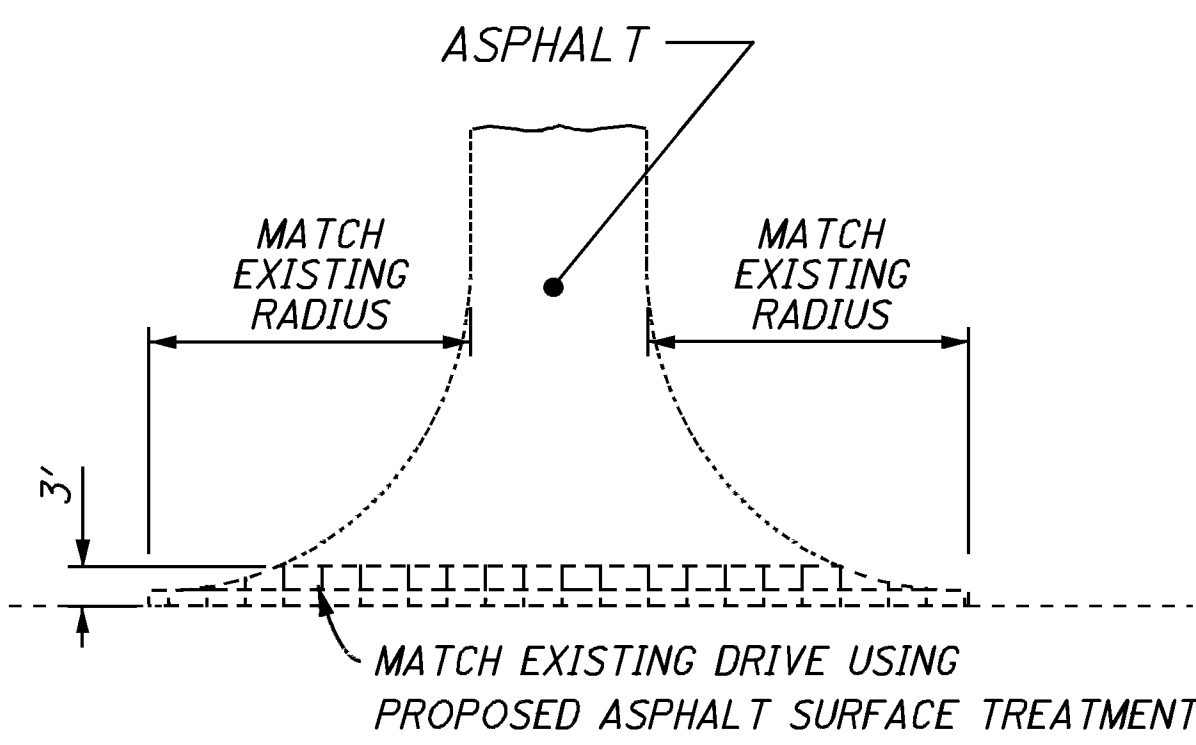
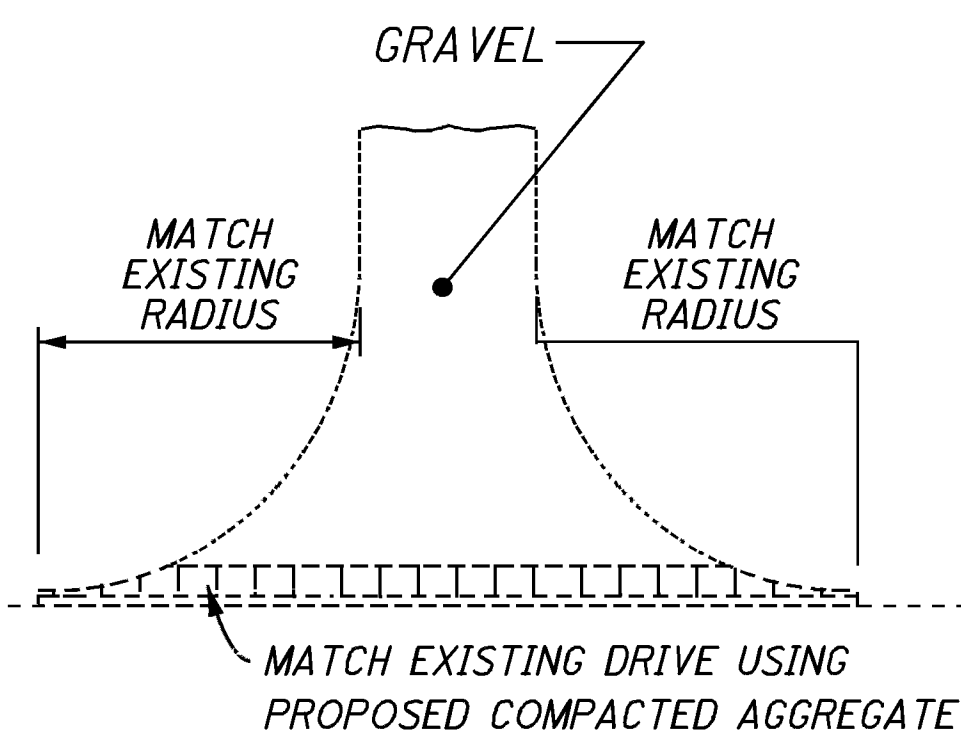
PAVEMENT REPAIR
A.P.P. DETAIL



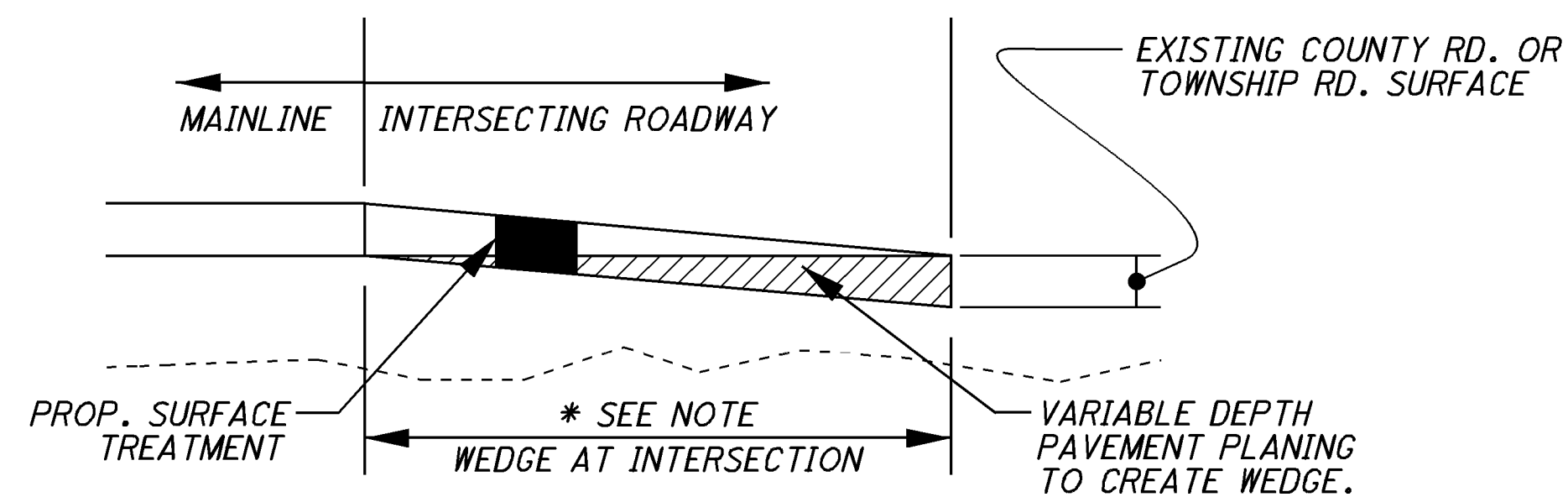
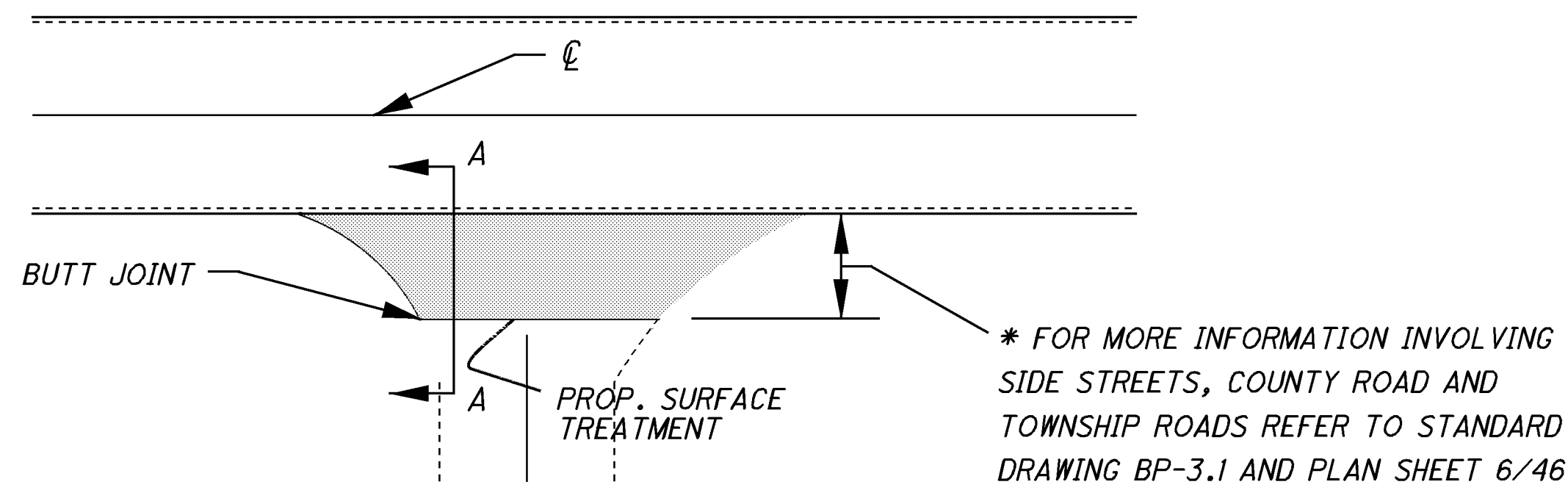
SHOULDER DETAIL



FOR ADDITIONAL INFORMATION ON PARTIAL DEPTH PAVEMENT REPAIRS, PAVEMENT REPAIR,
AND COMPACTED AGGREGATE SEE PLAN SHEETS 6/46 AND 7/46



TYPICAL DRIVE TAPERS DETAILS
FOR MORE INFORMATION INVOLVING DRIVE TAPERS REFER TO
STANDARD DRAWING BP-3.1 AND PLAN SHEETS 6/46



SECTION A-A

TYPICAL SIDE STREET, COUNTY AND TOWNSHIP ROAD DETAIL

COUNTY AND TOWNSHIP ROAD INTERSECTIONS TO BE PLANED AND PAVED

- TOWNSHIP ROAD 118 (WARE RD.)
- COUNTY ROAD 117 (LEWIS RD.)
- COUNTY ROAD 119 (DAVIS BROWN RD.)
- COUNTY ROAD 113 (TRADERSVILLE BRIGHTON RD.)
- TOWNSHIP ROAD 121 (THOMAS RD.)
- TOWNSHIP ROAD 290 (MAPLEWOOD RD.)
- COUNTY ROAD 5 (LAFAYETTE PLAIN CITY RD.)
- TOWNSHIP ROAD 116 (LITTLE DARBY RD.)
- TOWNSHIP ROAD 137 (BYERLY MILL RD.)
- TOWNSHIP ROAD 117 (SNYDER RD.)
- TOWNSHIP ROAD 115 (BYERLY RD.)
- MUNICIPAL ROAD 134 (COMMERCE PARKWAY RD.)

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PAVEMENT REPAIR, INTERSECTION AND DRIVEWAY
TYPICAL SECTION

MAD-29-0.00

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NOTIFICATION OF CONSTRUCTION INITIATION

AT LEAST FOURTEEN DAYS PRIOR TO ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS VIA EMAIL AT d06.pio@dot.state.oh.us AND THE DISTRICT WORK ZONE TRAFFIC MANAGER VIA EMAIL AT d06.mot@dot.state.oh.us OF THE ANTICIPATED START DATE OF ANY CONSTRUCTION ACTIVITIES, INCLUDING BUT NOT LIMITED TO THE PLACING OF WORK ZONE SIGNS. THE NOTIFICATION SHALL ALSO INCLUDE THE PROJECT NUMBER, PID, NAME AND PHONE NUMBER OF THE CONTRACTOR, A POINT OF CONTACT AND THE ANTICIPATED IMPACT ON TRAFFIC. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT WORK ZONE TRAFFIC MANAGER OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION INITIATION DATE.

PUBLIC OUTREACH AND NOTIFICATION:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE DISTRICT 6 PUBLIC INFORMATION OFFICE AT (740) 833-8260 TO COORDINATE EFFORTS TO NOTIFY ADJACENT RESIDENTS AND BUSINESSES OF THE UPCOMING RESURFACING PROJECT. ADVANCE NOTIFICATION SHALL OCCUR NO LATER THAN FOURTEEN (14) DAYS PRIOR TO THE FIRST DAY OF WORK.

GENERAL:

THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 108.02) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

ALIGNMENT AND PROFILE:

THE WORK PROPOSED BY THIS PROJECT IS FOR THE RESURFACING OF THE EXISTING PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED, AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

CONTINGENCY QUANTITIES:

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

WORK LIMITS:

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

FIRE HYDRANTS:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY PERMITS AND FEES THAT ARE REQUIRED FOR THE USE OF ANY FIRE HYDRANTS. A SIAMESE VALVE IS TO BE USED ON THE HYDRANT OUTLET IF A HOSE IS TO BE LEFT CONNECTED AND UNATTENDED.

DRAINAGE AT INTERSECTING STREETS:

AT INTERSECTING STREETS WHERE THE DRAINAGE IS TOWARD OR INTO THE PROJECT, SPECIAL CARE SHALL BE TAKEN BY THE CONTRACTOR TO MAINTAIN PROPER GRADE ALONG THE EDGE OF PAVEMENT SO THAT WATER WILL NOT POND. AT INTERSECTING STREETS, WHERE THE EDGE OF PAVEMENT CONTINUES ACROSS THE STREET, CARE SHALL BE TAKEN TO FEATHER DOWN AND FORM A NEAT SEAM WITH THE PROPER GRADE.

SIDE STREETS, COUNTY AND TOWNSHIP ROADS:

ESTIMATED QUANTITIES HAVE BEEN PROVIDED WITHIN THIS PLAN TO BE USED AS DIRECTED BY THE PROJECT ENGINEER TO PAVE THE EXISTING SIDE STREETS, COUNTY AND TOWNSHIP ROADS TRANSITION AREAS AS SHOWN ON PLAN SHEET 4/47. THE SIDE STREETS, COUNTY AND TOWNSHIP ROADS TRANSITION AREAS SHALL EXTEND A MINIMUM OF TWENTY FIVE FEET OR A DISTANCE GREATER THAN TWENTY FIVE BUT NOT GREATER THAN FORTY FEET WHICH, SHALL BE DETERMINED BY THE PROJECT ENGINEER, TO PROVIDE A SMOOTH TRANSITION BETWEEN THE EXISTING SIDE STREETS, COUNTY AND TOWNSHIP ROADS AND THE PROPOSED ASPHALT CONCRETE SURFACE COURSE. SIDE STREETS, COUNTY AND TOWNSHIP ROADS THAT ARE LOCATED IN CURB SECTIONS DO NOT REQUIRE PAVING. ALL PLANING, GRADING, TACK COAT, LABOR, MATERIAL, TOOLS, EQUIPMENT, INCIDENTALS SHALL BE INCLUDED IN THE PRICE BID FOR THEIR RESPECTIVE ITEMS OF WORK

RESIDENCE AND COMMERCIAL DRIVEWAYS:

ESTIMATED QUANTITIES HAVE BEEN PROVIDED WITHIN THIS PLAN TO BE USED AS DIRECTED BY THE PROJECT ENGINEER TO PAVE THE EXISTING DRIVE TAPER AREAS AS SHOWN ON PLAN SHEET 4/47. THE DRIVE TAPER PAVING SHALL EXTEND A MINIMUM OF THREE FEET OR A DISTANCE GREATER THAN THREE FEET WHICH, SHALL BE DETERMINED BY THE PROJECT ENGINEER, TO PROVIDE A SMOOTH TRANSITION BETWEEN THE EXISTING DRIVE AND THE PROPOSED ASPHALT CONCRETE SURFACE COURSE.

THE CONTRACTOR SHALL PROVIDE A BUTT JOINT AT ALL EXISTING ASPHALT OR CONCRETE DRIVES. SHOULD AN EXISTING ASPHALT DRIVE TAPER BE CONSIDERED BY THE PROJECT ENGINEER TO BE IN TOO POOR CONDITION (LOOSE AND BROKEN UP MATERIAL) TO BE NORMALLY PAVED OVER. THE CONTRACTOR SHALL REMOVE AND REPLACE THE MINIMAL AMOUNT OF UNACCEPTABLE ASPHALT (LOOSE AND BROKEN UP MATERIAL) TO PROVIDE A REASONABLE TRANSITION BETWEEN THE EXISTING DRIVE AND THE PROPOSED ASPHALT CONCRETE SURFACE COURSE. DRIVEWAYS THAT ARE LOCATED IN CURB SECTIONS DO NOT REQUIRE PAVING. ALL PLANING, GRADING, TACK COAT, LABOR, MATERIAL, TOOLS, EQUIPMENT, INCIDENTALS AND THE REMOVAL OF LOOSE AND BROKEN UP MATERIAL TO COMPLETE THE DRIVE APPROACHES SHALL BE INCLUDED IN THE PRICE BID FOR THEIR RESPECTIVE ITEMS OF WORK

EXISTING MAILBOXES AND MAILBOX POSTS:

GREAT CARE SHALL BE TAKEN TO PREVENT DAMAGE TO ANY OF THE EXISTING MAILBOXES OR MAILBOX POSTS DURING THE PAVING OPERATIONS. ANY MAILBOX OR MAILBOX POST WHICH BECOMES DAMAGED BY THE CONTRACTOR'S PAVING OPERATIONS SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

WATER QUALITY PROTECTION:

NO TOXIC OR HAZARDOUS MATERIALS SUCH AS SEALANTS, PAINT, SOLVENTS, CLEANING AGENTS, EARTHEN MATERIALS, WASTE-WATER, FUELS OR DEBRIS OF ANY KIND SHALL BE DISCHARGED TO ANY STREAMS, DRAINAGE COURSES, OR BODIES OF WATER. ALL ASPHALT OR CONCRETE GRINDINGS, EXCESS ASPHALTIC OR CONCRETE MATERIALS OR ANY OTHER DEBRIS GENERATED DURING RESURFACING OR OTHER SIMILAR ACTIVITIES SHALL BE NOT BE DISPOSED OF WITHIN A FLOOD PLAIN BELOW THE 100-YEAR FLOOD ELEVATION. THE CONTRACTOR SHALL TAKE GREAT CARE AND ALL PRECAUTIONS NECESSARY TO PREVENT THE CONTAMINATION OF THE PUBLIC DRINKING WATER SUPPLY, ALL PROJECT RELATED REFUELING AND MAINTENANCE ACTIVITIES SHALL BE PERFORMED IN AN ENVIRONMENTALLY RESPONSIBLE MANNER AND ALL PRECAUTIONS NECESSARY TO PREVENT LIQUIDS USED TO REPAIR, CLEAN, SEAL, OR TREAT ANY BRIDGE STRUCTURE (I.E. PAINT, SEALER, SOLVENT) FROM ENTERING STREAMS, WETLANDS OR OTHER WATERS OF THE UNITED STATES AND TAKE THE APPROPRIATE ACTIONS IN THE EVENT OF A RELEASE. THE CONTRACTOR IS REQUIRED AND SHALL BE HELD RESPONSIBLE FOR THE CLEAN UP AND REMEDIATION OF ANY AND ALL SPILLS.

WATERSHED PROTECTION:

IT IS ESSENTIAL THAT ALL ACTIVITIES ASSOCIATED WITH THIS WORK BE PERFORMED IN A MANNER CONSISTENT WITH BEST WATERSHED MANAGEMENT PRACTICES INCLUDING, BUT NOT LIMITED TOO.

AREAS OF DISTURBED GROUND SHALL HAVE APPROPRIATE EROSION AND SEDIMENT CONTROLS. IF HAZARDOUS/TOXIC MATERIALS INCLUDING BUT NOT LIMITED TO FUELS, OILS, BITUMEN'S PAINTS, SEALANTS, OR OTHER CHEMICALS, ARE STORED ON SITE, THEY SHALL BE STORED IN A DOUBLE-CONTAINMENT MANNER. ALL EQUIPMENT REPAIRS, MAINTENANCE, AND MECHANICAL WORK THAT COULD RESULT IN THE RELEASE OF HAZARDOUS/TOXIC MATERIALS SHALL BE PERFORMED IN AN APPROPRIATELY CONTAINED AREA, PREFERABLY OFF SITE OR AN APPROPRIATE OFF-SITE FACILITY.

IN THE EVENT THAT ANY HAZARDOUS/TOXIC MATERIALS INCLUDING, BUT NOT LIMITED TO FUELS, OILS, BITUMEN'S PAINTS, SEALANTS, OR OTHER CHEMICALS ARE SPILLED INTO ANY RIVER OR ANY OTHER WATERCOURSES OR ON ANY GROUND SURFACES, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY EMERGENCY SERVICES BY CALLING 911, THE OHIO EPA @ (800)282-9378. THE CONTRACTOR SHOULD BE PREPARED TO PROVIDE DETAILED INFORMATION RELATIVE TO THE TYPE AND QUANTITY OF MATERIAL THAT HAS BEEN SPILLED AS WELL AS THE EXACT LOCATION AND THE EXACT TIME AT WHICH THE SPILL OCCURRED.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR INFORMING ALL SUBCONTRACTORS AND OTHER AGENTS OF THESE RESPONSIBILITIES, PRECAUTIONS, AND PROHIBITIONS.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN:

ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF THE PROJECT. ANY WORK, WHICH IS DETERMINED TO BE NECESSARY BY THE PROJECT ENGINEER, SHALL BE PERFORMED AFTER THE PERFORMANCE OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE AS PER PLAN AND BEFORE THE PLACEMENT OF ANY ASPHALT CONCRETE SURFACE COURSE. THE REPAIR AREAS SHALL BE OF VARYING LENGTH AND SHALL HAVE AN AVERAGE WIDTH OF NOT LESS THAN 4 FEET. THE AVERAGE DEPTH OF EACH REPAIR SHALL BE 4 INCHES. ALL AREAS SHALL BE REFILLED WITH AN EQUAL AMOUNT OF ITEM 301 - ASPHALT CONCRETE BASE. SEE PLAN DETAIL ON PLAN SHEET 4/47.

NO MORE PARTIAL PAVEMENT REPAIR, AS PER PLAN SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY AND ALL REPAIRED AREAS ARE TO BE INCLUDED INTO THE GENERAL RESURFACING.

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GENERAL NOTES

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ITEM 253 - PAVEMENT REPAIR, AS PER PLAN:

ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF THE PROJECT. ANY WORK, WHICH IS DETERMINED TO BE NECESSARY BY THE PROJECT ENGINEER, SHALL BE PERFORMED BEFORE THE PLACEMENT OF THE ASPHALT CONCRETE SURFACE COURSE. THE REPAIRS AREAS SHALL CONSIST OF REMOVING 13.5 INCHES OF THE EXISTING PAVEMENT AND REPLACING 13.5 INCHES OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22 FOR AN AVERAGE WIDTH OF 4 FEET. SEE DETAIL ON PLAN SHEET 4/47. NO MORE PAVEMENT REPAIR, AS PER PLAN SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY AND ALL WORK SHALL BE COMPLETED PRIOR TO RESURFACING AND ALL REPAIRED AREAS ARE TO BE INCLUDED INTO THE GENERAL RESURFACING.

ITEM 407 - TACK COAT:

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. FOR ESTIMATING PURPOSES ONLY, THE PLAN INDICATES AN AVERAGE APPLICATION RATE OF TACK COAT AT 0.075 GALLON PER SQUARE YARD TACK COAT. A COVER AGGREGATE SHALL BE USED IF HEAVY TRACKING OF THE TACK COAT ON TO THE EXISTING PAVEMENT SHOULD OCCUR DURING THE PAVING OPERATIONS. THE ADDITIONAL COST OF THE COVER AGGREGATE SHALL BE INCLUDED IN THE COST OF THIS ITEM.

ITEM 442 - ASPHALT CONCRETE, SURFACE COURSE, 12.5mm, TYPE A (448):

ITEM 448 - ASPHALT CONCRETE, SURFACE COURSE, TYPE 1, PG64-22: GREAT CARE SHALL BE TAKEN TO MAINTAIN THE EXISTING PAVEMENT CROSS-SLOPE (CROWN), INTERSECTION CROSS-SLOPES (CROWN) AS WELL AS ALL LONGITUDINAL SLOPES WITHIN THE EXISTING CROSSWALKS DURING THE PAVING OPERATIONS.

ITEM 604 - MONUMENT BOX ADJUSTED TO GRADE:

THIS ITEM OF WORK WILL PROVIDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO ADJUST TO GRADE THE EXISTING MONUMENT BOX TO 1/4 INCH BELOW THE PROPOSED ASPHALT ELEVATION AT THE FOLLOWING LOCATIONS.

- MADISON AND CHAMPAIGN COUNTY LINE = 1 EACH
- TOWNSHIP ROAD 121 (THOMAS ROAD) AND SR-29 = 1 EACH
- SR-38 AND SR-29 = 1 EACH
- COUNTY ROAD 5 (LAFAYETTE PLAIN CITY ROAD) AND SR-19 = 1 EACH
- US-42 AND SR-29 = 1 EACH

ITEM 604 - MONUMENT BOX ADJUSTED TO GRADE: = 5 EACH

ITEM 617 - COMPACTED AGGREGATE:

THIS ITEM HAS BEEN PROVIDED TO ASSURE THAT THERE IS NO DROP-OFF AND SHALL BE USED AS DIRECTED BY THE PROJECT ENGINEER. SEE DETAIL ON PLAN SHEET 4/47.

ITEM 617 - WATER:

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED TO BE USED AS DIRECTED BY THE PROJECT ENGINEER.

- "AS DIRECTED BY THE ENGINEER" = 2 M/GAL.
- ITEM 617 - WATER: = 2 M/GAL.

ITEM 623 - CONSTRUCTION LAYOUT STAKES, AS PER PLAN:

THIS ITEM SHALL CONSIST OF STATIONING USING 3 FT LATH STAKES OR PAINT MARKINGS. THE STAKES OR PAINT MARKINGS SHALL BE SPACED AT 200 FT INTERVALS AND EXTEND THROUGHOUT THE LENGTH OF THE PROJECT. PLACEMENT OF THE STAKES OR PAINT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED, MISSING STAKES, OR PAINT MARKINGS. PAINT MARKINGS SHALL BE PLACED ON CURBS AND USED IN AREAS WHERE THE PLACEMENT OF STAKES IS NOT POSSIBLE AND APPROVED BY THE PROJECT ENGINEER.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH PRICE SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

ITEM 632 - LOOP DETECTOR TIE IN:

THIS ITEM SHALL BE USED TO CONNECT AND SPLICE THE PROPOSED LOOP DETECTORS TO THE APPROPRIATE EXISTING LEAD IN CABLE INSIDE THE PULL BOX.

ITEM 632 - DETECTOR LOOP, AS PER PLAN:

THE LOCATIONS, SIZES AND SHAPES OF PROPOSED LOOP DETECTORS WILL BE THE SAME AS EXISTING. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THE EXISTING LOOP DETECTORS LISTED IN THE PLAN BEFORE THE PAVEMENT PLANING DESTROYS THEM. ALL LOOP WIRE SHALL BE IDENTIFIED WITH A PLASTIC TAG (WBLT, EBRT, ETC.) AT THE SPLICE POINT OR AT ENTRANCE TO THE CONTROL CABINET. WHEN A PULLBOX IS NOT USED, THE SOLDERED SPLICE SHALL BE MADE IN AN ANCHOR BASE, STRAIN POLE OR A CONDUIT RISER SPECIFIED BY THE PROJECT ENGINEER, EXCEPT WHERE A CONTROLLER CABINET IS MOUNTED ON THAT POLE IN WHICH CASE THE LOOP WIRE SHALL BE ROUTED DIRECTLY INTO THE CABINET. THE CONTRACTOR SHALL PROVIDE THE PROJECT ENGINEER PRIOR TO THE COMMENCEMENT OF WORK, A COPY OF THE IMSA CERTIFICATION PAPERS FOR ALL SIGNAL TECHNICIANS WORKING ON THIS PROJECT. ALL DETECTOR LOOPS SHALL BE REPLACED AND FUNCTIONAL WITHIN 48 HOURS OF BEING REMOVED. THE CONTRACTOR SHALL ALSO PROTECT ANY INLET OR CATCH BASIN FROM FOREIGN MATERIAL OR CONSTRUCTION DEBRIS ENTERING THE INLET OR CATCH BASIN WHILE CUTTING DETECTOR LOOPS.

ALL STOP LINE INDUCTANCE DETECTOR LOOPS SHOWN IN THE PLANS SHALL BE THE POWERHEAD CONFIGURATION SHOWN ON TC-82.10. THE WIDTH SHALL BE AS SPECIFIED ON TC-82.10 AND THE LENGTH SHALL BE AS CURRENTLY CALLED FOR IN THE PLANS. THE STOP LINE DETECTOR LOOPS SHALL NOT BE WIRED TO ANY OTHER LOOPS AND SHALL HAVE ITS OWN DETECTOR CHANNEL. THE LOCATION OF THESE LOOPS SHALL BE SUCH THAT THE POWERHEAD IS LOCATED AT THE STOP LINE, NOT PAST IT.

ALL DILEMMA ZONE INDUCTANCE DETECTOR LOOPS CALLED FOR IN THE PLANS SHALL BE THE ANGULAR DESIGN DETECTION (ADD) LOOP AS SHOWN ON TC-82.10. DIMENSIONS SHALL BE AS SPECIFIED ON TC-82.10.

ALL STOP LINE DETECTION SHALL BE TESTED FOR A BICYCLE TARGET AND ALL DILEMMA DETECTION ZONES SHALL BE TESTED FOR A MOTORCYCLE TARGET.

OHIO DEPARTMENT OF TRANSPORTATION
400 EAST WILLIAM STREET
DELAWARE, OHIO 43015
1-740-833-8332

ITEM 644 - THERMOPLASTIC PAVEMENT MARKING:

THE CONTRACTOR SHALL REMOVE ANY INCORRECTLY PLACED WORK ZONE PAVEMENT MARKINGS PRIOR TO THE PLACEMENT OF ALL PERMANENT PAVEMENT MARKINGS. THE LOCATIONS, SIZES AND SHAPES OF PROPOSED PAVEMENT MARKINGS WILL BE THE SAME AS EXISTING. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THE EXISTING PAVEMENT MARKINGS BEFORE THE PAVEMENT PLANING OR RESURFACING OBLITERATES THEM. ANY PAVEMENT MARKING WHICH IS PLACED AT THE WRONG LOCATION SHALL BE REMOVED AND REPLACED AT THE CORRECT LOCATION AT THE CONTRACTORS EXPENSE. ALL PERMANENT PAVEMENT MARKINGS SHALL BE COMPLETED WITHIN 5 CONSECUTIVE DAYS OF THE PLACEMENT OF THE ASPHALT CONCRETE, SURFACE COURSE.

ITEM 690 - SPECIAL MISC.: SAFETY EDGE TREATMENT:

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING A SAFETY EDGE TREATMENT PARALLEL TO THE PROPOSED ROADWAY SHOULDER SURFACE BY ATTACHING A DEVICE TO THE SCREED OF THE PAVER THAT CONFINES THE ASPHALT SURFACE MATERIAL AT THE END GATE OF THE PAVER SCREED, CONSTRAINS THE ASPHALT HEAD REDUCING THE AREA BY 10% TO 15% INCREASING THE DENSITY OF ASPHALT MATERIAL AND EXTRUDE THE ASPHALT MATERIAL AS A COMPACTED WEDGE SHAPE AT THE EDGE OF THE PROPOSED EDGE OF PAVEMENT WITH AN 30 TO 35 DEGREE ANGLE. THE CONTRACTOR SHALL NOT USE A CONVENTIONAL SINGLE PLATE STRIKE OFF TO CONSTRUCT THE SAFETY EDGE TREATMENT.

THE DEVICE SHALL MAINTAIN CONTACT BETWEEN THE PROPOSED ROADWAY SHOULDER SURFACE AND ALLOW FOR AUTOMATIC TRANSITION BETWEEN CROSS ROADS, DRIVEWAYS AND OTHER OBSTRUCTIONS. SHORT SECTIONS OF HANDWORK WILL BE ALLOWED WHEN NECESSARY FOR TRANSITIONS, TURNOUTS OR AS OTHERWISE AUTHORIZED BY CENTRAL OFFICE CONSTRUCTION.

THE CONTRACTOR SHALL BE PERMITTED TO USE A SIMILAR DEVICE NOT LISTED BELOW. THE CONTRACTOR SHALL PROVIDE PROOF THAT REQUESTED SIMILAR DEVICE HAS BEEN USED ON PREVIOUS PROJECTS WITH ACCEPTABLE WEDGE COMPACTION RESULTS. IF THE CONTRACTOR IS UNABLE TO PROVIDE PROOF OF ACCEPTABLE WEDGE COMPACTION RESULTS, THE CONTRACTOR SHALL CONSTRUCT A TEST SECTION, PRIOR TO CONSTRUCTING ANY SAFETY EDGE TREATMENT, TO DEMONSTRATE THAT REQUESTED SIMILAR DEVICE CAN PRODUCE THE DESIRED WEDGE COMPACTION RESULTS. THE CONTRACTOR SHALL CONTACT CENTRAL OFFICE CONSTRUCTION DEPARTMENT JULIA MILLER (614) 644-6622 PRIOR TO THE CONSTRUCTION OF TEST SECTION FOR THE PROPOSED SIMILAR DEVICE.

THE FOLLOWING CONTACT INFORMATION FOR WEDGE SHAPE COMPACTION DEVICES IS LISTED BELOW:

TRANSTECH SYSTEMS, INCORPORATED
1594 STATE STREET
SCHENECTADY, NY 12304
1-800-724-6306
WWW.TRANSTECHSYS.COM

ADVANT-EDGE PAVING EQUIPMENT LLC
P.O. BOX 9163
NISKAYUNA, NY 12309-0163
PH. 518-280-6090
CONTACT: GARY D. ANTONELLI
CELL 518-368-5699
WEBSITE: WWW.ADVANTEDGEPAVING.COM

ASPHALT QUANTITIES FOR THIS ITEM ARE INCLUDED IN ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (448).

PAYMENT FOR ITEM 690 - SPECIAL MISC.: SAFETY EDGE TREATMENT SHALL BE MADE FOR THE COMPLETED SAFETY EDGE TREATMENT AND SHALL INCLUDE THE COST OF ALL TEST SECTIONS, LABOR, EQUIPMENT, SHOULDER PREPARATION, SHOULDER GRADING, MATERIALS AND ALL OTHER INCIDENTALS NECESSARY TO CONSTRUCT IN PLACE THE SAFETY EDGE TREATMENT.

ITEM 690 - SPECIAL MISC.: SAFETY EDGE TREATMENT - LIN. FT

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ITEM 614 - MAINTAINING TRAFFIC:

ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION). COPIES ARE AVAILABLE FROM,

THE OHIO DEPARTMENT OF TRANSPORTATION
BUREAU OF TRAFFIC,
1980 WEST BROAD STREET
COLUMBUS, OHIO 43223

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK LISTED IN THE GENERAL SUMMARY FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER.

CONSTRUCTION OPERATIONS SHALL NOT BEGIN UNTIL ALL TRAFFIC CONTROL IS IN PLACE AND APPROVED BY ODOT PERSONNEL. THE CONSTRUCTION INSPECTOR SHALL APPROVE ALL TEMPORARY TRAFFIC CONTROL DEVICES FOR CONDITION AND LOCATION BEFORE THE CONTRACTOR WILL BE ALLOWED TO BEGIN WORK. IF THE CONTRACTOR DOES NOT COMPLY WITH THE STANDARDS, HIS PERMIT SHALL BE REVOKED AND ALL WORK SHALL BE TERMINATED.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

USE OF STANDARD DRAWINGS

FOR THE PURPOSE OF THIS PROJECT, "MOVING OPERATION" SHALL BE LIMITED TO PAVEMENT MARKING STRIPING.

IT MAY BE NECESSARY TO EXTEND THE ADVANCE WARNING AND BUFFER ZONES BEYOND THE MINIMUM DISTANCES SHOWN ON THE STANDARD DRAWINGS. THIS MAY BE DUE TO HORIZONTAL ALIGNMENT, VERTICAL ALIGNMENT, RAMP LOCATIONS, OR OTHER SIGHT OBSTRUCTIONS. LOCATIONS OF THE TAPER ZONES MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER, BUT TAPER LENGTHS MUST MEET THE MINIMUM STANDARDS. TAPERS SHOULD BE PLACED IN TANGENT SECTIONS WHENEVER POSSIBLE. ADDITIONAL YIELD SIGNS MAY BE REQUIRED FOR RAMP WITHIN 1,000 FEET OF A WORK ZONE. PAYMENT SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

FOR ANY MULTILANE HIGHWAY, DEVICE SPACING SHALL BE A MAXIMUM OF 40' CENTER ON CENTER IN THE TAPERS AND 80' CENTER ON CENTER IN THE TANGENT SECTIONS.

RURAL PAVING AREA

TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF THE COMPLETED ASPHALT CONCRETE COURSES. WORK ZONES SHALL BE LIMITED TO A MAXIMUM LENGTH OF 5000 FEET.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

WORK SITE LIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR, AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

USE OF WEIGHTED CHANNELIZERS

THE WEIGHTED CHANNELIZERS MAY BE USED IN ACCORDANCE WITH THIS SECTION. THE WEIGHTED CHANNELIZERS SHALL BE PREDOMINANTLY ORANGE IN COLOR AND SHALL BE MADE OF LIGHTWEIGHT, FLEXIBLE, AND DEFORMABLE MATERIAL. THEY SHALL BE AT LEAST 42 INCHES IN HEIGHT WITH A WEIGHTED BASE. THEY MAY HAVE A HANDLE OR LIFTING DEVICE, WHICH EXTENDS ABOVE THE 42" MINIMUM HEIGHT.

THE MARKINGS ON THE WEIGHTED CHANNELIZERS SHALL BE HORIZONTAL, CIRCUMFERENTIAL, ALTERNATING ORANGE AND WHITE RETRO REFLECTIVE STRIPES 6 INCHES WIDE. EACH WEIGHTED CHANNELIZERS SHALL HAVE A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. ANY NON-RETRO REFLECTIVE SPACES BETWEEN THE HORIZONTAL ORANGE AND WHITE STRIPES SHALL NOT EXCEED 2 INCHES WIDE. THE WEIGHTED CHANNELIZERS SHALL HAVE A 4-INCH MINIMUM WIDTH, REGARDLESS OF ORIENTATION.

USE OF WEIGHTED CHANNELIZERS ON FREEWAYS AND MULTILANE HIGHWAYS SHALL BE LIMITED TO SHORT-TERM OPERATION FOR EITHER DAY OR NIGHT. UPON COMPLETION OF WORK, THE WEIGHTED CHANNELIZERS SHALL BE REMOVED. THE WEIGHTED CHANNELIZERS MAY AGAIN BE PLACED ON THE HIGHWAY WHEN THE WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT. ANY LANE CLOSURE USING CHANNELIZATION DEVICES, EXPECTED TO REMAIN FOR MORE THAN TWELVE HOURS, SHALL REQUIRE THE USE OF DRUMS OR BARRIERS. WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT.

WHEN USED AT NIGHT, WEIGHTED CHANNELIZERS SHALL ONLY BE PLACED IN THE TANGENT AREA. THE TANGENT AREA IS DEFINED AS THE AREA AFTER THE TRANSITION TAPER WHERE THE WORK TAKES PLACE. DRUMS SHALL BE USED IN THE TRANSITION TAPERS FOR NIGHT OPERATIONS. MAXIMUM SPACING OF THE WEIGHTED CHANNELIZERS SHALL BE 40 FEET.

STEPS SHOULD BE TAKEN TO ENSURE THAT THE WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND OR MOVING TRAFFIC. BALLASTS SHOULD NOT PRESENT A HAZARD IF THE WEIGHTED CHANNELIZERS ARE INADVERTENTLY STRUCK, NOR SHOULD THEY AFFECT THE VISIBILITY OF THE WEIGHTED CHANNELIZERS. ALL BALLASTS USED SHOULD BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

PUBLIC NOTIFICATION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE DISTRICT 6 PUBLIC INFORMATION OFFICE AT (740) 833-8260 TO COORDINATE EFFORTS TO NOTIFY ADJACENT RESIDENTS AND BUSINESSES OF THE UPCOMING RESURFACING PROJECT. ADVANCE NOTIFICATION SHALL OCCUR NO LATER THAN FOURTEEN (14) DAYS PRIOR TO THE FIRST DAY OF WORK.

THE CONTRACTOR IS RESPONSIBLE TO POST "EMERGENCY - NO PARKING" SIGNS 72 HOURS PRIOR TO BEGINNING ANY WORK ON THE STREET. THE "EMERGENCY - NO PARKING" SIGNS CAN BE POSTED FOR A MAXIMUM DURATION OF EIGHT WORKING DAYS. SHOULD THE WORK WITHIN THIS AREA FAIL TO BE COMPLETED WITHIN THE EIGHT DAY WORKING PERIOD, THE CONTRACTOR IS RESPONSIBLE FOR REMOVING THE EXISTING "EMERGENCY - NO PARKING" SIGNS AND REPOSTING NEW "EMERGENCY - NO PARKING" SIGNS. ALL COSTS FOR REPOSTING THE SIGNS SHALL BE PAID FOR BY THE CONTRACTOR.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW. THE PROJECT ENGINEER SHALL RECEIVE THIS NOTIFICATION PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE BUT IS NOT LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHOULD LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

ITEM OF WORK	DURATION OF CLOSURE	NOTIFICATION DUE TO DIST. 6 COMMUNICATIONS OFFICE
ROAD & RAMP CLOSURES	GREATER THAN 2 WEEKS	14 BUSINESS DAYS PRIOR TO CLOSURE
	GREATER THAN 12 HOURS LESS THAN 2 WEEKS	7 BUSINESS DAYS PRIOR TO CLOSURE
	LESS THAN 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES RESTRICTIONS	GREATER THAN 2 WEEKS	7 BUSINESS DAYS PRIOR TO CLOSURE
	LESS THAN 2 WEEKS	2 BUSINESS DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME FRAME TABLE.

PERMITTED LANE CLOSURE TIMES

ALL LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR THE ALLOWABLE TIMES SHOWN IN THE PERMITTED LANE CLOSURE TABLE LISTED BELOW. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT SHOWN IN THE UNAUTHORIZED LANE USED TABLE FOR EACH PERIOD OF TIME THAT A LANE REDUCTION, LANE RESTRICTION, RAMP REDUCTION OR RAMP RESTRICTION REMAINS BEYOND THE PERMITTED WORKING HOURS SHOWN IN THE PERMITTED LANE CLOSURE TABLE.

PERMITTED LANE CLOSURE TABLE

HOURS OF WORK	MAD-29	MONDAY - SATURDAY
NO RESTRICTIONS		

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**MAINTENANCE OF TRAFFIC
GENERAL NOTES**
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PERIODS OF NO WORK

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS.

HOLIDAYS

CHRISTMAS
NEW YEARS
MEMORIAL DAY
FOURTH OF JULY
LABOR DAY
THANKSGIVING

EVENTS

THE MADISON COUNTY FAIR
FARM SCIENCE REVIEW

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD.

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00 PM FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00 PM FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00 PM MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00 PM TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00 PM WEDNESDAY THROUGH 6:00 AM MONDAY
THANKSGIVING THURSDAY	5:00 AM WEDNESDAY THROUGH 5:00 AM MONDAY
FRIDAY	12:00 PM THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00 PM FRIDAY THROUGH 6:00 AM MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA WIDE. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN ACCORDANCE WITH THE UNAUTHORIZED LANE USE TABLE.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC:

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY.

"AS DIRECTED BY THE ENGINEER" = 5 CU.YDS.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC: = 5 CU.YDS.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE:

USE OF L.E.O.'S BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT GENERALLY BE PERMITTED AT PROJECT COST UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE PROJECT ENGINEER. LAW ENFORCEMENT OFFICERS (L.E.O.'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OHIO MANUAL UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING.

TRAFFIC CONTROL TASKS

- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
- WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY.

FOR LANE CLOSURES

- DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).
- WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

IN GENERAL, L.E.O.'S SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

LAW ENFORCEMENT OFFICERS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO CHASE MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF THE MOTORISTS ACTIONS ARE CONSIDERED TO BE WRECKLESS, THEN PURSUIT OF THE MOTORIST MAY BE APPROPRIATE.

THE L.E.O.'S WORK AT THE DIRECTION OF THE CONTRACTOR AND THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEO'S AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEO. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THEIR RESPECTIVE DUTIES, PLACEMENT AND WILL RESOLVE ANY ISSUES BETWEEN THE TWO PARTIES THAT MAY ARISE. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE SERVICES WITH,

THE MADISON COUNTY SHERIFF AT 1-740-852-1332
THE OHIO HIGHWAY PATROL 1-614-466-2660

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE: (CONTINUED)

THE L.E.O. SHOULD REPORT IN TO THE CONTRACTOR PRIOR TO THE START THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING THE SHIFT. THE L.E.O. IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF THE SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE L.E.O. SHOULD NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE, WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF THE SHIFT.

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED. ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A L.E.O. ARE TO BE INCLUDED IN THE UNIT BID PRICE ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR.

IF THE CONTRACTOR WISH TO UTILIZE THE L.E.O. FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN THAT REQUIRED IN THESE PLANS, THEY MAY DO SO AT THEIR OWN EXPENSE. PAYMENT FOR THE EXCESS ABOVE THE CONTRACT REQUIREMENTS WILL BE INCLUDED UNDER ITEM 614 - MAINTAINING TRAFFIC.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR). THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

40 WORKING DAYS @ 10 HOUR DAYS

ITEM 614 - L.E.O. WITH PATROL CAR = 400 L.E.O. HOURS

ITEM 614 - WORK ZONE MARKING SIGN:

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF MT-101.90, 614.04 AND 614.11.

W8-H13-36	"NO EDGE LINES"	= 22 EACH
R4-1-36	"DO NOT PASS"	= 20 EACH
R4-2-36	"PASS WITH CARE"	= 20 EACH

ITEM 614 - WORK ZONE MARKING SIGN = 62 EACH

WORK ZONE MARKINGS:

WHENEVER YELLOW CENTERLINES OR TURN-LANE LINES ARE PAVED OVER, REMOVED, OR OTHERWISE UNSERVICEABLE, AND AT LOCATIONS IDENTIFIED BY THE ENGINEER THE CONTRACTOR SHALL INSTALL TEMPORARY STRIPING AND WORK ZONE PAVEMENT MARKINGS AS PER THE REQUIREMENTS OF ITEM 614.04 AND 614.11.

THE CONTRACTOR SHALL REMOVE PRIOR TO THE PLACEMENT OF ALL PERMANENT PAVEMENT MARKINGS ALL CONFLICTING WORK ZONE MARKINGS.

AS SPECIFIED IN THE PLAN WORK ZONE CENTERLINE CLASS II, 740.06, TYPE 1 SHALL BE USED FOR ALL WORK ZONE CENTERLINE. WORK ZONE CENTERLINE CLASS III 642 PAINT SHALL BE USED FOR AUXILIARY MARKINGS ON ALL FINAL COURSES OF ASPHALT CONCRETE.

MAINTENANCE OF TRAFFIC
GENERAL NOTES

MAD-29-0.00

L O C A T I O N	C O U N T Y	R O U T E	B E G I N SLM	E N D SLM	L E N G T H FT	L E N G T H MILE	S I D E	DESCRIPTION	614 WORK ZONE LONG LINE MARKINGS			614 WORK ZONE AUXILIARY MARKINGS			
									WORK ZONE CENTERLINE CLASS II 740.06 TYPE 1 SURFACE MILE	WORK ZONE STOP LINE CLASS III 642 PAINT SURFACE FT.	WORK ZONE CENTERLINE CLASS II 740.06 TYPE 1 SURFACE MILE	WORK ZONE STOP LINE CLASS III 642 PAINT SURFACE FT.	WORK ZONE CENTERLINE CLASS II 740.06 TYPE 1 SURFACE MILE	WORK ZONE STOP LINE CLASS III 642 PAINT SURFACE FT.	
															WORK ZONE CENTERLINE CLASS II 740.06 TYPE 1 SURFACE MILE
1	MAD	29	0.00	12.98	68,534	12.98	℄	CENTERLINE							
			12.72	12.92	1,056	0.20		APPROACH WITH LEFT TURN LANE						54	
			12.72	12.98	1,373	0.26		INTERSECTION SR-29 AND US40						100	
			0.000					TOWNSHIP ROAD 118 (WARE RD.)						12	
			0.205					COUNTY ROAD 117 (LEWIS RD.)						12	
			1.126					COUNTY ROAD 119 (DAVID BROWN RD.)						12	
			3.036					COUNTY ROAD 113 (TRADERSVILLE BRIGHTON RD.)						24	
			3.036					TOWNSHIP ROAD 121 (THOMAS RD.)						12	
			5.980					SR-29 & SR-38						12	
			5.990					SR-29 & SR-38						12	
			7.457					TOWNSHIP ROAD 290 (MAPLEWOOD RD.)						12	
			7.760					COUNTY ROAD 5 (LAFAYETTE PLAIN CITY RD.)						24	
			8.564					TOWNSHIP ROAD 116 (LITTLE DARBY RD.)						12	
			9.583					SR-29 & US-42						12	
			9.594					SR-29 & US-42						12	
			9.754					TOWNSHIP ROAD 137 (BYERLY MILL RD.)						12	
			10.425					TOWNSHIP ROAD 117 (SNYDER RD.)						12	
			10.917					TOWNSHIP ROAD 115 (BYERLY RD.)						12	
			11.110					MUNICIPAL ROAD 134 (COMMERCE PARKWAY RD.)						12	
			12.98					SR-29 & US-40						12	
TOTALS CARRIED TO PLAN SHEET 13/46											13.46			382	

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 FOR
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 DATE
**MAINTENANCE OF TRAFFIC
 WORK ZONE PAVEMENT MARKING SUB SUMMARY**
MAD - 29 - 0.00
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 46

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UNDERGROUND UTILITIES AND UTILITIES OWNERSHIP:

THE IDENTITY AND THE LOCATION OF SOME OF THE EXISTING UNDERGROUND FACILITIES KNOWN TO BE LOCATED IN THE CONSTRUCTION AREA HAVE BEEN IDENTIFIED. THE CONTRACTOR SHALL GIVE NOTICE OF INTENT TO CONSTRUCT TO THE OHIO UTILITIES PROTECTION SERVICE, PRODUCERS UNDERGROUND PROTECTION SERVICE, AND OWNERS OF UNDERGROUND FACILITIES THAT ARE NOT MEMBERS OF A REGISTERED PROTECTION SERVICE IN ACCORDANCE WITH SECTION 153.64 OF THE OHIO REVISED CODE. THE ABOVE, MENTIONED NOTICE SHALL BE GIVEN AT LEAST TWO WORKING DAYS PRIOR TO THE START OF CONSTRUCTION THE FOLLOWING UTILITIES ARE LOCATED WITHIN THE WORK LIMITS OF THE PROJECT AND THE OWNERS SUBSCRIBE TO REGISTERED UNDERGROUND PROTECTION SERVICE.

OHIO UTILITY PROTECTION SERVICE 1-800-362-2764
PRODUCERS UNDERGROUND PROTECTION SERVICE 1-614-587-0486
NON-MEMBERS MUST BE CALLED DIRECTLY.

LISTED BELOW ARE THE KNOWN UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

AMERIGAS PROPANE
7265 INDUSTRIAL PARKWAY
PLAIN CITY, OH 43064
614.771.8500

WOW! INTERNET/CABLE/PHONE
3765 CORPORATE DRIVE
COLUMBUS, OH 43231
614.948.4653

AT&T LEGACY
5980-G WILCOX PLACE
DUBLIN, OH 43016
614.760.8320

BP OIL COMPANY
1771 BUCKEYE ROAD
LIMA, OH 45804
419.222.7221

BUCKEYE PIPELINE COMPANY
P O BOX 90
LIMA, OH 45802
419.993.8008

COLUMBIA GAS OF OHIO
1600 DUBLIN ROAD - EW 2
COLUMBUS, OH 43215
614.481.1056

WINDSTREAM
3701 COMMUNICATIONS WAY
EVANSVILLE, IN 47715
812.253.2168

SOUTH CENTRAL POWER COMPANY
2780 COONPATH ROAD
LANCASTER, OH 43130
740.689.6119

AEP
850 TECH CENTER DRIVE
GAHANNA, OH 43230
614.883.6831

COLUMBIA GAS TRANSMISSION CORPORATION
301 MAPLE STREET
SUGAR GROVE, OH 43155
740.746.2234

THE DAYTON POWER AND LIGHT COMPANY
1900 DRYDEN ROAD
DAYTON, OH 45439
937.331.4497

FRONTIER COMMUNICATIONS
1300 COLUMBUS SANDUSKY ROAD
MARION, OH 43302
740.383.0551

CITY OF LONDON
6 EAST 2ND STREET
LONDON, OH 43140
740.852.3243

MADISON COUNTY ENGINEER
825 US 42 NE
LONDON, OH 43140
740.852.9404

VERIZON BUSINESS
DEPT. 42864 LOC 107
2400 NORTH GLENVILLE
RICHARDSON, TX 75082
(FAX) 972.729.6240

MADISON ENERGY COOPERATIVE ASSOCIATION
5900 MAYFAIR ROAD, NW
NORTH CANTON, OH 44720
330.498.9130

OHIO AMERICAN WATER COMPANY
5481 BUENOS AIRES BLVD
WESTERVILLE, OH 43081
614.882.6586

AT&T
111 NORTH FOURTH STREET - 8TH FLOOR
COLUMBUS, OH 43215
614.223.7162

ODOT
400 EAST WILLIAM STREET
DELAWARE, OH 43015
740.833.8332

OHIO EDISON COMPANY
420 SOUTH YORK STREET
SPRINGFIELD, OH 45505
937.327.1283

MARATHON PETROLEUM
539 SOUTH MAIN STREET - ROOM 7642
FINDLAY, OH 45840-3295
419.421.2211

PIONEER RURAL ELECTRIC COOPERATIVE, INC.
344 WEST UNITED STATES ROUTE 36
P. O. BOX 604
PIQUA, OH 45356
937.773.2523

CENTURYLINK QCC
700 W MINERAL AVENUE
LITTLETON, COLORADO 80120
303.992.9931 OFFICE

SPRINT COMMUNICATIONS
11370 ENTERPRISE PARK DRIVE
SHARONVILLE, OH 45241
513.612.4204

VECTREN ENERGY DELIVER OF OHIO
6500 CLYO ROAD
CENTERVILLE, OH 45459
937.312.2544

UNION RURAL ELECTRIC COOPERATIVE, INC.
15461 US 36 E
P. O. BOX 393
MARYSVILLE, OH 43040-4239
937.642.1826

CENTURY LINK
441 WEST BROAD STREET
PATASKALA, OH 43062
740.927.8282

TIME WARNER CABLE
3760 INTERCHANGE DRIVE
COLUMBUS, OH 43204
614.481.5262

VILLAGE OF WEST JEFFERSON
28 EAST MAIN STREET
WEST JEFFERSON, OHIO 43162
614-879-8655

UTILITY NOTES

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SHEET NUMBER						FUNDING SPLITS				ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET
6-7	8-9	10	14-16	17-18		80% FED.	20% STATE								
			165			165				202	32000	165	FT	ROADWAY CURB REMOVED	
			86			86				203	10000	86	CU YD	EXCAVATION	
			1,114			1,114				204	10000	1,114	SQ YD	SUBGRADE COMPACTION	
5						5				604	39500	5	EACH	MONUMENT BOX ADJUSTED TO GRADE	
														EROSION CONTROL	
						1,000				832	30000	1,000	EACH	EROSION CONTROL	
														PAVEMENT	
			9,522			9,522				251	01001	9,522	SQ YD	PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN	6/46
			50			50				253	02001	50	CU YD	PAVEMENT REPAIR, AS PER PLAN	7/46
			2,823			2,823				254	01000	2,823	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE	
			21			21				301	46000	21	CU YD	ASPHALT CONCRETE BASE, PG64-22	
			21			21				304	20000	21	CU YD	AGGREGATE BASE	
			15,794			15,794				407	10000	15,794	GALLON	TACK COAT	
			2,310			2,310				442	20000	2,310	CU YD	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (448)	
			5,874			5,874				448	47020	5,874	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22	
2			866			866				617	10100	866	CU YD	COMPACTED AGGREGATE	
						2				617	25000	2	M GAL	WATER	
			129,994			129,994				SPECIAL	69098100	129,994	FT	MISC.: SAFETY EDGE TREATMENT	7/46

GENERAL SUMMARY

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COUNTY AND ROUTE	DIRECTION AND LANE	SLM BEGIN	SLM END	WIDTH OF REPAIR	LENGTH OF REPAIR	251	253	690	COUNTY AND ROUTE	DRIVE TYPE	NUMBER OF DRIVES	254	407	442	448	617		
						PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN	PAVEMENT REPAIR, AS PER PLAN	SPECIAL MISC.: SAFETY EDGE TREATMENT				PAVEMENT PLANING ASPHALT CONCRETE VARIABLE DEPTH	TACK COAT 0.075 GAL. PER SQ. YD.	ASPHALT SURFACE COURSE 12.5mm TYPE A 1.5"DEPTH	ASPHALT SURFACE COURSE PG64-22 1.5"DEPTH	COMPACTED AGGREGATE		
						SQ. YD.	CU. YD.	FT.				SQ. YD.	GAL.	CU. YD.	CU. YD.	CU. YD.		
MAD-29	EB & WB	"AS DIRECTED BY PROJECT ENGINEER"					50											
	EB	0.20	0.34	6'	720'	480			MAD-29	ASPHALT	20	20	10	12	8			
	EB	0.40	0.49	6'	480'	320				GRAVEL	64					64		
	EB	0.60	0.62	6'	120'	80				CONCRETE	1	1	1	1				
	EB	0.80	0.94	6'	720'	480												
	EB	1.00	1.02	6'	120'	80												
	EB	1.20	1.23	6'	160'	107												
	EB	1.40	1.44	6'	200'	133												
	EB	2.40	2.54	6'	720'	480												
	EB	2.60	2.74	4'	720'	320												
	EB	3.00	3.02	6'	120'	80												
	EB	4.00	4.06	4'	320'	142												
	EB	4.40	4.43	4'	160'	71												
	EB	4.60	4.62	4'	80'	36												
	EB	4.80	4.85	6'	240'	160												
	EB	5.00	5.05	4'	280'	124												
	EB	5.20	5.24	6'	200'	133												
	EB	5.40	5.44	6'	200'	133												
	EB	5.60	5.62	6'	80'	53												
	EB	6.20	6.22	4'	120'	53												
	EB	6.40	6.45	6'	240'	160												
	EB	7.20	7.28	6'	400'	267												
	EB	7.40	7.44	6'	200'	133												
	EB	7.60	7.65	6'	240'	160												
	EB	7.80	7.84	6'	200'	133												
	EB	8.00	8.04	6'	200'	133												
	EB	8.20	8.25	6'	240'	160												
	EB	8.80	8.88	6'	400'	267			MAD-29	ASPHALT	11	611	458	9	16			
	EB	9.20	9.28	6'	400'	267				GRAVEL	2					5		
	EB	9.40	9.48	6'	440'	293												
	EB	11.00	11.03	4'	160'	71												
	WB	0.20	0.25	6'	280'	187												
	WB	0.40	0.44	6'	200'	133												
	WB	0.80	0.90	6'	520'	347												
	WB	1.00	1.04	6'	200'	133												
	WB	1.20	1.24	6'	200'	133												
	WB	1.40	1.45	6'	280'	187												
	WB	1.80	1.81	6'	40'	27												
	WB	3.20	3.34	6'	720'	480												
	WB	3.60	3.62	6'	80'	53												
	WB	4.40	4.51	6'	600'	400												
	WB	4.60	4.74	4'	720'	320												
	WB	5.80	5.81	6'	40'	27												
	WB	6.20	6.29	6'	500'	333												
	WB	6.40	6.46	6'	320'	213												
	WB	7.00	7.01	6'	40'	27												
	WB	7.20	7.22	6'	120'	80												
	WB	10.80	10.87	6'	380'	253												
	WB	11.20	11.25	6'	280'	187												
	WB	11.60	11.65	6'	240'	160												
	WB	11.80	11.89	6'	500'	333												
	EB & WB	0.00	10.10					106,656										
	EB & WB	10.73	12.94					23,338										
TOTALS CARRIED TO PLAN SHEET 12/46						9,522	50	129,994	TOTALS CARRIED TO PLAN SHEET 15/46			632	469	22	24	69		

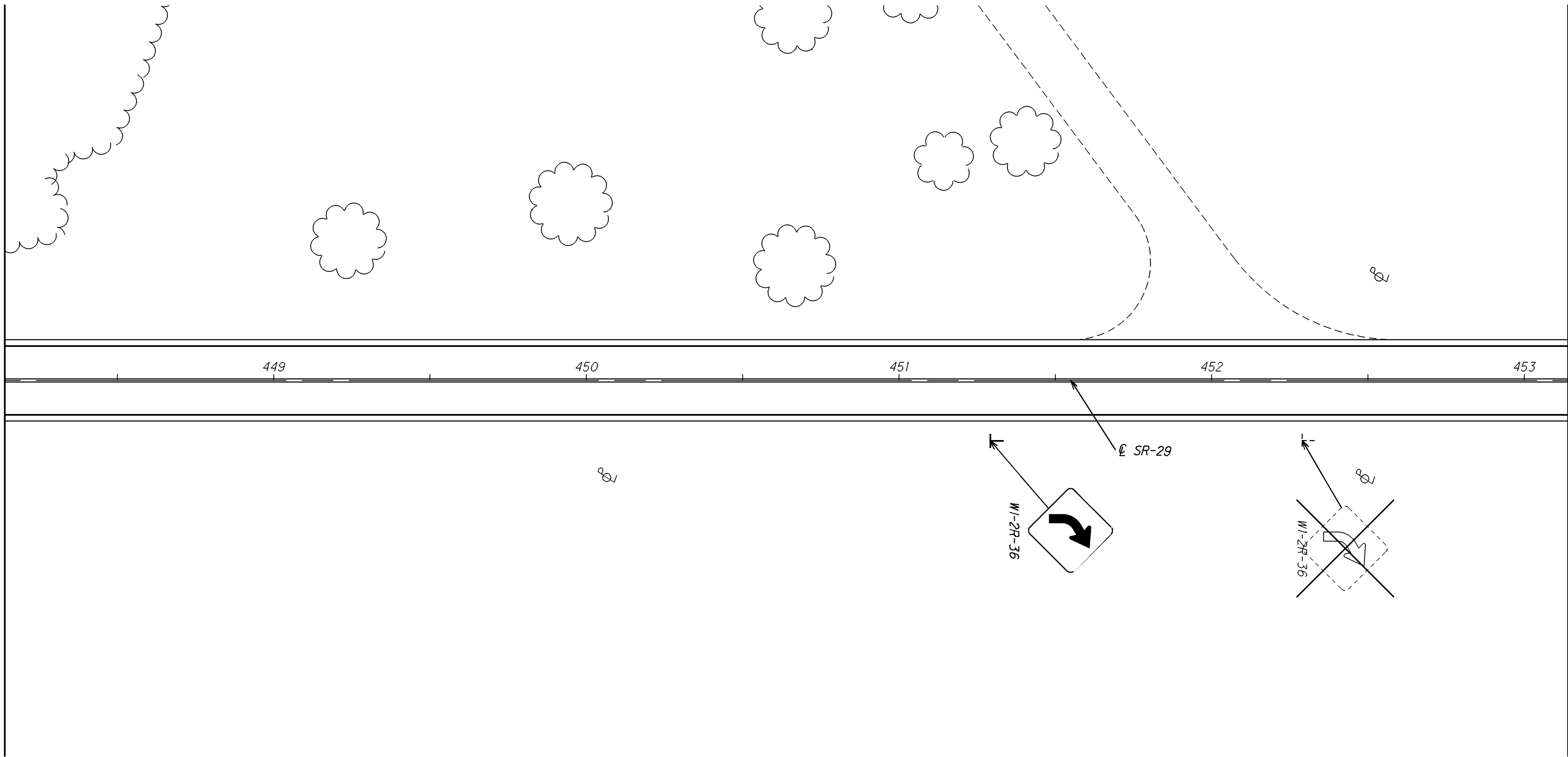
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PAVEMENT REPAIR SUB SUMMARY
MISCELLANEOUS PAVEMENT CALCULATIONS
MAD - 29 - 0.00

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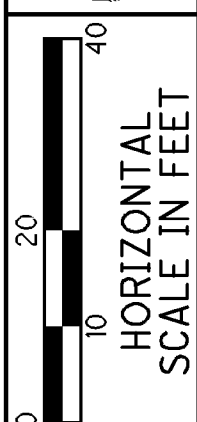
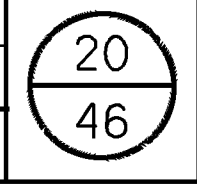
L O C A T I O N	C O U N T Y	R O U T E	S L M	S L M	T Y P E #	LENGTH	LEFT SHOULDER	PAVEMENT	RIGHT SHOULDER	202	203	203	204	254	304	301	407	442	448	617	REMARK
										CURB REMOVED	EXCAVATION	EXCAVATION	SUBGRADE COMPACTION	PAVEMENT PLANING ASPHALT CONCRETE	AGGREGATE BASE	ASHPHALT CONCRETE PG64-22	TACK COAT AT 0.075	ASPHALT SURFACE COURSE 12.5mm TYPE A (448)	ASPHALT SURFACE COURSE TYPE 1 PG64-22	COMPACTED AGGREGATE	
										FT.	CU. YD.	CU. YD.	SQ. YD.	SQ. YD.	CU. YD.	CU. YD.	GAL. PER SQ. YD.	CU. YD.	CU. YD.	CU. YD.	
1	MAD	29	0.000	0.610	1	3,221'	2'	22'	2'					217			698		388	40	CHAMPAIGN COUNTY LINE PROVIDED BUTT JOINT
			0.610	0.628	1	93'															STRUCTURE DEDUCT FOR MAD-29-0.61
			0.627	5.932	1	28,010'	2'	22'	2'					217			6,069		3,372	344	MAINLINE MAD-29
			5.932	6.027	1	500'								569			117		65	8	CARRIED FROM PLAN SHEET 19/46
			6.026	9.090	1	16,178'	2'	22'	2'								3,505		1,947	199	CHANGE IN ASPHALT TYPES
			9.090	9.529	2	2,318'	2'	22'	2'								502	279		29	CHANGE IN ASPHALT TYPES
			9.528	9.623	2	500'								434			51	28		3	CARRIED FROM PLAN SHEET 23/46
			9.623	10.100	2	2,519'	2'	22'	2'								546	303		31	MAINLINE MAD-29
																					SUSPEND WORK
																					MAD-70-10.27
																					RESUME WORK
			10.730	10.950	2	1,162'	2'	22'	2'								252	140		14	MAINLINE MAD-29
			10.950	11.140	2	1,003'								114			259	144		12	SEE PLAN SHEETS 24/46 AND 25/46
			11.140	11.330	2	1,003'	2'	22'	2'								217	121		12	MAINLINE MAD-29
			11.330	11.804	2	2,503'								138			822	457		30	SEE PLAN SHEETS 26/46 TO 30/46
			11.804	12.656	2	4,499'	2'	22'	2'								975	541		55	MAINLINE MAD-29
			12.656	12.940	2/3	1,500'				165		43	64	502	21	21	440	245		20	SEE PLAN SHEETS 31/46 TO 34/46
			4.13			43'		14'									5	4	4		MAD-29-4.13 EXTRA WIDTH
			7.03			76.5'		18'									11	6	6		MAD-29-7.03 EXTRA WIDTH
											43		1,050				856	20	68		ESTIMATED QUANTITIES FOR MAILBOX
																					632
																	469	22	24	69	PULLOVERS SEE PLAN SHEET 5/46
TOTALS CARRIED TO PLAN SHEET 12/46										165	86		1,114	2,823	21	21	15,794	2,310	5,874	866	

CALCULATED ROR	CHECKED C.C.
PAVEMENT CALCULATIONS	
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15 46	

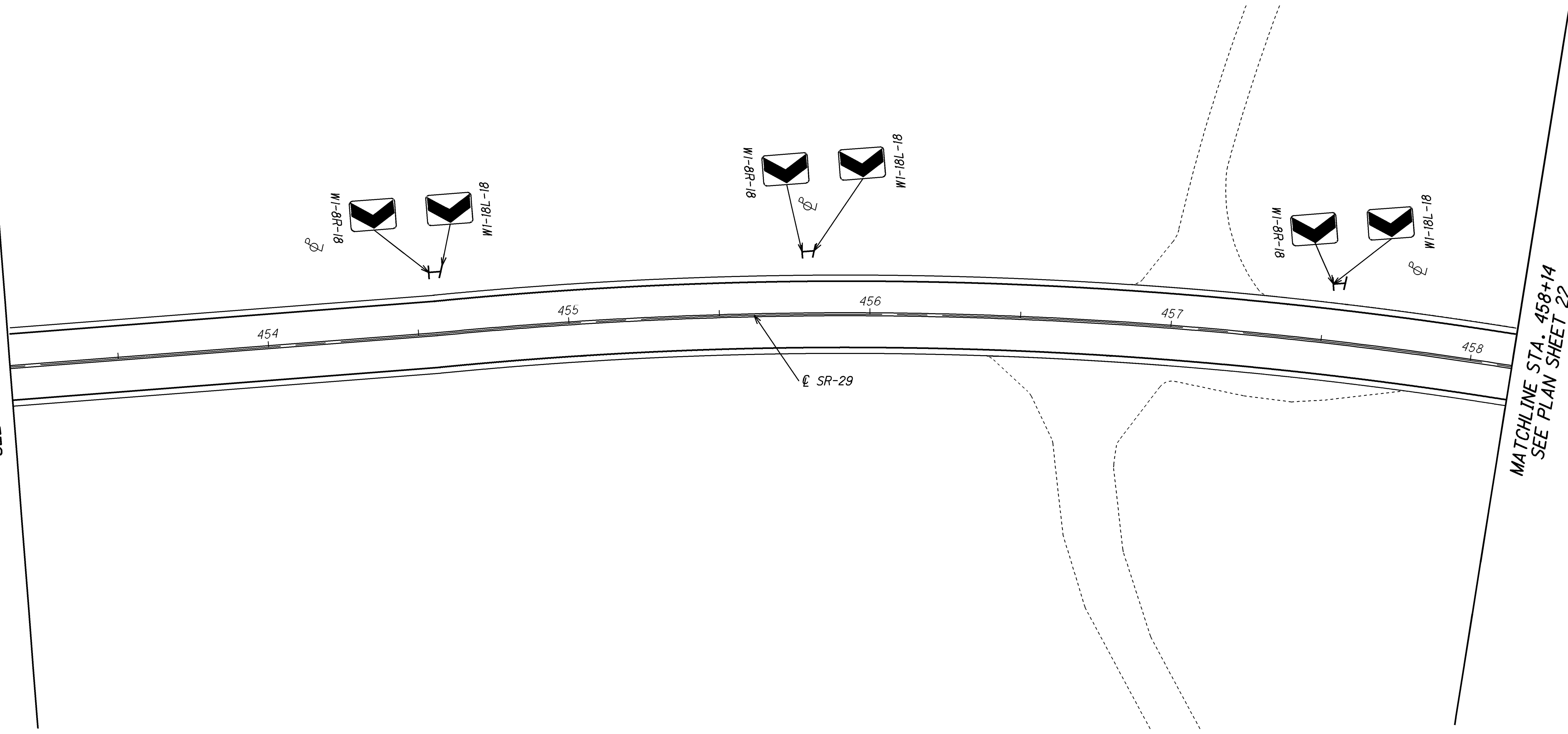
SLM 8.487 EQUALS
STA. 448+14



S T A T I O N	S T A T I O N	T Y P E C A T O R Y	PAVEMENT AND SHOULDER WIDTH	630	630	630	630	630									REMARK
				GROUND MOUNTED SUPPORT #3 POST FT.	SIGN POST REFLECTOR EACH	SIGN, FLAT SHEET SQ. FT.	REMOVAL GROUND MOUNTED SIGN AND DISPOSAL EACH	REMOVAL GROUND MOUNTED POST SUPPORT & DISPOSAL EACH									
451+30				14	1	6											1 PROPOSED WARNING ARROW
452+30							1	1									1 EXISTING WARNING ARROW
TOTALS CARRIED TO PLAN SHEET 22/46				14	1	6	1	1									



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PLAN VIEW MAD-29 SLM 8.487 TO SLM 8.582
STA. 448+14 TO STA. 453+14
MAD-29-0.00


MATCHLINE STA. 453+14
SEE PLAN SHEET 20



S T A T I O N	S T A T I O N	T Y P E I C A L	PAVEMENT AND SHOULDER WIDTH	630	630	630										REMARK
				GROUND MOUNTED SUPPORT #3 POST	SIGN POST REFLECTOR	SIGN, FLAT SHEET										
454+60				28	2	3										2 CHEVRONS
455+80				28	2	3										2 CHEVRONS
457+60				28	2	3										2 CHEVRONS
TOTALS CARRIED TO PLAN SHEET 22/46				84	6	9										

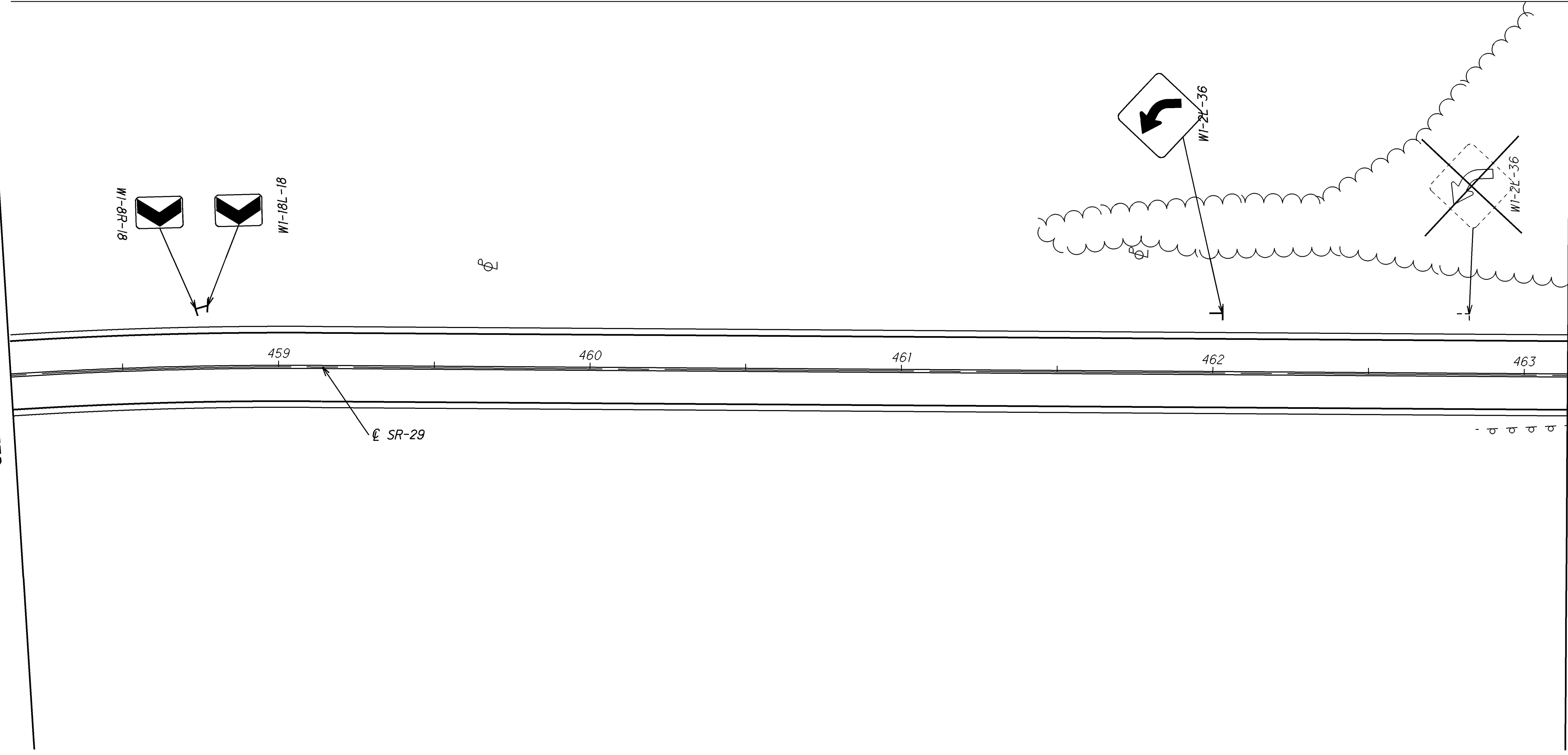
PLAN VIEW MAD-29 SLM 8.582 TO SLM 8.676
STA. 453+14 TO STA. 458+14

MAD-29-0.00

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0 10 20 40
HORIZONTAL
SCALE IN FEET

MATCHLINE STA. 458+14
SEE PLAN SHEET 21



STA. 463+14 EQUALS
SLM 8.771



CALCULATED
RDK
CHECKED
CJR

PLAN VIEW MAD-29 SLM 8.676 TO SLM 8.771
STA. 458+14 TO STA. 463+14

S T A T I O N	S T A T I O N	T Y P E I C A L	PAVEMENT AND SHOULDER WIDTH	630	630	630	630	630										REMARK	
				GROUND MOUNTED SUPPORT #3 POST	SIGN POST REFLECTOR	SIGN, FLAT SHEET	REMOVAL GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL GROUND MOUNTED POST SUPPORT & DISPOSAL											FT.
458+80				28	2	3												2 CHEVRONS	
462+05				14	1	6												1 PROPOSED WARNING ARROW	
462+82							1	1										1 EXISTING WARNING ARROW	
TOTALS CARRIED FROM PLAN SHEET 20/46				14	1	6	1	1											
TOTALS CARRIED FROM PLAN SHEET 21/46				84	6	9													
TOTALS CARRIED TO PLAN SHEET 16/46				140	10	24	2	2											

MAD-29-0.00

