

WILMINGTON RD BENCHMARKS	
SOUTH BOLT OF GR TERM. STA. 10+58.53, 18.64' LT EL. 944.97 N 524767.65 E 1521512.26	
MNS - SOUTH SIDE OF POLE STA. 17+39.40, 18.68' LT EL. 971.73 N 524706.22 E 1522192.28	
MNS - SOUTH SIDE OF POLE STA. 22+14.54, 37.23' LT EL. 978.26 N 524654.71 E 1523025.44	
MNS - SOUTH SIDE OF POLE STA. 25+78.70, 39.13' LT EL. 974.31 N 524602.65 E 1523025.44	

@ R/W WILM. CURVE DATA
 P.I.Sta. 12+55.80
 $\Delta = 10^\circ 23' 00''$ (RT)
 $Dc = 1^\circ 28' 00''$
 $R = 3,906.53'$
 $T = 354.95'$
 $L = 707.95'$
 $E = 16.09'$
 $C = 706.99'$
 $C.B. = S 86^\circ 44' 13'' E$

@ CONSTR. WILM. CURVE DATA
 P.I.Sta. 12+55.82
 $\Delta = 10^\circ 23' 02''$ (RT)
 $Dc = 1^\circ 28' 00''$
 $R = 3,906.53'$
 $T = 354.97'$
 $L = 707.99'$
 $E = 16.09'$
 $C = 707.02'$
 $C.B. = S 86^\circ 46' 07'' E$

@ R/W WILMINGTON RD STA. 24+50.00=
 @ RAMP O STA. 0+00.00 =
 @ RAMP P STA. 14+27.67

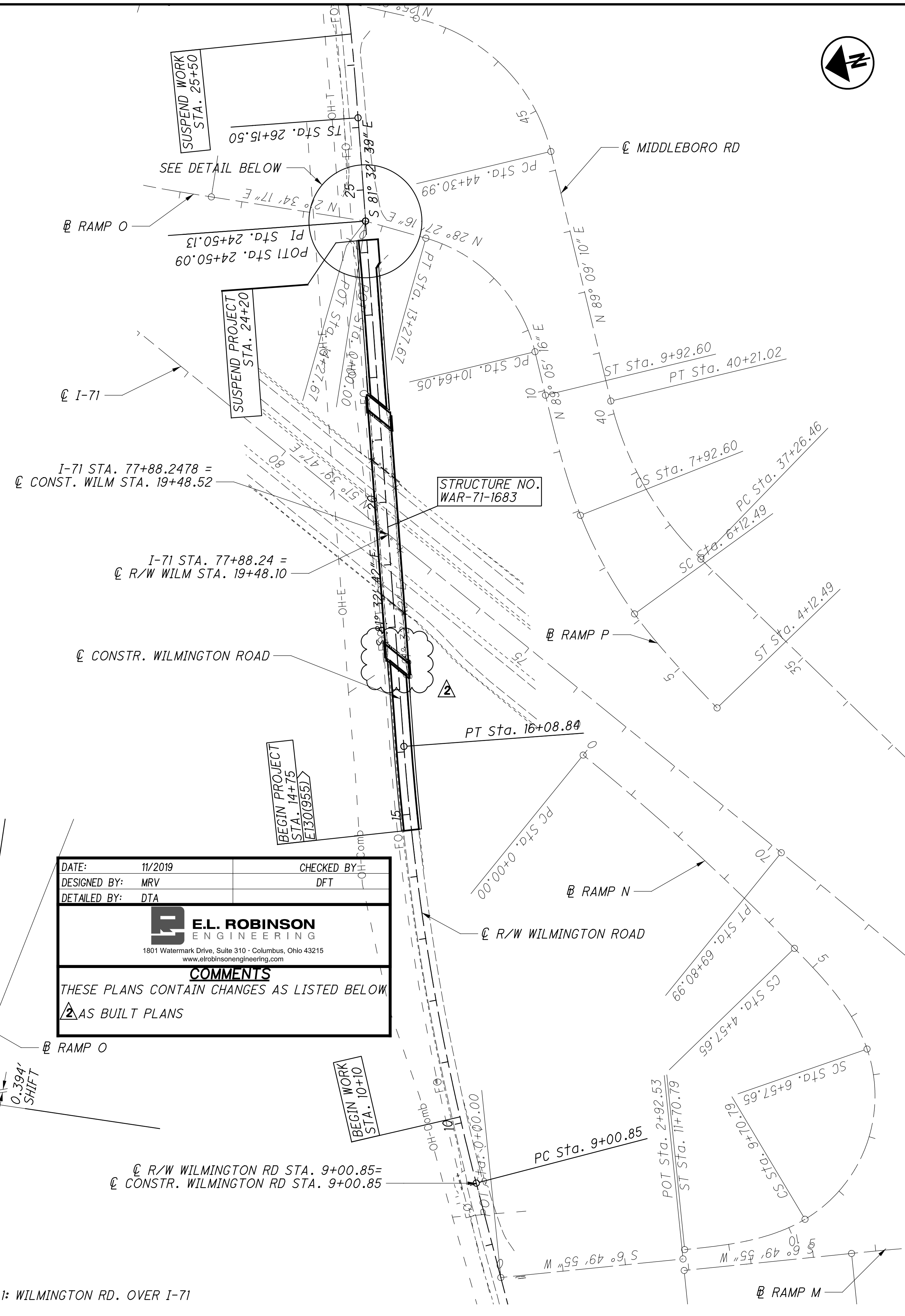
@ R/W WILMINGTON RD STA. 9+00.85=
 @ CONSTR. WILMINGTON RD STA. 9+00.85

DATE: 11/2019	CHECKED BY: DFT
DESIGNED BY: MRV	
DETAILED BY: DTA	

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COMMENTS
 THESE PLANS CONTAIN CHANGES AS LISTED BELOW
 AS BUILT PLANS

LOCATION 1: WILMINGTON RD. OVER I-71

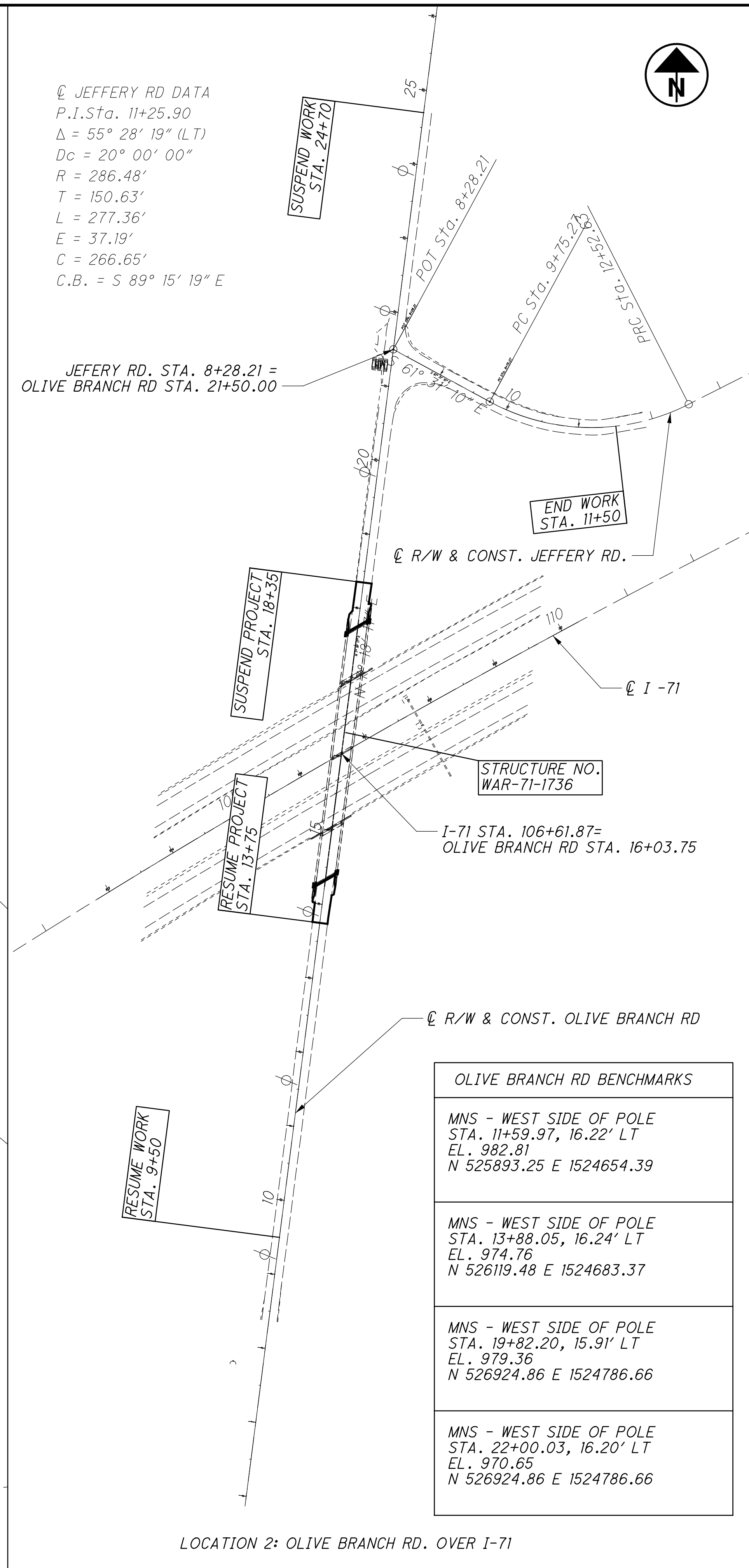


@ JEFFERY RD DATA
 P.I.Sta. 11+25.90
 $\Delta = 55^\circ 28' 19''$ (LT)
 $Dc = 20^\circ 00' 00''$
 $R = 286.48'$
 $T = 150.63'$
 $L = 277.36'$
 $E = 37.19'$
 $C = 266.65'$
 $C.B. = S 89^\circ 15' 19'' E$

JEFFERY RD. STA. 8+28.21 =
 OLIVE BRANCH RD STA. 21+50.00

OLIVE BRANCH RD BENCHMARKS	
MNS - WEST SIDE OF POLE STA. 11+59.97, 16.22' LT EL. 982.81 N 525893.25 E 1524654.39	
MNS - WEST SIDE OF POLE STA. 13+88.05, 16.24' LT EL. 974.76 N 526119.48 E 1524683.37	
MNS - WEST SIDE OF POLE STA. 19+82.20, 15.91' LT EL. 979.36 N 526924.86 E 1524786.66	
MNS - WEST SIDE OF POLE STA. 22+00.03, 16.20' LT EL. 970.65 N 526924.86 E 1524786.66	

LOCATION 2: OLIVE BRANCH RD. OVER I-71



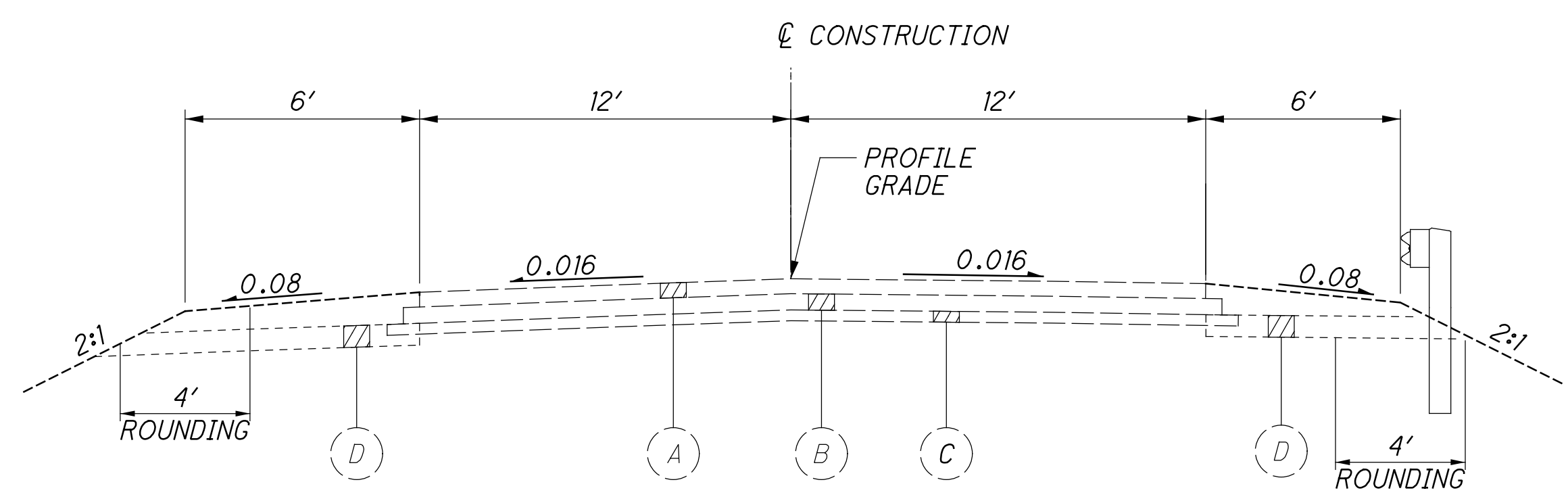
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CALCULATED BY: JTW
 CHECKED BY: MJC

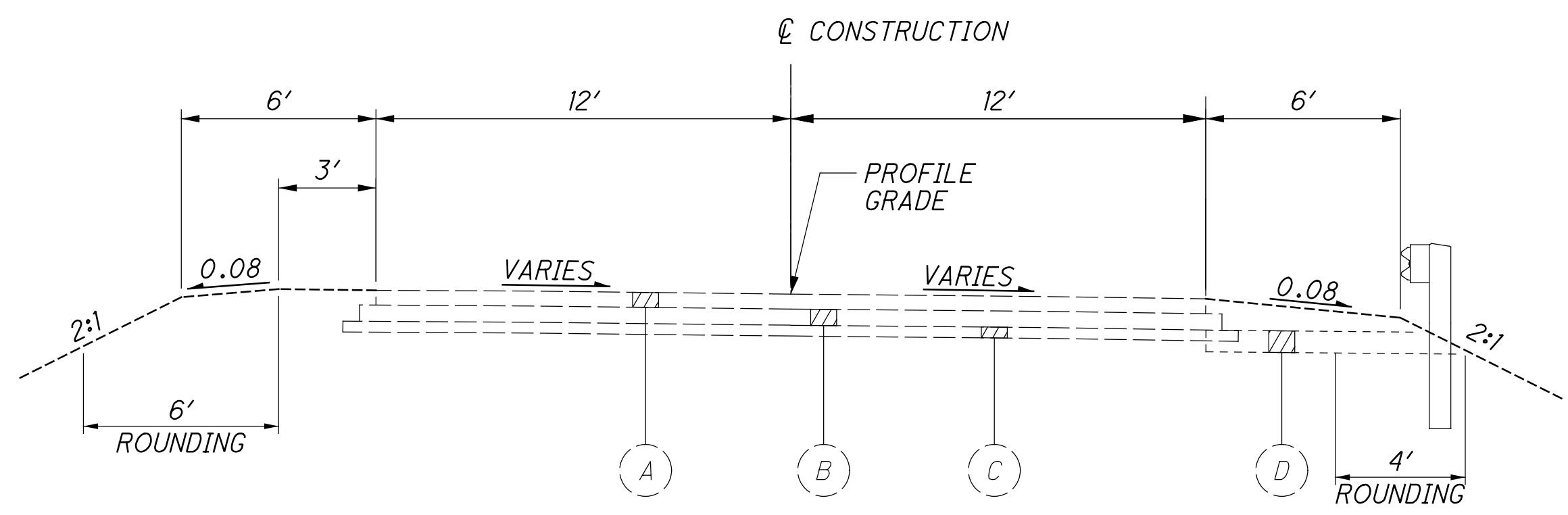
HORIZONTAL SCALE IN FEET
 0 50 100 200

SCHEMATIC PLAN

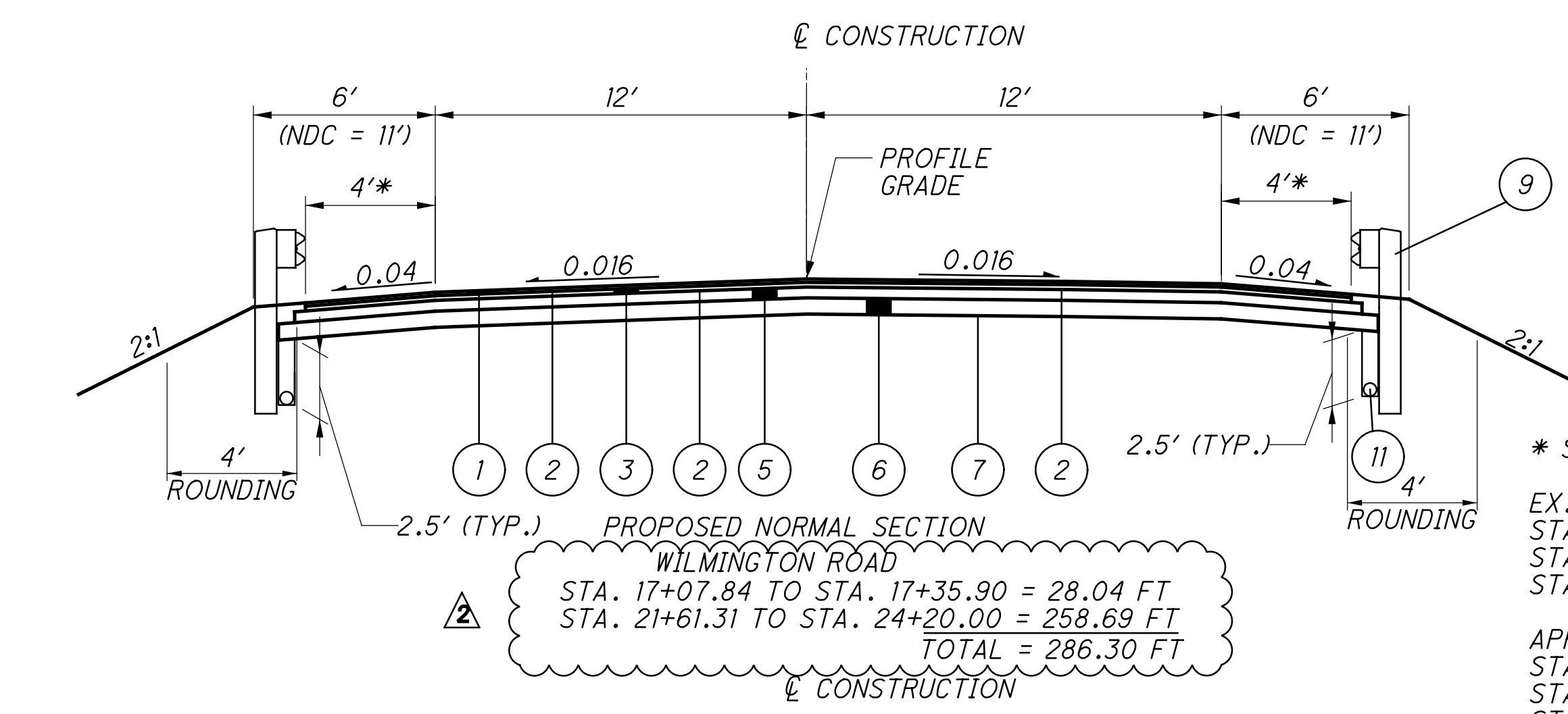
WAR-71-16.81



EXISTING NORMAL SECTION
WILMINGTON ROAD
STA. 4+50.00 TO STA. 8+75.00
STA. 16+35.00 TO STA. 17+35.01
STA. 21+61.19 TO STA. 26+15.00

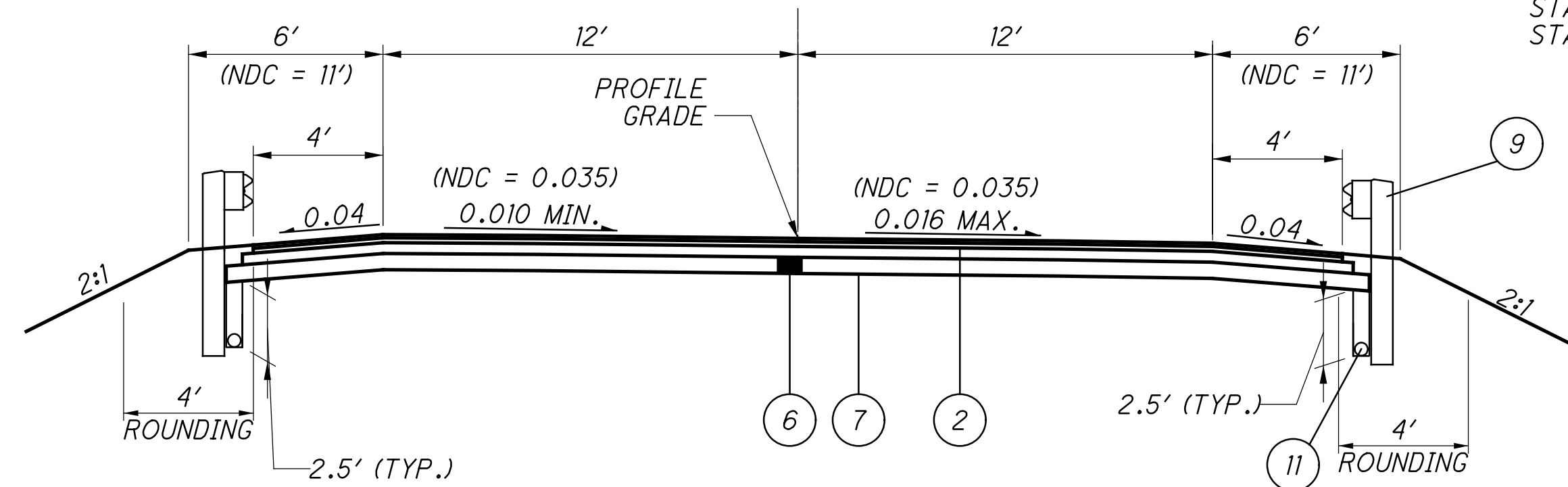


EXISTING SUPERELEVATED SECTION
WILMINGTON ROAD
STA. 8+75.00 TO STA. 16+35.00
STA. 26+15.00 TO STA. 32+00.00

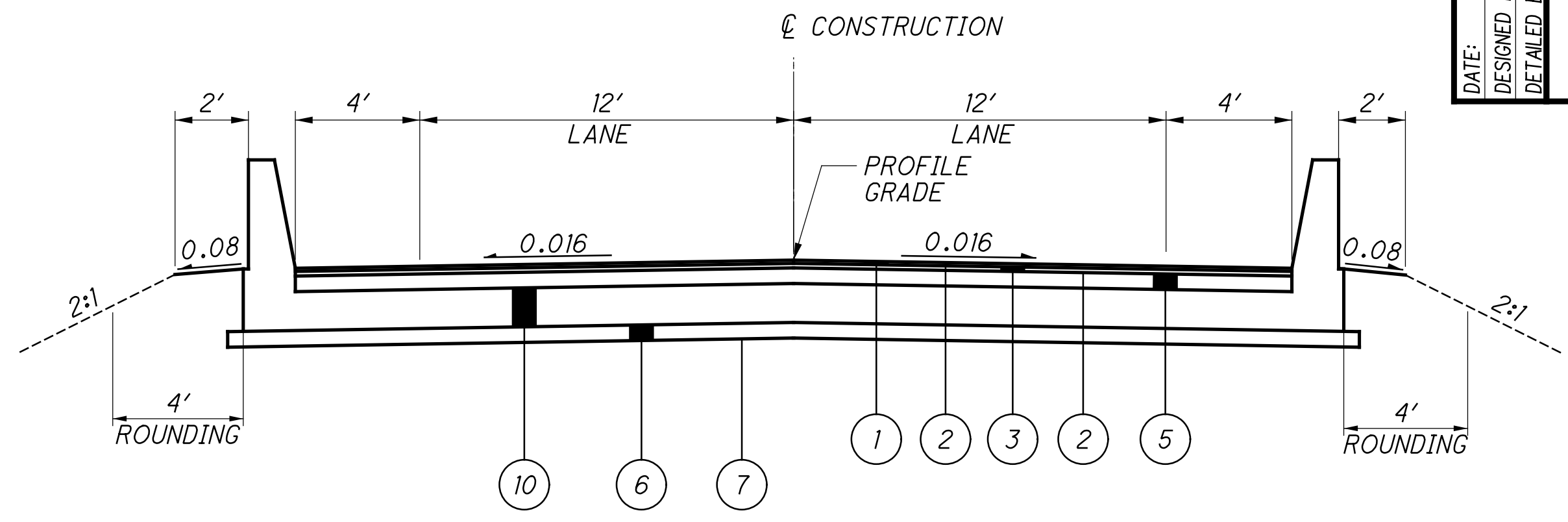


PROPOSED NORMAL SECTION
WILMINGTON ROAD
STA. 17+07.84 TO STA. 17+35.90 = 28.04 FT
STA. 21+61.31 TO STA. 24+20.00 = 258.69 FT
TOTAL = 286.30 FT

* SHOULDER SLOPE TRANSITION
EX. GRADED SHOULDER
STA. 14+54.55 TO STA. 14+75.00 EB
STA. 14+54.55 TO STA. 14+75.00 WB
STA. 24+20.00 TO STA. 24+40.45 WB
APPROACH SLAB
STA. 16+97.29 TO STA. 17+17.74 EB
STA. 21+52.79 TO STA. 21+73.24 EB
STA. 17+23.59 TO STA. 17+44.04 WB
STA. 21+79.09 TO STA. 21+99.54 WB



PROPOSED SUPERELEVATED SECTION
WILMINGTON ROAD
STA. 14+75.00 TO STA. 17+07.84 = 232.84 FT



PROPOSED APPROACH SLAB SECTION
WILMINGTON ROAD
STA. 17+35.90 TO STA. 17+60.90 = 25.00 FT
STA. 21+36.31 TO STA. 21+61.31 = 25.00 FT
TOTAL = 50.00 FT
FOR ADDITIONAL DETAILS, SEE STANDARD DRAWING AS-2-15

DATE: 11/2019	CHECKED BY: DFT
DESIGNED BY: MRV	DTA
DETAILED BY: DTA	

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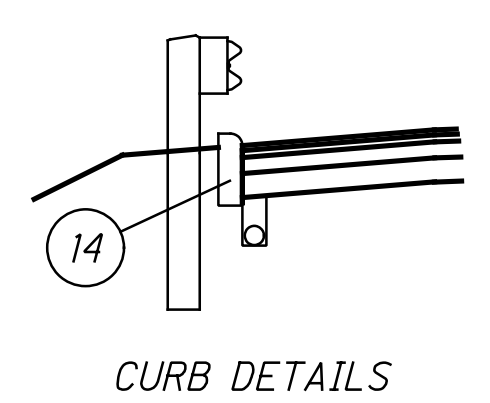
LEGEND: EXISTING

- (A) EX. 5.5" ASPHALT CONCRETE
- (B) EX. 6" AGGREGATE BASE
- (C) EX. 4" SUBBASE
- (D) EX. 8" STONE UNDERDRAIN
- (E) EX. 1-1/2" ASPHALT CONCRETE
- (E) EX. APPROACH SLAB

LEGEND: PROPOSED

- (1) ITEM 441 - 1-1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
- (2) ITEM 407 - NON-TRACKING TACK COAT
- (3) ITEM 441 - 1-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
- (4) NOT USED
- (5) ITEM 301 - ASPHALT CONCRETE BASE (t=6")
- (6) ITEM 304 - 6" AGGREGATE BASE
- (7) ITEM 204 - SUBGRADE COMPACTION, PROOF ROLLING
- (8) ITEM 441 - 1-3/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
- (9) ITEM 606 - GUARDRAIL, TYPE MGS WITH LONG POSTS
- (10) ITEM 526 - REINFORCED CONCRETE APPROACH SLAB (t = 13"), AS PER PLAN
- (11) ITEM 605 - 6" SHALLOW PIPE UNDERDRAINS WITH FABRIC WRAP
- (12) ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE - 1-3/4"

- (13) ITEM 301 - ASPHALT CONCRETE BASE (t=7")
- (14) ITEM 609 - CURB, TYPE 4-C
- (15) ITEM 617 - COMPACTED AGGREGATE



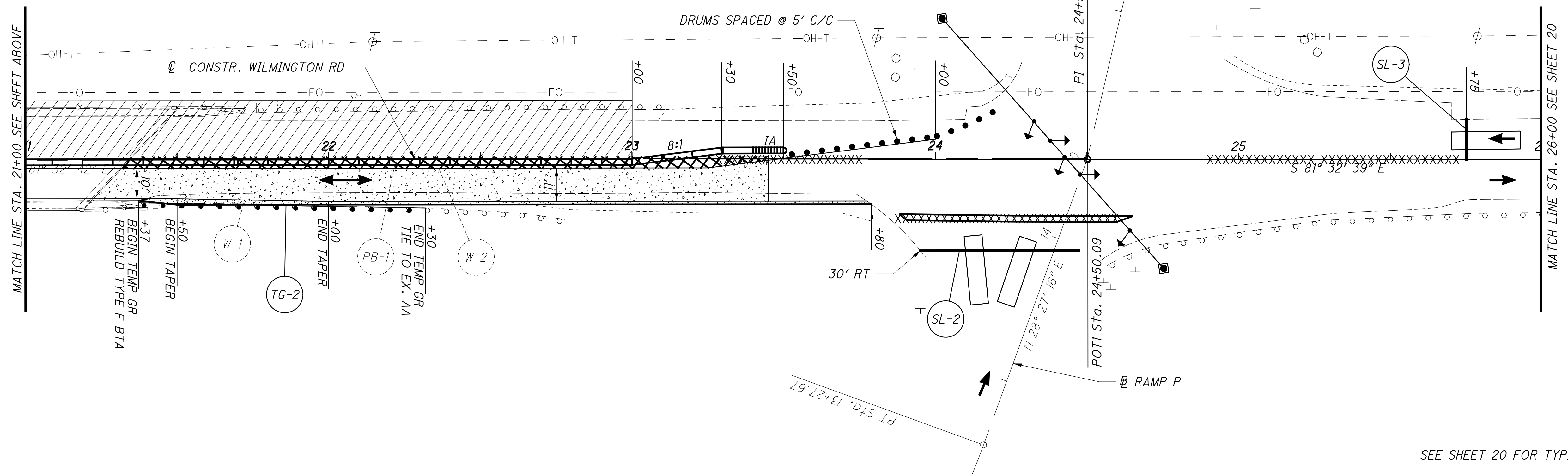
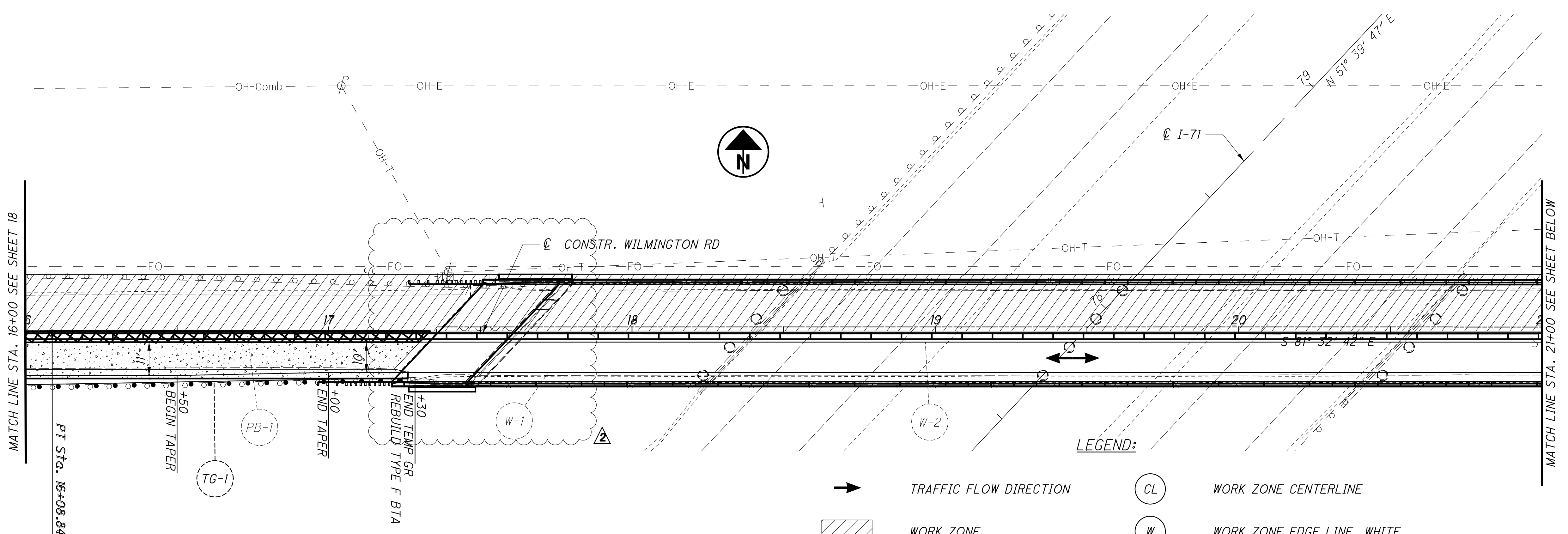
WILMINGTON ROAD:
STA. 16+94.48 TO STA. 17+20.48, RT
STA. 17+24.54 TO STA. 17+50.54, LT
STA. 21+46.28 TO STA. 21+72.28, RT
STA. 21+76.34 TO STA. 22+02.34, LT
HARVEYSBURG ROAD:
STA. 17+00.00 TO STA. 18+82.00, LT
STA. 18+89.17 TO STA. 19+02.05, LT
STA. 19+02.40 TO STA. 19+15.28, RT
STA. 22+67.41 TO STA. 22+80.29, LT
STA. 22+80.65 TO STA. 22+93.53, RT
STA. 22+87.00 TO STA. 25+90.00, LT
STA. 23+00.00 TO STA. 25+90.00, RT

TYPICAL SECTIONS

WAR - 71-16.81

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NOTES:

1. SEE STANDARD CONSTRUCTION DRAWING MT-96.11 FOR ADDITIONAL DETAILS INCLUDING ADVANCE AND WORKZONE SIGNAGE.
2. SEE SHEET 25 FOR TEMPORARY SIGNAL DETAILS.
3. PRIOR TO START OF PHASE I THE CONTRACTOR SHALL CONSTRUCT THE TEMPORARY PAVEMENT ADJACENT TO THE SHOULDERS.
4. PLACE ADVANCE WARNING SIGNAGE ALONG THE NORTHBOUND IR 71 EXIT RAMP PER MT-96.11. THE ONE LANE ROAD AHEAD SIGN IS TO BE OMITTED.

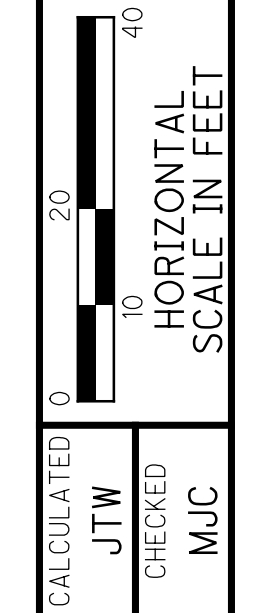
LEGEND:

- ➔ TRAFFIC FLOW DIRECTION
- ▨ WORK ZONE
- ▒ PAVEMENT FOR MAINTAINING TRAFFIC
- PROPOSED PAVEMENT COMPLETED IN PRIOR PHASE
- DRUMS
- ▬ IMPACT ATTENUATOR
- CL WORK ZONE CENTERLINE
- W WORK ZONE EDGE LINE, WHITE
- SL WORK ZONE STOP LINE
- TG TEMPORARY GUARDRAIL
- ➔ TEMPORARY TRAFFIC SIGNAL HEAD
- XXXX EXISTING PAVEMENT MARKING REMOVED

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DESIGNED BY:	MRV		
DETAILED BY:	DTA		

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COMMENTS
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MAINTENANCE OF TRAFFIC PLAN - PHASE I
STA. 16+00.00 TO STA. 26+00.00

WAR-71-16.81

SEE SHEET 20 FOR TYPICAL SECTIONS

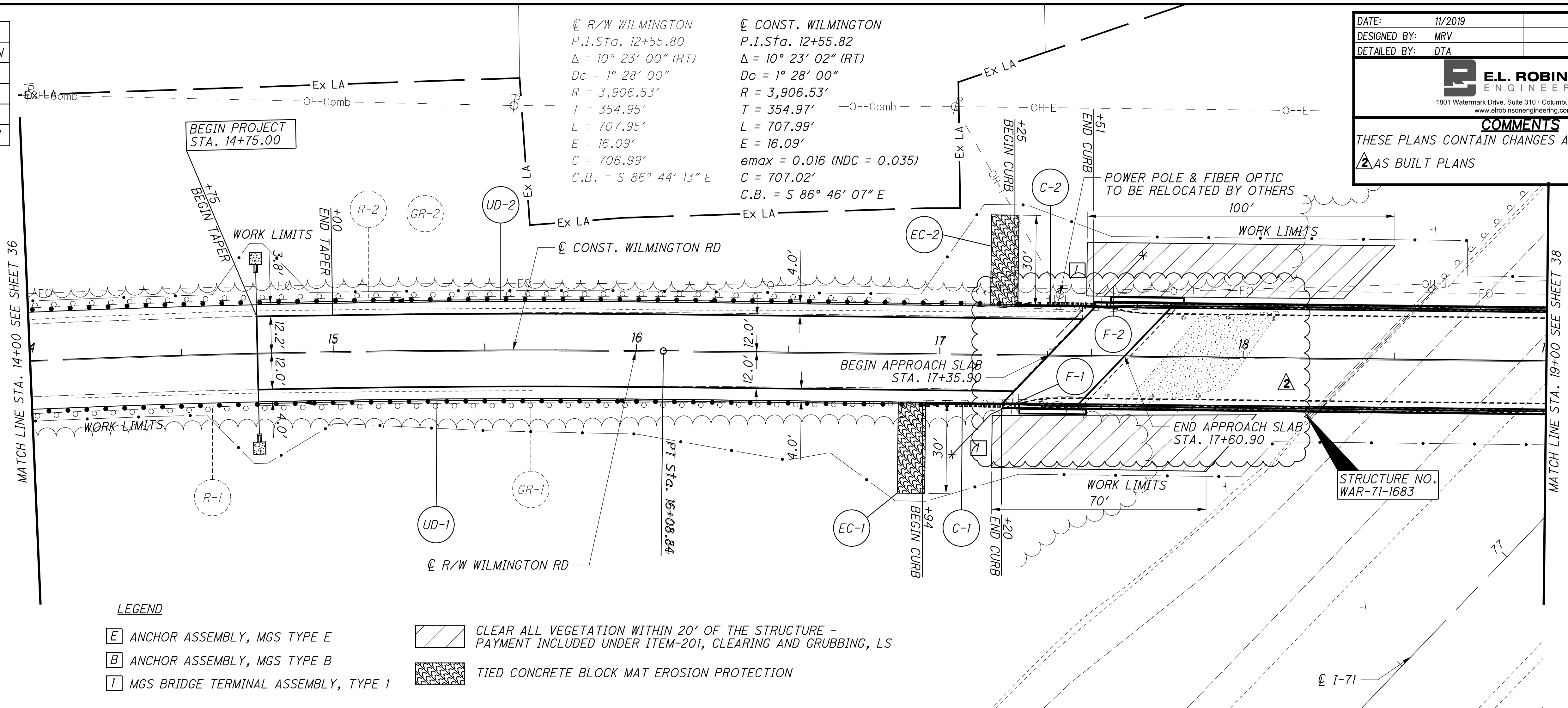
CROSS REFERENCES	
SHEET #	DESCRIPTION
32	ROADWAY
33	REMOVAL
34	EARTHWORK
35	UNDERDRAIN

DATE: 11/2019
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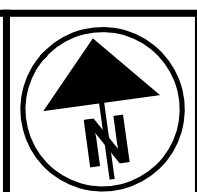
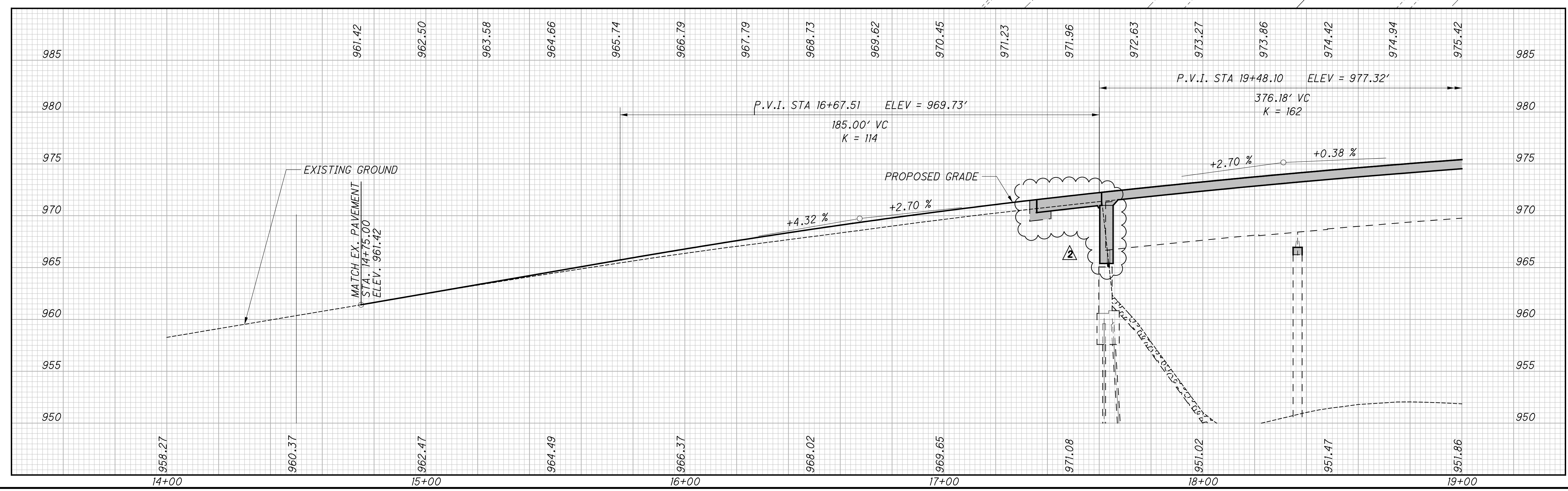
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LEGEND

- ANCHOR ASSEMBLY, MGS TYPE E
- ANCHOR ASSEMBLY, MGS TYPE B
- MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1
- CLEAR ALL VEGETATION WITHIN 20' OF THE STRUCTURE - PAYMENT INCLUDED UNDER ITEM-201, CLEARING AND GRUBBING, LS
- TIED CONCRETE BLOCK MAT EROSION PROTECTION



SCALE IN FEET
 HORIZONTAL
 1" = 40'

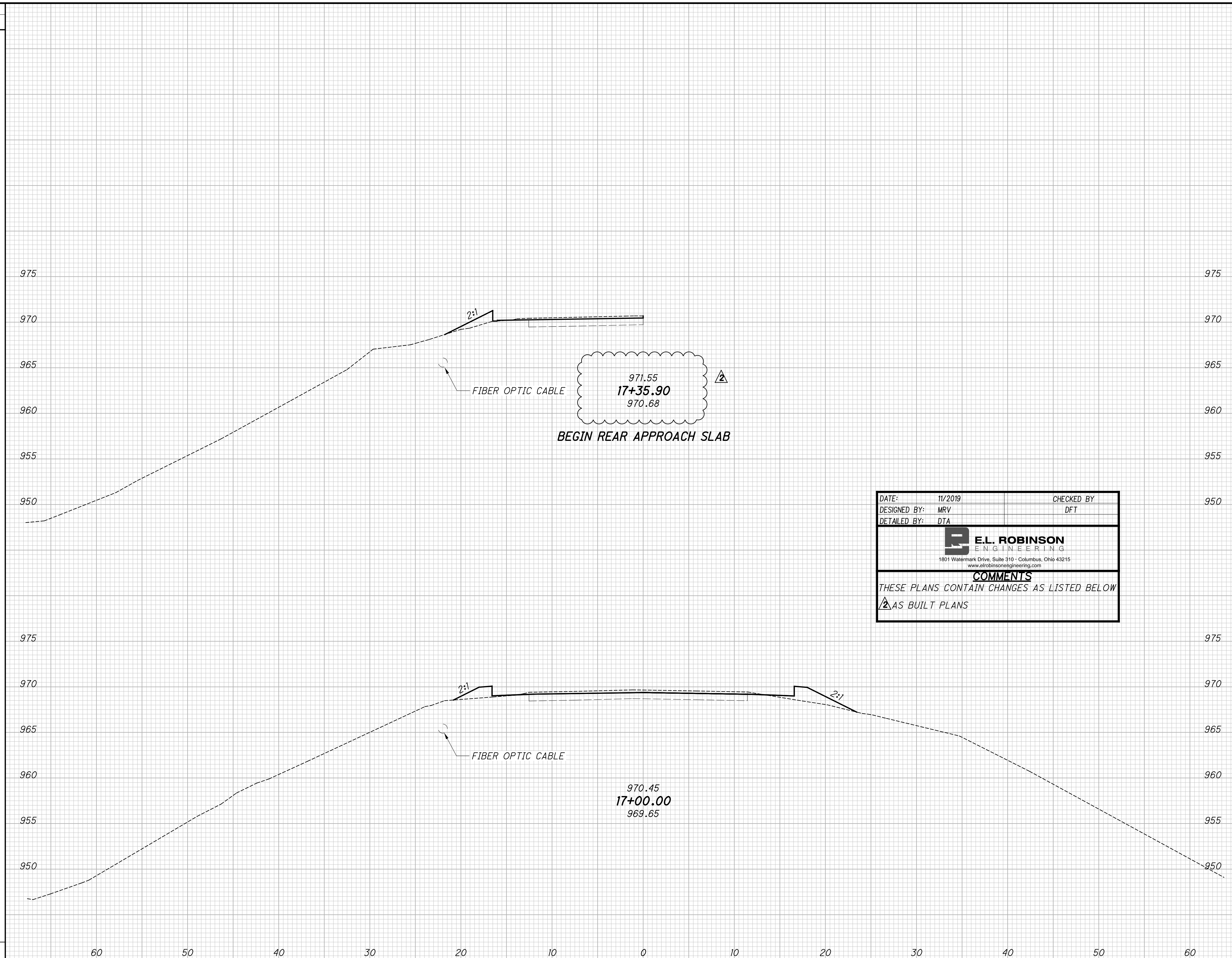
PLAN AND PROFILE - WILMINGTON ROAD
 STA. 14+00 TO STA. 19+00

WAR-71-16.81

37
 160

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971.55
17+35.90
970.68

BEGIN REAR APPROACH SLAB

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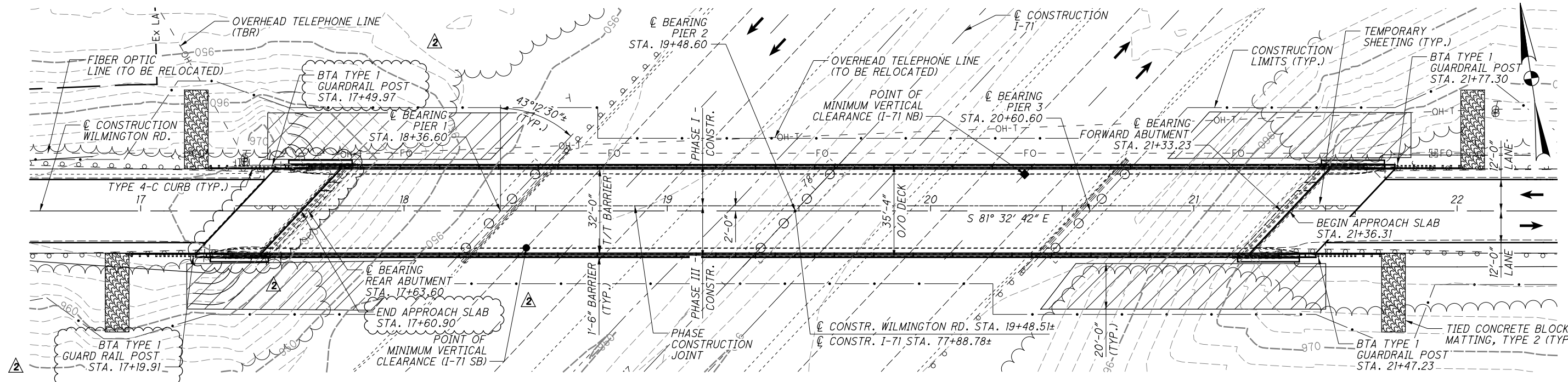
970.45
17+00.00
969.65

END	AREA		VOLUME		CALCULATED	CHECKED
	CUT	FILL	CUT	FILL		
	0	13				
			0	26		
			1	47		
	1	73				

CROSS SECTIONS - WILMINGTON RD
STA. 17+00.00 TO STA. 17+35.51

WAR-71-16.81

43
160



BENCHMARK DATA

BM #1 STA. 10+58.53,	ELEV. 944.97,	OFFSET 18.64' RT.
BM #2 STA. 17+39.40,	ELEV. 971.73,	OFFSET 18.68' LT.
BM #3 STA. 22+14.54,	ELEV. 978.26,	OFFSET 37.23' LT.
BM #4 STA. 25+78.70,	ELEV. 974.31,	OFFSET 39.13' LT.

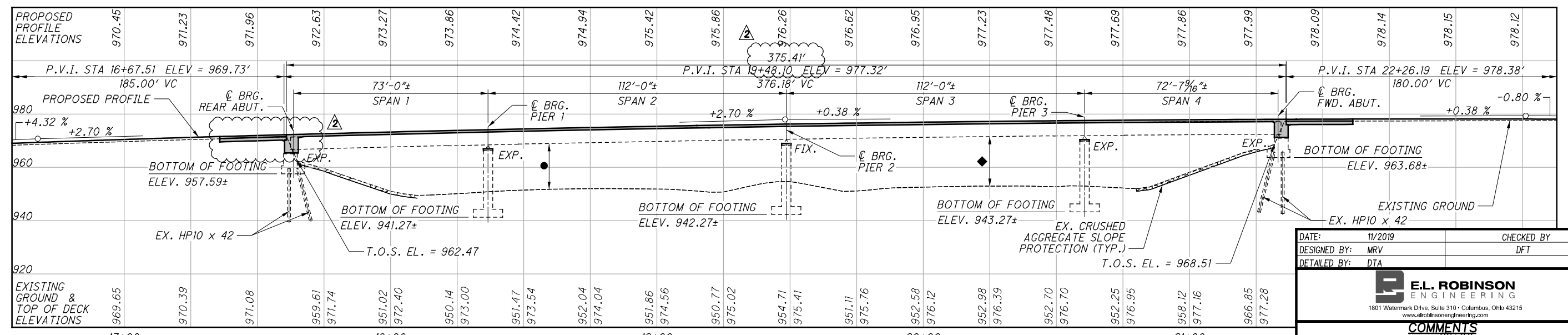
FOR ADDITIONAL BENCHMARK INFORMATION, SEE ROADWAY PLANS.

- NOTES:**
- EARTHWORK LIMITS SHOWN ARE APPROXIMATE. ACTUAL SLOPES SHALL CONFORM TO PLAN CROSS SECTIONS.
 - A DATUM CORRECTION OF -0.73' WAS USED TO ESTABLISH THE BOTTOM OF FOOTING ELEVATIONS FROM THE EXISTING PLANS.

- PROPOSED WORK:**
- REMOVE DECK, ABUTMENT BACKWALLS, EXPANSION JOINTS, APPROACH SLABS, ABUTMENT BEARINGS, AND INDICATED CROSSFRAMES.
 - RAISE BRIDGE TO CONSTRUCT PORTIONS OF NEW PIER COLUMNS, INSTALL NEW BEARINGS AT THE ABUTMENTS, AND RESET THE BEARINGS AT THE PIERS.
 - CONSTRUCT NEW PORTIONS OF THE ABUTMENTS INCLUDING WINGWALLS AND SEMI-INTEGRAL DIAPHRAGM GUIDES
 - CONSTRUCT NEW PORTIONS OF THE SUPERSTRUCTURE INCLUDING CROSSFRAMES, SEMI-INTEGRAL DIAPHRAGMS, DECK, BARRIERS, AND APPROACH SLABS.
 - INSTALL VANDAL PROTECTION FENCING, SEAL ALL NEW CONCRETE SURFACES, AND REPAIR PAINT DAMAGED DURING CONSTRUCTION.
 - REMOVE VEGETATION WITHIN 20' OF THE BRIDGE.

DESIGN TRAFFIC:
2018 ADT = 3040 2018 ADTT = 90
2038 ADT = 6200 2038 ADTT = 183
DIRECTIONAL DISTRIBUTION = 0.98

- LEGEND**
- TBR - TO BE REMOVED
 - 16'-6" REQUIRED MINIMUM VERTICAL CLEARANCE
 - 16'-8" ACTUAL MINIMUM VERTICAL CLEARANCE (I-71 SB)
 - ◆ 18'-2" ACTUAL MINIMUM VERTICAL CLEARANCE (I-71 NB)
 - ⊕ - BENCHMARK
 - ▨ - VEGETATION TO BE REMOVED



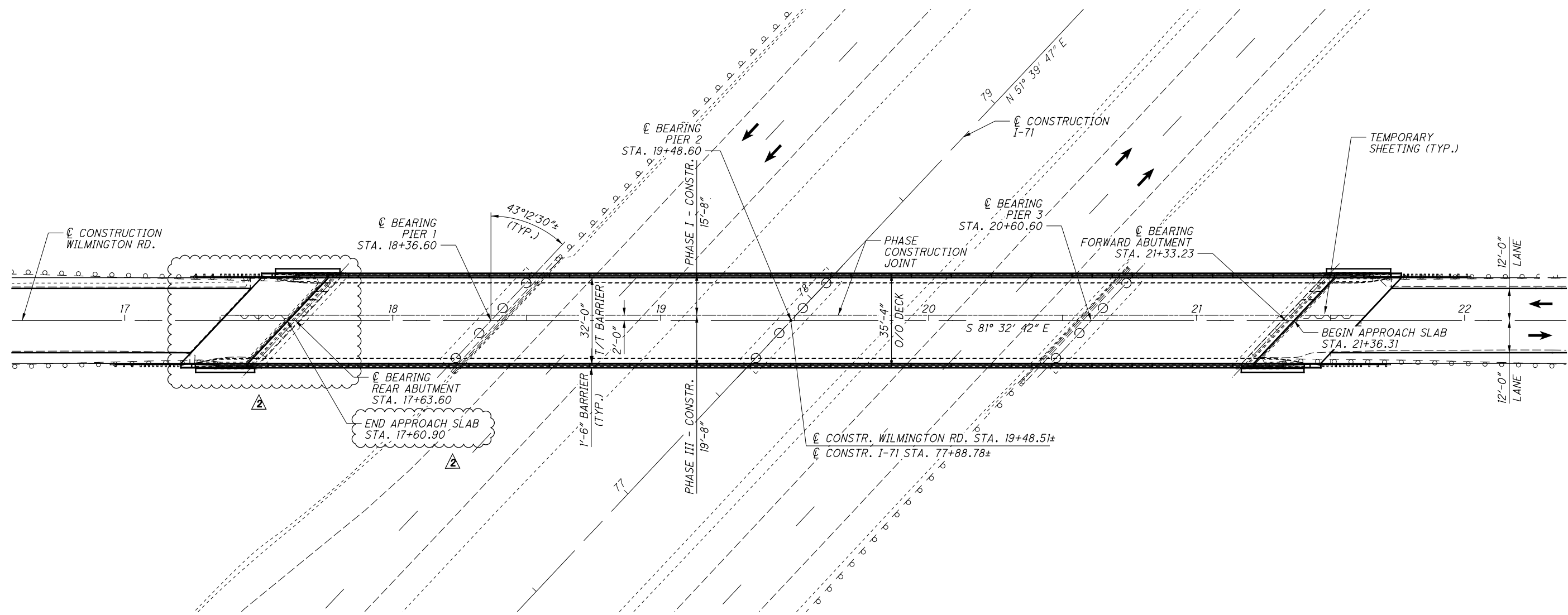
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GENERAL PLAN

DATE:	11/2019	CHECKED BY:	DFT
DESIGNED BY:	MRV		
DETAILED BY:	DTA		
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COMMENTS THESE PLANS CONTAIN CHANGES AS LISTED BELOW AS BUILT PLANS			

WAR-71-16.81
PID No. 91727

GENERAL PLAN
BRIDGE NO. WAR-71-1683
I-71 UNDER WILMINGTON RD.

DESIGNED	MRV	CHECKED	DFT
DRAWN	MRV	REVIEWED	RER
DATE	07/2016	STRUCTURE FILE NUMBER	8302316

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DATE:	11/2019	CHECKED BY:	DFT
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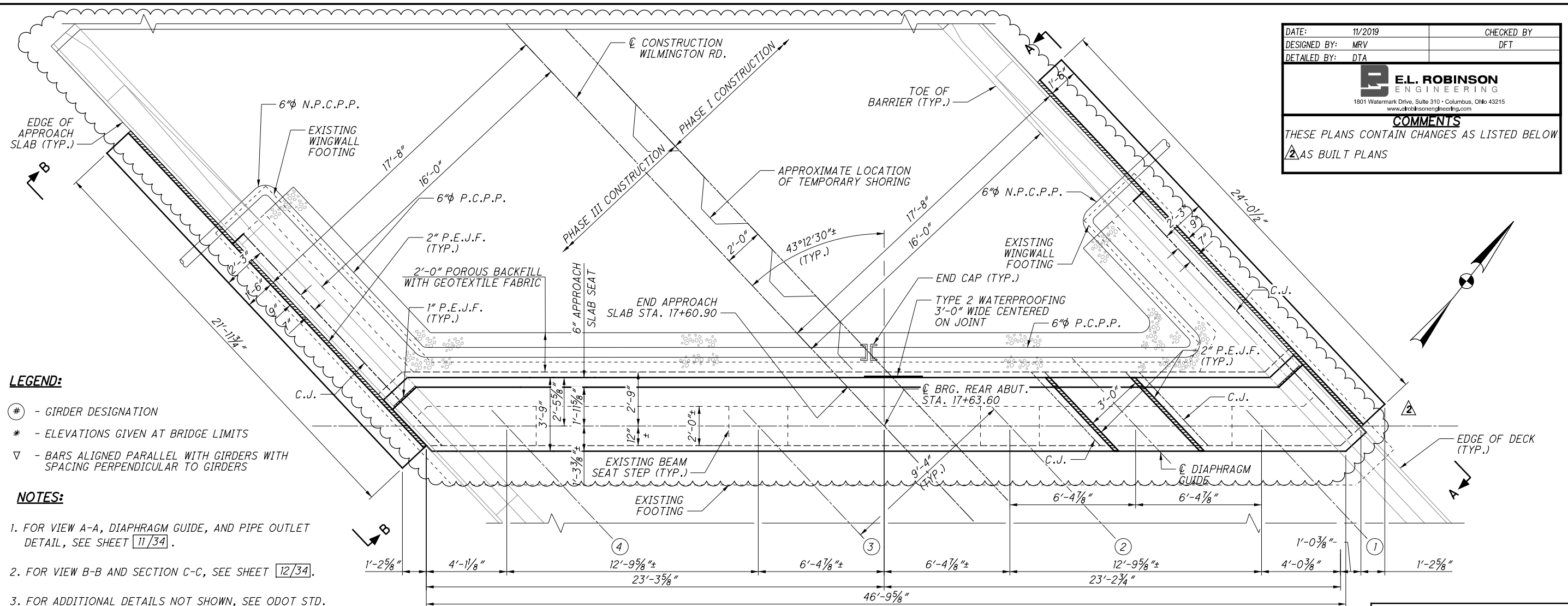
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DATE: 07/2016
 REVISION: RER
 DRAWN: JAA
 CHECKED: MRV
 STRUCTURE FILE NUMBER: 8302316

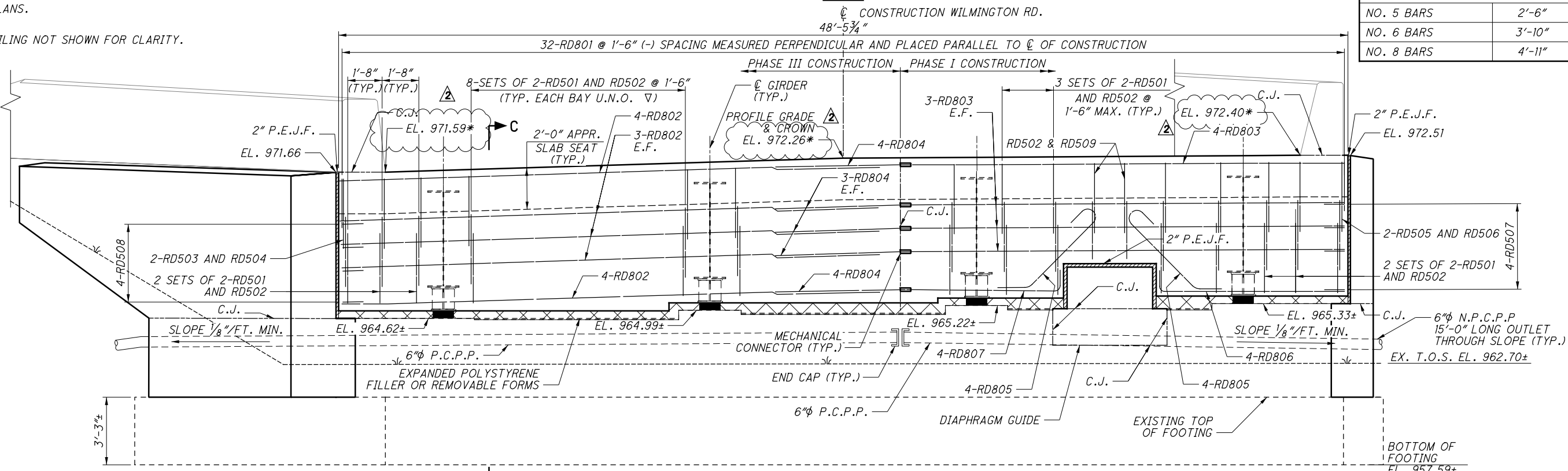
REAR ABUTMENT PLAN AND ELEVATION
 BRIDGE NO. WAR-71-1683
 I-71 UNDER WILMINGTON RD.

WAR-71-16.81
 PID No. 91727

10/34
 82/160



PLAN

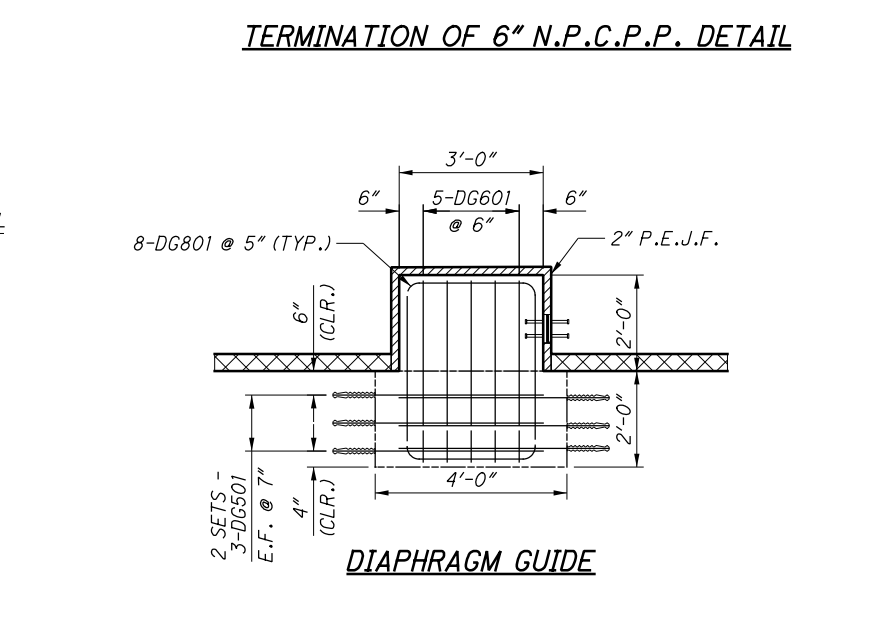
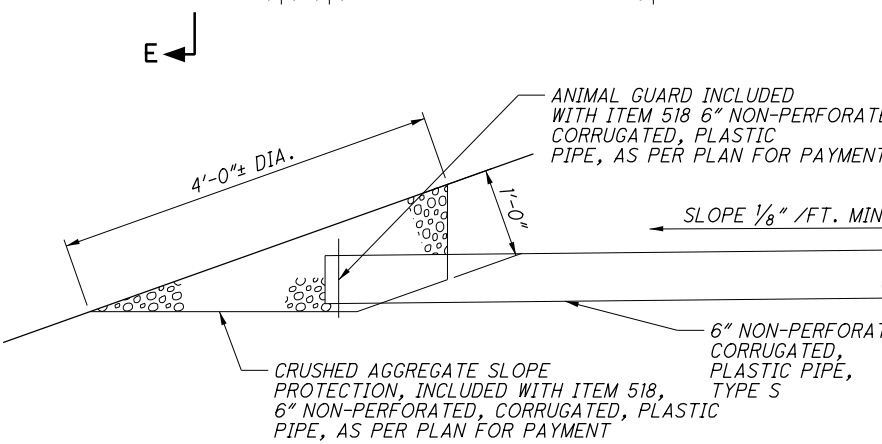
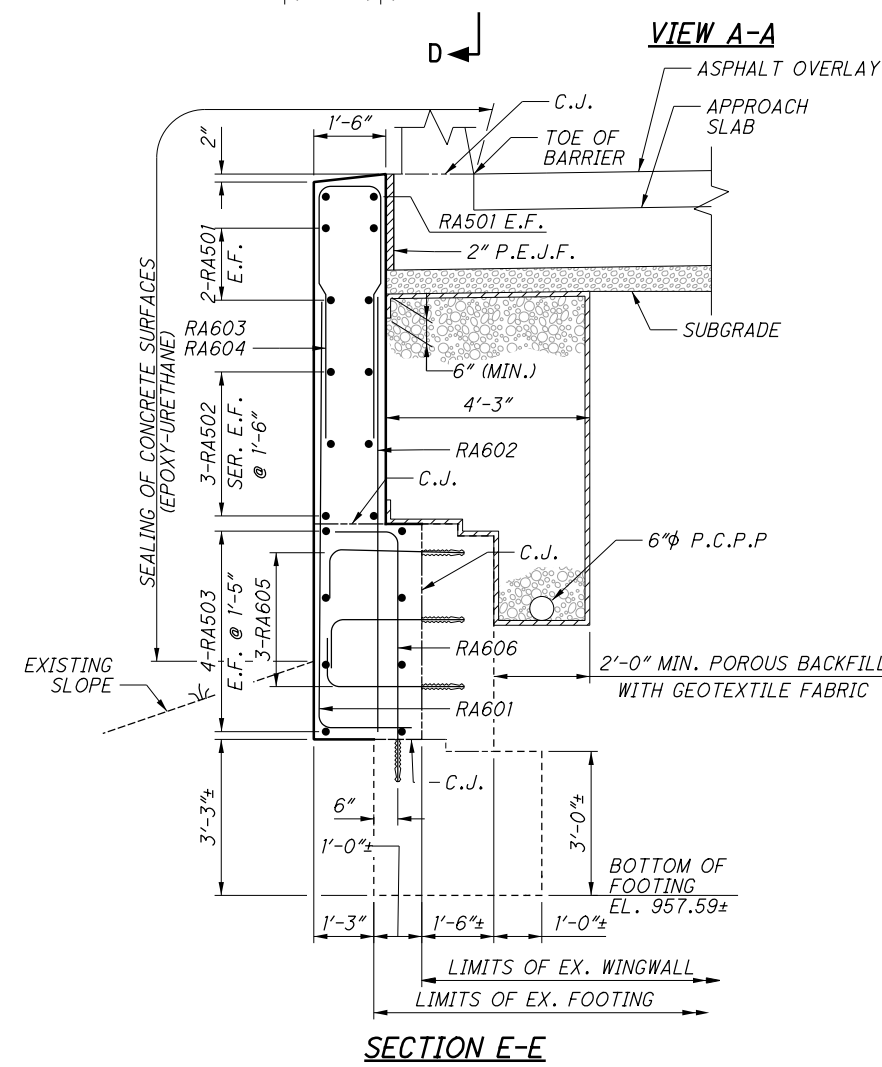
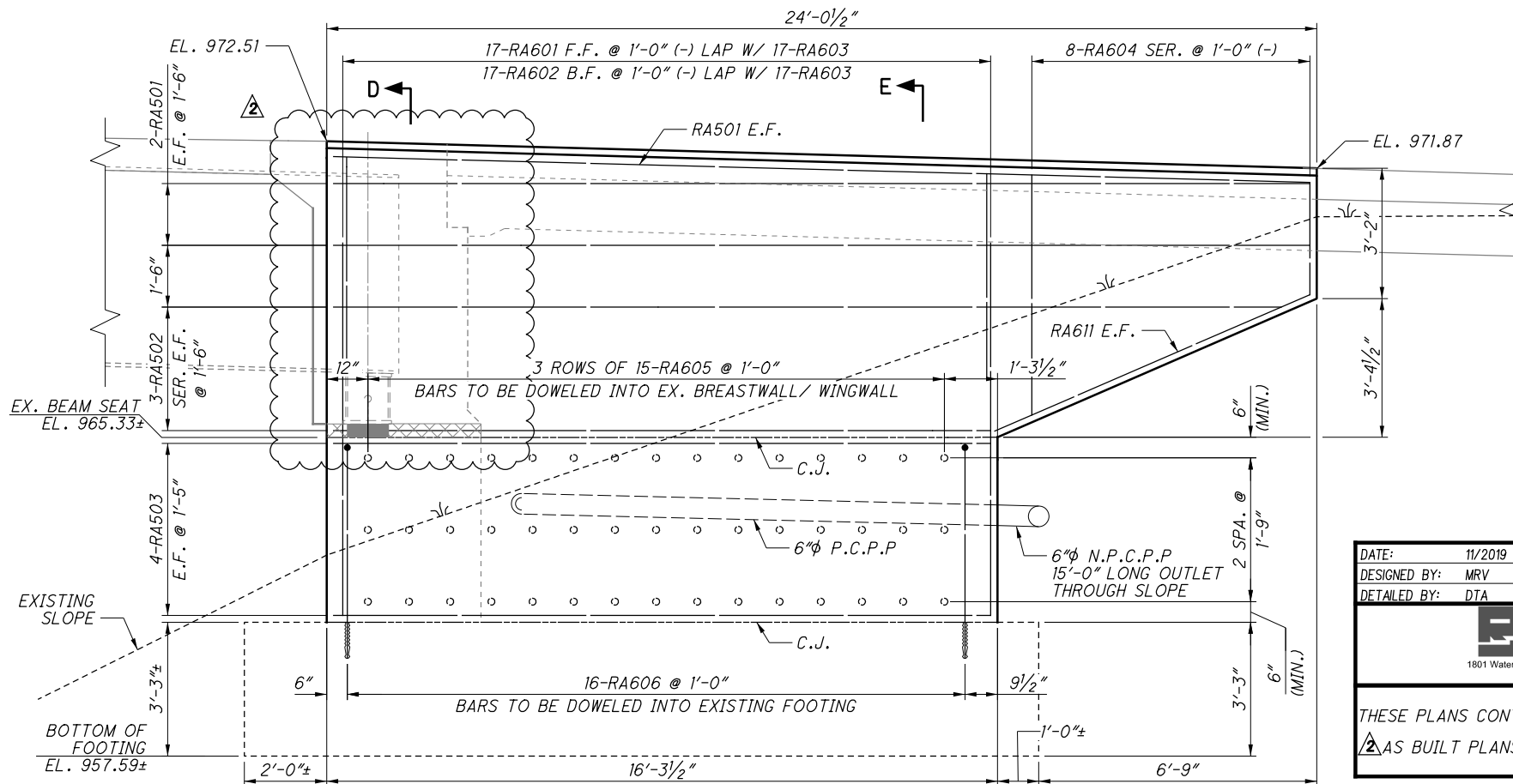


ELEVATION

MIN. LAP LENGTHS	
NO. 5 BARS	2'-6"
NO. 6 BARS	3'-10"
NO. 8 BARS	4'-11"

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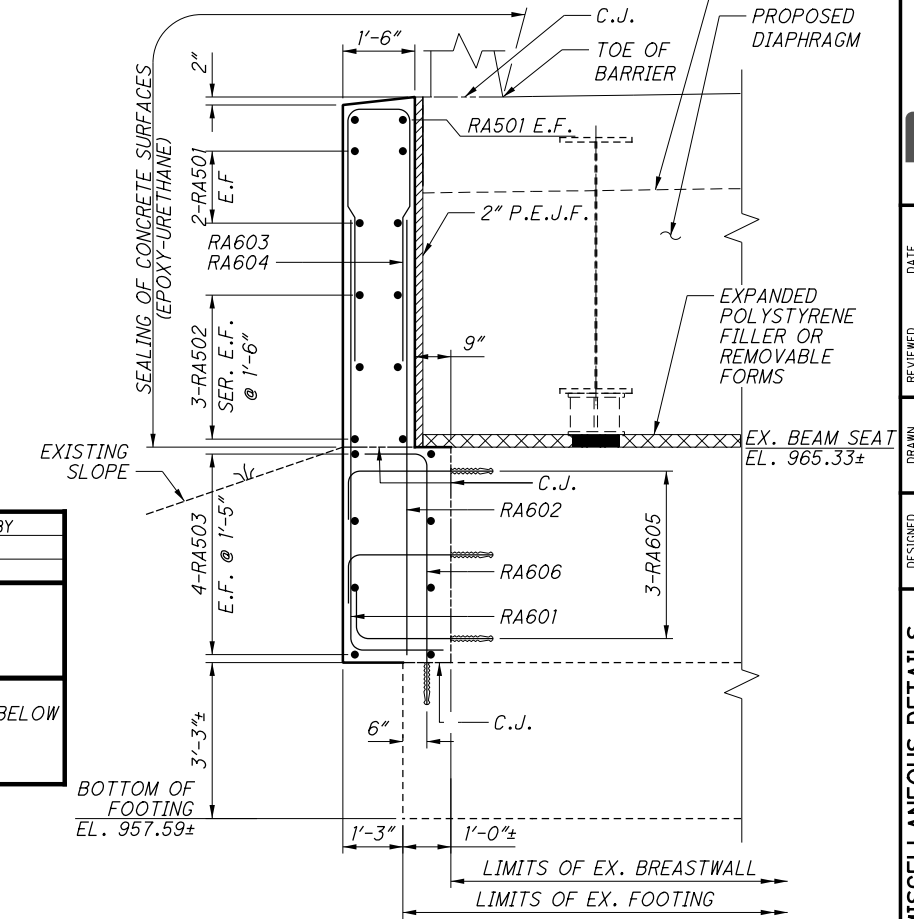
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DETAILED BY:	DTA		

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- NOTES:**
- MINIMUM EMBEDMENT DEPTH OF DOWELS TO BE 10 1/2 INCHES. FURNISH ONE OF THE FOLLOWING EPOXY TYPES FROM THE FOLLOWING MANUFACTURERS/SUPPLIERS: HILTI HIT RE 500, HILTI, 5400 SOUTH 122 ND EAST AVENUE, TULSA, OK 74146, PHONE: (800) 879-6000; POWERS FASTENERS PE1000+EPOXY, POWERS FASTENERS, 2 POWERS LANE, BREWSTER, NY 10509, PHONE: (914) 235-6300; SIMPSON STRONG-TIE EDOT SET-XP, SIMPSON STRONG-TIE, COLUMBUS, OH 43228, PHONE: (800) 899-5099. ALL MATERIALS, LABOR, AND INCIDENTALS REQUIRED FOR THE INSTALLATION OF THE DOWLS SHALL BE INCLUDED UNDER ITEM 510 - DOWEL HOLES WITH NON-SHRINK, NON-METALLIC GROUT, AS PER PLAN.
 - FOR ADDITIONAL DIAPHRAGM GUIDE DETAILS, SEE SHEET 10/34.
 - FOR BEARING DETAILS, SEE SHEET 19/34.
 - FOR ADDITIONAL DETAILS NOT SHOWN, SEE ODOT STD. DWG. SICD-1-96, ODOT STD. DWG. SICD-2-14, AND EXISTING PLANS.
 - EXISTING PILING NOT SHOWN FOR CLARITY.
 - REINFORCING STEEL MAY REQUIRE FIELD CUTTING OR BENDING TO BE PROPERLY FITTED. PAYMENT IS INCIDENTAL TO THE ITEM.
 - FOR REAR ABUTMENT RIGHT WINGWALL AND SEMI-INTEGRAL DIAPHRAGM DETAILS, SEE SHEET 12/34.
 - ADDITIONAL REINFORCEMENT, CONCRETE, LABOR AND INCIDENTALS REQUIRED TO FACILITATE THE CONSTRUCTION OF THE SEMI-INTEGRAL DIAPHRAGM GUIDE SHALL BE PAID FOR UNDER ITEM 511 - SEMI-INTEGRAL DIAPHRAGM GUIDE, AS PER PLAN.

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DATE: 07/2016
REVIEWED: RER
STRUCTURE FILE NUMBER: 8302316

DRAWN: JAA
CHECKED: MRV

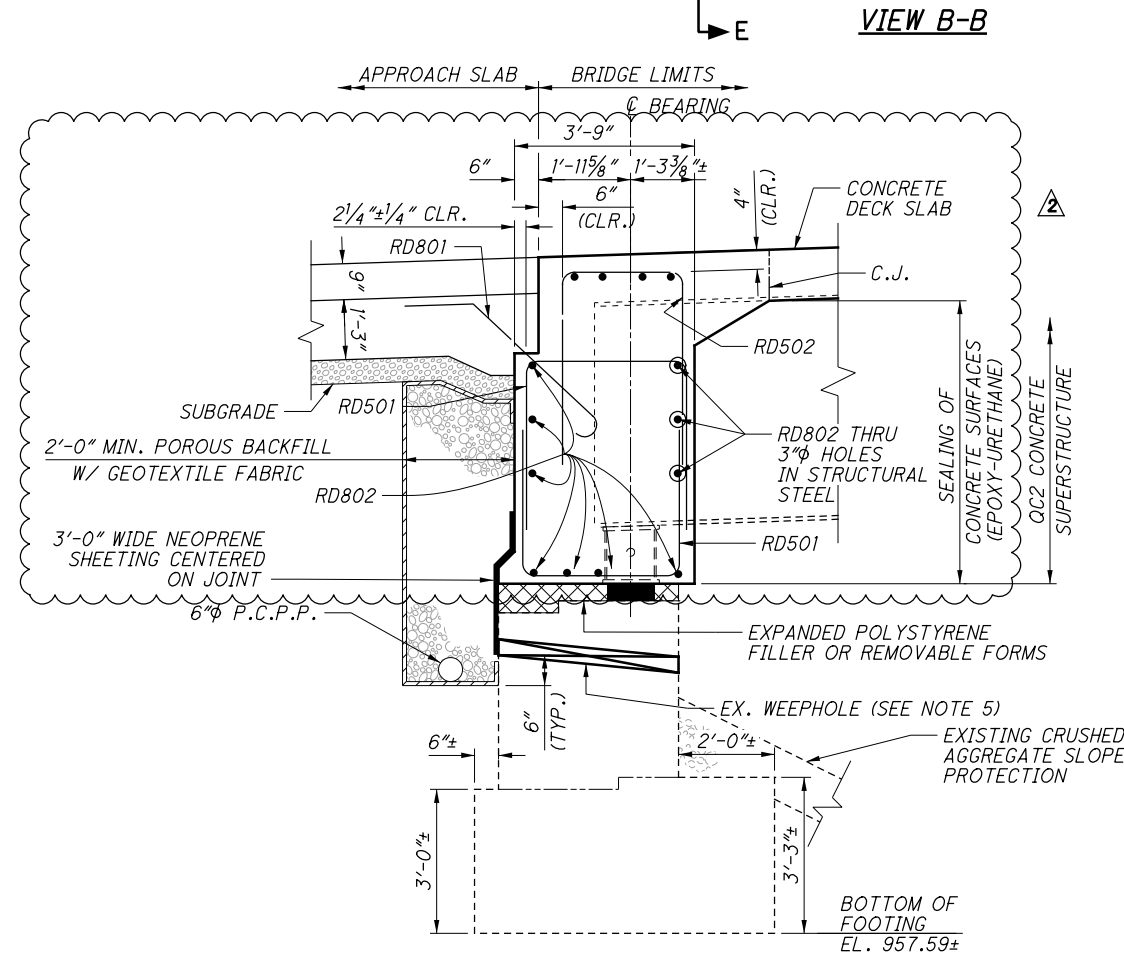
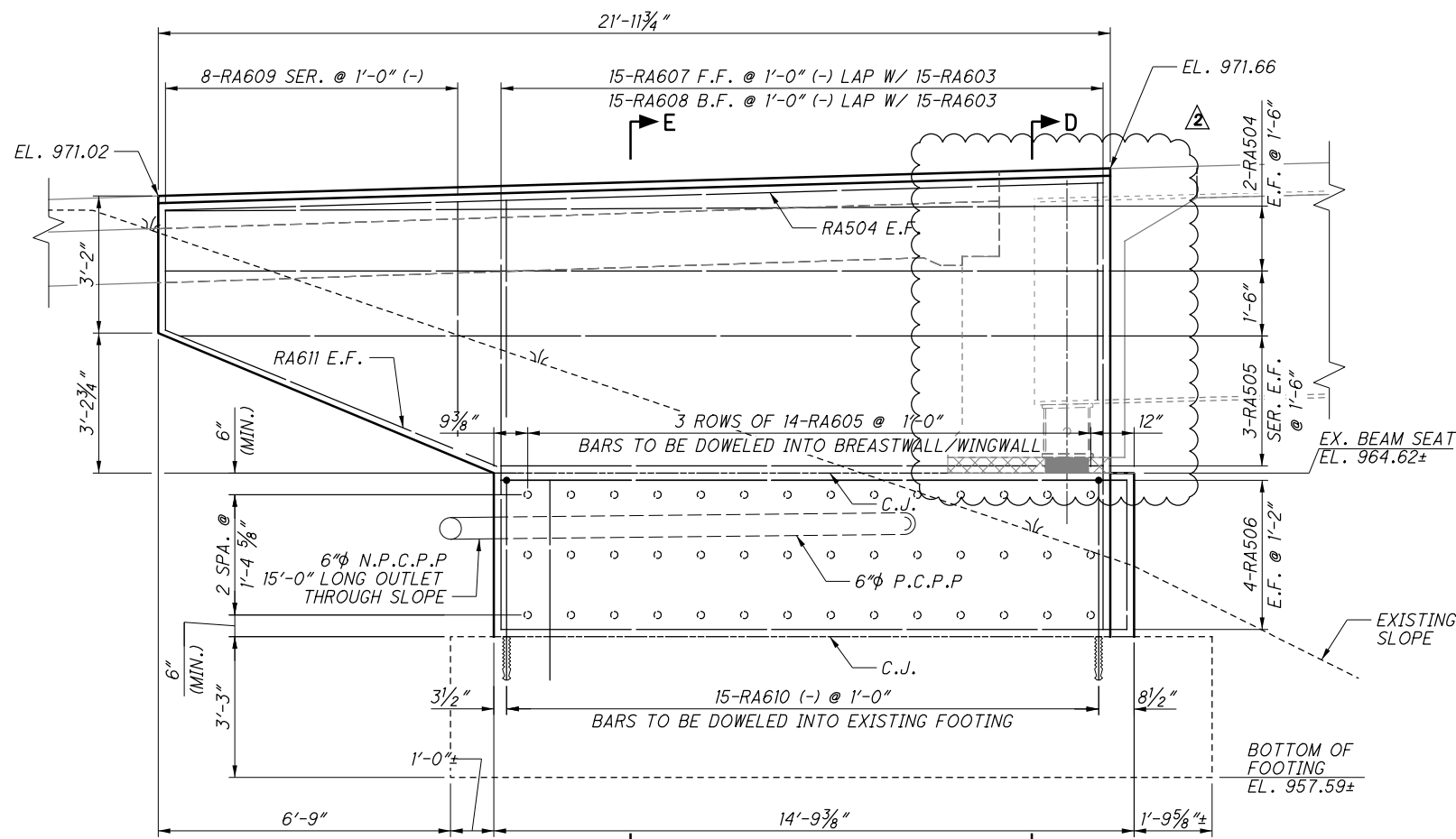
DESIGNED: TASPAN
CHECKED: MRV

REAR ABUTMENT LEFT WINGWALL AND MISCELLANEOUS DETAILS
BRIDGE NO. WAR-71-1683
I-71 UNDER WILMINGTON RD.

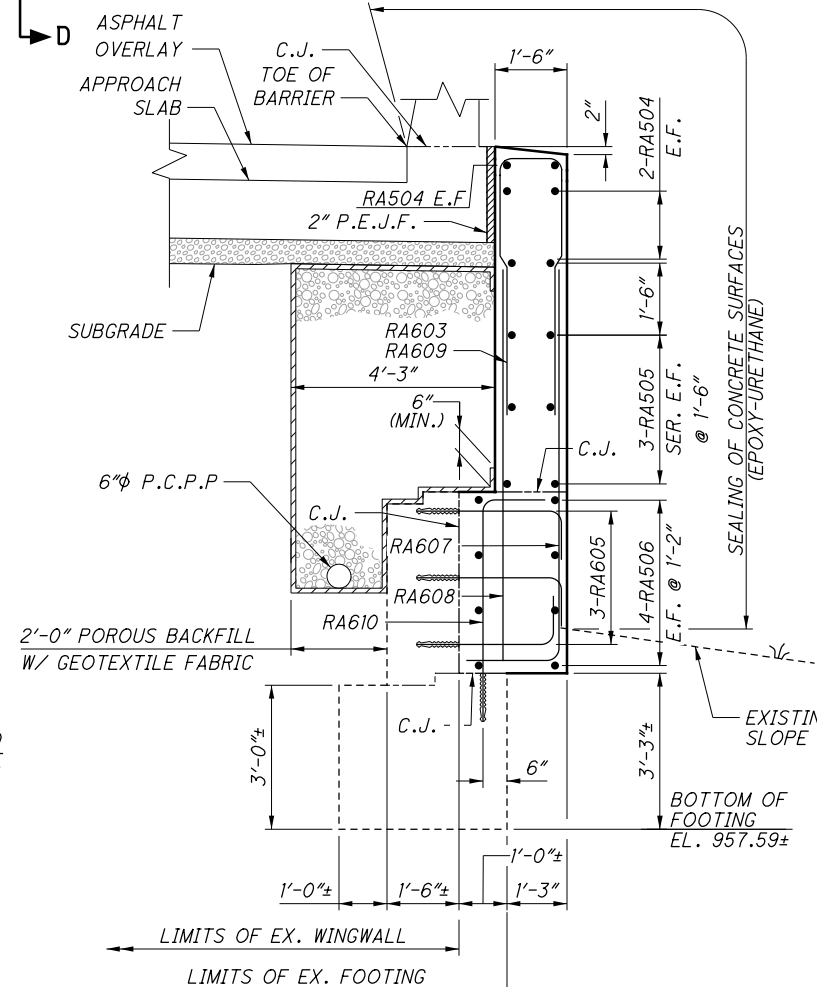
WAR-71-16.81
PID No. 91727

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SECTION C-C



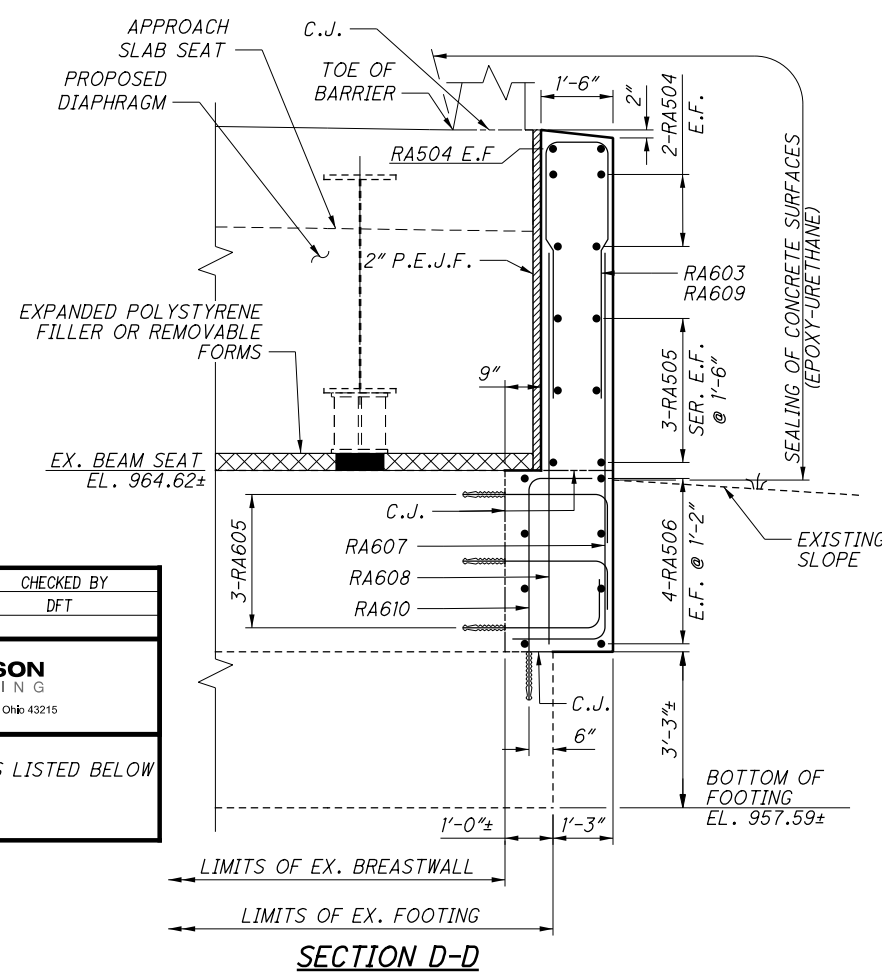
SECTION E-E

DATE:	11/2019	CHECKED BY:	DFT
DESIGNED BY:	MRV		
DETAILED BY:	DTA		

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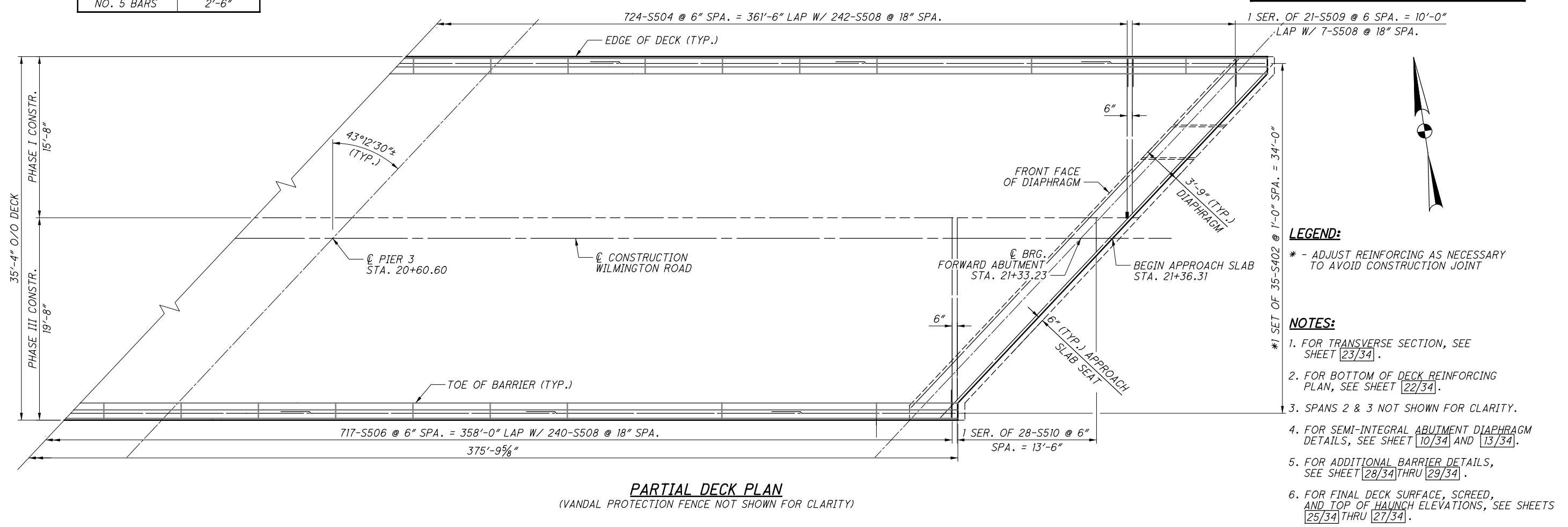
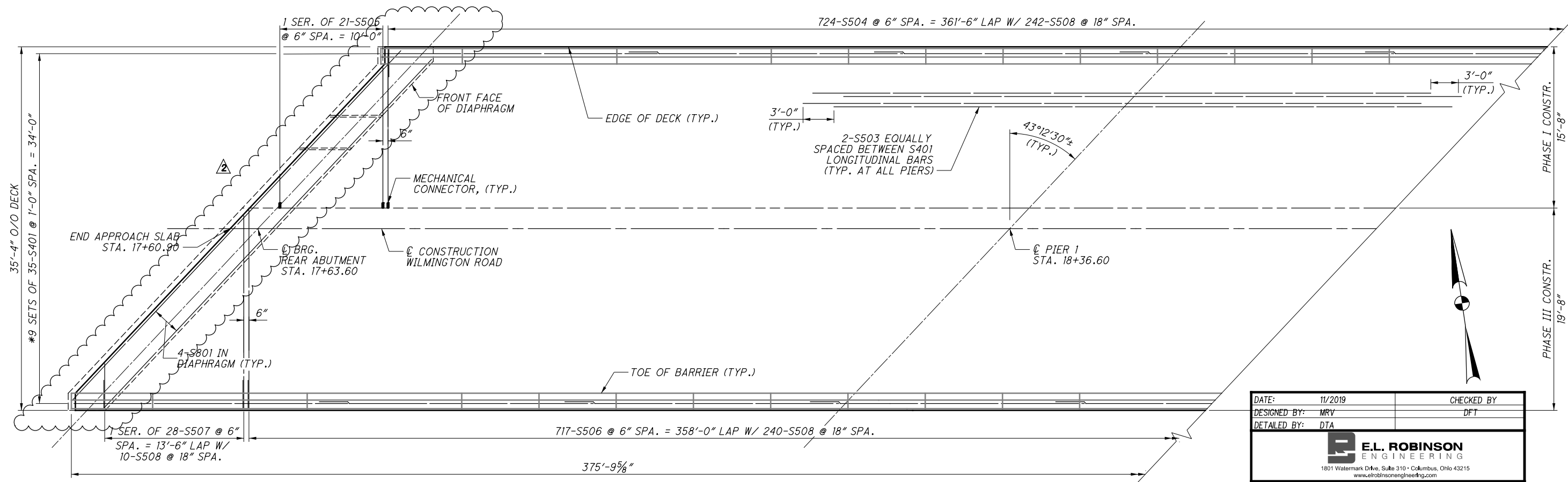
COMMENTS
 THESE PLANS CONTAIN CHANGES AS LISTED BELOW
 AS BUILT PLANS

- NOTES:**
- MINIMUM EMBEDMENT DEPTH OF DOWELS TO BE 10 1/2 INCHES. FURNISH ONE OF THE FOLLOWING EPOXY TYPES FROM THE FOLLOWING MANUFACTURES/SUPPLIERS: HILTI HIT RE 500, HILTI, 5400 SOUTH 122 ND EAST AVENUE, TULSA, OK 74146, PHONE: (800) 879-6000; POWERS FASTENERS PE1000+EPOXY, POWERS FASTENERS, 2 POWERS LANE, BREWSTER, NY 10509, PHONE: (914) 235-6300; SIMPSON STRONG-TIE EDOT SET-XP, SIMPSON STRONG-TIE, COLUMBUS, OH 43228, PHONE: (800) 899-5099. ALL MATERIALS, LABOR, AND INCIDENTALS REQUIRED FOR THE INSTALLATION OF THE DOWLS SHALL BE INCLUDED UNDER ITEM 510 - DOWEL HOLES WITH NON-SHRINK, NON-METALLIC GROUT, AS PER PLAN.
 - FOR ADDITIONAL DIAPHRAGM GUIDE DETAILS, SEE SHEET 10/34.
 - FOR BEARING DETAILS, SEE SHEET 19/34.
 - FOR ADDITIONAL DETAILS NOT SHOWN, SEE ODOT STD. DWG. SICD-1-96, ODOT STD. DWG. SICD-2-14, AND EXISTING PLANS.
 - CLEAR EXISTING WEEPHOLES AND FILL WITH NON-SHRINK, NON-METALLIC GROUT PER CMS 705.20. ALL MATERIALS, LABOR, AND INCIDENTALS FOR PAYMENT SHALL BE INCLUDED IN ITEM 518 POROUS BACKFILL, WITH GEOTEXTILE FABRIC, AS PER PLAN.
 - ABUTMENT DIAPHRAGM CONCRETE, PHASED CONSTRUCTION: PLACE THE DIAPHRAGM CONCRETE ENCASEING THE STRUCTURAL MEMBER ENDS OF AN INDIVIDUAL PHASE AFTER THE DECK PLACEMENT IN THE ADJACENT SPAN IS COMPLETE. PROCEDURES THAT PLACE THE ABUTMENT DIAPHRAGM WITH THE DECK CONCRETE MAY BE APPROVED BY THE ENGINEER IF THE PLACEMENT SUBMITTAL CAN ASSURE THAT THE DECK CONCRETE IN THE ADJACENT SPAN WILL BE PLACED BEFORE CONCRETE IN THE DIAPHRAGM HAS REACHED ITS INITIAL SET.
 - EXISTING PILING NOT SHOWN FOR CLARITY.
 - REINFORCING STEEL MAY REQUIRE FIELD CUTTING OR BENDING TO BE PROPERLY FITTED. PAYMENT IS INCIDENTAL TO THE ITEM.
 - FOR REAR ABUTMENT LEFT WINGWALL, DIAPHRAGM GUIDE, AND THE TERMINATION OF THE 6" NON-PERFORATED CORRUGATED PLASTIC PIPE DETAILS, SEE SHEET 11/34.

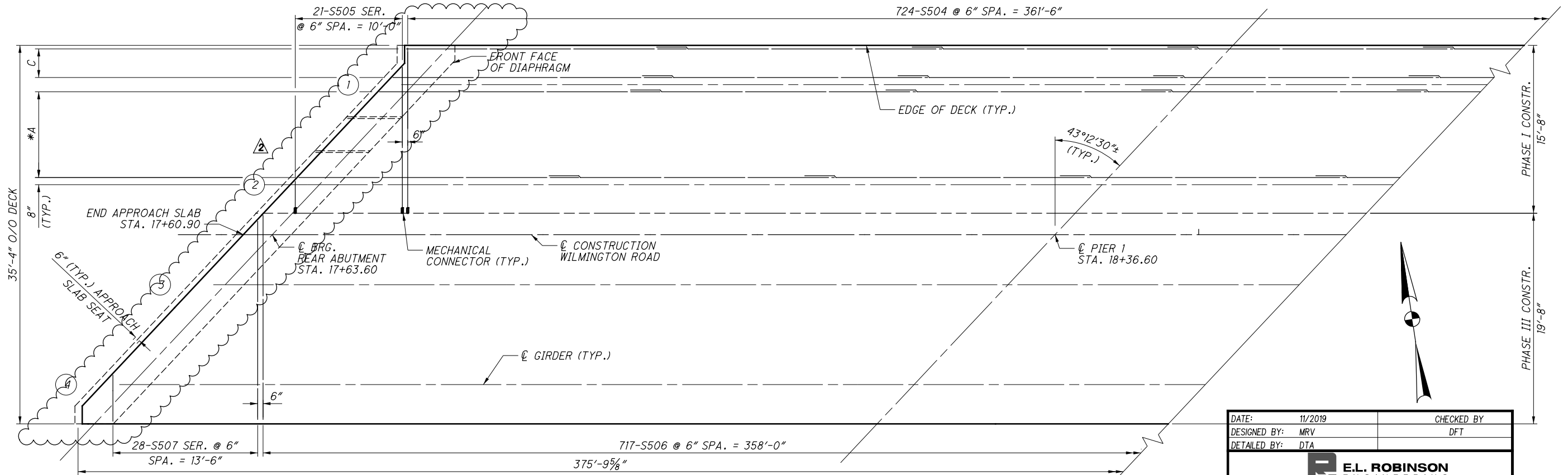


SECTION D-D

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- REINFORCING:**
- A - 9 SETS OF 13-S501 @ 8" SPA. = 8'-0" (TYP. EACH BAY)
 - B - 1 SET OF 13-S502 @ 8" SPA. = 8'-0" (TYP. EACH BAY)
 - C - 9 SETS OF 5-S501 @ 8" SPA. = 2'-8" (TYP.)
 - D - 1 SET OF 5-S502 @ 8" SPA. = 2'-8" (TYP.)

MIN. LAP LENGTHS	
NO. 4 BARS	2'-0"
NO. 5 BARS	2'-6"

PARTIAL DECK PLAN

- LEGEND:**
- # - GIRDER DESIGNATION
 - * - ADJUST REINFORCING AS NECESSARY TO AVOID CONSTRUCTION JOINT

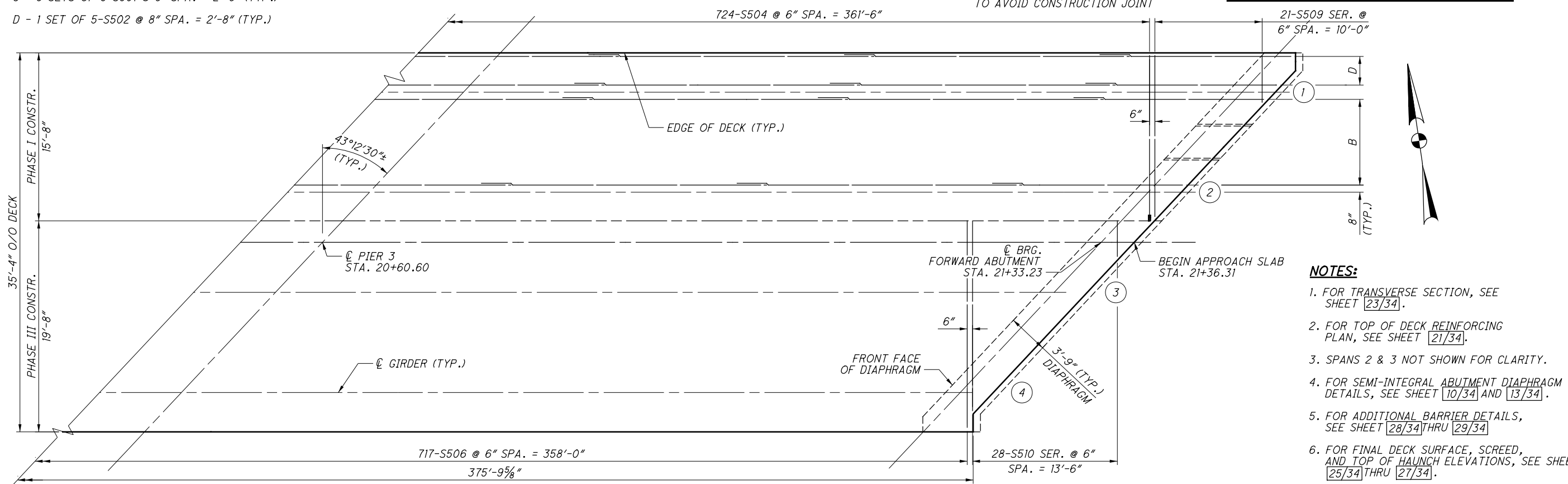
DATE:	11/2019	CHECKED BY:	DFT
DESIGNED BY:	MRV		
DETAILED BY:	DTA		

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COMMENTS

THESE PLANS CONTAIN CHANGES AS LISTED BELOW

▲ AS BUILT PLANS



- NOTES:**
1. FOR TRANSVERSE SECTION, SEE SHEET [23/34].
 2. FOR TOP OF DECK REINFORCING PLAN, SEE SHEET [21/34].
 3. SPANS 2 & 3 NOT SHOWN FOR CLARITY.
 4. FOR SEMI-INTEGRAL ABUTMENT DIAPHRAGM DETAILS, SEE SHEET [10/34] AND [13/34].
 5. FOR ADDITIONAL BARRIER DETAILS, SEE SHEET [28/34] THRU [29/34].
 6. FOR FINAL DECK SURFACE, SCREED, AND TOP OF HAUNCH ELEVATIONS, SEE SHEETS [25/34] THRU [27/34].

PARTIAL DECK PLAN

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DATE: 07/2016
 REVIEWED: RER
 STRUCTURE FILE NUMBER: 8302316

DRAWN: FTB
 CHECKED: DFT

DESIGNED: MRV
 CHECKED: DFT

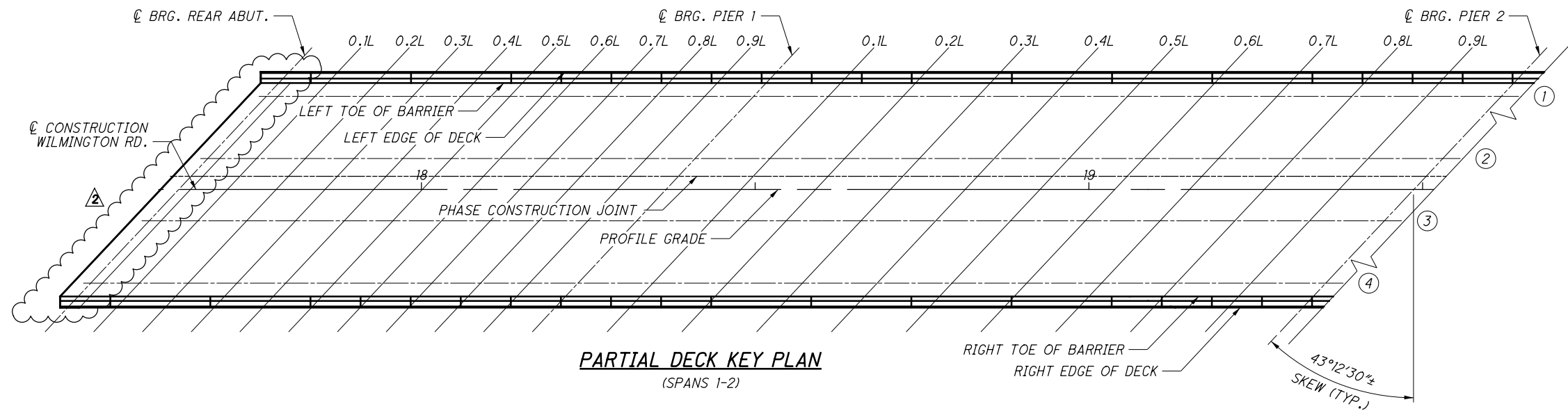
DECK PLAN - BOTTOM REINFORCING
 BRIDGE NO. WAR-71-1683
 I-71 UNDER WILMINGTON RD.

WAR-71-16.81
 PID No. 91727

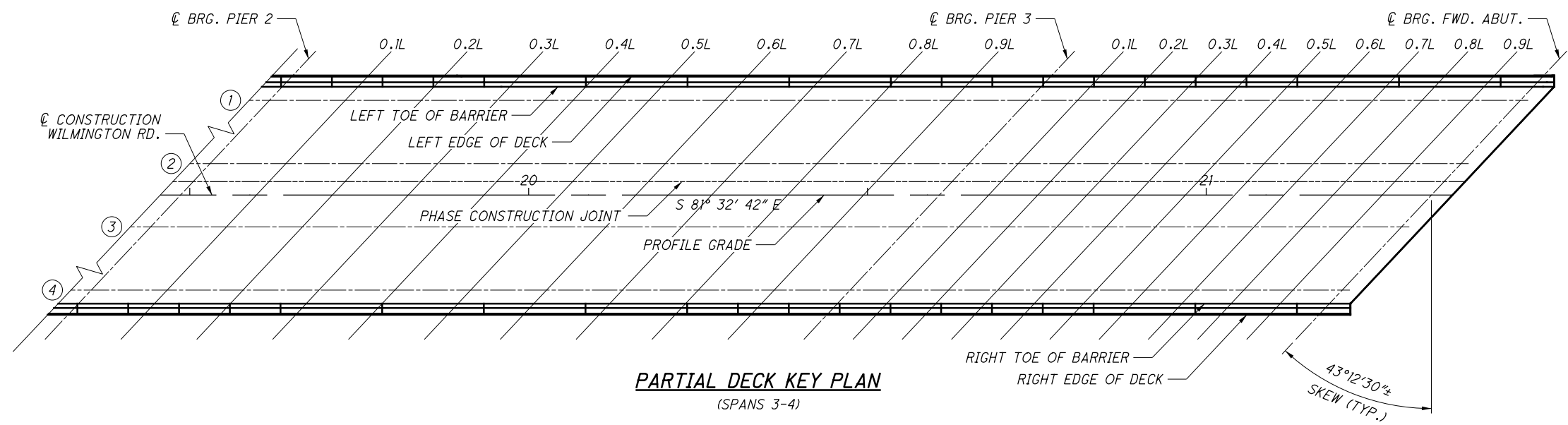
22 / 34

94
160

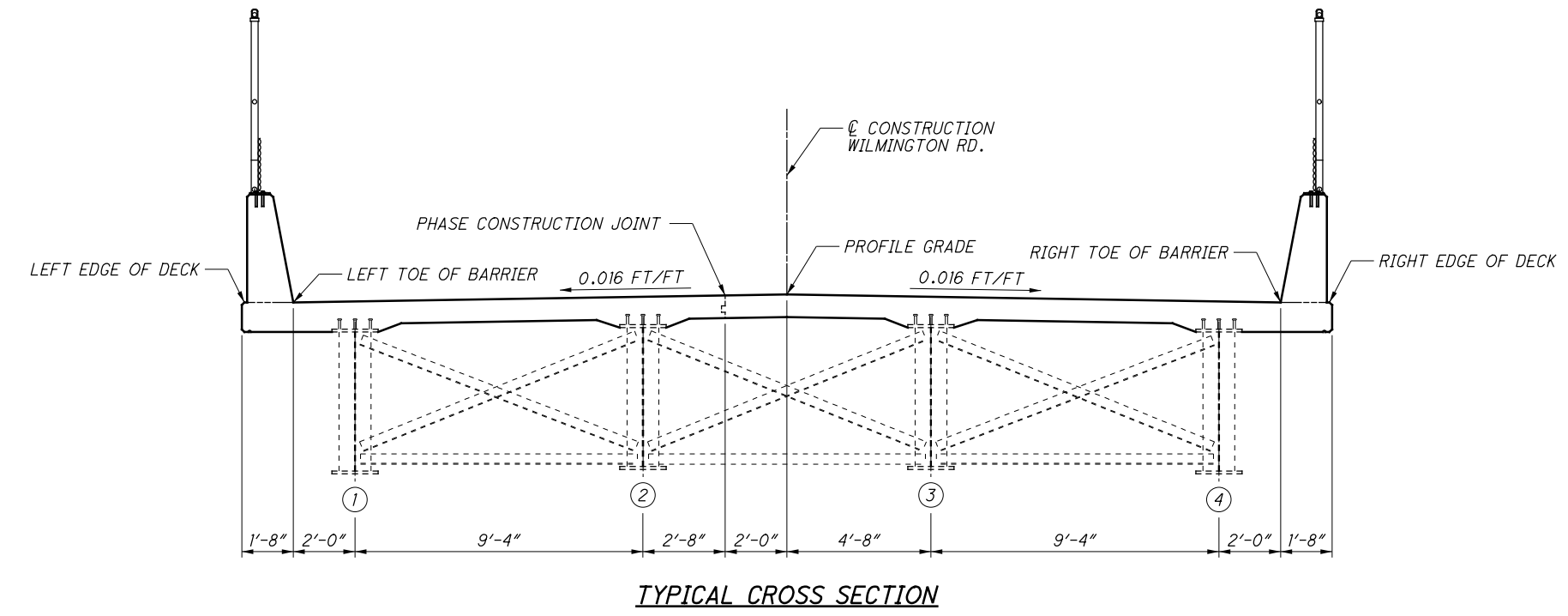
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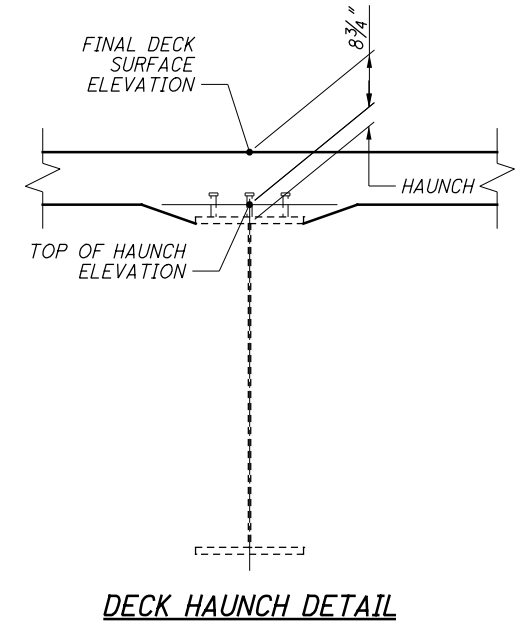
PARTIAL DECK KEY PLAN
(SPANS 1-2)



PARTIAL DECK KEY PLAN
(SPANS 3-4)



TYPICAL CROSS SECTION

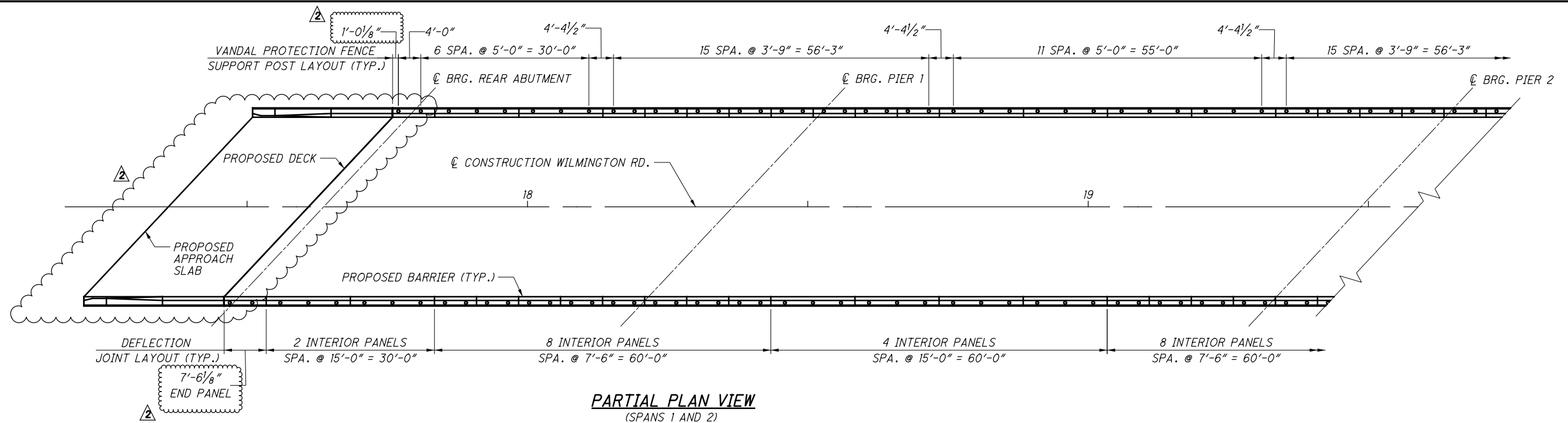


DATE:	11/2019	CHECKED BY:	DFT
DESIGNED BY:	MRV		
DETAILED BY:	DTA		
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COMMENTS THESE PLANS CONTAIN CHANGES AS LISTED BELOW AS BUILT PLANS			

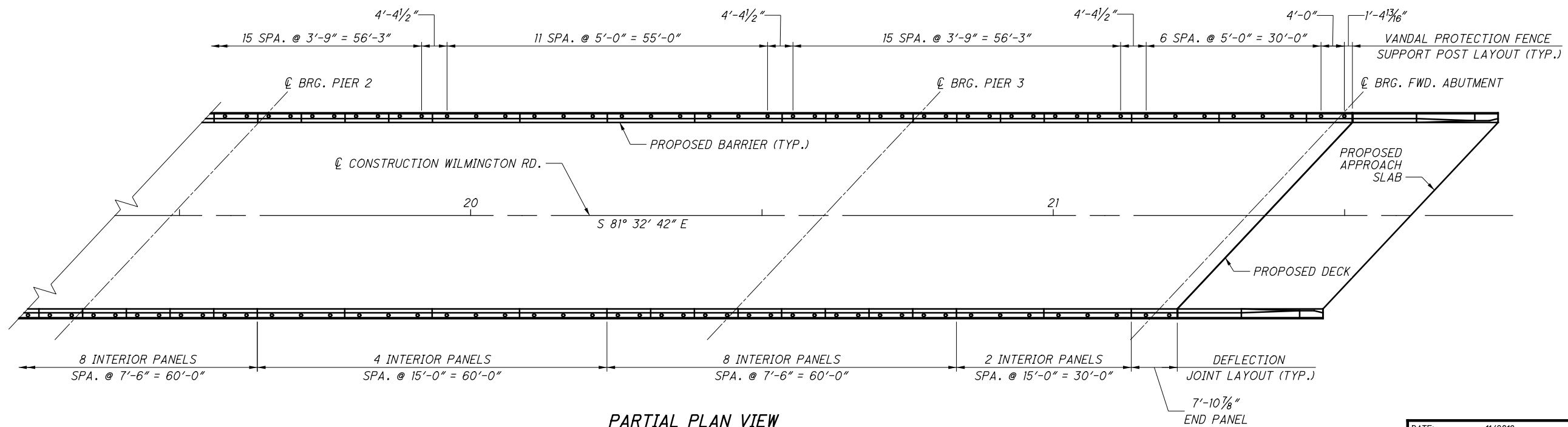
- LEGEND:**
 # - GIRDER DESIGNATION
- NOTES:**
 1. FOR FINAL DECK SURFACE, SCREED, AND TOP OF HAUNCH ELEVATIONS, SEE SHEETS 25/34 THRU 27/34.



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PARTIAL PLAN VIEW (SPANS 1 AND 2)



PARTIAL PLAN VIEW (SPANS 4 AND 3)

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DETAILED BY:	DTA		
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COMMENTS THESE PLANS CONTAIN CHANGES AS LISTED BELOW 2 AS BUILT PLANS			

NOTES:

1. THE VANDAL PROTECTION FENCE SHALL BE A 6'-0" STRAIGHT FENCE CONFORMING TO POST SECTION PS-4 WITH BASE PLATE BP-5.
2. FOR ADDITIONAL DETAILS, SEE ODOT STD. DWG. VPF-1-90

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DESIGNED	TAS/MRV	CHECKED	DFT
DRAWN	TAS	REVISED	
REVIEWED	RER	STRUCTURE FILE NUMBER	8302316
DATE	07/2016		

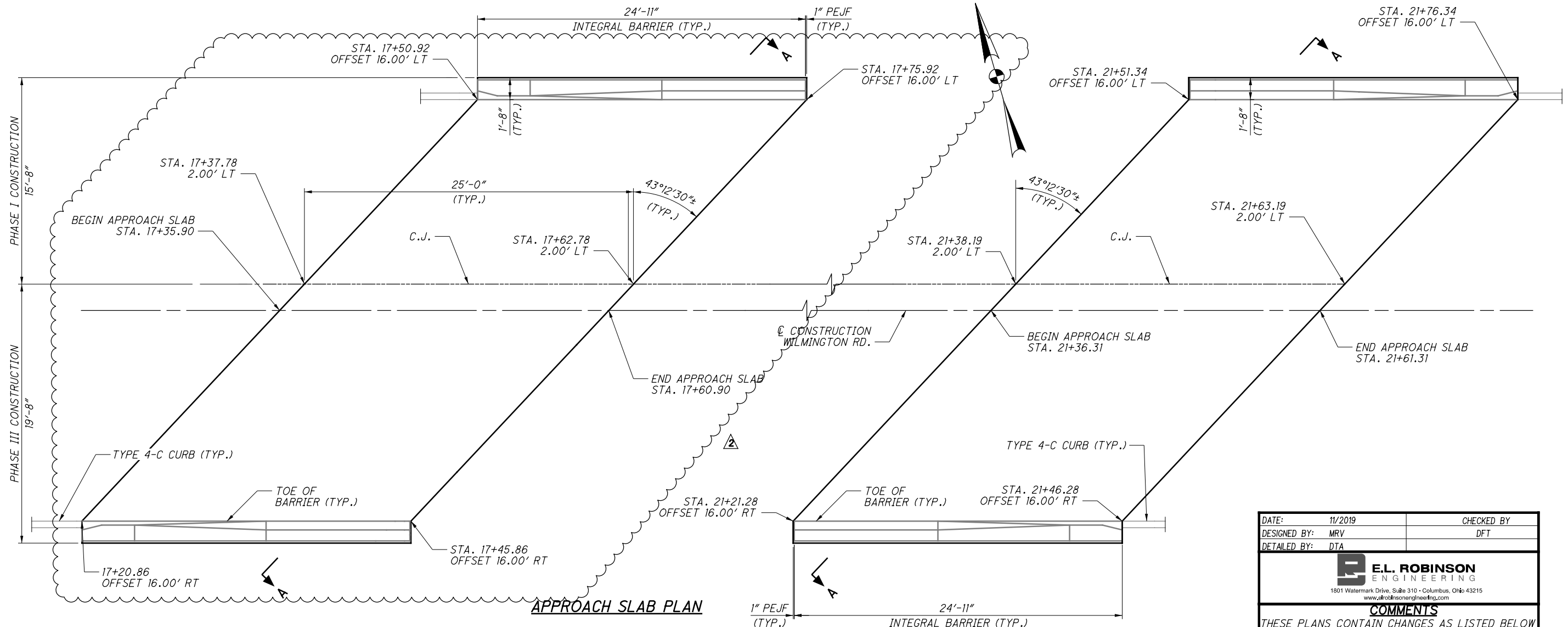
BARRIER DETAILS (2 OF 2)
 BRIDGE NO. WAR-71-1683
 I-71 UNDER WILMINGTON RD.

WAR-71-16.81
 PID No. 91727

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160

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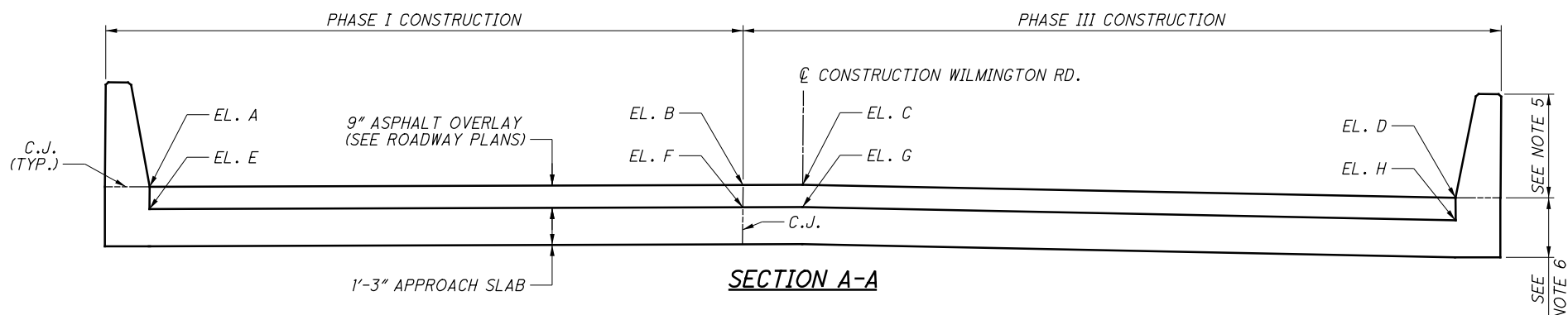
APPROACH SLAB PLAN

DATE:	11/2019	CHECKED BY:	DFT
DESIGNED BY:	MRV		
DETAILED BY:	DTA		

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COMMENTS
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 AS BUILT PLANS

LOCATION	TOP OF ASPHALT OVERLAY								TOP OF APPROACH SLAB							
	STA.	EL. A	STA.	EL. B	STA.	EL. C	STA.	EL. D	STA.	EL. E	STA.	EL. F	STA.	EL. G	STA.	EL. H
BEGIN REAR	17+50.92	971.73	17+37.77	971.58	17+35.89	971.56	17+20.86	970.85	17+50.92	970.98	17+37.77	970.83	17+35.89	970.81	17+20.86	970.10
END REAR	17+75.92	972.40	17+62.77	972.28	17+60.89	972.26	17+45.86	971.59	17+75.92	971.65	17+62.77	971.53	17+60.89	971.51	17+45.86	970.84
BEGIN FORWARD	21+51.34	977.83	21+38.19	978.02	21+36.31	978.04	21+21.28	977.72	21+51.34	977.08	21+38.19	977.27	21+36.31	977.29	21+21.28	976.97
END FORWARD	21+76.34	977.88	21+63.19	978.09	21+61.31	978.12	21+46.28	977.82	21+76.34	977.13	21+63.19	977.34	21+61.31	977.37	21+46.28	977.07



SECTION A-A

- NOTES:**
- FOR ADDITIONAL APPROACH SLAB DETAILS, SEE ODOT STANDARD DRAWINGS AS-1-15 AND AS-2-15 TYPE B INSTALLATION.
 - FOR BARRIER ELEVATIONS, SEE SHEET 28/34.
 - FOR ADDITIONAL BARRIER TRANSITION DETAILS, SEE ODOT STANDARD DRAWING SBR-1-13.
 - FOR PAYMENT OF ITEMS RELATED TO ASPHALT OVERLAY, SEE ROADWAY PLANS.
 - CONCRETE TO BE PAID FOR UNDER ITEM 511, CLASS QC2 CONCRETE WITH QC/QA, BRIDGE DECK (PARAPET), AS PER PLAN.
 - CONCRETE TO BE PAID FOR UNDER ITEM 526, REINFORCED CONCRETE APPROACH SLABS WITH QC/QA (T=15").