

CITY OF COLUMBIANA COL - FAIRFIELD SCH. RD. & LISBON ST. COLUMBIANA COUNTY STATE OF OHIO

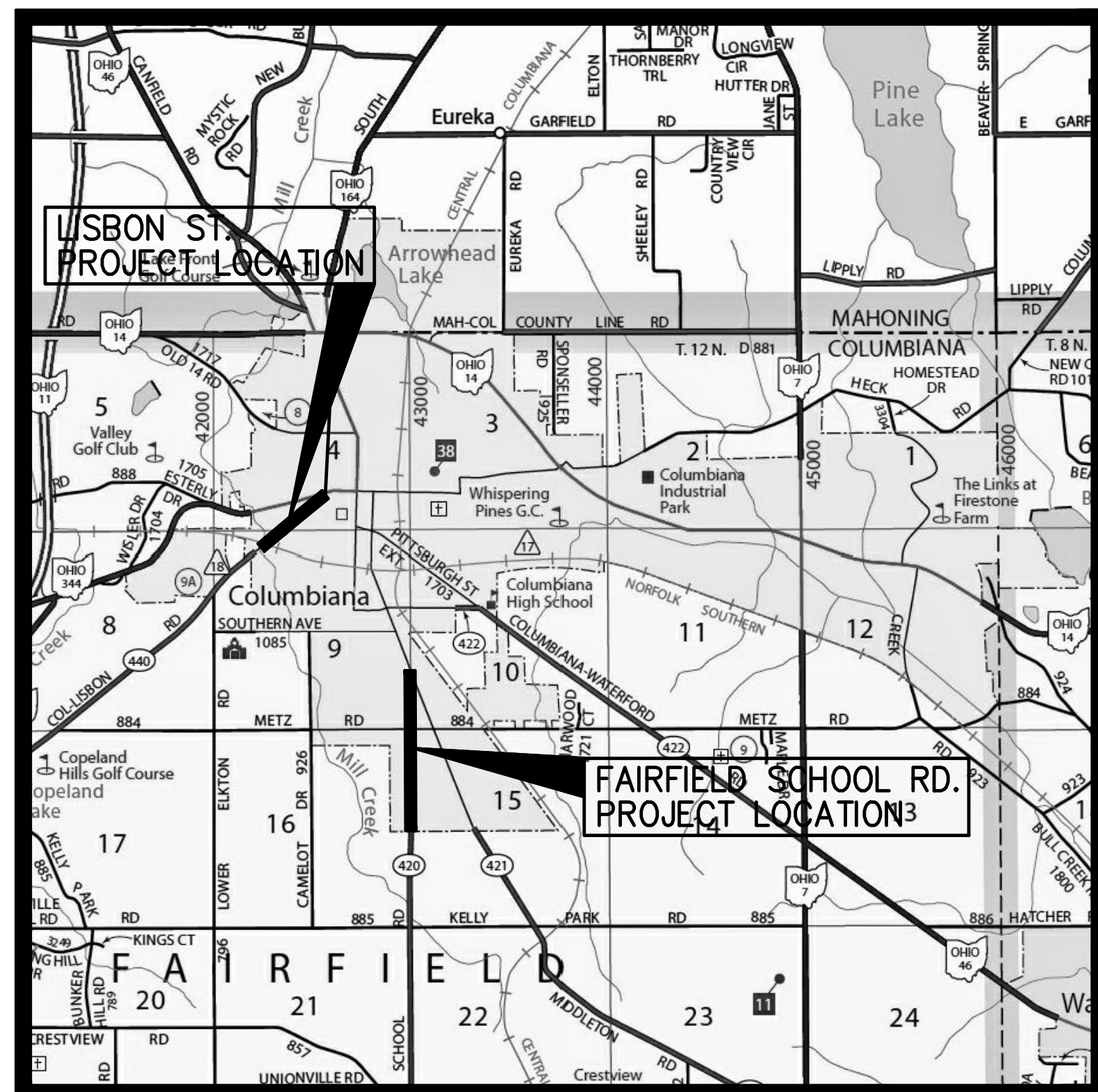
PROJECT DESCRIPTION

PAVEMENT PLANING, RESURFACING, FULL DEPTH PAVEMENT REPAIR, DRAINAGE IMPROVEMENTS, AND PAVEMENT MARKINGS FOR APPROXIMATELY 0.83 MILES ALONG FAIRFIELD SCHOOL ROAD FROM THE SOUTHERN CORPORATION LIMITS AND NORTHERLY TO THE INTERSECTION OF FAIRFIELD AVENUE.

PAVEMENT PLANING, RESURFACING, PAVEMENT REPAIR, INSTALLATION OF CURB RAMPS, DRAINAGE IMPROVEMENTS, AND PAVEMENT MARKINGS FOR APPROXIMATELY 0.45 MILES ALONG LISBON STREET FROM THE SOUTHWESTERN CITY LIMITS AND NORTHEASTERLY TO THE INTERSECTION OF WEST PARK AVENUE.

2023 SPECIFICATIONS

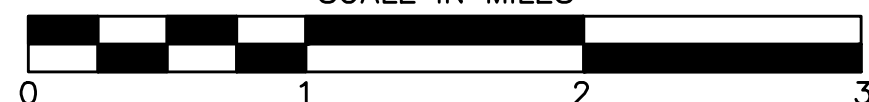
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.



LOCATION MAP

LATITUDE: 40° 52' 12" LONGITUDE: 80° 41' 20" FAIRFIELD SCHOOL ROAD
 LATITUDE: 40° 53' 11" LONGITUDE: 80° 42' 00" LISBON STREET

SCALE IN MILES



ROADS TO BE IMPROVED

DESIGN DESIGNATION

CURRENT ADT (2024)	2,565
DESIGN YEAR ADT (2036)	2,822
DESIGN HOURLY VOLUME (2036)	282.2
DIRECTIONAL DISTRIBUTION	55%
TRUCKS (24 HOUR B&C)	1.8%
DESIGN SPEED	35 M.P.H.
LEGAL SPEED	35 M.P.H.

DESIGN FUNCTIONAL CLASSIFICATION

-URBAN MAJOR COLLECTOR
 (FAIRFIELD SCHOOL ROAD & LISBON STREET)

UNDERGROUND UTILITIES

Contact Two Working Days
 Before You Dig



OHIO811, 8-1-1, or 1-800-362-2764
 (Non-members must be called directly)

PLAN PREPARED BY:
Dallis Dawson & Associates
 CIVIL ENGINEERS AND LAND SURVEYORS
 48745 Calcutta Smith Ferry Rd.
 East Liverpool, OH 43920
 (330) 385-7836

ENGINEERS SEAL:

 SIGNED: *[Signature]*
 DATE: 1-8-2024

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PROJECT DISTURBED AREAS

PROJECT EARTH DISTURBED AREA = 0.95 ACRES
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA = 0.10 ACRES
 NOTICE OF INTENT EARTH DISTURBED AREA = N/A (NOI NOT REQUIRED) *

* ROUTINE MAINTENANCE PROJECT

STANDARD CONSTRUCTION DRAWINGS								SUPPLEMENTAL SPECIFICATIONS	
BP-1.1	7/28/00	MH-1	7/15/22	MT-97.10	4/19/19	TC-41.20	10/18/13	800-2023	1/19/24
BP-2.1	1/21/22	MH-3	7/21/23	MT-97.12	1/20/17	TC-41.50	10/18/13	832	7/21/23
BP-2.2	1/15/21			MT-99.20	4/19/19	TC-42.10	10/18/13	902	7/19/19
BP-2.5	1/21/22			MT-101.60	4/21/23	TC-42.20	10/18/13		
BP-3.1	1/21/22			MT-101.90	7/17/20	TC-52.10	10/18/13		
BP-5.1	7/15/22			MT-105.10	1/17/20	TC-52.20	1/15/21		
BP-7.1	7/21/23			MT-110.10	7/19/13	TC-71.10	4/21/23		
						TC-74.10	7/21/23		
CB-2-2B	1/20/23								
CB-2-3	1/20/23								
CB-3	7/16/21								
CB-3A	7/16/21								
DM-1.1	7/17/20								
DM-4.3	1/15/16								
DM-4.4	1/15/16								
									SPECIAL PROVISIONS

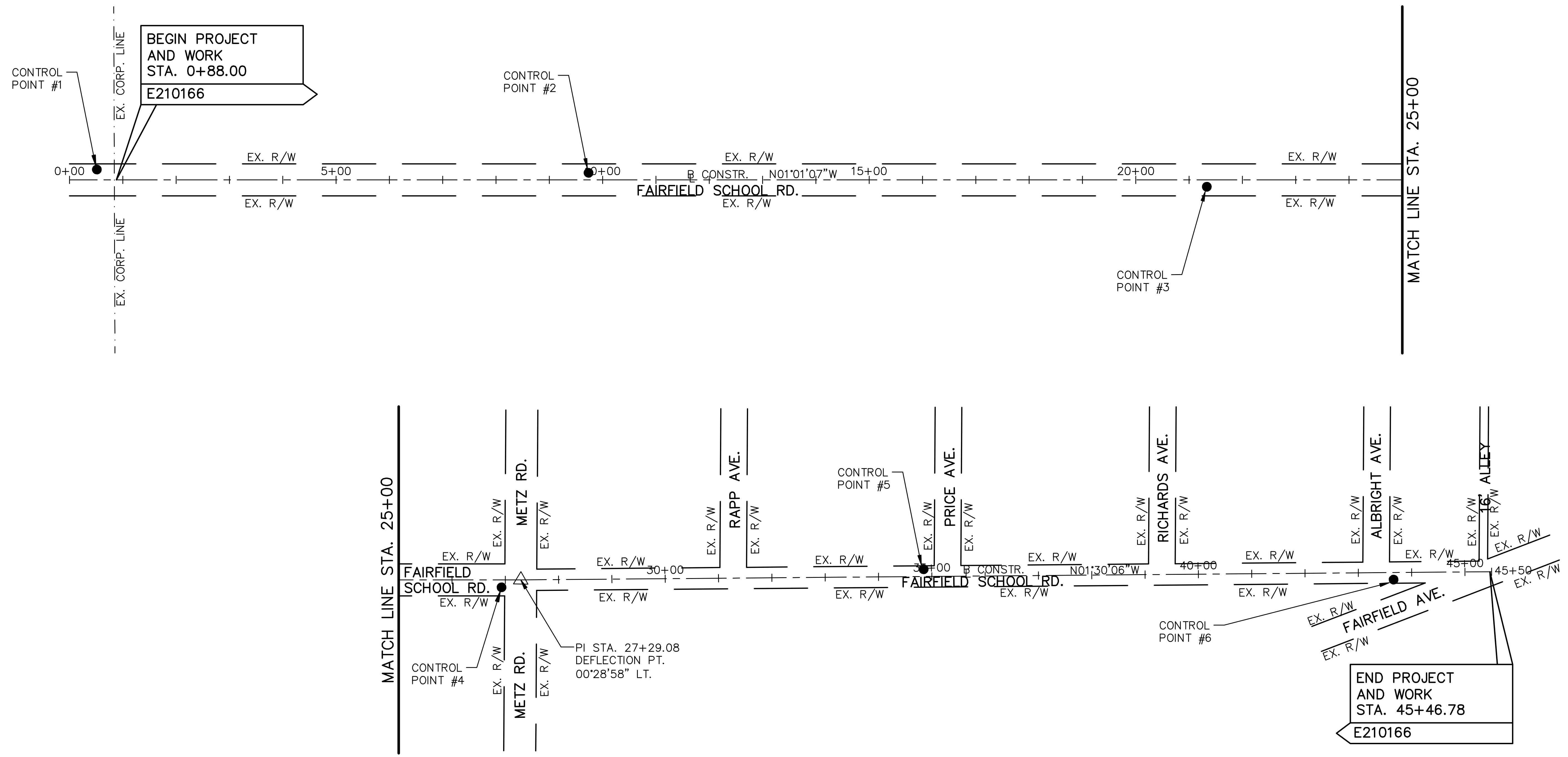
APPROVED *[Signature]*
 DATE 1-8-2024 CITY MANAGER
 CITY OF COLUMBIANA

FEDERAL PROJECT NO. E210166
 PID NO. 114039
 CONSTRUCTION PROJECT NO.
 RAILROAD INVOLVEMENT NONE
 COL - FAIRFIELD SCH. RD. & LISBON ST.
 1/60

CAL: J.F.B.
DATE: 10/23
CHK: D.G.D.
BY: D.G.D.
DATE: 10/23

SCHEMATIC PLAN & CONTROL REFERENCE
FAIRFIELD SCHOOL ROAD

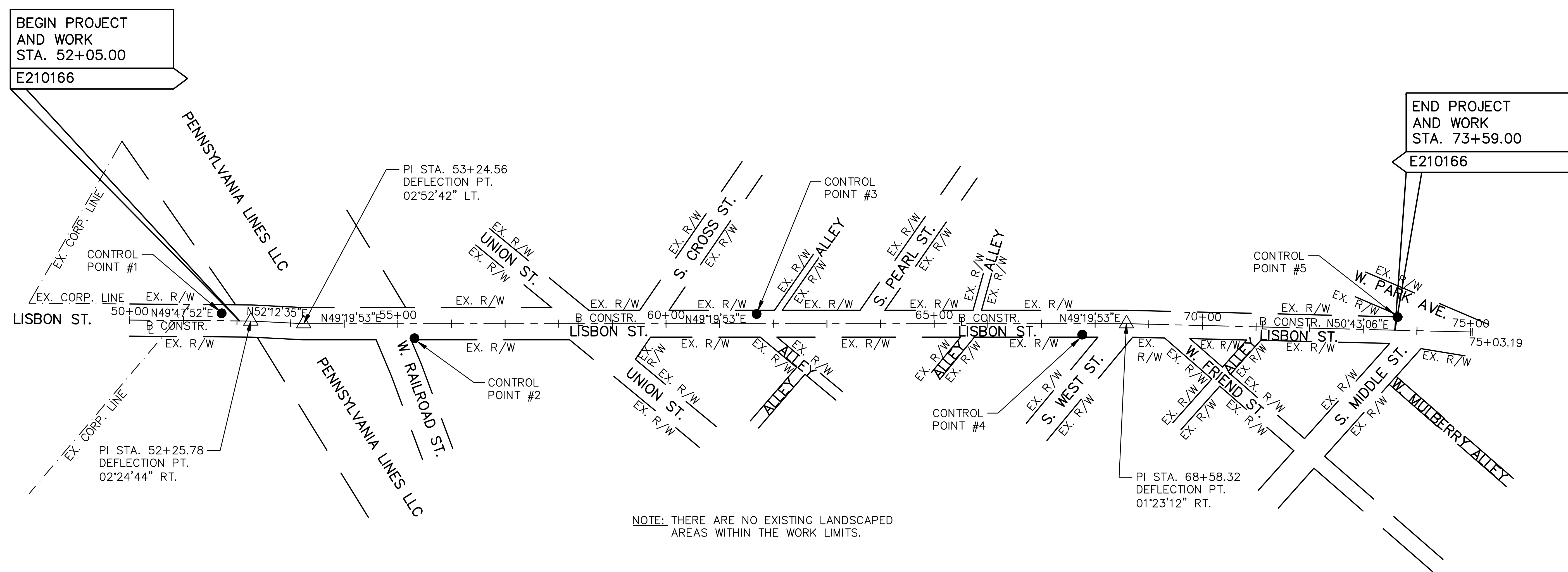
COL - FAIRFIELD SCH. RD.
& LISBON ST.



NOTE: THERE ARE NO EXISTING LANDSCAPED
AREAS WITHIN THE WORK LIMITS.

CONTROL POINT REFERENCE COORDINATES (FAIRFIELD SCHOOL ROAD)				
CONTROL POINT #	TYPE (STATION, OFFSET)	NORTHING	EASTING	ELEVATION
1	3/8" IPS W/ CAP (STA. 0+51.42, 19.41' LT.)	441348.0633	2469379.6023	1172.27
2	3/8" IPS W/ CAP (STA. 9+73.57, 13.23' LT.)	442270.1756	2469369.3867	1152.00
3	3/8" IPS W/ CAP (STA. 21+33.24, 12.91' RT.)	443430.1264	2469374.9090	1171.66
4	3/8" IPS W/ CAP (STA. 26+92.96, 13.88' RT.)	443989.7760	2469365.9300	1175.38
5	3/8" IPS W/ CAP (STA. 34+84.83, 13.57' LT.)	444780.7814	2469318.0388	1170.56
6	3/8" IPS W/ CAP (STA. 43+66.02, 13.25' RT.)	445662.3684	2469321.7615	1153.78

CONSTRUCTION BASELINE COORDINATES		
TYPE (STATION)	NORTHING	EASTING
POINT (STA. 0+00.00)	441296.9990	2469399.9263
DEFL. POINT (STA. 27+29.08)	444025.6505	2469351.4088
POINT (STA. 45+50.00)	445845.9415	2469303.6929



NOTE: THERE ARE NO EXISTING LANDSCAPED AREAS WITHIN THE WORK LIMITS.

CONTROL POINT REFERENCE COORDINATES (LISBON STREET)

CONTROL POINT #	TYPE (STATION, OFFSET)	NORTHING	EASTING	ELEVATION
1	MAG NAIL (STA. 51+71.30, 13.93' LT.)	448826.3003	2465322.2064	1086.57
2	3/8" IPS W/ CAP (STA. 55+31.07, 27.13' RT.)	449025.3597	2465625.1904	1096.04
3	3/8" IPS W/ CAP (STA. 61+68.82, 18.09' LT.)	449475.2740	2466079.4480	1110.98
4	3/8" IPS W/ CAP (STA. 67+75.89, 20.39' RT.)	449841.7014	2466564.9775	1118.96
5	3/8" IPS W/ CAP (STA. 73+63.12, 24.55' LT.)	450249.4969	2466989.4138	1124.85

CONSTRUCTION BASELINE COORDINATES

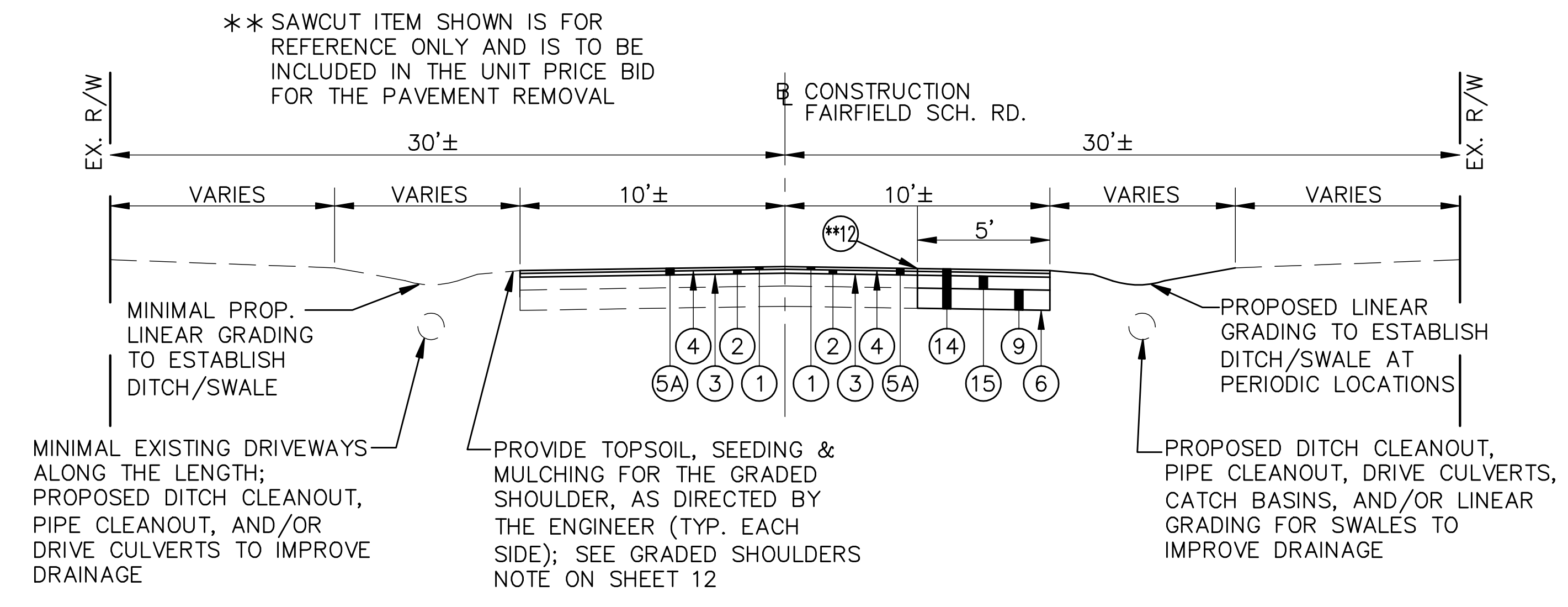
TYPE (STATION)	NORTHING	EASTING
POINT (STA. 50+00.00)	448705.0929	2465200.3670
DEFL. POINT (STA. 52+25.78)	448850.8321	2465372.8119
DEFL. POINT (STA. 53+24.56)	448911.3632	2465450.8756
DEFL. POINT (STA. 68+58.32)	449910.8870	2466614.2221
POINT (STA. 75+03.19)	450319.1722	2467113.3733

BEGIN PROJECT AND WORK
STA. 52+05.00
E210166

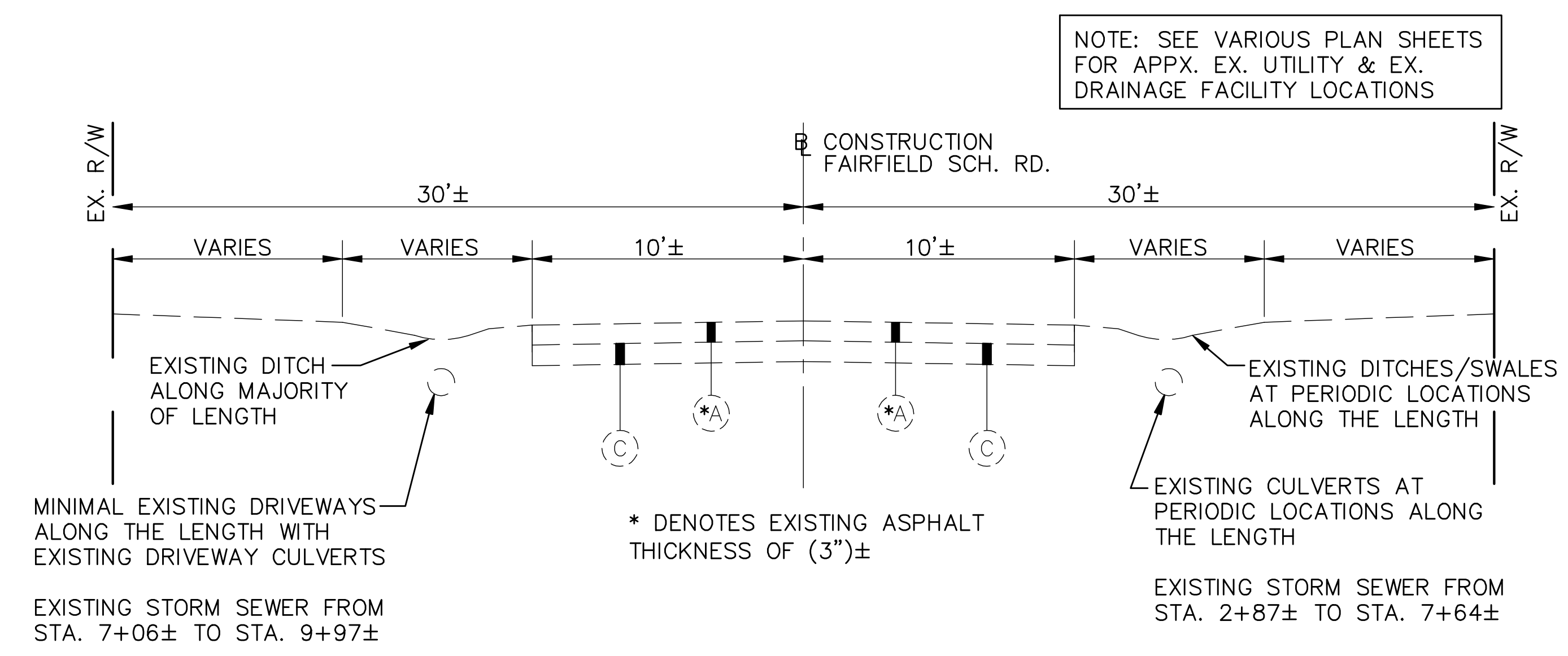
END PROJECT AND WORK
STA. 73+59.00
E210166

NOTE: SEE VARIOUS PLAN SHEETS FOR APPX. EX. UTILITY & EX. DRAINAGE FACILITY LOCATIONS

NOTE: SEE THE VARIOUS PLAN SHEETS AND DRAINAGE DETAILS FOR THE PERTINENT PROPOSED LINEAR GRADING AND PROPOSED DRAINAGE IMPROVEMENTS



FAIRFIELD SCHOOL RD. - PROPOSED SECTION
 STA. 0+88.00 TO STA. 27+29.08



FAIRFIELD SCHOOL RD. - EXISTING SECTION
 STA. 0+88.00 TO STA. 27+29.08

SPECIAL NOTES FOR ASPHALT PAVEMENT PLANING OPERATIONS ALONG FAIRFIELD SCHOOL ROAD

THE QUANTITY SHOWN ON SHEET 22 FOR THE AREA OF 1-1/2" THICK ASPHALT PAVEMENT PLANING DOES NOT INCLUDE PAVEMENT PLANING WITHIN THE (5') OR (7') FULL DEPTH PAVEMENT REMOVAL AREAS ALONG THE RIGHT SIDE OF THE ROADWAY.

THE 1-1/2" THICK ASPHALT PAVEMENT PLANING SHALL BE PERFORMED TO THE CROWN OF THE EXISTING PAVEMENT IN ORDER TO MAINTAIN THE PAVEMENT CROSS SLOPE; THE CONTRACTOR WILL NOT BE PERMITTED TO STRADDLE THE CROWN OF THE ROADWAY DURING PAVEMENT PLANING OPERATIONS.

PAVEMENT PLANING ALONG THE LEFT SIDE OF THE ROADWAY SHALL NOT BE PERFORMED UNTIL THE CONTRACTOR IS READY FOR PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE & SURFACE COURSES. THE NUMBER OF DAYS BETWEEN THE PAVEMENT PLANING OPERATIONS AND THE PLACEMENT OF THE ASPHALT OVERLAY ALONG THE LEFT SIDE SHALL BE MINIMIZED AS MUCH AS PRACTICAL IN ORDER TO MINIMIZE THE EXPOSURE OF THE PLANED SURFACE TO THE WEATHER, TRAFFIC, EQUIPMENT, ETC.

ASPHALT PAVEMENT PLANING FOR THE REMAINING (3') TO (5') WIDTH OF EXISTING PAVEMENT ADJACENT TO THE FULL DEPTH REMOVAL/REPLACEMENT AREA ALONG THE RIGHT SIDE OF THE ROADWAY, AND PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE COURSE FOR THE FULL WIDTH OF THE RIGHT SIDE LANE OF THE ROADWAY SHALL BE PERFORMED PRIOR TO THE REMOVAL OF THE PERTINENT WORK ZONE M.O.T. & DETOURS. THE NUMBER OF DAYS BETWEEN THE PAVEMENT PLANING OPERATIONS AND THE PLACEMENT OF THE FULL WIDTH ASPHALT CONCRETE INTERMEDIATE COURSE SHALL BE MINIMIZED AS MUCH AS PRACTICAL IN ORDER TO MINIMIZE THE EXPOSURE OF THE PLANED SURFACE TO THE WEATHER, DRIVEWAY TRAFFIC, EQUIPMENT, ETC. SEE SHEET 13 FOR ADDITIONAL NOTES.

LEGEND (PROPOSED)

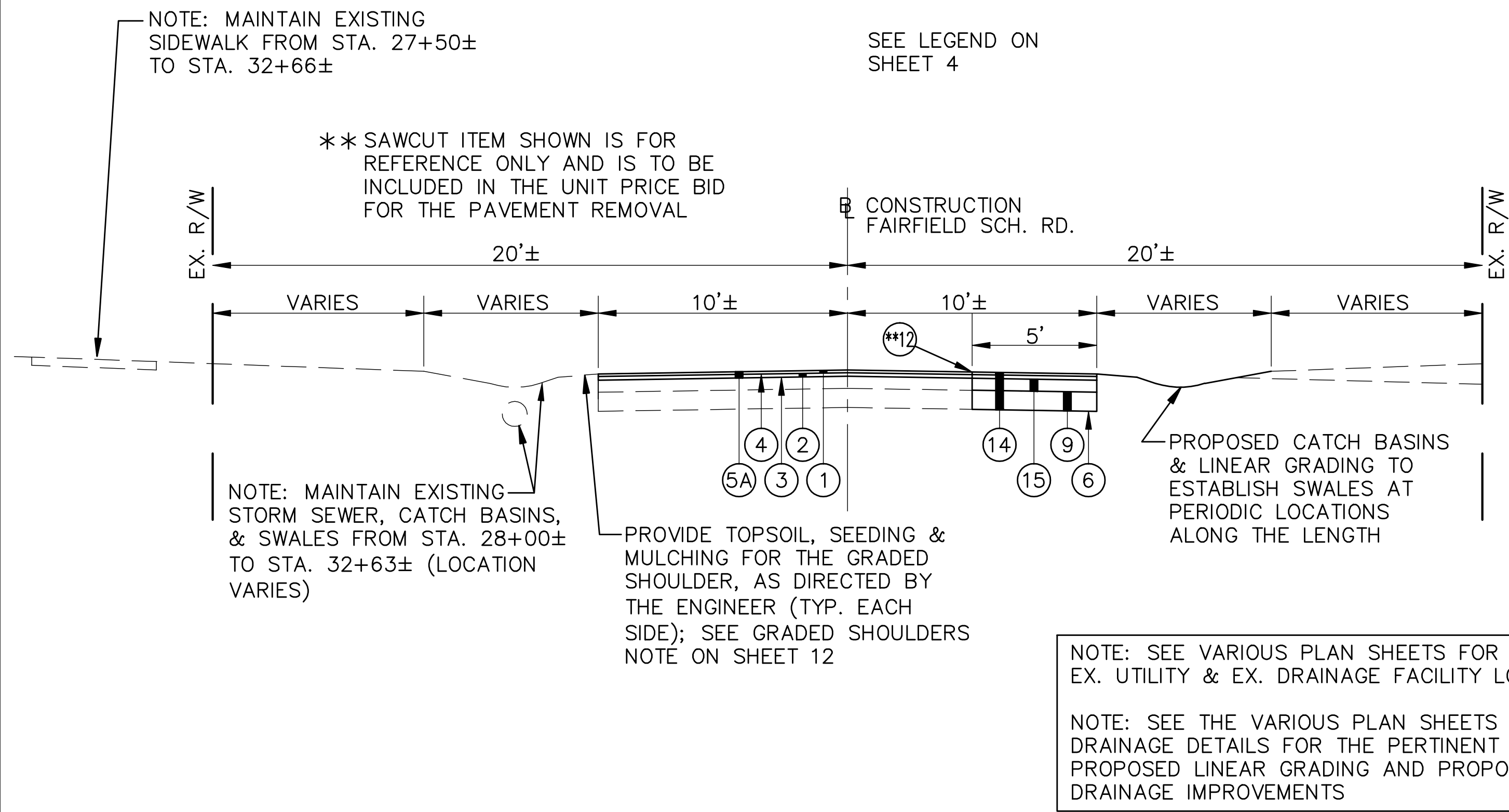
- ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (PG64-22)
- ② ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
- ③ ITEM 407 - TACK COAT, 702.13 (APPLIED AT 0.085 GAL./S.Y.)
- ④ ITEM 407 - TACK COAT
- ⑤ ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (T=3"±)
- ⑤A ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (T=1-1/2"±)
- ⑥ ITEM 204 - SUBGRADE COMPACTION
- ⑦ ITEM 608 - 4" OR 6" CONCRETE WALK, AS PER PLAN

LEGEND (PROPOSED)

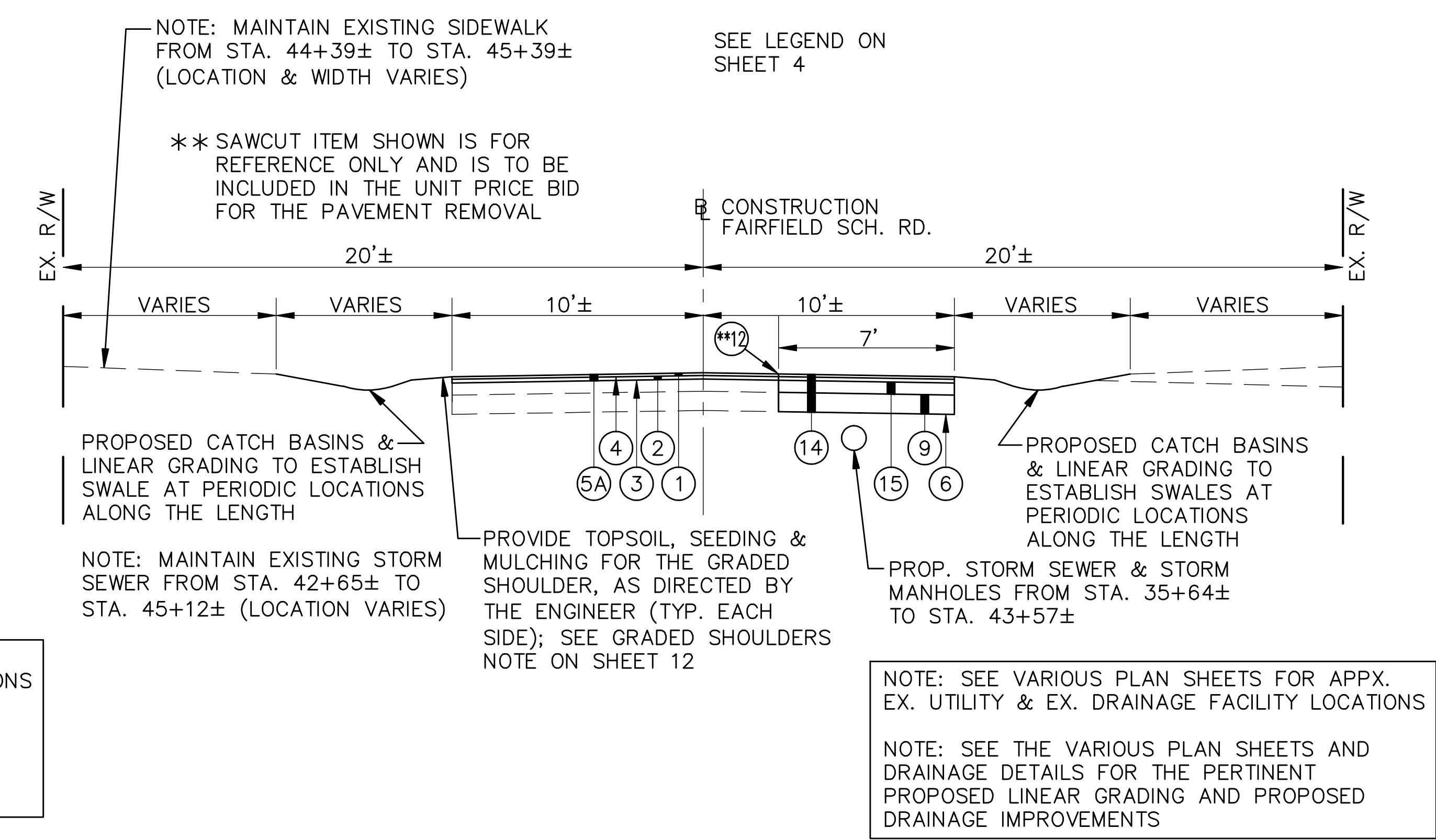
- ⑧ ITEM 304 - 4" AGGREGATE BASE, AS PER PLAN
- ⑨ ITEM 304 - 6" AGGREGATE BASE, AS PER PLAN
- ⑩ ITEM 609 - CURB, TYPE 6
- ⑪ ITEM 255 - FULL DEPTH PAVEMENT REMOVAL WITH RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN
- ⑫ ITEM 255 - FULL DEPTH PAVEMENT SAWING (ITEM SHOWN FOR REFERENCE ONLY)
- ⑬ ITEM 305 - CONCRETE BASE MISC.: 12" CONCRETE BASE, CLASS QC MS
- ⑭ ITEM 202 - PAVEMENT REMOVED, AS PER PLAN
- ⑮ ITEM 301 - 3" ASPHALT CONCRETE BASE, PG64-22, (449)

LEGEND (EXISTING)

- (A) EXISTING ASPHALT CONCRETE (VARIABLE THICKNESS)
- (B) EXISTING CONCRETE BASE (THICKNESS VARIES)
- (C) EXISTING AGGREGATE SUBBASE (VARIABLE THICKNESS)
- (D) EXISTING STONE OR CONCRETE CURB
- (E) EXISTING SIDEWALK



FAIRFIELD SCHOOL RD. - PROPOSED SECTION
 STA. 27+29.08 TO STA. 35+50.00



FAIRFIELD SCHOOL RD. - PROPOSED SECTION
 STA. 35+50.00 TO STA. 45+46.78

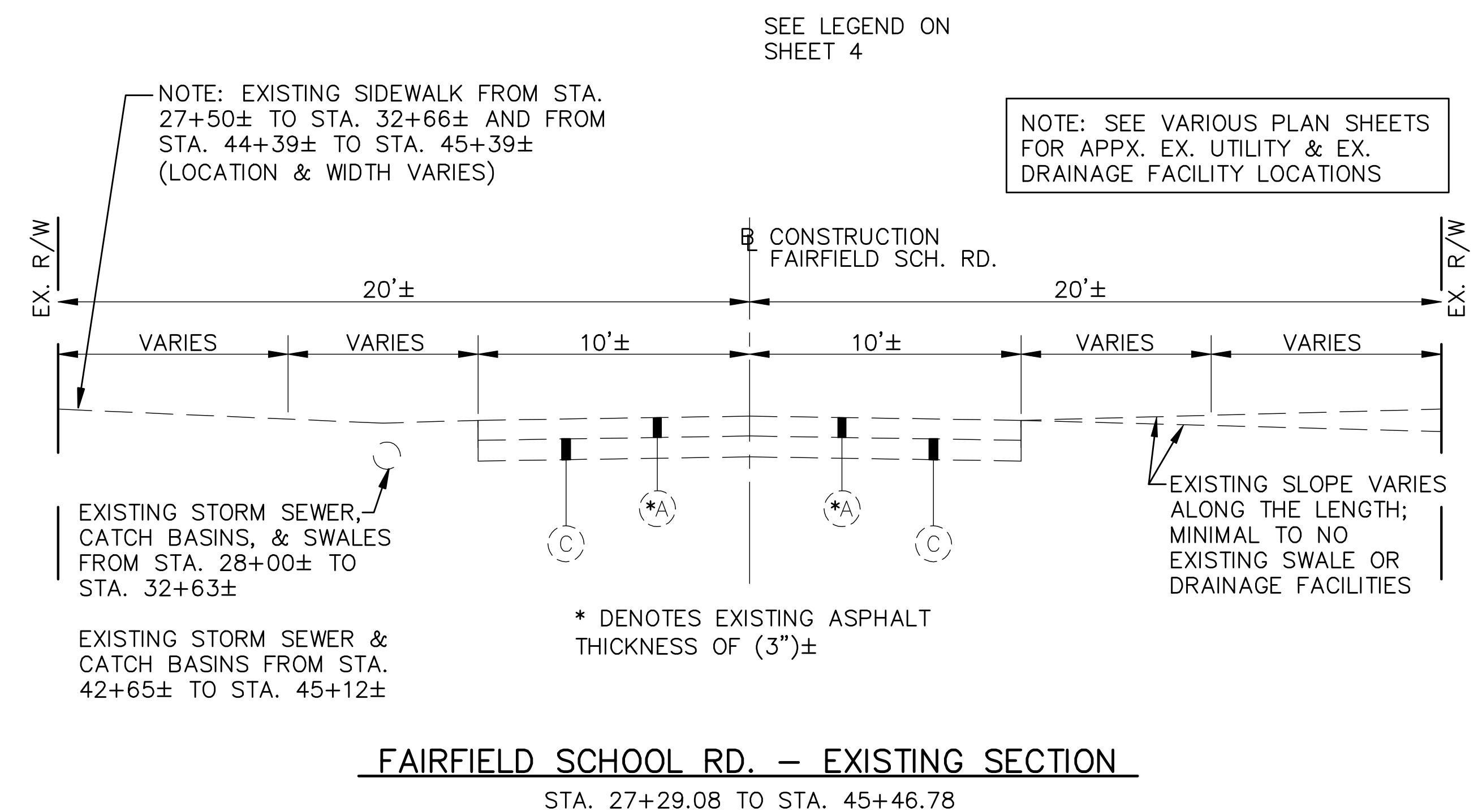
SPECIAL NOTES FOR ASPHALT PAVEMENT PLANING OPERATIONS ALONG FAIRFIELD SCHOOL ROAD

THE QUANTITY SHOWN ON SHEET 22 FOR THE AREA OF 1-1/2" THICK ASPHALT PAVEMENT PLANING DOES NOT INCLUDE PAVEMENT PLANING WITHIN THE (5') OR (7') FULL DEPTH PAVEMENT REMOVAL AREAS ALONG THE RIGHT SIDE OF THE ROADWAY.

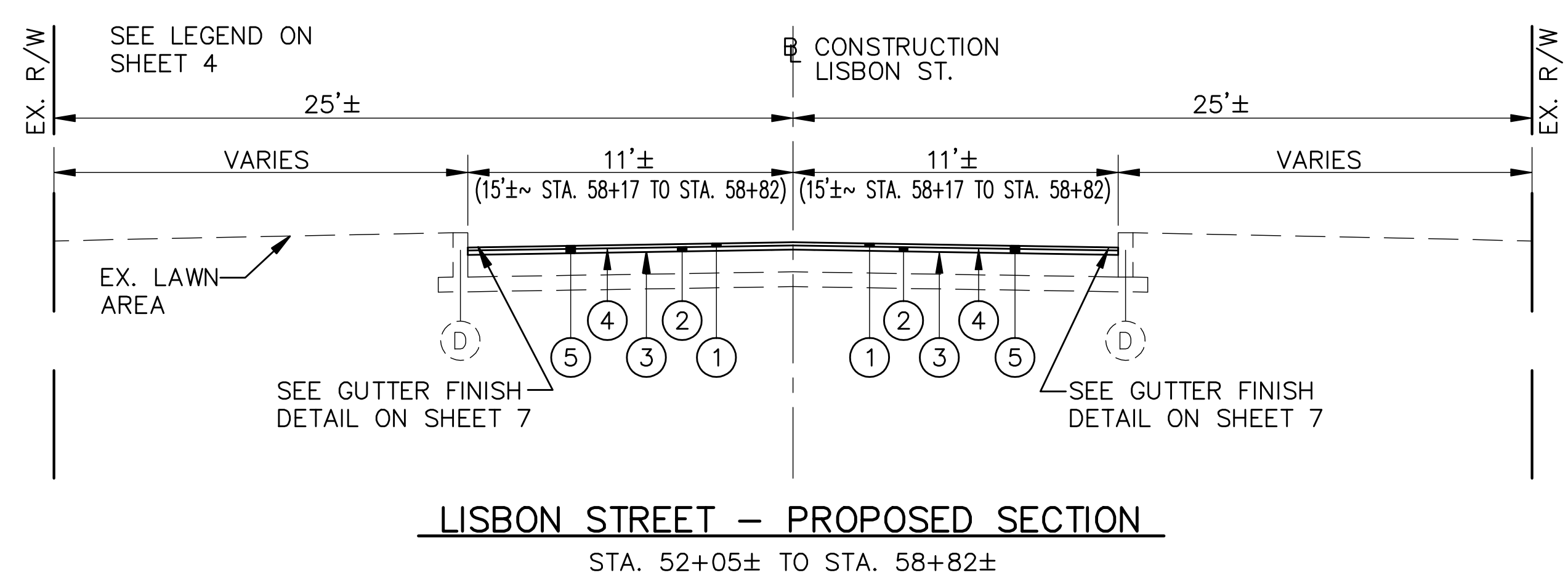
THE 1-1/2" THICK ASPHALT PAVEMENT PLANING SHALL BE PERFORMED TO THE CROWN OF THE EXISTING PAVEMENT IN ORDER TO MAINTAIN THE PAVEMENT CROSS SLOPE; THE CONTRACTOR WILL NOT BE PERMITTED TO STRADDLE THE CROWN OF THE ROADWAY DURING PAVEMENT PLANING OPERATIONS.

PAVEMENT PLANING ALONG THE LEFT SIDE OF THE ROADWAY SHALL NOT BE PERFORMED UNTIL THE CONTRACTOR IS READY FOR PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE & SURFACE COURSES. THE NUMBER OF DAYS BETWEEN THE PAVEMENT PLANING OPERATIONS AND THE PLACEMENT OF THE ASPHALT OVERLAY ALONG THE LEFT SIDE SHALL BE MINIMIZED AS MUCH AS PRACTICAL IN ORDER TO MINIMIZE THE EXPOSURE OF THE PLANED SURFACE TO THE WEATHER, TRAFFIC, EQUIPMENT, ETC.

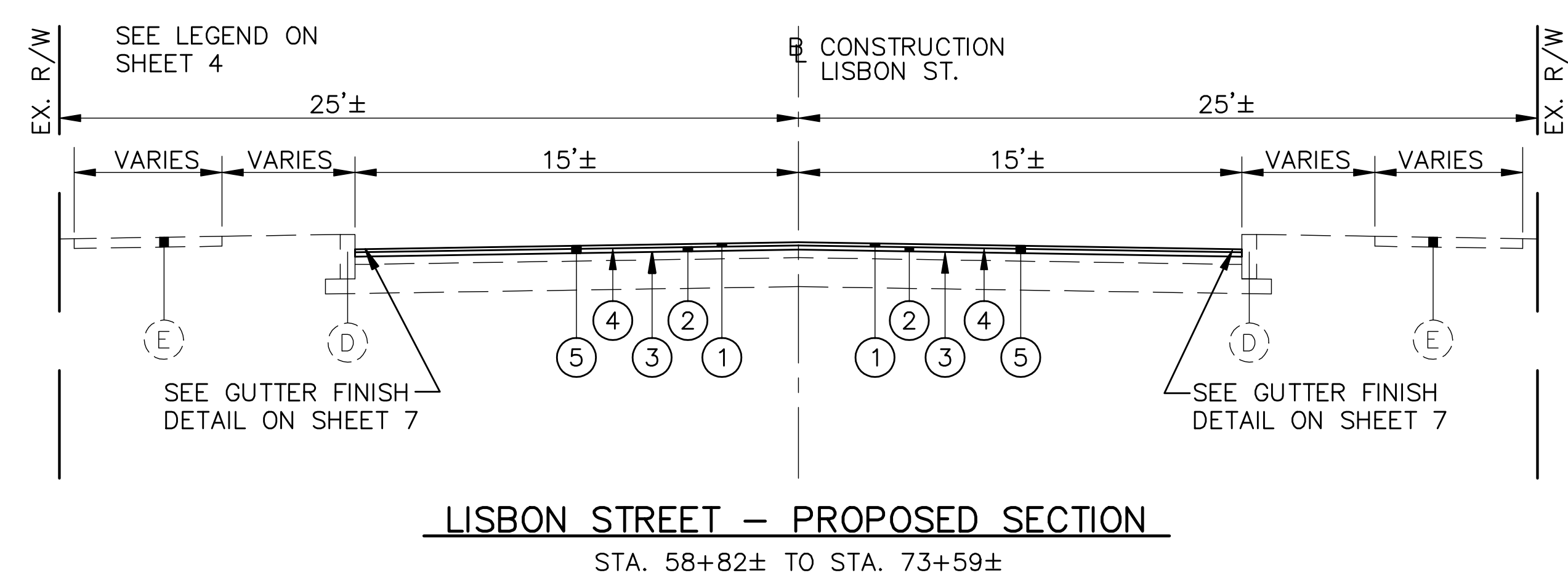
ASPHALT PAVEMENT PLANING FOR THE REMAINING (3') TO (5') WIDTH OF EXISTING PAVEMENT ADJACENT TO THE FULL DEPTH REMOVAL/REPLACEMENT AREA ALONG THE RIGHT SIDE OF THE ROADWAY, AND PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE COURSE FOR THE FULL WIDTH OF THE RIGHT SIDE LANE OF THE ROADWAY SHALL BE PERFORMED PRIOR TO THE REMOVAL OF THE PERTINENT WORK ZONE M.O.T. & DETOURS. THE NUMBER OF DAYS BETWEEN THE PAVEMENT PLANING OPERATIONS AND THE PLACEMENT OF THE FULL WIDTH ASPHALT CONCRETE INTERMEDIATE COURSE SHALL BE MINIMIZED AS MUCH AS PRACTICAL IN ORDER TO MINIMIZE THE EXPOSURE OF THE PLANED SURFACE TO THE WEATHER, DRIVEWAY TRAFFIC, EQUIPMENT, ETC. SEE SHEET 13 FOR ADDITIONAL NOTES.



FAIRFIELD SCHOOL RD. - EXISTING SECTION
 STA. 27+29.08 TO STA. 45+46.78



LISBON STREET – PROPOSED SECTION
 STA. 52+05± TO STA. 58+82±

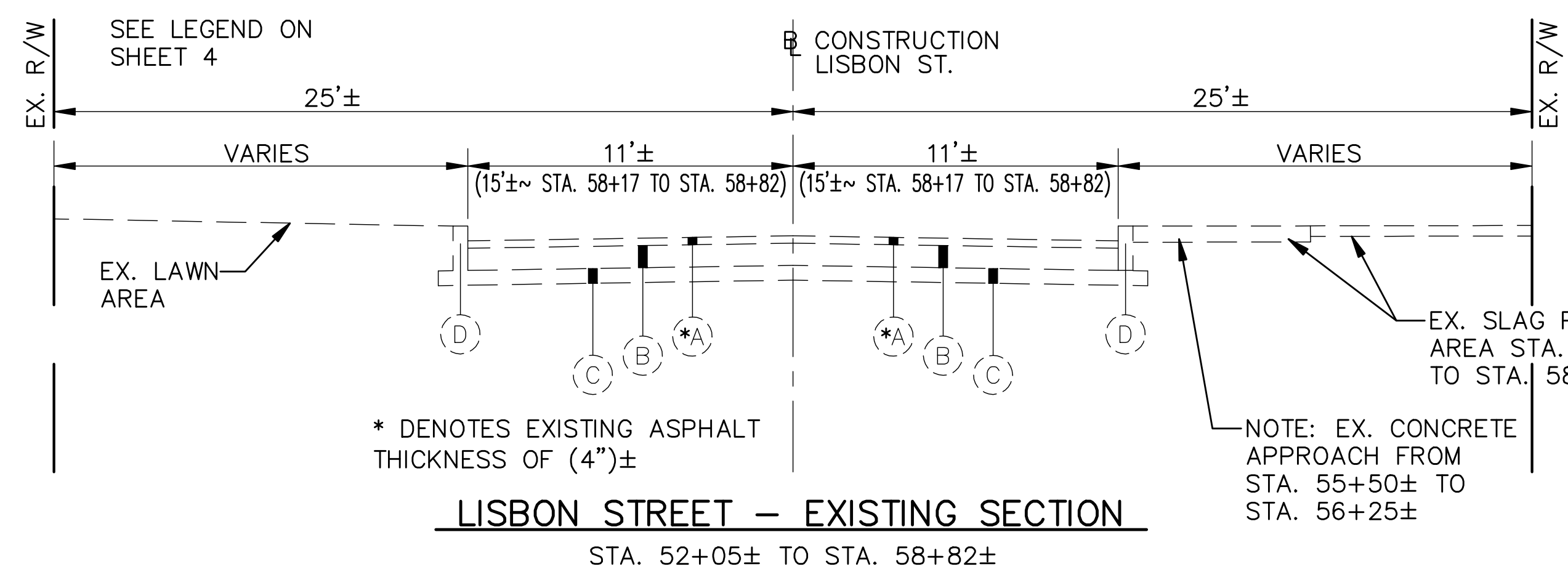


LISBON STREET – PROPOSED SECTION
 STA. 58+82± TO STA. 73+59±

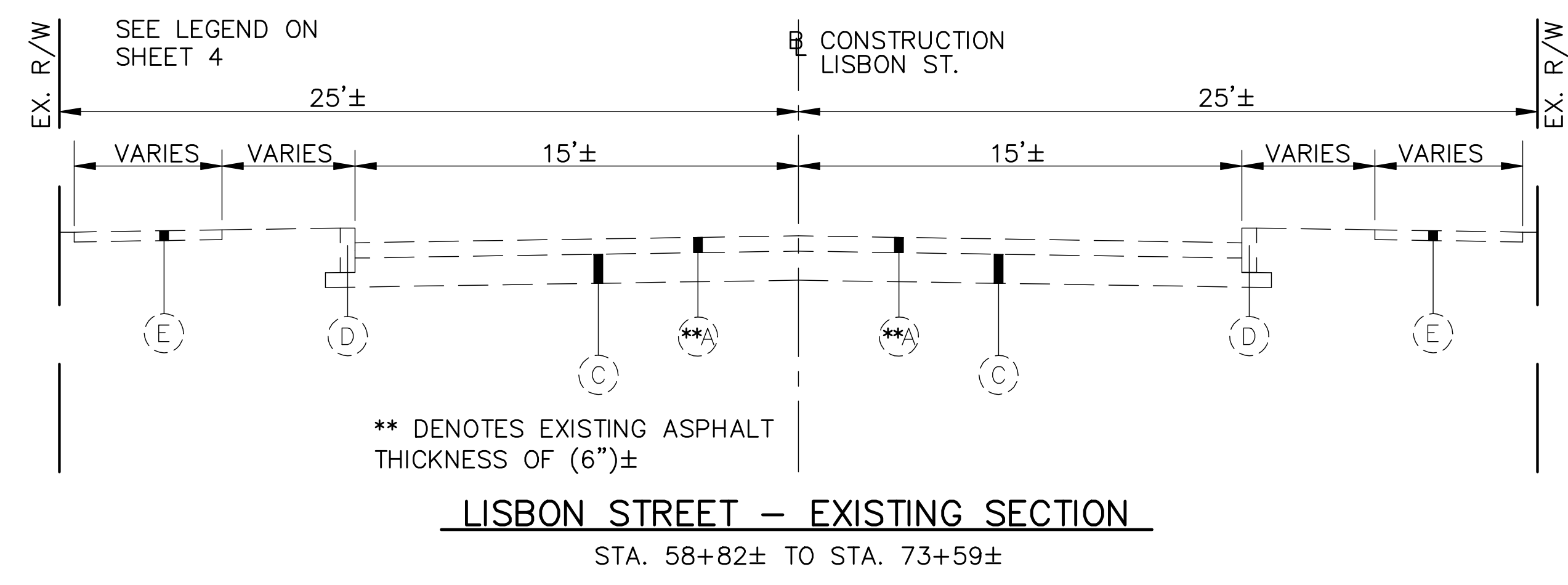
SPECIAL NOTES FOR ASPHALT PAVEMENT PLANING OPERATIONS ALONG LISBON STREET

THE 3" THICK ASPHALT PAVEMENT PLANING SHALL BE PERFORMED TO THE CROWN OF THE EXISTING PAVEMENT IN ORDER TO MAINTAIN THE PAVEMENT CROSS SLOPE; THE CONTRACTOR WILL NOT BE PERMITTED TO STRADDLE THE CROWN OF THE ROADWAY DURING PAVEMENT PLANING OPERATIONS.

PAVEMENT PLANING ALONG LISBON STREET SHALL NOT BE PERFORMED UNTIL THE CONTRACTOR IS READY FOR PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE & SURFACE COURSES. THE NUMBER OF DAYS BETWEEN THE PAVEMENT PLANING OPERATIONS AND THE PLACEMENT OF THE ASPHALT OVERLAY SHALL BE MINIMIZED AS MUCH AS PRACTICAL IN ORDER TO MINIMIZE THE EXPOSURE OF THE PLANED SURFACE TO THE WEATHER, TRAFFIC, EQUIPMENT, ETC.



LISBON STREET – EXISTING SECTION
 STA. 52+05± TO STA. 58+82±

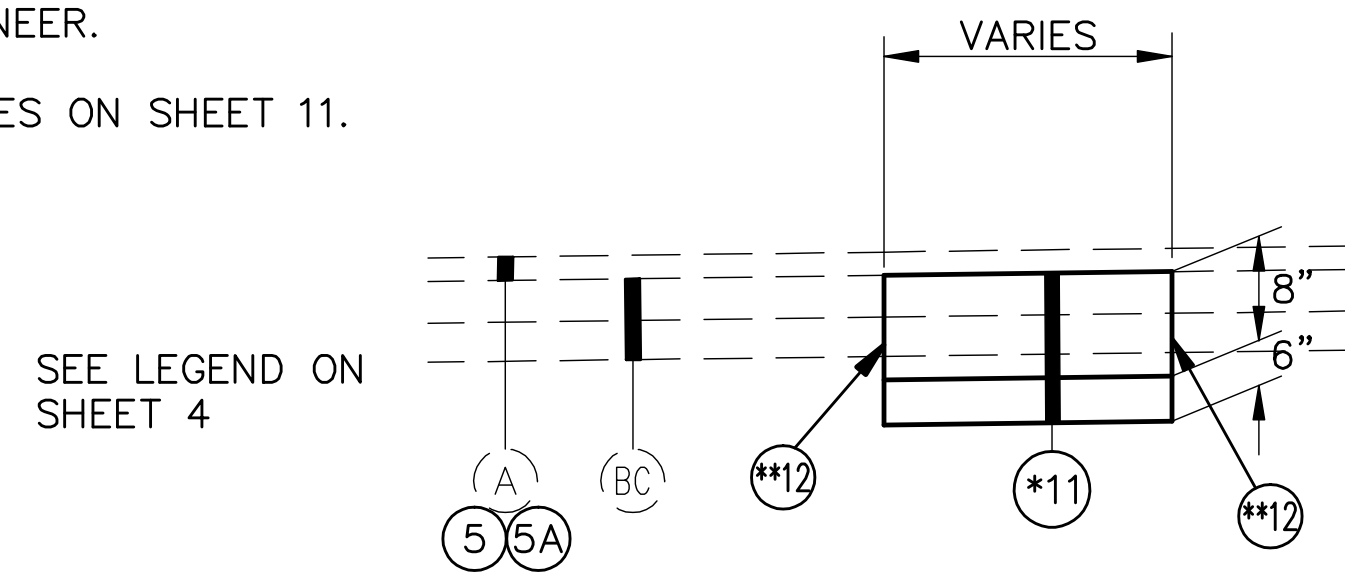


LISBON STREET – EXISTING SECTION
 STA. 58+82± TO STA. 73+59±

* RIGID PAVEMENT REPLACEMENT, CLASS QC MS, AS PER PLAN SHALL CONSIST OF ITEM 305: 8" CONCRETE BASE AND ITEM 304: 6" AGGREGATE BASE.

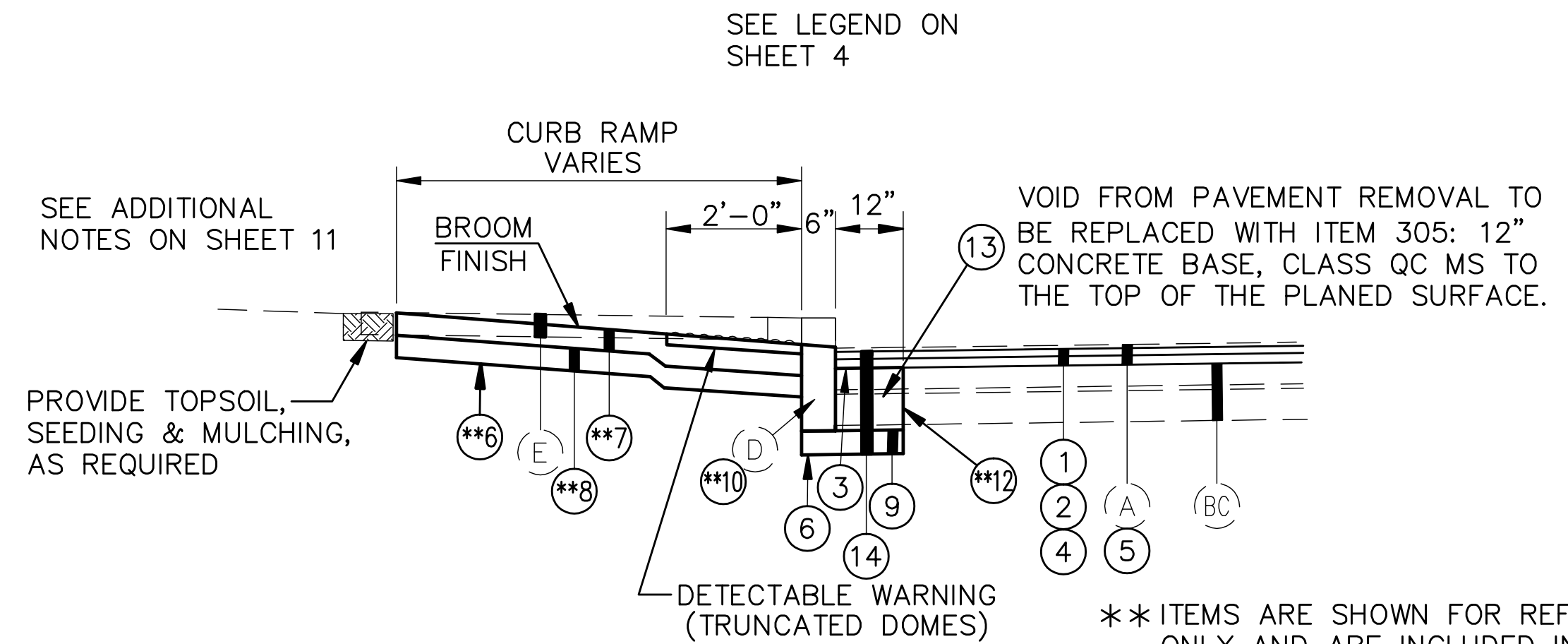
UPON COMPLETION OF THE PAVEMENT PLANING, RIGID PAVEMENT REPLACEMENT SHALL BE PLACED TO THE TOP OF THE PLANED SURFACE AND SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.

SEE AS PER PLAN NOTES ON SHEET 11.



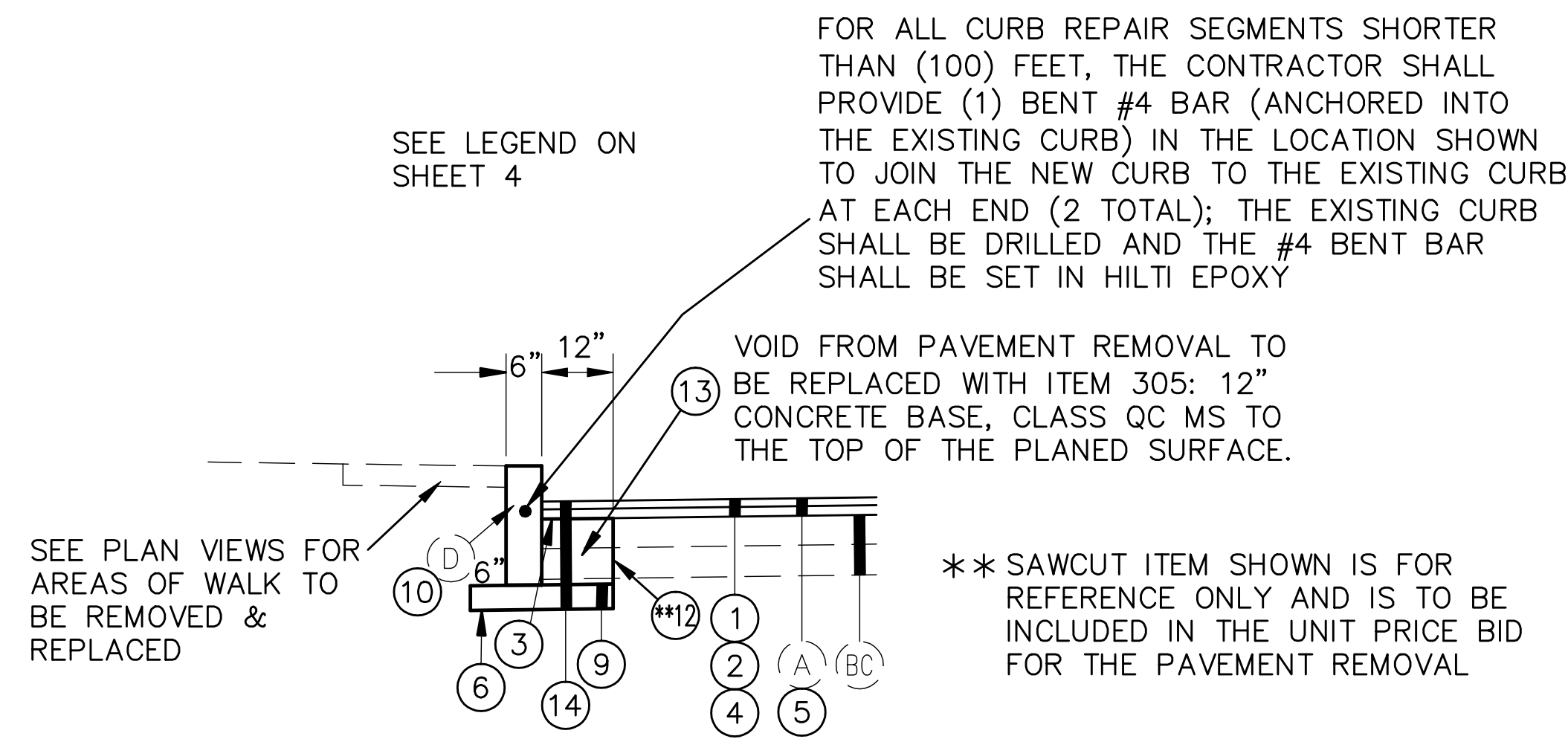
ITEM 255: FULL DEPTH PAVEMENT REMOVAL WITH RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN

** ITEMS ARE SHOWN FOR REFERENCE ONLY AND ARE INCLUDED IN THE UNIT PRICE BID FOR FULL DEPTH PAVEMENT REMOVAL WITH RIGID REPLACEMENT



CURB RAMP, AS PER PLAN
SEE ODOT STANDARD CONSTRUCTION DRAWING BP-7.1

** ITEMS ARE SHOWN FOR REFERENCE ONLY AND ARE INCLUDED IN THE UNIT PRICE BID FOR THE CURB RAMP AND/OR PAVEMENT REMOVAL



CURB REPAIR/REPLACEMENT DETAIL

SEE LEGEND ON SHEET 4

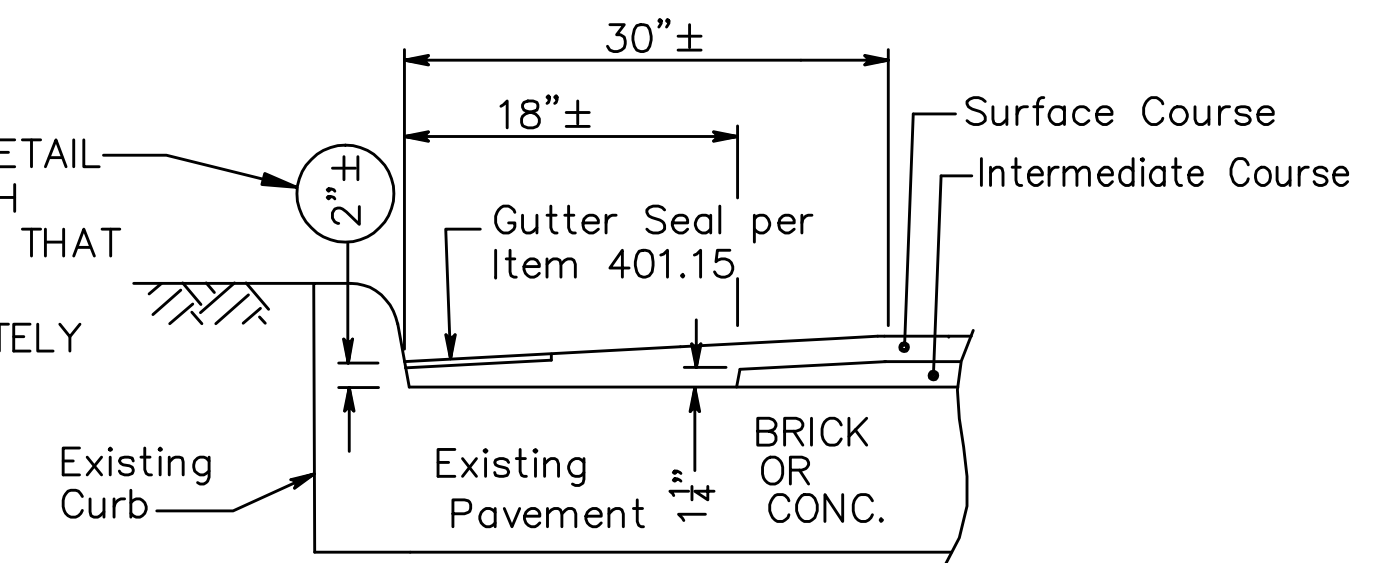
FOR ALL CURB REPAIR SEGMENTS SHORTER THAN (100) FEET, THE CONTRACTOR SHALL PROVIDE (1) BENT #4 BAR (ANCHORED INTO THE EXISTING CURB) IN THE LOCATION SHOWN TO JOIN THE NEW CURB TO THE EXISTING CURB AT EACH END (2 TOTAL); THE EXISTING CURB SHALL BE DRILLED AND THE #4 BENT BAR SHALL BE SET IN HILTI EPOXY

VOID FROM PAVEMENT REMOVAL TO BE REPLACED WITH ITEM 305: 12" CONCRETE BASE, CLASS QC MS TO THE TOP OF THE PLANED SURFACE.

** SAWCUT ITEM SHOWN IS FOR REFERENCE ONLY AND IS TO BE INCLUDED IN THE UNIT PRICE BID FOR THE PAVEMENT REMOVAL

SEE PLAN VIEWS FOR AREAS OF WALK TO BE REMOVED & REPLACED

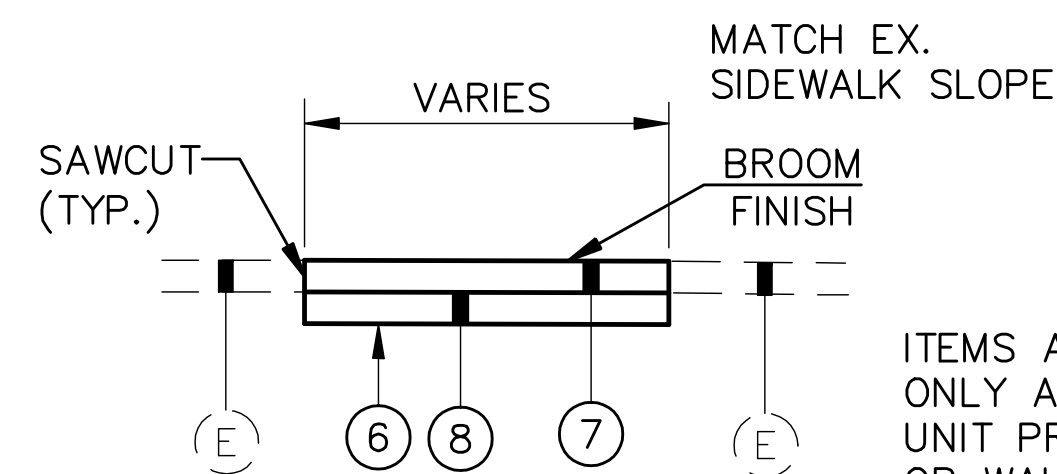
THE ASPHALT CONCRETE GUTTER FINISH DETAIL AS SHOWN SHALL BE IN ACCORDANCE WITH ODOT STANDARD DRAWING BP-3.1 EXCEPT THAT THE THICKNESS AT THE CURB SHALL BE APPROXIMATELY 2" GIVING AN APPROXIMATELY 3"-4" CURB REVEAL.



IN ORDER TO ACHIEVE PROPER COMPACTION OF THE ASPHALT CONCRETE FOR THE GUTTER FINISH, THE CONTRACTOR SHALL USE A SMALLER WIDTH ROLLER OF ADEQUATE WEIGHT WHERE THE MAJORITY OF THE WIDTH OF THE ROLLER WILL BE WITHIN THE GUTTER FINISH LIMITS. HAND PLATE TAMPERS OR HAND ROLLERS WILL NOT BE PERMITTED.

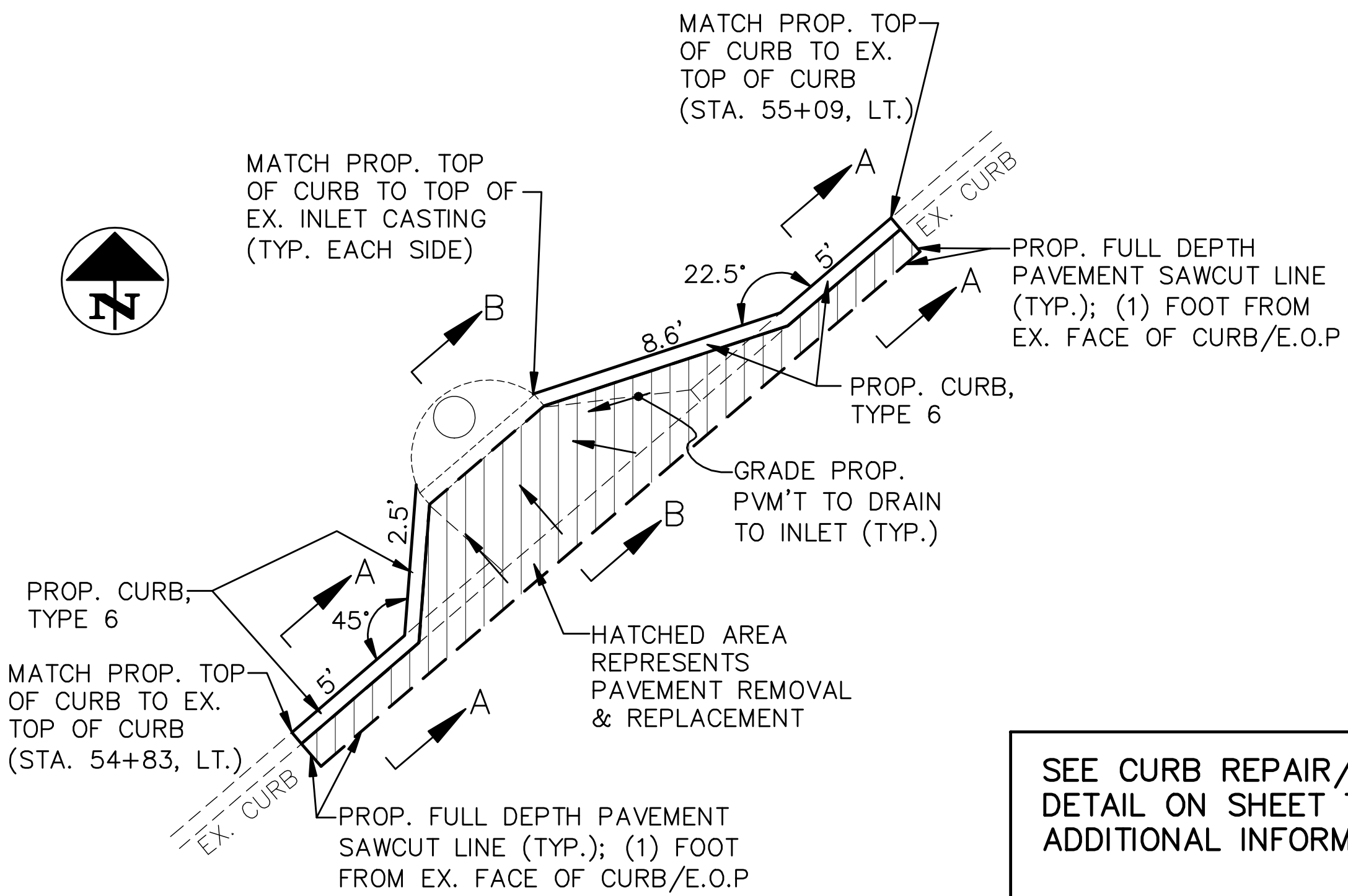
GUTTER FINISH - LISBON STREET CURB SECTIONS ONLY

4"/6" CONCRETE WALK, AS PER PLAN TO BE USED FOR WALK REPLACEMENT AREAS WHERE SPECIFIED IN THE PLANS; SEE ADDITIONAL NOTES ON SHEET 11



ITEMS ARE SHOWN FOR REFERENCE ONLY AND ARE INCLUDED IN THE UNIT PRICES BID FOR THE NEW WALK OR WALK REMOVAL

4" OR 6" CONCRETE WALK, AS PER PLAN

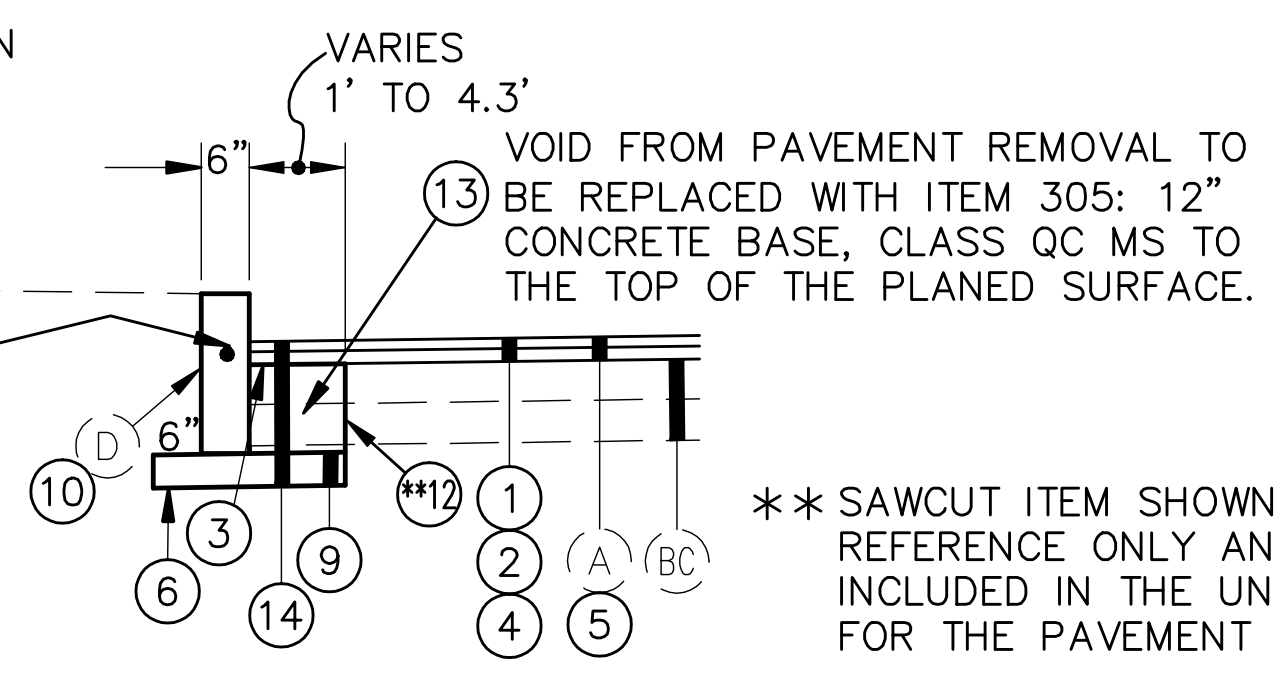


PLAN VIEW
(NOT TO SCALE)

SEE CURB REPAIR/REPLACEMENT
DETAIL ON SHEET 7 FOR
ADDITIONAL INFORMATION

SEE PLAN VIEW ON SHEET 43
FOR ADDITIONAL INFORMATION

SEE LEGEND ON
SHEET 4

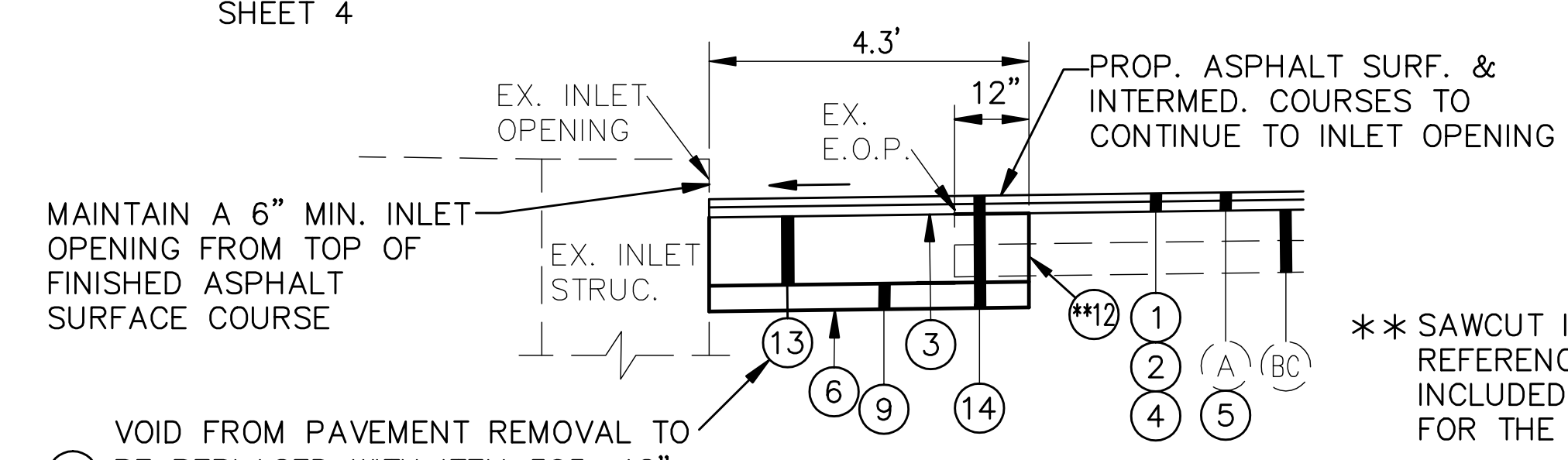


SECTION A-A
(NOT TO SCALE)

PROVIDE (1) BENT #4 BAR (ANCHORED INTO THE EXISTING CURB) IN THE LOCATION SHOWN TO JOIN THE NEW CURB TO THE EXISTING CURB AT EACH END (2 TOTAL); THE EXISTING CURB SHALL BE DRILLED AND THE #4 BENT BAR SHALL BE SET IN HILTI EPOXY

** SAWCUT ITEM SHOWN IS FOR REFERENCE ONLY AND IS TO BE INCLUDED IN THE UNIT PRICE BID FOR THE PAVEMENT REMOVAL

SEE LEGEND ON
SHEET 4



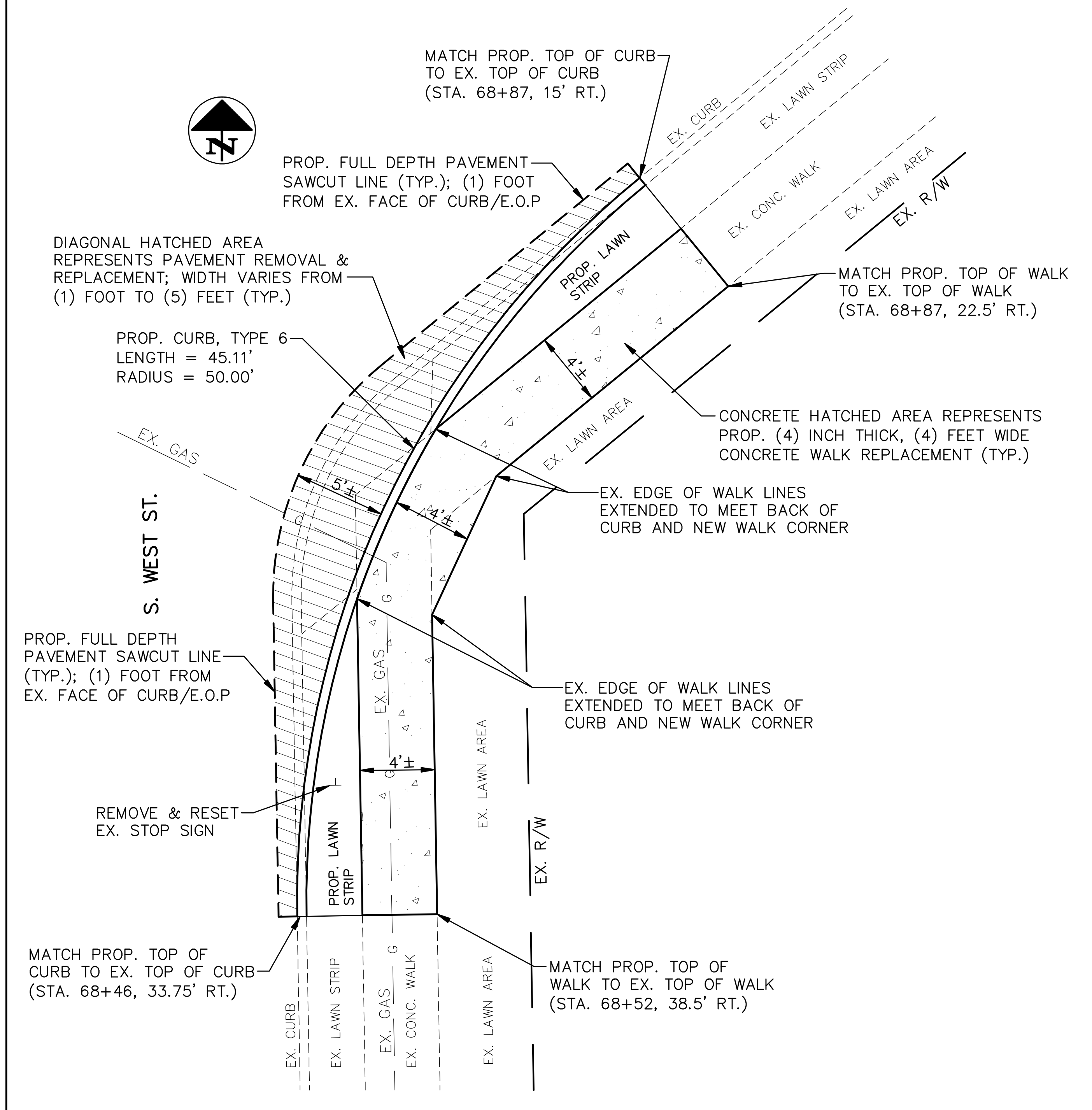
SECTION B-B
(NOT TO SCALE)

VOID FROM PAVEMENT REMOVAL TO BE REPLACED WITH ITEM 305: 12" CONCRETE BASE, CLASS QC MS TO THE TOP OF THE PLANED SURFACE.

** SAWCUT ITEM SHOWN IS FOR REFERENCE ONLY AND IS TO BE INCLUDED IN THE UNIT PRICE BID FOR THE PAVEMENT REMOVAL

CURB/PAVEMENT DETAILS

(LISBON STREET - STA. 54+83 TO STA. 55+09, LEFT SIDE)



PAVEMENT WIDENING DETAIL
(LISBON STREET - STA. 68+46 TO STA. 68+87, RIGHT SIDE)
(NOT TO SCALE)

SEE CURB REPAIR/REPLACEMENT
DETAIL ON SHEET 7 FOR
ADDITIONAL INFORMATION

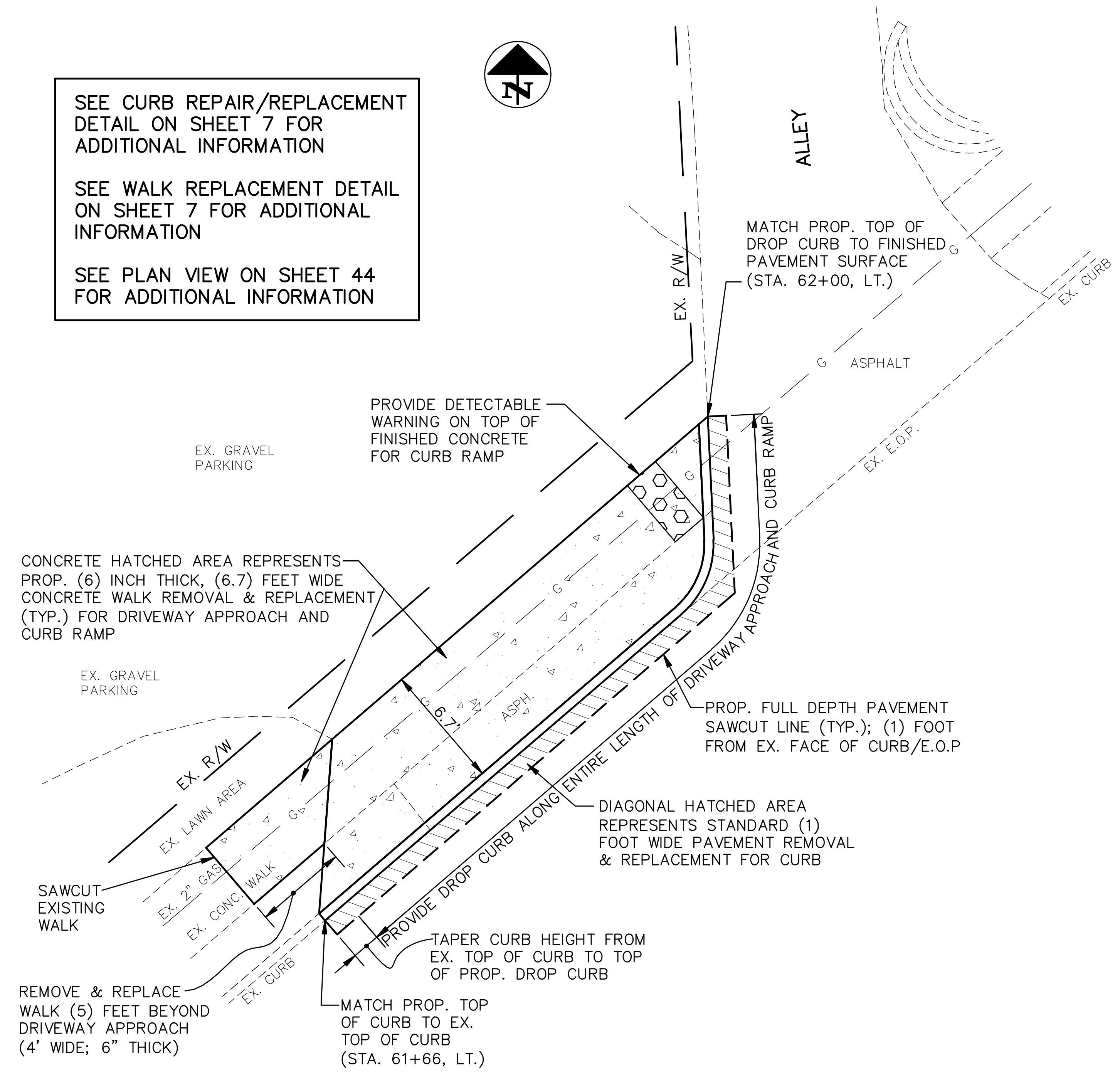
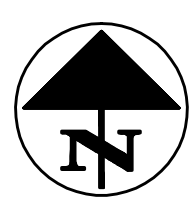
SEE WALK REPLACEMENT DETAIL
ON SHEET 7 FOR ADDITIONAL
INFORMATION

SEE PLAN VIEW ON SHEET 45
FOR ADDITIONAL INFORMATION

SEE CURB REPAIR/REPLACEMENT
 DETAIL ON SHEET 7 FOR
 ADDITIONAL INFORMATION

SEE WALK REPLACEMENT DETAIL
 ON SHEET 7 FOR ADDITIONAL
 INFORMATION

SEE PLAN VIEW ON SHEET 44
 FOR ADDITIONAL INFORMATION



CURB RAMP/DRIVEWAY APPROACH DETAIL
(LISBON STREET – STA. 61+64 TO STA. 62+00, LEFT SIDE)
 (NOT TO SCALE)

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

COMCAST
ATTN. DAVE TATAREK
100 WELDAY AVE., SUITE A
WINTERSVILLE, OHIO 43953
(724) 384 2416 (OFFICE)
(412) 897 8202 (CELL)

CITY OF COLUMBIANA
WATER DEPARTMENT
28 WEST FRIEND STREET
COLUMBIANA, OHIO 44408
ATTN. MIKE NULF
(330) 482 2173

AT&T
50 WEST BOWERY STREET
AKRON, OHIO 44308
ATTN. TORRICE ROBINSON
TCS DESIGN ENGINEER
YOUNGSTOWN LEGAL MANDATE
(330) 734 5117

CITY OF COLUMBIANA
WASTE WATER DEPARTMENT
28 WEST FRIEND STREET
COLUMBIANA, OHIO 44408
ATTN. BRIAN DICKEN
(330) 482 2173

COLUMBIA GAS OF OHIO
1020 WEST SALEM STREET
SALEM, OHIO 44460
ATTN. CHRISTOPHER ROBISON
(419) 957 6633

CITY OF COLUMBIANA
ELECTRIC DEPARTMENT
28 WEST FRIEND STREET
COLUMBIANA, OHIO 44408
ATTN. DOUG STURGEON
(330) 482 2173

OHIO EDISON
730 SOUTH AVENUE
YOUNGSTOWN, OHIO 44502
ATTN. SAMUEL ARISMAN
OFFICE: (330) 740 7506
CELL: (330) 610 7331

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C. EXISTING UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE LOCATION ONLY. NEITHER THE OWNER NOR THE CONSULTING ENGINEER WARRANT OR REPRESENT THE INFORMATION SHOWN TO BE COMPLETE OR ACCURATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INVESTIGATION AND LOCATION OF ALL EXISTING UTILITIES PRIOR TO AND DURING CONSTRUCTION.

ROADWAY

THE CONTRACTOR SHALL COORDINATE WORK AND OBTAIN PERMITS PRIOR TO WORKING WITHIN ANY CITY RIGHT-OF-WAYS FROM THE FOLLOWING:

CITY OF COLUMBIANA
28 WEST FRIEND STREET
COLUMBIANA, OHIO 44408
ATTN: LANCE WILLARD, CITY MANAGER
(330) 482 2173

SOIL CONDITIONS

A SOILS REPORT FOR THE PROJECT HAS NOT BEEN PERFORMED. THE ENGINEER DOES NOT MAKE ANY REPRESENTATION AS TO THE UNDERLYING SOIL, ROCK, OR GROUND WATER WHICH MAY BE ENCOUNTERED DURING CONSTRUCTION OF THE PROPOSED WORK.

ELEVATION DATUM

ALL ELEVATIONS ARE RELATIVE TO THE NAVD88 VERTICAL DATUM.

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, ANY POWER-OPERATED CONSTRUCTION TYPE DEVICE SHALL NOT BE OPERATED BETWEEN THE HOURS OF 8:00 PM AND 7:00 AM. IN ADDITION, ANY SUCH DEVICE SHALL NOT BE OPERATED AT ANY TIME IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

DAILY CLEANUP

THE CONTRACTOR SHALL DAILY REMOVE ALL DIRT, MUD, SOIL, DEBRIS, AND FOREIGN MATTER THAT MAY BE TRACKED ONTO EXISTING ROADWAYS, STREETS, DRIVES OR WALKS BY ANY PROJECT EQUIPMENT OR SUPPLIER EQUIPMENT. THE CONTRACTOR SHALL PROVIDE A POWER BROOM FOR THIS TASK. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE INCLUDED IN THE VARIOUS PRICES BID IN THE PROPOSAL.

PROTECTION AND RESTORATION OF PROPERTY

THE CONTRACTOR SHALL ADHERE TO SECTION 107.10 OF THE ODOT C&MS FOR THE PROTECTION AND RESTORATION OF PROPERTY. ANY RETAINING WALLS, STEPS, LANDSCAPE ITEMS, BUILDINGS, MAIL BOXES, STREET SIGNS, ROAD SIGNS, UTILITY POLES, ETC. DISTURBED OR DAMAGED BY THE CONTRACTOR SHALL BE RESTORED TO THEIR ORIGINAL OR BETTER CONDITION AT THE EXPENSE OF THE CONTRACTOR.

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

ITEM 202: PAVEMENT REMOVED, AS PER PLAN

FOR SPECIFIC AREAS OF PAVEMENT REMOVAL (I.E. THE (5) FOOT & (7) FOOT WIDTH PAVEMENT REMOVAL ALONG THE RIGHT SIDE OF FAIRFIELD SCHOOL ROAD; THE PAVEMENT REMOVAL ASSOCIATED WITH CURB/CURB RAMP INSTALLATION; ETC.), THE CONTRACTOR SHALL INCLUDE ALL NECESSARY EXCAVATION IN ORDER TO ESTABLISH THE PROPOSED SUBGRADE FOR THE PLACEMENT OF THE PROPOSED PAVEMENT REPLACEMENT BUILDUP.

ALL REMOVED PAVEMENT & EXCAVATED MATERIAL SHALL BE PROPERLY DISPOSED OF.

ALL EQUIPMENT, LABOR, MATERIALS, AND INCIDENTALS FOR THE COMPLETE REMOVAL & DISPOSAL AS DESCRIBED IN THE NOTES ABOVE AND AS DESCRIBED IN THE TYPICAL SECTIONS ON SHEETS 4, 5, 7, & 8 SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 202: PAVEMENT REMOVED, AS PER PLAN.

ITEM 254: PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (T=1-1/2"± OR T=3"±)

DURING PAVEMENT PLANING OPERATIONS ALONG ALL OF THE ROADWAYS, THE EXISTING ASPHALT CONCRETE SHALL BE PLANED DOWN THE AVERAGE DEPTH SPECIFIED IN THE PLANS (I.E. 1-1/2"± ALONG FAIRFIELD SCHOOL ROAD, AND 3"± ALONG LISBON STREET). IN AREAS OF EXCESSIVE BUILDUP ALONG LISBON STREET, NO LESS THAN 2" OF THE EXISTING INTACT ASPHALT CONCRETE PAVEMENT SHALL BE LEFT FOLLOWING PAVEMENT PLANING OPERATIONS.

WITHIN THE CONCRETE BASE SECTIONS ALONG LISBON STREET, THE CONTRACTOR SHALL TAKE EVERY PRECAUTION SO AS NOT TO DISTURB THE EXISTING CONCRETE BASE. ANY CONCRETE THAT IS DISTURBED OR DAMAGED SHALL BE IMMEDIATELY REPLACED WITH ITEM 301: ASPHALT CONCRETE BASE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL ADJUST THE PLANING MACHINE AS NECESSARY TO PREVENT MILLING OF THE EXISTING CONCRETE BASE.

ALL CUTTINGS SHALL BECOME PROPERTY OF THE CITY OF COLUMBIANA, AND AFTER LOADING THE CUTTINGS THE CONTRACTOR SHALL HAUL AND STOCKPILE THE CUTTINGS AT THE COLUMBIANA FIRE STATION LOCATED AT 500 EAST METZ ROAD. STOCKPILING OF THE MATERIAL SHALL BE COORDINATED WITH THE CITY OF COLUMBIANA.

PAYMENT FOR THE HAULING AND STOCKPILING OF THE AFOREMENTIONED CUTTINGS SHALL BE INCLUDED IN THE UNIT PRICES BID FOR ITEM 690 SPECIAL - HAULING AND STOCKPILING OF PAVEMENT PLANING CUTTINGS (T=1-1/2"± OR T=3"±). THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 690 SPECIAL - HAULING AND STOCKPILING OF PAVEMENT PLANING CUTTINGS (T=1-1/2"±)	<u>8,127</u> SQ. YD.
ITEM 690 SPECIAL - HAULING AND STOCKPILING OF PAVEMENT PLANING CUTTINGS (T=3"±)	<u>7,414</u> SQ. YD.

ITEM 690: SPECIAL-PAVEMENT PLANING, ASPHALT CONCRETE, (RAILROAD BRIDGE)

THE FOLLOWING PERTAINS TO ASPHALT PAVEMENT PLANING OPERATIONS UNDER AND/OR IMMEDIATELY ADJACENT TO THE RAILROAD BRIDGE ALONG LISBON STREET.

DURING ASPHALT PAVEMENT PLANING OPERATIONS UNDER THE RAILROAD BRIDGE, WITHIN (25) FEET OF THE RAILROAD BRIDGE TO THE NORTHEAST, AND TO THE PROPOSED BUTT JOINT TO THE SOUTHWEST OF THE RAILROAD BRIDGE, AND DUE TO HEIGHT RESTRICTIONS UNDER THE RAILROAD BRIDGE, THE CONTRACTOR SHALL UTILIZE A SKID-STEER-MOUNTED PAVEMENT PLANER TO PERFORM THE NECESSARY ASPHALT PAVEMENT PLANING OPERATIONS.

THE EXISTING ASPHALT CONCRETE SHALL BE PLANED DOWN AN AVERAGE DEPTH OF 3"± AND/OR TO THE EXISTING CONCRETE BASE. THE CONTRACTOR SHALL TAKE EVERY PRECAUTION SO AS NOT TO DISTURB THE EXISTING CONCRETE BASE. THE CONTRACTOR SHALL MAKE ANY NECESSARY ADJUSTMENTS DURING PLANING OPERATIONS TO PREVENT MILLING OF THE EXISTING CONCRETE BASE. ANY CONCRETE THAT IS DISTURBED OR DAMAGED SHALL BE IMMEDIATELY REPLACED WITH ITEM 301: ASPHALT CONCRETE BASE AT THE CONTRACTOR'S EXPENSE.

ALL PERTINENT SPECIFICATIONS DESCRIBED IN ODOT C&MS ITEM 254 RELATING TO PAVEMENT PLANING OF ASPHALT CONCRETE SHALL BE ADHERED TO.

ALL PAVEMENT PLANING CUTTINGS FROM THE PAVEMENT PLANING PERFORMED UNDER THE RAILROAD BRIDGE, WITHIN (25) FEET OF THE RAILROAD BRIDGE TO THE NORTHEAST, AND/OR TO THE PROPOSED BUTT JOINT TO THE SOUTHWEST OF THE RAILROAD BRIDGE, SHALL BE REMOVED AND PROPERLY DISPOSED OF.

NOTE: THE CONTRACTOR SHALL TAKE CARE SO AS NOT TO DAMAGE THE EXISTING STONE BRIDGE ABUTMENTS/WINGWALLS, OVERHEAD BRIDGE SUPERSTRUCTURE, AND/OR OTHER BRIDGE APPURTENANCES. ANY DAMAGE TO THE AFOREMENTIONED SHALL BE REPAIRED BY THE CONTRACTOR AT THEIR OWN EXPENSE.

ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS TO PERFORM THE AFOREMENTIONED SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 690: SPECIAL-PAVEMENT PLANING, ASPHALT CONCRETE, (RAILROAD BRIDGE).

INTERSECTIONS

ALL SIDE STREET AND ALLEY INTERSECTIONS WITHIN THE PROJECT LIMITS SHALL BE PLANED TO A DISTANCE DESIGNATED IN THE PLANS BACK FROM THE EDGE OF THE MAIN LINE PAVING, TO ASSURE A SMOOTH TRANSITION FROM THE MAIN LINE TO THE BALANCE OF THE INTERSECTING SIDE STREET OR ALLEY, AS DIRECTED BY THE CONSTRUCTION ENGINEER OR INSPECTOR.

A BUTT JOINT AS PER O.D.O.T. STANDARD DRAWING BP-3.1 SHALL BE PROVIDED WHERE THE PROPOSED PAVEMENT MEETS THE EXISTING PAVEMENT, AS SHOWN IN THESE PLANS, ON ALL SIDE STREETS, ALLEYS AND AT THE BEGINNING AND END OF THE PROJECT.

JOB MIX FORMULAS

THIS PROJECT IS PARTIALLY FUNDED THROUGH THE OHIO DEPARTMENT OF TRANSPORTATION (ODOT). THE CONTRACTOR SHALL SUBMIT AN ODOT APPROVED JOB MIX FORMULA (JMF) FOR EACH PAVEMENT MIX TO BE USED ON THE PROJECT INCLUDING BUT NOT LIMITED TO ASPHALT, CONCRETE, 304 AGGREGATE BASE, AS PER PLAN, ETC.

ITEM 304 - AGGREGATE BASE, AS PER PLAN

ALL ITEM 304-AGGREGATE BASE USED ON THIS PROJECT (INCLUDING BACKFILL MATERIAL) SHALL BE IN ACCORDANCE WITH THE ODOT C&MS EXCEPT THAT THE FOLLOWING WILL BE THE ONLY PERMITTED MATERIALS USED WITH ADDITIONAL NOTES:

- "CRUSHED CARBONATE STONE (CCS)"; MUST MEET THE GRADATION AND PHYSICAL REQUIREMENTS OF ODOT 703.17

- "CRUSHED GRAVEL"; MUST MEET THE GRADATION AND PHYSICAL REQUIREMENTS OF ODOT 703.17; MUST BE MANUFACTURED FROM MATERIAL RETAINED ON THE 1/2" SIEVE; MUST CONTAIN THE PERTINENT FINE MATERIAL TO PREVENT IT FROM BECOMING UNSTABLE

- "CRUSHED AIR-COOLED BLAST FURNACE SLAG (ACBFS)"; MUST MEET THE GRADATION AND PHYSICAL REQUIREMENTS OF ODOT 703.17; MUST MEET THE REQUIREMENTS OF ODOT SUPPLEMENT 1027; MATERIAL MUST BE VERIFIED AS BEING ENVIRONMENTALLY PERMISSIBLE

CALC. BY: J.F.B.
DATE: 10/23
CHKD. BY: D.G.D.
DATE: 10/23

GENERAL NOTES

COL - FAIRFIELD SCH. RD. & LISBON ST.

DRIVEWAYS AND PARKING LOTS (LISBON STREET)

A BUTT JOINT AS PER O.D.O.T. STANDARD DRAWING BP-3.1 SHALL BE PROVIDED WHERE THE PROPOSED PAVEMENT MEETS THE EXISTING PAVEMENT FOR ALL DRIVEWAYS AND PARKING LOTS WITHIN THE PROJECT LIMITS ALONG LISBON STREET.

DRIVEWAYS (FAIRFIELD SCHOOL ROAD)

SINCE THE PROPOSED ASPHALT PAVEMENT BUILDUP ALONG FAIRFIELD SCHOOL ROAD WILL BE APPROXIMATELY 1-1/2" HIGHER UPON COMPLETION OF THE PLACEMENT OF THE ASPHALT SURFACE COURSE (I.E. 1-1/2" PAVEMENT PLANING WITH 3" ASPHALT OVERLAY), THE CONTRACTOR WILL BE REQUIRED TO PLACE ASPHALT PAVEMENT WITHIN THE PERTINENT DRIVEWAY APPROACHES TO A MAXIMUM LENGTH OF (5) FEET BACK FROM THE MAINLINE EDGE OF PAVEMENT IN ORDER TO PROVIDE A SMOOTH TRANSITION FROM THE PERTINENT DRIVEWAY APPROACHES TO THE MAINLINE PAVEMENT.

- ANY VEGETATION WITHIN THE (5) FOOT LIMITS OF THE DRIVEWAY APPROACH SHALL BE SPRAYED WITH HERBICIDE IN ADVANCE OF THE PLACEMENT OF THE PROPOSED ASPHALT PAVEMENT WITHIN THE DRIVEWAY APPROACH.
- A BUTT JOINT SHALL BE PROVIDED FOR ALL SLAG & ASPHALT DRIVEWAY APPROACHES WHERE THE PROPOSED ASPHALT WILL MEET THE EXISTING PAVEMENT.
- THE ASPHALT THICKNESS SHALL BE TAPERED AT ALL CONCRETE DRIVEWAY APPROACHES TO PROVIDE A SMOOTH TRANSITION WHERE THE PROPOSED ASPHALT WILL MEET THE EXISTING DRIVEWAY PAVEMENT.
- ALL JOINTS WHERE THE NEW ASPHALT PAVEMENT MEETS THE EXISTING DRIVEWAY PAVEMENT AND/OR AT THE EDGE OF MAINLINE PAVEMENT SHALL BE SEALED.
- QUANTITIES FOR THE ASPHALT PAVEMENT TO ACCOMMODATE THE AFOREMENTIONED DRIVEWAY APPROACHES ARE INCLUDED IN THE CALCULATIONS ON SHEET 22.

NOTE: THE AFOREMENTIONED WILL NOT BE REQUIRED TO BE PERFORMED FOR THE DRIVEWAY APPROACHES THAT ARE PROPOSED TO BE REMOVED & REPLACED AS PART OF THE PROPOSED VARIOUS DRAINAGE IMPROVEMENTS ALONG FAIRFIELD SCHOOL ROAD, SINCE THESE WILL ALREADY BE ADJUSTED TO ACCOMMODATE THE 1-1/2" HIGHER TOP OF FINISHED ASPHALT SURFACE COURSE.

TACK/ASPHALT PLACEMENT RAILROAD BRIDGE/LISBON STREET

THE FOLLOWING PERTAINS TO THE PLACEMENT OF TACK COAT AND ASPHALT PAVING OPERATIONS UNDER AND/OR IMMEDIATELY ADJACENT TO THE RAILROAD BRIDGE ALONG LISBON STREET.

DURING PLACEMENT OF TACK COAT AND/OR ASPHALT PAVING OPERATIONS UNDER THE RAILROAD BRIDGE, WITHIN (25) FEET OF THE RAILROAD BRIDGE TO THE NORTHEAST, AND TO THE PROPOSED BUTT JOINT TO THE SOUTHWEST OF THE RAILROAD BRIDGE, AND DUE TO HEIGHT RESTRICTIONS UNDER THE RAILROAD BRIDGE, IT IS ANTICIPATED THAT THE TRUCKS THAT TRANSPORT THE TACK COAT AND/OR ASPHALT MATERIAL WILL BE TOO TALL FOR THE AVAILABLE CLEARANCE UNDER THE BRIDGE. IT IS ALSO ANTICIPATED THAT THE TYPICAL PLACEMENT OF TACK COAT FROM THE TACK TRUCK AND THE TYPICAL TRANSFERRING OF ASPHALT PAVEMENT FROM THE TRUCK TO THE PAVING MACHINE WILL NOT BE POSSIBLE.

THE CONTRACTOR WILL BE REQUIRED TO ADJUST THE MEANS & METHODS USED FOR THE PLACEMENT OF TACK COAT UNDERNEATH AND/OR ADJACENT TO THE RAILROAD BRIDGE.

THE CONTRACTOR WILL BE REQUIRED TO ADJUST THE MEANS & METHODS USED FOR THE PLACEMENT OF THE ASPHALT PAVEMENT UNDERNEATH AND/OR ADJACENT TO THE RAILROAD BRIDGE.

THE CONTRACTOR MAY ALSO NEED TO ADJUST THE ROUTE TAKEN BY THE TACK TRUCK AND/OR ASPHALT PAVING TRUCKS IN ORDER TO ACCOMMODATE THE PAVING OPERATIONS LOCATED ON THE SOUTHWEST SIDE OF THE RAILROAD BRIDGE.

ALL COSTS FOR THE AFOREMENTIONED SHALL BE INCLUDED IN THE PERTINENT UNIT PRICES BID FOR THE ASPHALT SURFACE, ASPHALT INTERMEDIATE, AND/OR TACK COAT ITEMS.

UNSUITABLE FOUNDATION SOILS

IF UNSUITABLE FOUNDATION SOILS ARE ENCOUNTERED IN AREAS OF THE REMOVED ROADBED DURING ANY REQUIRED FULL DEPTH REMOVAL AND REPLACEMENT, THEY SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL AND GEOTEXTILE FABRIC (AS NECESSARY) MEETING THE REQUIREMENTS OF 204.02. THE LOCATIONS AND DIMENSIONS WILL BE AS DETERMINED BY THE CONSTRUCTION ENGINEER.

UNSUITABLE SUBGRADE REMOVAL & REPLACEMENT AND THE PLACEMENT OF GEOTEXTILE FABRIC SHALL BE IN ACCORDANCE WITH ODOT C&MS ITEM 204.

ALL WORK FOR UNSUITABLE SUBGRADE REMOVAL & REPLACEMENT AND THE PLACEMENT OF GEOTEXTILE FABRIC SHALL BE AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES FOR THE WORK AS DESCRIBED ABOVE HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

- ITEM 204 400 CU. YD. EXCAVATION OF SUBGRADE
- ITEM 204 400 CU. YD. GRANULAR EMBANKMENT
- ITEM 204 2800 SQ. YD. GEOTEXTILE FABRIC

ITEM 255: FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN

DURING THE PAVEMENT PLANING OPERATIONS, THE CONSTRUCTION ENGINEER WILL VISUALLY OBSERVE THE EXISTING PAVEMENT, AND BASED UPON THEIR BEST JUDGMENT AS TO THE CONDITION OF THE EXISTING CONCRETE BASE AND/OR PLANED SURFACE, DECIDE WHETHER OR NOT FULL DEPTH REMOVAL AND RIGID REPLACEMENT IS NECESSARY.

RELATIVE TO AREAS OF OBSERVED PATCHING POSSIBLY DUE TO PRIOR UTILITY TRENCHING, IF IT IS DECIDED AFTER PAVEMENT PLANING THAT THE BASE MATERIAL IS IN POOR CONDITION BASED UPON THE CONSTRUCTION ENGINEER'S BEST JUDGMENT, THEN THE CONSTRUCTION ENGINEER SHALL DIRECT THE CONTRACTOR TO PROVIDE FULL DEPTH REMOVAL AND RIGID REPLACEMENT.

AS DETAILED IN THE TYPICAL SECTIONS, THE EXISTING PAVEMENT BUILDUP ALONG THE ROADWAYS IS COMPRISED PRIMARILY OF A VARIABLE THICKNESS OF ASPHALT CONCRETE ON TOP OF A CONCRETE BASE AND/OR ON TOP OF AN AGGREGATE/STONE SUBBASE.

PAVEMENT REPLACEMENT FOR THE FULL DEPTH REPAIRS ALONG ALL OF THE ROADWAYS SHALL BE IN ACCORDANCE WITH ITEM 255: FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN AND IN ACCORDANCE WITH ODOT STANDARD DRAWINGS BP-2.1, BP-2.2, AND BP-2.5 AS SO MODIFIED IN THIS NOTE OR ON THE CONSTRUCTION DRAWINGS. THE PROPOSED CONCRETE USED FOR THE PAVEMENT REPLACEMENT SHALL BE ITEM 305: CONCRETE BASE, CLASS QC MS AND SHALL COMPLY WITH THE REQUIREMENTS OF SECTION 451.07 (I.E. MESH REINFORCEMENT REQUIRED). THE PAVEMENT REPLACEMENT SHALL EXTEND FROM THE TOP OF EXISTING CONCRETE BASE AND/OR TOP OF PLANED SURFACE DOWN 14" (8" OF ITEM 305: CONCRETE BASE AND 6" OF ITEM 304: AGGREGATE BASE, AS PER PLAN).

THE USE OF DOWEL BARS, TIE BARS, AND/OR HOOK BOLTS TO JOIN THE PROPOSED CONCRETE TO ANY EXISTING CONCRETE BASE MATERIAL WILL BE REQUIRED.

ALL FULL DEPTH PAVEMENT SAWING REQUIRED TO PERFORM THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH ITEM 255.

ALL EQUIPMENT, LABOR, MATERIALS, AND INCIDENTALS NECESSARY FOR THE COMPLETE FULL DEPTH REMOVAL AND REPLACEMENT OF PAVEMENT AREAS AS DESCRIBED ABOVE AND DESCRIBED IN THE DETAILS ON SHEET 7 SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 255: FULL DEPTH PAVEMENT REMOVAL WITH RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN. ALL FULL DEPTH PAVEMENT REMOVAL WITH RIGID REPLACEMENT SHALL BE AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITY FOR THE WORK AS DESCRIBED ABOVE HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

- ITEM 255: FULL DEPTH PAVEMENT REMOVAL WITH 100 SQ. YD. RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN

ITEM 608 - 4"/6" CONCRETE WALK, AS PER PLAN

FOR ALL WALK REMOVAL AND REPLACEMENT QUANTITIES LISTED IN THE SUB-SUMMARIES ON THE PERTINENT PLAN VIEW SHEETS, THE CONTRACTOR SHALL ALSO INCLUDE THE NECESSARY ITEM 659: SEEDING AND MULCHING ITEMS LISTED ON SHEET 12 IN THE UNIT PRICE BID FOR ITEM 608: 4"/6" CONCRETE WALK, AS PER PLAN.

DURING WALK REMOVAL AND REPLACEMENT, ANY SIGNS AND SIGN POSTS REQUIRING REMOVING AND RESETTING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 608: 4"/6" CONCRETE WALK, AS PER PLAN.

ITEM 608: CURB RAMP, AS PER PLAN (FOR ALL CURB RAMPS)

IN ADDITION TO THE INCIDENTALS REQUIRED IN ITEM 608 OF THE ODOT C&MS FOR CURB RAMP INSTALLATION, THE CONTRACTOR SHALL ALSO INCLUDE THE NECESSARY ITEM 659: SEEDING AND MULCHING ITEMS LISTED ON SHEET 12 IN THE VARIOUS UNIT PRICES BID FOR ITEM 608: CURB RAMP, AS PER PLAN.

DURING REMOVAL OF THE NECESSARY ITEMS FOR THE CURB RAMPS AND DURING CURB RAMP INSTALLATION, ANY SIGNS AND SIGN POSTS REQUIRING REMOVING AND RESETTING SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES BID FOR ITEM 608: CURB RAMP, AS PER PLAN.

ALL DETECTABLE WARNING DEVICES USED FOR THE CURB RAMPS SHALL BE IDENTICAL (I.E. NO MIXING AND MATCHING OF TYPES, COLORS, ETC.) AND IF POSSIBLE SHALL MATCH THE EXISTING DETECTABLE WARNING DEVICES PREVIOUSLY USED ALONG LISBON STREET.

ITEM 608 - DETECTABLE WARNING, AS PER PLAN

RELATIVE TO THE CURB RAMP THAT IS TO BE INSTALLED ALONG THE LEFT SIDE OF LISBON STREET AT STA. 61+96 WHICH IS TO BE INSTALLED IN COORDINATION WITH THE DRIVEWAY APPROACH BETWEEN STA. 61+64 & STA. 62+00 (SEE DETAILS ON SHEET 9), THE CONTRACTOR WILL BE REQUIRED TO INSTALL A DETECTABLE WARNING DEVICE ONTO THE FINISHED CONCRETE PAVEMENT TO SERVE AS THE PROPOSED CURB RAMP.

THE ALLOWABLE DETECTABLE WARNING DEVICE SHALL BE IN ACCORDANCE WITH THE OHIO DEPARTMENT OF TRANSPORTATION'S CURRENT QUALIFIED PRODUCTS LIST (QPL) AND SHALL BE APPROVED BY THE CITY OF COLUMBIANA AND THE CONSTRUCTION ENGINEER.

ALL DETECTABLE WARNING DEVICES SHALL BE IDENTICAL (I.E. NO MIXING AND MATCHING OF TYPES, COLORS, ETC.), SHALL MATCH THE DETECTABLE WARNING DEVICES BEING USED FOR THE FULLY CONSTRUCTED CURB RAMPS, AND SHALL MATCH THE EXISTING DETECTABLE WARNING DEVICES PREVIOUSLY USED ALONG LISBON STREET.

THE CONTRACTOR SHALL INCLUDE ALL OF THE NECESSARY INCIDENTALS FOR THE COMPLETE INSTALLATION OF THE DETECTABLE WARNING DEVICE.

ALL WORK SHALL BE COORDINATED WITH THE CONSTRUCTION ENGINEER AND THE CITY OF COLUMBIANA. ALL OF THE AFOREMENTIONED SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 608: DETECTABLE WARNING, AS PER PLAN.

ITEM 638 - VALVE BOX ADJUSTED TO GRADE, AS PER PLAN

QUANTITIES FOR THE ADJUSTMENT TO GRADE OF WATER AND GAS VALVE BOXES ARE SHOWN ON THE PLAN VIEW SHEETS. IN ADDITION, A QUANTITY HAS BEEN ADDED FOR USE BY THE CONTRACTOR TO RAISE OR LOWER ANY WATER OR GAS VALVE BOXES ENCOUNTERED AS A RESULT OF THE PLANING OPERATIONS THAT ARE NOT SHOWN IN THE PLANS AND DEEMED BY THE ENGINEER TO NEED ADJUSTING. THE ADJUSTMENT TO GRADE OF ALL GAS VALVE BOXES SHALL BE COORDINATED WITH THE GAS COMPANY.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

- ITEM 638 VALVE BOX ADJUSTED TO GRADE, A.P.P. 2 EACH

ALL LABOR AND NECESSARY ITEMS NEEDED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 638 - VALVE BOX ADJUSTED TO GRADE, AS PER PLAN.

CALC. BY: J.F.B.
DATE: 10/23
CHKD. BY: D.G.D.
DATE: 10/23

GENERAL NOTES

COL - FAIRFIELD SCH. RD. & LISBON ST.

ITEM 690 SPECIAL – MISCELLANEOUS METAL

DURING THE ADJUSTMENT TO GRADE OF THE VARIOUS UTILITY STRUCTURES, EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE CONSTRUCTION ENGINEER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY OR LIGHT DUTY) FOR THE PARTICULAR STRUCTURE IN QUESTION. ALL MATERIAL SHALL MEET ITEM 611 OF THE SPECIFICATIONS AND SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 690 SPECIAL – MISCELLANEOUS METAL 2000 POUNDS

THE CONTRACTOR IS CAUTIONED TO USE EXTREME CARE IN THE REMOVAL, STORAGE AND REPLACEMENT OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED WITH THE PROPER NEW CASTINGS AT THE EXPENSE OF THE CONTRACTOR.

ALL UNSUITABLE OR DAMAGED CASTINGS SHALL BE CAREFULLY REMOVED AND STORED WITHIN THE RIGHT OF WAY FOR SALVAGE BY CITY OF COLUMBIANA.

REVIEW OF DRAINAGE FACILITIES

PRIOR TO THE START OF WORK AND AGAIN BEFORE FINAL ACCEPTANCE, PERFORM AN INSPECTION WITH REPRESENTATIVES OF THE DEPARTMENT, CONTRACTOR AND LOCALS OF ALL EXISTING DRAINAGE FACILITIES THAT ARE TO REMAIN IN SERVICE WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES IS DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION ARE MAINTAINED BY THE DEPARTMENT.

CONFIRM ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE-MENTIONED PARTIES ARE MAINTAINED AND LEFT IN A CONDITION COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. THE CONTRACTOR IS RESPONSIBLE TO CORRECT ANY CHANGE IN THE CONDITION RESULTING FROM THEIR OPERATIONS AS DIRECTED AND APPROVED BY THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE IS INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

ITEM 611 – CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN

RELATIVE TO EXISTING CATCH BASINS LOCATED ALONG THE ROADWAYS, EXISTING CATCH BASINS SHALL BE ADJUSTED TO GRADE AFTER PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE COURSE AND PRIOR TO PLACEMENT OF THE ASPHALT CONCRETE SURFACE COURSE. THE PAVEMENT SHALL BE SAWCUT AROUND THE EXISTING CATCH BASINS AND THE CATCH BASINS SHALL BE ADJUSTED TO MEET THE PROPOSED FINISH GRADE OF THE ASPHALT CONCRETE SURFACE COURSE. THE PAVEMENT SHALL BE EXCAVATED DOWN 10" FROM THE SURFACE OF THE NEW ASPHALT CONCRETE INTERMEDIATE COURSE (APPROXIMATELY 8" DOWN FROM THE SURFACE OF THE EXISTING BASE) AND SHALL BE FILLED WITH 10" OF CLASS QC MS CONCRETE.

PRIOR TO PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE COURSE, THE CONTRACTOR WILL BE PERMITTED TO SAWCUT THE EXISTING PAVEMENT AND LOOSEN THE MATERIAL AROUND THE EXISTING CATCH BASIN TO SIMPLIFY EXCAVATION AFTER THE ASPHALT CONCRETE INTERMEDIATE COURSE IS PLACED. THE CONTRACTOR WILL STILL BE REQUIRED TO SAWCUT THE NEW ASPHALT CONCRETE INTERMEDIATE COURSE PRIOR TO REMOVING THE MATERIAL FOR THE ADJUSTMENT TO GRADE OF THE EXISTING CATCH BASIN.

IN ADDITION, A QUANTITY HAS BEEN ADDED FOR USE BY THE CONTRACTOR TO RAISE OR LOWER ANY CATCH BASIN ENCOUNTERED AS A RESULT OF THE PLANING OPERATIONS THAT ARE NOT SHOWN IN THE PLANS AND DEEMED BY THE CONSTRUCTION ENGINEER TO NEED ADJUSTING.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 611: CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN 2 EACH

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 611: CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN.

ITEM 611 – MANHOLE ADJUSTED TO GRADE, AS PER PLAN

RELATIVE TO EXISTING MANHOLES LOCATED ALONG THE ROADWAYS, EXISTING MANHOLES SHALL BE ADJUSTED TO GRADE AFTER PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE COURSE AND PRIOR TO PLACEMENT OF THE ASPHALT CONCRETE SURFACE COURSE. THE PAVEMENT SHALL BE SAWCUT AROUND THE EXISTING MANHOLES AND THE MANHOLES SHALL BE ADJUSTED TO MEET THE PROPOSED FINISH GRADE OF THE ASPHALT CONCRETE SURFACE COURSE. THE PAVEMENT SHALL BE EXCAVATED DOWN 10" FROM THE SURFACE OF THE NEW ASPHALT CONCRETE INTERMEDIATE COURSE (APPROXIMATELY 8" DOWN FROM THE SURFACE OF THE EXISTING BASE) AND SHALL BE FILLED WITH 10" OF CLASS QC MS CONCRETE.

PRIOR TO PLACEMENT OF THE ASPHALT CONCRETE INTERMEDIATE COURSE, THE CONTRACTOR WILL BE PERMITTED TO SAWCUT THE EXISTING PAVEMENT AND LOOSEN THE MATERIAL AROUND THE MANHOLE TO SIMPLIFY EXCAVATION AFTER THE ASPHALT CONCRETE INTERMEDIATE COURSE IS PLACED. THE CONTRACTOR WILL STILL BE REQUIRED TO SAWCUT THE NEW ASPHALT CONCRETE INTERMEDIATE COURSE PRIOR TO REMOVING THE MATERIAL FOR THE ADJUSTMENT TO GRADE OF THE EXISTING MANHOLE.

THE CONTRACTOR SHALL COORDINATE THE ADJUSTMENT TO GRADE OF ANY TELEPHONE MANHOLES WITH THE TELEPHONE COMPANY.

IN ADDITION, A QUANTITY HAS BEEN ADDED FOR USE BY THE CONTRACTOR TO RAISE OR LOWER ANY MANHOLE ENCOUNTERED AS A RESULT OF THE PLANING OPERATIONS THAT ARE NOT SHOWN IN THE PLANS AND DEEMED BY THE CONSTRUCTION ENGINEER TO NEED ADJUSTING.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 611: MANHOLE ADJUSTED TO GRADE, AS PER PLAN 2 EACH

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 611: MANHOLE ADJUSTED TO GRADE, AS PER PLAN.

ITEM 611 – INLET RECONSTRUCTED TO GRADE, AS PER PLAN (STA. 60+19, LISBON STREET)

THE FOLLOWING IS RELATIVE TO THE EXISTING CURB INLET LOCATED AT STA. 60+19 ALONG THE LEFT SIDE OF LISBON STREET, AND MORE SPECIFICALLY TO THE EXISTING CURB INLET TOP.

THE EXISTING CURB INLET TOP IS A HALF MOON WITH A STEEL FRAME BOTTOM PLATE, STEEL FRAME FRONT PLATE, AND STEEL FRAME CELLS WITH CONCRETE POURED WITHIN THE CELLS TO MAKE UP THE TOP SURFACE. APPROXIMATE DIMENSIONS ARE 66" ACROSS THE FRONT/CURB INLET OPENING, AND 28" RADIUS (FROM FRONT TO BACK).

WHEN VIEWING THE CURB INLET FROM THE ROADWAY THE LEFT SIDE, FRONT CORNER OF THE CATCH BASIN IS DAMAGED (I.E. MISSING A PORTION OF THE STEEL FRAME BOTTOM PLATE, FRONT PLATE, CELL).

THE CONTRACTOR WILL BE REQUIRED TO CAREFULLY REMOVE THE ENTIRE CATCH BASIN TOP; REPAIR THE AFOREMENTIONED PORTION OF THE MISSING STEEL FRAME; AND PLACE NEW CONCRETE IN THE NEWLY FORMED CELL.

- THE DIMENSIONS OF THE FRAME REPAIR SHALL CLOSELY MATCH THE DIMENSIONS OF THE UNDAMAGED, RIGHT SIDE OF THE FRAME.
- THE STEEL USED TO REPAIR THE FRAME SHALL MATCH THE THICKNESS, MATERIAL TYPE, ETC. OF THE EXISTING FRAME.
- THE STEEL USED SHALL BE CUT/TORCHED TO THE APPROXIMATE DIMENSIONS NEEDED FOR THE REPAIR AND SHALL BE WELDED TO THE EXISTING STEEL FRAME, FULL PENETRATION. ALL SURFACES SHALL BE PROPERLY PREPARED & CLEANED.
- THE CONCRETE USED TO FILL IN THE CELL SHALL BE 4,000 PSI CONCRETE; BAG MIX IS ACCEPTABLE.

PRIOR TO REINSTALLING THE CURB INLET TOP BACK ONTO THE INLET STRUCTURE, THE CONTRACTOR SHALL MAKE ANY NECESSARY REPAIRS TO THE TOP OF THE CURB INLET STRUCTURE IN ORDER TO ALLOW FOR THE PROPER, SOUND PLACEMENT OF THE CURB INLET TOP, & TO ENSURE THAT THE TOP SURFACE OF THE CURB INLET TOP MATCHES THE ADJACENT EXISTING TOP OF CURB; AND THEN RE-INSTALL THE CURB INLET TOP. THE CONTRACTOR SHALL PROVIDE ANY NECESSARY REMOVAL WORK, BRICK, MORTAR, CONCRETE, ETC. TO PROPERLY REPAIR THE CURB INLET STRUCTURE AND TO PROPERLY RESET THE CURB INLET TOP. PROVIDE ANY NECESSARY SEEDING & MULCHING.

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 611: INLET RECONSTRUCTED TO GRADE, AS PER PLAN (STA. 60+19, LISBON STREET).

ITEM 611 – INLET RECONSTRUCTED TO GRADE, AS PER PLAN (STA. 59+83, LISBON STREET)

THE FOLLOWING IS RELATIVE TO THE EXISTING CURB INLET LOCATED AT STA. 59+83 ALONG THE RIGHT SIDE OF LISBON STREET, AND MORE SPECIFICALLY TO THE EXISTING CURB INLET TOP.

THE EXISTING CURB INLET TOP IS IN ACCEPTABLE CONDITION FOR REUSE; HOWEVER, THE CURB INLET TOP IS NOT PROPERLY SET ONTO THE EXISTING CURB INLET STRUCTURE.

THE CONTRACTOR WILL BE REQUIRED TO CAREFULLY REMOVE THE ENTIRE CATCH BASIN TOP; MAKE ANY NECESSARY REPAIRS TO THE TOP OF THE CURB INLET STRUCTURE IN ORDER TO ALLOW FOR THE PROPER, SOUND PLACEMENT OF THE CURB INLET TOP, & TO ENSURE THAT THE TOP SURFACE OF THE CURB INLET TOP MATCHES THE ADJACENT EXISTING TOP OF CURB; AND THEN RE-INSTALL THE CURB INLET TOP. THE CONTRACTOR SHALL PROVIDE ANY NECESSARY REMOVAL WORK, BRICK, MORTAR, CONCRETE, ETC. TO PROPERLY REPAIR THE CURB INLET STRUCTURE AND TO PROPERLY RESET THE CURB INLET TOP. PROVIDE ANY NECESSARY SEEDING & MULCHING.

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 611: INLET RECONSTRUCTED TO GRADE, AS PER PLAN (STA. 59+83, LISBON STREET).

SEEDING AND MULCHING

THE FOLLOWING ITEMS ARE TO BE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

- ITEM 659 TOPSOIL
- ITEM 659 SEEDING AND MULCHING, CLASS 1
- ITEM 659 COMMERCIAL FERTILIZER
- ITEM 659 LIME
- ITEM 659 WATER

NOTE: ALL SEED MIXES SHALL BE "CLASS 1" FOR RESIDENTIAL LAWNS.

THE AFOREMENTIONED SHALL BE APPLIED TO ALL AREAS OF LAWNS OR GRASS AREAS DISTURBED DURING THE CURB RAMP, SIDEWALK, CURB CONSTRUCTION, AND/OR INLETS RECONSTRUCTED TO GRADE, AND ARE TO BE INCLUDED IN THE VARIOUS UNIT PRICES BID FOR THE AFOREMENTIONED ITEMS.

ANY ADDITIONAL QUANTITIES FOR SEEDING AND MULCHING THAT ARE NOT TO BE INCLUDED IN THE AFOREMENTIONED ITEMS ARE SEPARATELY ITEMIZED IN THE PLANS.

GRADED SHOULDERS (FAIRFIELD SCHOOL ROAD)

SINCE THE PROPOSED ASPHALT PAVEMENT BUILDUP ALONG FAIRFIELD SCHOOL ROAD WILL BE APPROXIMATELY 1-1/2" HIGHER UPON COMPLETION OF THE PLACEMENT OF THE ASPHALT SURFACE COURSE (I.E. 1-1/2" PAVEMENT PLANING WITH 3" ASPHALT OVERLAY), SOME AREAS OF THE EXISTING GRADED SHOULDER MAY REQUIRE THE PLACEMENT OF TOPSOIL IN ORDER TO MEET THE FINISHED SURFACE OF THE ASPHALT PAVEMENT. TOPSOIL SHALL BE PLACED AT A MAXIMUM WIDTH OF (2) FEET AND AT AN APPROXIMATE THICKNESS OF 1-1/2".

- THE PLACEMENT OF TOPSOIL SHALL BE COORDINATED WITH ALL AREAS REQUIRING LINEAR GRADING, AND VICE VERSA.
- SEEDING & MULCHING, COMMERCIAL FERTILIZER, LIME, AND WATER SHALL BE APPLIED TO ALL AREAS WHERE TOPSOIL IS PLACED.
- NOTE: QUANTITIES FOR SEEDING & MULCHING, COMMERCIAL FERTILIZER, LIME, AND WATER IN AREAS REQUIRING LINEAR GRADING ARE ALREADY INCLUDED IN THE SUB-SUMMARIES ON THE VARIOUS PLAN SHEETS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

- ITEM 659 TOPSOIL 80 C.Y.
- ITEM 659 SEEDING AND MULCHING, CLASS 1 1,500 S.Y.
- ITEM 659 COMMERCIAL FERTILIZER 0.20 TON
- ITEM 659 LIME 0.31 ACRE
- ITEM 659 WATER 12.15 MGAL

WATERS OF THE U.S. PLAN NOTE:

WATERS OF THE US HAVE BEEN IDENTIFIED WITHIN THE PROJECT AREA. THE CONTRACTOR SHALL EXERCISE CAUTION TO ENSURE THAT NO IMPACTS OCCUR TO WATERS OF THE U.S. NO TEMPORARY OR PERMANENT FILL OF ANY TYPE MAY BE PLACED IN ANY STREAM OR WETLAND AS PART OF THIS PROJECT. ANY ACTIVITIES OCCURRING IN STREAMS OR WETLANDS WOULD REQUIRE PERMITS FROM THE U.S. ARMY CORPS OF ENGINEERS AND/OR THE OHIO EPA.

ANY OTHER SITE PROPOSED BY THE CONTRACTOR FOR OFF PROJECT ANCILLARY CONSTRUCTION (STAGING AREAS, WASTE LOCATIONS, AND/OR BORROW LOCATIONS) MUST MEET THE REQUIREMENTS OF CMS 105.16.

CHECKED BY: J.F.B. DATE: 10/23
DRAWN BY: D.G.D. DATE: 10/23

GENERAL NOTES

COL – FAIRFIELD SCH. RD. & LISBON ST.

ITEM 614--MAINTAINING TRAFFIC

SEVEN (7) DAYS PRIOR TO THE COMMENCEMENT OF THE PROJECT, THE CONTRACTOR SHALL CONTACT IN WRITING THE CITY OF COLUMBIANA POLICE DEPARTMENT, THE CITY MANAGER, THE COLUMBIANA COUNTY SHERIFF'S DEPARTMENT, AND THE COLUMBIANA COUNTY ENGINEER'S OFFICE SO THEY CAN INFORM THE LOCAL TRAVELING PUBLIC OF THE IMPENDING WORK, LANE CLOSURES, ETC.

M.O.T. NOTES FOR ALL CONSTRUCTION ACTIVITIES (EXCEPT FOR THE (5' & 7') WIDE PAVEMENT REMOVAL AND REPLACEMENT & PERTINENT STORM SEWER IMPROVEMENTS ALONG THE RIGHT SIDE OF FAIRFIELD SCHOOL ROAD):

DURING ROADWAY IMPROVEMENTS ALONG THE LENGTH OF THE PROJECT, THE CONTRACTOR SHALL ZONE OFF THE AMOUNT OF WORK THAT CAN BE COMPLETED IN ONE DAYS TIME.

REFLECTIVE DRUMS EQUIPPED WITH FLASHING WARNING LIGHTS SHALL BE UTILIZED DURING CONSTRUCTION.

THE CONTRACTOR IS PERMITTED TO PERFORM WORK SIMULTANEOUSLY IN DIFFERENT WORK ZONES (SEE ADDITIONAL NOTES BELOW). A MINIMUM OF ONE LANE TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES BY THE USE OF THE EXISTING PAVEMENT. AFTER EACH DAYS WORK, THE REFLECTIVE DRUMS USED FOR M.O.T. SHALL BE REMOVED FROM THE ROADWAY, AND TRAFFIC SHALL BE PERMITTED ON THE PREVIOUSLY CLOSED LANE.

THE CONTRACTOR SHALL PROVIDE FLAGGERS DURING PAVEMENT PLANING OPERATIONS, FULL DEPTH PAVEMENT REMOVAL & RIGID REPLACEMENT, ASPHALT PAVING OPERATIONS, AND DURING ALL OTHER CONSTRUCTION ACTIVITIES IN ORDER TO MAINTAIN TRAFFIC INTO AND OUT OF EXISTING DRIVEWAYS, SIDE STREETS, ALLEYS, AND ALONG THE ROADWAY CORRIDORS WITHIN THE PROJECT LIMITS.

-NOTE: THE CONTRACTOR MAY UTILIZE LAW ENFORCEMENT OFFICERS (LEO'S) AS FLAGGERS, BUT ALL COSTS WILL BE AT THE CONTRACTOR'S EXPENSE.

IMMEDIATELY UPON COMPLETION OF PAVEMENT PLANING OPERATIONS AROUND ANY MANHOLES, CATCH BASINS, VALVE BOXES, OR MONUMENT BOXES, THE CONTRACTOR SHALL PROVIDE TEMPORARY "FIRM AND UNYIELDING MATERIAL" AROUND THE AFOREMENTIONED.

FOR THE ADJUSTMENT TO GRADE OF ALL MANHOLES, CATCH BASINS, AND VALVE BOXES, THE CONTRACTOR SHALL REFER TO THE SPECIFIC NOTES IN THE GENERAL NOTES.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE CONSTRUCTION ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE LOCAL TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE CONSTRUCTION ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

DURING CURB RAMP INSTALLATION, CURB REMOVAL & REPLACEMENT, AND/OR SIDEWALK REMOVAL & REPLACEMENT, THE CONTRACTOR SHALL PROVIDE THE NECESSARY BARRICADES AND PEDESTRIAN DETOURS IN ACCORDANCE WITH THE PERTINENT ODOT STANDARD CONSTRUCTION DRAWINGS AND ODOTCD SPECIFICATIONS.

M.O.T. NOTES FOR CONSTRUCTION ACTIVITIES RELATED TO THE (5' & 7') WIDE PAVEMENT REMOVAL & REPLACEMENT ALONG THE RIGHT SIDE OF FAIRFIELD SCHOOL ROAD AND FOR THE DRAINAGE IMPROVEMENTS, LINEAR GRADING, ETC. ALONG THE RIGHT SIDE OF FAIRFIELD SCHOOL ROAD:

-SEE SHEETS 16-19 FOR NOTES & DETAILS OF THE M.O.T. & DETOURS RELATED TO THIS WORK.

-THERE SHALL BE TWO (2) WORK ZONES ASSOCIATED WITH THIS WORK.

-"WORK ZONE 1" SHALL BE FROM METZ ROAD AND SOUTH TO THE CORPORATION LIMITS (STA. 0+90 TO STA. 27+00).

-"WORK ZONE 2" SHALL BE FROM METZ ROAD AND NORTH TO FAIRFIELD AVENUE (STA. 27+50 TO STA. 44+50).

-M.O.T. WILL NOT BE PERMITTED TO BE SET UP, AND WORK WILL NOT BE PERMITTED TO BE PERFORMED SIMULTANEOUSLY WITHIN THE TWO WORK ZONES (I.E. M.O.T. SHALL BE SET UP FOR "WORK ZONE 1"; WORK SHALL BE PERFORMED FOR "WORK ZONE 1"; M.O.T. SHALL BE REMOVED FOR "WORK ZONE 1"; AND THEN M.O.T SET UP, WORK, AND M.O.T. REMOVAL FOR "WORK ZONE 2" SHALL OCCUR, AND/OR VICE VERSA).

-EACH WORK ZONE SHALL BE SET UP AS ONE-WAY TRAFFIC ALONG WITH THE CORRESPONDING DETOURS AS FURTHER DETAILED ON SHEETS 16-19.

-REFLECTIVE DRUMS WITH FLASHING WARNING LIGHTS SHALL BE UTILIZED ALONG THE LENGTH OF THE WORK ZONE IN ACCORDANCE WITH THE DROP-OFF REQUIREMENTS DEFINED IN ODOT STANDARD CONSTRUCTION DRAWING MT-101.90 AND AS FURTHER DETAILED ON SHEETS 16-19.

-PRIOR TO THE REMOVAL OF THE M.O.T. FOR EITHER WORK ZONE, THE FOLLOWING SHALL BE PERFORMED, AT A MINIMUM: PERFORM THE 1-1/2" PAVEMENT PLANING TO THE REMAINING (3' OR 5') WIDTH OF EXISTING ASPHALT PAVEMENT (I.E. FROM THE EDGE OF TRENCH TO THE CENTERLINE CROWN) WITHIN THE RIGHT SIDE (NORTHBOUND LANE), AND PLACE THE 1-3/4" OF ASPHALT CONCRETE INTERMEDIATE COURSE WITHIN THE ENTIRE WIDTH OF THE RIGHT SIDE LANE. THIS WILL PREVENT HAVING 1-1/2" DROP-OFF IN THE MIDDLE OF THE LANE, AND WILL PREVENT HAVING A CONSTRUCTION JOINT FOR THE 301 & INTERMEDIATE AT THE SAME LOCATION.

(M.O.T. NOTES CONTINUED IN NEXT COLUMN)

ITEM 614--MAINTAINING TRAFFIC (CONT'D)

SPECIAL NOTES FOR ASPHALT PAVEMENT PLANING:
SEE SPECIAL NOTES FOR ASPHALT PAVEMENT PLANING IN THE TYPICAL SECTIONS ON SHEETS 4-6 FOR BOTH FAIRFIELD SCHOOL ROAD AND LISBON STREET.

M.O.T. NOTES FOR ALL CONSTRUCTION ACTIVITIES:

THE CONTRACTOR SHALL DESIGNATE A QUALIFIED INDIVIDUAL, OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE CONSTRUCTION ENGINEER, TO PERIODICALLY INSPECT, DOCUMENT, REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL INSPECT, DOCUMENT, REPAIR AND/OR REPLACE ALL TRAFFIC CONTROL DEVICES (IF REQUIRED) AT THE BEGINNING AND END OF EACH WORK DAY, AND AT LEAST ONCE DURING THE DAY WHILE WORK IS BEING CONDUCTED ON THE PROJECT.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ODOT C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, THE PERTINENT ODOT STANDARD CONSTRUCTION DRAWINGS, AND THE PERTINENT SECTIONS OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS AS DESCRIBED IN THE NOTES ABOVE, IN THE M.O.T. DETAILS ON SHEETS 14-19, AND IN ODOT C&MS ITEM 614 SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614--MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLANS.

MAINTAINING PAVEMENT SURFACE

THE FOLLOWING WILL BE REQUIRED DURING FULL DEPTH REMOVAL AND REPLACEMENT OPERATIONS (RIGID OR FLEXIBLE) AND DURING ANY NECESSARY PAVEMENT REMOVAL FOR SIDE STREET APPROACHES/DRIVEWAYS.

ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS, ALLEYS, SIDE STREETS, AND THE ROADWAY PAVEMENTS SHALL BE MAINTAINED AT ALL TIMES UNTIL THE PAVEMENT REPLACEMENT HAS BEEN COMPLETED BY THE USE OF TEMPORARY ITEM 304: AGGREGATE BASE, AS PER PLAN.

THE THICKNESS OF THE TEMPORARY 304 SHALL COINCIDE WITH THE THICKNESS OF PAVEMENT REMOVAL AND REPLACEMENT BEING PERFORMED AS FURTHER DEFINED IN THE TYPICAL SECTIONS AND AS FURTHER DESCRIBED BELOW:

-I.E. 9" THICK OF TEMPORARY 304 FOR THE (5' & 7') WIDE PAVEMENT REMOVAL WITH FLEXIBLE REPLACEMENT ALONG THE RIGHT SIDE OF FAIRFIELD SCHOOL ROAD (6" OF ITEM 304--AGGREGATE BASE, AS PER PLAN, AND 3" OF ITEM 301--ASPHALT CONCRETE BASE).

-I.E. 8" THICK OF TEMPORARY 304 IN AREAS REQUIRING FULL DEPTH REMOVAL WITH RIGID REPLACEMENT AND/OR PAVEMENT REPAIR FOR STORM SEWER TRENCHES (8" OF ITEM 305--CONCRETE BASE).

-I.E. 9" THICK OF TEMPORARY 304 IN AREAS REQUIRING DRIVEWAY APPROACH WORK. (6" OF ITEM 304--AGGREGATE BASE, AS PER PLAN, AND 3" OF ASPHALT SURFACE/INTERMEDIATE COURSES).

THE COSTS ASSOCIATED WITH THE AFOREMENTIONED 304 ITEM SHALL ALSO INCLUDE THE REMOVAL & DISPOSAL OF THE ITEM.

STEEL PLATES MAY ALSO BE USED BUT SHALL ONLY BE USED WHEN APPROVED BY THE CONSTRUCTION ENGINEER. THE USE OF STEEL PLATES WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614: MAINTAINING TRAFFIC, MISC.: TEMPORARY 304 AGGREGATE BASE.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 614 MAINTAINING TRAFFIC, MISC.: TEMPORARY 304 AGGREGATE BASE 200 C.Y.

SEQUENCE OF OPERATIONS

THE CONTRACTOR SHALL SUBMIT, IN WRITING, A DETAILED SCHEDULE TO THE CONSTRUCTION ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT. THE DETAILED SCHEDULE SHALL INCLUDE TASKS WITH ANTICIPATED COMMENCEMENT & COMPLETION DATES THAT RELATE TO THE VARIOUS BID ITEMS FOR THE PROJECT AS LISTED IN THE GENERAL SUMMARY. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE CITY OFFICIALS AND AGENCIES AS DETAILED ON THIS SHEET.

ALTERNATE METHODS

IF THE CONTRACTOR SO ELECTS, HE/SHE MAY SUBMIT ALTERNATE METHODS FOR THE MAINTENANCE OF TRAFFIC AND/OR SEQUENCE OF OPERATIONS, PROVIDED THAT THE MAINTAINING TRAFFIC PROVISIONS ARE FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THEREFROM. NO ALTERNATE PLAN WILL BE PUT INTO EFFECT UNTIL THE PROPER APPROVAL HAS BEEN GRANTED, IN WRITING, BY THE CITY OF COLUMBIANA AND THE CONSTRUCTION ENGINEER.

CONTRACTOR'S EQUIPMENT -- OPERATION AND STORAGE

IN ADDITION TO THE REQUIREMENTS OF ITEM 614 OF THE CONSTRUCTION & MATERIALS SPECIFICATION, THE FOLLOWING SHALL APPLY: THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL; A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM; THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE EQUIPPED WITH AT LEAST ONE AMBER FLASHING LIGHT.

AFTER EACH WORK DAY, ON WEEKENDS, OR AT OTHER TIMES OF SUSPENSION OF WORK, THE EQUIPMENT SHALL BE STORED AT A STORAGE AREA REMOVED FROM THE RIGHT-OF-WAY, UNLESS OTHERWISE NOTED IN THE PLANS. THE LOCATION SHALL HAVE PRIOR APPROVAL OF THE CONSTRUCTION ENGINEER AND THE COLUMBIANA CITY MANAGER.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER DURING CONSTRUCTION FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING CONTINGENCY QUANTITY HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 WATER 10 M GAL.

WORK ZONE MARKINGS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS PER THE REQUIREMENTS OF CMS 614.04 AND 614.11.

ITEM 614 WORK ZONE CENTERLINE, CLASS 1, 642 PAINT 2.30 MILE
ITEM 614 WORK ZONE CENTERLINE, CLASS 1, 740.06, TYPE 1 1.15 MILE

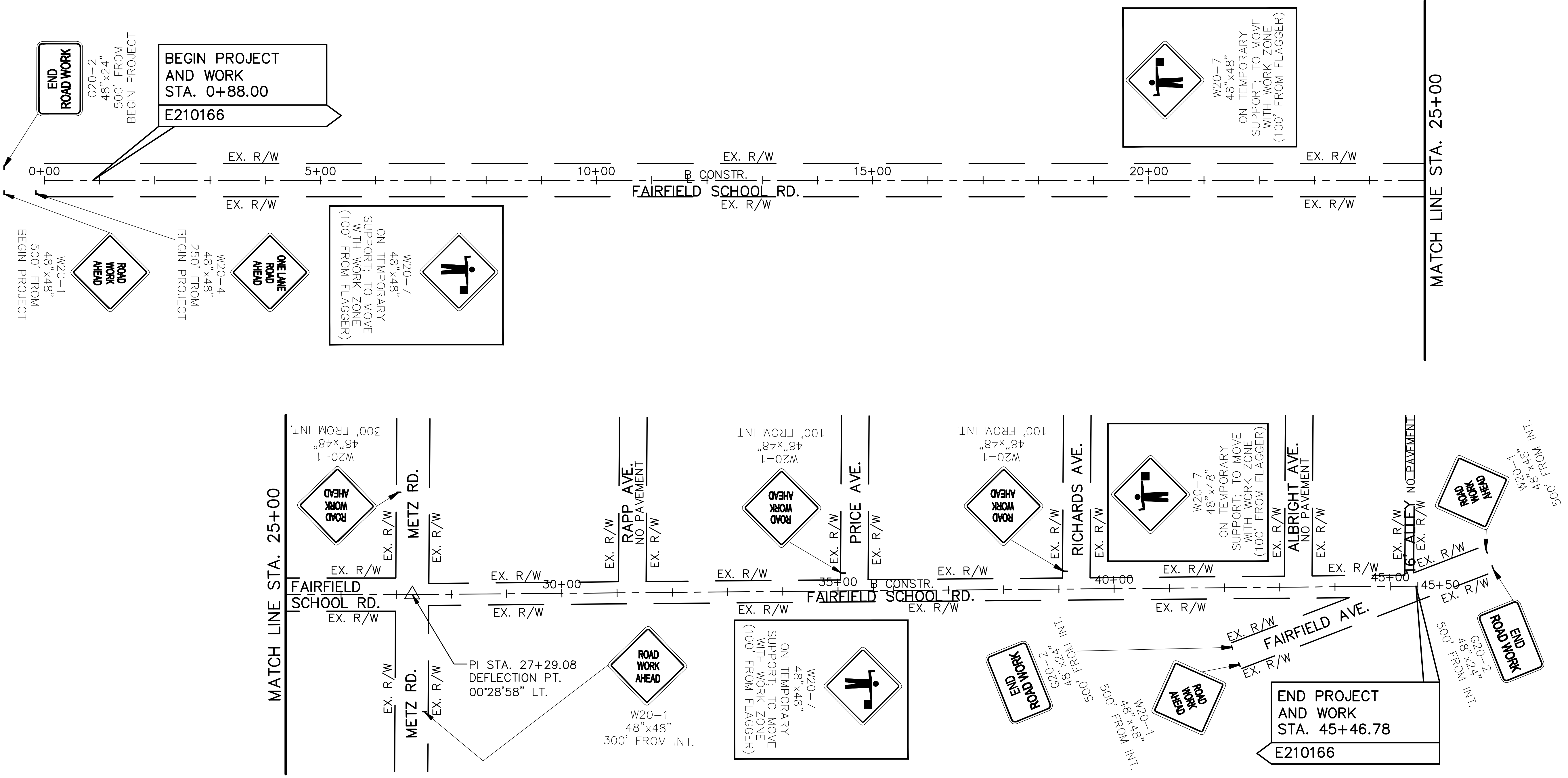
NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE CONSTRUCTION ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS (HTTPS://WWW.TRANSPORTATION.OHIO.GOV/WPS/PORTAL/GOV/ODOT/WORKING/PERMITS/SPECIAL-HAULING-PERMITS) SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE CONSTRUCTION ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	≥ 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	≤ 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	≥ 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.



SCHEMATIC M.O.T. NOTES

SEE SHEET 13 FOR MAINTENANCE OF TRAFFIC NOTES.

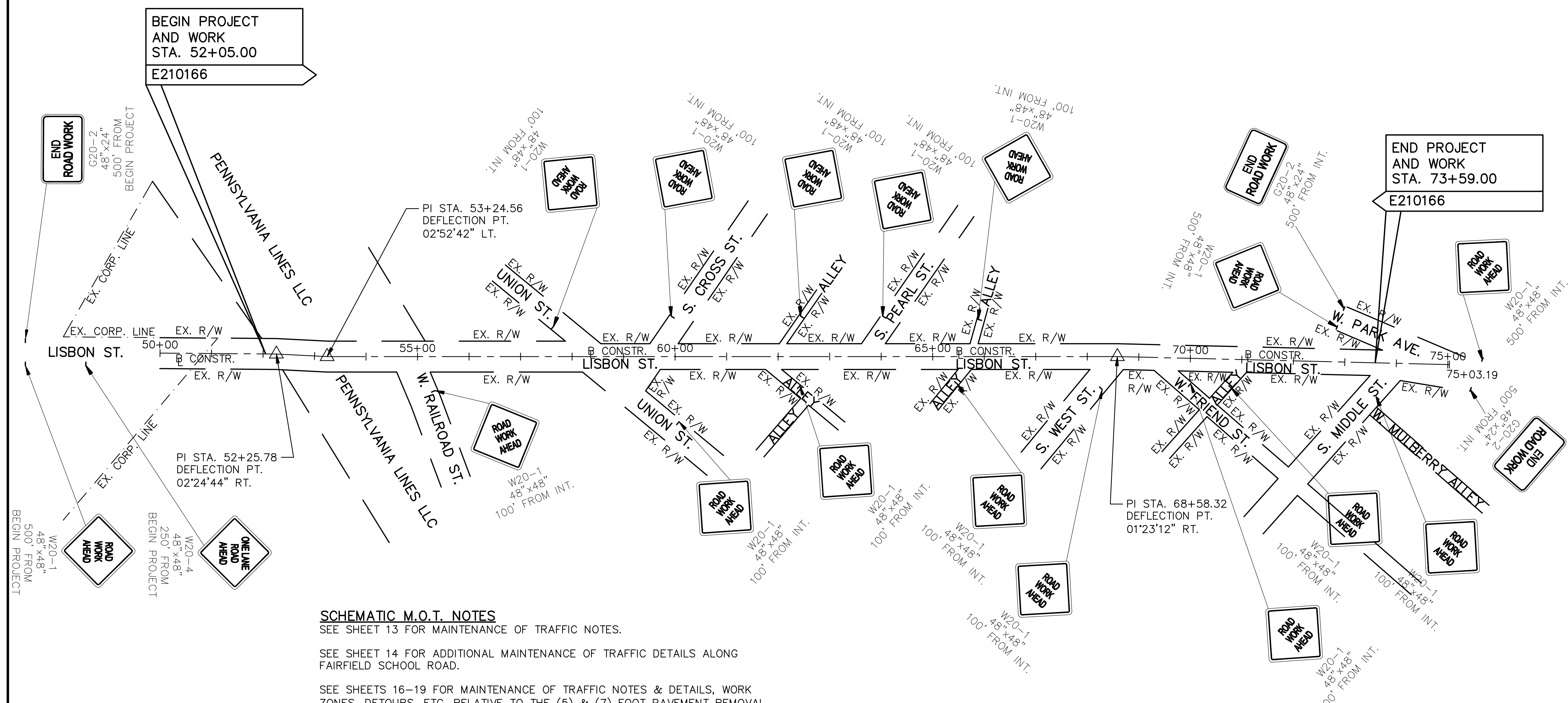
SEE SHEET 15 FOR MAINTENANCE OF TRAFFIC DETAILS ALONG LISBON STREET.

SEE SHEETS 16-19 FOR MAINTENANCE OF TRAFFIC NOTES & DETAILS, WORK ZONES, DETOURS, ETC. RELATIVE TO THE (5) & (7) FOOT PAVEMENT REMOVAL AND REPLACEMENT ALONG THE RIGHT SIDE OF FAIRFIELD SCHOOL ROAD.

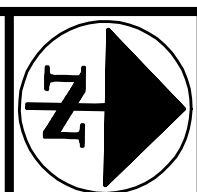
ALL "ROAD WORK AHEAD", "END ROAD WORK", AND "ONE LANE ROAD AHEAD" SIGNS SHALL BE INSTALLED ON GROUND MOUNTED SUPPORTS (TEMPORARY SIGN SUPPORTS WILL NOT BE PERMITTED), AND SHALL REMAIN FOR THE DURATION OF THE PROJECT.

FLAGGER AHEAD SIGNS SHALL BE INSTALLED ON TEMPORARY SUPPORTS, AND SHALL MOVE WITH THE PERTINENT WORK ZONE.

THE PERTINENT MAINTENANCE OF TRAFFIC ODOT STANDARD CONSTRUCTION DRAWINGS LISTED IN THE TITLE SHEET AND GIVEN AT THE END OF THE CONSTRUCTION PLAN SET SHALL BE UTILIZED FOR THE SPECIFIC WORK BEING PERFORMED (I.E. STATIONARY OPERATIONS, PAVING OPERATIONS, LONG LINE PAVEMENT MARKINGS, ETC.).



SCHMATIC M.O.T. NOTES
 SEE SHEET 13 FOR MAINTENANCE OF TRAFFIC NOTES.
 SEE SHEET 14 FOR ADDITIONAL MAINTENANCE OF TRAFFIC DETAILS ALONG FAIRFIELD SCHOOL ROAD.
 SEE SHEETS 16-19 FOR MAINTENANCE OF TRAFFIC NOTES & DETAILS, WORK ZONES, DETOURS, ETC. RELATIVE TO THE (5) & (7) FOOT PAVEMENT REMOVAL AND REPLACEMENT ALONG THE RIGHT SIDE OF FAIRFIELD SCHOOL ROAD.
 ALL "ROAD WORK AHEAD", "END ROAD WORK", AND "ONE LANE ROAD AHEAD" SIGNS SHALL BE INSTALLED ON GROUND MOUNTED SUPPORTS (TEMPORARY SIGN SUPPORTS WILL NOT BE PERMITTED), AND SHALL REMAIN FOR THE DURATION OF THE PROJECT.
 FLAGGER AHEAD SIGNS SHALL BE INSTALLED ON TEMPORARY SUPPORTS, AND SHALL MOVE WITH THE PERTINENT WORK ZONE.
 THE PERTINENT MAINTENANCE OF TRAFFIC ODOT STANDARD CONSTRUCTION DRAWINGS LISTED IN THE TITLE SHEET AND GIVEN AT THE END OF THE CONSTRUCTION PLAN SET SHALL BE UTILIZED FOR THE SPECIFIC WORK BEING PERFORMED (I.E. STATIONARY OPERATIONS, PAVING OPERATIONS, LONG LINE PAVEMENT MARKINGS, ETC.).



CHK: J.F.B. DATE: 10/23
BY: D.G.D. DATE: 10/23

MAINTENANCE OF TRAFFIC SCHEMATIC PLAN
FAIRFIELD SCHOOL ROAD - WORK ZONE 1

COL - FAIRFIELD SCH. RD. & LISBON ST.

"WORK ZONE 1" NOTES

THE CONTRACTOR SHALL ERECT, MAINTAIN, AND REMOVE ALL MOT SIGNING, SUPPORTS, ETC.

THE CONTRACTOR SHALL PROVIDE THE NECESSARY SIGN POST SUPPORTS FOR ALL MOT SIGNING (TEMPORARY SIGN SUPPORTS WILL NOT BE PERMITTED).

ONE-WAY SIGNS SHALL BE PLACED AT ALL DRIVEWAYS AND SIDE STREETS WITHIN THE WORK ZONE LIMITS AS SHOWN IN THE SCHEMATIC ON THIS SHEET.

ROAD CLOSED SIGNING SHALL BE PROVIDED WITH TYPE III BARRICADES.

SEE SHEET 17 FOR DETOUR SIGNING FOR "WORK ZONE 1".

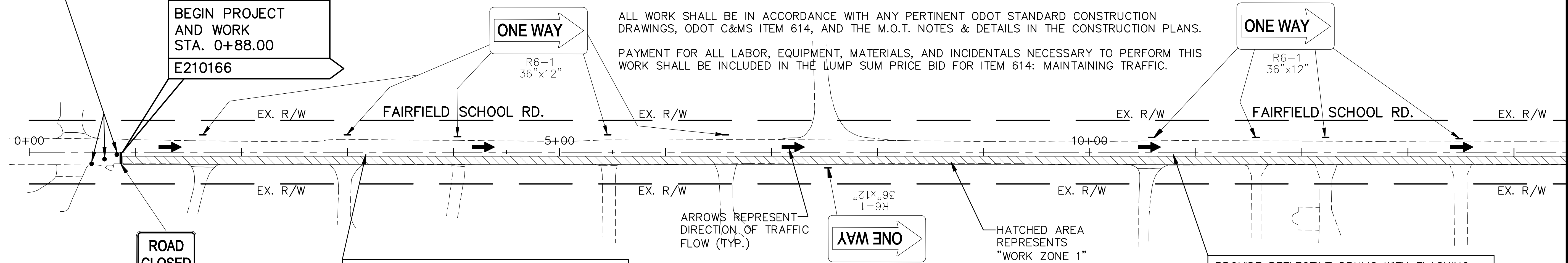
SEE SHEET 13 FOR ADDITIONAL M.O.T. NOTES.

ALL WORK SHALL BE IN ACCORDANCE WITH ANY PERTINENT ODOT STANDARD CONSTRUCTION DRAWINGS, ODOT C&MS ITEM 614, AND THE M.O.T. NOTES & DETAILS IN THE CONSTRUCTION PLANS.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614: MAINTAINING TRAFFIC.

PROVIDE REFLECTIVE DRUMS WITH FLASHING WARNING LIGHTS ON TAPER TO DRIVEWAY, WITH DRUMS SPACED AT (10) FEET ON CENTER.

BEGIN PROJECT AND WORK STA. 0+88.00 E210166



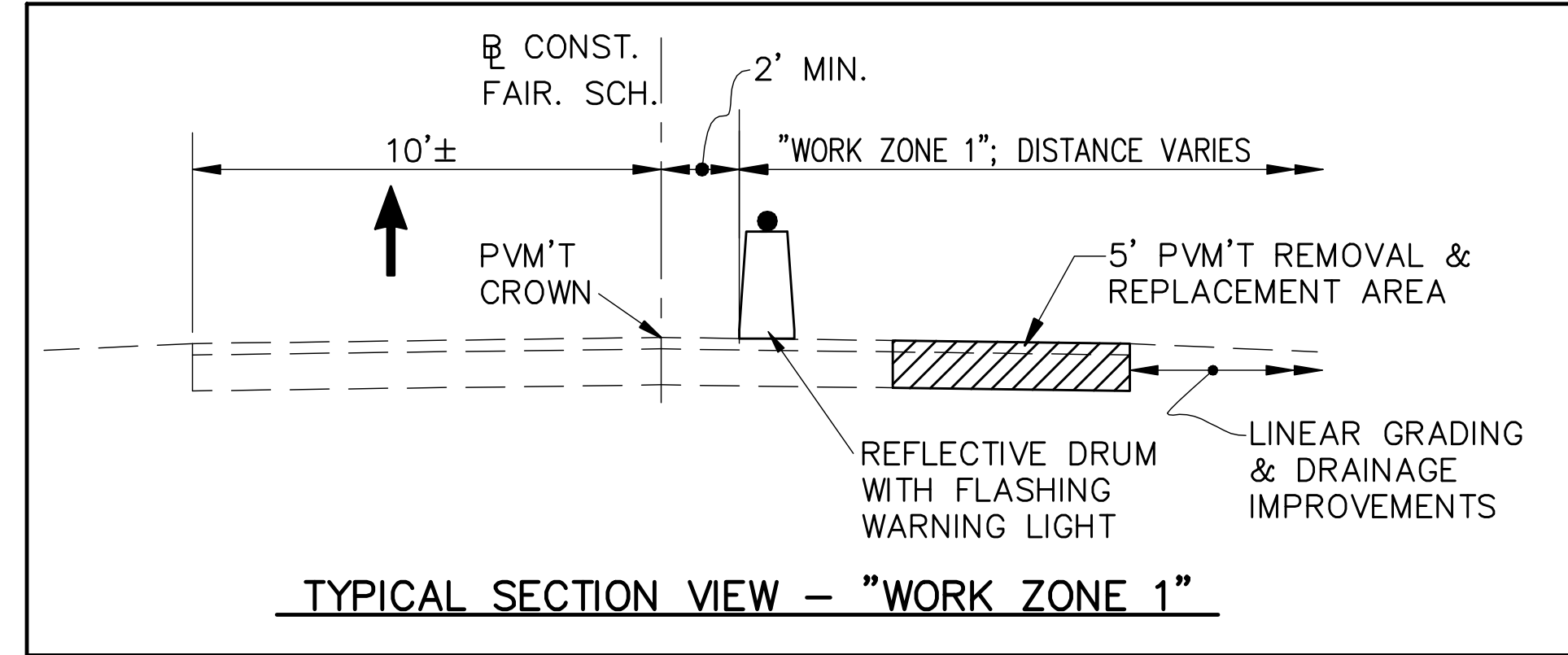
ROAD CLOSED
R11-2

ROAD CLOSED SIGNING SHALL ALSO INCLUDE TYPE III BARRICADES AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH ODOT STANDARD DRAWING 101.60; KEEP ONE LANE OPEN TO TRAFFIC

PROVIDE REFLECTIVE DRUMS WITH FLASHING WARNING LIGHTS SPACED AT (40) FEET ON CENTER FOR ENTIRE WORK ZONE.
DRUMS ARE TO BE PLACED TO ALLOW FOR A SINGLE TRAVEL LANE.
PROVIDE SUFFICIENT GAPS BETWEEN DRUMS AT ALL DRIVEWAYS.

ARROWS REPRESENT DIRECTION OF TRAFFIC FLOW (TYP.)

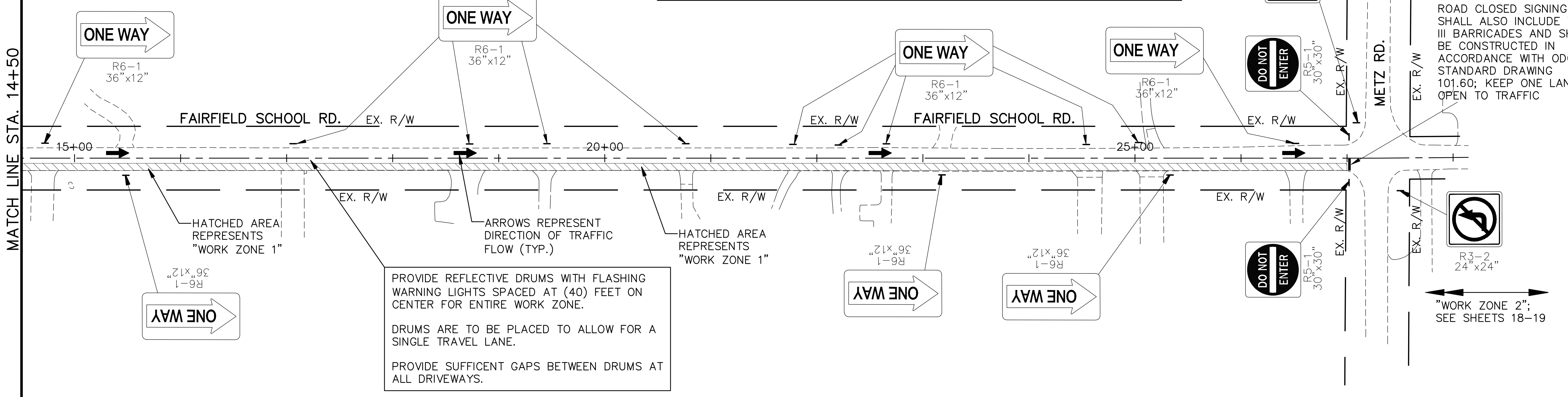
HATCHED AREA REPRESENTS "WORK ZONE 1"



TYPICAL SECTION VIEW - "WORK ZONE 1"

PROVIDE REFLECTIVE DRUMS WITH FLASHING WARNING LIGHTS SPACED AT (40) FEET ON CENTER FOR ENTIRE WORK ZONE.
DRUMS ARE TO BE PLACED TO ALLOW FOR A SINGLE TRAVEL LANE.
PROVIDE SUFFICIENT GAPS BETWEEN DRUMS AT ALL DRIVEWAYS.

MATCH LINE STA. 14+50



ROAD CLOSED
R11-2

ROAD CLOSED SIGNING SHALL ALSO INCLUDE TYPE III BARRICADES AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH ODOT STANDARD DRAWING 101.60; KEEP ONE LANE OPEN TO TRAFFIC

PROVIDE REFLECTIVE DRUMS WITH FLASHING WARNING LIGHTS SPACED AT (40) FEET ON CENTER FOR ENTIRE WORK ZONE.
DRUMS ARE TO BE PLACED TO ALLOW FOR A SINGLE TRAVEL LANE.
PROVIDE SUFFICIENT GAPS BETWEEN DRUMS AT ALL DRIVEWAYS.

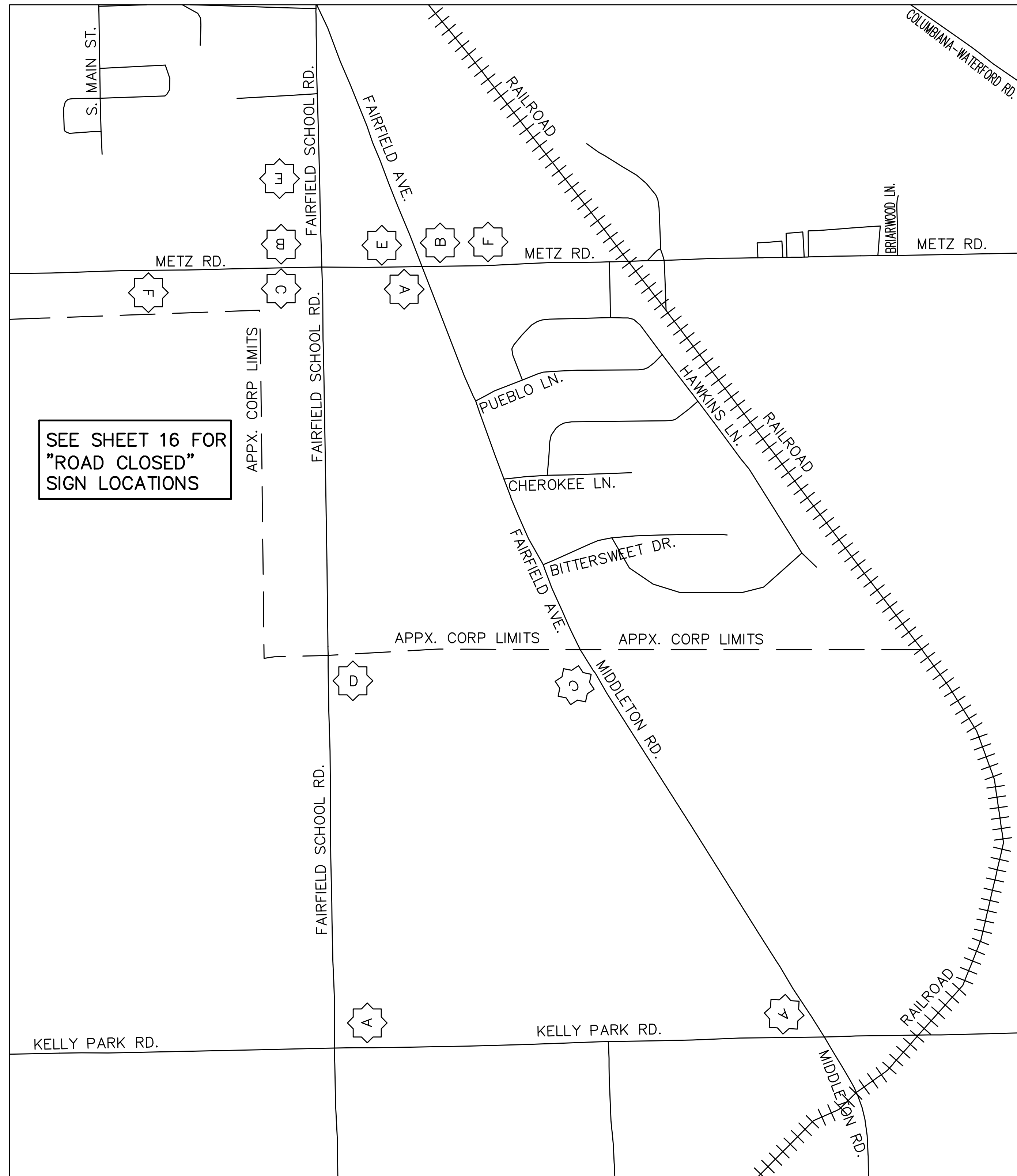
HATCHED AREA REPRESENTS "WORK ZONE 1"

ARROWS REPRESENT DIRECTION OF TRAFFIC FLOW (TYP.)

HATCHED AREA REPRESENTS "WORK ZONE 1"

"WORK ZONE 2"; SEE SHEETS 18-19

METZ RD.



DETOUR ROUTE MAP – WORK ZONE 1



DETOUR SIGN LEGEND

- A DETOUR M4-9
- B DETOUR M4-9
- C DETOUR M4-9
- D END DETOUR M4-8a
- E ROAD CLOSED AHEAD W20-3
TYPE B FLASHING WARNING LIGHT
- F DETOUR AHEAD W20-2
TYPE B FLASHING WARNING LIGHT

NOTICE OF ROAD CLOSURE AND DETOURS ROAD WORK TO BEGIN ON MONTH / DAY / YEAR FOR DETOUR INFORMATION CALL: (INSERT CONTACT #)

CONTRACTOR TO PROVIDE NOTICE OF CLOSURE SIGNS IN THE FOLLOWING LOCATIONS:

- SOUTH-BOUND ALONG FAIRFIELD SCHOOL ROAD APPROXIMATELY 100' SOUTH OF THE METZ ROAD INTERSECTION.
- NORTH-BOUND ALONG FAIRFIELD SCHOOL ROAD APPROXIMATELY 100' NORTH OF THE SOUTHERLY CORPORATION LIMITS.
- EAST-BOUND ALONG METZ ROAD APPROXIMATELY 200' WEST OF THE FAIRFIELD SCHOOL ROAD INTERSECTION.
- WEST-BOUND ALONG METZ ROAD APPROXIMATELY 200' EAST OF THE FAIRFIELD SCHOOL ROAD INTERSECTION.

ALL SIGNS SHALL INCLUDE THE PROPER GROUND MOUNTED SUPPORT POSTS. THE GROUND MOUNTED SUPPORT POSTS, SIGN PROPERTIES AND LETTERING PROPERTIES SHALL BE IN ACCORDANCE WITH ODOT CMS 630 AND THE CURRENT OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

NOTE: THE CONTRACTOR SHALL ERECT, MAINTAIN, AND REMOVE THE DETOUR AND ALL OTHER MOT SIGNING AND SUPPORTS. SIGNS SHALL ALSO INCLUDE THE NECESSARY SIGN POST SUPPORTS. ONCE THE PERTINENT WORK BEGINS WITHIN THE DETOUR, IT SHALL BE COMPLETED IN ITS ENTIRETY.

ALL COSTS ASSOCIATED WITH THE DETOUR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614: MAINTAINING TRAFFIC.

HORIZONTAL SCALE IN FEET

 0 10 20 30 40 50 60 70 80 90 100

 CALC. BY: J.F.B. DATE: 10/23

 CHKD. BY: D.G.D. DATE: 10/23

"WORK ZONE 2" NOTES

THE CONTRACTOR SHALL ERECT, MAINTAIN, AND REMOVE ALL MOT SIGNING, SUPPORTS, ETC.

THE CONTRACTOR SHALL PROVIDE THE NECESSARY SIGN POST SUPPORTS FOR ALL MOT SIGNING (TEMPORARY SIGN SUPPORTS WILL NOT BE PERMITTED).

ONE-WAY SIGNS SHALL BE PLACED AT ALL DRIVEWAYS AND SIDE STREETS WITHIN THE WORK ZONE LIMITS AS SHOWN IN THE SCHEMATIC ON THIS SHEET.

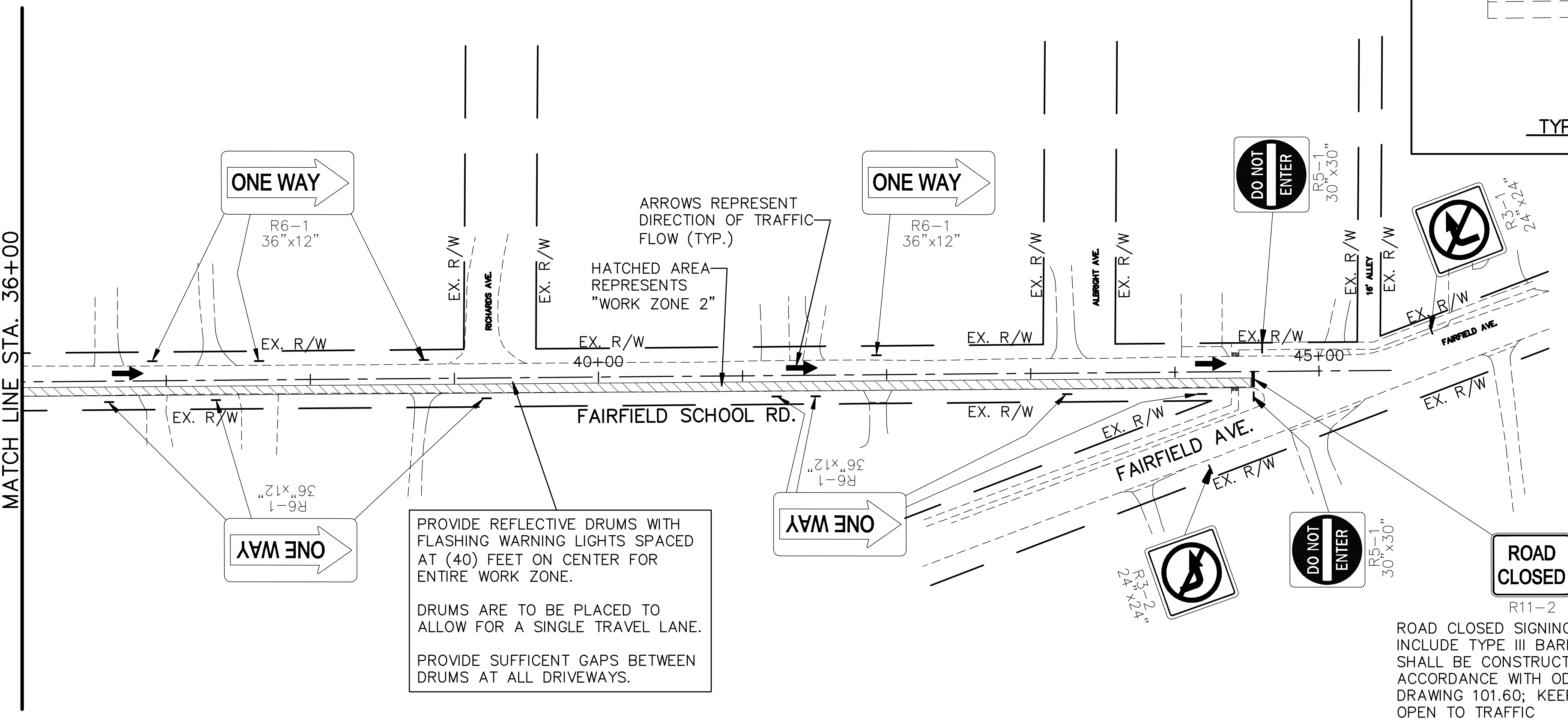
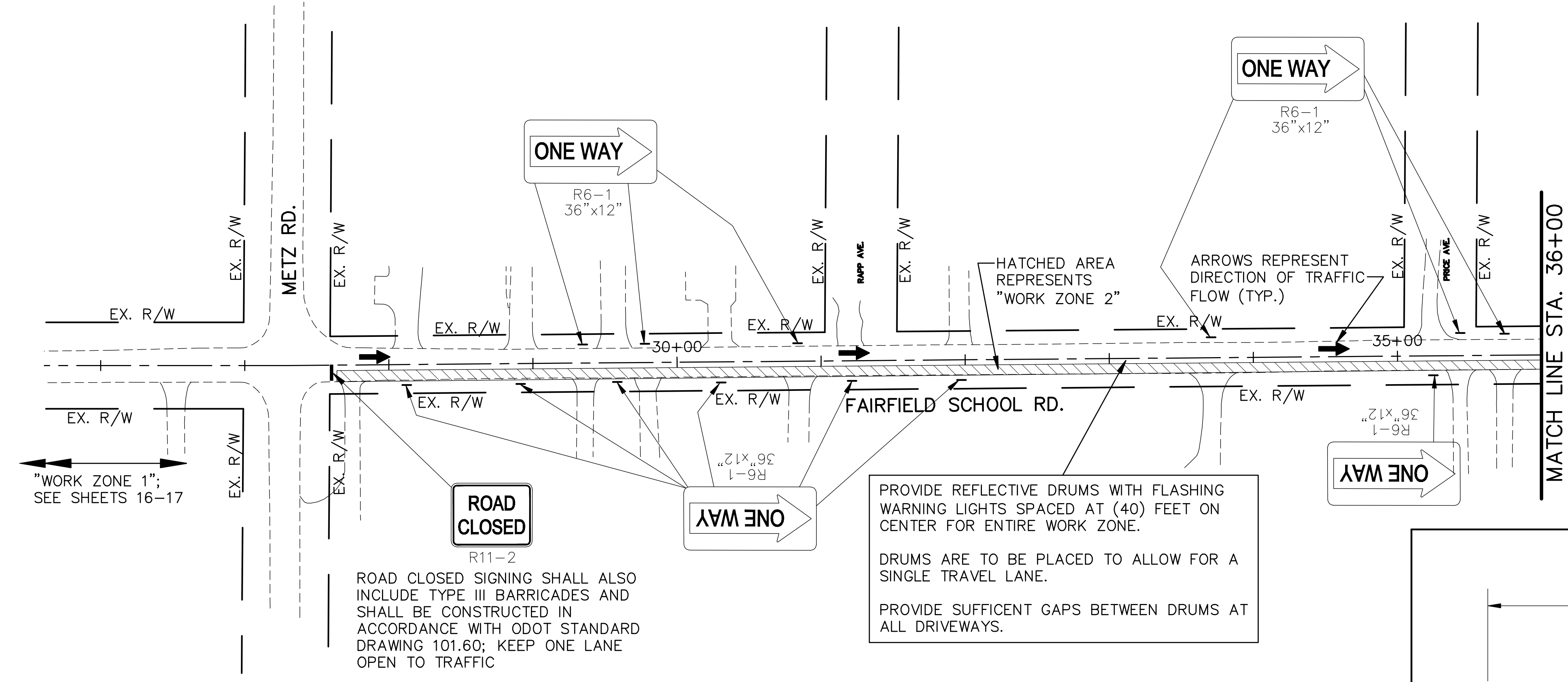
ROAD CLOSED SIGNING SHALL BE PROVIDED WITH TYPE III BARRICADES.

SEE SHEET 19 FOR DETOUR SIGNING FOR "WORK ZONE 2".

SEE SHEET 13 FOR ADDITIONAL M.O.T. NOTES.

ALL WORK SHALL BE IN ACCORDANCE WITH ANY PERTINENT ODOT STANDARD CONSTRUCTION DRAWINGS, ODOT C&MS ITEM 614, AND THE M.O.T. NOTES & DETAILS IN THE CONSTRUCTION PLANS.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614: MAINTAINING TRAFFIC.



PROVIDE REFLECTIVE DRUMS WITH FLASHING WARNING LIGHTS SPACED AT (40) FEET ON CENTER FOR ENTIRE WORK ZONE.

DRUMS ARE TO BE PLACED TO ALLOW FOR A SINGLE TRAVEL LANE.

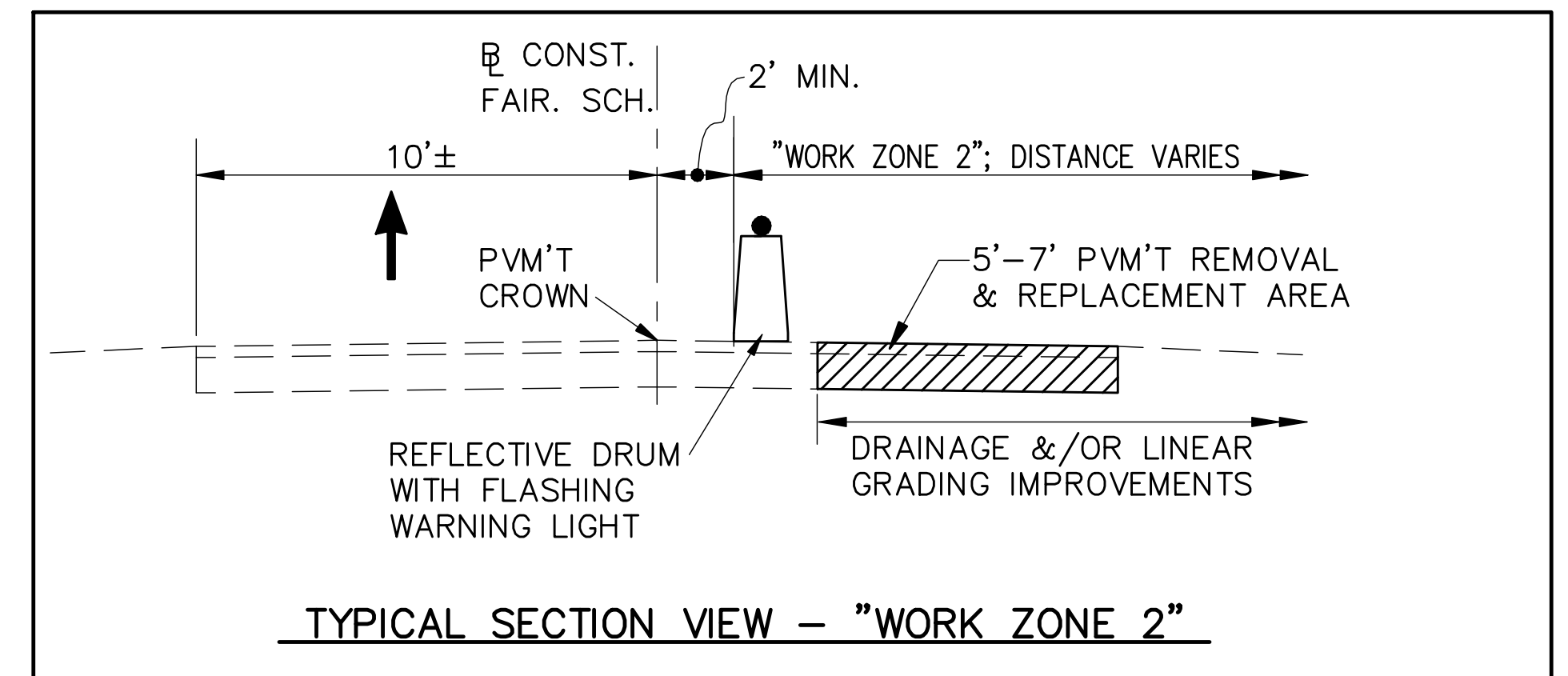
PROVIDE SUFFICIENT GAPS BETWEEN DRUMS AT ALL DRIVEWAYS.

ROAD CLOSED SIGNING SHALL ALSO INCLUDE TYPE III BARRICADES AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH ODOT STANDARD DRAWING 101.60; KEEP ONE LANE OPEN TO TRAFFIC

PROVIDE REFLECTIVE DRUMS WITH FLASHING WARNING LIGHTS SPACED AT (40) FEET ON CENTER FOR ENTIRE WORK ZONE.

DRUMS ARE TO BE PLACED TO ALLOW FOR A SINGLE TRAVEL LANE.

PROVIDE SUFFICIENT GAPS BETWEEN DRUMS AT ALL DRIVEWAYS.



ROAD CLOSED SIGNING SHALL ALSO INCLUDE TYPE III BARRICADES AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH ODOT STANDARD DRAWING 101.60; KEEP ONE LANE OPEN TO TRAFFIC



SEE SHEET 18 FOR
"ROAD CLOSED"
SIGN LOCATIONS

**NOTICE OF ROAD
CLOSURE AND DETOURS
ROAD WORK TO BEGIN ON
MONTH / DAY / YEAR
FOR DETOUR INFORMATION
CALL: (INSERT CONTACT #)**

DETOUR SIGN LEGEND

- A M4-9
- B M4-9
- C M4-9
- D M4-8a
- E TYPE B FLASHING WARNING LIGHT
W20-3
- F TYPE B FLASHING WARNING LIGHT
W20-2

CONTRACTOR TO PROVIDE NOTICE OF CLOSURE SIGNS IN THE FOLLOWING LOCATIONS:

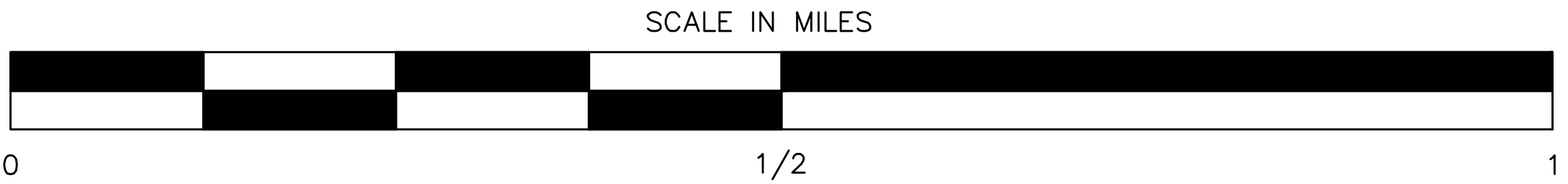
- SOUTH-BOUND ALONG FAIRFIELD SCHOOL ROAD APPROXIMATELY 100' SOUTH OF THE FAIRFIELD AVENUE INTERSECTION.
- NORTH-BOUND ALONG FAIRFIELD SCHOOL ROAD APPROXIMATELY 100' NORTH OF THE METZ ROAD INTERSECTION.

ALL SIGNS SHALL INCLUDE THE PROPER GROUND MOUNTED SUPPORT POSTS. THE GROUND MOUNTED SUPPORT POSTS, SIGN PROPERTIES AND LETTERING PROPERTIES SHALL BE IN ACCORDANCE WITH ODOT CMS 630 AND THE CURRENT OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

NOTE: THE CONTRACTOR SHALL ERECT, MAINTAIN, AND REMOVE THE DETOUR AND ALL OTHER MOT SIGNING AND SUPPORTS. SIGNS SHALL ALSO INCLUDE THE NECESSARY SIGN POST SUPPORTS. ONCE THE PERTINENT WORK BEGINS WITHIN THE DETOUR, IT SHALL BE COMPLETED IN ITS ENTIRETY.

ALL COSTS ASSOCIATED WITH THE DETOUR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614: MAINTAINING TRAFFIC.

DETOUR ROUTE MAP – WORK ZONE 2



CALC. BY: J.F.B. DATE: 10/23
CHKD. BY: D.G.D. DATE: 10/23

(*) AREAS CALCULATED BY COMPUTER

AREA OF ASPHALT PAVEMENT
INCLUDING SIDE ROADS.

AREA OF ASPHALT PAVEMENT
INCLUDING SIDE ROADS.

FAIRFIELD SCHOOL ROAD

STA. 0+88.00 TO STA. 45+46.78 = 4,458.78 L.F.
(*) 97,427 SQ. FT. ÷ 9 = 10,825 S.Y.

TOTAL = 10,825 S.Y.

DRIVEWAY APPROACHES (NOT BEING REMOVED & REPLACED)

(39) TOTAL @ 15 S.Y. (AVERAGE) = 585 S.Y.

TOTAL = 585 S.Y.

FAIRFIELD SCHOOL ROAD (RIGHT SIDE PAVEMENT REPLACEMENT)

5 FOOT WIDE - STA. 0+88.00 TO STA. 35+50.00 = 3,462.00 L.F.
(*) 17,310 SQ. FT. ÷ 9 = 1,923 S.Y.
7 FOOT WIDE - STA. 35+50.00 TO STA. 45+46.78 = 996.78 L.F.
(*) 6,977 SQ. FT. ÷ 9 = 775 S.Y.

TOTAL = 2,698 S.Y.

LISBON STREET

RR BRIDGE - STA. 52+05.00 TO STA. 53+64.00 = 159 L.F.
(*) 3,586 SQ. FT. ÷ 9 = 398 S.Y.
STA. 53+64.00 TO STA. 73+59.00 = 1,995 L.F.
(*) 63,147 SQ. FT. ÷ 9 = 7,016 S.Y.

TOTAL = 7,414 S.Y.

FAIRFIELD SCHOOL ROAD

FAIRFIELD SCHOOL ROAD

LISBON STREET

ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE,
AS PER PLAN (T=1-1/2"±)

TOTALS FROM ABOVE=(10,825 SQ. YD.)-(2,698 S.Y.) = 8,127 S.Y.
(NOTE: DOES NOT INCLUDE PAVEMENT PLANING WITHIN THE
5'/7' WIDE PAVEMENT REMOVAL/REPLACEMENT AREAS)

USE 8,127 SQ. YD.

ITEM 202 PAVEMENT REMOVED, AS PER PLAN

TOTAL FROM ABOVE = 2,698 SQ. YD.

USE 2,698 SQ. YD.

ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE,
AS PER PLAN (T=3"±)

TOTAL FROM ABOVE = 7,016 SQ. YD.

USE 7,016 SQ. YD.

ITEM 407 TACK COAT (@ 0.055 GAL./SQ. YD.)

10,825 S.Y. (0.055) = 595.38 GAL.
585 S.Y. (0.055) = 32.17 GAL.
627.55 GAL.

USE 628 GAL.

ITEM 204 SUBGRADE COMPACTION

TOTAL FROM ABOVE = 2,698 SQ. YD.

USE 2,698 SQ. YD.

ITEM 690 SPECIAL - PAVEMENT PLANING, ASPHALT CONCRETE,
(RAILROAD BRIDGE)

TOTAL FROM ABOVE = 398 SQ. YD.

USE 398 SQ. YD.

ITEM 407 TACK COAT, 702.13 (@ 0.085 GAL./SQ. YD.)

10,825 S.Y. (0.085) = 920.13 GAL.

USE 920 GAL.

ITEM 301 3" ASPHALT CONCRETE BASE, PG64-22, (449)

2,698 S.Y. (0.250 FT.) ÷ 3 = 224.83 CU. YD.

USE 225 CU. YD.

ITEM 407 TACK COAT (@ 0.055 GAL./SQ. YD.)

7,414 S.Y. (0.055) = 407.77 GAL.

USE 408 GAL.

ITEM 441 1 1/4" ASPHALT CONCRETE SURFACE COURSE,
TYPE 1, (448), (PG64-22)

10,825 S.Y. (0.104 FT.) ÷ 3 = 375.27 CU. YD.
(NOTE: @ 1-1/2" THICK) 585 S.Y. (0.125 FT.) ÷ 3 = 24.38 CU. YD.
399.65 CU. YD.

USE 400 CU. YD.

ITEM 304 6" AGGREGATE BASE, AS PER PLAN

2,698 S.Y. (0.500 FT.) ÷ 3 = 449.66 CU. YD.

USE 450 CU. YD.

ITEM 407 TACK COAT, 702.13 (@ 0.085 GAL./SQ. YD.)

7,414 S.Y. (0.085) = 630.19 GAL.

USE 630 GAL.

ITEM 441 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE,
TYPE 2, (448)

10,825 S.Y. (0.146 FT.) ÷ 3 = 526.82 CU. YD.

USE 527 CU. YD.

ITEM 441 1 1/4" ASPHALT CONCRETE SURFACE COURSE,
TYPE 1, (448), (PG64-22)

7,414 S.Y. (0.104 FT.) ÷ 3 = 257.02 CU. YD.

USE 257 CU. YD.

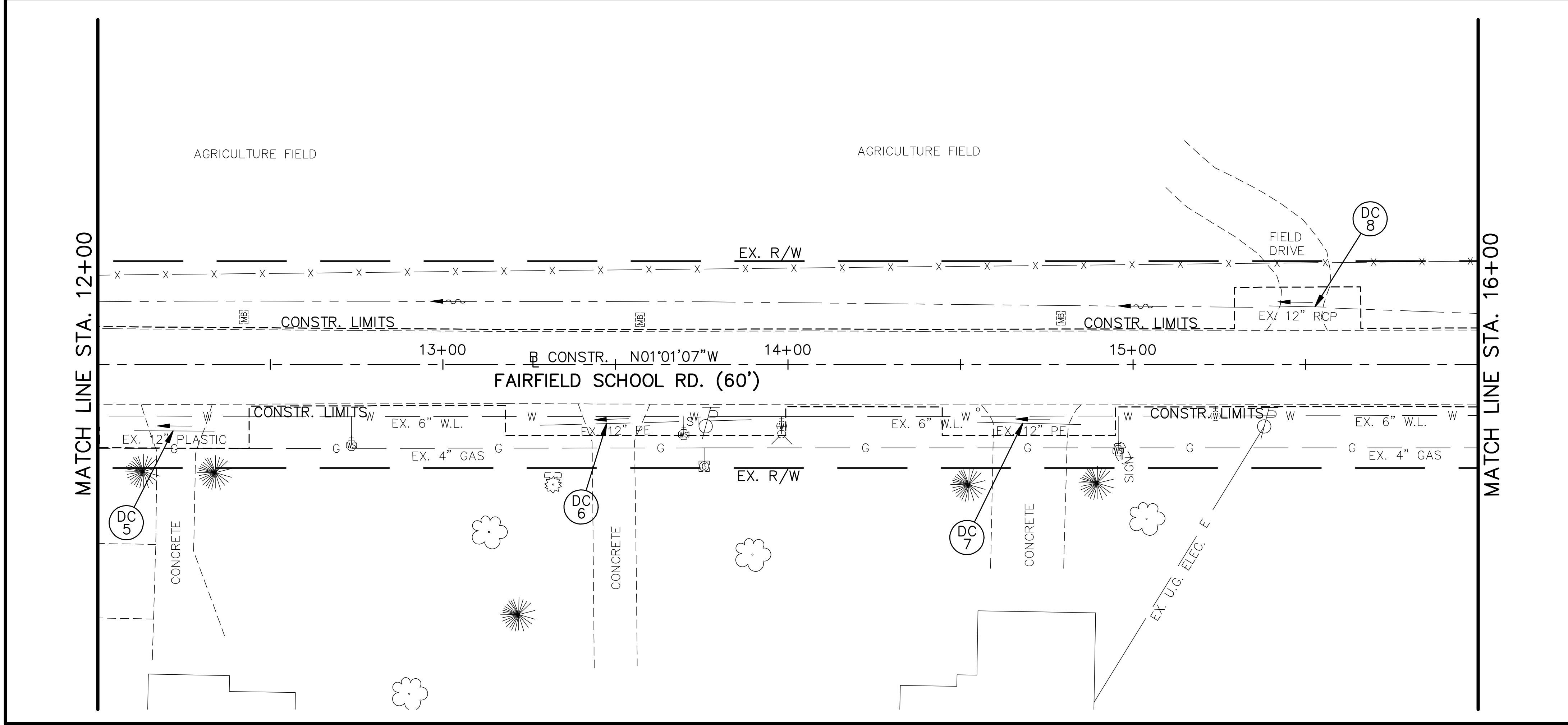
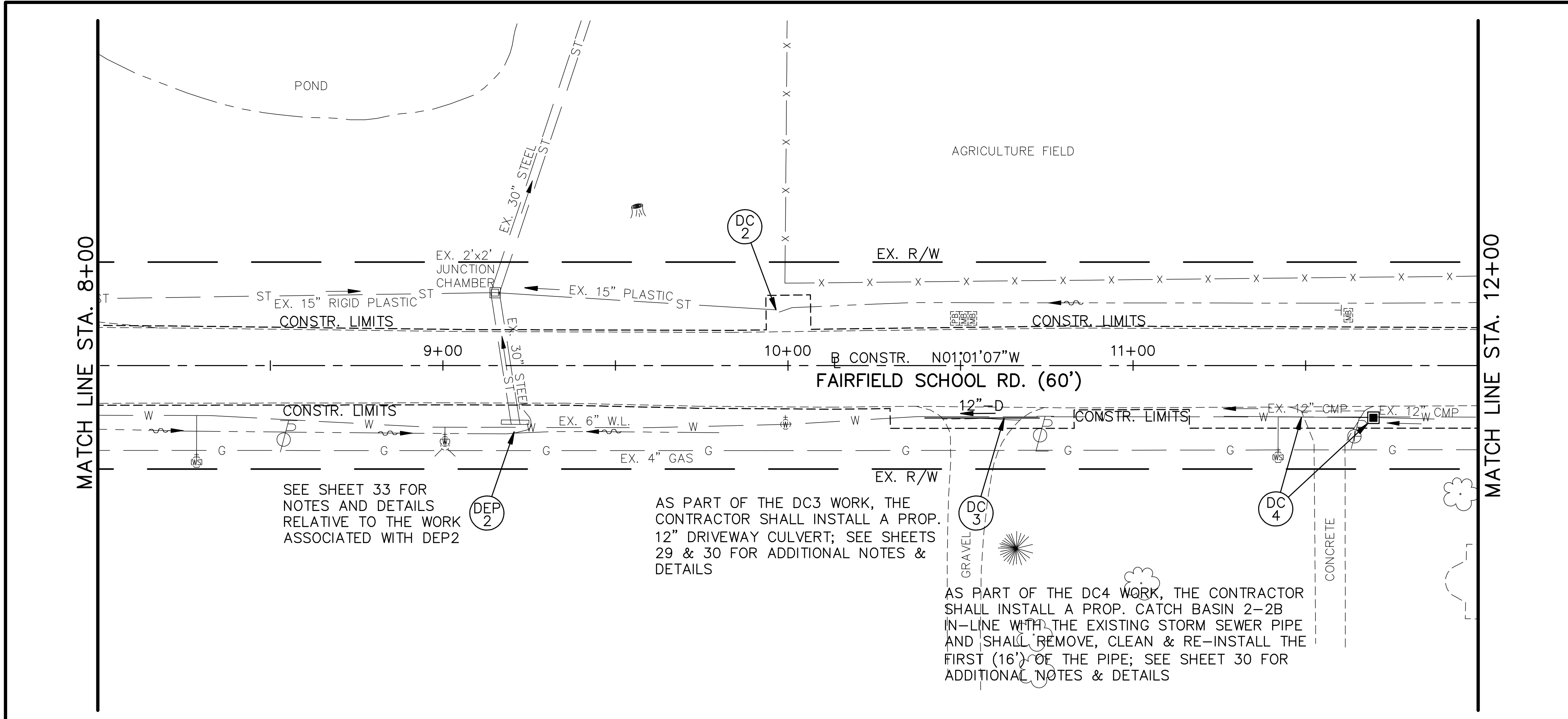
ITEM 441 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE,
TYPE 2, (448)

7,414 S.Y. (0.146 FT.) ÷ 3 = 360.81 CU. YD.

USE 361 CU. YD.

CALCULATIONS

COL - FAIRFIELD SCH. RD.
& LISBON ST.



REF. NO.	STATION TO STATION	SIDE	ESTIMATED QUANTITIES		TOTALS CARRIED TO GENERAL SUMMARY
			FOOT	FOOT	
209		DITCH CLEANOUT, AS PER PLAN	10		10
601		RIPRAP, TYPE D, AS PER PLAN	5		5
601		CRUSHED AGGREGATE SLOPE PROTECTION, AS PER PLAN	3		3
611		12\"/>	33		33
611		CONDUIT, MISC. PIPE REMOVED AND RE-INSTALLED	16		16
611		CONDUIT, MISC. PIPE CLEANOUT	5		5
611		CATCH BASIN, NO. 2-2B		1	1
DC2	9+15~10+07	LT.			
DC3	10+40~10+73	RT.			
DC4	11+26~11+87	RT.			
DC5	12+01~12+44	RT.			
DC6	13+18~13+99	RT.			
DC7	14+45~14+95	RT.			
DC8	15+29~15+66	LT.			
DEP2	9+21	RT.			
SEE SHIT. NO.					
			29.30		29.30
			29.30		29.30
			29.30		29.30
			29.30		29.30
			29.30		29.30
			33		33

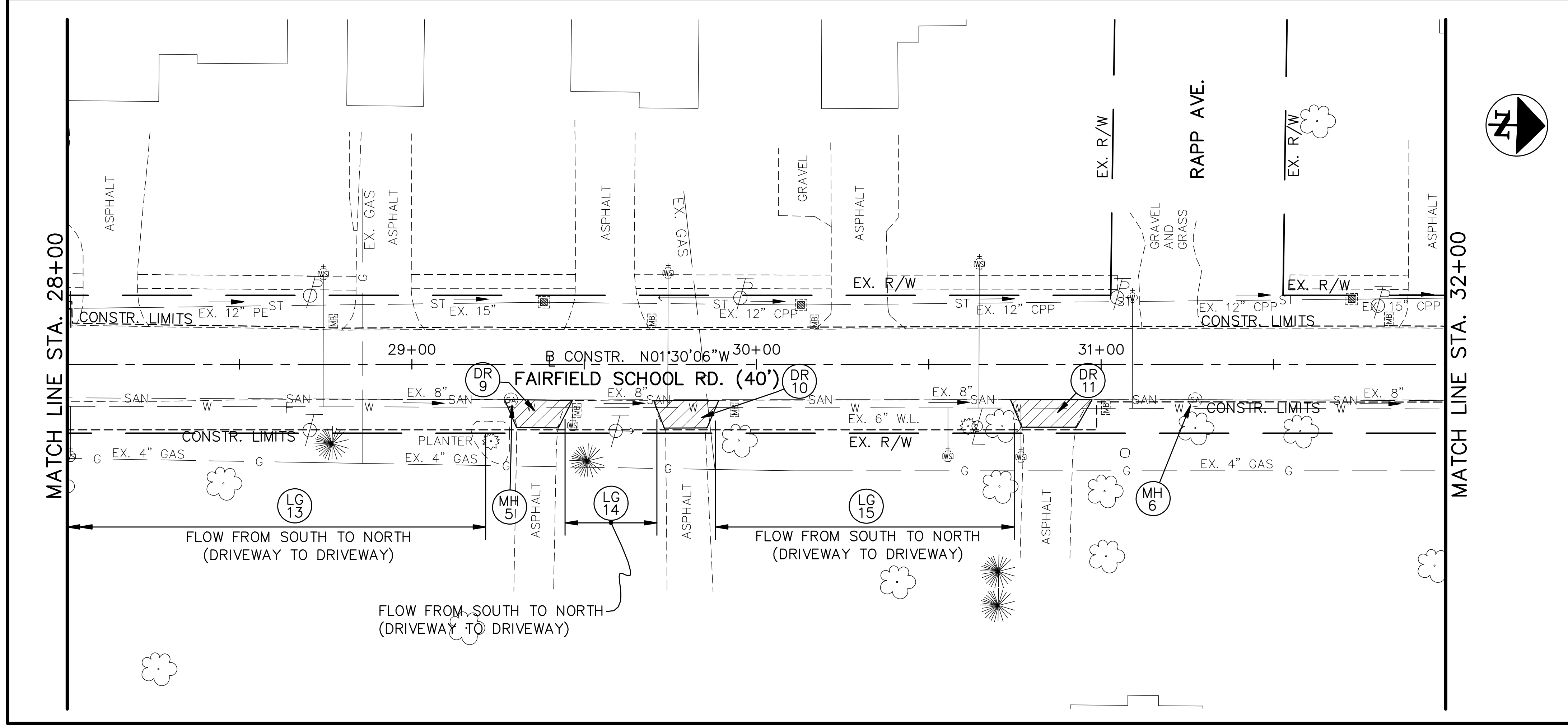
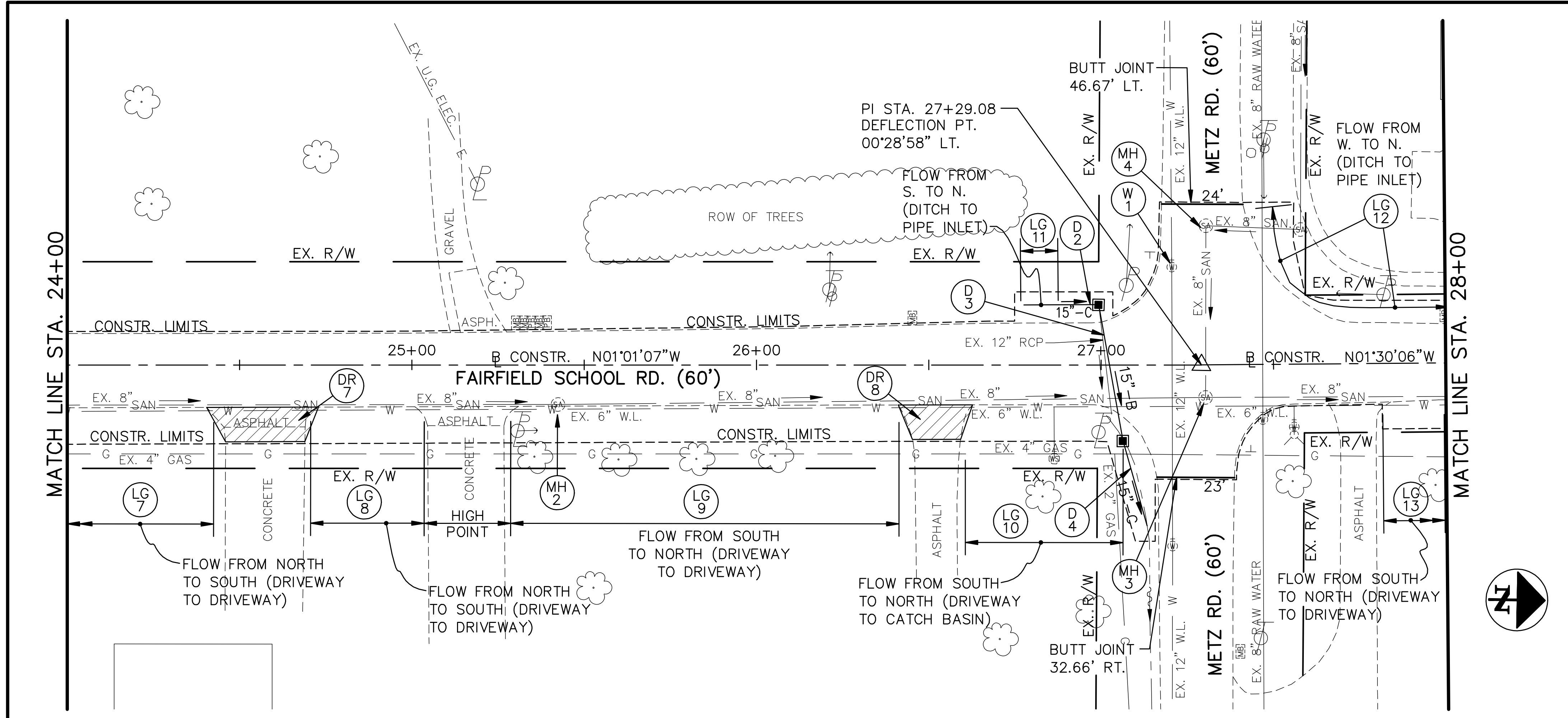
COL - FAIRFIELD SCH. RD. & LISBON ST.

PLAN VIEW (FAIRFIELD SCHOOL ROAD)
STA. 8+00 TO STA. 16+00

24
60

DATE: 10/23
BY: J.F.B.
CHECKED: D.G.D.
DATE: 10/23

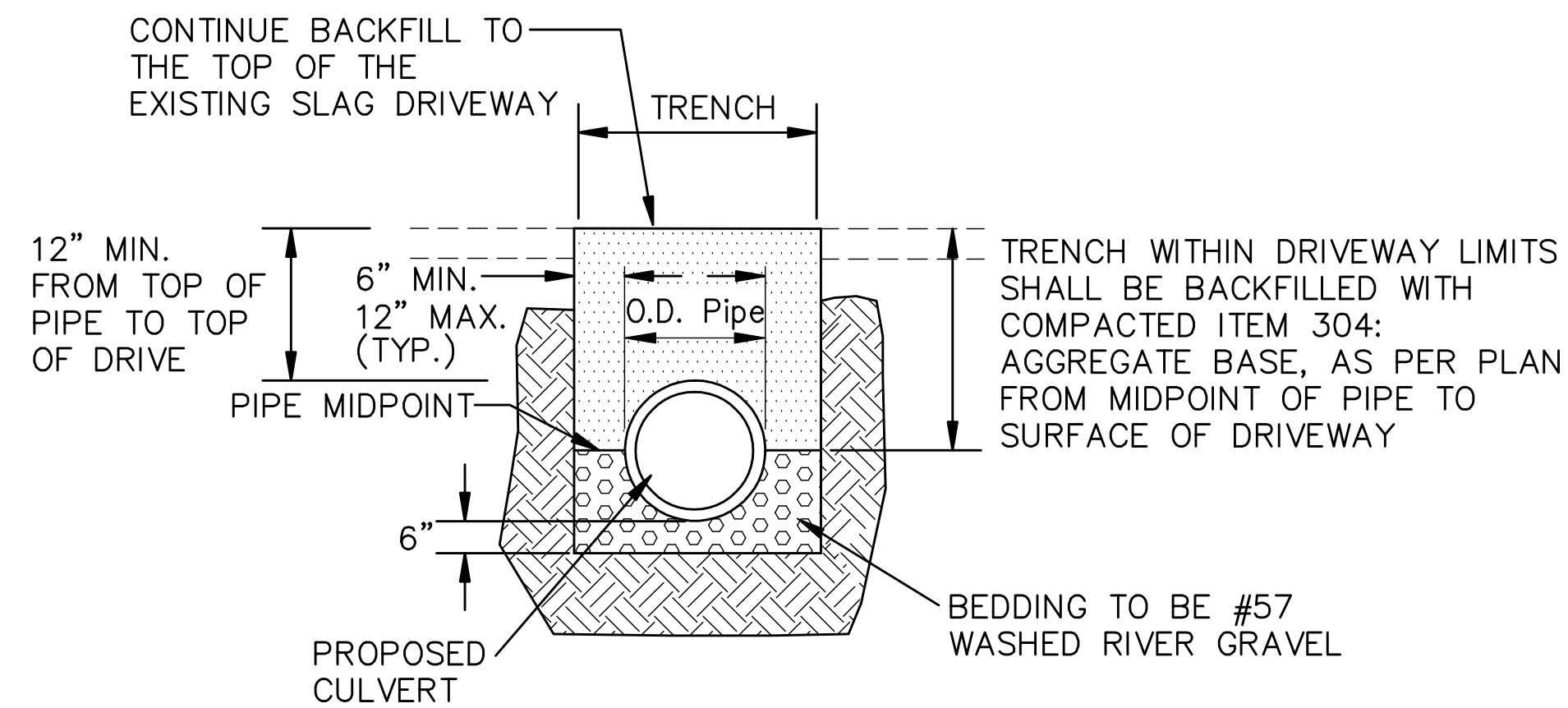
HORIZONTAL SCALE IN FEET



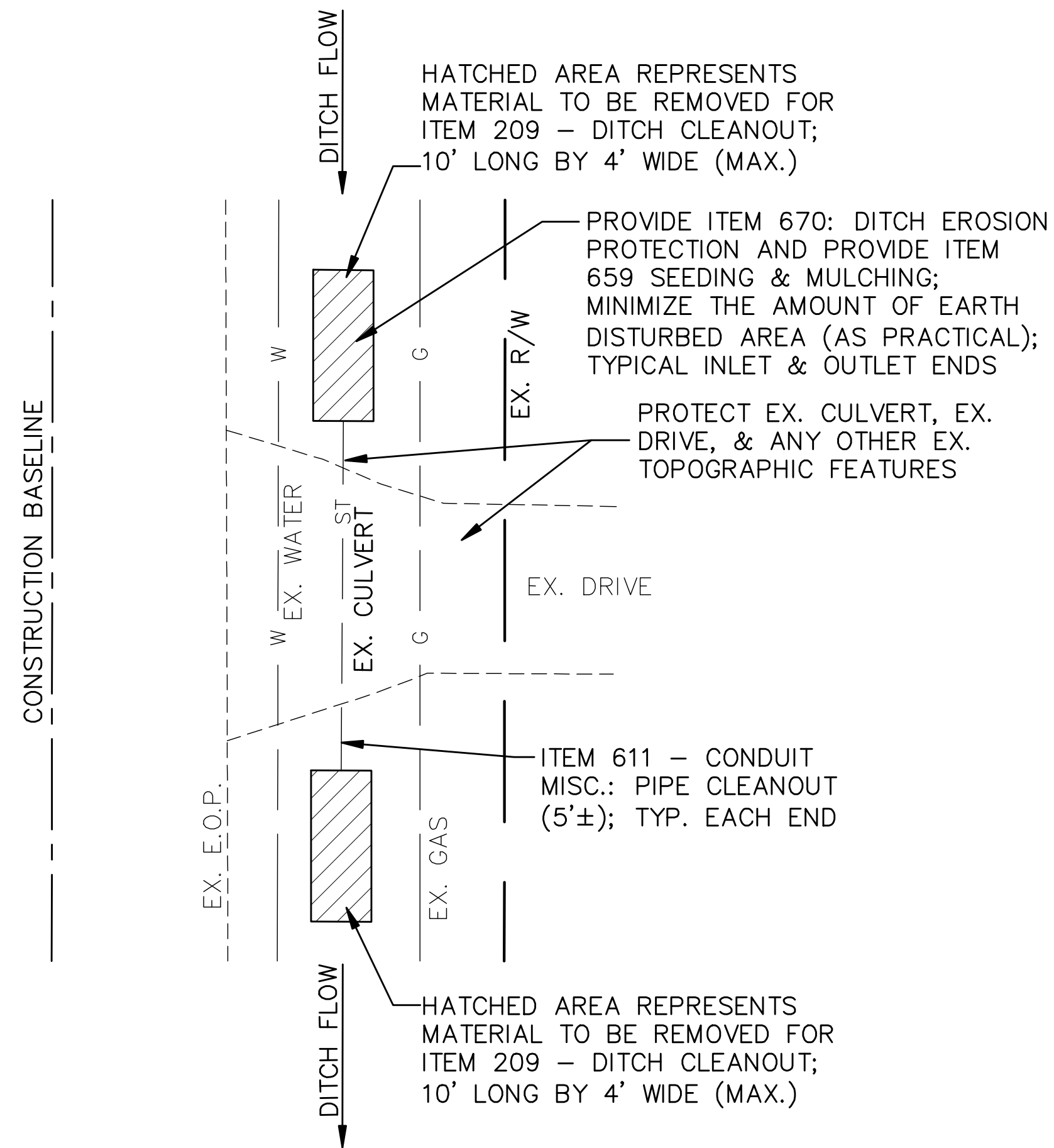
ESTIMATED QUANTITIES

REF. NO.	STATION TO STATION	SIDE	209	611	638	659	659	659	670	SEE SHIT. NO.
LG7	24+00~24+43	RT.	0.4							
LG8	24+71~25+04	RT.	0.3							
LG9	25+29~26+41	RT.	1.1							
LG10	26+61~27+06	RT.	0.5							
LG11	26+78~26+88	LT.	0.1							
LG12	27+50~28+01	LT.	0.7							
LG13	27+82~29+21	RT.	1.4							
LG14	29+89~30+13	RT.	0.3							
LG15	30+13~30+75	RT.	0.9							
MH2	25+42	RT.		1						
MH3	27+30	RT.		1						
MH4	27+31	LT.		1						
MH5	29+29	RT.		1						
MH6	31+27	RT.		1						
W1	27+21	LT.			1					
TOTALS CARRIED TO GENERAL SUMMARY			5.7	5	1	381	0.09	0.11	3.08	128

REF. NO.	STATION TO STATION	SIDE	202	204	304	305	441	452	611	611	611	670	SEE SHIT. NO.
D2	26+87.57~26+99.26	LT.											
D3	26+99.26~27+06.32	L/R	14						12	1	9		
D4	27+06.32~27+13.35	RT.							26	1	7		
DR7	24+58	RT.	31	31	4						2		
DR8	26+53	RT.	20	20	4								
DR9	29+36	RT.	14	14	3								
DR10	29+79	RT.	14	14	3								
DR11	30+85	RT.	18	18	3								
TOTALS CARRIED TO GENERAL SUMMARY			111	97	17	14	6	31	38	2			



**TRENCH DETAIL
DRIVEWAY CULVERT**
STA. 10+55, RT. (DRIVE CULVERT)
STA. 23+20, LT. (FIELD DRIVE CULVERT)

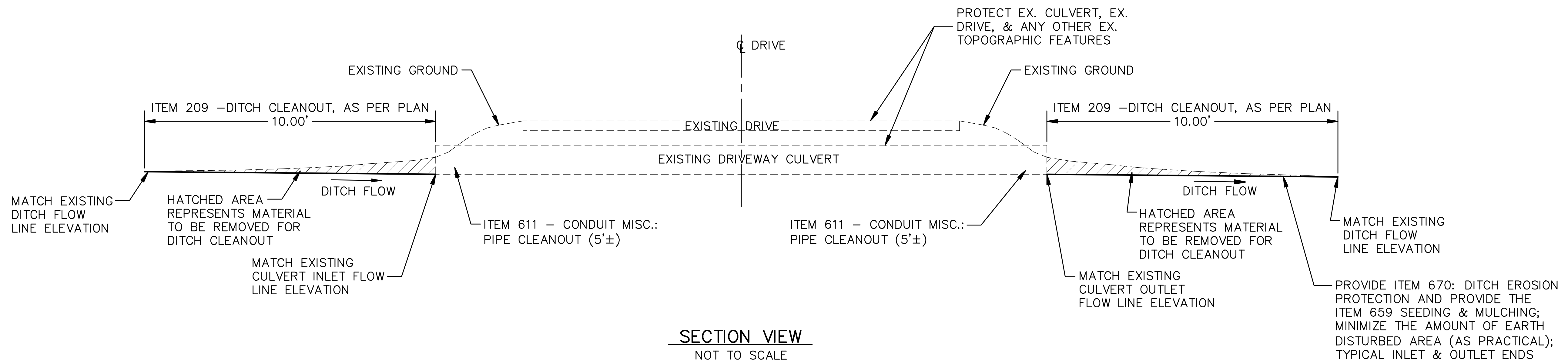


PLAN VIEW
NOT TO SCALE

THE DETAILS SHOWN ON THIS SHEET ARE A GENERAL REPRESENTATION OF THE WORK TO BE PERFORMED IN ORDER TO ALLOW FOR PROPER CONVEYANCE OF DRAINAGE WITHIN THE EXISTING DITCH AND INTO/OUT OF THE EXISTING DRAINAGE PIPES.

THE SIDE OF ROADWAY, DITCH FLOW DIRECTION, UTILITY LOCATIONS, ETC. VARY WITH EACH DRIVEWAY CULVERT AND/OR STORM SEWER INLET LOCATION. SEE THE PERTINENT PLAN VIEW SHEETS FOR ADDITIONAL INFORMATION.

SEE SHEET 30 FOR NOTES RELATED TO ITEM 209: DITCH CLEANOUT, AS PER PLAN; ITEM 611: CONDUIT MISC.: PIPE CLEANOUT; AND FOR INFORMATION REGARDING EACH DRIVEWAY CULVERT AND STORM SEWER INLET LOCATION.



SECTION VIEW
NOT TO SCALE

DITCH CLEANOUT AND PIPE CLEANOUT TYPICAL DETAILS

ITEM 209 - DITCH CLEANOUT, AS PER PLAN

THE FOLLOWING NOTES AND THE DETAILS ON SHEET 29 RELATE TO THE NECESSARY CLEANOUT (MINOR EXCAVATION) OF THE EXISTING DITCHES IN ORDER TO RE-ESTABLISH THE FLOW LINES OF THE INLET/OUTLET ENDS OF PERTINENT EXISTING DRIVEWAY CULVERTS AS WELL AS THE INLET END OF PERTINENT EXISTING OPEN-ENDED STORM SEWERS IN ORDER TO ALLOW FOR PROPER CONVEYANCE OF DRAINAGE WITHIN THE EXISTING DITCH AND INTO/OUT OF THE EXISTING DRAINAGE PIPES.

IT IS ANTICIPATED THAT A LENGTH OF APPROXIMATELY (10) FEET ON BOTH THE INLET &/OR OUTLET ENDS OF THE PERTINENT EXISTING DRAINAGE PIPES AND A MAXIMUM WIDTH OF (4) FEET WILL BE SUFFICIENT TO RE-ESTABLISH THE EXISTING FLOW LINES.

DITCH CLEANOUT SHALL INCLUDE THE PROPER DISPOSAL OF ALL REMOVED MATERIAL.

ITEM 670 DITCH EROSION PROTECTION SHALL BE APPLIED TO THE AREAS OF EARTH DISTURBED DURING CLEANOUT AND WITHIN THE FLOW LINE OF THE DITCH. A MAXIMUM WIDTH OF (4) FEET FOR THE DITCH EROSION PROTECTION IS ANTICIPATED.

PROVIDE ALL NECESSARY 659 ITEMS (SEEDING, MULCHING, ETC.) AS FURTHER DESCRIBED IN THE GENERAL NOTES ON SHEET 12. ALL SEEDING SHALL BE A CLASS 1 SEED MIX AS FURTHER DESCRIBED IN THE GENERAL NOTES ON SHEET 12.

THE CONTRACTOR SHALL MINIMIZE THE AMOUNT OF DITCH AND ADJACENT LAWN AREAS DISTURBED DURING DITCH CLEANOUT. ANY AREAS DISTURBED BEYOND THE LIMITS OF THE DITCH CLEANOUT SHALL BE PROPERLY PREPARED, SEEDED & MULCHED, ETC. AT THE DIRECTION OF THE CONSTRUCTION ENGINEER AND AT THE EXPENSE OF THE CONTRACTOR.

THE CONTRACTOR SHALL TAKE CARE SO AS NOT TO DAMAGE THE EXISTING DRAINAGE CONDUITS, ANY EXISTING DRIVEWAYS, AND/OR ANY SURROUNDING TOPOGRAPHIC FEATURES. ANY DAMAGE TO THE AFOREMENTIONED SHALL BE REPAIRED BY THE CONTRACTOR AT THEIR OWN EXPENSE.

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS DESCRIBED ABOVE INCLUDING THE PERTINENT 659 & 670 ITEMS SHALL BE INCLUDED IN THE PER FOOT UNIT PRICE BID FOR ITEM 209: DITCH CLEANOUT, AS PER PLAN.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEETS FOR QUANTITIES.

DRAINAGE CULVERT/PIPE NOTES (LEFT SIDE, SOUTH OF METZ ROAD)

THE FOLLOWING IS A LIST OF THE EXISTING (AND/OR NEW) DRIVEWAY CULVERTS AND/OR STORM SEWER INLETS/OUTLETS LOCATED ALONG THE LEFT SIDE OF THE ROADWAY (SOUTH OF METZ ROAD) AND ARE LISTED FROM NORTH TO SOUTH.

STA. 23+20, LT. (NEW FIELD DRIVE CULVERT)
-INSTALL NEW 12" CONDUIT, TYPE D (AGGREGATE DRIVE).
-PROVIDE DITCH CLEANOUT AT INLET & OUTLET ENDS.
-SEE PERTINENT PLAN VIEW FOR ADDITIONAL DETAILS.

STA. 15+48, LT. (FIELD DRIVE CULVERT)
-EX. 12" RCP CONDUIT; APPX PIPE LENGTH IS 17'.
-EX. INLET EXPOSED; PROVIDE DITCH CLEANOUT.
-EX. OUTLET EXPOSED; PROVIDE DITCH CLEANOUT.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET AT EACH END.

STA. 9+97, LT. (STORM SEWER INLET ONLY)
-EX. 15" PE CONDUIT.
-EX. INLET EXPOSED; PROVIDE DITCH CLEANOUT.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET.

STA. 7+06, LT. (STORM SEWER INLET ONLY)
-EX. 15" PE CONDUIT.
-EX. INLET EXPOSED; PROVIDE DITCH CLEANOUT.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEETS FOR QUANTITIES.

DRAINAGE CULVERT/PIPE NOTES (RIGHT SIDE, SOUTH OF METZ ROAD)

THE FOLLOWING IS A LIST OF THE EXISTING AND/OR NEW DRIVEWAY CULVERTS LOCATED ALONG THE RIGHT SIDE OF THE ROADWAY (SOUTH OF METZ ROAD). THE DRIVEWAY CULVERTS ARE LISTED IN THE DIRECTION OF DRAINAGE FLOW (I.E. NORTH TO SOUTH).

STA. 17+05, RT.
-EX. 12" VCP CONDUIT; APPX PIPE LENGTH IS 44'.
-EX. INLET EXPOSED; PROVIDE DITCH CLEANOUT.
-EX. OUTLET EXPOSED; PROVIDE DITCH CLEANOUT.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET AT EACH END.

STA. 14+70, RT.
-EX. 12" PE CONDUIT; APPX PIPE LENGTH IS 30'.
-EX. INLET EXPOSED; PROVIDE DITCH CLEANOUT.
-EX. OUTLET EXPOSED; PROVIDE DITCH CLEANOUT.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET AT EACH END.

STA. 13+50, RT.
-EX. 12" PE CONDUIT; APPX PIPE LENGTH IS 61'.
-EX. INLET EXPOSED; PROVIDE DITCH CLEANOUT.
-EX. OUTLET EXPOSED; PROVIDE DITCH CLEANOUT.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET AT EACH END.

STA. 12+23, RT.
-EX. 12" PE CONDUIT; APPX PIPE LENGTH IS 23'.
-EX. INLET EXPOSED; PROVIDE DITCH CLEANOUT.
-EX. OUTLET EXPOSED; PROVIDE DITCH CLEANOUT.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET AT EACH END.

STA. 11+57, RT.
-EX. 12" CMP CONDUIT; APPX. PIPE LENGTH IS 61'.
-EX. PIPE IS SEPARATED @ APPX. 16' FROM THE PIPE INLET
-REMOVE THE FIRST 16' SECTION OF PIPE, WHICH WILL BE RE-USED.
-CLEAN THE REMOVED SECTION OF PIPE.
-CLEAN THE FIRST (5) FEET OF THE INLET END OF THE EXISTING PIPE TO REMAIN TO THE SOUTH.
-INSTALL A NEW CB-2-2B, AND CONNECT IT TO THE EXISTING PIPE TO REMAIN TO THE SOUTH.
-RE-INSTALL THE REMOVED 16' SECTION OF PIPE; CONNECT IT TO THE NEW CATCH BASIN; AND BED/BACKFILL THE RE-INSTALLED 16' SECTION OF PIPE.
-PROVIDE DITCH CLEANOUT AT THE INLET END OF THE 16' SECTION OF RE-INSTALLED PIPE.
-PROVIDE DITCH CLEANOUT AT OUTLET END OF PIPE.
-PROVIDE PIPE CLEANING FOR THE FIRST (5) FEET AT THE OUTLET END.

STA. 10+55, RT. (NEW DRIVEWAY CULVERT)
-INSTALL NEW 12" CONDUIT, TYPE D (AGGREGATE DRIVE).
-PROVIDE DITCH CLEANOUT AT INLET & OUTLET ENDS.
-SEE PERTINENT PLAN VIEW FOR ADDITIONAL DETAILS.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEETS FOR QUANTITIES.

CATCH BASIN NOTES (RIGHT SIDE, SOUTH OF METZ ROAD)

THE FOLLOWING IS RELATIVE TO A NEW CATCH BASIN THAT IS TO BE INSTALLED IN-LINE WITH THE EXISTING STORM SEWER SYSTEM LOCATED ALONG THE RIGHT SIDE OF THE ROADWAY (SOUTH OF METZ ROAD).

STA. 5+32.00, RT. (SOUTH SIDE OF EX. DRIVEWAY)
-EXCAVATE TO UNCOVER THE EXISTING STORM PIPE.
-CUT & REMOVE A SUFFICIENT LENGTH OF THE EXISTING STORM PIPE IN ORDER TO INSTALL A NEW CATCH BASIN NO. 2-2B.
-THE CONTRACTOR SHALL FIELD VERIFY THE SIZE/MATERIAL OF THE EXISTING STORM PIPE.
-INSTALL THE NEW CATCH BASIN NO. 2-2B AND PROVIDE ALL NECESSARY INCIDENTALS FOR A COMPLETE INSTALLATION, AND ALL NECESSARY INCIDENTALS TO CONNECT THE EXISTING DRAINAGE PIPES TO THE NEW CATCH BASIN.
-A (4) FOOT MAXIMUM PIECE OF OF CONDUIT ON EACH SIDE OF THE NEW CATCH BASIN WILL BE PERMITTED TO CONNECT THE EXISTING STORM PIPE TO THE NEW CATCH BASIN.
-THE CONTRACTOR SHALL PROVIDE A MASONRY COLLAR TO CONNECT THE NEW STORM PIPE TO THE EXISTING STORM PIPE IN ACCORDANCE WITH ODOT STANDARD CONSTRUCTION DRAWING DM-1.1.
-PROVIDE THE NECESSARY DITCH/SWALE CLEANOUT AS WELL AS THE NECESSARY GRADING AROUND THE CATCH BASIN TO ENSURE PROPER DRAINAGE FLOW TO THE NEW CATCH BASIN GRATE.
-SEE PERTINENT PLAN VIEW FOR ADDITIONAL DETAILS.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEETS FOR QUANTITIES.

ITEM 611 - CONDUIT MISC.: PIPE CLEANOUT

IN ADDITION TO THE DITCH CLEANOUT TO BE PERFORMED AS FURTHER DESCRIBED IN THE NOTES ON THIS SHEET, THE FOLLOWING NOTES AND THE DETAILS ON SHEET 29 RELATE TO THE NECESSARY CLEANOUT OF THE EXISTING DRAINAGE PIPES (DRIVEWAY CULVERTS, STORM SEWER INLETS/OUTLETS, ETC.) IN ORDER TO ALLOW FOR PROPER CONVEYANCE OF DRAINAGE FROM THE EXISTING ROADWAY DITCHES THROUGH THE EXISTING DRAINAGE PIPES.

PIPE CLEANOUT WORK SHALL BE PERFORMED AFTER THE DITCH CLEANING OPERATIONS ARE COMPLETE.

PRIOR TO PERFORMING THE PIPE CLEANING OPERATIONS, THE CONTRACTOR, CONSTRUCTION ENGINEER, AND REPRESENTATIVES OF THE CITY SHALL VISUALLY OBSERVE THE SPECIFIC PIPE TO BE CLEANED IN ORDER TO DETERMINE THE EXTENT OF THE CLEANING TO BE PERFORMED.

-BASED UPON FIELD OBSERVATIONS, IT APPEARS THAT ONLY THE FIRST (5) FEET OF THE INLET/OUTLET ENDS OF THE PIPES NEED TO BE CLEANED, WHICH APPEARS MAY BE ACCOMPLISHED WITH MISCELLANEOUS HAND TOOLS (SHOVEL, SPUD BAR, ETC.)
-ALL MATERIAL/DEBRIS REMOVED FROM THE PIPES SHALL BE REMOVED FROM THE SITE AND PROPERLY DISPOSED OF.

ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY FOR THE COMPLETE CLEANOUT OF ANY DRAINAGE PIPES INCLUDING DISPOSAL OF ANY REMOVED MATERIAL AS DESCRIBED IN THE NOTES ABOVE SHALL BE INCLUDED IN PER FOOT UNIT PRICE BID FOR ITEM 611: CONDUIT MISC.: PIPE CLEANOUT.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEETS FOR QUANTITIES.

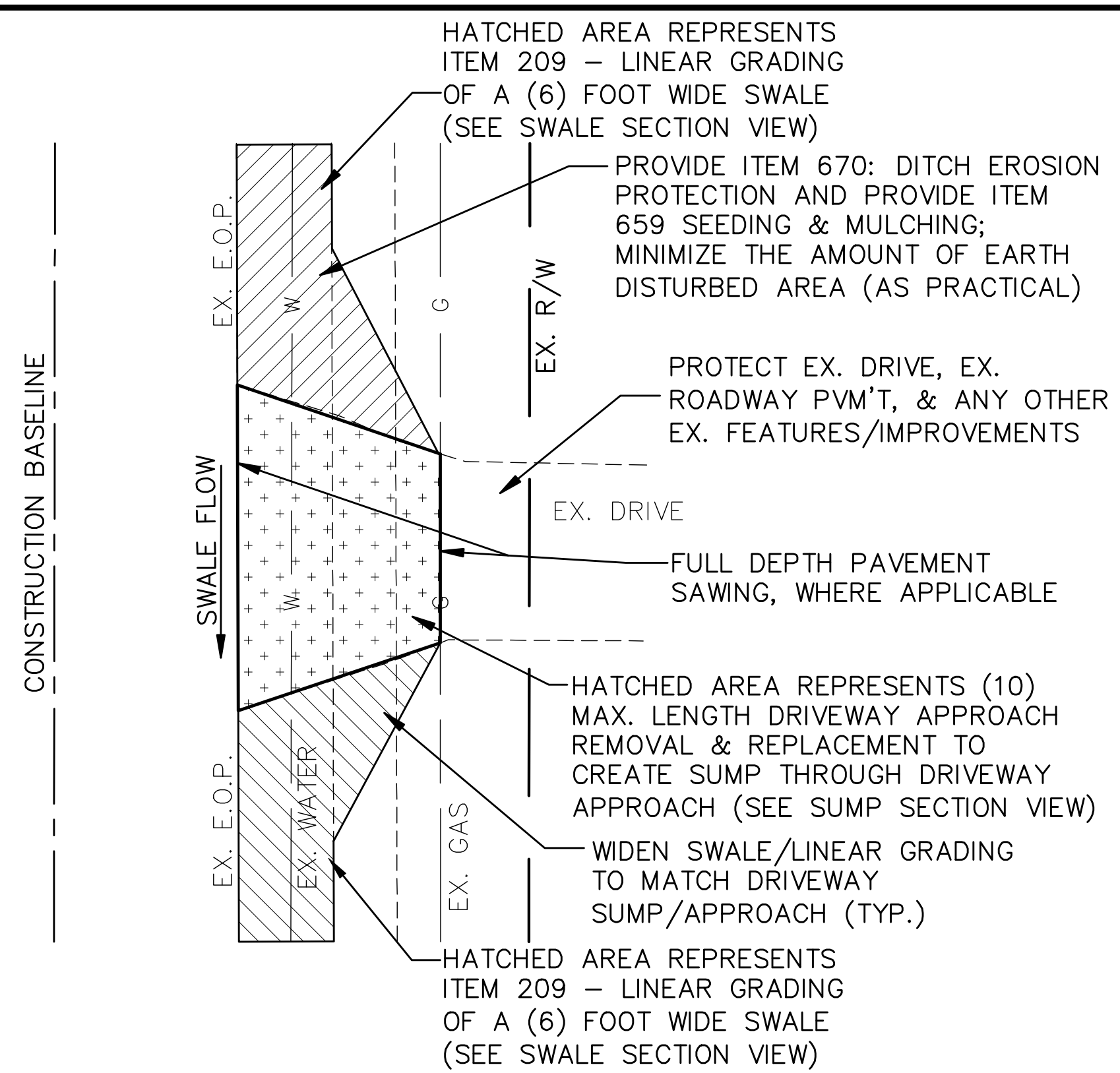
ITEM 611 - CONDUIT MISC.: PIPE REMOVED, CLEANED & RE-INSTALLED

AS PART OF THE WORK TO BE PERFORMED AS DESCRIBED IN THE "STA. 11+57" NOTES TO THE LEFT, THE CONTRACTOR WILL BE REQUIRED TO REMOVE THE FIRST 16' SECTION OF PIPE, WHICH WILL BE RE-USED; CLEAN THE REMOVED SECTION OF PIPE; RE-INSTALL THE REMOVED 16' SECTION OF PIPE; CONNECT IT TO THE NEW CATCH BASIN; AND BED/BACKFILL THE RE-INSTALLED 16' SECTION OF PIPE.

ALL MATERIAL/DEBRIS REMOVED FROM THE PIPE SHALL BE REMOVED FROM THE SITE AND PROPERLY DISPOSED OF.

ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY FOR THE COMPLETE REMOVAL, CLEANING, AND RE-INSTALLATION OF THE EXISTING PIPE INCLUDING DISPOSAL OF ANY REMOVED MATERIAL AS DESCRIBED IN THE NOTES ABOVE SHALL BE INCLUDED IN PER FOOT UNIT PRICE BID FOR ITEM 611: CONDUIT MISC.: PIPE REMOVED, CLEANED & RE-INSTALLED.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEET FOR QUANTITIES.



PLAN VIEW - TYPICAL SWALE & DRIVEWAY SUMP DETAIL
 (STA. 18+85 TO STA. 27+00, RIGHT SIDE)
 NOT TO SCALE

ITEM 209 - LINEAR GRADING, AS PER PLAN (STA. 18+85 TO STA. 27+00, RT.)

THE FOLLOWING NOTES AND THE TYPICAL SWALE DETAIL ABOVE RELATE TO THE NECESSARY LINEAR GRADING TO ESTABLISH PROPOSED SWALES IN ORDER TO ALLOW FOR PROPER CONVEYANCE OF DRAINAGE ALONG THE RIGHT SIDE OF THE ROADWAY BETWEEN STA. 18+75 AND STA. 27+00.

- NOTE: FROM STA. 25+00 TO STA. 27+00, THE DRAINAGE FLOWS FROM SOUTH TO NORTH.
- NOTE: FROM STA. 18+85 TO STA. 25+00, THE DRAINAGE FLOWS FROM NORTH TO SOUTH.

THE SLOPE OF THE PROPOSED SWALES SHALL APPROXIMATELY FOLLOW THE SLOPE OF THE EXISTING ROADWAY.

- THE SLOPE & DEPTH OF THE PROPOSED SWALES, BOTH INTO AND OUT OF THE PROPOSED DRIVEWAY APPROACH SUMP, SHALL BE ADJUSTED TO COINCIDE WITH THE FINISHED GRADES OF THE PROPOSED DRIVEWAY APPROACH SUMP, AND TO ALLOW FOR PROPER DRAINAGE RUNOFF CONVEYANCE THROUGH THE DRIVEWAY APPROACH (I.E. NO PONDING OF WATER).

LINEAR GRADING FOR THE SWALES SHALL INCLUDE THE PROPER DISPOSAL OF ALL REMOVED MATERIAL.

ITEM 670 DITCH EROSION PROTECTION SHALL BE APPLIED TO THE AREAS OF EARTH DISTURBED DURING LINEAR GRADING AND WITHIN THE FLOW LINE OF THE EXISTING SWALE. A MAXIMUM WIDTH OF (2) FEET FOR THE DITCH EROSION PROTECTION IS ANTICIPATED.

PROVIDE ALL NECESSARY 659 ITEMS (SEEDING, MULCHING, ETC.) AS FURTHER DESCRIBED IN THE GENERAL NOTES ON SHEET 12. ALL SEEDING SHALL BE A CLASS 1 SEED MIX AS FURTHER DESCRIBED IN THE GENERAL NOTES ON SHEET 12.

THE CONTRACTOR SHALL MINIMIZE THE AMOUNT OF ADJACENT LAWN AREAS DISTURBED DURING LINEAR GRADING FOR THE SWALES. ANY AREAS DISTURBED BEYOND THE LIMITS OF THE LINEAR GRADING FOR THE SWALES SHALL BE PROPERLY PREPARED, SEEDED & MULCHED, ETC. AT THE DIRECTION OF THE CONSTRUCTION ENGINEER.

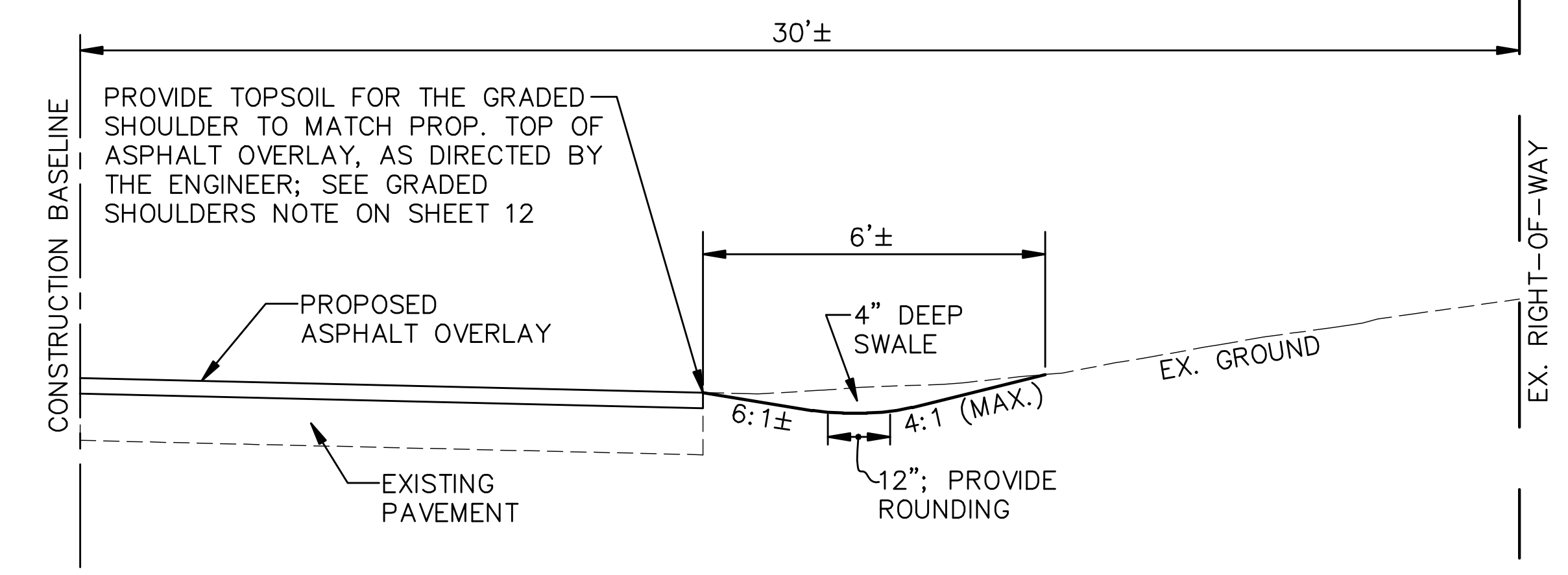
THE CONTRACTOR SHALL TAKE CARE SO AS NOT TO DAMAGE ANY EXISTING DRIVEWAYS, AND/OR ANY SURROUNDING TOPOGRAPHIC FEATURES. ANY DAMAGE TO THE AFOREMENTIONED SHALL BE REPAIRED BY THE CONTRACTOR AT THEIR OWN EXPENSE.

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS DESCRIBED ABOVE SHALL BE INCLUDED IN THE PER FOOT UNIT PRICE BID FOR ITEM 209: LINEAR GRADING, AS PER PLAN.

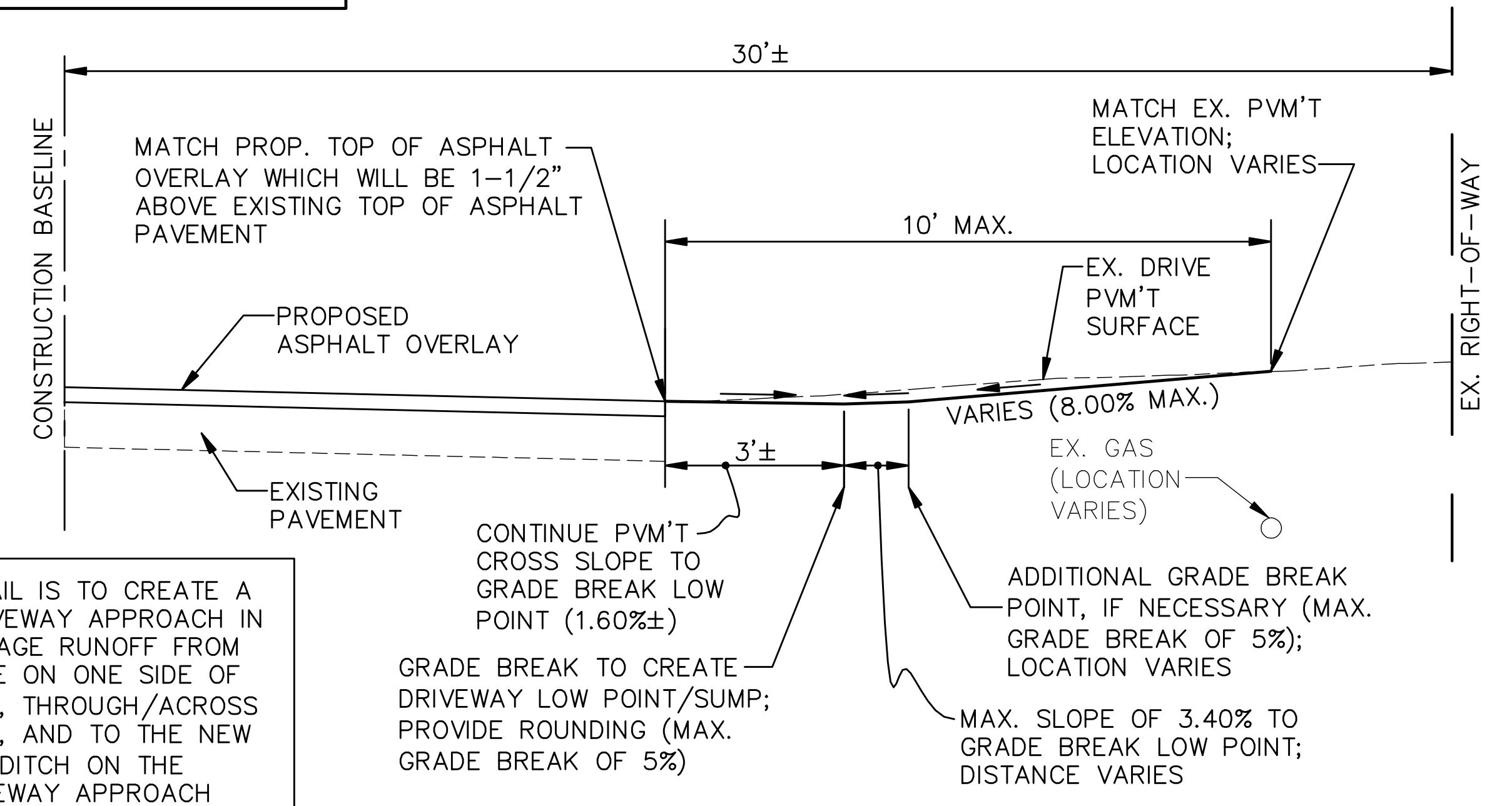
NOTE: THE PERTINENT 659 & 670 ITEMS WILL BE PAID FOR SEPARATELY.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEETS FOR QUANTITIES.

THE DETAILS SHOWN ON THIS SHEET ARE A GENERAL REPRESENTATION OF THE WORK TO BE PERFORMED IN SPECIFIC LOCATIONS SHOWN IN ORDER TO ALLOW FOR PROPER CONVEYANCE OF DRAINAGE WITHIN SWALES OFF OF THE ROADWAY AND ACROSS THE PERTINENT DRIVEWAYS. THE SWALE FLOW DIRECTION, UTILITY LOCATIONS, ETC. VARY WITH EACH DRIVEWAY LOCATION. SEE THE PERTINENT PLAN VIEW SHEETS FOR ADDITIONAL INFORMATION.



SECTION VIEW - TYPICAL SWALE DETAIL
 (STA. 18+85 TO STA. 27+00, RIGHT SIDE)
 NOT TO SCALE



THE INTENT OF THIS DETAIL IS TO CREATE A SUMP AT A SPECIFIC DRIVEWAY APPROACH IN ORDER TO CONVEY DRAINAGE RUNOFF FROM THE NEW ROADWAY SWALE ON ONE SIDE OF THE DRIVEWAY APPROACH, THROUGH/ACROSS THE DRIVEWAY APPROACH, AND TO THE NEW SWALE AND/OR EXISTING DITCH ON THE OTHER SIDE OF THE DRIVEWAY APPROACH

EX. PAVEMENT TYPE	PAVEMENT REPLACEMENT
ASPHALT DRIVE OR SLAG DRIVE	ITEM 204-SUBGRADE COMPACTION ITEM 304-6" AGGREGATE BASE, AS PER PLAN ITEM 441-3" ASPH. CONC. SURF. COURSE, (PG64-22), TYPE 1, (448)
CONCRETE DRIVE	ITEM 204-SUBGRADE COMPACTION ITEM 304-4" AGGREGATE BASE, AS PER PLAN ITEM 452-6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS

NOTE: PROVIDE FULL DEPTH PAVEMENT SAWING PRIOR TO REMOVING THE EXISTING DRIVEWAY APPROACH PAVEMENT FOR ALL ASPHALT & CONCRETE DRIVEWAYS

SECTION VIEW - TYPICAL DRIVEWAY APPROACH SUMP DETAIL
 (DRIVEWAYS BETWEEN STA. 18+85 & STA. 27+00, RIGHT SIDE)
 NOT TO SCALE

ITEM 209 – LINEAR GRADING, AS PER PLAN (NORTH OF METZ ROAD)

THE FOLLOWING NOTES AND THE TYPICAL SWALE DETAILS ON THIS SHEET RELATE TO THE NECESSARY LINEAR GRADING TO ESTABLISH PROPOSED SWALES IN ORDER TO ALLOW FOR PROPER CONVEYANCE OF DRAINAGE TO BE DIRECTED TO THE PROPOSED CATCH BASIN GRATES ALONG THE LEFT & RIGHT SIDES OF THE ROADWAY TO THE NORTH OF METZ ROAD (BETWEEN STA. 27+82 AND STA. 43+12).

- BETWEEN STA. 27+82 TO STA. 30+75 ALONG THE RIGHT SIDE, ALL DRAINAGE WITHIN THE SWALES WILL BE CONVEYED ACROSS THE DRIVEWAYS.
- ALL OTHER DRAINAGE WITHIN THE SWALES WILL BE CONVEYED TO THE PROPOSED CATCH BASIN GRATES THAT WILL BE INSTALLED.
- DRAINAGE WITHIN THE SWALES WILL FLOW FROM SOUTH TO NORTH ALONG THE ENTIRE LENGTH (EXCEPT AS NOTED BELOW).
- WHERE A CATCH BASIN IS CENTERED ABOUT (2) DRIVEWAYS, THE PROPOSED SWALES SHALL BE DIRECTED FROM EACH DRIVEWAY TO THE PROPOSED CATCH BASIN.
- THE NORTHERN SIDE OF THE DRIVEWAY AT STA. 30+85, RT. CURRENTLY FLOWS TO THE NORTH TO THE EXISTING LOW POINT BETWEEN STA. 32+00 & STA. 33+50, RT. (NO SWALE NEEDED).
- THE SOUTHERN SIDE OF THE DRIVEWAY AT STA. 33+72, RT. CURRENTLY FLOWS TO THE SOUTH TO THE EXISTING LOW POINT BETWEEN STA. 32+00 & STA. 33+50, RT. (NO SWALE NEEDED).
- THE SOUTHERN SIDE OF THE DRIVEWAY AT STA. 35+20, LT. CURRENTLY FLOWS TO THE SOUTH TO THE EXISTING LOW POINT BETWEEN STA. 33+00 & STA. 34+50, LT. (NO SWALE NEEDED).
- THE AREA BETWEEN STA. 39+00, RT. & 41+75, RT. CURRENTLY DRAINS AWAY FROM THE ROADWAY (NO SWALE NEEDED).
- SEE THE PERTINENT PLAN VIEW SHEETS & DRAINAGE PROFILES FOR ADDITIONAL INFORMATION.

THE SLOPE OF THE PROPOSED SWALES SHALL APPROXIMATELY FOLLOW THE SLOPE OF THE EXISTING ROADWAY.

LINEAR GRADING FOR THE SWALES SHALL INCLUDE THE REMOVAL OF ALL MATERIAL FROM THE SITE AND THE PROPER DISPOSAL OF ALL REMOVED MATERIAL.

ITEM 670 DITCH EROSION PROTECTION SHALL BE APPLIED TO THE AREAS OF EARTH DISTURBED DURING LINEAR GRADING AND WITHIN THE FLOW LINE OF THE EXISTING SWALE. A MAXIMUM WIDTH OF (2) FEET FOR THE DITCH EROSION PROTECTION IS ANTICIPATED.

PROVIDE ALL NECESSARY 659 ITEMS (SEEDING, MULCHING, ETC.) AS FURTHER DESCRIBED IN THE GENERAL NOTES ON SHEET 12. ALL SEEDING SHALL BE A CLASS 1 SEED MIX AS FURTHER DESCRIBED IN THE GENERAL NOTES ON SHEET 12.

THE CONTRACTOR SHALL MINIMIZE THE AMOUNT OF ADJACENT LAWN AREAS DISTURBED DURING LINEAR GRADING FOR THE SWALES. ANY AREAS DISTURBED BEYOND THE LIMITS OF THE LINEAR GRADING FOR THE SWALES SHALL BE PROPERLY PREPARED, SEEDED & MULCHED, ETC. AT THE DIRECTION OF THE CONSTRUCTION ENGINEER.

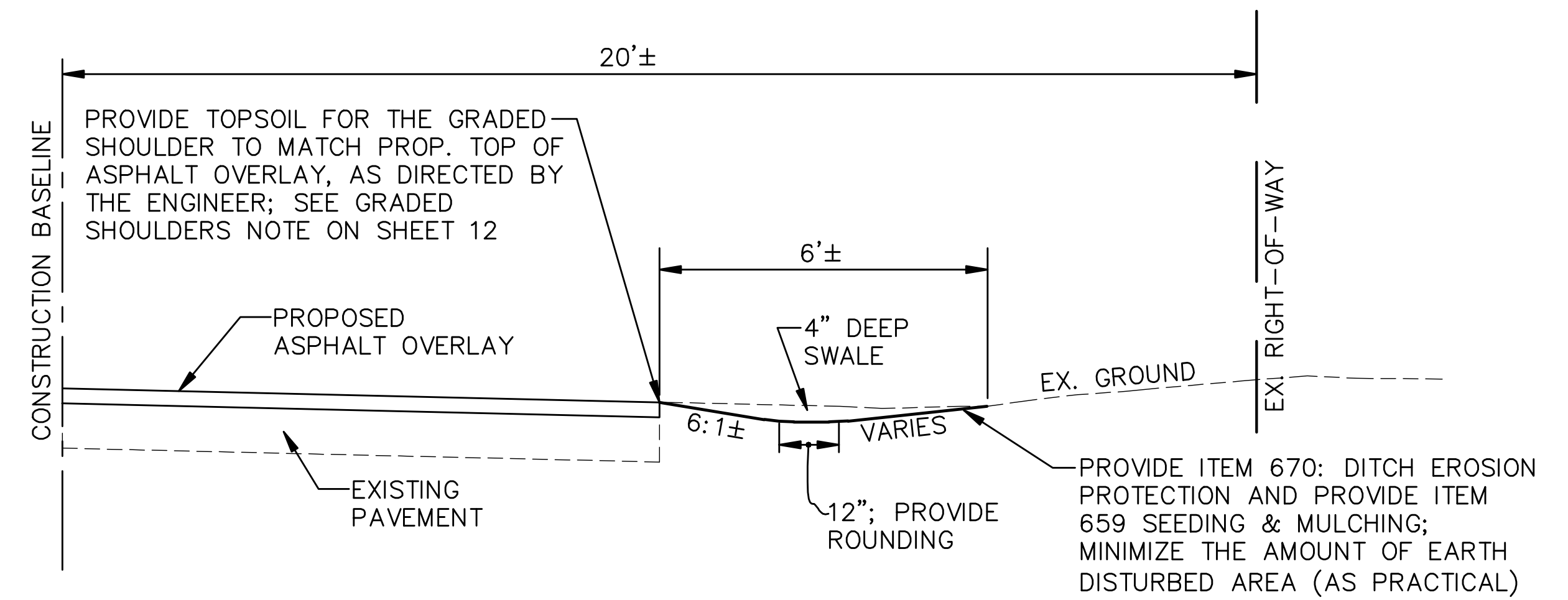
THE CONTRACTOR SHALL TAKE CARE SO AS NOT TO DAMAGE ANY EXISTING ROADWAY/DRIVEWAY PAVEMENT, ANY EXISTING UTILITY FACILITIES, AND/OR ANY SURROUNDING TOPOGRAPHIC FEATURES. ANY DAMAGE TO THE AFOREMENTIONED SHALL BE REPAIRED BY THE CONTRACTOR AT THEIR OWN EXPENSE.

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS DESCRIBED ABOVE SHALL BE INCLUDED IN THE PER FOOT UNIT PRICE BID FOR ITEM 209: LINEAR GRADING, AS PER PLAN.

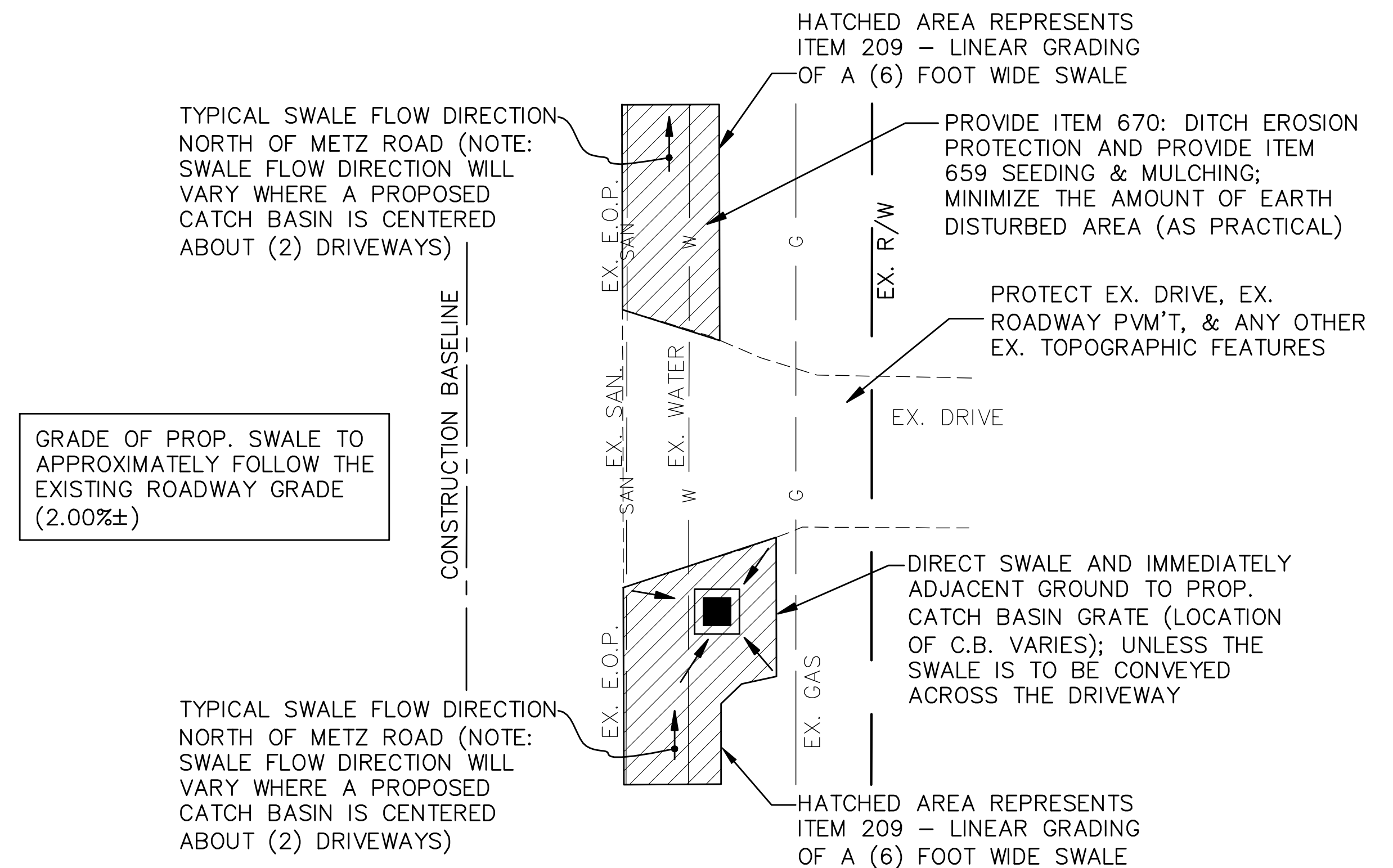
NOTE: THE PERTINENT 659 & 670 ITEMS WILL BE PAID FOR SEPARATELY.

NOTE: SEE SUB-SUMMARIES IN THE PERTINENT PLAN VIEW SHEETS FOR QUANTITIES.

THE DETAILS SHOWN ON THIS SHEET ARE A GENERAL REPRESENTATION OF THE WORK TO BE PERFORMED IN ORDER TO ALLOW FOR THE PROPER CONVEYANCE OF DRAINAGE WITHIN THE PROPOSED SWALES WHICH WILL BE DIRECTED TO THE PROPOSED CATCH BASIN GRATES AND/OR ACROSS EXISTING DRIVEWAY APPROACHES. THE SWALE FLOW DIRECTION, UTILITY LOCATIONS, ETC. MAY VARY. SEE THE PERTINENT PLAN VIEW SHEETS AND DRAINAGE PROFILES FOR ADDITIONAL INFORMATION.



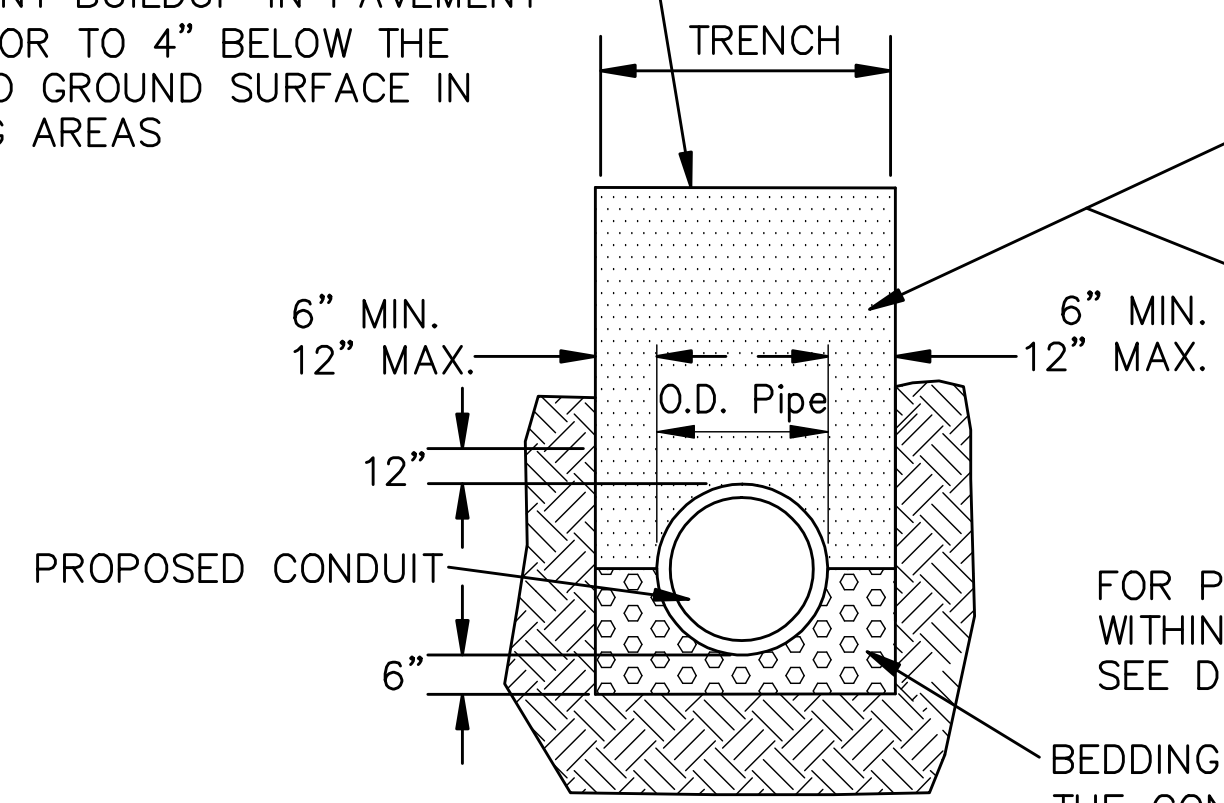
SECTION VIEW
NOT TO SCALE



PLAN VIEW
NOT TO SCALE

- TYPICAL SWALE DETAILS (NORTH OF METZ ROAD)**
- (STA. 27+82 TO STA. 30+75, RIGHT SIDE)
 - (STA. 33+83 TO STA. 38+66, RIGHT SIDE)
 - (STA. 42+02 TO STA. 42+75, RIGHT SIDE)
 - (STA. 35+30 TO STA. 43+12, LEFT SIDE)

BACKFILL TO BE PLACED TO THE BOTTOM OF CONCRETE OR ASPHALT PAVEMENT BUILDUP IN PAVEMENT AREAS OR TO 4" BELOW THE FINISHED GROUND SURFACE IN SEEDING AREAS



ALL TRENCHES UNDER EXISTING DRIVES, ROADWAYS, OR WITHIN 2 FT. OF PAVED AREAS SHALL BE BACKFILLED WITH COMPACTED ITEM 304: AGGREGATE BASE, AS PER PLAN AND AS PER O.D.O.T. ITEM 611, TYPE "B" CONDUIT.

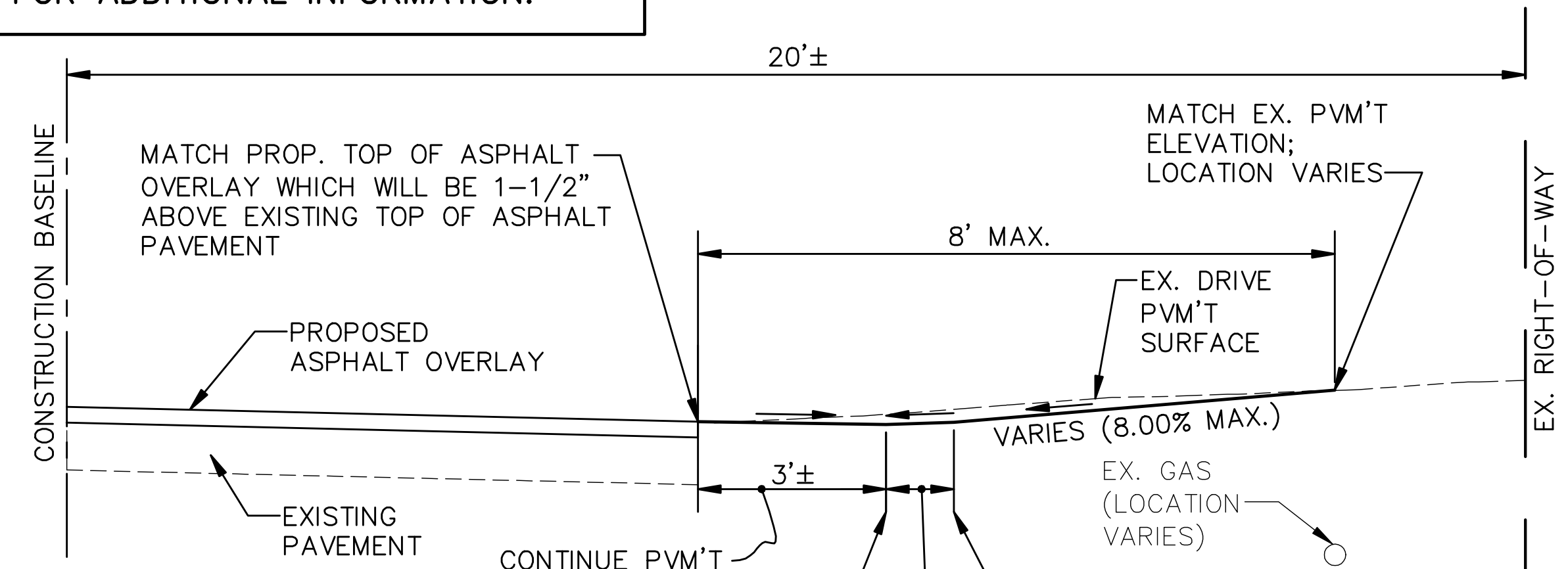
ALL TRENCHES NOT UNDER OR WITHIN 2' OF ROADWAY OR DRIVE PAVEMENT MAY BE BACKFILLED AND COMPACTED WITH ACCEPTABLE EXCAVATED MATERIAL APPROVED BY THE ENGINEER AS PER ODOT ITEM 611, TYPE "C" CONDUIT.

FOR PAVEMENT REPLACEMENT WITHIN ALL CONDUIT TRENCHES, SEE DETAILS TO RIGHT

BEDDING TO BE DETERMINED IN THE CONTRACTOR'S INSTALLATION PLAN PER ITEM 611

TYPICAL STORM SEWER CONDUIT TRENCH DETAIL

THE DETAIL SHOWN BELOW IS A GENERAL REPRESENTATION OF THE WORK TO BE PERFORMED IN ORDER TO ALLOW FOR THE PROPER CONVEYANCE OF DRAINAGE WITHIN THE PROPOSED SWALES WHICH WILL BE DIRECTED ACROSS EXISTING DRIVEWAY APPROACHES. THE SWALE FLOW DIRECTION, UTILITY LOCATIONS, ETC. MAY VARY. SEE THE PERTINENT PLAN VIEW SHEETS AND DRAINAGE PROFILES FOR ADDITIONAL INFORMATION.



THE INTENT OF THIS DETAIL IS TO CREATE A SUMP AT A SPECIFIC DRIVEWAY APPROACH IN ORDER TO CONVEY DRAINAGE RUNOFF FROM THE NEW ROADWAY SWALE ON ONE SIDE OF THE DRIVEWAY APPROACH, THROUGH/ACROSS THE DRIVEWAY APPROACH, AND TO THE NEW SWALE AND/OR EXISTING SWALE ON THE OTHER SIDE OF THE DRIVEWAY APPROACH

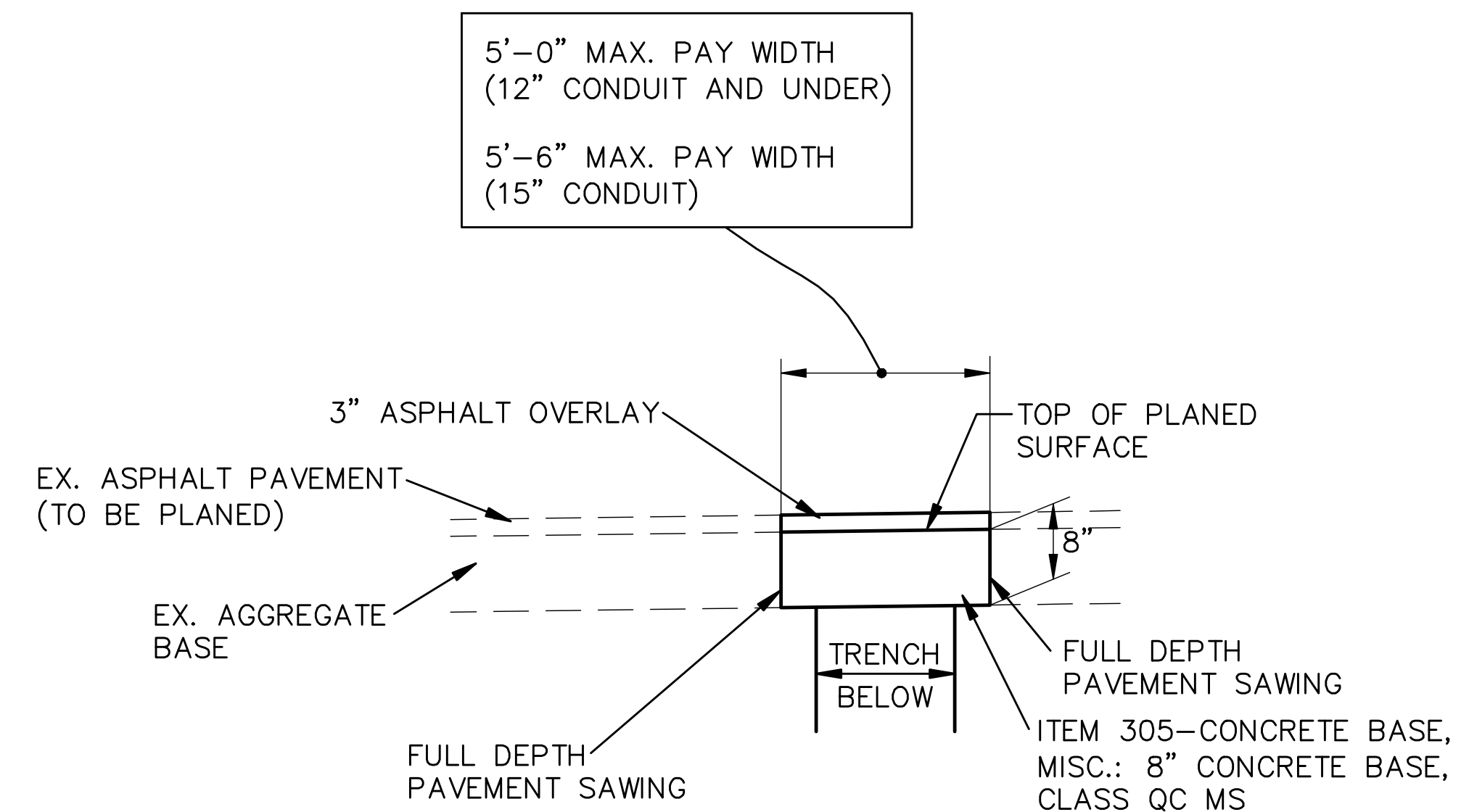
CONTINUE PVM'T CROSS SLOPE TO GRADE BREAK LOW POINT (1.60%±)
 GRADE BREAK TO CREATE DRIVEWAY LOW POINT/SUMP; PROVIDE ROUNDING (MAX. GRADE BREAK OF 5%)

ADDITIONAL GRADE BREAK POINT, IF NECESSARY (MAX. GRADE BREAK OF 5%; LOCATION VARIES)
 MAX. SLOPE OF 3.40% TO GRADE BREAK LOW POINT; DISTANCE VARIES

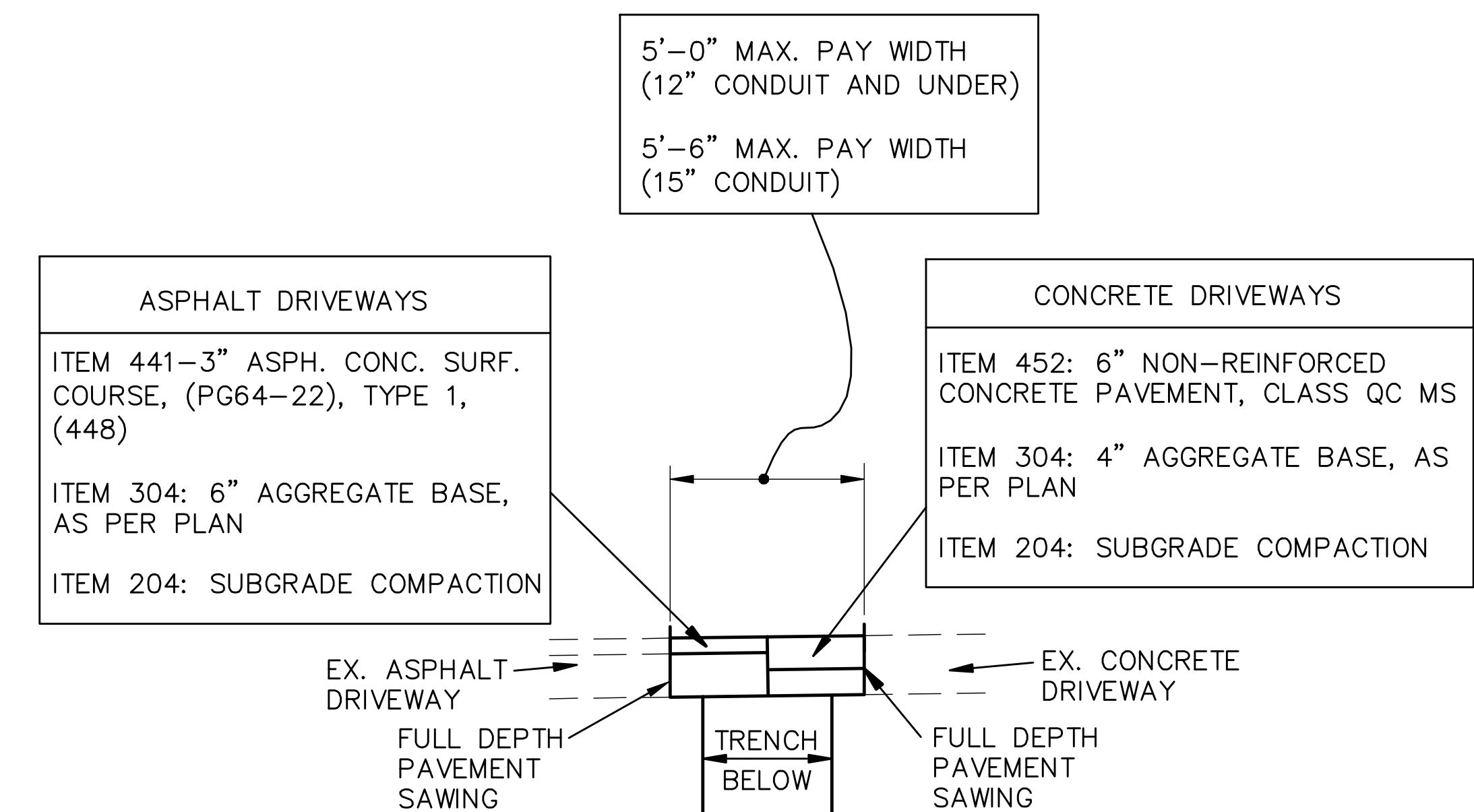
EX. PAVEMENT TYPE	PAVEMENT REPLACEMENT
ASPHALT DRIVE OR SLAG DRIVE	ITEM 204-SUBGRADE COMPACTION ITEM 304-6" AGGREGATE BASE, AS PER PLAN ITEM 441-3" ASPH. CONC. SURF. COURSE, (PG64-22), TYPE 1, (448)
CONCRETE DRIVE	ITEM 204-SUBGRADE COMPACTION ITEM 304-4" AGGREGATE BASE, AS PER PLAN ITEM 452-6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS

NOTE: PROVIDE FULL DEPTH PAVEMENT SAWING PRIOR TO REMOVING THE EXISTING DRIVEWAY APPROACH PAVEMENT FOR ALL ASPHALT & CONCRETE DRIVEWAYS

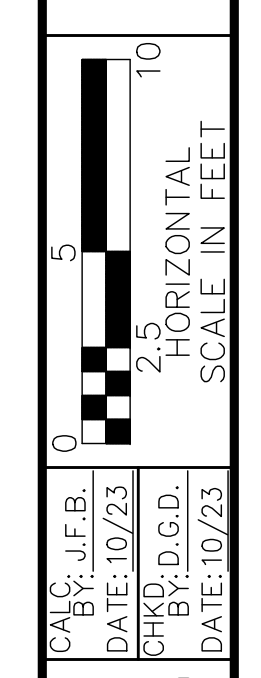
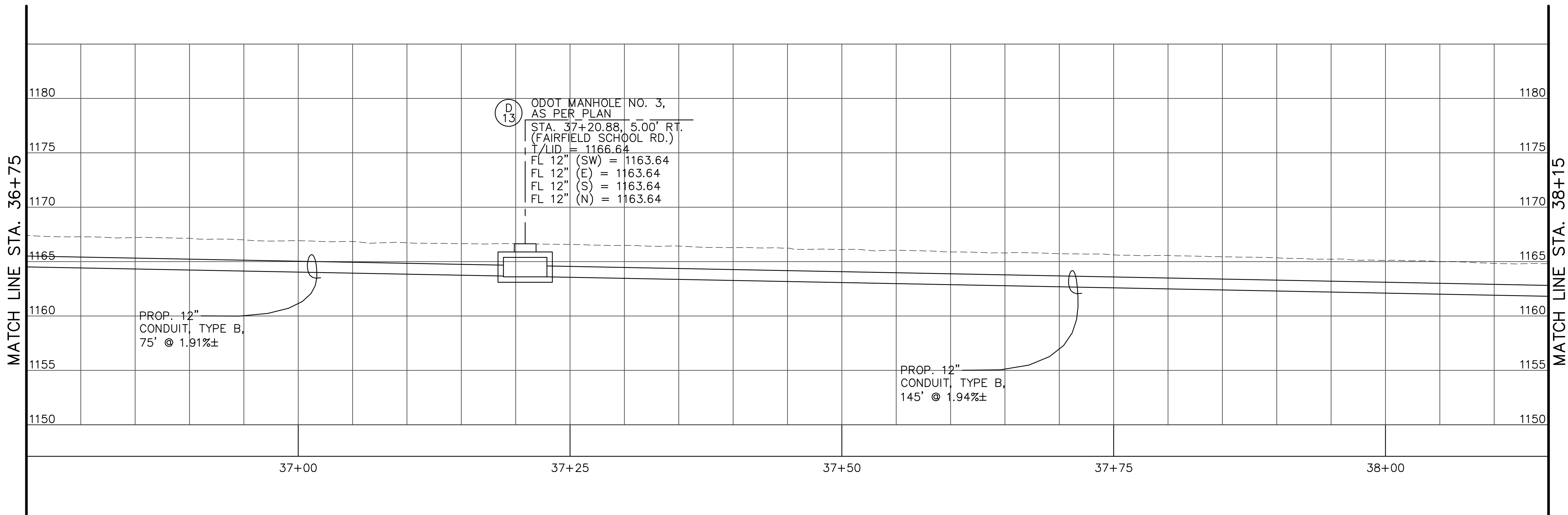
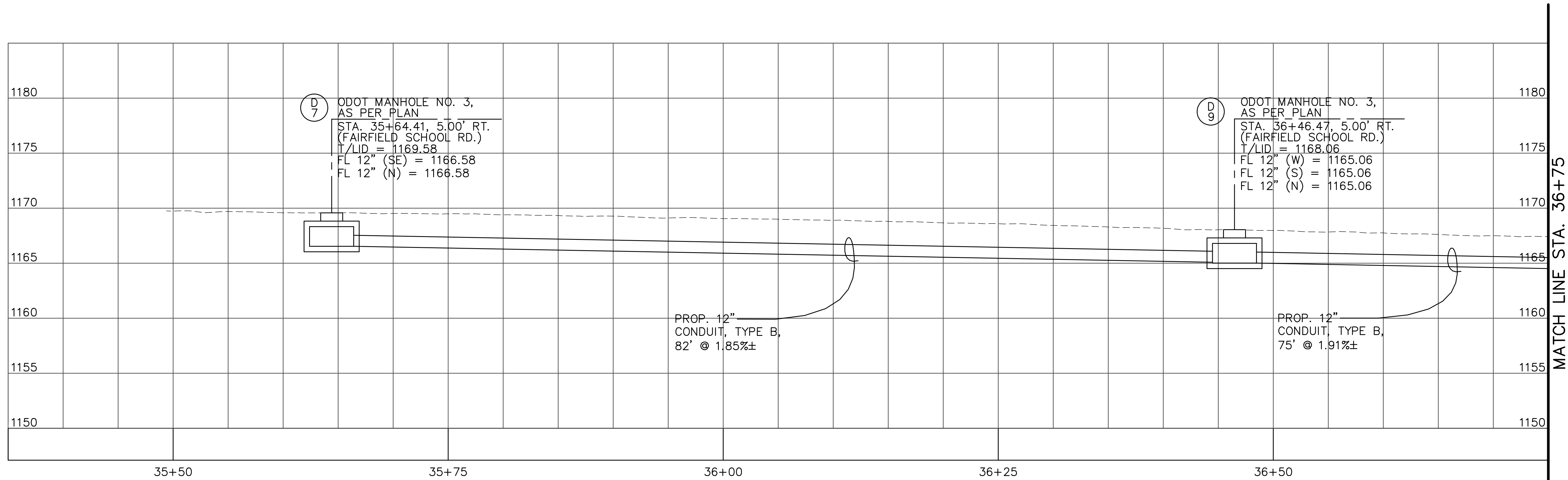
SECTION VIEW - TYPICAL DRIVEWAY APPROACH SUMP DETAIL
 (DRIVEWAYS BETWEEN STA. 27+82 & STA. 30+75, RIGHT SIDE)
 NOT TO SCALE



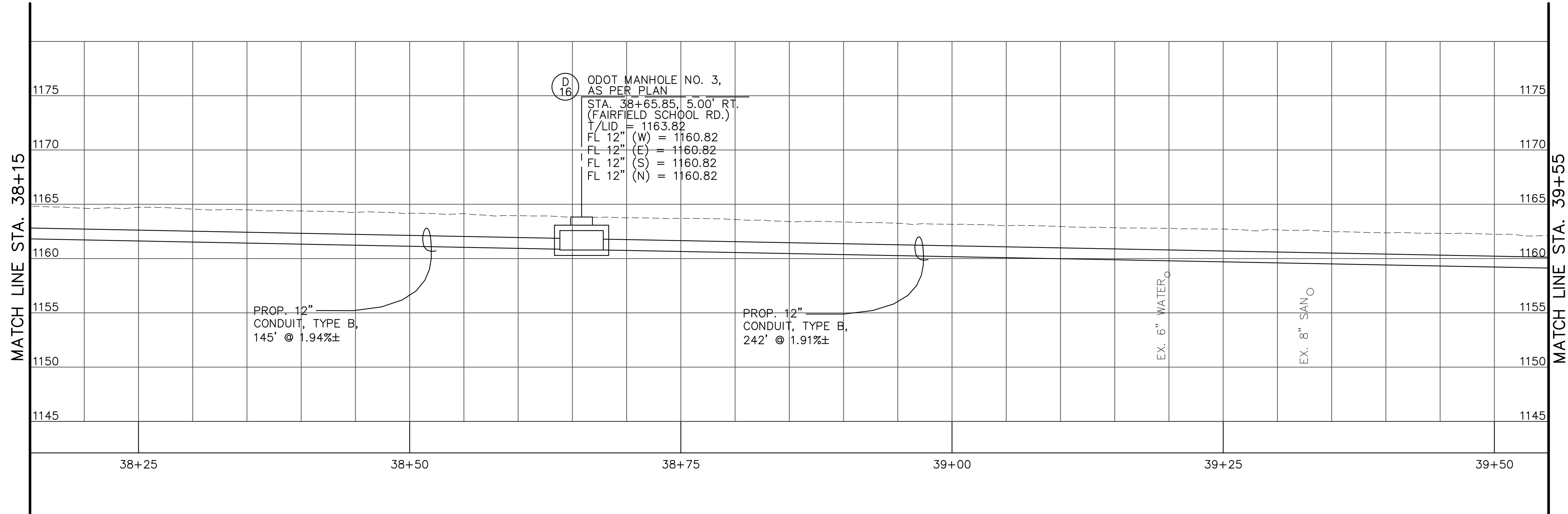
ROADWAY PAVEMENT REPLACEMENT - PROPOSED CONDUIT TRENCHES
 ITEM 305-CONCRETE BASE, MISC.: 8" CONCRETE BASE, CLASS QC MS



DRIVEWAY PAVEMENT REPLACEMENT - PROPOSED CONDUIT TRENCHES



COL - FAIRFIELD SCH. RD. & LISBON ST.
 DRAINAGE PROFILE & DRAINAGE DETAILS
 STA. 35+64.41 TO STA. 38+15.00 (FAIRFIELD SCHOOL RD.)



(D)
 16
 ODOT MANHOLE NO. 3,
 AS PER PLAN
 STA. 38+65.85, 5.00' RT.
 (FAIRFIELD SCHOOL RD.)
 T/LID = 1163.82
 FL 12" (W) = 1160.82
 FL 12" (E) = 1160.82
 FL 12" (S) = 1160.82
 FL 12" (N) = 1160.82

PROP. 12"
 CONDUIT, TYPE B,
 145' @ 1.94%±

PROP. 12"
 CONDUIT, TYPE B,
 242' @ 1.91%±

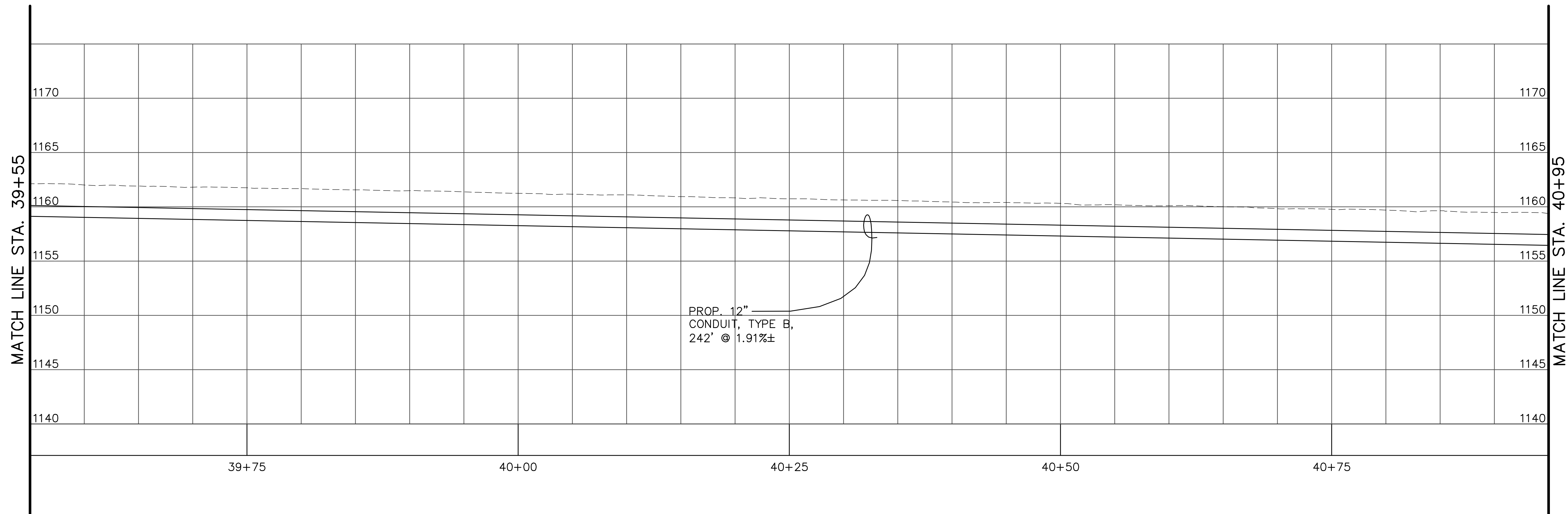
EX. 6" WATER

EX. 8" SAN



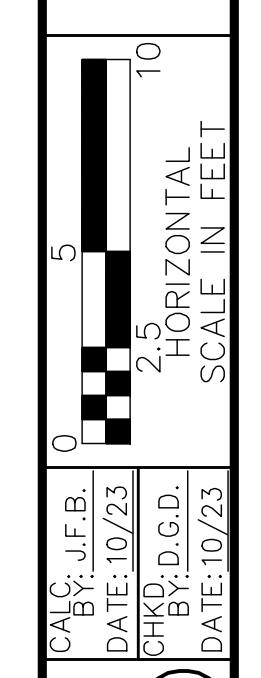
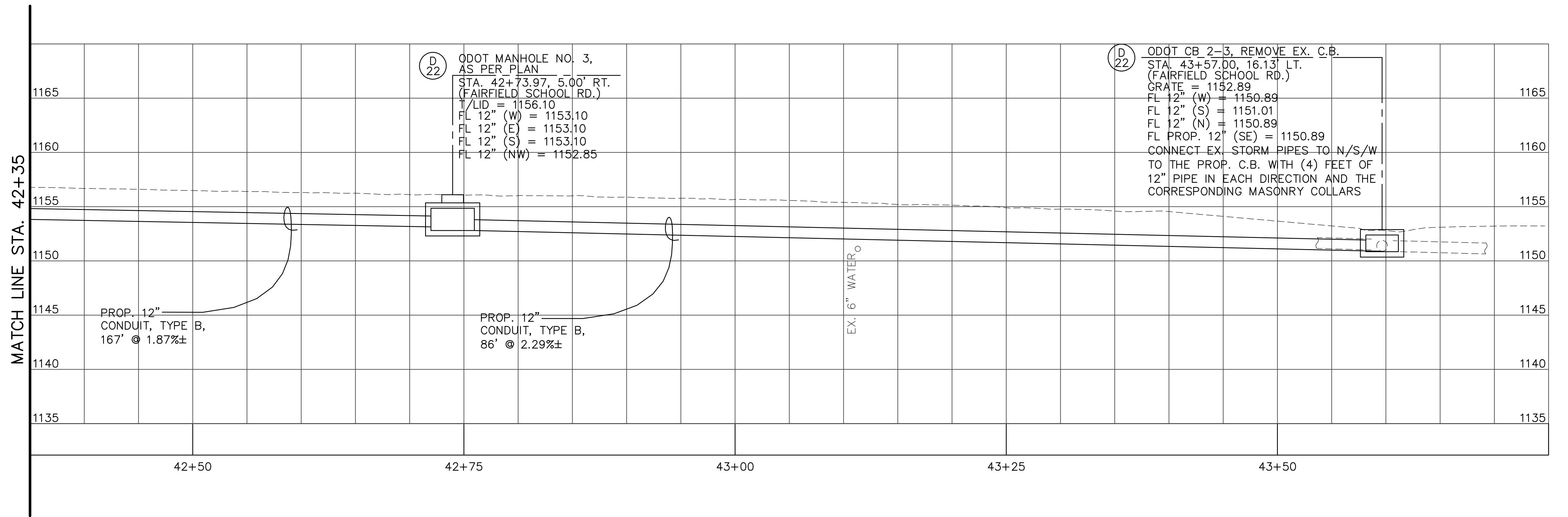
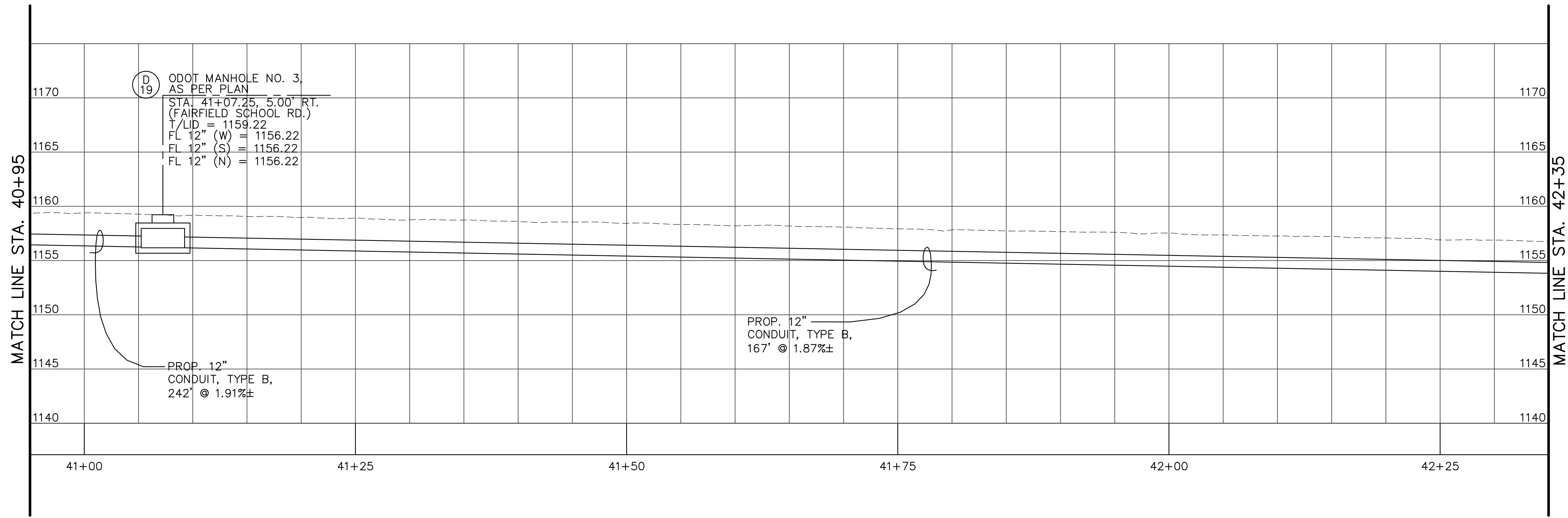
CALC. J.F.B.
 DATE: 10/23
 CHK. BY: D.G.D.
 DATE: 10/23

DRAINAGE PROFILE & DRAINAGE DETAILS
 STA. 38+15.00 TO STA. 40+95.00 (FAIRFIELD SCHOOL RD.)



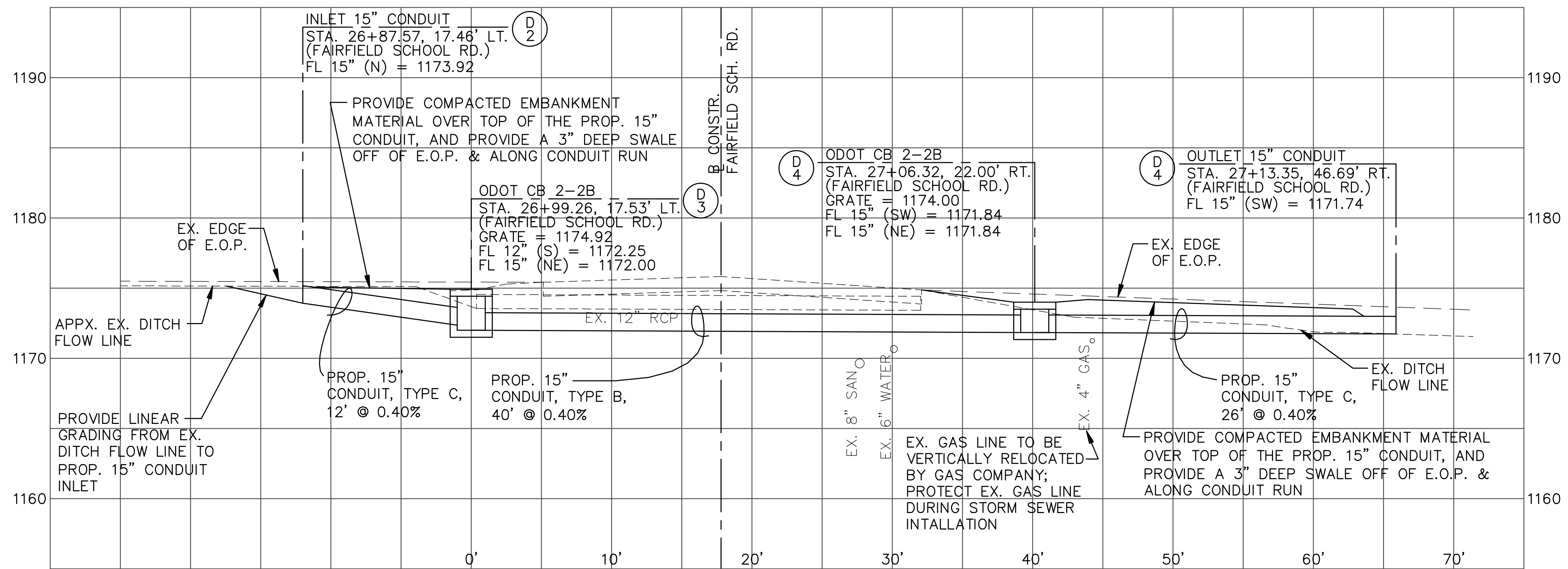
PROP. 12"
 CONDUIT, TYPE B,
 242' @ 1.91%±

COL - FAIRFIELD SCH. RD.
 & LISBON ST.



COL - FAIRFIELD SCH. RD. & LISBON ST. STA. 40+95.00 TO STA. 43+57.00 (FAIRFIELD SCHOOL RD.)

SEE PLAN VIEW ON SHEET 26
FOR WORK BEING DONE WITHIN
THE RIGHT OF WAY OF FAIRFIELD
SCHOOL RD. AND METZ RD.



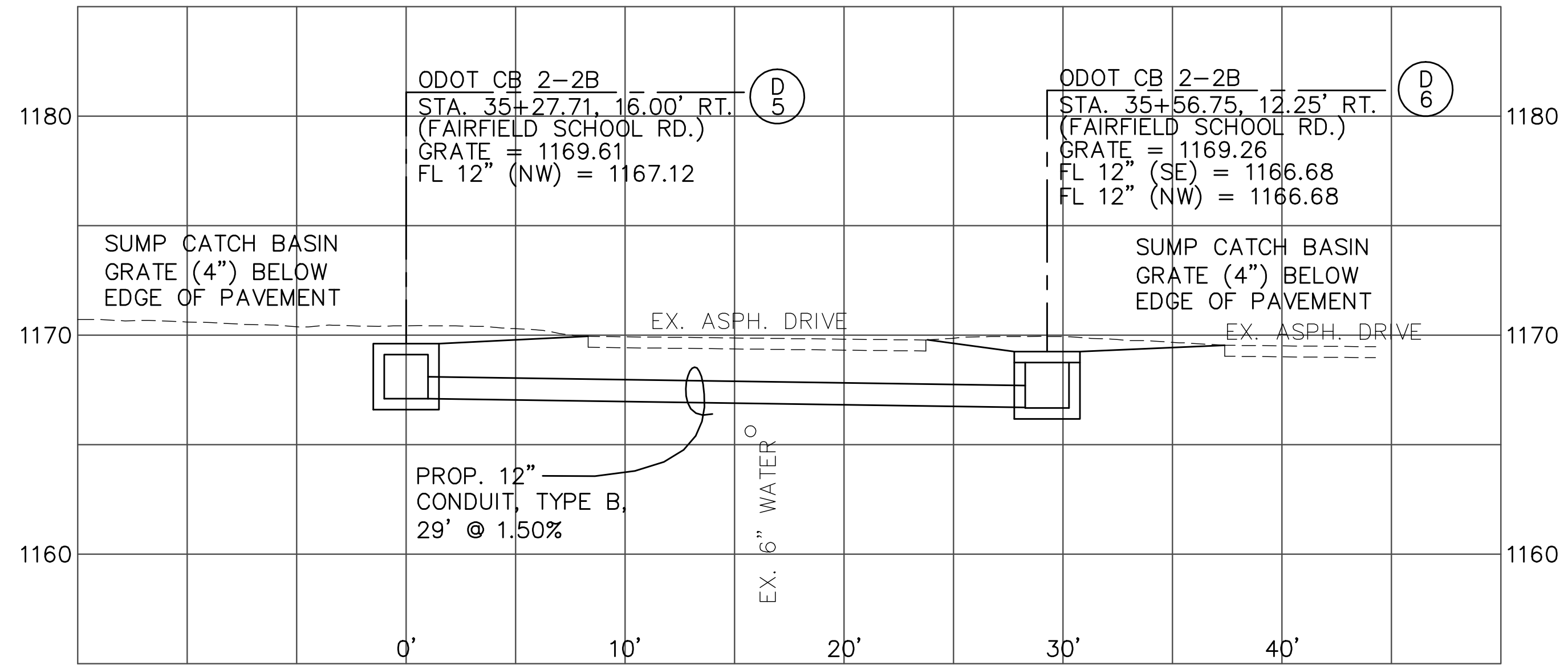
DRAINAGE PROFILE FROM STA. 26+99.26, 17.53' LT.
TO STA. 27+06.32, 22.00' RT.

CALC: J.F.B.
DATE: 10/23
CHKD: D.G.D.
DATE: 10/23

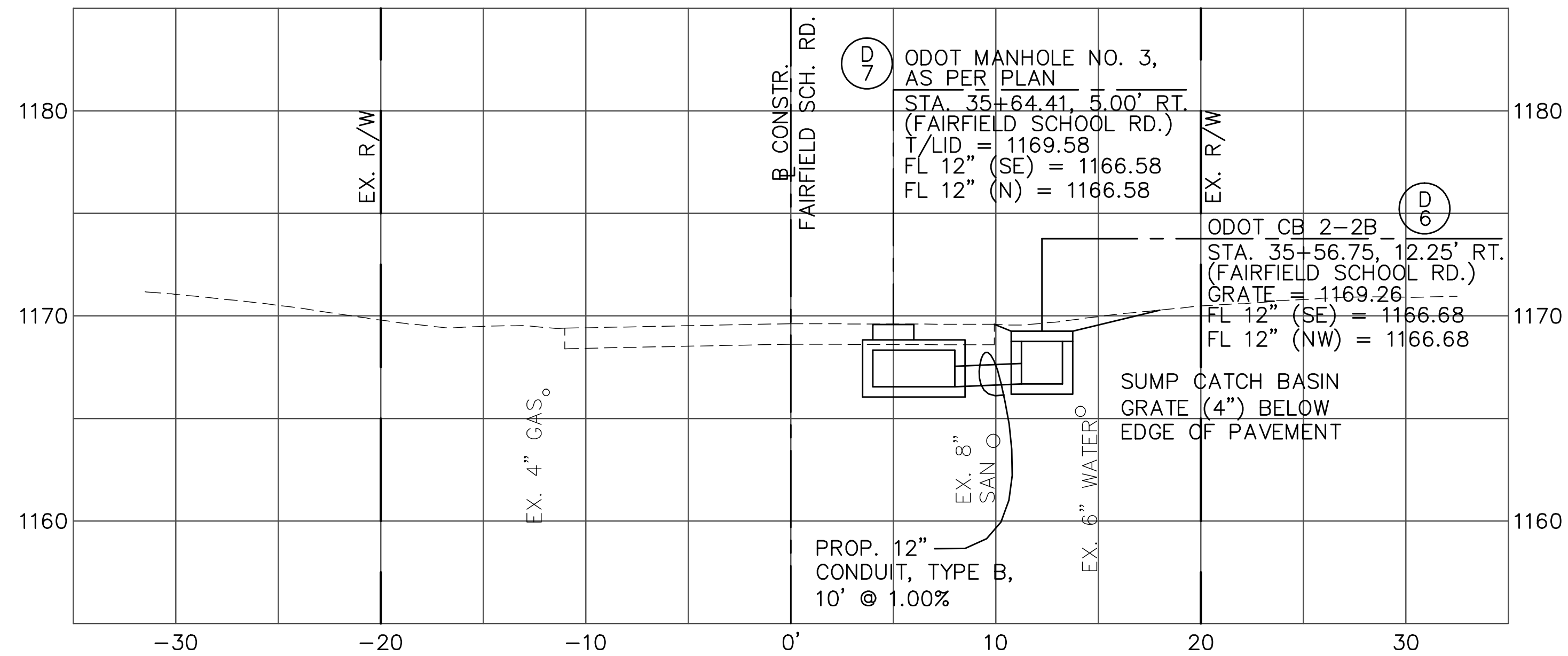
0 5 10
2.5
HORIZONTAL
SCALE IN FEET

DRAINAGE PROFILE & DRAINAGE DETAILS
(FAIRFIELD SCHOOL RD.)

COL - FAIRFIELD SCH. RD.
& LISBON ST.



DRAINAGE PROFILE FROM STA. 35+27.71, 16.00' RT.
TO STA. 35+56.75, 12.25' RT.



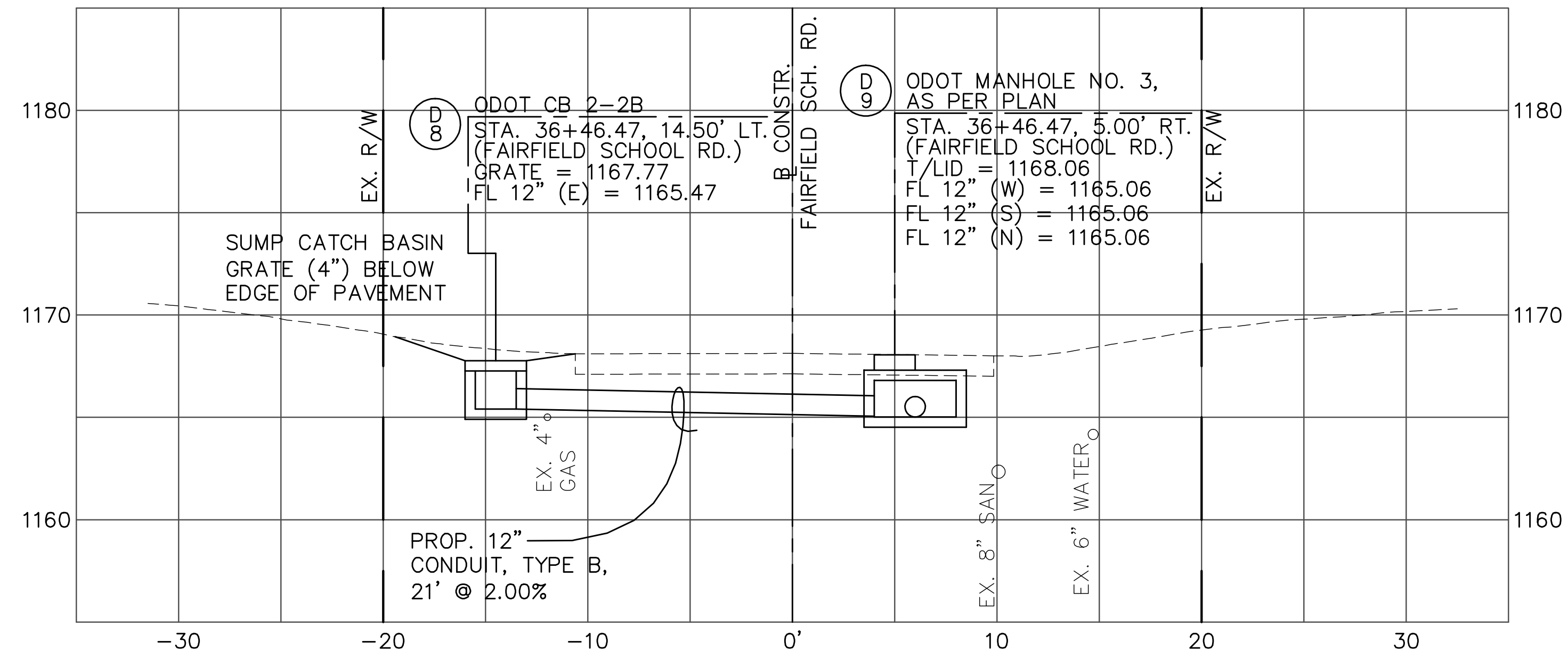
DRAINAGE PROFILE FROM STA. 35+56.75, 12.25' RT.
TO STA. 35+64.41, 5.00' RT.

CALC: J.F.B.
DATE: 10/23
CHKD: D.G.D.
BY: D.G.D.
DATE: 10/23

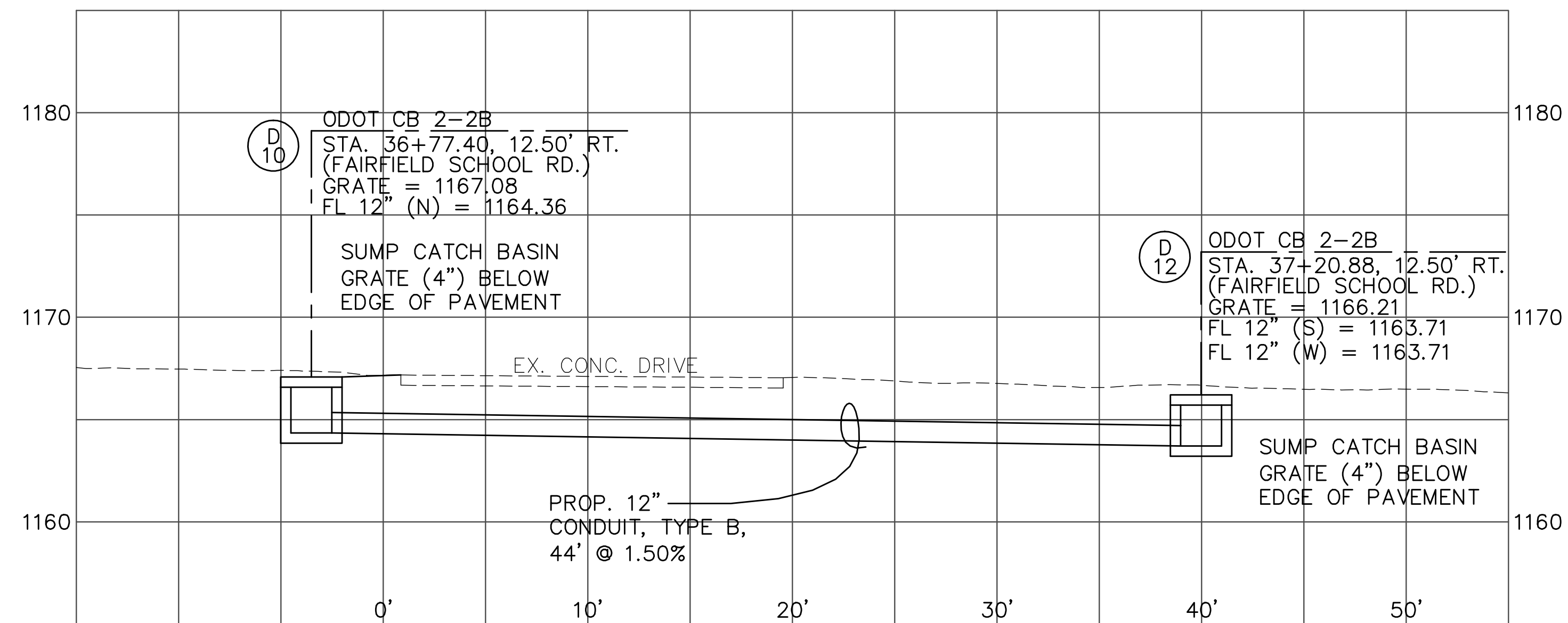
0 5 10
2.5
HORIZONTAL
SCALE IN FEET

DRAINAGE PROFILE & DRAINAGE DETAILS
(FAIRFIELD SCHOOL RD.)

COL - FAIRFIELD SCH. RD.
& LISBON ST.



DRAINAGE PROFILE AT STA. 36+46.47

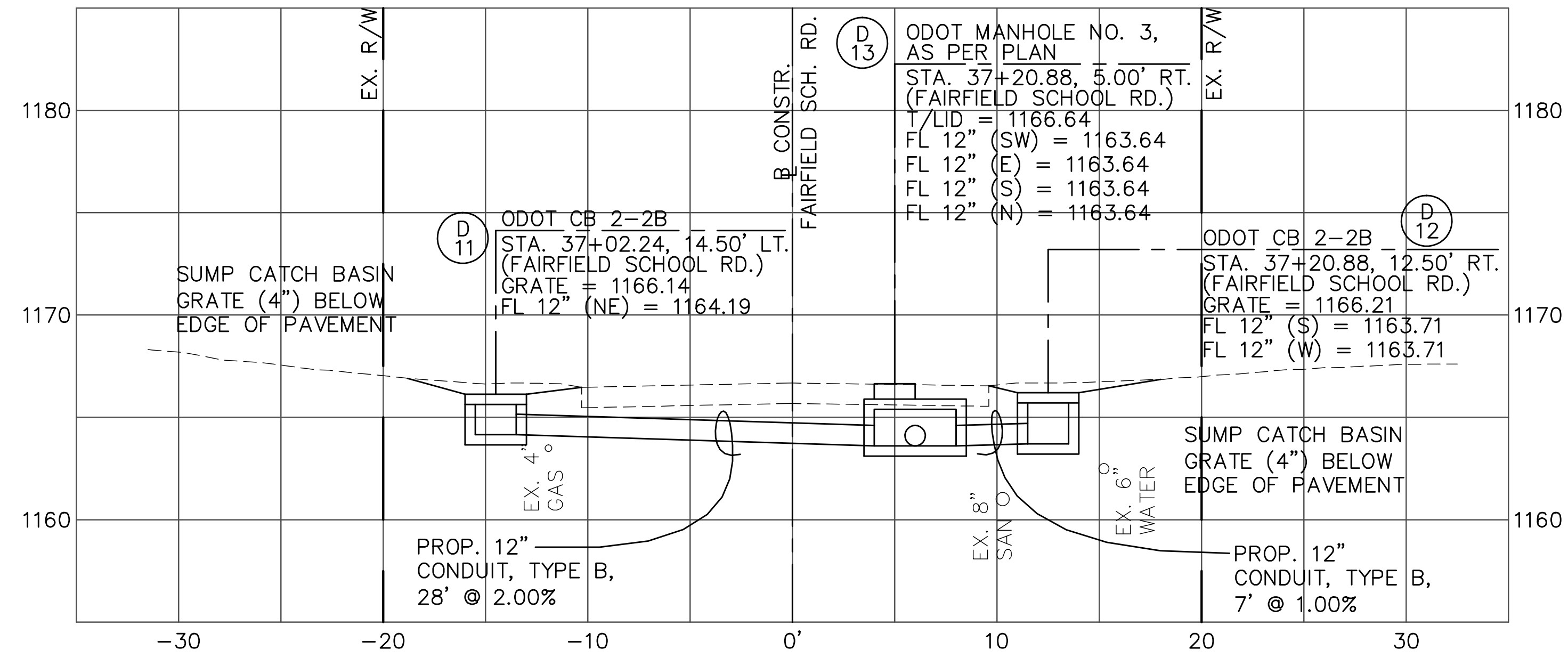


DRAINAGE PROFILE FROM STA. 36+77.40, 12.50' RT.
TO STA. 37+20.88, 12.50' RT.

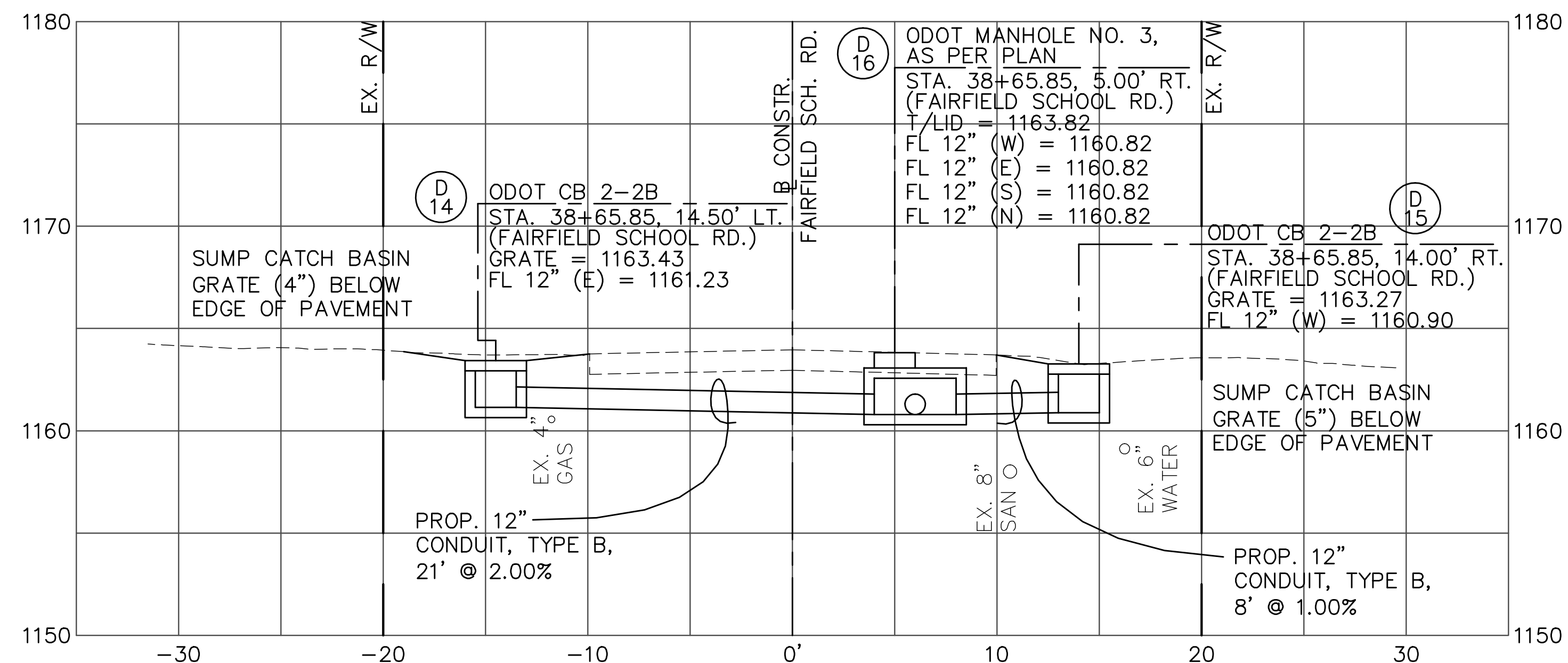
CALC: J.F.B.
DATE: 10/23
CHKD: D.G.D.
DATE: 10/23

DRAINAGE PROFILE & DRAINAGE DETAILS
(FAIRFIELD SCHOOL RD.)

COL - FAIRFIELD SCH. RD.
& LISBON ST.



DRAINAGE PROFILE FROM STA. 37+02.24, 14.50' LT. TO STA. 37+20.88, 12.50' RT.

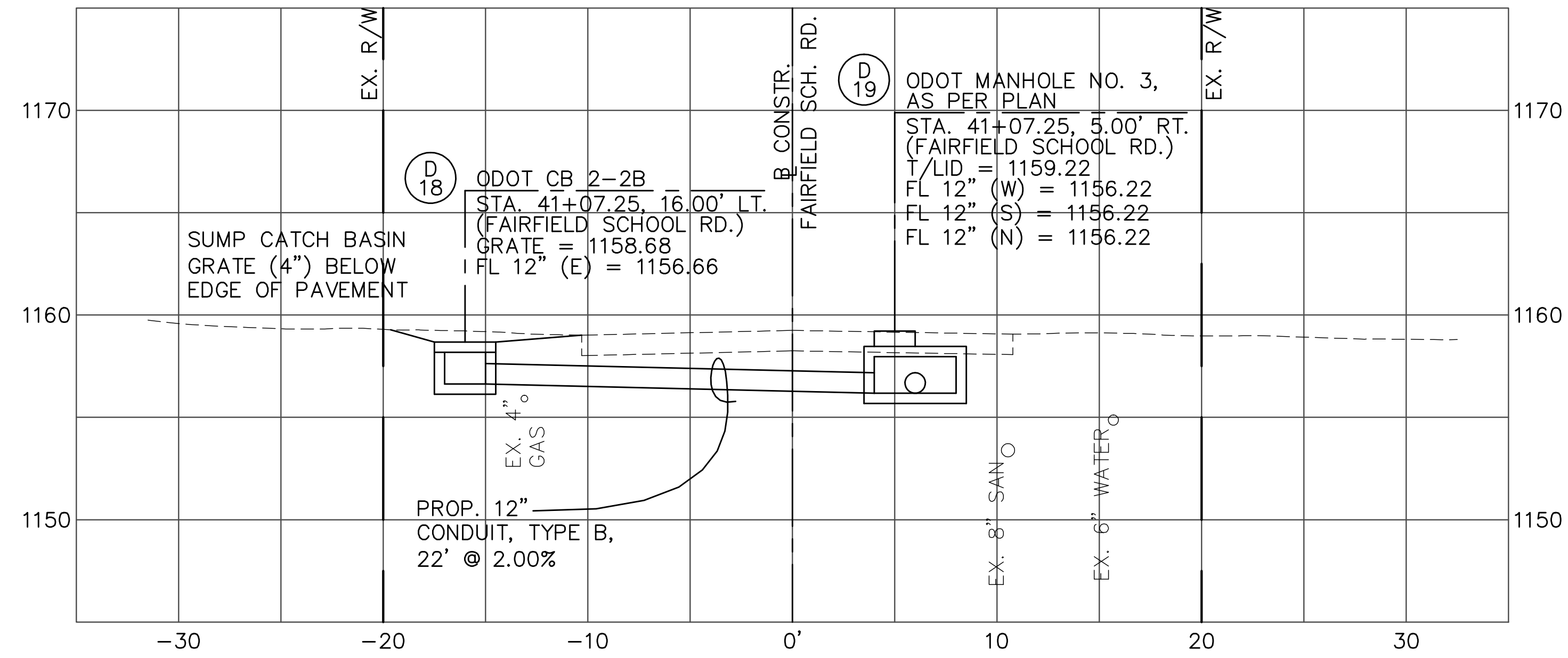


DRAINAGE PROFILE AT STA. 38+65.85

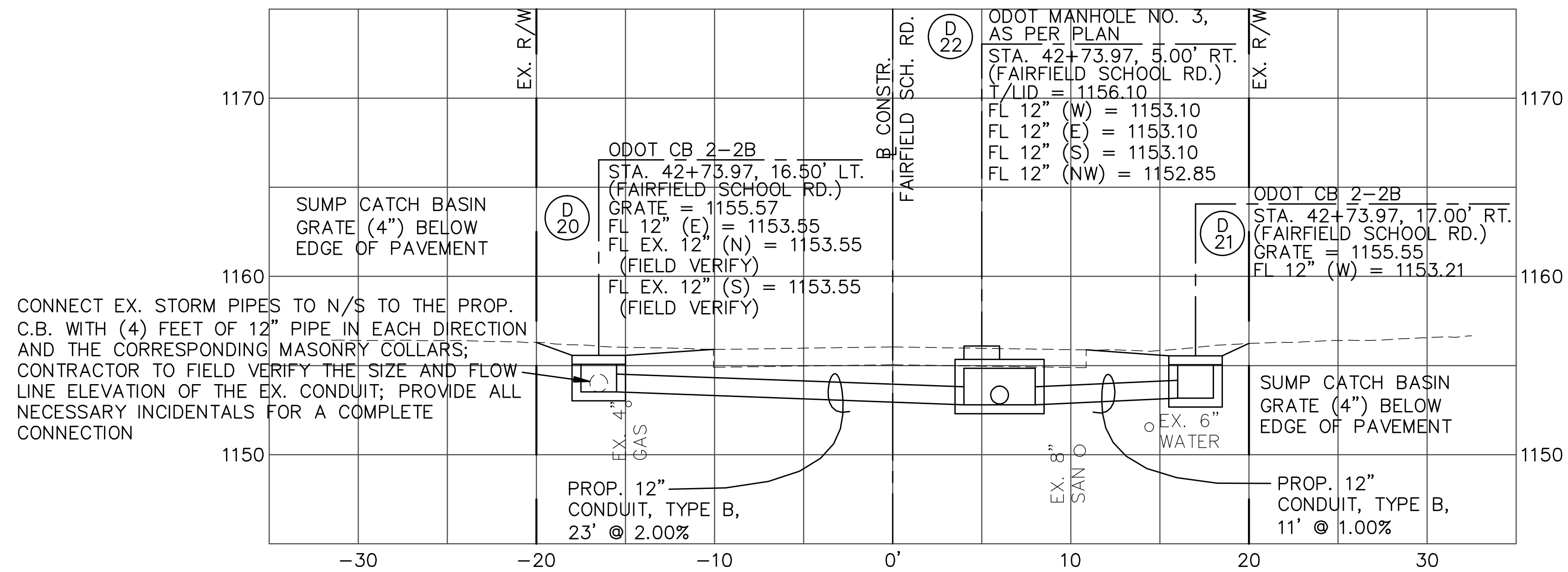
CALC: J.F.B.
DATE: 10/23
CHKD: D.G.D.
BY: D.G.D.
DATE: 10/23

DRAINAGE PROFILE & DRAINAGE DETAILS
(FAIRFIELD SCHOOL RD.)

COL - FAIRFIELD SCH. RD.
& LISBON ST.



DRAINAGE PROFILE AT STA. 41+07.25



CONNECT EX. STORM PIPES TO N/S TO THE PROP.
 C.B. WITH (4) FEET OF 12" PIPE IN EACH DIRECTION
 AND THE CORRESPONDING MASONRY COLLARS;
 CONTRACTOR TO FIELD VERIFY THE SIZE AND FLOW
 LINE ELEVATION OF THE EX. CONDUIT; PROVIDE ALL
 NECESSARY INCIDENTALS FOR A COMPLETE
 CONNECTION

DRAINAGE PROFILE AT STA. 42+73.97

ITEM 611 – MANHOLE, NO. 3, AS PER PLAN

THE FOLLOWING NOTES AND THE DETAILS ON THIS SHEET RELATE TO THE PROPOSED STORM SEWER MANHOLES TO BE INSTALLED ALONG FAIRFIELD SCHOOL ROAD FROM STA. 35+64.41 TO STA. 43+57.00.

ALL STORM SEWER MANHOLES ARE TO BE IN ACCORDANCE WITH ODOT C&MS ITEM 611 AND ODOT STANDARD CONSTRUCTION DRAWINGS MH-1 AND MH-3.

THE PROPOSED STORM SEWER MANHOLES ARE TO BE INSTALLED/ORIENTED SO THAT THE MANHOLE LID IS CENTERED ABOUT THE NORTHBOUND TRAVEL LANE OF THE ROADWAY (I.E. 5'± TO THE RIGHT/EAST OF THE BASELINE OF CONSTRUCTION).

- THE MANHOLE STEPS ARE TO BE INSTALLED IN LINE WITH THE MANHOLE LID.
- THE ACTUAL CENTER OF THE MANHOLE STRUCTURES ARE TO BE INSTALLED 6'± TO THE RIGHT OF THE BASELINE OF CONSTRUCTION.

ALL STATIONING & OFFSETS FOR THE PROPOSED STORM SEWER MANHOLES ARE BASED ABOUT THE CENTER OF THE MANHOLE LID (I.E. NOT THE ACTUAL CENTER OF THE STRUCTURE).

FLAT TOP SLABS SHALL BE UTILIZED FOR THE TOP PORTION OF THE MANHOLES (I.E. NO ECCENTRIC TOPS).

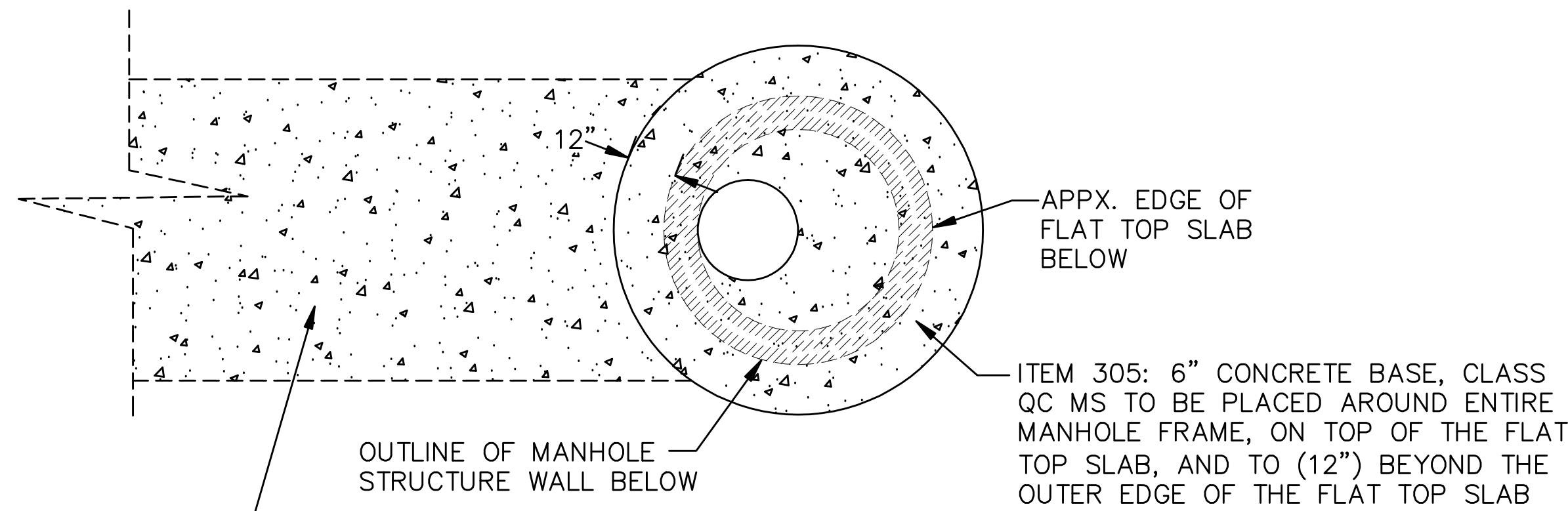
FRAMES FOR ALL MANHOLE LIDS SHALL BE PROPERLY ANCHORED TO THE FLAT TOP SLAB.

FRAMES FOR ALL MANHOLE LIDS SHALL BE PROPERLY ADJUSTED TO MATCH THE PROPOSED ASPHALT OVERLAY FINISHED CROSS SLOPE AND TO MATCH THE PROPOSED ASPHALT OVERLAY FINISHED GRADE.

ITEM 305: 6" CONCRETE BASE, CLASS QC MS SHALL BE PLACED AROUND THE MANHOLE FRAME, ON THE ENTIRE FLAT TOP SLAB, AND TO 12" BEYOND THE OUTER EDGE OF THE FLAT TOP SLAB.

ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY FOR THE COMPLETE INSTALLATION OF THE PROPOSED STORM MANHOLES AS DESCRIBED IN THE NOTES & DETAILS ON THIS SHEET, ODOT C&MS ITEM 611, AND THE PERTINENT STANDARD CONSTRUCTION DRAWINGS SHALL BE INCLUDED IN PER EACH UNIT PRICE BID FOR ITEM 611: MANHOLE NO. 3, AS PER PLAN.

NOTE: SEE THE DETAILS ON THIS SHEET, THE PERTINENT PLAN VIEW SHEETS, AND THE PERTINENT DRAINAGE PROFILES FOR ADDITIONAL INFORMATION.

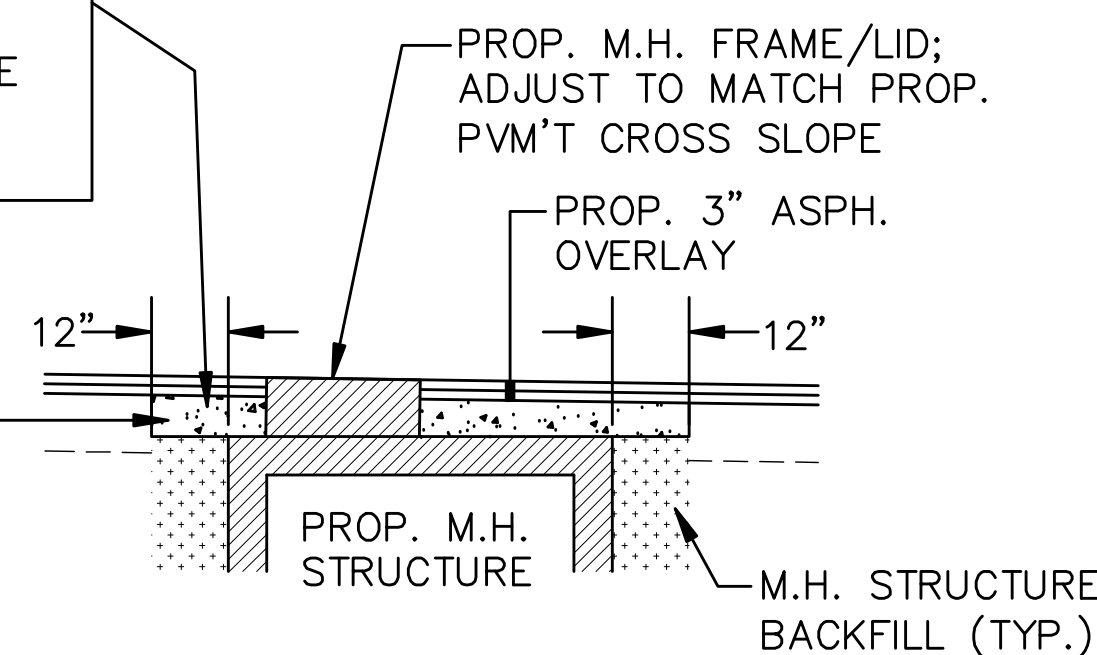


PLAN VIEW

NOT TO SCALE

NOTE: IN AREAS WHERE A PROPOSED STORM CONDUIT WILL BE INSTALLED TO THE LEFT (WEST) SIDE OF THE ROAD, THE CONTRACTOR SHALL PLACE THE CONCRETE AROUND THE MANHOLE MONOLITHICALLY WITH THE CONCRETE PAVEMENT REPLACEMENT FOR THE STORM CONDUIT

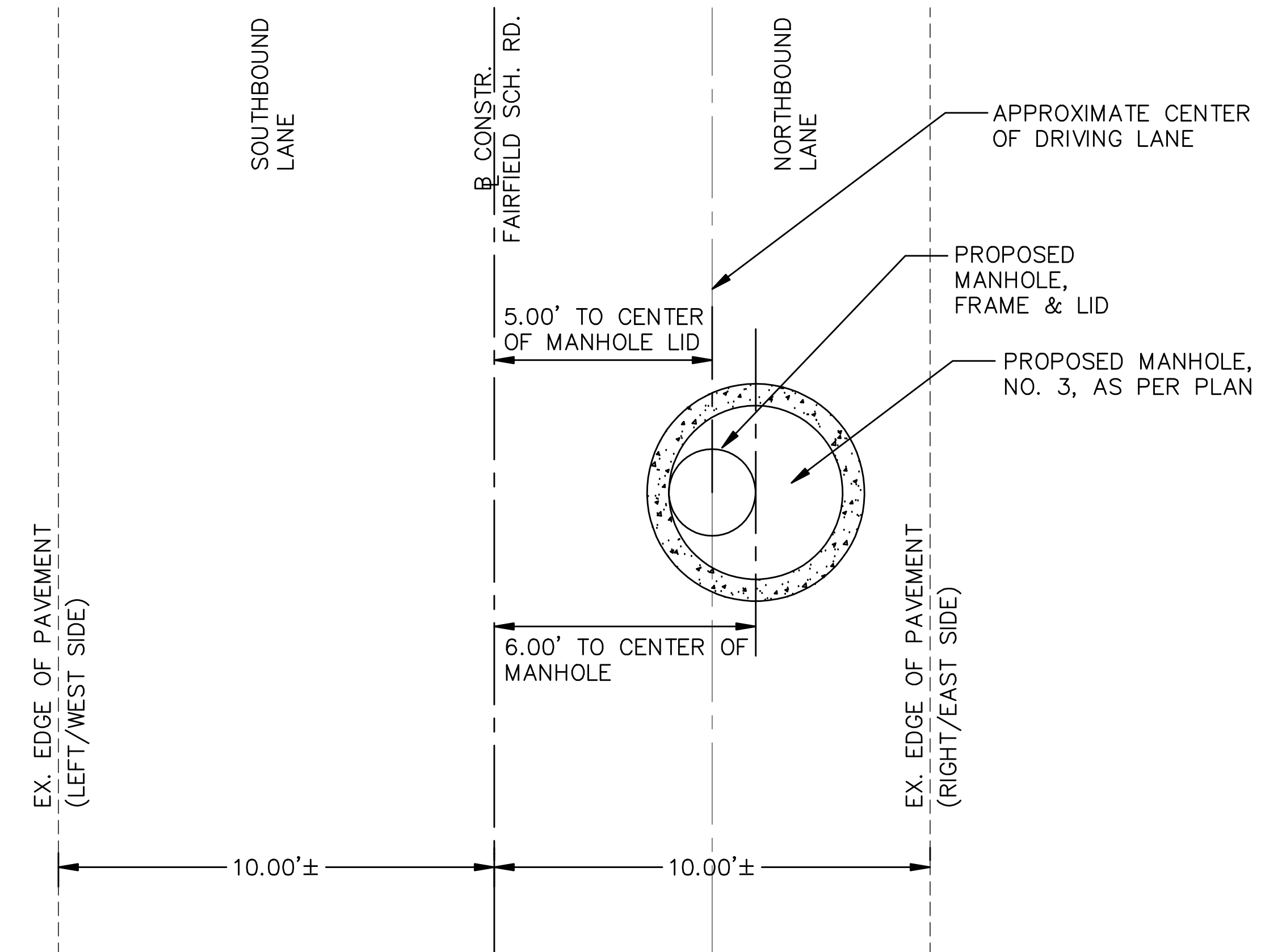
ITEM 305: 6" CONCRETE BASE, CLASS QC MS TO BE PLACED AROUND ENTIRE MANHOLE FRAME, ON TOP OF THE FLAT TOP SLAB, AND TO (12") BEYOND THE OUTER EDGE OF THE FLAT TOP SLAB



SECTION VIEW

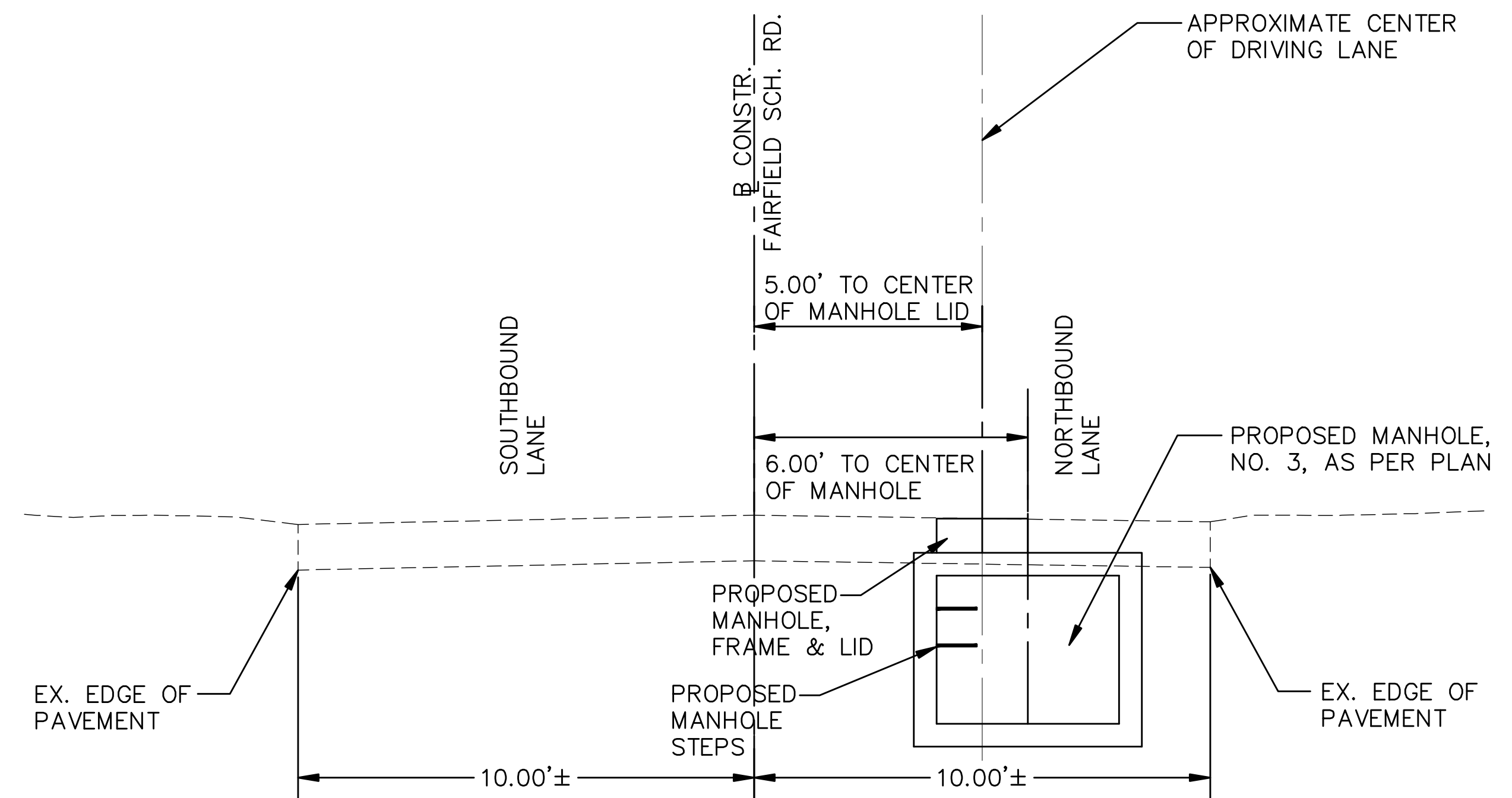
NOT TO SCALE

PROPOSED MANHOLE, NO. 3, AS PER PLAN – TYPICAL PAVEMENT DETAIL



PROPOSED MANHOLE, NO. 3, AS PER PLAN – TYPICAL PLAN VIEW

NOT TO SCALE



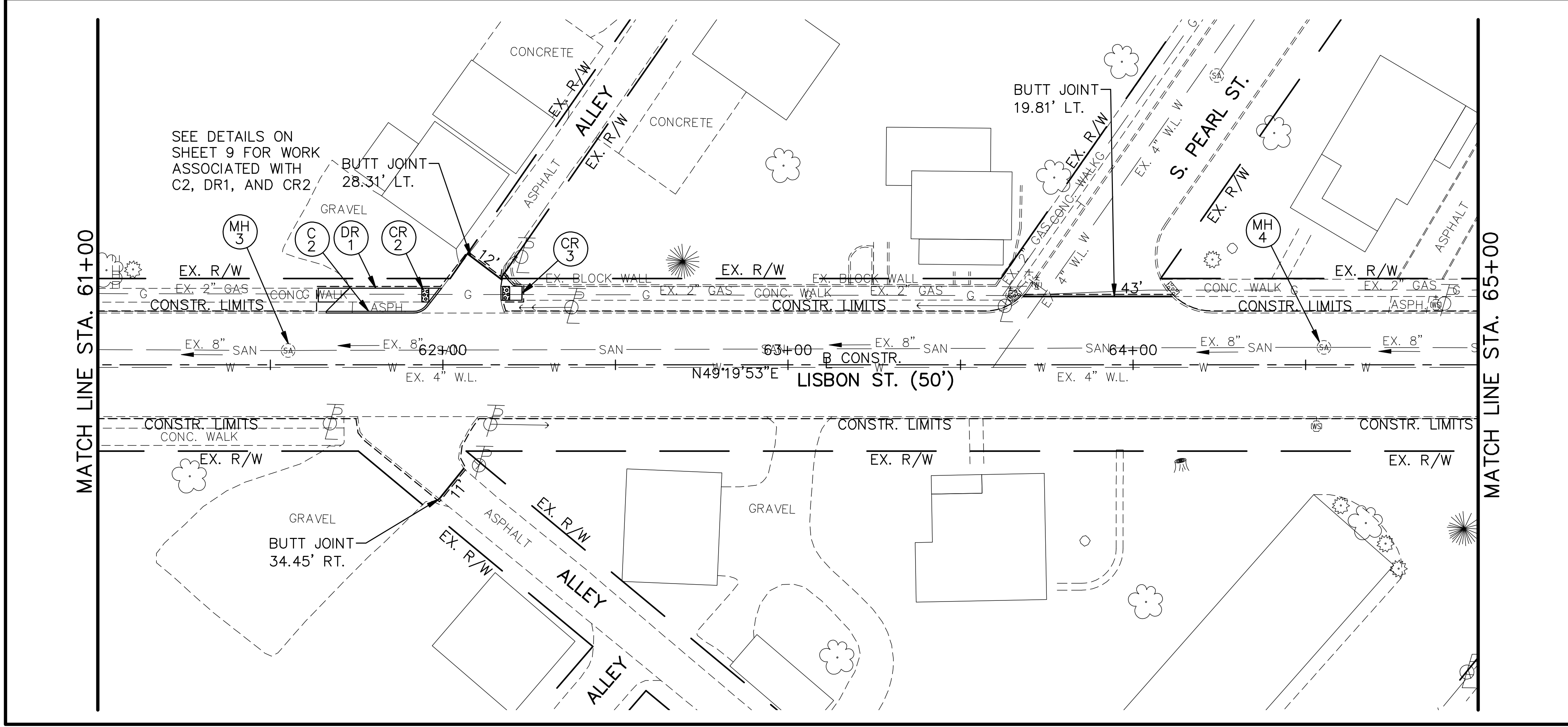
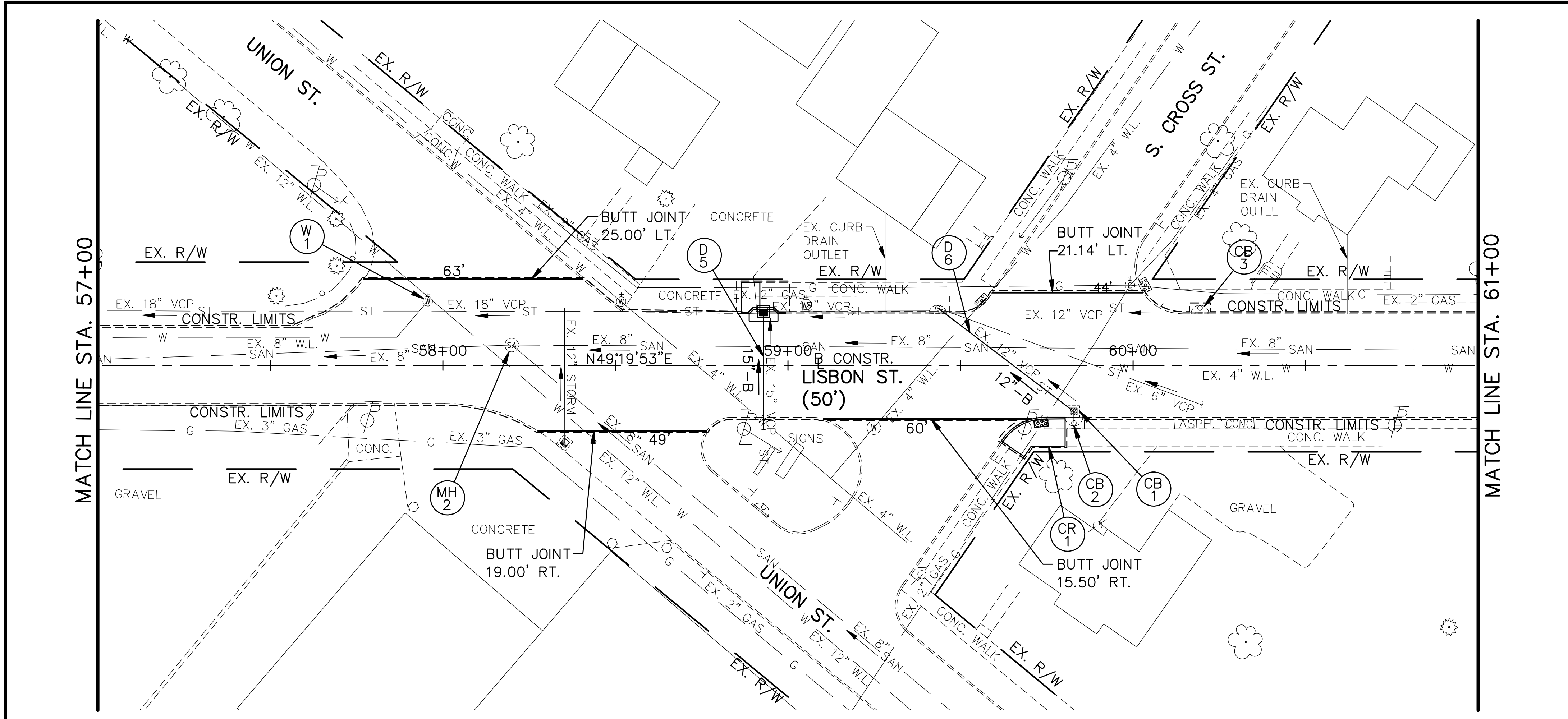
PROPOSED MANHOLE, NO. 3, AS PER PLAN – TYPICAL SECTION VIEW

NOT TO SCALE



DRAINAGE DETAILS – MANHOLE, NO. 3, AS PER PLAN
FAIRFIELD SCHOOL RD. – STA. 35+64.41 TO STA. 43+57.00

COL – FAIRFIELD SCH. RD. & LISBON ST.



REF. NO.	STATION TO STATION	SIDE	ESTIMATED QUANTITIES		SEE SHIT. NO.
			FOOT	S.F.	
C2	61+66~62+00	LT.	37	37	9
CB1	59+83	RT.			12
CB2	59+83	RT.			12
CB3	60+19	LT.			9
CR1	59+73	RT.			50
CR2	61+96	LT.			50
CR3	62+17	LT.			9
D5	58+86.50~58+99.50	L/R	23		
D6	59+43~59+83	L/R	26		
DR1	61+64~62+00	LT.			9
MH2	58+20	LT.			
MH3	61+55	LT.			
MH4	64+55	LT.			
WT	57+96	LT.			
TOTALS CARRIED TO GENERAL SUMMARY			49	314	
611	PAVEMENT REMOVED AS PER PLAN	S.Y.	11		
202	WALK REMOVED	S.F.	37		
202	CURB REMOVED	FOOT	37		
202	PIPE AND UNDER REMOVED	FOOT	9		
204	SUBGRADE COMPACTION	S.Y.	9		
304	AGGREGATE BASE AS PER PLAN	C.Y.	2		
305	CONCRETE BASE MISC. CLASS OC MS	S.Y.	4		
305	CONCRETE BASE MISC. CLASS OC MS	S.Y.	4		
608	6" CONCRETE WALK AS PER PLAN	S.F.	249		
608	CURB RAMP, (TYPE A1) AS PER PLAN	S.F.	138		
608	CURB RAMP, (TYPE A1) AS PER PLAN	S.F.	138		
608	CURB RAMP, (TYPE A1) AS PER PLAN	S.F.	207		
608	CURB, TYPE 6 AS PER PLAN	FOOT	37		
608	DETECTABLE WARNING, (TYPE B3) AS PER PLAN	S.F.	8		
611	15" CONDUIT, TYPE B	FOOT	47		
611	12" CONDUIT, TYPE B	FOOT	47		
611	CATCH BASIN, NO. 3A AS PER PLAN	EACH	1		
611	CATCH BASIN ADJUSTED TO GRADE AS PER PLAN	EACH	1		
611	INLET RECONSTRUCTED TO GRADE AS PER PLAN (STA. 59+83)	EACH	1		
611	INLET RECONSTRUCTED TO GRADE AS PER PLAN (STA. 60+19)	EACH	1		
611	MANHOLE ADJUSTED TO GRADE AS PER PLAN	EACH	3		
611	VALVE BOX ADJUSTED TO GRADE AS PER PLAN	EACH	1		
638	CONCRETE BUTT JOINT	EA	1		

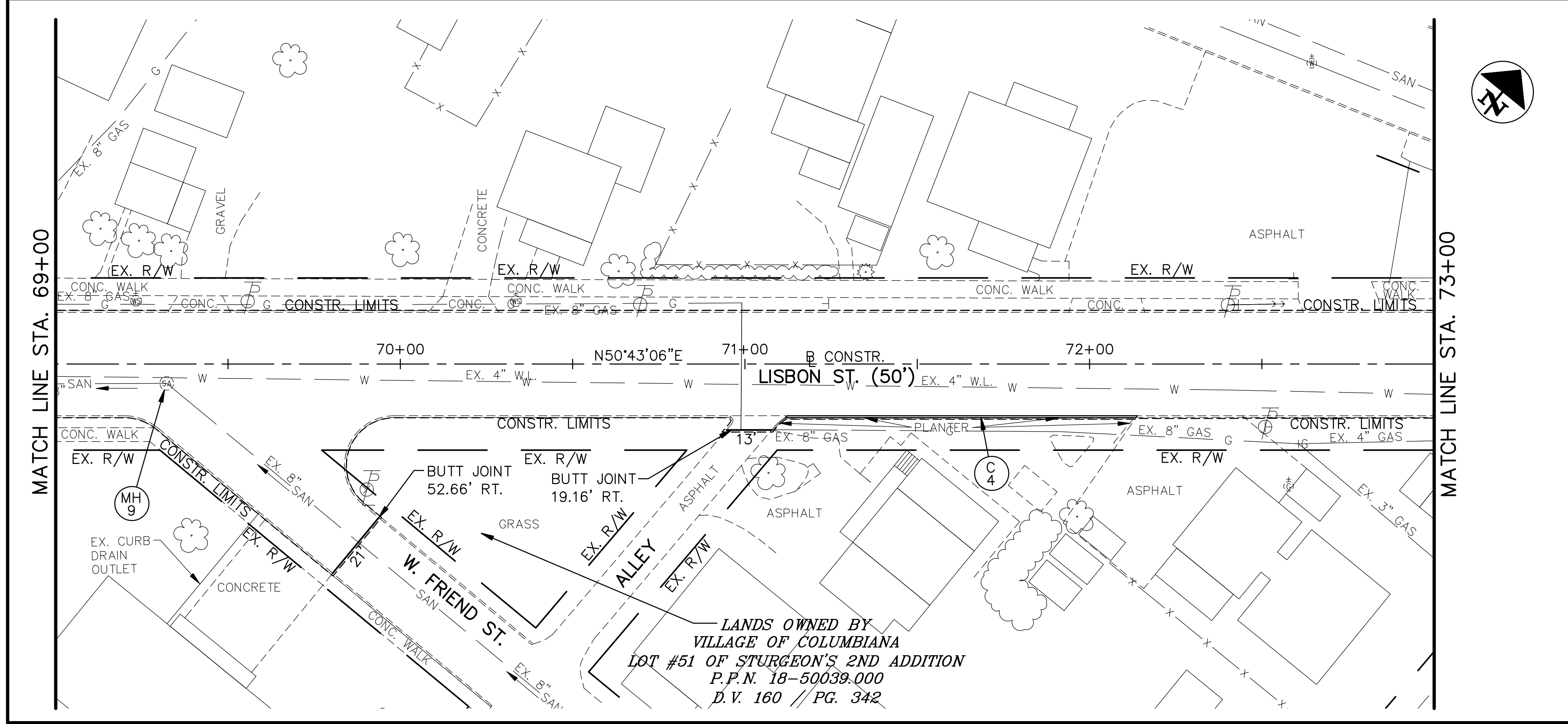
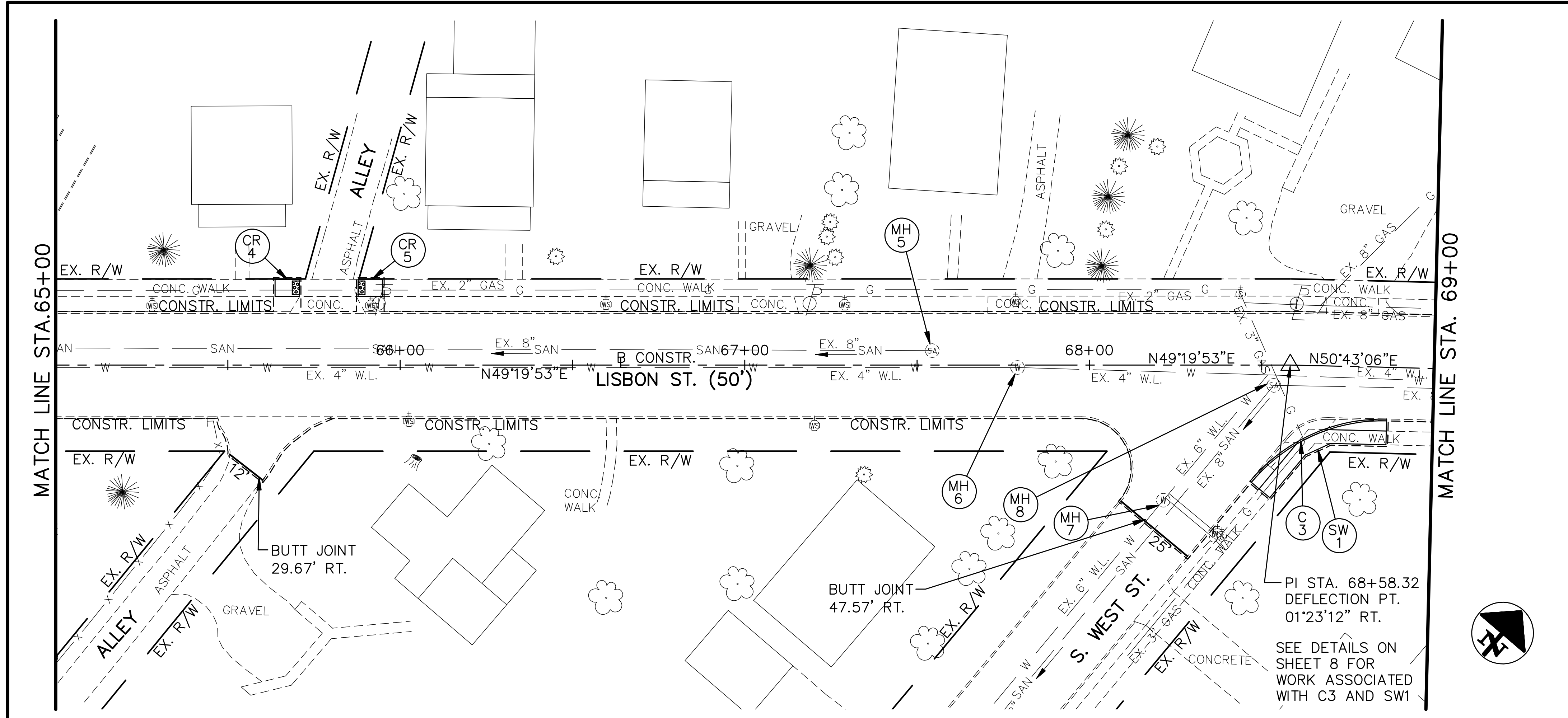
COL - FAIRFIELD SCH. RD. & LISBON ST.

PLAN VIEW (LISBON STREET) STA. 57+00 TO STA. 65+00

DATE: 10/23
 CHK'D BY: D.G.D.
 DATE: 10/23

SCALE: 1" = 20' HORIZONTAL
 1" = 40' VERTICAL

44
60



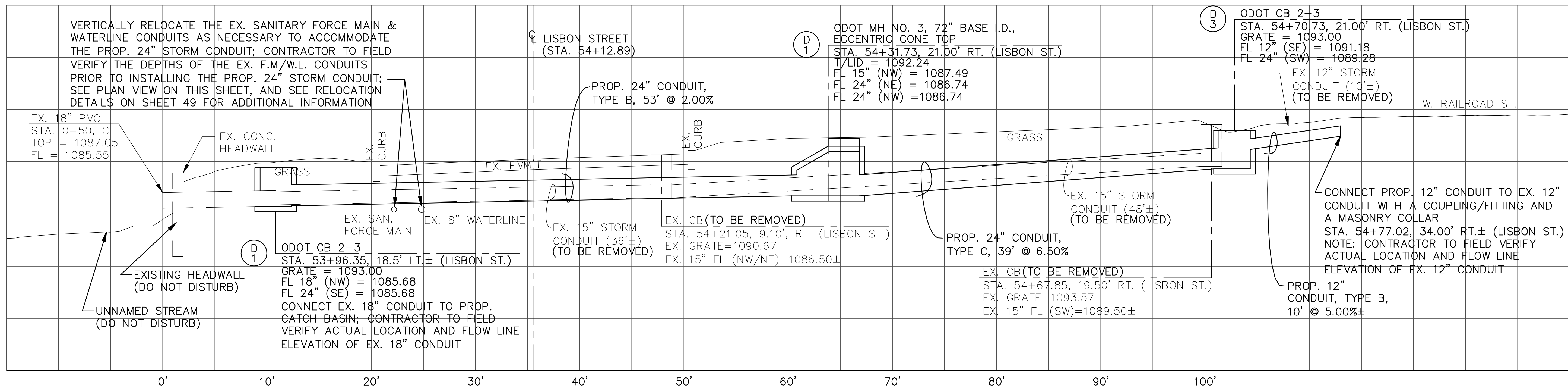
REF. NO.	STATION TO STATION	SIDE	ESTIMATED QUANTITIES		SEE SHIT. NO.
			REMOVED	AS PER PLAN	
C3	68+46~68+87	RT.	6	12	8
C4	71+12~72+14	RT.	12	12	8
CR4	65+70	LT.	35	35	
CR5	65+89	LT.	35	35	
MH5	67+54	LT.			
MH6	67+79	RT.			
MH7	68+21	RT.			
MH8	68+54	RT.			
MH9	69+32	RT.			
SW1	68+46~68+87	RT.	208		
TOTALS CARRIED TO GENERAL SUMMARY			18	278	150
TOTALS CARRIED TO GENERAL SUMMARY			42	7	26
TOTALS CARRIED TO GENERAL SUMMARY			19	3	4
TOTALS CARRIED TO GENERAL SUMMARY			14	14	12
TOTALS CARRIED TO GENERAL SUMMARY			35	35	
TOTALS CARRIED TO GENERAL SUMMARY			46	102	
TOTALS CARRIED TO GENERAL SUMMARY			170	170	
TOTALS CARRIED TO GENERAL SUMMARY			70	148	5

COL - FAIRFIELD SCH. RD. & LISBON ST.
 PLAN VIEW (LISBON STREET)
 STA. 65+00 TO STA. 73+00

DATE: 10/23
 CHECKED BY: D.G.D.
 DATE: 10/23

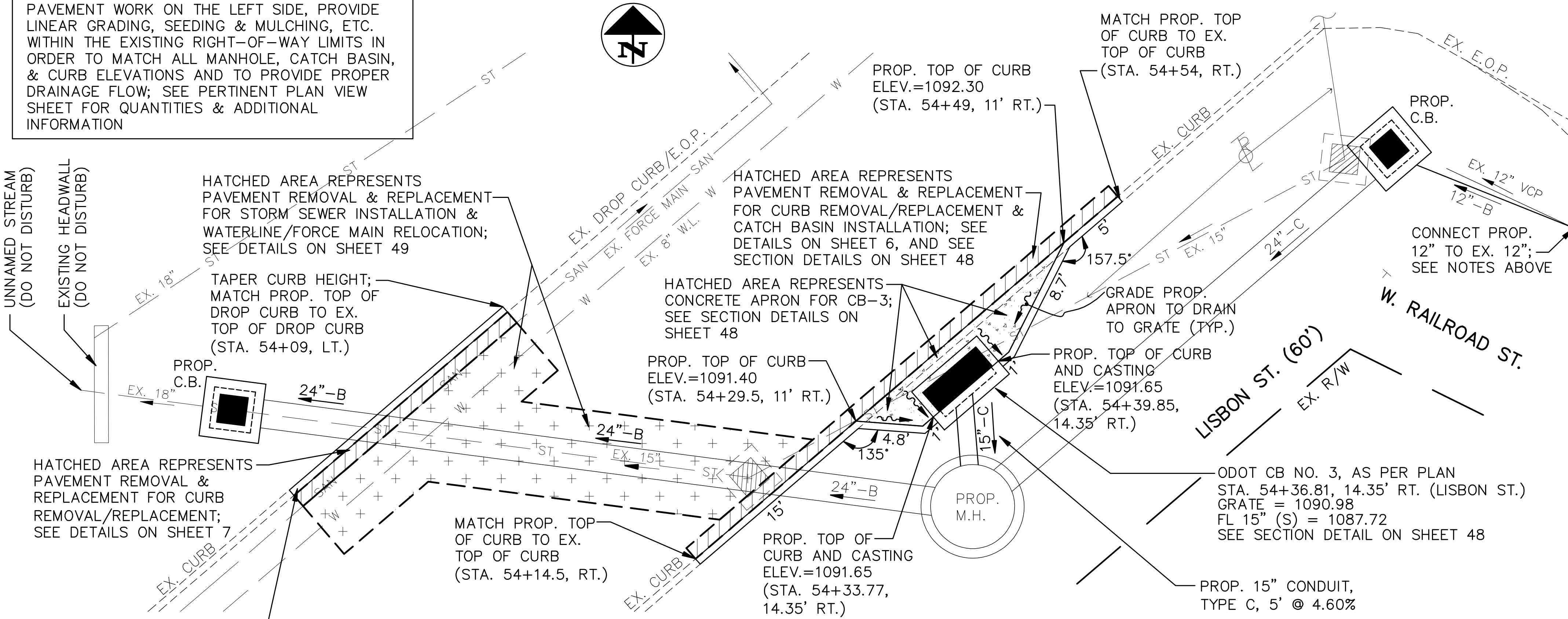
HORIZONTAL SCALE IN FEET
 0 10 20 40

45
 60



UPON COMPLETION OF ALL DRAINAGE AND PAVEMENT WORK ON THE LEFT SIDE, PROVIDE LINEAR GRADING, SEEDING & MULCHING, ETC. WITHIN THE EXISTING RIGHT-OF-WAY LIMITS IN ORDER TO MATCH ALL MANHOLE, CATCH BASIN, & CURB ELEVATIONS AND TO PROVIDE PROPER DRAINAGE FLOW; SEE PERTINENT PLAN VIEW SHEET FOR QUANTITIES & ADDITIONAL INFORMATION

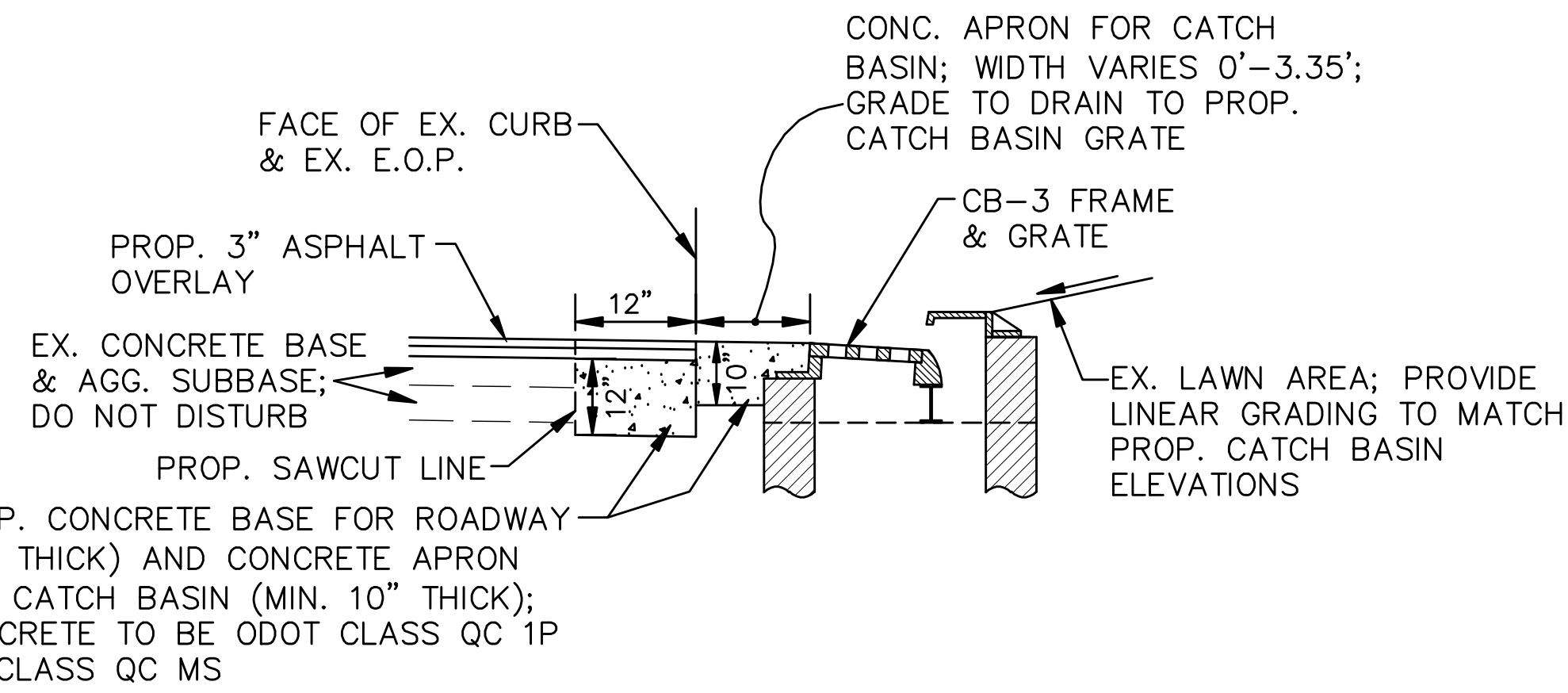
SEE CATCH BASIN NO. 3 SECTION DETAIL ON SHEET 48.
 SEE STORM SEWER, WATERLINE & FORCE MAIN CONDUIT TRENCH DETAILS ON SHEET 48.
 SEE ROADWAY PAVEMENT REPLACEMENT DETAILS ON SHEET 48.
 FOR CURB REMOVAL & REPLACEMENT DETAILS, SEE SHEET 6, AND ALSO REFER TO THE CATCH BASIN NO. 3 SECTION DETAIL ON SHEET 48.



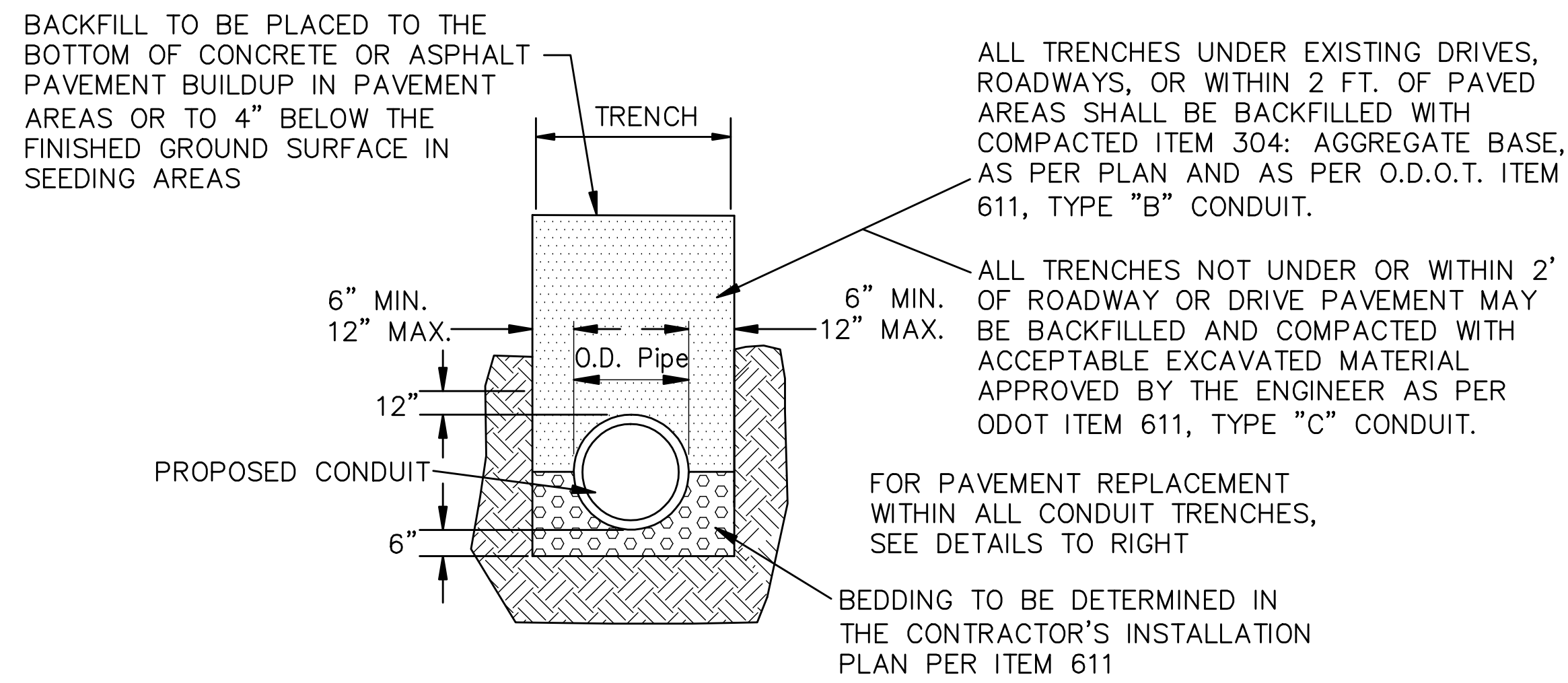
UPON COMPLETION OF ALL DRAINAGE AND PAVEMENT WORK ON THE RIGHT SIDE, PROVIDE LINEAR GRADING, SEEDING & MULCHING, ETC. WITHIN THE EXISTING RIGHT-OF-WAY LIMITS IN ORDER TO MATCH ALL MANHOLE, CATCH BASIN, & CURB ELEVATIONS AND TO PROVIDE PROPER DRAINAGE FLOW; SEE PERTINENT PLAN VIEW SHEET FOR QUANTITIES & ADDITIONAL INFORMATION

CURB/PAVEMENT REPLACEMENT AND CB NO. 3 DETAILS
 (LISBON STREET - STA. 53+96 TO STA. 54+54, LEFT & RIGHT SIDES)

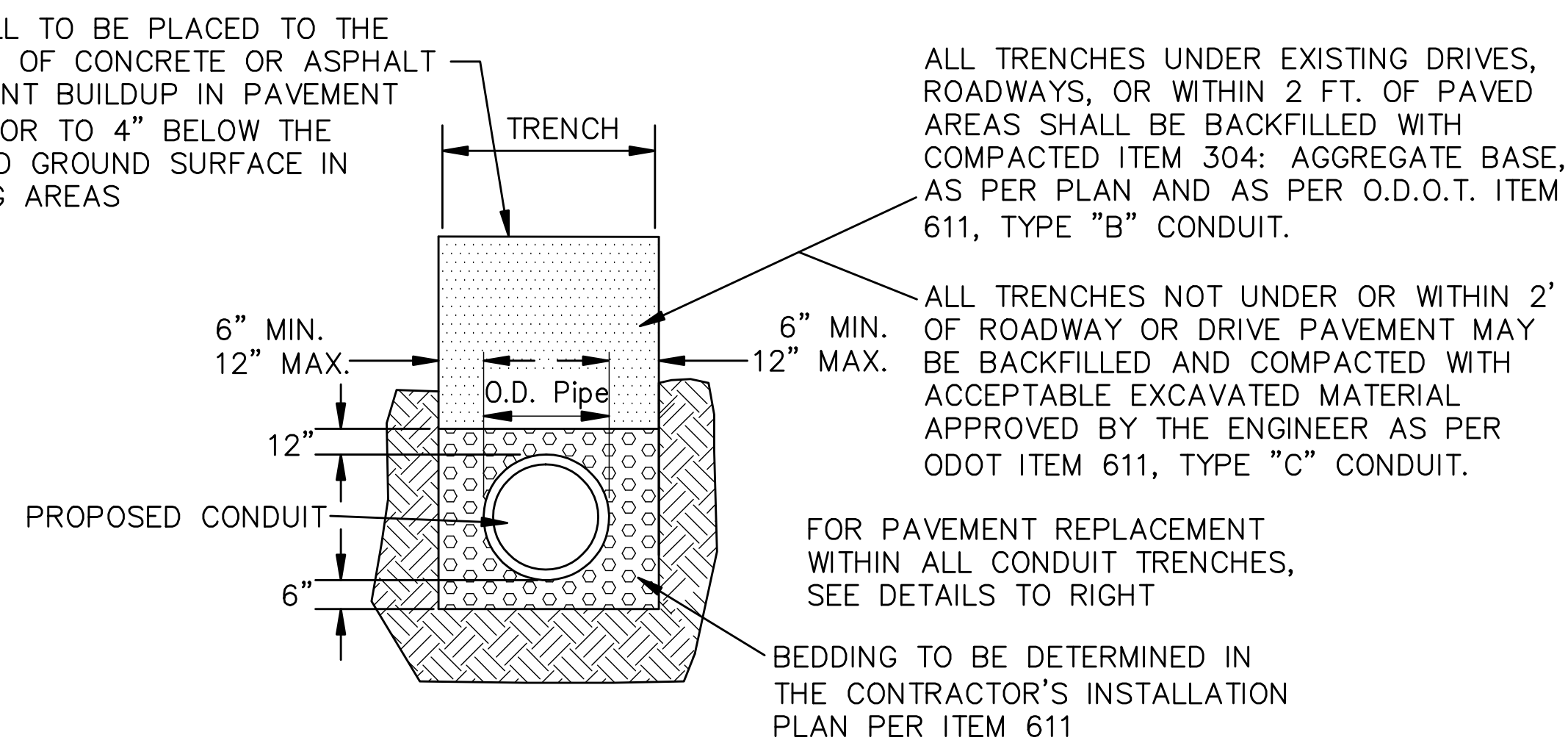




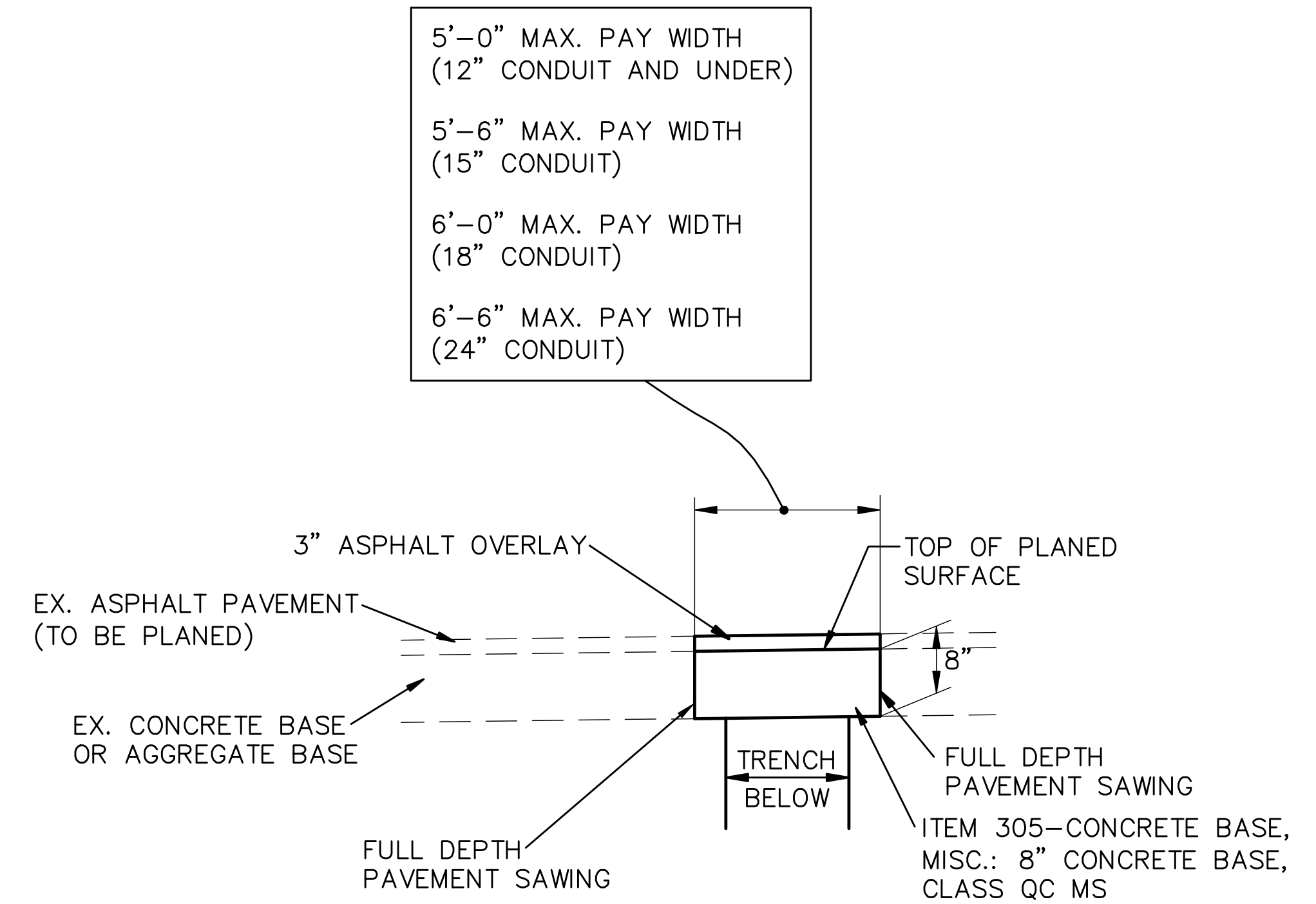
CATCH BASIN NO. 3, AS PER PLAN - SECTION DETAIL



TYPICAL STORM SEWER CONDUIT TRENCH DETAIL



TYPICAL WATER MAIN/SANITARY FORCE MAIN CONDUIT TRENCH DETAIL

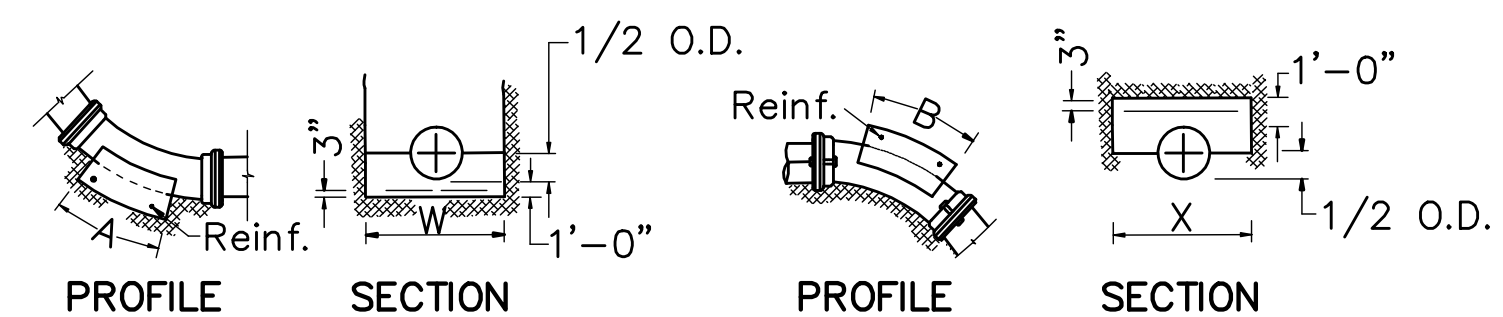


ROADWAY PAVEMENT REPLACEMENT - PROPOSED CONDUIT TRENCHES
ITEM 305-CONCRETE BASE, MISC.: 8" CONCRETE BASE, CLASS QC MS



DRAINAGE DETAILS (LISBON STREET)
STA. 53+96.35 TO STA. 54+77.02

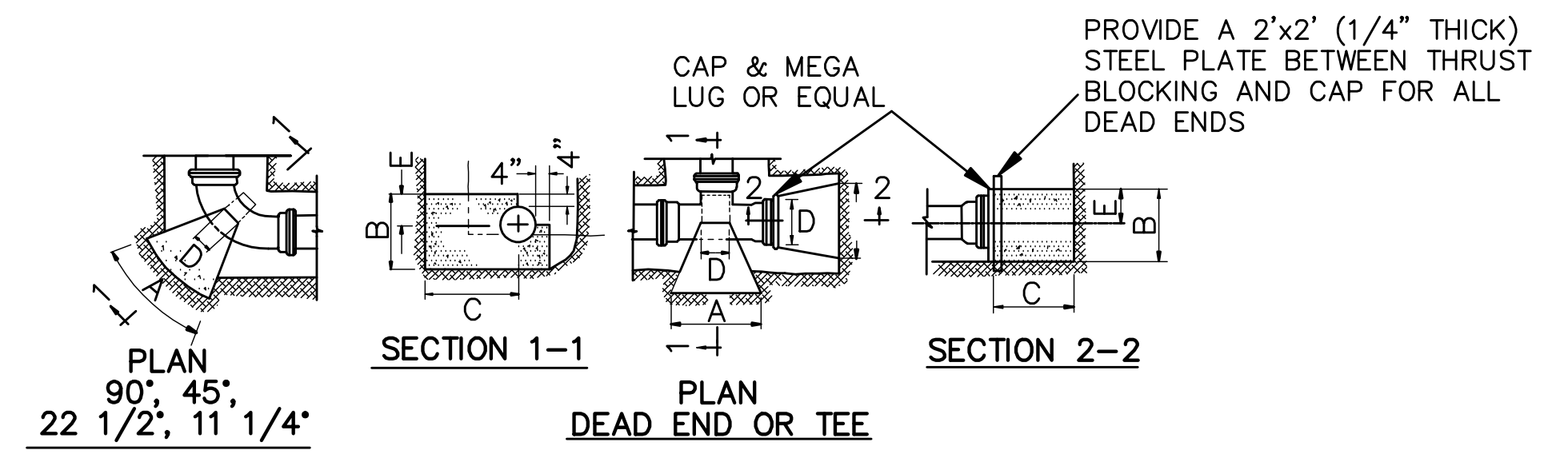
COL - FAIRFIELD SCH. RD. & LISBON ST.



REINFORCING FOR SAG AND CREST BENDS
 11 1/4" Bend - #4 @ 8 1/2" c/c
 22 1/2" Bend - #4 @ 5 1/4" c/c
 45" Bend - #6 @ 6" c/c

THRUST BLOCKING DETAILS
 FOR BENDS IN VERTICAL PLANE
 WORKING PRESSURE UP TO 350 PSI

ALL FITTINGS SHALL BE RESTRAINED WITH MECHANICAL JOINT RESTRAINTS CONFORMING TO "MEGA-LUG" OR APPROVED EQUAL IN ADDITION TO THE CONCRETE THRUST BLOCKING AS SHOWN ABOVE. JOINT RESTRAINTS SHALL ALSO BE USED ON (1) PIPE JOINT BEFORE AND AFTER ALL FITTINGS; INCLUDING BUT NOT LIMITED TO ALL VALVES, TEES AND BENDS.



Note: All concrete blocking must have its entire face (A+B) bearing against undisturbed soil.

THRUST BLOCKING DETAILS
 FOR BENDS IN HORIZONTAL PLANES
 WORKING PRESSURE UP TO 350 PSI

TABLE OF MINIMUM DIMENSIONS

DIA.	11 1/4" Bend			22 1/2" Bend			45" Bend		
	A	W	C.Y.	A	W	C.Y.	A	W	C.Y.
4"				1'-0"	1'-3"	.06	1'-0"	1'-4"	.06
6"				1'-0"	2'-4"	.12	1'-0"	2'-8"	.12
8"	1'-0"	1'-0"	.05	1'-0"	2'-4"	.12	1'-8"	3'-3"	.27
10"	1'-0"	1'-3"	.07	1'-3"	3'-0"	.20	2'-3"	3'-6"	.41
12"	1'-0"	2'-0"	.11	1'-4"	3'-8"	.27	3'-0"	4'-0"	.67
16"	2'-0"	2'-0"	.25	2'-3"	4'-3"	.59	4'-8"	4'-6"	1.30
20"	2'-3"	2'-3"	.36	2'-11"	4'-11"	1.02	5'-6"	5'-0"	1.95

DIA.	11 1/4" Bend			22 1/2" Bend			45" Bend		
	B	X	C.Y.	B	X	C.Y.	B	X	C.Y.
4"				1'-0"	1'-3"	.06	1'-0"	1'-4"	.06
6"				1'-0"	2'-4"	.12	1'-0"	2'-8"	.12
8"	1'-0"	1'-0"	.05	1'-0"	2'-4"	.12	1'-8"	3'-3"	.27
10"	1'-0"	1'-3"	.07	1'-3"	3'-0"	.20	2'-3"	3'-6"	.41
12"	1'-0"	2'-0"	.11	1'-4"	3'-8"	.27	3'-0"	4'-0"	.67
16"	2'-0"	2'-0"	.25	2'-3"	4'-3"	.59	4'-8"	4'-6"	1.30
20"	2'-3"	2'-3"	.36	2'-11"	4'-11"	1.02	5'-6"	5'-0"	1.95

NOTE: Blocking designs based on working pressure 305 psi and 100 psi water hammer and soil bearing 3000 psf. All Mechanical joints shall be wrapped with 6 mil plastic prior to placing concrete. Concrete shall be plant mix, 4,000 psi at 28-day strength; All thrust blocks to be formed. Quantity shown is for each thrust block. ADDITIONAL BLOCKING AS DIRECTED BY ENGINEER

TABLE OF MINIMUM DIMENSIONS

DIA.	11 1/4" Bend			22 1/2" Bend			45" Bend			90° Bend			Tee-Dead End		
	A	B	C.Y.	A	B	C.Y.	A	B	C.Y.	A	B	C.Y.	A	B	C.Y.
4"				1'-4"	1'-4"	.19	1'-4"	1'-4"	.19	2'-0"	1'-4"	.25	1'-6"	1'-4"	.19
6"				1'-4"	1'-4"	.19	2'-2"	1'-8"	.31	3'-4"	1'-10"	.44	2'-6"	2'-10"	.54
8"	1'-4"	1'-4"	.18	2'-0"	1'-8"	.29	3'-3"	1'-10"	.43	3'-6"	2'-3"	.56	3'-4"	2'-3"	.52
10"	1'-8"	1'-8"	.26	2'-8"	1'-10"	.38	4'-0"	2'-3"	.61	5'-8"	3'-0"	1.07	4'-6"	2'-8"	.77
12"	2'-0"	1'-9"	.42	3'-4"	2'-0"	.91	5'-0"	2'-8"	1.52	6'-8"	3'-4"	2.38	5'-6"	3'-2"	1.92
16"	2'-8"	2'-3"	.90	4'-6"	2'-8"	1.44	6'-6"	3'-8"	2.57	9'-2"	4'-8"	4.22	7'-6"	4'-0"	3.01
20"	3'-6"	2'-7"	1.34	5'-8"	3'-2"	2.66	7'-8"	5'-0"	5.68	10'-6"	6'-0"	9.33	8'-6"	5'-2"	6.51

DIMENSION C
 4"-10" Dia.=2'-0"
 12"-16" Dia.=3'-0"
 18"-20" Dia.=4'-0"
 24" Dia.=5'-0"

DIMENSION D
 4"-10" Dia.=1'-0"
 12"-24" Dia.=2'-0"

DIMENSION E
 For all Diameters
 E=D/2+4"

NOTE: Blocking designs based on working pressure 305 psi and 100 psi water hammer and soil bearing 3000 psf. All Mechanical joints shall be wrapped with 6 mil plastic prior to placing concrete. Concrete shall be plant mix, 4,000 psi at 28-day strength; All thrust blocks to be formed. Quantity shown is for each thrust block. ADDITIONAL BLOCKING AS DIRECTED BY ENGINEER

ALL EQUIPMENT, MATERIAL, LABOR AND INCIDENTALS NECESSARY FOR A COMPLETE WATER MAIN OR SANITARY FORCE MAIN RELOCATION ARE TO BE INCLUDED IN THE PERTINENT UNIT PRICES BID.

ALL BEDDING AND BACKFILL SHALL BE IN ACCORDANCE WITH THE TYPICAL TRENCH DETAIL ON SHEET 48.

INSTALL CONCRETE THRUST BLOCKING AT ALL FITTINGS; SEE THRUST BLOCKING DETAILS AND TABLES ON THIS SHEET.

ALL FITTINGS SHALL BE RESTRAINED IN ACCORDANCE WITH THE NOTES ON THIS SHEET.

WATER MAIN RELOCATION NOTE:

DURING INSTALLATION OF THE PROPOSED DRAINAGE SYSTEM ACROSS LISBON STREET, A VERTICAL CONFLICT BETWEEN THE PROPOSED DRAINAGE CONDUIT AND THE EXISTING WATER MAIN IS POSSIBLE. THE CONTRACTOR SHALL CAREFULLY EXCAVATE THE AREA TO DETERMINE WHETHER OR NOT THERE IS A VERTICAL CONFLICT. IF A CONFLICT IS DETERMINED, THEN THE CONTRACTOR SHALL RELOCATE THE EXISTING WATER MAIN IN ACCORDANCE WITH THE DETAILS ON THIS SHEET AND AT THE DIRECTION OF THE CONSTRUCTION ENGINEER.

ALL WORK SHALL BE COORDINATED WITH THE CITY OF COLUMBIANA WATER DEPARTMENT.

ONLY THE CITY OF COLUMBIANA WATER DEPARTMENT EMPLOYEES MAY OPERATE WATER VALVES OR HYDRANTS. CONTACT THE CITY OF COLUMBIANA WATER DEPARTMENT (48) HOURS IN ADVANCE TO SCHEDULE VALVE OR HYDRANT OPERATIONS.

THE EXISTING WATER MAIN IS ANTICIPATED TO BE (8") DIAMETER, BUT SHALL BE FIELD VERIFIED BY THE CONTRACTOR.

ALL WORK FOR ANY NECESSARY WATERLINE VERTICAL RELOCATION SHALL BE COMPLETED WITHIN ONE (1) WORKING DAY.

THE COST FOR THE RELOCATION OF THE EXISTING WATER MAIN AS DESCRIBED IN THE NOTES AND DETAILS ON THIS SHEET SHALL INCLUDE ALL NECESSARY LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS FOR A COMPLETE, WATER TIGHT RELOCATION AND SHALL BE INCLUDED IN THE PERTINENT UNIT PRICE BID FOR THE ITEM SPECIFIED IN THE PLAN VIEW SUB-SUMMARY ON SHEET 43.

SANITARY FORCE MAIN RELOCATION NOTES

DURING INSTALLATION OF THE PROPOSED DRAINAGE SYSTEM ACROSS LISBON STREET, A VERTICAL CONFLICT BETWEEN THE PROPOSED DRAINAGE CONDUIT AND THE EXISTING SANITARY FORCE MAIN IS POSSIBLE. THE CONTRACTOR SHALL CAREFULLY EXCAVATE THE AREA TO DETERMINE WHETHER OR NOT THERE IS A VERTICAL CONFLICT. IF A CONFLICT IS DETERMINED, THEN THE CONTRACTOR SHALL RELOCATE THE EXISTING SANITARY FORCE MAIN IN ACCORDANCE WITH THE DETAILS ON THIS SHEET AND AT THE DIRECTION OF THE CONSTRUCTION ENGINEER.

ALL WORK SHALL BE COORDINATED WITH THE CITY OF COLUMBIANA WASTE WATER DEPARTMENT.

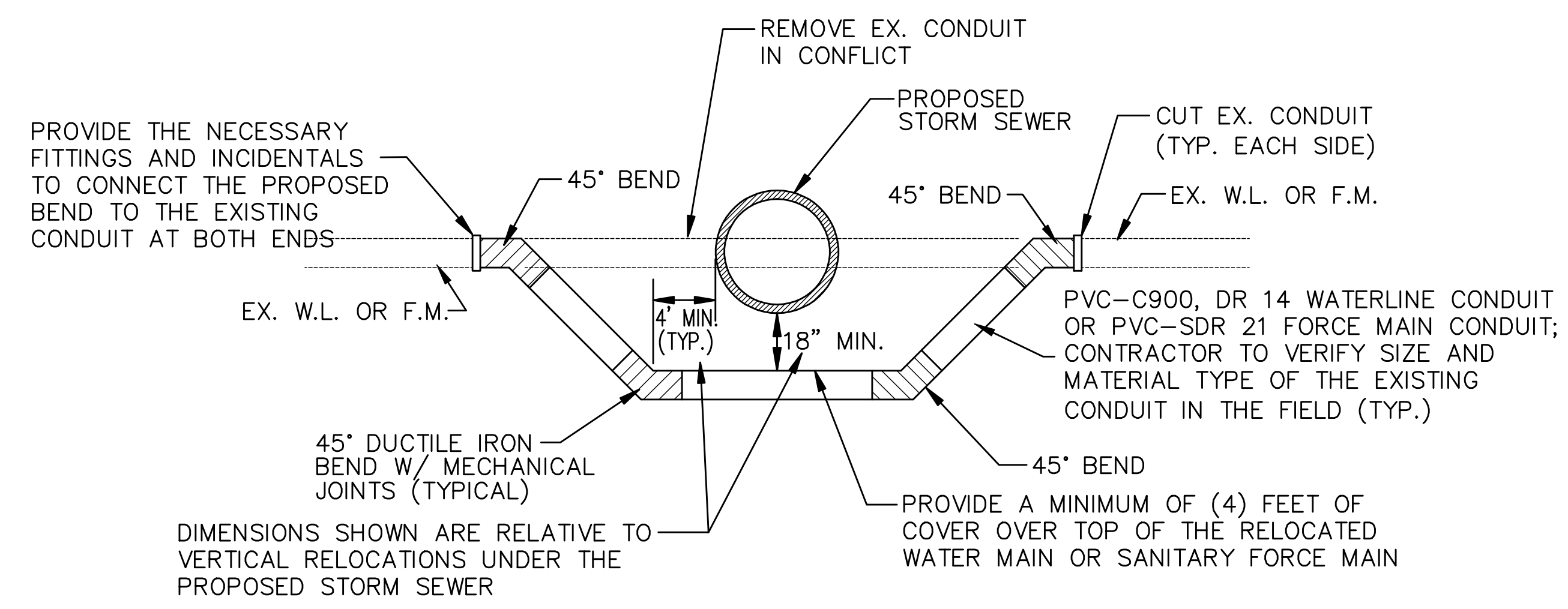
ONLY THE CITY OF COLUMBIANA WASTE WATER DEPARTMENT EMPLOYEES MAY OPERATION SANITARY LIFT STATION EQUIPMENT, VALVES, ETC. CONTACT THE CITY OF COLUMBIANA WASTE WATER DEPARTMENT (48) HOURS IN ADVANCE TO SCHEDULE OPERATIONS OF THE EXISTING SANITARY LIFT STATION EQUIPMENT, VALVES, ETC.

THE EXISTING SANITARY FORCE MAIN IS ANTICIPATED TO BE (6") DIAMETER, BUT SHALL BE FIELD VERIFIED BY THE CONTRACTOR.

ALL WORK FOR ANY NECESSARY SANITARY FORCE MAIN VERTICAL RELOCATION SHALL BE COMPLETED WITHIN ONE (1) WORKING DAY.

DURING THE SANITARY FORCE MAIN REPLACEMENT, THE CONTRACTOR WILL BE REQUIRED TO PUMP THE SEWAGE FROM THE EXISTING SANITARY LIFT STATION WET WELL (LOCATED ON THE NORTH & WEST SIDE OF THE RAILROAD BRIDGE) INTO A PROPER TANKER TRUCK (AUTHORIZED AND LICENSED TO TRANSPORT SANITARY SEWAGE), AND WILL BE REQUIRED TO TRANSPORT THE SANITARY SEWAGE TO THE WASTE WATER TREATMENT PLANT FOR PROPER TREATMENT & DISPOSAL, UNLESS OTHERWISE DIRECTED BY THE CITY WASTE WATER DEPARTMENT.

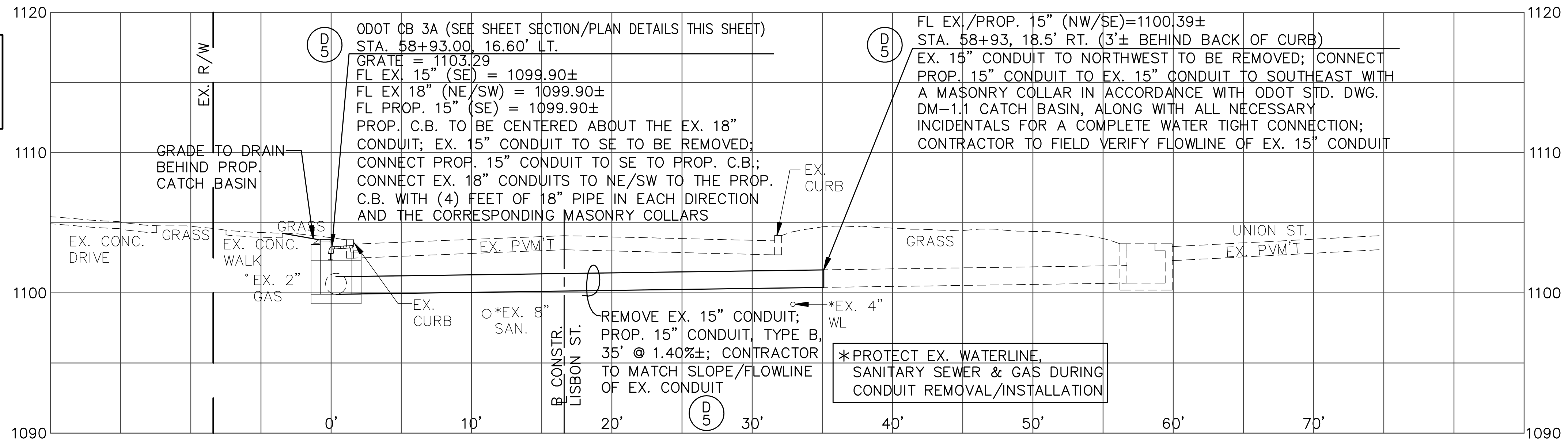
THE COST FOR THE RELOCATION OF THE EXISTING SANITARY FORCE MAIN AS DESCRIBED IN THE NOTES ABOVE AND IN THE DETAILS ON THIS SHEET SHALL INCLUDE ALL NECESSARY LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS FOR A COMPLETE, WATER TIGHT RELOCATION AND SHALL BE INCLUDED IN THE PERTINENT UNIT PRICE BID FOR THE CONTINGENCY ITEM SPECIFIED IN THE PLAN VIEW SUB-SUMMARY ON SHEET 43.



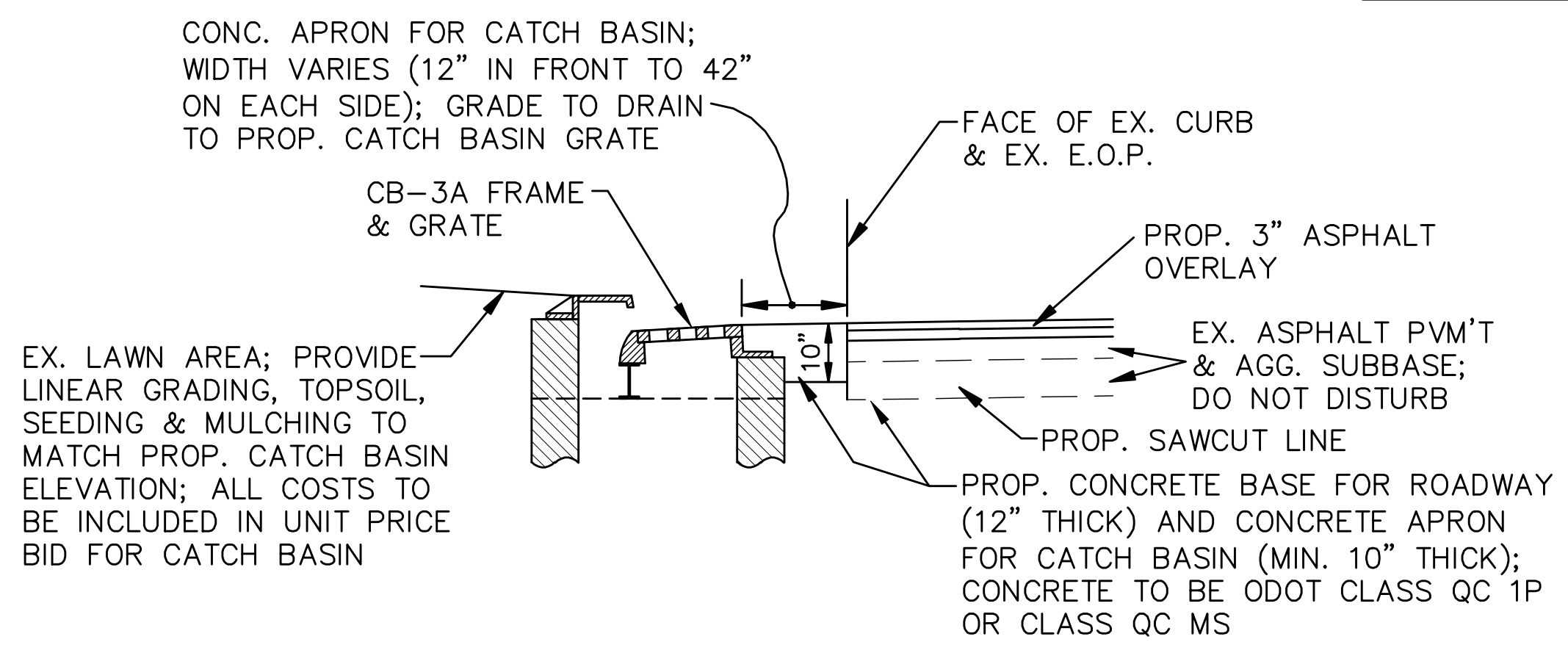
VERTICAL CONFLICT - SECTION VIEW

**EXISTING WATER MAIN AND/OR SANITARY FORCE MAIN RELOCATION DETAILS
 PROPOSED STORM SEWER INSTALLATION ALONG LISBON STREET**

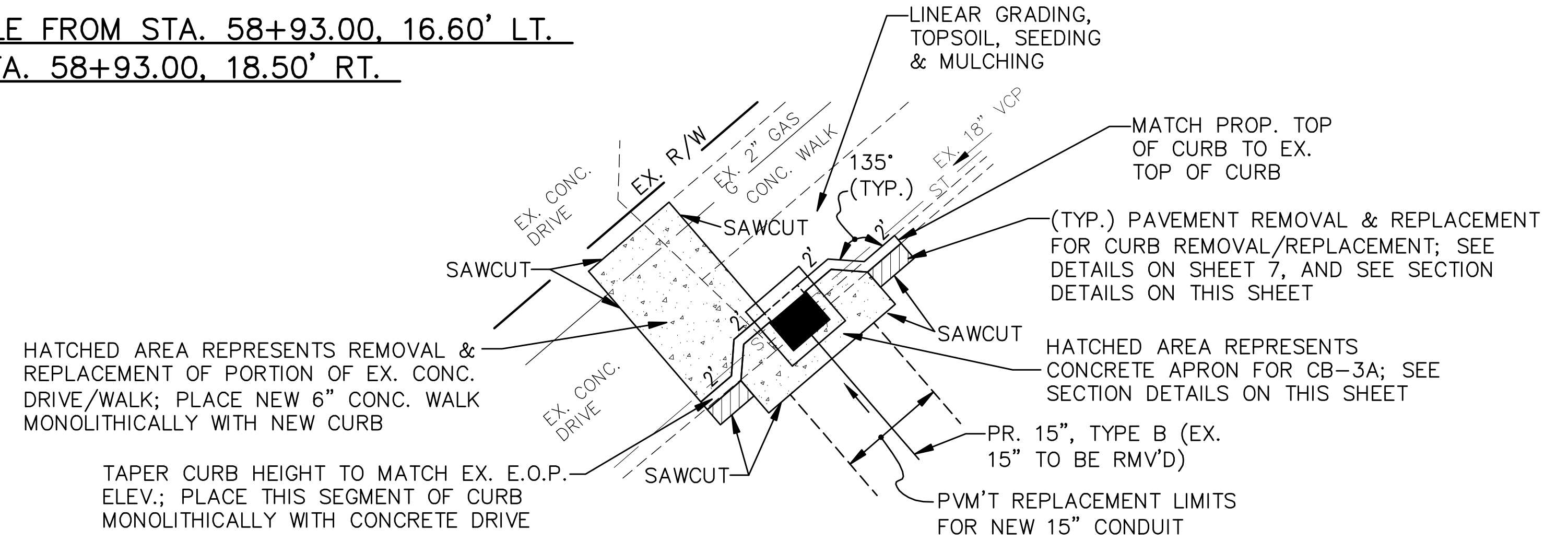
SEE SHEET 48 FOR STORM SEWER TRENCH DETAILS AND PAVEMENT REPLACEMENT DETAILS



DRAINAGE PROFILE FROM STA. 58+93.00, 16.60' LT. TO STA. 58+93.00, 18.50' RT.

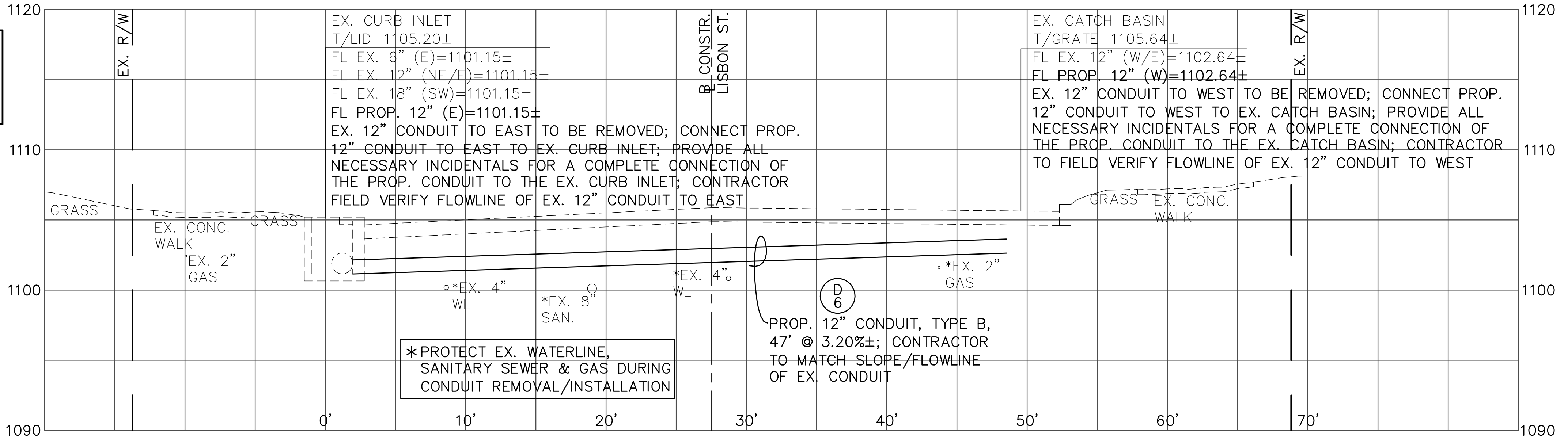


CATCH BASIN NO. 3A - SECTION DETAIL



CATCH BASIN NO. 3A - PLAN VIEW DETAIL

SEE SHEET 48 FOR STORM SEWER TRENCH DETAILS AND PAVEMENT REPLACEMENT DETAILS

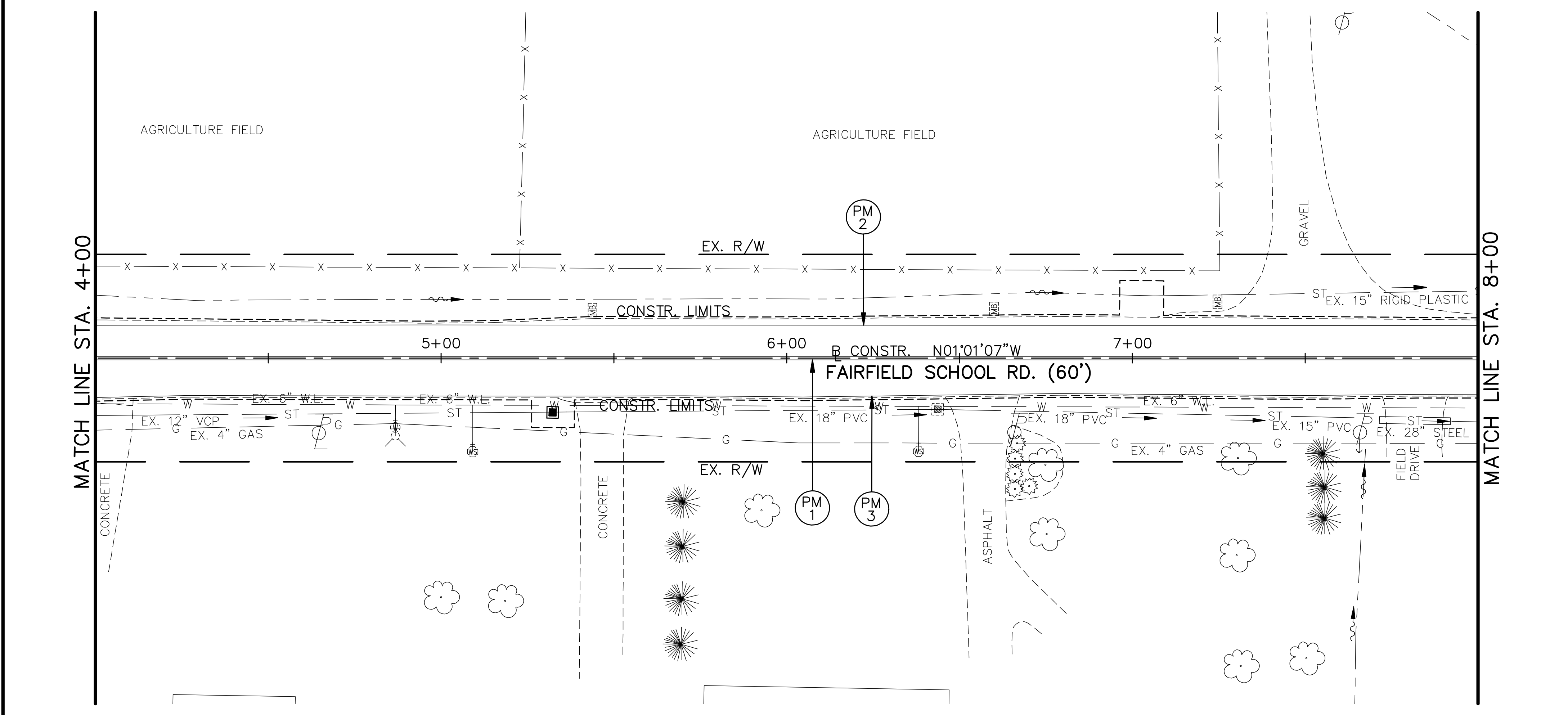
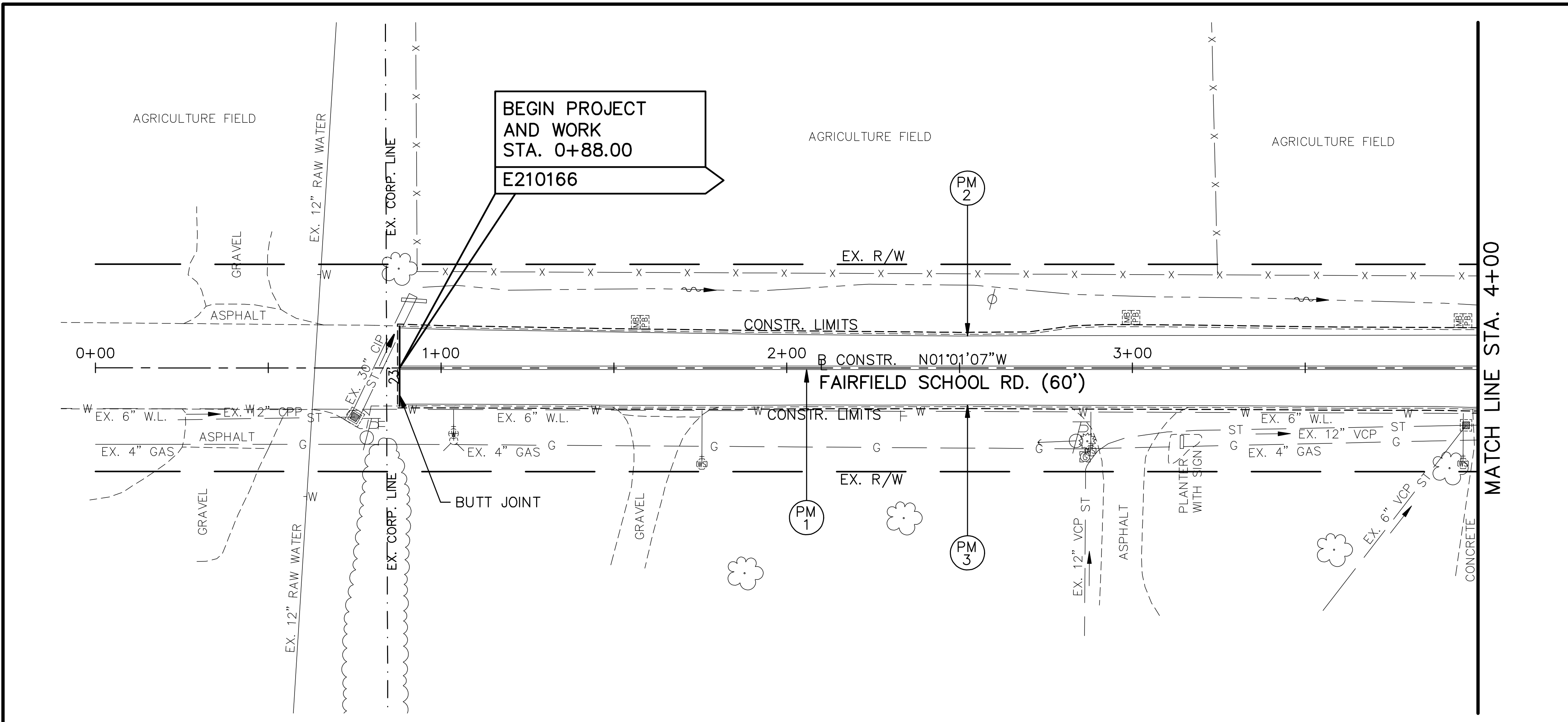


DRAINAGE PROFILE FROM STA. 59+43.50, 16.70' LT. TO STA. 59+83.00, 13.40' RT.



DRAINAGE PROFILE & DRAINAGE DETAILS (LISBON STREET)

COL - FAIRFIELD SCH. RD. & LISBON ST.



ESTIMATED QUANTITIES		SEE SHIT. NO.
REF. NO.	STATION TO STATION	SIDE
PM1	0+88~8+00	CL
PM2	0+88~8+00	LT.
PM3	0+88~8+00	RT.
TOTALS CARRIED TO GENERAL SUMMARY		0.28
644	EDGE LINE, 4'	MILE 0.14
644	CENTER LINE	MILE 0.14
TOTALS CARRIED TO GENERAL SUMMARY		0.14

TOTALS CARRIED TO GENERAL SUMMARY

COL - FAIRFIELD SCH. RD. & LISBON ST.

PAVEMENT MARKING PLAN (FAIRFIELD SCHOOL ROAD)

STA. 0+00 TO STA. 8+00

DATE: 10/23

DATE: 10/23

MATCH LINE STA. 16+00

MATCH LINE STA. 20+00

MATCH LINE STA. 20+00

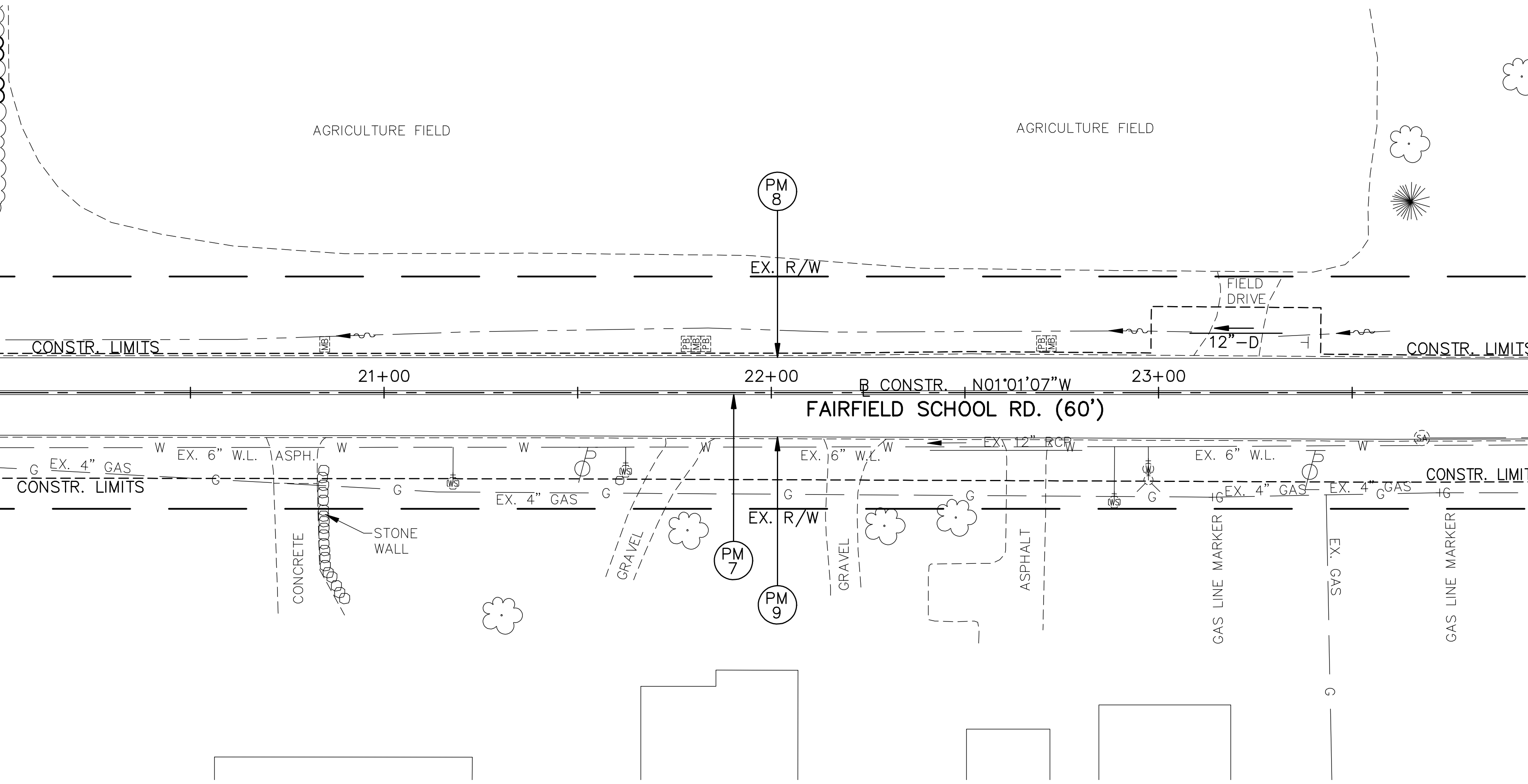
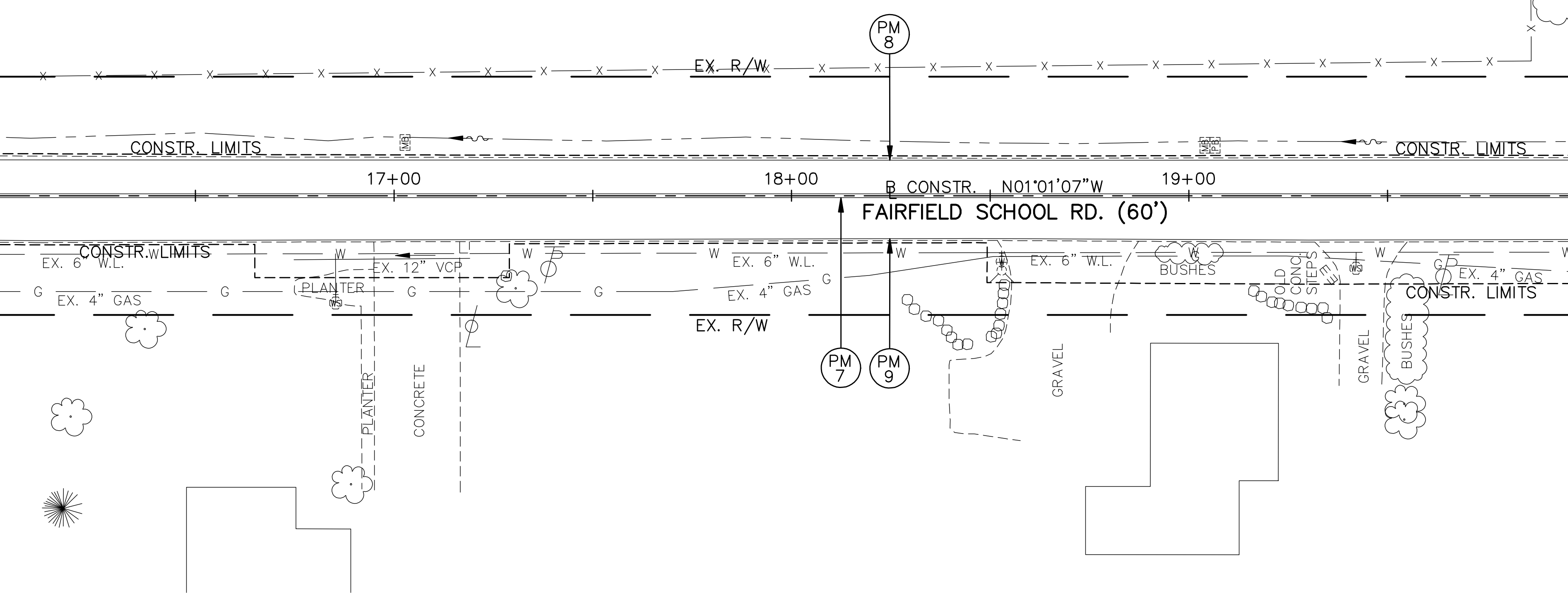
MATCH LINE STA. 24+00

AGRICULTURE FIELD

AGRICULTURE FIELD

AGRICULTURE FIELD

AGRICULTURE FIELD



ESTIMATED QUANTITIES

REF. NO.	STATION TO STATION	SIDE	EDGE LINE, 4"	MILE	CENTER LINE	MILE	SEE SHT. NO.	TOTALS CARRIED TO GENERAL SUMMARY	
PM7	16+00~24+00	CL	0.15						
PM8	16+00~24+00	LT.	0.15						
PM9	16+00~24+00	RT.	0.15						
TOTALS CARRIED TO GENERAL SUMMARY							0.30	0.15	

TOTALS CARRIED TO GENERAL SUMMARY

SEE SHT. NO.

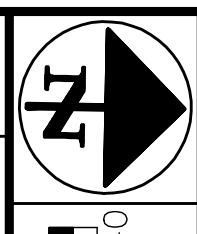
COL - FAIRFIELD SCH. RD. & LISBON ST.

PAVEMENT MARKING PLAN (FAIRFIELD SCHOOL ROAD)

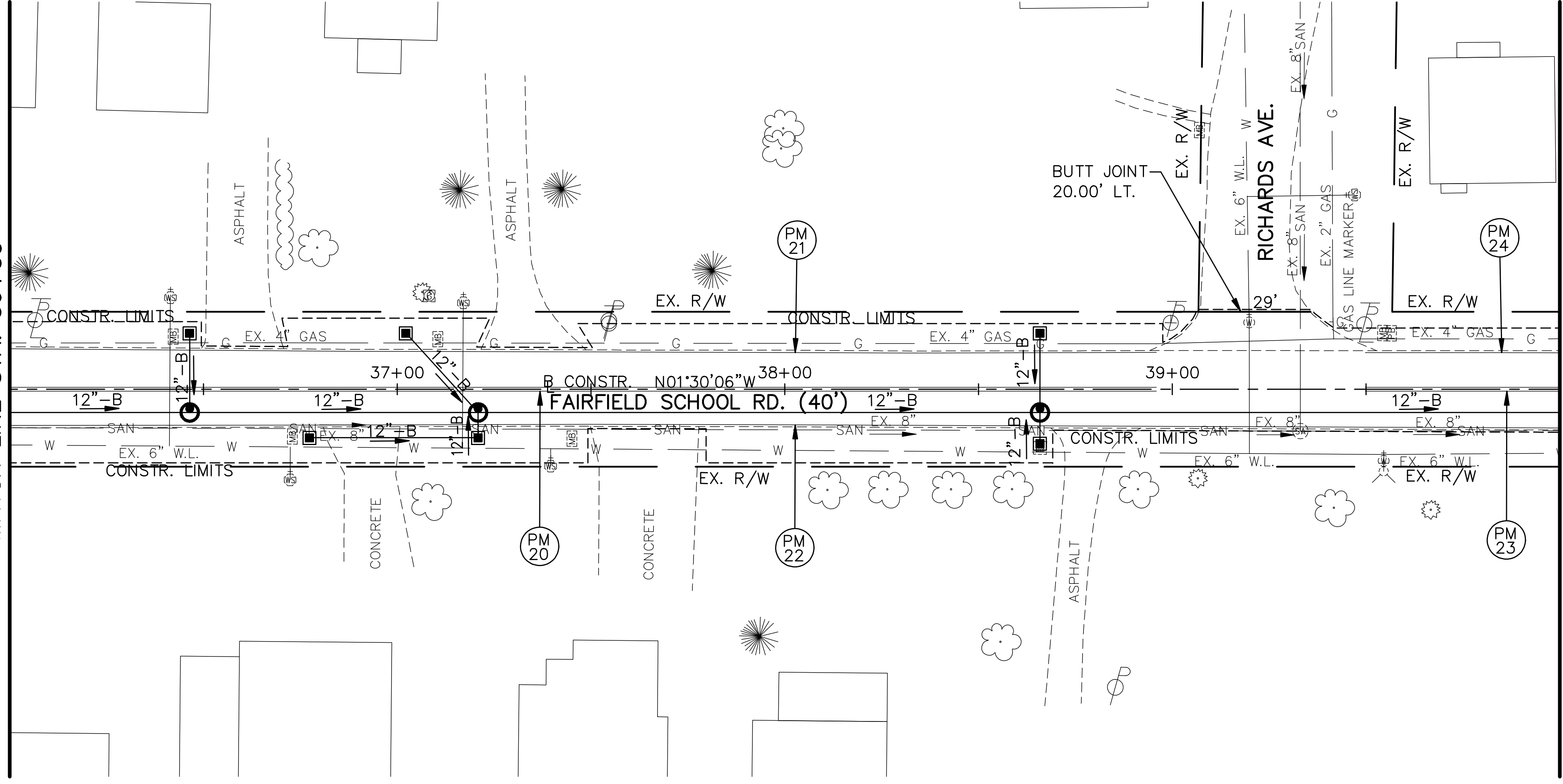
STA. 16+00 TO STA. 24+00

DATE: 10/23

DATE: 10/23

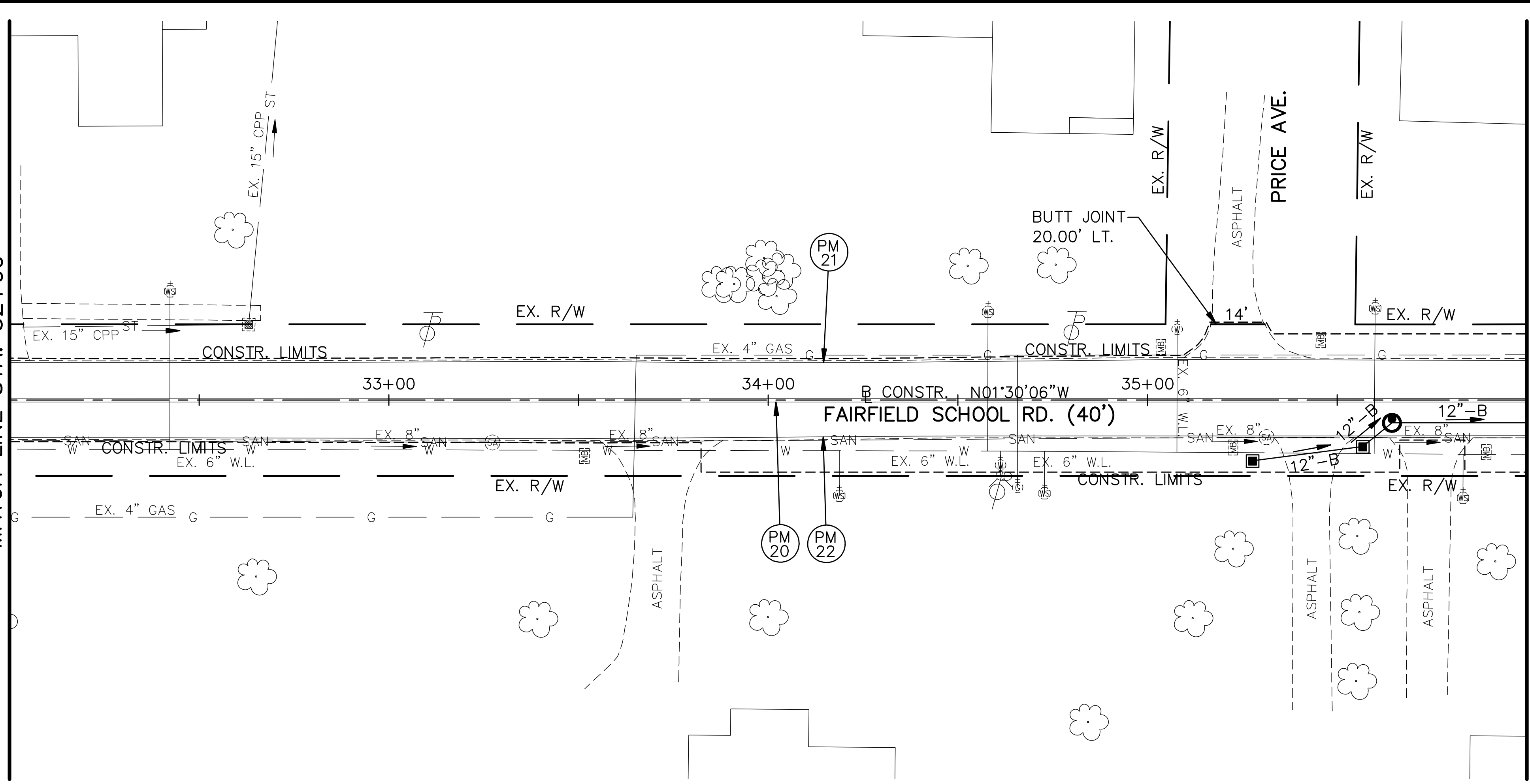


MATCH LINE STA. 36+00



MATCH LINE STA. 40+00

MATCH LINE STA. 32+00



MATCH LINE STA. 36+00

ESTIMATED QUANTITIES

REF. NO.	STATION TO STATION	SIDE	EDGE LINE, 4"	CENTER LINE	SEE SHT. NO.	
PM20	32+00~38+96	CL	0.13	0.13		
PM21	32+00~38+96	LT.	0.13			
PM22	32+00~40+00	RT.	0.15			
PM23	39+50~40+00	CL	0.01			
PM24	39+50~40+00	LT.	0.01			
TOTALS CARRIED TO GENERAL SUMMARY					0.29	0.14

TOTALS CARRIED TO GENERAL SUMMARY

COL - FAIRFIELD SCH. RD. & LISBON ST.

PAVEMENT MARKING PLAN (FAIRFIELD SCHOOL ROAD)
STA. 32+00 TO STA. 40+00

DATE: 10/23
BY: J.F.B.
CHECKED: D.G.D.
DATE: 10/23

SCALE IN FEET
HORIZONTAL: 1" = 40'
VERTICAL: 1" = 20'

SEE SHT. NO.

