


LEGEND

 - CITY LIMITS

DESIGN DESIGNATION

	<u>ASD-96-0.00 TO 1.52</u>	<u>ASD-96-1.52 TO 4.72</u>	<u>ASD-96-4.72 TO 5.46</u>
CURRENT ADT (2013) =	3000	4080	4240
DESIGN YEAR ADT (2025) =	3590	4640	4390
DESIGN HOURLY VOLUME (2025) =	390	510	480
DIRECTIONAL DISTRIBUTION =	0.59	0.53	0.66
TRUCKS (24 HOUR B&C) =	0.06	0.05	0.06

LEGAL SPEED ----- 55 MPH ASD-96-0.00 TO ASD-96-3.56
 LEGAL SPEED ----- 50 MPH ASD-96-3.56 TO ASD-96-4.00
 LEGAL SPEED ----- 35 MPH ASD-96-4.00 TO ASD-96-5.46

DESIGN FUNCTIONAL CLASSIFICATION:

MAJOR COLLECTOR/MINOR ARTERIAL
 NHS PROJECT ----- NO

GENERAL

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

CABLE
ARMSTRONG UTILITIES
TAD SEDWICK
1215 CLAREMONT AVENUE
ASHLAND, OHIO 44805
419-289-0161 X. 50603

CITY
CITY OF ASHLAND ENGINEER
SHANE KREMSER, P.E.
206 CLAREMONT AVENUE, 3RD FLOOR
ASHLAND, OH 44805
PHONE: 419-289-8331

PHONE
FRONTIER COMMUNICATIONS
JIM SAUBER
1534 S.R. 511 SOUTH
ASHLAND, OHIO 44805
419-282-6551

ELECTRIC
OHIO EDISON COMPANY
TRAVIS BALLOG
1717 ASHLAND ROAD
MANSFIELD, OHIO 44905
419-521-6213

ELECTRIC
FIRELANDS ELECTRIC CO-OP
DENNY MARUGG
ONE ENERGY PLACE
NEW LONDON, OHIO 44851
419-929-1571

GAS
COLUMBIA GAS OF OHIO
JARROD SWINEHART
1021 N. MAIN ST.
MANSFIELD, OHIO 44903
419-528-1137

PHONE
AT&T, CONSULTANT FOR AT&T
TONY LYLE, PROJECT ENGINEER
HLC ENGINEERING & SURVEYING
5980-G WILCOX
DUBLIN, OHIO 43106
614-760-8320

ODOT DISTRICT 3 TRAFFIC
906 CLARK AVENUE
ASHLAND, OH 44805
419-281-0513 EXT. 207-2822

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIALS SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROGRESSION OF WORK

GUARDRAIL SHALL BE REMOVED PRIOR TO ANY EMBANKMENT WORK AT THE GUARDRAIL RUN. GUARDRAIL WORK SHALL BE DONE AFTER RESURFACING AND BERM WORK SO AS TO ESTABLISH PROPER GRADES FROM WHICH TO CONSTRUCT THE RAIL.

ROADWAY

ITEM 209 PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN

PREPARE THE SHOULDER FOR PAVING A CONSISTENT SAFETY EDGE IN BOTH THICKNESS AND WIDTH.

PRIOR TO PAVING THE SAFETY EDGE, GRADE AN AREA 10 INCHES WIDE, BEGINNING AT THE EDGE OF THE PAVED ROADWAY, TO PROVIDE A LEVEL SURFACE FREE OF VEGETATION FOR CONSTRUCTION OF THE SAFETY EDGE. IF NECESSARY, EXCAVATE THE GRADED AREA TO THE DEPTH NECESSARY TO CONSTRUCT THE SAFETY EDGE. COMPACT THE GRADED SHOULDER ACCORDING TO 617.05 OR AS DIRECTED BY THE ENGINEER. THE GRADED SHOULDER BEYOND THE 10 INCH WIDE AREA FOR THE SAFETY EDGE SHALL BE GRADED AT A 10:1 SLOPE.

A QUANTITY OF 8.52 MILES HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN.

APPROXIMATE LOCATIONS OF KNOWN CASTINGS

INLETS	
ROUTE	SLM
SR 96	4.47
SR 96	4.49
SR 96	4.70
SR 96	4.72
SR 96	5.05
SR 96	5.07
SR 96	5.11
SR 96	5.14
SR 96	5.17
SR 96	5.19
SR 96	5.23
SR 96	5.27
SR 96	5.29
SR 96	5.45

MANHOLES	
ROUTE	SLM
SR 96	0.45
SR 96	0.47
SR 96	4.45
SR 96	4.55
SR 96	4.67
SR 96	4.79
SR 96	4.80
SR 96	4.85
SR 96	4.92
SR 96	4.96
SR 96	5.02
SR 96	5.35

CATCH BASINS	
ROUTE	SLM
SR 96	4.70
SR 96	4.71

WATER MAINS	
ROUTE	SLM
SR 96	4.71
SR 96	4.77
SR 96	4.94
SR 96	5.16
SR 96	5.27
SR 96	5.28
SR 96	5.29

DRAINAGE

ITEM 604 - CASTINGS ADJUSTED TO GRADE

THE CASTING TO BE ADJUSTED MAY OR MAY NOT HAVE AN EXISTING FRAME. THE WORK SHALL CONSIST OF ADJUSTING THE EXISTING CASTING TO THE SATISFACTION OF THE ENGINEER. IT IS NOT INTENDED TO PLACE NEW FRAMES WHERE NONE CURRENTLY EXIST. THE CONTRACTOR IS REMINDED TO FIELD CHECK ALL ADJUSTMENT TO GRADE ITEMS PRIOR TO BIDDING, AS NO ADDITIONAL COMPENSATION WILL BE GRANTED FOR LABOR AND MATERIALS REQUIRED TO SATISFACTORILY ADJUST CASTINGS WITHOUT FRAMES.

MANHOLES ADJUSTED TO GRADE:

- 01/STR/PV: 2 EACH
- 02/S<2/PV: 10 EACH

PAVEMENT

**ITEM 253, PAVEMENT REPAIRS, AS PER PLAN
ITEM 253 - PAVEMENT REPAIR, MISC.: LONGITUDINAL PAVEMENT REPAIRS**

THESE ITEMS OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. CORING HAS BEEN PERFORMED TO HELP DETERMINE THE COMPONENTS THAT MAY BE ENCOUNTERED DURING THIS ITEM OF WORK. THE PAVEMENT CORING INFORMATION IS SHOWN ON SHEET 4.

THE ENGINEER SHALL DESIGNATE THE LOCATIONS AND LIMITS OF THE AREAS TO BE REPAIRED. PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE INTERMEDIATE AND/OR SURFACE COURSE. THE REPAIR AREAS SHALL BE SAW CUT AND EXCAVATED TO PROVIDE STRAIGHT AND VERTICAL SURFACES AROUND THE PERIMETER OF THE REPAIR AREA. PAVEMENT PLANING MAY BE USED AS AN ALTERNATIVE TO SAW CUTTING AND EXCAVATING. THE PAVEMENT SHALL BE REMOVED WITHIN THE DESIGNATED AREAS BY METHODS WHICH WILL NOT DAMAGE ADJACENT PAVEMENT. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MAXIMUM DEPTH OF 9", BASED ON THE PAVEMENT DESIGN AND AN AVERAGE DEPTH OF 3" AND AN AVERAGE WIDTH OF 5 FT IN RURAL AREAS AND AN AVERAGE DEPTH OF 4" AND AN AVERAGE WIDTH OF 6 FT IN URBAN AREAS FOR ESTIMATING PURPOSES. THE MATERIALS REMOVED SHALL BE DISPOSED OF IN ACCORDANCE WITH 105.16 AND 105.17.

THE CONTRACTOR SHALL BE CAPABLE OF PERFORMING PAVEMENT REPAIRS 2 FEET WIDE.

REPLACEMENT MATERIAL SHALL BE ITEM 301, ITEM 448 TYPE 2, OR ITEM 442 19MM MATERIAL AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. ITEM 301 ASPHALT CONCRETE, PG64-22 CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 3" AND 12" WITH A MAXIMUM PAVEMENT LIFT OF 6". ITEM 448 TYPE 2 OR ITEM 442 19MM CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 1.5" AND 5" WITH A MAXIMUM PAVEMENT LIFT OF 3". THE CONTRACTOR HAS THE OPTION OF USING EITHER ITEM 301, ITEM 448 TYPE 2, OR ITEM 442 19MM MATERIAL WHEN THE PAVEMENT REPAIR IS BETWEEN 3" AND 5" DEEP. ITEM 448 TYPE 2 OR ITEM 442 19MM MATERIAL SHALL BE PG64-22 FOR MEDIUM MIX DESIGN PAVEMENTS AND PG64-28 FOR HEAVY MIX DESIGN PAVEMENTS. ALL EXISTING PAVEMENT AREAS WHICH WILL BE IN CONTACT WITH THE PAVEMENT REPAIR SHALL BE CLEANED AND COATED PER CMS 401.14, USING AN ASPHALT MATERIAL COMPLYING WITH 407.02. ALL COMPACTION SHALL BE ACHIEVED BY MECHANICAL METHODS TO THE SATISFACTION OF THE ENGINEER.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES ITEM 253 PAVEMENT REPAIR, MISC.: PARTIAL DEPTH IS TO BE A MAXIMUM OF 4" DEEP AND ITEM 253 PAVEMENT REPAIR, AS PER PLAN IS FOR DEPTHS GREATER THAN 4". PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, (BY TICKET WEIGHT CONVERSION), OF ITEM 253 - PAVEMENT REPAIR, AS PER PLAN OR ITEM 253 - PAVEMENT REPAIR, MISC.: PARTIAL DEPTH. THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ITEM 253 PAVEMENT REPAIR, MISC.: LONGITUDINAL PAVEMENT REPAIRS
1344 CU. YD. (01/STR/PV)
745 CU. YD. (03/S<2/PV)

ITEM 253 PAVEMENT REPAIR, FULL DEPTH, AS PER PLAN
100 CU. YD. (01/STR/PV)
50 CU. YD. (03/S<2/PV)

SUMMARY FOR ITEM 253 PAVEMENT REPAIR, MISC.: LONGITUDINAL PAVEMENT REPAIRS (FOR INFORMATION ONLY)

ASD-96 EASTBOUND:

01/STR/PV:	
SLM 0.00-1.00:	167 CU. YD.
SLM 1.00-2.00:	281 CU. YD.
SLM 2.00-3.00:	144 CU. YD.
SLM 3.00-4.00:	114 CU. YD.
SLM 4.00-4.25:	23 CU. YD.
03/S<2/PV:	
SLM 4.25-5.00:	172 CU. YD.
SLM 5.00-5.46:	76 CU. YD.

ASD-96 WESTBOUND:

01/STR-PV:	
SLM 0.00-1.00:	241 CU. YD.
SLM 1.00-2.00:	192 CU. YD.
SLM 2.00-3.00:	143 CU. YD.
SLM 3.00-3.91:	39 CU. YD.
03/S<2/PV:	
SLM 3.91-4.50:	188 CU. YD.
SLM 4.50-5.46:	309 CU. YD.

PAVEMENT

PAVEMENT CORING INFORMATION

CO.	ROUTE	SLM	ASPHALT DEPTH (IN.)	BRICK DEPTH (IN.)	WHEEL TRACK / SHOULDER	DIRECTION	YEAR CORED
ASD	96	0.52	11	4.0	INSIDE	EB	2011
ASD	96	0.52	9.5	4.0	OUTSIDE	EB	2011
ASD	96	0.52	5	0.0	SHOULDER	EB	2011
ASD	96	2.08	12.5	4.0	INSIDE	EB	2011
ASD	96	2.08	9.5	4.0	OUTSIDE	EB	2011
ASD	96	2.08	5.5	0.0	SHOULDER	EB	2011
ASD	96	3.82	10	4.0	INSIDE	EB	2011
ASD	96	3.82	10	4.0	OUTSIDE	EB	2011
ASD	96	3.82	3	0.0	SHOULDER	EB	2011
ASD	96	4.75	5.5	4.0	INSIDE	EB	2011
ASD	96	4.75	6	4.0	OUTSIDE	EB	2011
ASD	96	4.75	6	4.0	SHOULDER	EB	2011

BUTT JOINTS

BUTT JOINTS SHALL NOT BE CUT AND LEFT OPEN TO TRAFFIC. THEY SHALL BE FILLED IN WITH A TEMPORARY ASPHALT CONCRETE WEDGE USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

CONSTRUCTION "BUMP" (W8-1-36) AND "ADVISORY SPEED" (W13-1-24) SIGNS SHALL BE ERECTED AND MAINTAINED DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. THESE SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC.

SAFETY EDGE

IN ADDITION TO THE REQUIREMENTS OF 401.12, ATTACH A DEVICE TO THE SCREED OF THE PAVER THAT CONFINES THE MATERIAL AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A COMPACTED WEDGE SHAPE PAVEMENT EDGE OF APPROXIMATELY 30 DEGREES (NOT STEEPER THAN 40 DEGREES). ENSURE THE DEVICE MAINTAINS CONTACT WITH THE EXISTING SURFACE, AND ALLOW FOR AUTOMATIC TRANSITION TO CROSS ROADS, DRIVEWAYS, AND OBSTRUCTIONS. DO NOT USE CONVENTIONAL SINGLE PLATE STRIKE OFF.

CONSTRUCTION OF SAFETY EDGE CAN BE OMITTED AT LOCATIONS WHERE EXISTING WIDTH OF GRADED SHOULDER OR BERM IS LESS THAN 12". PROJECTS WITH VARYING CONDITIONS SHOULD USE SAFETY EDGE WHERE POSSIBLE. PLAN PREPARATION HAS MADE EVERY REASONABLE ATTEMPT TO IDENTIFY POSSIBLE SAFETY EDGE LOCATIONS.

USE THE TRANSTECH SHOULDER WEDGE MAKER, THE CARLSON SAFETY EDGE END GATE, THE ADVENT-EDGER, THE TROXLER SAFESLOPE, OR A SIMILAR APPROVED-EQUAL DEVICE THAT PRODUCES THE SAME WEDGE CONSOLIDATION RESULTS. CONTACT INFORMATION FOR THESE WEDGE SHAPE COMPACTION DEVICES IS THE FOLLOWING:

TRANSTECH SYSTEMS, INC.
1594 STATE STREET
SCHENECTADY, NY 12304
1-800-724-6306
www.transtechsys.com

ADVANT-EDGE PAVING EQUIPMENT LLC
P.O. BOX 9163
NISKAYUNA, NY 12309-0163
518-280-6090
www.advantageedgepaving.com

CARLSON SAFETY EDGE END GATE
18450 50TH AVENUE EAST
TACOMA, WA 98446
253-875-8000

TROXLER ELECTRONICS LABORATORIES INC.
3008 E. CORNWALLIS RD.
RESEARCH TRIANGLE PARK, NC 27709
1-877-TROXLER
www.troxerlabs.com

IF ELECTING TO USE A SIMILAR DEVICE, PROVIDE PROOF THAT THE DEVICE HAS BEEN USED ON PREVIOUS PROJECTS WITH ACCEPTABLE RESULTS OR CONSTRUCT A TEST SECTION PRIOR TO THE BEGINNING OF WORK AND DEMONSTRATE WEDGE COMPACTION TO THE SATISFACTION OF THE ENGINEER. SHORT SECTIONS OF HANDWORK WILL BE ALLOWED WHEN NECESSARY FOR TRANSITIONS AND TURNOUTS OR OTHERWISE AUTHORIZED BY THE ENGINEER.

IN ADDITION TO THE REQUIREMENTS OF 401.16, MAKE THE FIRST ROLLER PASS 8 TO 12 INCHES (200 TO 300 MM) AWAY FROM TAPERED EDGE. DO NOT ROLL THE TAPER.

PAVEMENT

INTERSECTIONS AND DRIVES

RURAL-INTERSECTIONS SHALL BE PLANED AND PAVED TO THE END OF THE RADII OR AS DIRECTED BY THE ENGINEER. (TO PROVIDE A SMOOTH TRANSITION BETWEEN THE TWO HIGHWAYS, AND TO ELIMINATE WATER POCKETS).

URBAN-INTERSECTIONS SHALL BE PLANED AND PAVED TO THE BACK OF CROSSWALKS OR AS DIRECTED BY THE ENGINEER. (TO PROVIDE A SMOOTH TRANSITION BETWEEN THE TWO HIGHWAYS, AND TO ELIMINATE WATER POCKETS).

EXISTING PAVED DRIVES SHALL BE PAVED SO AS TO PROVIDE A SMOOTH TRANSITION BETWEEN THE HIGHWAY AND THE DRIVE, (DISTANCE FROM EDGE OF ROADWAY MAY VARY AT EACH DRIVE) AS DIRECTED BY THE ENGINEER.

EXISTING AGGREGATE DRIVES SHALL BE PAVED WITH AN APRON AN AVERAGE WIDTH OF 4 FT. THE SLOPE OF THIS APRON SHALL BE THE SAME AS THE ADJACENT PAVEMENT SLOPE OR AS DIRECTED BY THE ENGINEER. ANY GRADING NEEDED TO PAVE THE APRON SHALL BE INCLUDED IN THE RELATED ASPHALT ITEM FOR PAYMENT. ITEM 617 COMPACTED AGGREGATE, AS PER PLAN SHALL BE PLACED ADJACENT TO THIS APRON TO PROVIDE A SMOOTH TRANSITION FROM THE APRON TO THE EXISTING DRIVE, (WIDTH OF THIS 617 APPLICATION MAY VARY) AS DIRECTED BY THE ENGINEER. AN ADDITIONAL QUANTITY OF ITEM 617 HAS BEEN ESTIMATED TO COMPLETE THIS WORK AND IS SHOWN AS AN EXTRA AREA ON THE PAVEMENT & SHOULDER DATA SHEET.

ANY HAZARD OR UNSAFE CONDITION RESULTING FROM THE ABOVE WORK MUST BE CORRECTED IMMEDIATELY. THE CONTRACTOR IS REMINDED OF SECTIONS 105.01, 107.07 & 614.02A OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE (CURBED SECTION)

THE INTENT OF THE PLANING IS TO MILL THE SPECIFIED DEPTH ALONG THE CURB CONTINGENT ON THE FOLLOWING: THE MAXIMUM CROSS SLOPE SHALL BE 0.02 WHILE THE MINIMUM CROSS SLOPE SHALL BE 0.01. THE PAVEMENT SLOPE SHALL BE CONTINUOUS BETWEEN THE CROWN AND THE CURB WHILE TRYING TO ACHIEVE THE TYPICAL CROSS SLOPE OF 0.016. THE CROWN OF THE PAVEMENT SHALL BE LOCATED BETWEEN THE TRAVELED LANES, OR AS DIRECTED BY THE ENGINEER. THE MILLING DEPTH SHALL BE CONTROLLED FROM THE CURB, TO PRODUCE A CROSS SLOPE IN CONFORMANCE WITH THE ABOVE GUIDELINES.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER. IF THERE IS NO INFORMATION IN THE PLANS TO CHANGE THE SUPERELEVATION, THE INTENT IS TO MAINTAIN THE EXISTING SUPERELEVATION.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE INTO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. THE 14 CALENDAR DAYS SHALL BE CONSIDERED AN INTERIM COMPLETION DATE (SECTION 108) AND FOR EACH CALENDAR DAY BEYOND THE 14 DAYS THAT THE ROADWAY REMAINS EXPOSED TO THE PLANED SURFACE, THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE OF \$1000 PER DAY.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF PAVEMENT PLANING, ASPHALT CONCRETE.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (NON-CURBED SECTION)

THE INTENT OF THE PLANING IS TO MILL 2.25 INCHES AT THE CENTER OF PAVEMENT AT NON-CURBED AREAS. THE PAVEMENT SLOPE SHALL BE 0.010 MINIMUM AND 0.016 PREFERRED, CONTINUOUS BETWEEN THE CROWN AND THE PROPOSED EDGELINE/SHOULDER. THE MILLING DEPTH SHALL BE CONTROLLED FROM THE CENTER OF PAVEMENT IN CONFORMANCE WITH THE ABOVE GUIDELINES.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER. IF THERE IS NO INFORMATION IN THE PLANS TO CHANGE THE SUPERELEVATION, THE INTENT IS TO MAINTAIN THE EXISTING SUPERELEVATION.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. FOR EACH CALENDAR DAY BEYOND THE 14 DAYS THAT THE ROADWAY REMAINS EXPOSED TO THE PLANED SURFACE, THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE OF \$1000 PER DAY.

DRAINAGE SLOTS SHALL BE CUT INTO THE SHOULDER(S) AT THE LOW POINT OF EACH PLANED SECTION TO PREVENT TRAPPED WATER PUDDLES, AND REFILLED DURING RESURFACING. CUTTING AND FILLING DRAINAGE SLOTS SHALL BE INCLUDED IN PAYMENT WITH ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (NON-CURBED SECTION) CONTINUED

THE AMOUNT OF GRINDINGS RESULTING FROM THIS WORK MAY PRODUCE UNEXPECTED VOLUMES OF GRINDINGS DUE TO THE EXISTING TRANSVERSE SLOPE OF THE PAVEMENT.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE.

ITEM 254 - PATCHING PLANED SURFACE

AN ESTIMATED QUANTITY OF ITEM 254 - PATCHING PLANED SURFACE HAS BEEN SET UP TO BE USED AS DIRECTED BY THE ENGINEER AS DESCRIBED IN CMS 254.04. THE LIMIT OF THE PATCHING DEPTH IS 0 TO 2 IN.

**ITEM 407, TACK COAT
ITEM 407, TACK COAT FOR INTERMEDIATE COURSE**

AS PER 407.06 THE APPLICATION RATES SHALL BE 0.08 GAL. PER SQ. YD. PRIOR TO THE INTERMEDIATE COURSE AND SHALL BE 0.04 GAL PER SQ. YD. PRIOR TO THE SURFACE COURSE FOR ESTIMATING PURPOSES ONLY. THE RATE OF APPLICATION SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. A COMPLETE PAVEMENT SURFACE COVERAGE SHALL BE REQUIRED. AREAS OF TACK STRIPPED BY CONSTRUCTION EQUIPMENT OR TRAFFIC SHALL BE RE-COATED PRIOR TO PLACING ASPHALT CONCRETE. ALL COSTS AS DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID PER GALLON FOR ITEM 407, TACK COAT AND ITEM 407 TACK COAT FOR INTERMEDIATE COURSE.

ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 9.5 MM, TYPE A (448), AS PER PLAN

ALL OPEN TRANSVERSE JOINTS SHALL BE TAPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC. A "BUMP" SIGN (W8-1-36) SHALL BE ERECTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER JOINT HAS BEEN CLOSED. PLACEMENT OF SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

CARE SHALL BE TAKEN TO MATCH EXISTING PAVEMENT ELEVATIONS AT EXISTING PAVED BERMS, DRIVES, INTERSECTIONS, ETC.

REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:
MIX DESIGN: FOR Ndes USE 50 GYRATIONS, FOR Nmax USE 75 GYRATIONS.
MINIMUM TOTAL PG BINDER CONTENT IS 6.0 PERCENT.
USE A PG 64-22 BINDER.
MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT IS 20 PERCENT.
WHEN AN AGGREGATE SOURCE IS SPECIALLY DESIGNATED WITH AN SR ON THE AGGREGATE GRAVITY LIST DO NOT USE THE AGGREGATE EXCEPT AS ALLOWED FOR MEDIUM TRAFFIC IN THE GUIDELINES FOR MAINTAINING ADEQUATE PAVEMENT FRICTION IN SURFACE PAVEMENT.
QUALITY CONTROL: DO NOT PERFORM Nmax IN QUALITY CONTROL TESTING. DO NOT TAKE EXTRA ASPHALT BINDER SAMPLES AS OUTLINED IN CMS 442.05.

ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, 9.5MM, TYPE A (448), AS PER PLAN (VARIABLE, 1 1/2)

THIS ITEM SHALL BE USED FOR CORRECTION OF CROWN, PROFILE AND ANY OTHER IRREGULARITIES.

ALL OPEN TRANSVERSE JOINTS SHALL BE TAPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC. A "BUMP" SIGN (W8-1-36) SHALL BE ERECTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER JOINT HAS BEEN CLOSED. PLACEMENT OF SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:
MIX DESIGN: FOR Ndes USE 50 GYRATIONS, FOR Nmax USE 75 GYRATIONS.
MINIMUM TOTAL PG BINDER CONTENT IS 6.0 PERCENT.
USE A PG 64-22 BINDER.
MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT IS 20 PERCENT.
QUALITY CONTROL: DO NOT PERFORM Nmax IN QUALITY CONTROL TESTING. DO NOT TAKE EXTRA ASPHALT BINDER SAMPLES AS OUTLINED IN CMS 442.05.

DESIGN FILE: I:\projects\86718\roadway\sheets\86718\GN001.dgn
WORKSTATION: baraty
MODELNAME: Design
DATE: 1/7/2013

CALCULATED
NRF
CHECKED
ADB

GENERAL NOTES

ASD-96-0-0.00

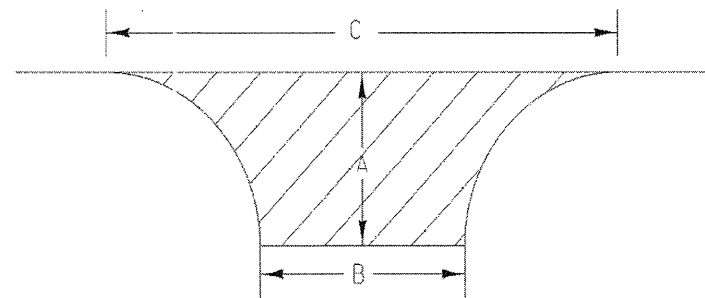
ITEM 442 - ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM. TYPE A (448), AS PER PLAN (VARIABLE, 1.75%)

THIS ITEM SHALL BE USED FOR CORRECTION OF CROWN, PROFILE AND ANY OTHER IRREGULARITIES.

ALL OPEN TRANSVERSE JOINTS SHALL BE TAPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC. A "BUMP" SIGN (W8-1-36) SHALL BE ERECTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER JOINT HAS BEEN CLOSED. PLACEMENT OF SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:
 MIX DESIGN: FOR N_{des} USE 50 GYRATIONS, FOR N_{max} USE 75 GYRATIONS. USE A PG 64-22 BINDER.
 MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT IS 30 PERCENT. APPLY 703.05 FOR COARSE AND FINE AGGREGATE EXCEPT GRADATION FOR FINE AGGREGATE DOES NOT APPLY.
 QUALITY CONTROL: DO NOT PERFORM N_{max} IN QUALITY CONTROL TESTING. DO NOT TAKE EXTRA ASPHALT BINDER SAMPLES AS OUTLINED IN CMS 442.05.

INTERSECTION AREA CALCULATIONS



INTERSECTION NAME	A (FT)	B (FT)	C (FT)	AREA (SQ YD)
01/STR/PV				
TR 1453 (RT)	19	27	75	91
TR 1433 (RT)	14	28	70	65
TR 1443 (LT)	23	17	63	83
TR 1353 (RT)	17	22	50	59
TR 1353 (LT)	14	22	59	53
TR 1233 (LT)	13	20	42	39
TR 1253 (RT)	30	29	88	162
TR 1253 (LT)	16	22	60	62
TR 1193 (LT)	12	33	74	62
TR 1153 (RT)	15	33	66	73
TR 1153 (LT)	14	32	69	69
02/S<2/PV				
VALENTINE VALLEY DR (RT)	18	20	45	57
WARD DR (LT)	10	29	46	39
MOWRY RD (LT)	12	31	49	49
RIDGE RD (LT)	15	30	50	61
EDISON ST (LT)	17	24	51	62
MASTERS AVE (LT)	31	28	82	158
PARKSIDE DR (RT)	15	34	41	61
MONROE ST (LT)	20	20	38	58
VINE ST (LT)	23	53	98	174
LINDALE AVE (RT)	17	19	42	50
DORCHESTER ST (RT)	16	25	50	59
PLEASANT ST (LT)	25	46	101	179
VESPER CT	8	12	18	12
VESPER ST (RT)	18	18	26	41
W 4TH ST (LT)	45	19	81	198
PERRY ST (LT)	19	26	92	101
RACE ST (RT)	19	32	59	87
GOODMAN CT (LT)	8	12	18	12

RAILROAD CROSSINGS

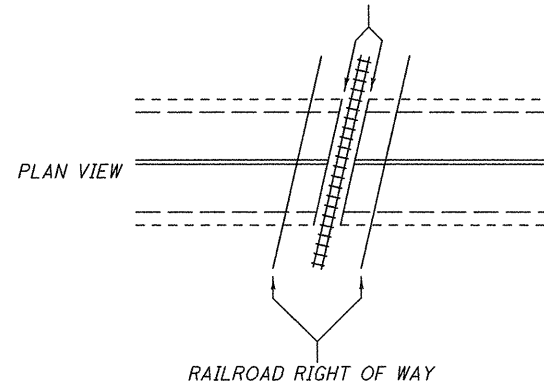
PRIOR TO ANY WORK AT RAILROAD CROSSINGS THE CONTRACTOR SHALL CONTACT THE AFFECTED RAILROAD AUTHORITY SO AS TO MAKE THEM AWARE OF THE PROGRESS AND SCHEDULE OF WORK. THE CONTRACTOR SHALL COOPERATE WITH THE RAILROAD SO AS TO ELIMINATE ANY SAFETY CONCERNS. FLAGGING WILL BE REQUIRED BY THE RAILROAD. REFER TO THE RAILROAD LIABILITY INSURANCE PROPOSAL NOTE.

THE CROWN SHALL BE WORKED OUT OF THE RESURFACED PAVEMENT ON EACH SIDE OF THE RAILROAD CROSSING, BEGINNING 50 FEET FROM THE NEAREST RAIL, BY RAISING THE EDGES OF THE RESURFACED PAVEMENT TO MEET THE PLATFORM ELEVATION.

SUSPEND AND RESUME RESURFACING AT THE HEADER TIE, AS DIRECTED BY THE ENGINEER.

DETAIL - PAVING AT RAILROAD CROSSING

BUTT JOINT/BEGIN AND END RESURFACING AS DIRECTED BY THE ENGINEER

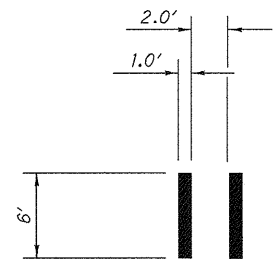


- NOTE:
- DO NOT DISTURB RAILROAD GATES
 - RE-INSTALL PAVEMENT MARKINGS
 - RAILROAD MAY DIRECT ENGINEER ON THE LOCATION OF BUTT JOINTS. OTHERWISE OMIT AND RESUME RESURFACING AT THE EDGE OF THE EXISTING CROSSING SURFACE ON BOTH SIDES OF THE TRACK.

TRAFFIC CONTROL

ITEM 644 - CROSSWALK LINE, AS PER PLAN

THE MARKING DETAIL SHOWN BELOW SHALL APPLIED TO ALL CROSSWALKS INSIDE OF CITY LIMITS ON ASD-96 MAINLINE AND ALL INTERSECTIONS GETTING THE RADIUS PAVED.



MAINTENANCE OF TRAFFIC

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO CONSTRUCT A TEMPORARY ASPHALT WEDGE FROM THE EXISTING PAVEMENT TO THE PLANED SURFACE AT BUTT JOINTS AND OTHER LOCATIONS THAT RESULT IN A DROP-OFF IN EXCESS OF 1.5 INCHES. THIS QUANTITY SHALL ALSO BE USED AT PLANED SURFACES WHERE A TEMPORARY ASPHALT WEDGE IS NEEDED AROUND CASTINGS. BEFORE RESURFACING OF THE PAVEMENT, THE TEMPORARY WEDGE SHALL BE REMOVED AND THE COST SHALL BE CONSIDERED INCIDENTAL TO ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC
 25 CU. YD. (01/STR/PV)
 25 CU. YD. (02/S<2/PV)

ITEM 614, MAINTAINING TRAFFIC

A MINIMUM OF ONE (1) LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES USING FLAGGERS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 - WORK ZONE MARKING SIGN

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR TEMPORARY WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS, 614.04.

- 01/STR/PV:
 WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE = 11 EACH
 WORK ZONE MARKING SIGN: (R4-1-24) DO NOT PASS = 9 EACH
 WORK ZONE MARKING SIGN: (R4-2-24) PASS WITH CARE = 8 EACH
 TOTAL = 28 EACH
- 02/S<2/PV:
 WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE = 3 EACH
 TOTAL = 3 EACH

ITEM SPECIAL, MAILBOX SUPPORT SYSTEM

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF EXISTING NON-STANDARD MAILBOX SUPPORTS AND FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED HARDWARE IN ACCORDANCE WITH THE DETAILS SHOWN, AND ATTACHING AN OWNER SUPPLIED MAILBOX, AT LOCATIONS DETERMINED BY THE ENGINEER.

IN ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE BOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL SUPPLY ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION. SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO MAILBOXES MAY BE MOUNTED ON A SINGLE POST. [HARDWARE SHALL BE COMMERCIAL GRADE GALVANIZED STEEL.]

WOOD POSTS SHALL BE NOMINAL 4 IN. x 4 IN. (S4S) OR 4 1/2 IN. DIAMETER ROUND, AND CONFORM TO 710.14. STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2 IN. I.D., AND CONFORM TO AASHTO M 181.

POSTS SHALL BE SET AS PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WORK WITH THE LOCAL POST MASTER AND NOTIFYING THE PROPERTY OWNERS PRIOR TO WORK.

GROUP MAILBOX SUPPORTS SHALL BE PLACED ON 3 FT. CENTERS AND THE TURNOUT LENGTHENED TO ACCOMMODATE THE GROUPING.

WHERE GUARDRAIL EXISTS, MAILBOXES AND THEIR SUPPORTS SHALL BE PLACED BEHIND THE GUARDRAIL. SUPPORTS MUST STILL MEET THE BREAKAWAY REQUIREMENTS LISTED ABOVE.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DESCRIBED ABOVE.

ITEM SPECIAL-MAILBOX SUPPORT SYSTEM, SINGLE

01/STR/PV-----4 SYSTEMS

MAILBOX APPROACHES

THE MAILBOX APPROACHES SHALL BE PAVED WITH 1.0" ITEM 442 INTERMEDIATE COURSE AND 1.25" ITEM 442 SURFACE COURSE. THEY SHALL CONFORM AS MUCH AS PRACTICAL TO STANDARD DRAWING BP-4.1 OR AS DIRECTED BY THE ENGINEER.

GRADING SHALL BE PERFORMED IN THESE AREAS TO OBTAIN A BASE WHICH WILL ALLOW THE FINISHED GRADE TO BE FLUSH WITH ADJACENT PAVEMENT. A QUANTITY OF ITEM 617 COMPACTED AGGREGATE, AS PER PLAN HAS BEEN PROVIDED FOR AREAS WHERE THE SHOULDER IS LOW PRIOR TO GRADING AND/OR LOW AREAS CAUSED BY THE REMOVAL OF UNSUITABLE MATERIAL. QUANTITIES TO PERFORM THIS WORK HAVE BEEN INCLUDED IN THE GENERAL SUMMARY AND ARE ESTIMATED AS FOLLOWS.

ITEM 209 - GRADING MAILBOX APPROACHES:

S.R. 96 01/STR/PV = 38 EACH

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

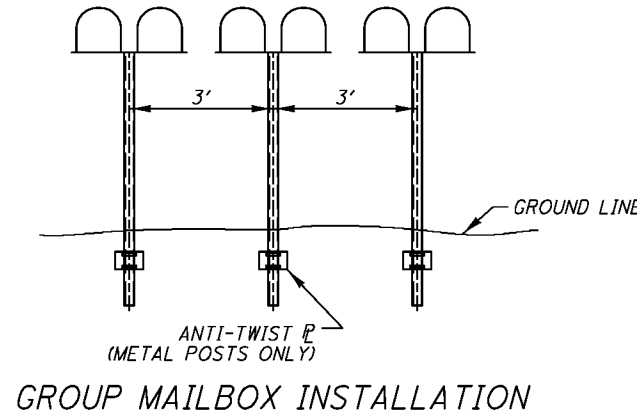
S.R. 96 01/STR/PV = 76 CU. YD.

LOCATIONS OF MAILBOX SUPPORT SYSTEM TO BE REPLACED

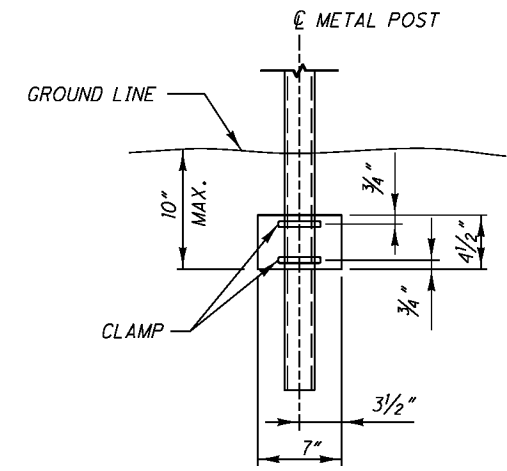
ADDRESSES AND/OR LOCATIONS OF MAILBOX SUPPORT SYSTEM TO BE REPLACED:

SINGLE SUPPORT SYSTEMS (01/STR/PV):

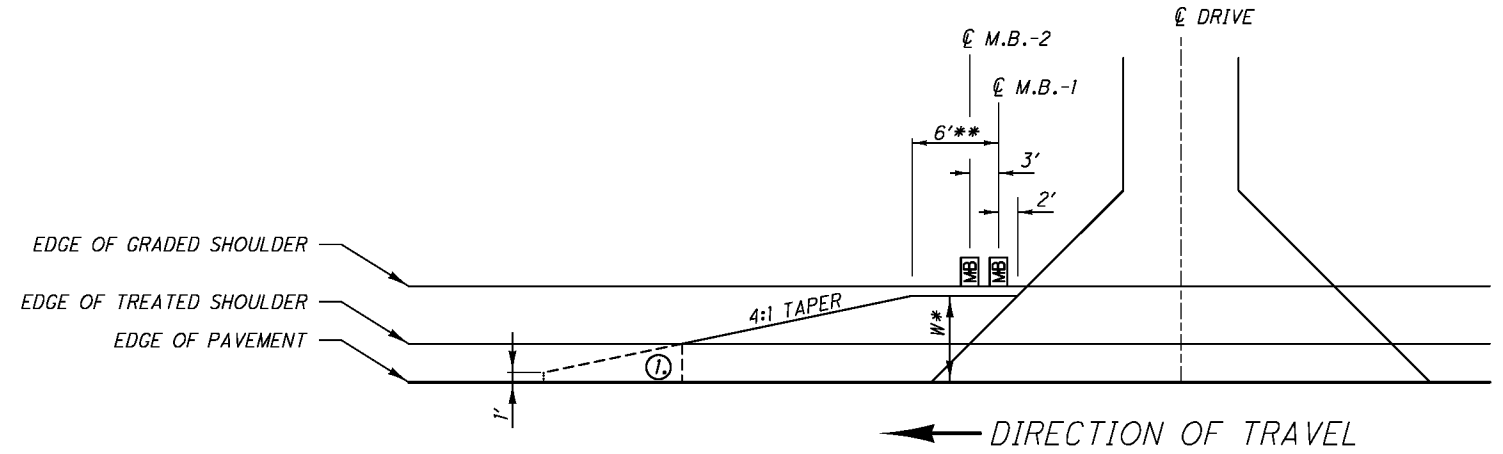
- 1257 SR 96
- 1444 SR 96
- 1458 SR 96
- 1468 SR 96



GROUP MAILBOX INSTALLATION



ANTI-TWIST PLATE



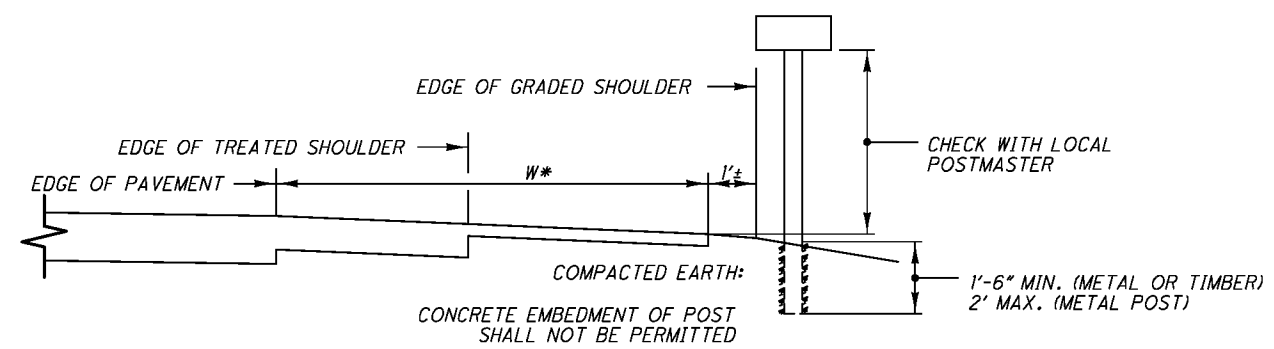
① END MAILBOX TURNOUT AT EDGE OF TREATED SHOULDER OR 1' WHICH EVER IS GREATER.

W* NOTES

- 1) WHERE EXISTING STANDARD MAILBOX POSTS ARE BEHIND GUARDRAIL AND ARE TO REMAIN IN PLACE, TURNOUT WIDTH SHALL EXTEND TO FACE OF GUARDRAIL.
- 2) WHERE NO GUARDRAIL IS REQUIRED, TURNOUT WIDTH SHALL EXTEND TO FACE OF EXISTING STANDARD MAILBOX WITH MAILBOX REMAINING IN PLACE.
- 3) IF THE MAILBOX SUPPORT IS SPECIFIED TO BE REMOVED AND REERECTED OR REPLACED, WHERE GUARDRAIL IS REQUIRED, TURNOUT WIDTH SHALL EXTEND TO FACE OF GUARDRAIL AND MAILBOX SHALL BE INSTALLED BEHIND THE GUARDRAIL.
- 4) IF THE MAILBOX SUPPORT IS SPECIFIED TO BE REMOVED AND REERECTED OR REPLACED, WHERE NO GUARDRAIL IS REQUIRED, TURNOUT WIDTH SHALL BE 6 FT. MINIMUM, EXCEPT WHERE FIELD CONDITIONS WILL NOT PERMIT.

**** NOTE**

- 1) 6' FOR SINGLE MAILBOX SUPPORT, ADD 3 FT. FOR EACH ADDITIONAL MAILBOX.



CROSS SECTION / ELEVATION VIEW

FOR DETAILS NOT SHOWN SEE STANDARD DRAWING BP-4.1

DESIGN FILE: i:\projects\86718\roadway\sheets\86718GM001.dgn
 WORKSTATION: nfoster DATE: 12/10/2012

CALCULATED
 NRF
 CHECKED
 ADB

MAILBOX FACILITIES

ASD-96-0.00

DESIGN FILE: \\projects\86718\roadway\sheets\86718GG001.dgn
 WORKSTATION: baroty DATE: 1/9/2013
 MODEL NAME: Design

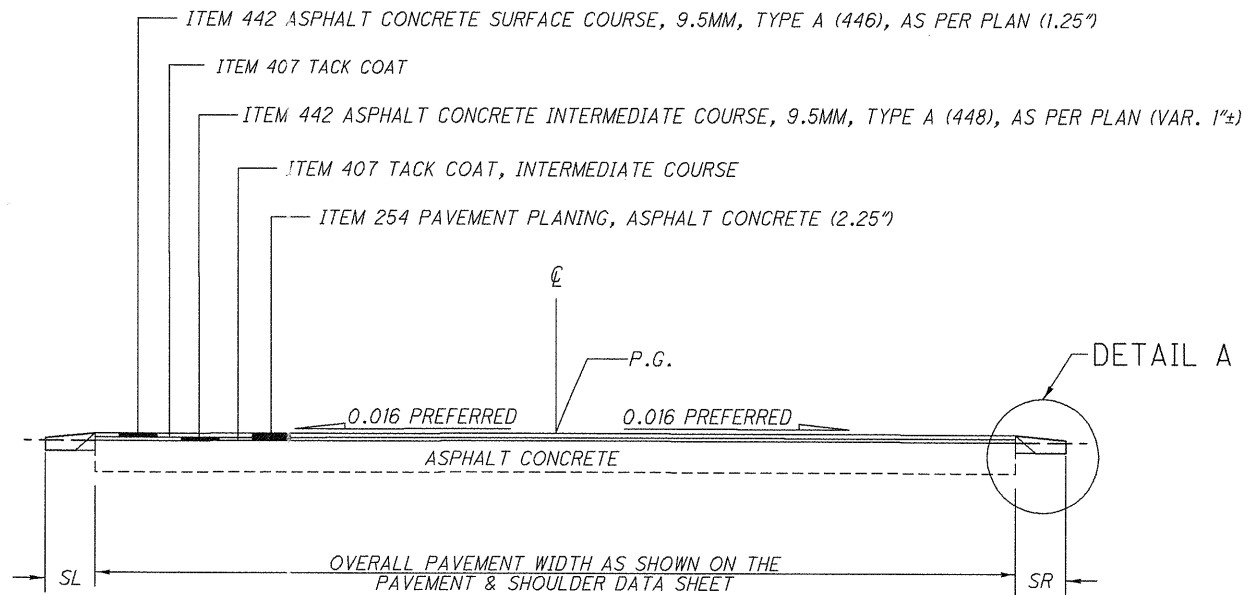
01/STR/PV				02/S<2/PV				03/S<2/PV			FUNDING SPLIT TOTALS			ITEM	ITEM EXT.	TOTAL	UNIT	DESCRIPTION	REF. SHEET				
3	5	6	8	14	17	3	5	8	17	3	10	17	01/STR/PV							02/S<2/PV	03/S<2/PV		
																		ROADWAY					
											134				134			202	30000	134	SQ FT	WALK REMOVED	
				112.5										112.5				202	38000	112.5	FT	GUARDRAIL REMOVED	
				137.5										137.5				202	38200	137.5	FT	GUARDRAIL REMOVED FOR REUSE	
				4										4				202	42000	4	EACH	ANCHOR ASSEMBLY REMOVED, TYPE A	
				4.38										4.38				209	15000	4.38	STATION	RESHAPING UNDER GUARDRAIL	
			7.99					0.53						7.99	0.53			209	72051	8.52	MILE	PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN	8
	38													38				209	80000	38	EACH	GRADING MAILBOX APPROACHES	
				12.5										12.5				606	13000	12.5	FT	GUARDRAIL, TYPE 5	
				137.5										137.5				606	16500	137.5	FT	GUARDRAIL REBUILT, TYPE 5	
				4										4				606	26100	4	EACH	ANCHOR ASSEMBLY, TYPE E	
											134							608	10000	134	SQ FT	4" CONCRETE WALK	
											548							608	52020	548	SQ FT	CURB RAMP, TYPE A2	
											232							608	52044	232	SQ FT	CURB RAMP, TYPE B3	
											2							608	53020	2	SQ FT	DETECTABLE WARNING	
				12										12				626	00100	12	EACH	BARRIER REFLECTOR	
		4												4				SPECIAL	69050100	4	EACH	MAILBOX SUPPORT SYSTEM, SINGLE	
				</																			

* - FOR TYPICALS, SEE SHEET 9

COUNTY	ROUTE	LOG POINT TO LOG POINT		LENGTH		WIDTH FEET AVG. *TYPICAL	PAVEMENT AREA SQ YD	254			407	407	442		442		442		209	AGGREGATE SHOULDER PROPOSED WIDTH		AGGREGATE SHOULDER AREA SQ YD	617		617						
				MILE	FEET			PAVEMENT PLANING, ASPHALT CONCRETE (2.25")	PAVEMENT PLANING, ASPHALT CONCRETE (3.0")	PATCHING PLANED SURFACE	TACK COAT @ 0.08 GAL/SY	TACK COAT FOR INTERM. COURSE @ 0.04 GAL/SY	ASPHALT CONCRETE INTERMEDIATE COURSE, 9.5 MM, TYPE A (448), AS PER PLAN (VAR. 1.75")	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (448), AS PER PLAN (VAR. 1.75")	ASPHALT CONCRETE SURFACE COURSE, 9.5MM, TYPE A (446), AS PER PLAN	ASPHALT CONCRETE, SURFACE COURSE, 9.5MM, TYPE A, (446) (SAFETY EDGE) AS PER PLAN	PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN	SL	SR	1.5 INCHES	SHOULDER PREPARATION										
				STRAIGHT LINE MILEAGE				SQ.YD	SQ.YD	SQ.YD	GALLON	GALLON	INCH	CU.YD.	INCH	CU.YD.	INCH	CU.YD.	CU.YD.	MILE	FT		FT	SQ YD	CU YD	SQ.YD					
<i>01/STR/PV</i>																															
ASD	96	0.00	0.12	0.12	633.6	28.0	1	1,971	1,971		197	158	79	1.00	55			1.25	68			3	0.24	2.0	2.0	282	12	282			
ASD	96	0.12	0.50	0.38	2006.4	27.0	1	6,019	6,019		602	482	241	1.00	167			1.25	209			11	0.76	2.0	2.0	892	37	892			
ASD	96	0.50	1.00	0.50	2640	26.5	1	7,773	7,773		777	622	311	1.00	216			1.25	270			14	1.00	2.0	2.0	1,173	49	1,173			
ASD	96	1.00	1.50	0.50	2640	27.0	1	7,920	7,920		792	634	317	1.00	220			1.25	275			14	1.00	2.0	2.0	1,173	49	1,173			
ASD	96	1.50	2.00	0.50	2640	26.5	1	7,773	7,773		777	622	311	1.00	216			1.25	270			14	1.00	2.0	2.0	1,173	49	1,173			
ASD	96	2.00	2.50	0.50	2640	26.5	1	7,773	7,773		777	622	311	1.00	216			1.25	270			14	1.00	2.0	2.0	1,173	49	1,173			
ASD	96	2.50	3.00	0.50	2640	27.0	1	7,920	7,920		792	634	317	1.00	220			1.25	275			14	1.00	2.0	2.0	1,173	49	1,173			
ASD	96	3.00	3.50	0.50	2640	27.0	1	7,920	7,920		792	634	317	1.00	220			1.25	275			14	1.00	2.0	2.0	1,173	49	1,173			
ASD	96	3.50	3.91	0.41	2164.8	27.0	1	6,494	6,494		649	520	260	1.00	180			1.25	226			12	0.82	2.0	2.0	962	40	962			
ASD	96	3.91	4.25	0.34	1795.2	14.0	1	2,793	2,793		279	223	112	1.00	78			1.25	97			5	0.17	2.0	2.0	399	17	399			
EXTRA AREA FOR INTERSECTIONS								818	818			65	33	1.00	23			1.25	28												
EXTRA AREA FOR PAVED DRIVES								279				22						1.25	10												
EXTRA AREA FOR AGGREGATE DRIVES								864										1.25	30					864				36	864		
EXTRA AREA FOR EX. & PR. MAILBOX APPROACHES								640				51						1.25	22												
TOTAL									65174		6436	5288	2607		1811					2325			115	7.99				436	10,437		
<i>02/S<2/PV</i>																															
ASD	96	3.91	4.25	0.34	1795.2	14.0	1	2,793	2,793		279	223	112	1.00	78			1.25	97			5	0.17	2.0	2.0	399	17	399			
ASD	96	4.25	4.43	0.18	950.4	28.0	1	2,957	2,957		296	237	118	1.00	82			1.25	103			5	0.36	2.0	2.0	422	18	422			
ASD	96	4.43	5.00	0.57	3009.6	26.0	2	8,694	8,694		869	696	348			1.75	423	1.25	302												
ASD	96	5.00	5.46	0.46	2428.8	26.0	2	7,017	7,017		702	561	281			1.75	341	1.25	244												
EXTRA AREA FOR INTERSECTIONS								1458	206	1252		117	58	1.00	6	1.75	61	1.25	51												
EXTRA AREA FOR PAVED DRIVES								657				53						1.25	23												
EXTRA AREA FOR AGGREGATE DRIVES								189										1.25	7					189				8	189		
EXTRA AREA FOR EX. & PR. MAILBOX APPROACHES																															
TOTAL									5,956	16,963	2,146	1,886	917		166			825	826		10	0.53							43	1,010	
GRAND TOTAL									71,130	16,963	8,582	7,174	3,524		1,977			825	3,151		125	8.52							479	11,447	

DESIGN FILE: \\projects\86718\roadway\sheets\86718GQ001.dgn
 WORKSTATION: barabaty
 MODEL NAME: Design
 DATE: 1/7/2013

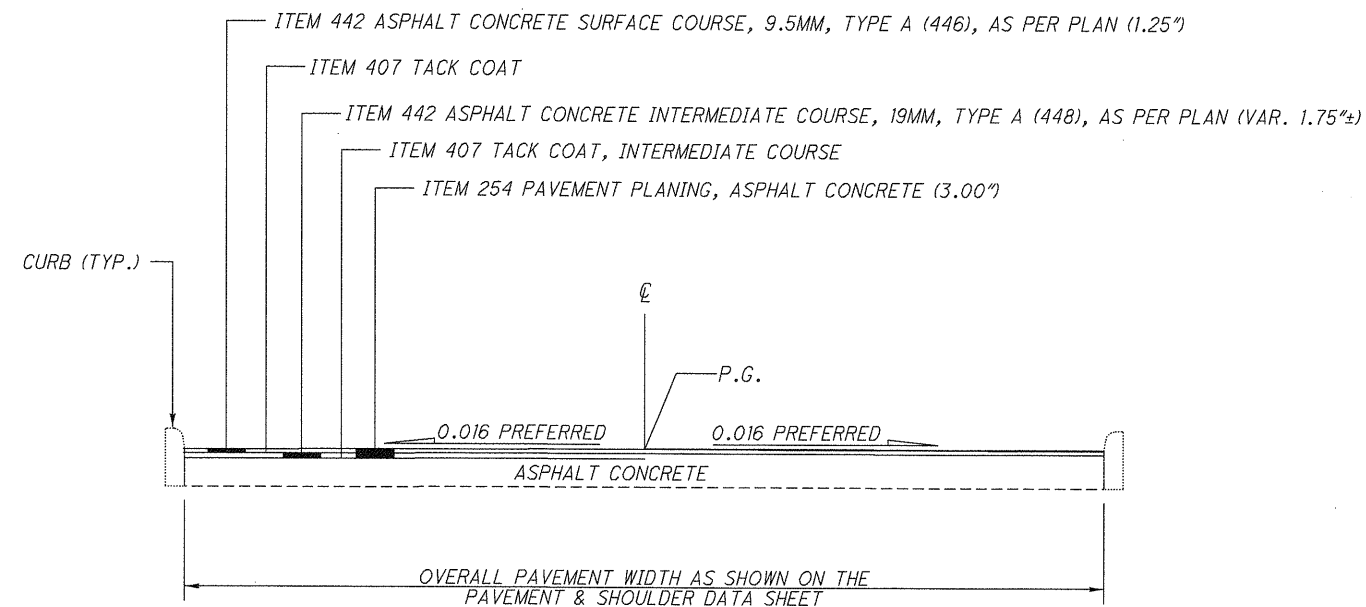
CALCULATED
 NRF
 CHECKED
 ADB
PAVEMENT AND SHOULDER DATA
ASD-96-0.00
 8
 17



TYPICAL 1

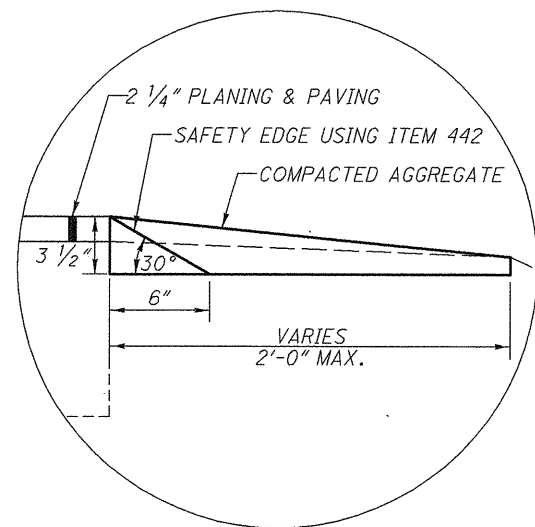
SLM 0.00 TO 4.43

SEE NOTE 1 ON SHEET 17 FOR THRU LANE WIDTH STRIPING.

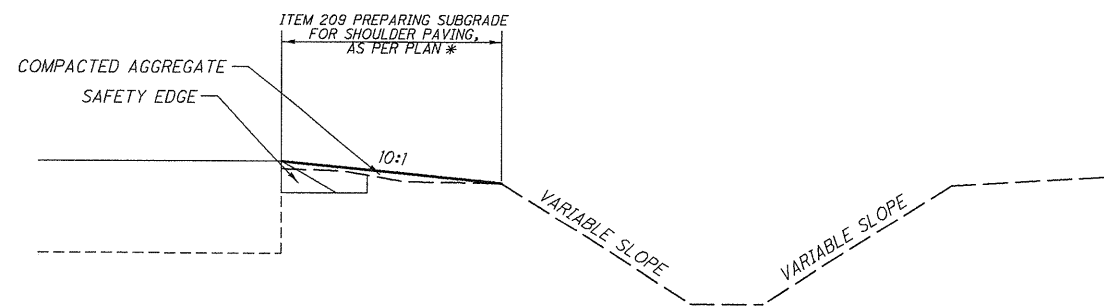


TYPICAL 2

SLM 4.43 TO 5.46



DETAIL A
SAFETY EDGE



ITEM 209 PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN

* SEE ITEM 209 PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN NOTE FOR ADDITIONAL DETAILS

LOCATION	202		608		608		608		608	
	WALK REMOVED		4" CONCRETE WALK		CURB RAMP, TYPE A2		CURB RAMP, TYPE B3		DETECTABLE WARNINGS	
	WEST	EAST	WEST	EAST	WEST	EAST	WEST	EAST	WEST	EAST
	SQ FT		SQ FT		SQ FT		SQ FT		SQ FT	
ASD-96EB, VESPER ST	32	19	32	19	45	45				
ASD-96EB, VESPER CT					18	27				
ASD-96EB, DORCHESTER ST	16	21	16	21	38			32		
ASD-96EB, LINDALE AVE		23		23			62	44		
ASD-96EB, PARKSIDE DR					55					
ASD-96WB, VINE/WICK ST								43	2	
ASD-96 WB, MONORE ST					32	32				
ASD-96WB, PARKSIDE DR					129			51		
ASD-96WB, EDISON ST					36	53				
ASD-96WB, RIDGE RD		23		23		38				
TOTAL	48	86	48	86	353	195	62	170	2	

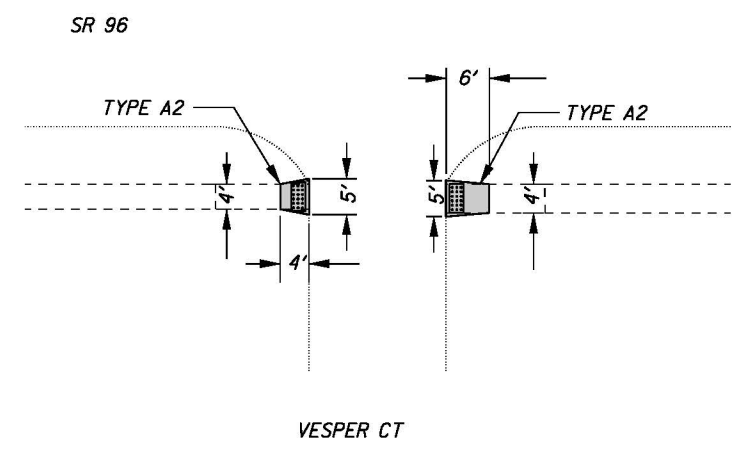
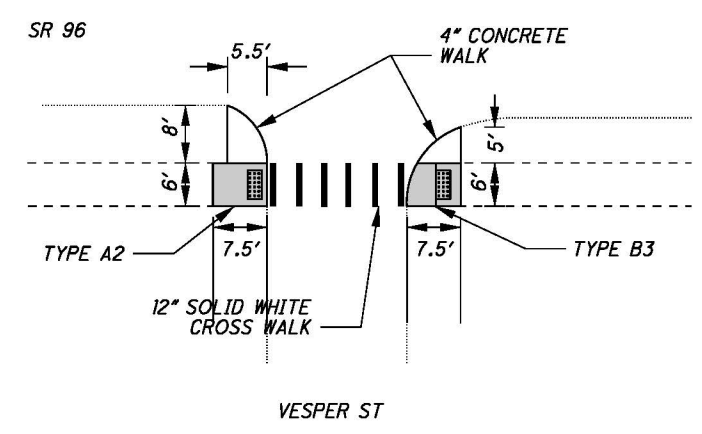
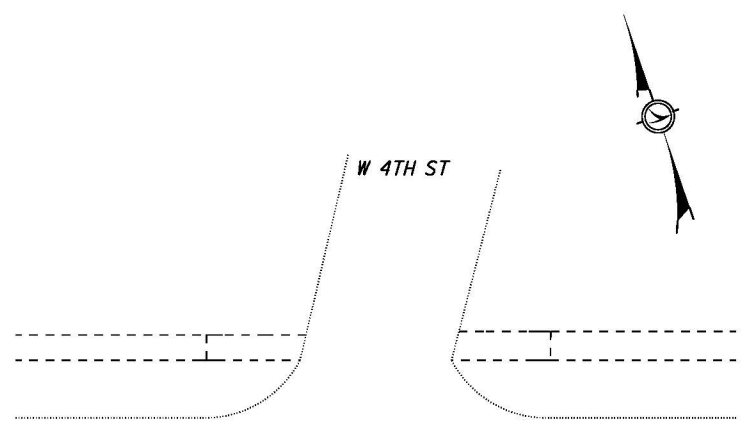
LOCATION	644
	CROSSWALK LINE, AS PER PLAN (740.04)
	FT
EDISON ST	54
MASTERS AVE	54
PARKSIDE DR	78
MONROE ST	54
WICK/ VINE ST	114
LINDALE AVE	42
DORCHESER ST	54
PLEASANT ST	120
VESPER ST	36
W. 4TH ST	96
PERRY ST	60
COTTAGE RD	72
TOTAL	834

LEGEND

■ - NEW CURB RAMP AREA

□ - 4" CONCRETE WALK

NOTE: TABLE PROVIDED FOR INFORMATIONAL USE ONLY. THE CALCULATED CROSSWALK QUANTITY HAS BEEN CARRIED TO THE PAVEMENT MARKING / RPM SUB-SUMMARY SHEET.

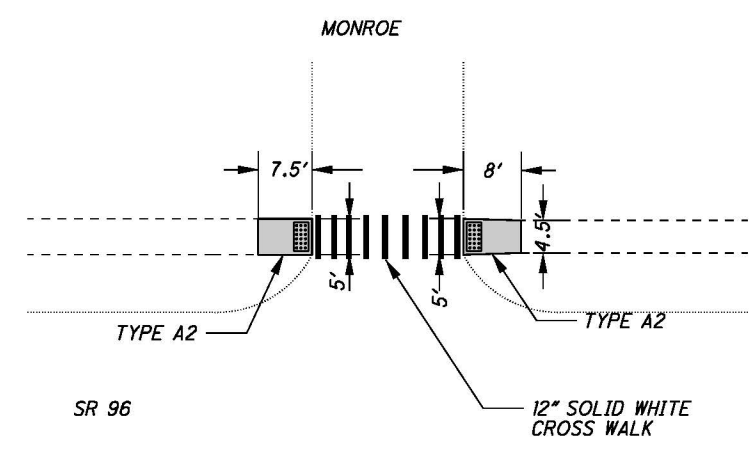
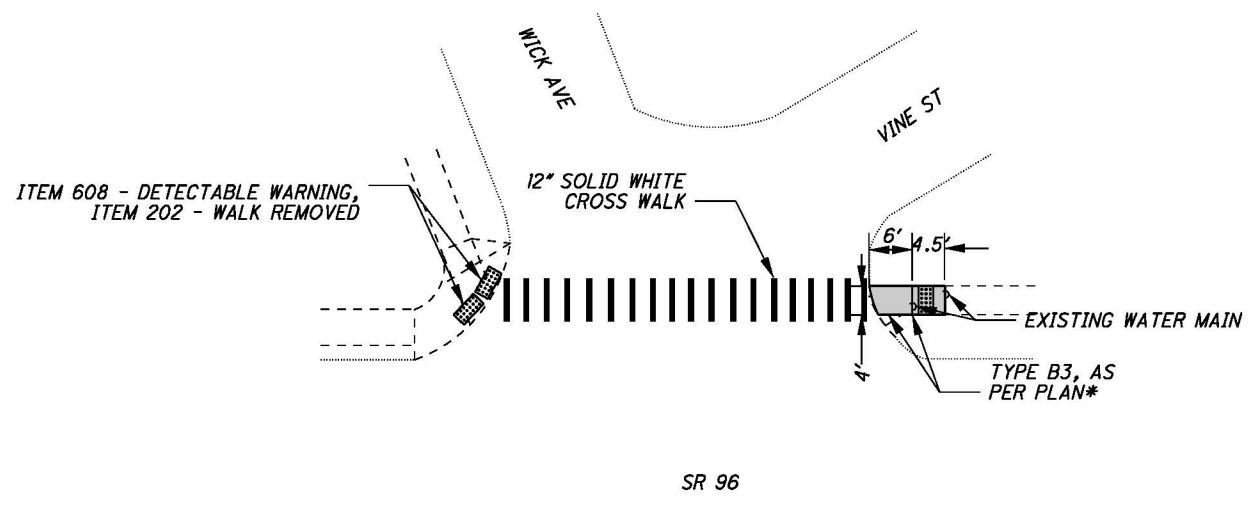
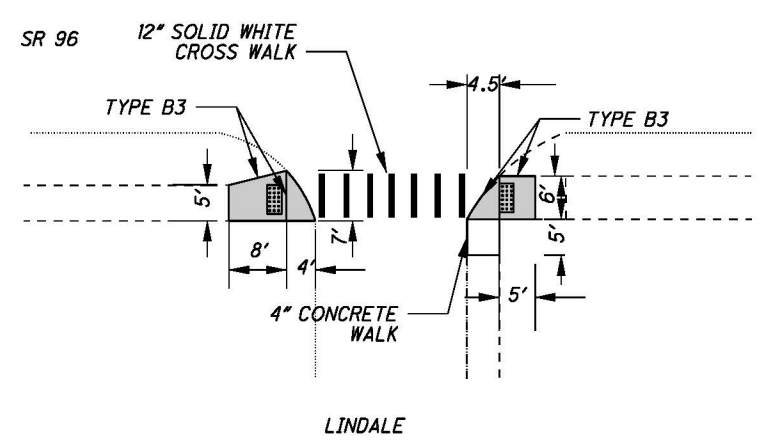
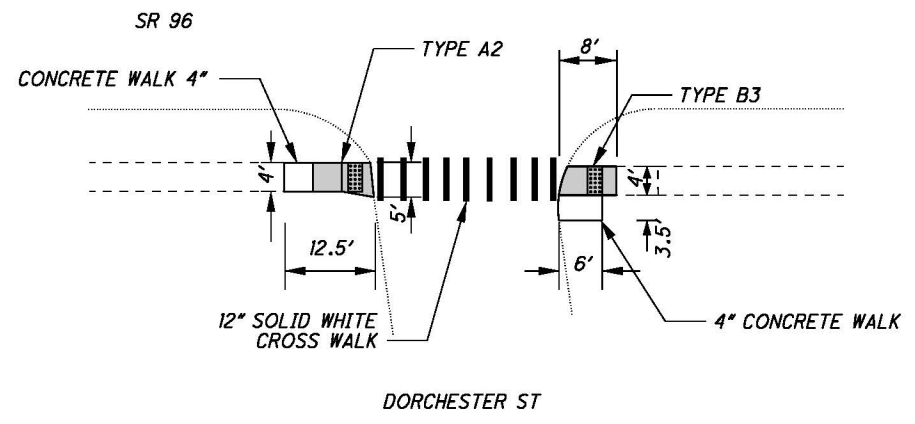


NOTES:

FOR ADDITIONAL DETAILS, SEE SCD BP-7.1, NEW CURB RAMPS.

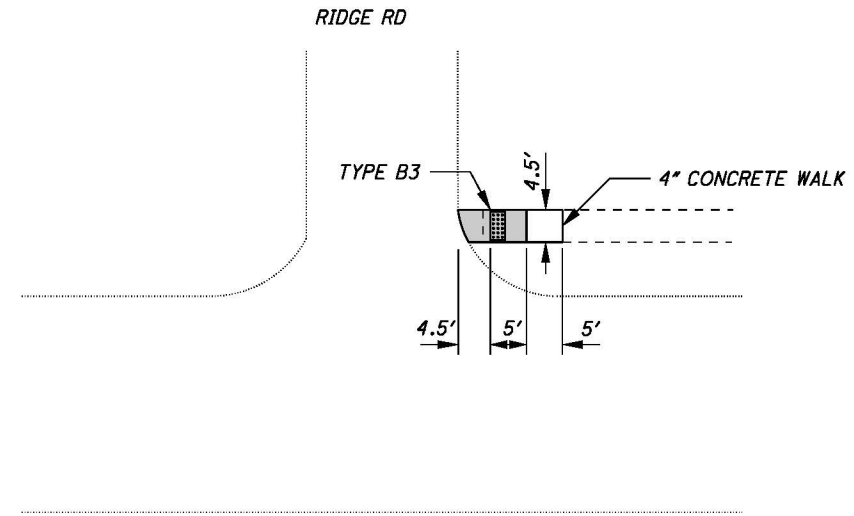
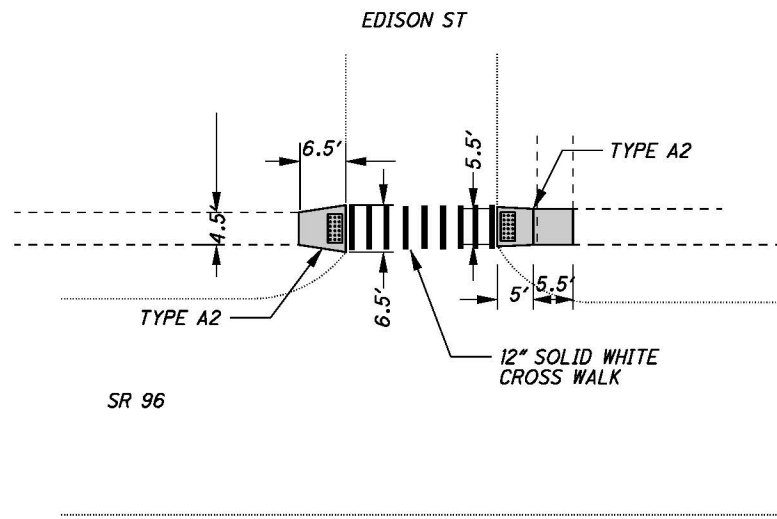
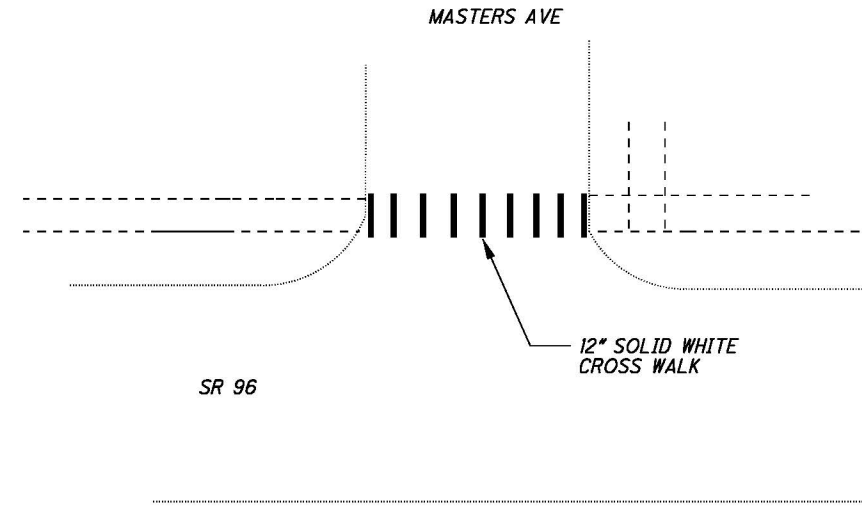
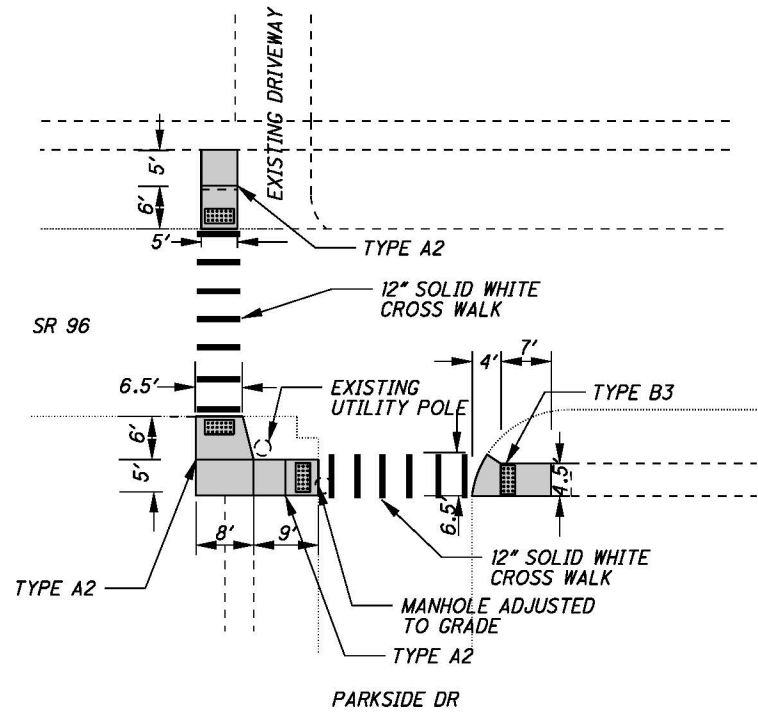
AREAS CALCULATED ARE FOR ESTIMATING PURPOSES ONLY. ACTUAL AREAS SHALL BE VERIFIED BY THE PROJECT ENGINEER.

ALL QUANTITIES CARRIED TO THE GENERAL SUMMARY SHEET.



*PROPOSED CURB RAMP DOES NOT FLAIR OUT LIKE EXISTING SIDEWALK. SMALL SEEDING QUANTITY IS INCIDENTAL FOR THIS LOCATION.

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 WORKSTATION: foster
 MODELNAME: Design
 DATE: 12/10/2012



CONNECTING GUARDRAIL TO EXISTING RAIL

IN LOCATIONS WHERE TYPE 5 GUARDRAIL, TERMINAL ASSEMBLIES, ETC. ARE TO BE CONNECTED TO EXISTING RAIL SOME MODIFICATIONS MAY BE REQUIRED, INCLUDING EXTRA POSTS, DRILLING HOLES AND POSSIBLY PARTIAL SECTIONS OF ADDITIONAL RAIL ELEMENTS. THE COST OF THIS ADDITIONAL WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR TYPE 5 GUARDRAIL. IF ADDITIONAL PORTIONS OF RAIL ELEMENT ARE USED THE LINEAL MEASUREMENT OF THIS ADDITIONAL PORTION SHALL BE ADDED FOR PAYMENT.

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A "W-BEAM RAIL SPLICE" AS SHOWN ON STANDARD CONSTRUCTION DRAWING GR-1.1. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

LOCATIONS OF GUARDRAIL

THE GUARDRAIL PROTECTION PROVIDED IN THIS PLAN SHALL BE LOCATED IN THE FIELD TO ASSURE THAT THE INSTALLATION WILL AFFORD THE MAXIMUM PROTECTION FOR TRAFFIC. THIS LOCATION SHALL BE POSITIONED AS FAR AS POSSIBLE FROM THE EDGE OF PAVEMENT WHILE MAINTAINING PROPER GRADE IN FRONT OF GUARDRAIL AS PER STANDARD DRAWINGS AND PLAN DETAILS.

SUGGESTED SEQUENCE OF GUARDRAIL WORK

1. GUARDRAIL WORK IS TO BEGIN AFTER THE LINEAR GRADING IS COMPLETED AND THE 617 MATERIAL IS PLACED.
2. REMOVE THE GUARDRAIL.
3. PERFORM THE RESHAPING UNDER GUARDRAIL INCLUDING COMPLETING THE EMBANKMENT, AS PER PLAN.
4. REBUILD/CONSTRUCT THE GUARDRAIL RUN.
5. INSTALL BARRIER REFLECTORS.

ITEM 202 - ANCHOR ASSEMBLY REMOVED, TYPE A

THIS ITEM SHALL INCLUDE THE REMOVAL OF THE EXISTING TYPE A, ANCHOR ASSEMBLY INCLUDING ALL POSTS, HARDWARE, RAIL ELEMENTS, AND CONCRETE ANCHORS. ALL ITEMS REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE PROPERLY DISPOSED OF.

THE EXISTING CONCRETE ANCHOR AND CONCRETE AT POSTS SHALL BE REMOVED ENTIRELY. ALL HOLES REMAINING AFTER REMOVAL SHALL BE FILLED WITH GRANULAR MATERIAL OR EXCESS MATERIAL RESULTING FROM GUARDRAIL CONSTRUCTION. ALL FILL MATERIAL SHALL BE THOROUGHLY COMPACTED AND LEVELED, AS DIRECTED BY THE ENGINEER.

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 202, ANCHOR ASSEMBLY REMOVED, TYPE A.

ITEM 209 - RESHAPING UNDER GUARDRAIL

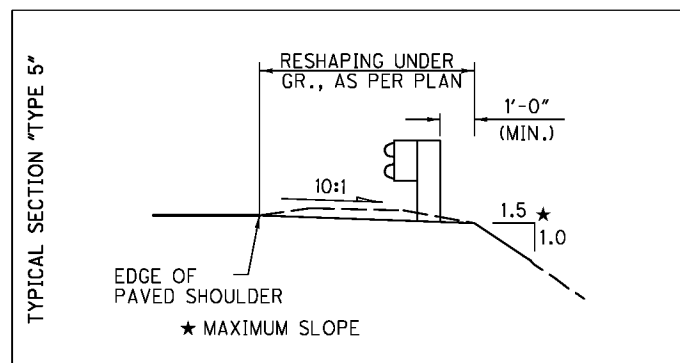
THIS ITEM SHALL BE USED AT LOCATIONS INDICATED IN THE PLANS.

THIS WORK SHALL BE COMPLETED AT LOCATIONS SPECIFIED FOR WORK AS WELL AS PER CMS 209.05 AND AS DESCRIBED HEREIN, AND SHALL AT ALL TIMES BE AS DIRECTED BY THE ENGINEER.

THE AREA IN FRONT OF, UNDER, AND BEHIND THE GUARDRAIL SHALL BE GRADED AND RESHAPED TO PROVIDE AN AREA THAT HAS A SLOPE OF 10:1 MAXIMUM (SEE DETAIL BELOW AS WELL AS THE GUARDRAIL DETAIL SHEETS FOR FURTHER DETAILS AND INFORMATION OF THE LIMITS OF THIS WORK).

EXCESS MATERIAL RESULTING SHALL BE USED ELSEWHERE FOR THIS ITEM IF SO DIRECTED OR DISPOSED OF PROPERLY. IF EXTRA MATERIAL IS REQUIRED IT SHALL BE PAID FOR WITH ITEM 203 - EMBANKMENT, AS PER PLAN. THIS WORK SHALL NOT BE STARTED UNTIL AFTER THE RESURFACING AND BERM WORK HAS BEEN COMPLETED.

THE ABOVE WORK SHALL BE PAID FOR PER STATION WITH ITEM 209, RESHAPING UNDER GUARDRAIL, AS PER PLAN WITH THE EXCEPTION OF ANY EXTRA MATERIAL REQUIRED TO MEET THE SLOPE REQUIREMENTS WHICH SHALL BE PAID BY ITEM 203 - EMBANKMENT, AS PER PLAN.



ITEM 606 - GUARDRAIL, TYPE 5

THIS ITEM SHALL BE USED WHEN GUARDRAIL REQUIRES REPAIRS IN WHICH THE RAIL ELEMENT IS NOT REUSABLE. ALSO, THIS ITEM WILL BE USED TO RE-ALIGN GUARDRAIL RUNS, AS DIRECTED BY THE ENGINEER.

PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS AND EQUIPMENT, AS DESCRIBED IN 606.05 AND TO INCLUDE REMOVAL AND REPLACEMENT OF ANY AND ALL DAMAGED MATERIAL, (INCLUDING THE RAIL ELEMENT), INCLUDING REPLACEMENT OF ANY MATERIALS DAMAGED DURING DISMANTLING OR ANY MATERIALS WHICH MAY HAVE DETERIORATED TO THE POINT THEY CANNOT BE REUSED.

ITEM 606 - ANCHOR ASSEMBLY, TYPE E

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE GUARDRAIL END TERMINALS AS LISTED ON ROADWAY ENGINEERING'S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE FACE OF THE TYPE E IMPACT HEAD SHALL BE COVERED WITH A SHEET OF TYPE G REFLECTIVE SHEETING, PER CMS 730.19.

THE CONTRACTOR MAY USE A SALVAGED EXTRUDER WHEN ASSEMBLING THE ITEM 606 ANCHOR ASSEMBLY, TYPE E. ALL WELDS ON THE EXTERIOR OF THE SALVAGED EXTRUDER SHALL NOT BE DAMAGED AND THE FEEDER SHUTE SHALL NOT BE BENT.

REFER TO THE MANUFACTURER'S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND, THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 27 3/4 INCHES FROM THE EDGE OF THE SHOULDER.

ON SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, TYPE E, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING ALL RELATED TRANSITIONS, REFLECTIVE SHEETING, HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

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WORKSTATION: fofster DATE: 12/10/2012

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GUARDRAIL NOTES

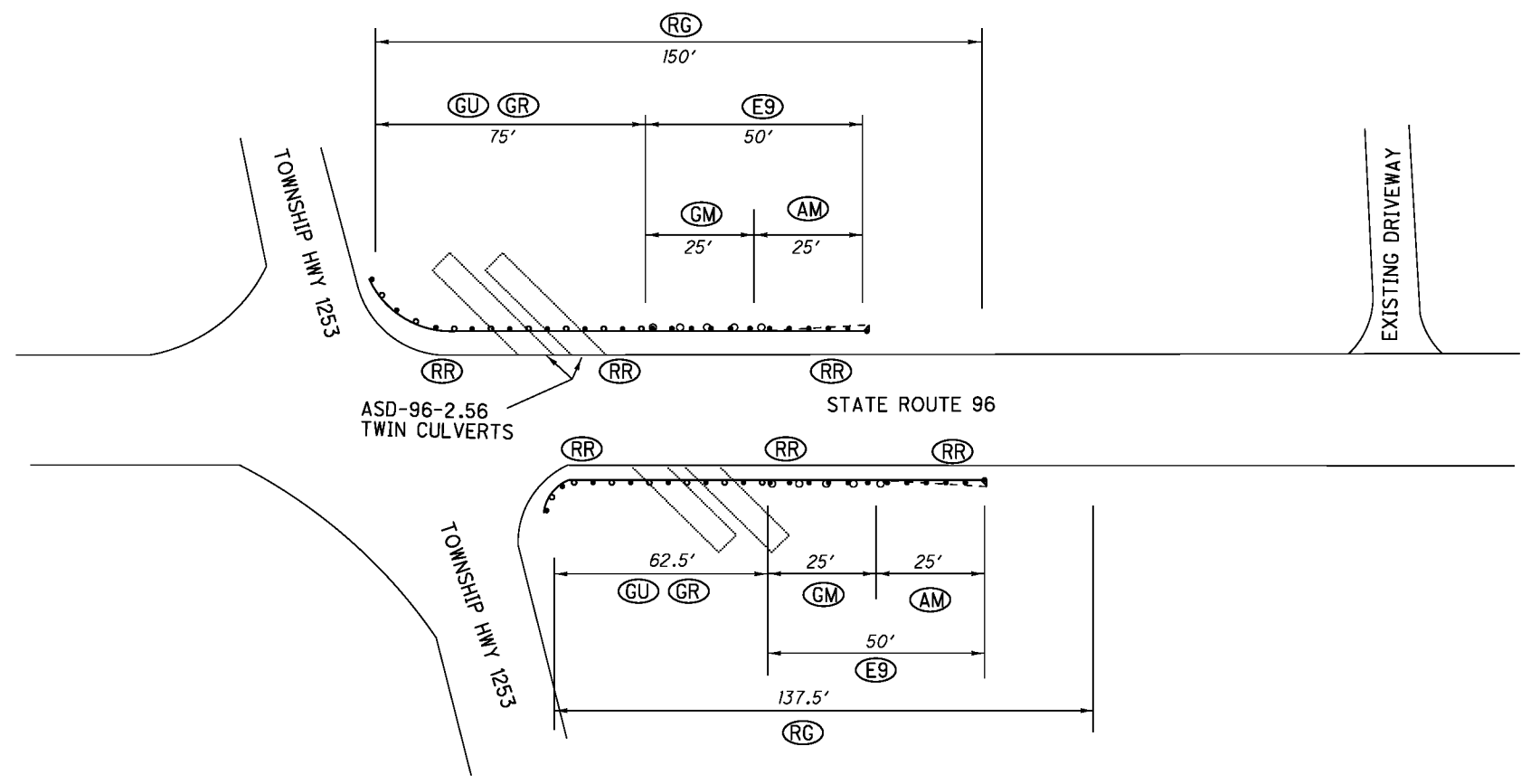
ASD - 96 - 0.00

13
17

FUNDING PARTICIPATION	SHEET	LOCATION	202			209			606			626		
			GUARDRAIL REMOVED FT	GUARDRAIL REMOVED FOR REUSE FT	ANCHOR ASSEMBLY REMOVED , TYPE A EACH	RESHAPING UNDER GUARDRAIL STATION	GUARDRAIL , TYPE 5 FT	GUARDRAIL REBUILT, TYPE 5 FT	ANCHOR ASSEMBLY, TYPE E EACH	BARRIER REFLECTORS EACH				
01/STR/PV	13	STRUCTURE ASD-96-2.56	50	137.5	2	2.88		137.5	2		6			
01/STR/PV	14	ASD-96-3.73	62.5		2	1.50	12.5		2		6			
TOTALS CARRIED TO GENERAL SUMMARY			112.5	137.5	4	4.38	12.5	137.5	4		12			

CALCULATED	NRF
	CHECKED
ADB	
ROADWAY SUB-SUMMARY	
ASD - 96 - 0.00	
14	17

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 WORKSTATION: nfoster DATE: 12/10/2012



LOCATION	ITEM	DESCRIPTION	UNIT	QUANTITY		TOTAL
				LEFT	RIGHT	
(GM)	202	GUARDRAIL REMOVED	FT	25	25	50
(AM)	202	ANCHOR ASSEMBLY REMOVED, TYPE A, AS PER PLAN	EACH	1	1	2
(GU)	202	GUARDRAIL REMOVED FOR REUSE	FT	75	62.5	137.5
(RG)	209	RESHAPING UNDER GUARDRAIL, AS PER PLAN	STATION	1.50	1.38	2.88
(GR)	606	GUARDRAIL REBUILT, TYPE 5	FT	75	62.5	137.5
(E9)	606	ANCHOR ASSEMBLY, TYPE E	EACH	1	1	2
(RR)	626	BARRIER REFLECTORS	EACH	3	3	6

ALL QUANTITIES
 CARRIED TO ROADWAY
 SUB-SUMMARY SHEET.



NOT TO SCALE

CALCULATED
 ERS
 CHECKED
 ADB

GUARDRAIL DETAIL (CULVERT ASD-96-2.56)

ASD-96-0.00

AUXILIARY & LONG LINE MARKINGS

ROUTE	COUNTY	STATION / SLM		HIGHWAY MILES	614				642, TYPE 1					644											SPECIAL					
					WORK ZONE LANE LINE, CLASS I, 642 PAINT	WORK ZONE CENTER LINE, CLASS II, 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	EDGE LINE		CENTER LINE			AUXILIARY MARKINGS (740.04)																
									TOTAL (PAY QUANTITY) (WHITE)	TOTAL (PAY QUANTITY) (YELLOW)	LANE LINE	SOLID LINE EQUIVALENT	TOTAL (PAY QUANTITY)	8" CHANNELIZING LINE	STOP LINE	CROSSWALK LINE, AS PER PLAN	TRANSVERSE / DIAGONAL LINE (WHITE)	TRANSVERSE / DIAGONAL LINE (YELLOW)	ISLAND MARKING	RAILROAD SYMBOL MARKING	SCHOOL SYMBOL MARKING		PARKING LOT STALL MARKING	LANE ARROW				WORD ON PAVEMENT "ONLY"		DOTTED LINE, 4"
FROM	TO	MILE	MILE	FT	FT	MILE	MILE	MILE	MILE	MILE	FT	FT	FT	FT	FT	SQ FT	EACH	72 INCH	96 INCH	FT	LEFT	RIGHT	THROUGH	COMBINATION	72 INCH	96 INCH	FT	EACH	EACH	
01/STR/PV																														
SR 96	ASD	0.00	3.91	3.91		11.73			7.82			6.15	3.91		20						2									
SR 96	ASD	3.91	4.25	0.34		0.51			0.34			0.34	0.17																	
ADDITIONAL QUANTITY FOR SIDE ROADS																														
TOTAL						12.24			8.16			6.49	4.09		40						2									
02/S<2/PV																														
SR 96	ASD	3.91	4.25	0.34		0.51			0.34			0.34	0.17																	
SR 96	ASD	4.25	4.43	0.18		0.54			0.36			0.36	0.18																	
SR 96	ASD	4.43	5.46	1.03		3.09						2.06	1.03																	
ADDITIONAL QUANTITY FOR SIDE ROADS																														
TOTAL						4.14			0.70			2.76	1.38																	
03/S<2/PV																														
SR 96	ASD	4.71	5.46	0.75										36	108															
ADDITIONAL QUANTITY FOR SIDE ROADS																														
TOTALS TO GENERAL SUMMARY				5.46		16.38			8.86			9.25	5.47		114	834					2									

RAISED PAVEMENT MARKERS

ROUTE	COUNTY	STATION/SLM		DETAIL	621					621					REMARKS	DETAIL	DESCRIPTION
					RAISED PAVEMENT MARKER REMOVED	RPM	PRISMATIC RETRO-REFLECTOR TYPES										
							ONE-WAY	TWO-WAY									
FROM	TO	EACH	EACH	WHITE	YELLOW / YELLOW	WHITE / RED	YELLOW / RED	BLUE / BLUE									
SR 96	ASD	0.00	4.27	GAP	278	278		278					CONTINUOUS ROUTE TREATMENT		1	MULTILANE UNDIVIDED TYPICAL SPACING	
															2	TAPERED ACCEL. LANE	
															3	DECELERATION LANE	
															4	PARALLEL ACCEL LANE	
															5	MULTILANE DIVIDED/EXPRESSWAY	
															6	STOP APPROACH	
															7	2 LANE APPR. WITH TURN LANE	
															8	THROUGH APPROACH	
															9	3 LANE APPR. WITH TURN LANE	
															10	3 LANE DIVIDED TO 2 LANE TRANSITION	
															11	3 LANE UNDIVIDED TO 2 LANE TRANSITION	
															12	TWO LANE NARROW BRIDGE	
															13	TWO WAY LEFT TURN LANE	
															14	ONE LANE BRIDGE	
															15	HORIZONTAL CURVE	
															16	HORIZONTAL CURVE ALT.	
															17	STOP APPROACH ALT.	
															18	FIRE HYDRANT	
															GAP	CENTER LINE AT 80 FT. TYP.	
NOTES:																	
1) THRU LANES SHALL BE 11'-0" WIDE IN NON-CURBED SECTIONS AND STRIPED ACCORDING TO TC-73.10. THRU LANES SHALL BE 12'-0" WIDE IN CURBED SECTIONS.																	
2) STOP LINES SHALL BE PLACED AT A 4' OFFSET FROM THE EDGE LINE. ALL STOP LINES SHALL BE PLACED AT A MAXIMUM LENGTH OF 15'.																	
3) FOR ALL WORK ZONE MARKINGS, THE 642 PAINT USED SHALL BE TYPE 1.																	
4) STOP LINES & CENTER LINES SHALL BE STRIPED ALONG THE PROPOSED OVERLAY PORTIONS OF THE FOLLOWING SIDE ROADS:																	
01/STR/PV:																	
CR 1353 (LT), & TR HWY 1153 (RT)																	
TOTALS TO GENERAL SUMMARY					278	278											

DESIGN FILE: \\projects\86718\roadway\sheets\86718TS001.dgn
 WORKSTATION: foster
 MODEL NAME: Design
 DATE: 12/10/2012

PAVEMENT MARKING / RPM SUB-SUMMARY

ASD-96-0.00