

DELINEATION OF PORTABLE AND PERMANENT BARRIER
BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND PERMANENT CONCRETE BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE UNDER EITHER OF THE FOLLOWING CONDITIONS: ALONG TAPERS AND TRANSITION AREAS; OR ALONG CURVES (OUTSIDE ONLY) WITH DEGREE OF CURVATURE GREATER THAN OR EQUAL TO 3 DEGREES.

THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.

DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT-101.70.

TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT-101.70.

FOR QUANTITIES PERTAINING TO THIS NOTE, SEE MOT SUBSUMMARY ON SHEET P.19.

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE FOLLOWING ITEMS: ITEM 614, BARRIER REFLECTOR, TYPE 1, UNIDIRECTIONAL; ITEM 614, OBJECT MARKER, ONE WAY; AND ITEM 614, INCREASED BARRIER DELINEATION.

ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A PORTABLE CHANGEABLE MESSAGE SIGN (PCMS). THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONTINUED)

DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 8 SIGN MONTH ASSUMING 2 PCMS SIGN(S) FOR 4 MONTH(S)

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID EACH FOR ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARD (BIDIRECTIONAL), AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS (WHERE SHOWN ON MOT PLAN, SHEETS P.12 - P.13), HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

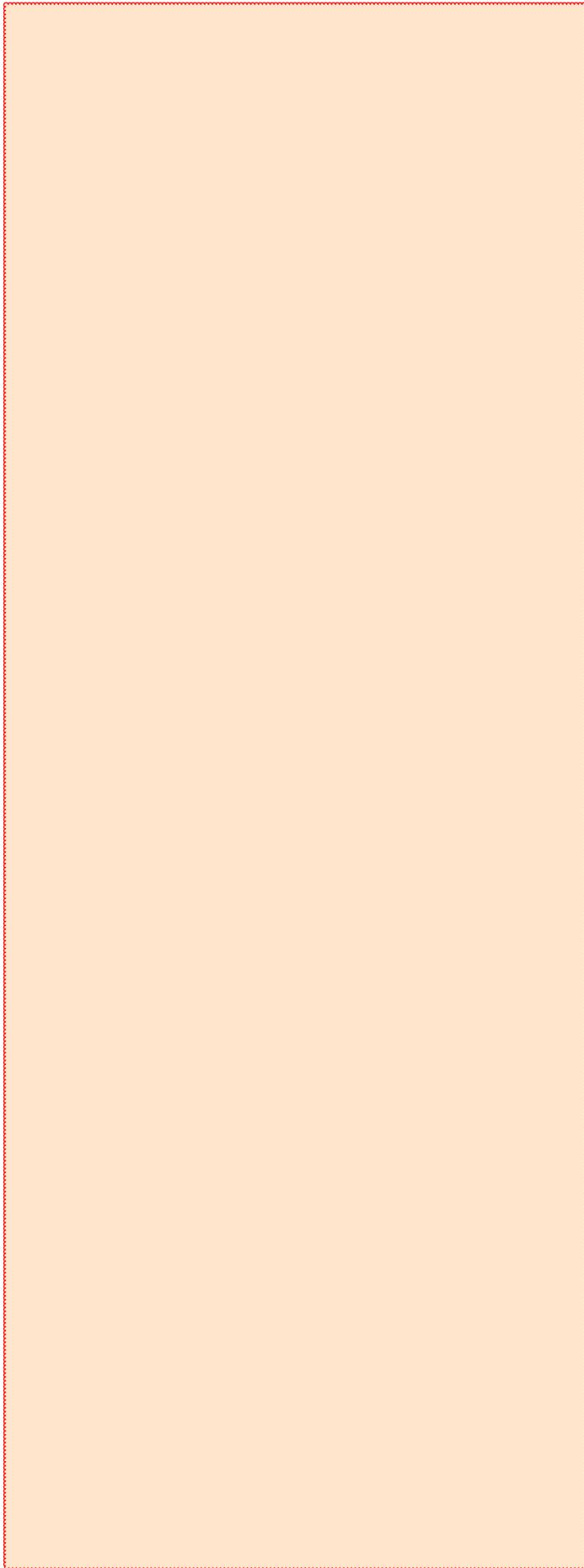
ODOT ITS EQUIPMENT

THE CONTRACTOR SHALL PROTECT THE EXISTING ODOT ITS EQUIPMENT FROM DAMAGE DURING CONSTRUCTION. IF DAMAGED DUE TO CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL REPAIR/REPLACE THE EQUIPMENT AT HIS/HER COST, AS PER THE ENGINEER.

THE REPAIRED/REPLACED ITS EQUIPMENT SHALL PERFORM AT THE SAME LEVEL OR BETTER AS THE PREVIOUS EQUIPMENT.

THE CONTRACTOR SHALL MAINTAIN ALL PREEXISTING OR NEWLY INSTALLED PERMANENT ITS/TRAFFIC DEVICES AND INFRASTRUCTURE DURING CONSTRUCTION ACCORDING TO ODOT SUPPLEMENTAL SPECIFICATION 809.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, AND ANY PERTINENT ITEMS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 809, MAINTAINING ITS DURING CONSTRUCTION.



SIDE ROAD STRUCTURES OVER FREEWAY

THE THREE LANE, TWO WAY TRAFFIC ON THE FREEWAY SHALL BE MAINTAINED AT ALL TIMES DURING THE REHABILITATION OF THE EXISTING STRUCTURE OVER THE FREEWAY, EXCEPT AS FOLLOWS, BUT NOT LIMITED TO:

1. DEMOLITION OF THE EXISTING BRIDGE PARAPETS AND DECK.
2. CONSTRUCTION OF THE PROPOSED PARAPET AND BRIDGE DECK OVER THE FREEWAY WHERE THE ENGINEER BELIEVES THE TEMPORAY CLOSURE OF A TRAFFIC LANE AND/OR SHOULDER IS WARRANTED.
3. DEMOLITION OF THE EXISTING VPF AND INSTALLATION OF THE PROPOSED VPF.

THE CONTRACTOR SHALL CLOSE THE LANE DIRECTLY BELOW THE PROPOSED WORK LOCATION, THE WORK LIMITS SHALL BE APPROVED BY THE ENGINEER, THE LANE CLOSURE SHALL BE AS REQUIRED BY THE PLCM. THIS SHALL BE DONE TO PROTECT THE UNDERPASS ROADWAY TRAFFIC DURING REMOVAL OF EXISTING AND THE CONSTRUCTION OF NEW CONCRETE PARAPETS, BRIDGE DECK, AND VANDAL PROTECTION FENCE (VPF).

THE NET AND/OR PLATFORMS ARE CONSIDERED TO BE THE "MEANS AND METHOD" OF CONTRACTOR. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS AND APPROVALS PRIOR TO THEIR INSTALLATION. THESE ITEMS SHALL CONFORM WITH OSHA REQUIREMENTS AND SHALL HAVE APPROVAL FROM THE ODOT OFFICE OF STRUCTURAL ENGINEERING.

THE PROTECTION ITEMS SHALL REMAIN IN PLACE UNTIL WORK HAS BEEN COMPLETED AND APPROVED BY THE ENGINEER.

THE EXISTING VERTICAL CLEARANCE OVER THE FREEWAY SHALL BE MAINTAINED AT ALL TIMES.

COSTS FOR THE ABOVE WORK SHALL BE CONSIDERED INCIDENTAL AND INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN
 THIS ITEM SHALL CONSIST OF PLACING COMPACTED AGGREGATE TO A THICKNESS OF 6" AT THE LIMITS SHOWN ON SHEETS P.12 - P.13. ALL REQUIREMENTS OF ITEM 617 SHALL APPLY IN ADDITION TO THE FOLLOWING;

THE LOCATIONS WHERE COMPACTED AGGREGATE WILL BE PLACED SHALL BE VOID OF ALL VEGETATION AND DEBRIS. IF EMBANKMENT IS REQUIRED TO ASSURE THE AGGREGATE THICKNESS DESIRED, IT SHALL BE PLACED AND COMPACTED AS PER THE ENGINEER'S APPROVAL.

PLACE EMBANKMENT ALONG THE EDGE OF THE COMPACTED AGGREGATE TO FORM A SMOOTH TRANSITION, THEN SEED, MULCH AND WATER AS PER THE ENGINEER.

ITEMS TO BE DISPOSED OF SHALL BE DONE SO AS PER SECTIONS 105.16 AND 105.17 OF THE CURRENT YEAR, CM&S.

PAYMENT SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 617, COMPACTED AGGREGATE, AS PER PLAN AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT, AND MATERIALS TO COMPLETE THE ABOVE WORK.

SEQUENCE OF CONSTRUCTION

I.R.-275

THE POSTED LEGAL SPEED LIMIT IS 65 MPH. THE MAINTENANCE OF TRAFFIC (MOT) DESIGN SPEED SHALL ALSO BE 65 MPH. THE ODOT ITS EQUIPMENT SHALL BE PROTECTED AT ALL TIMES, SEE NOTE ON SHEET P.08.

FOR WORK BEING DONE ON THE BRANCH HILL MIAMIVILLE ROAD BRIDGE OVER I.R.-275, MOT TRAFFIC SHALL BE MAINTAINED WITHIN THE EXISTING 12' LANES AND BOTH SHOULDERS. THE SHOULDERS SHALL BE REDUCED TO A 2' WIDTH BY USE OF PORTABLE BARRIERS (PB) AND DRUMS AS PER SCD MT-95.45. SEE SHEETS P.12 - P.13 FOR FURTHER INFORMATION.

FOR CONSTRUCTION TRAFFIC ACCESS TO THE MEDIAN, SEE MOT PLAN SHEETS P.12 - P.13 AND SCD MT-103.15.

PLACE ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN, TO THE LIMITS SHOWN ON MOT PLAN SHEETS P.12 - P.13. FOR FURTHER COMPACTED AGGREGATE INFORMATION, SEE NOTE ON THIS SHEET.

WITH TRAFFIC IN THIS PATTERN THE CONTRACTOR SHALL REMOVE THE EXISTING MEDIAN GUARDRAIL AND RELOCATE THE EXISTING MEDIAN CABLE RAIL, SEE MOT PLAN SHEETS P.12 - P.13. REMOVE AND/OR DEMOLISH PORTIONS OF THE BRANCH HILL MIAMIVILLE ROAD BRIDGE AS PER THE PLANS. CONSTRUCT THE PROPOSED PIER CAP, ABUTMENT AND WINGWALL MODIFICATIONS, BRIDGE DECK, PARAPETS, SIDEWALK AND VANDAL PROTECTION FENCE ITEMS. PERFORM PROPOSED PATCHING AS SHOWN IN THE PLANS. INSTALL THE PROPOSED GUARDRAIL, ATTENUATOR AND ASSOCIATED ITEMS AS PER THE PLANS. REINSTALL THE EXISTING MEDIAN CABLE RAIL TO IT'S ORIGINAL LOCATIONS AS PER THE PLANS, SEE NOTE THIS SHEET. COMPLETE ALL REMAINING PLAN ITEMS, AS APPROVED BY THE ENGINEER.

ONCE ALL WORK HAS BEEN COMPLETED IN THE MEDIAN AND APPROVED BY THE ENGINEER, THE CONTRACTOR SHALL REMOVE THE WORK ZONE IMPACT ATTENUATORS (WZIA), PORTABLE BARRIER (PB), LEVELING PADS (LP), COMPACTED AGGREGATE AND ALL RELATED MOT ITEMS. REGRADE, PLACE TOP SOIL, SEED AND MULCH, WATER AS PER THE ENGINEER.

ITEMS TO BE DISPOSED OF SHALL BE DONE SO AS PER SECTIONS 105.16 AND 105.17 OF THE CURRENT YEAR, CM&S.

WITH THE ABOVE WORK COMPLETED AND APPROVED BY THE ENGINEER, ALL LANES AND SHOULDERS SHALL BE OPEN TO THRU TRAFFIC.

LANE VALUE CONTRACT TABLE

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE "LANE VALUE CONTRACT TABLE" FOR EACH UNIT OF TIME A LANE/SHOULDER IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE LANE VALUE CONTRACT TABLE.

LANE VALUE CONTRACT TABLE					
SECTION (SLM)	EXISTING NUMBER OF LANES PER DIRECTION	LANE CLOSURES ARE NOT PERMITTED:			DISINCENTIVE AMOUNTS PER MINUTE PER LANE
		LANE REDUCTION	MON TO FRI	SAT AND SUN	
CLE-275-0.43					
I-275 (0.000 - 4.290)	3	3 to 2	7AM-9AM & 1PM-7PM	7AM-8AM & 3PM-6PM	\$285
		3 to 1	6AM-10PM	6AM-8PM	\$285
15 MINUTE SHORT DURATION LANE CLOSURES ARE PERMITTED 12AM-4AM MONDAY-FRIDAY					\$285
SHORT TERM SHOULDER CLOSURES ARE NOT PERMITTED 5AM-9AM AND 3PM-7PM MONDAY-FRIDAY					

SEQUENCE OF CONSTRUCTION

BRANCH HILL MIAMIVILLE ROAD

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. IF A PROPERTY HAS MORE THAN ONE ENTRANCE DRIVE, ONE DRIVE MAY BE CLOSED DURING CONSTRUCTION.

WHEN THE CONTRACTOR DEEMS THAT THE CLOSURE OF BRANCH HILL MIAMIVILLE ROAD IS NEEDED TO START THE PROPOSED BRIDGE AND ROADWAY WORK, A DETOUR HAS BEEN PROVIDED ON SHEET P.11.

THE CONTRACTOR SHALL THEN PERFORM ALL OF THE REMOVALS AS PER THE PLANS. CONSTRUCT THE EARTH WORK, PLACE THE PROPOSED DRAINAGE ITEMS AND THE PROPOSED PAVEMENT AS PER THE PLANS. PLACE ALL ROADWAY ITEMS, SIGNING, PAVEMENT MARKINS AND FENCING AS PER THE PLANS. COMPLETE ALL REMAINING INCIDENTAL PLAN ITEMS AT THIS TIME, TO BE APPROVED BY THE ENGINEER.

ITEMS TO BE DISPOSED OF SHALL BE DONE SO AS PER SECTIONS 105.16 AND 105.17 OF THE CURRENT YEAR, CM&S.

ONCE ALL OF THE ABOVE WORK HAS BEEN COMPLETED AND APPROVED BY THE ENGINEER, THE DETOUR FOR BRANCH HILL MIAMIVILLE ROAD SHALL BE REMOVED. ALL MOT ITEMS AND EQUIPMENT SHALL BE REMOVED, THEN BRANCH HILL MIAMIVILLE ROAD SHALL THEN BE OPEN TO THRU TRAFFIC.

DESIGN AGENCY

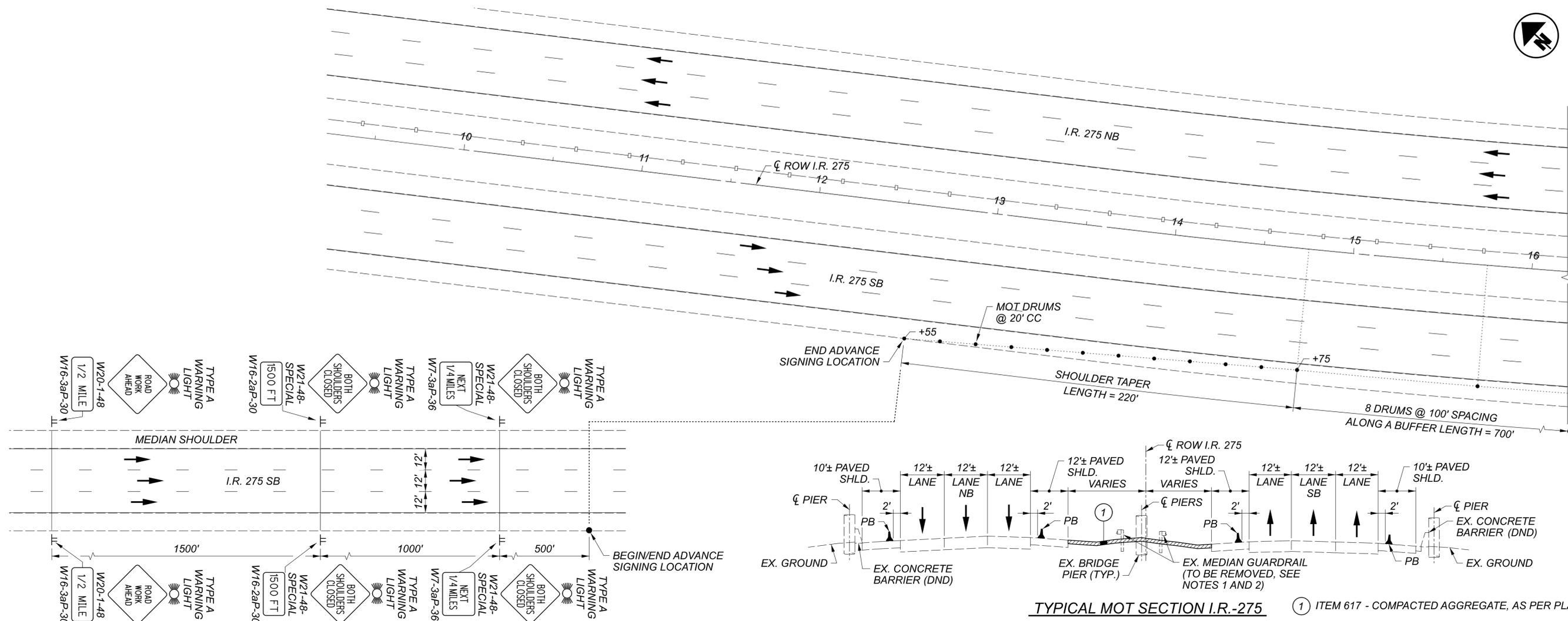


DESIGNER
DLB/AJV

REVIEWER
JPC 02/22/24

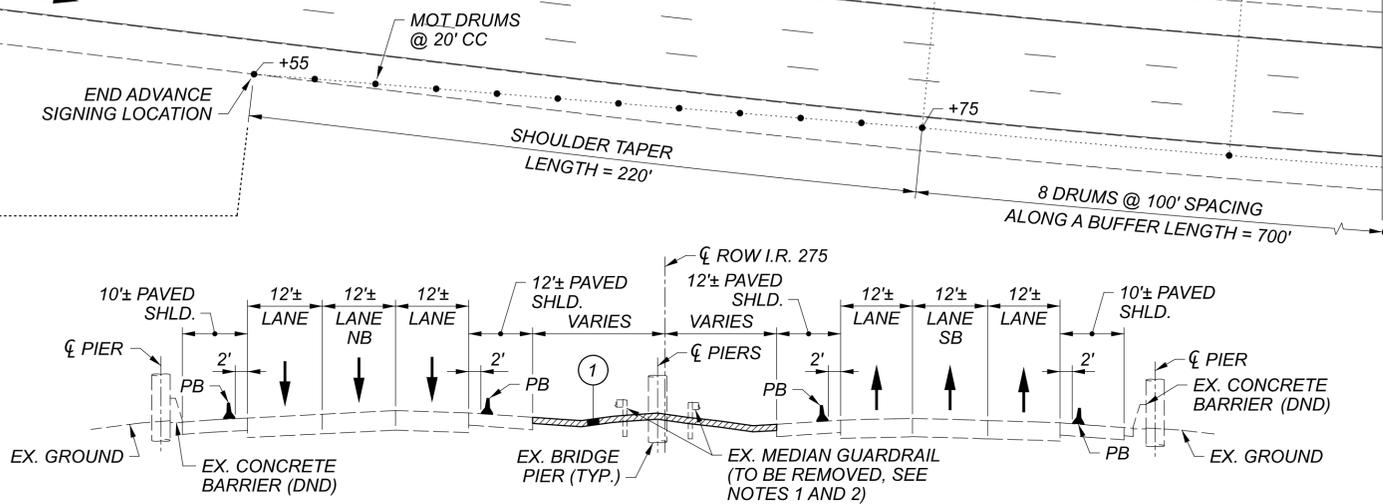
PROJECT ID
102753

SHEET TOTAL
P.10 70



SOUTHBOUND ADVANCE SIGNING DETAIL

NOT TO SCALE
NOTE: PROVIDE ADVANCE WARNING SIGNAGE AS PER MT-103.10 (NOT PICTURED)



TYPICAL MOT SECTION I.R.-275

SECTION SHOWN AT BRIDGE
NOT TO SCALE

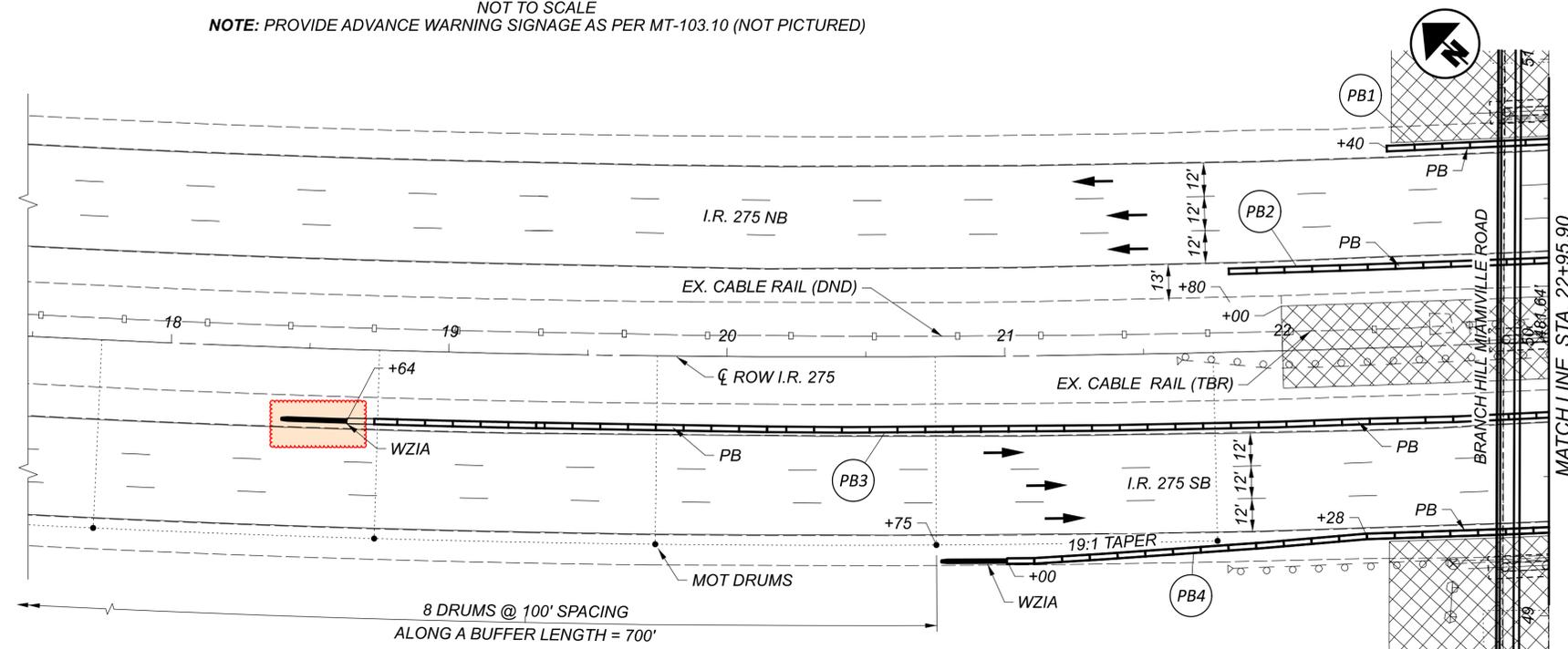
① ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

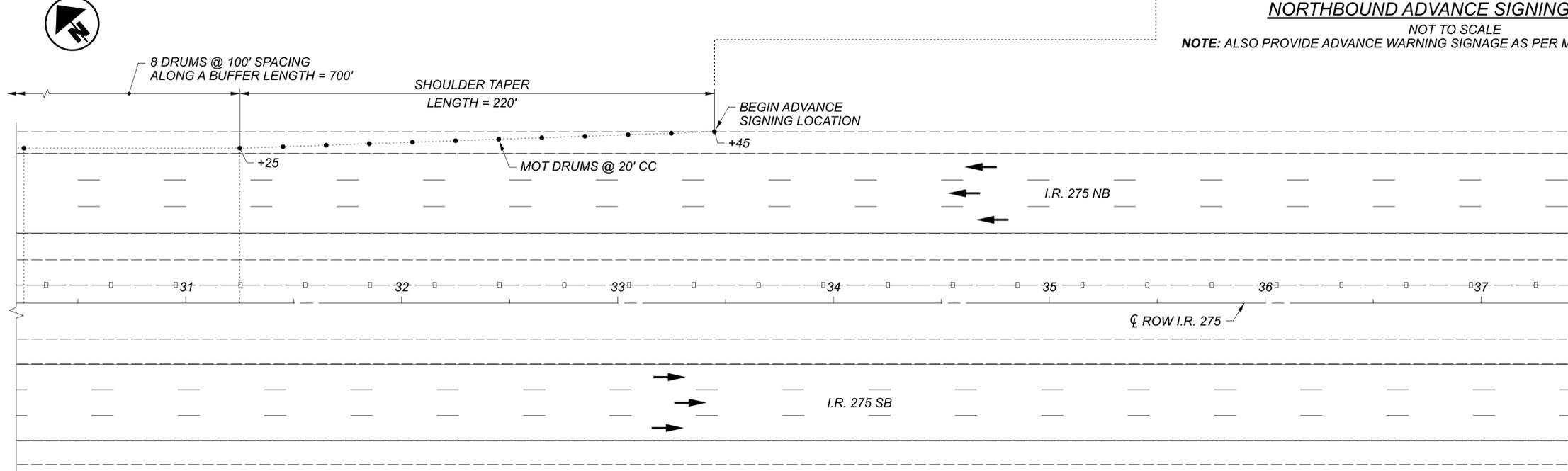
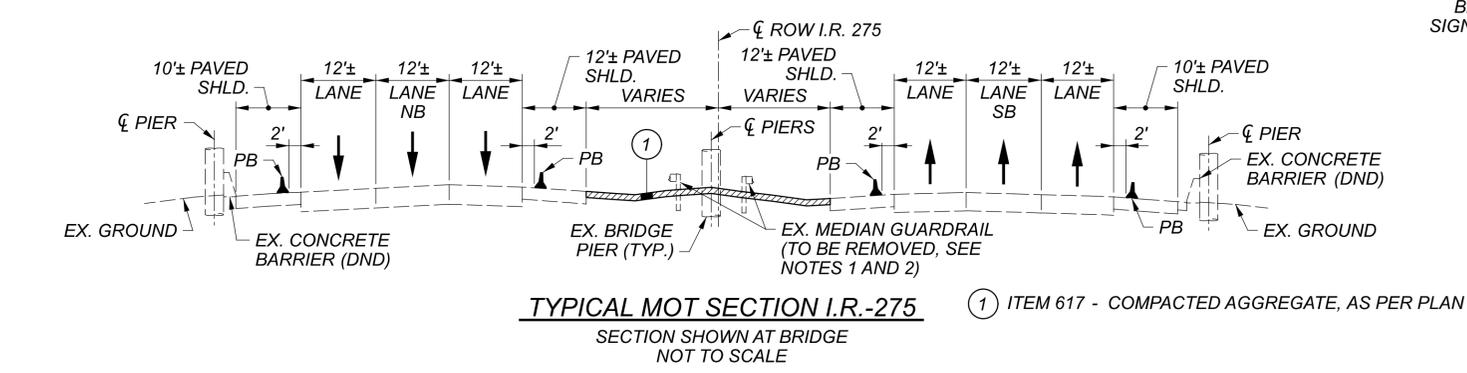
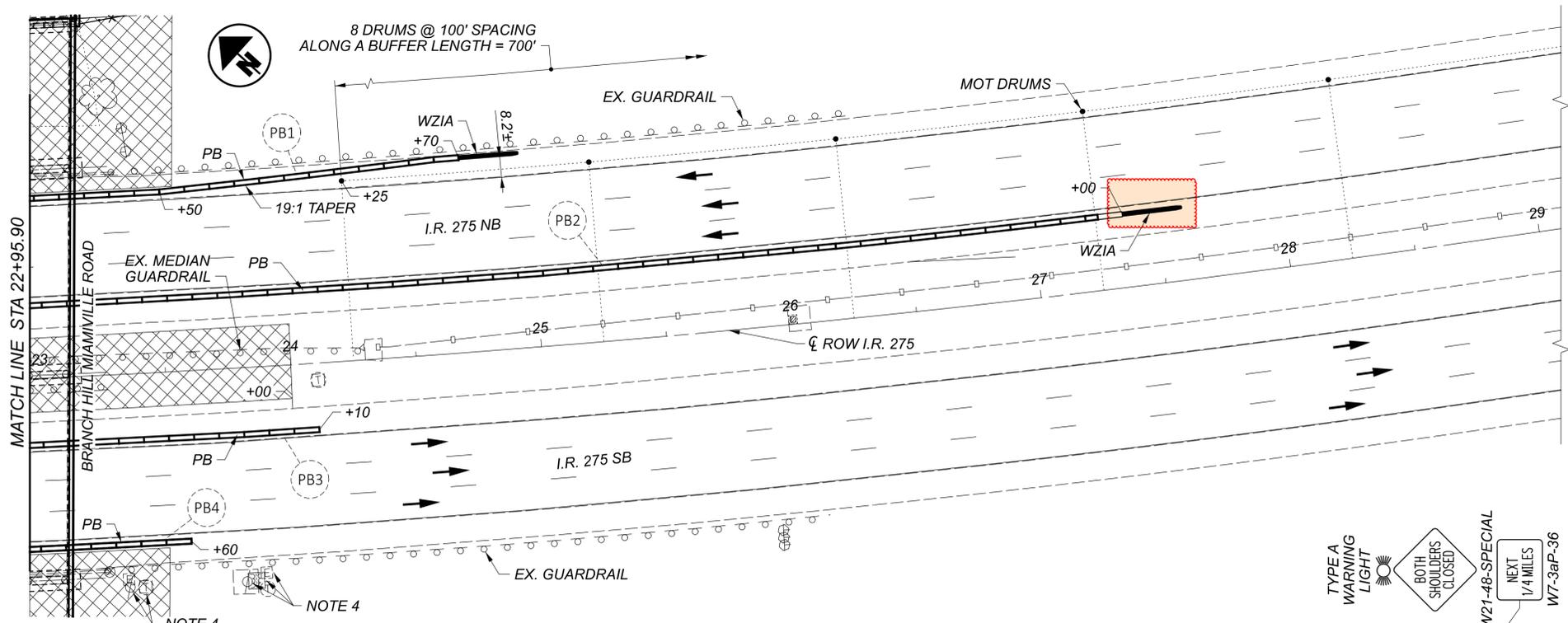
LEGEND

- • • • MOT DRUMS
- DIRECTION OF TRAVEL
- ▬ PORTABLE BARRIER, UNANCHORED (PB)
- ▬ WORK ZONE IMPACT ATTENUATOR, 65 MPH+ (WZIA)
- ▨ WORK AREA
- ▨ CONSTRUCTION ACCESS SURFACE MATERIAL, ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN (SEE TYPICAL SECTION)

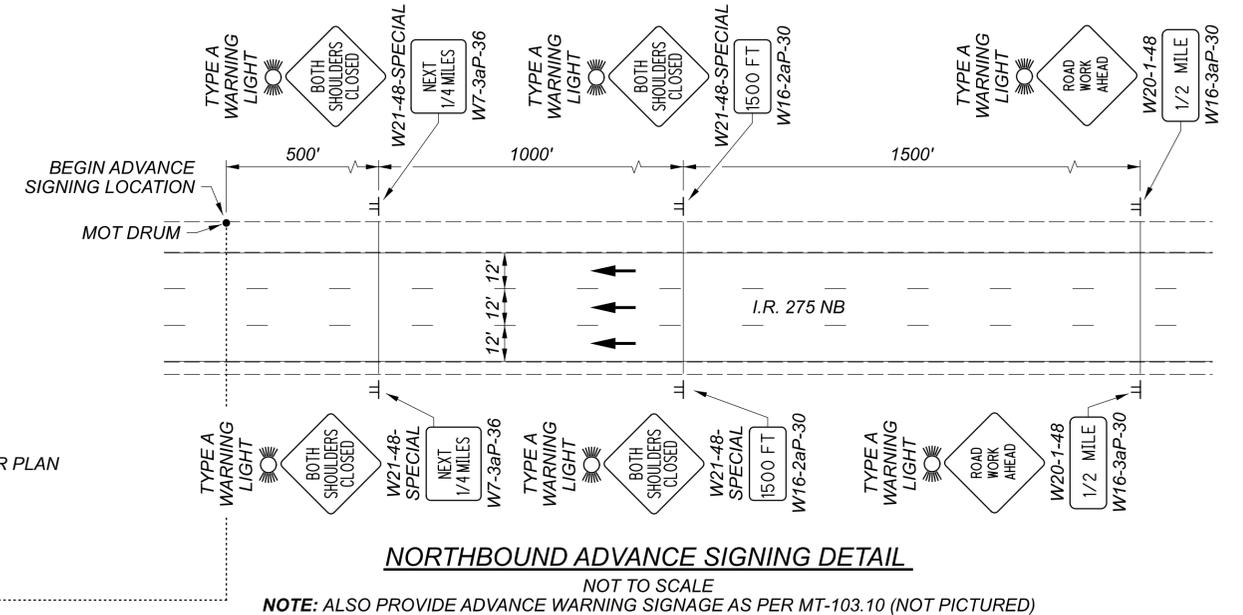
NOTES

1. THE EXISTING MEDIAN GUARDRAIL SHALL BE REMOVED TO ALLOW ACCESS DURING THE BRIDGE CONSTRUCTION. SEE THE ROADWAY PLANS FOR THE PROPOSED GUARDRAIL LIMITS AND ATTENUATORS.
2. THE EXISTING MEDIAN GUARDRAIL AND ATTENUATORS REMOVAL QUANTITIES SHALL BE INCLUDED WITH THE ROADWAY ESTIMATED QUANTITIES.
3. FOR FURTHER MOT INFORMATION NOT SHOWN, SEE SCD'S MT-95.45 AND MT-103.10.
4. ODOT ITS EQUIPMENT SHALL NOT BE DISTURBED. SEE MOT GENERAL NOTES FOR FURTHER INFORMATION.

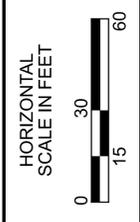




- NOTES**
1. THE EXISTING MEDIAN GUARDRAIL SHALL BE REMOVED TO ALLOW ACCESS DURING THE BRIDGE CONSTRUCTION. SEE THE ROADWAY PLANS FOR THE PROPOSED GUARDRAIL, MEDIAN CONCRETE BARRIER AND IMPACT ATTENUATORS.
 2. THE EXISTING MEDIAN GUARDRAIL AND ATTENUATORS REMOVAL QUANTITIES SHALL BE INCLUDED WITH THE ROADWAY ESTIMATED QUANTITIES.
 3. FOR FURTHER MOT INFORMATION NOT SHOWN, SEE SCD'S MT-95.45 AND MT-103.10.
 4. ODOT ITS EQUIPMENT SHALL NOT BE DISTURBED. SEE MOT GENERAL NOTES FOR FURTHER INFORMATION.



- LEGEND**
- • • • MOT DRUMS
 - ➔ DIRECTION OF TRAVEL
 - ▬ PORTABLE BARRIER, UNANCHORED (PB)
 - ▬ WORK ZONE IMPACT ATTENUATOR, 65 MPH+ (WZIA)
 - ▨ WORK AREA
 - ▨ CONSTRUCTION ACCESS SURFACE MATERIAL, ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN (SEE TYPICAL SECTION)



REF NO.	SHEET NO.	STATION TO STATION			SIDE	203				614				617		622	
						EXCAVATION				WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL)	BARRIER REFLECTOR, TYPE 1, UNIDIRECTIONAL	OBJECT MARKER, ONE WAY	INCREASED BARRIER DELINEATION	COMPACTED AGGREGATE, AS PER PLAN	PORTABLE BARRIER, UNANCHORED		
			TO			CY	EACH	EACH	EACH	FT			CY	FT			
		22+00.00		24+00.00	MED	189							190				
PB1	P.12	22+40.00		24+70.00	LT		1	5	5	205				205			
PB2	P.12	21+80.00		27+36.00	MED/LT		1	12	12	560				560			
PB3	P.12	18+64.00		24+10.00	MED/RT		1	11	11	550				550			
PB4	P.12	21+00.00		23+60.00	RT		1	5	5	235				235			
TOTALS CARRIED TO GENERAL SUMMARY						0	189	4	33	33	1550			190	1550		

MOT SUBSUMMARY