

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

CLE-50-5.46

ALTA FIBER
221 E. 4th ST, BLDG. 121-900
CINCINNATI, OH 45201
513-566-8039 (NANCY MORRIS)
NANCY.MORRIS@ALTA FIBER.COM

ALTA FIBER UNDERGROUND
513-565-7187 (BRECK COWAN)
BRECK.COWAN@CINBELL.COM

DUKE ENERGY ELECTRIC (DISTRIBUTION)
2010 DANA AVE
CINCINNATI, OH 45207
513-508-9609 (SHANE ERHART)
SHANE.ERHART@DUKE-ENERGY.COM

CHARTER COMMUNICATIONS
10920 KENWOOD ROAD
BLUE ASH, OHIO 45242
DL-SOUTHERN-OHIO-OUTSIDE-PLANT@CHARTER.COM
513-386-5499 (KENT RIEGER)
KENT.RIEGER@CHARTER.COM

CLERMONT COUNTY WATER RESOURCES
4400 HASKELL LANE
BATAVIA, OH 45103
513-479-4031 (TIM CHERRY)
TCHERRY@CLERMONTCOUNTYOHIO.GOV

DUKE ENERGY GAS
139 EAST 4th ST., ROOM 460A
CINCINNATI, OH 45202
OH/KYHOUSEBILL@DUKE-ENERGY.COM
513-287-2532 (DENISE GROSS)
DENISE.GROSS@DUKE-ENERGY.COM

CLE-727-5.46

ALTA FIBER
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513-508-9609 (SHANE ERHART)
SHANE.ERHART@DUKE-ENERGY.COM

WESTERN WATER COMPANY
1775 S.R. 28
GOSHEN, OH 45122
513-899-3211 EXT. 22 (KURT MEEKER)
KMEEKER@WESTERN-H2O.COM

CLI-134-11.07

CLINTON COUNTY (OH) ENGINEER'S OFFICE
1326 FIFE AVE.
WILMINGTON, OH 45177
(937) 382-2078 (ADAM J. FRICKE, P.E., P.S. -DEPUTY ENGINEER)
AFRICKE@CLINTONCOUNTYENGINEER.ORG

AES OHIO
1900 DRYDEN RD
DAYTON, OH 45439
937-554-9063 (WILLIAM WARD)
WILLIAM.WARD@AES.COM

FRONTIER COMMUNICATIONS
241 SOUTH NELSON AVENUE
WILMINGTON, OH 45177
937-283-5735 (DAVID LONGWORTH)
DAVID.M.LONGWORTH@FTR.COM

WESTERN WATER COMPANY
1775 S.R. 28
GOSHEN, OH 45122
513-899-3211 EXT. 22 (KURT MEEKER)
KMEEKER@WESTERN-H2O.COM

CITY OF WILMINGTON - PUBLIC SERVICES
69 N. SOUTH STREET
WILMINGTON, OH 45177
937-383-5882 (RICK SCHAFFER)

I-71 AND DOSTER RD

AT&T OHIO
7201 FAR HILLS AVE
DAYTON, OH,45459
937-708-1026 (ALAN STUTES)
AS1634@ATT.COM

AES OHIO
1900 DRYDEN RD
DAYTON, OH 45439
937-554-9063 (WILLIAM WARD)
WILLIAM.WARD@AES.COM

CLINTON COUNTY (OH) ENGINEER'S OFFICE
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FRONTIER COMMUNICATIONS
241 SOUTH NELSON AVENUE
WILMINGTON, OH 45177
937-283-5735 (DAVID LONGWORTH)
DAVID.M.LONGWORTH@FTR.COM

I-71 AND SMITH RD

AT&T OHIO
7201 FAR HILLS AVE
DAYTON, OH,45459
937-708-1026 (ALAN STUTES)
AS1634@ATT.COM

AES OHIO
1900 DRYDEN RD
DAYTON, OH 45439
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WILLIAM.WARD@AES.COM

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CITY OF WILMINGTON - PUBLIC SERVICES
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WILMINGTON, OH 45177
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I-71 AND I-73

AT&T OHIO
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DAYTON, OH,45459
937-708-1026 (ALAN STUTES)
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AES OHIO
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WILLIAM.WARD@AES.COM

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CITY OF WILMINGTON - PUBLIC SERVICES
69 N. SOUTH STREET
WILMINGTON, OH 45177
937-383-5882 (RICK SCHAFFER)

MCI/VERIZON
8800 GOVERNOR HILL DR
CINCINNATI, OH 45249
254-721-8977 (BRUCE TURKIEWICZ)
BRUCE.TURKIEWICZ@VERIZON.COM

WESTERN WATER COMPANY
1775 S.R. 28
GOSHEN, OH 45122
513-899-3211 EXT. 22 (KURT MEEKER)
KMEEKER@WESTERN-H2O.COM

ITS (FORMERLY ARTIMIS)
ODOT CENTRAL OFFICE OF TRAFFIC ENGINEERING
1980 WEST BROAD STREET COLUMBUS, OH 43223
614-387-0695 (PAUL BECK)
PAUL.BECK@DOT.OHIO.GOV

ODOT D8 TRAFFIC
505 SOUTH SR741
LEBANON, OHIO 45036
513-933-6692 (JIM JUDD)
JIM.JUDD@DOT.OHIO.GOV

ASPIRE ENERGY
300 TRACY BRIDGE ROAD
ORRVILLE, OH 44667
330-682-7726

ITEM 201, CLEARING AND GRUBBING, AS PER PLAN

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN.

REMOVE ANY TREES, BRUSH OR STUMPS NOT SPECIFICALLY MARKED FOR REMOVAL IF LOCATED UNDER OR WITHIN TEN FEET OF THE BRIDGE STRUCTURES. REMOVE BRUSH WITHIN THE LIMITS OF STATE RIGHT-OF-WAY. THE REMOVAL OF DEBRIS FROM AROUND THE ABUTMENTS AND/OR PIERS AS DIRECTED BY THE ENGINEER SHALL ALSO BE INCLUDED WITH THIS ITEM FOR PAYMENT. CONTRACTOR SHALL VERIFY POSITIVE DRAINAGE.

ALL PROVISIONS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

SURVEYING PARAMETERS

USE THE FOLLOWING VERTICAL POSITIONING AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

VERTICAL POSITIONING
ORTHOMETRIC HEIGHT DATUM: NAVD 88 (2011)
GEOID: GEOID 18

HORIZONTAL POSITIONING
REFERENCE FRAME: NAD83(2011) EPOCH2010.0
ELLIPSOID: GRS80
MAP PROJECTION: LAMBERT CONFORMAL CONIC
COORDINATE SYSTEM: OHIO SOUTH ZONE (SPC 3402)
COMBINED SCALE FACTOR : 1.0000000000
UNITS ARE IN U.S. SURVEY FEET. USE THE FOLLOWING CONVERSION FACTOR: 1 METER = 3.280833333 U.S. SURVEY FEET.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

TREE CLEARING RESTRICTIONS

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. THE CONTRACTOR SHALL NOT REMOVE TREES UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THE CONTRACTOR SHALL DEMARCATÉ CLEARING LIMITS IN THE FIELD TO AVOID ANY UNAUTHORIZED TREE CLEARING. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

ITEM 623 - CONSTRUCTION LAYOUT STAKES & SURVEYING, AS PER PLAN

PRIOR TO THE START OF CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL REFERENCE THE LENGTH OF THE PROJECT ON BOTH SIDES OF THE ROADWAY, IN A MANNER SATISFACTORY TO THE ENGINEER. THE PAVEMENT SHALL BE REFERENCED IN 25 FOOT INCREMENTS, OR IN INCREMENTS ACCEPTABLE TO THE ENGINEER, IN A SEMI-PERMANENT CONDITION.

ASBESTOS ABATEMENT

AN ASBESTOS SURVEY WAS CONDUCTED FOR THE FOLLOWING STRUCTURES LOCATED IN CLERMONT COUNTY: CLE-50-5.465 (SFN: 1300946) THAT CROSSES SUGARCAMP RUN & CLE-272-2.630 (SFN: 1304747) THAT CROSSES STONELICK CREEK. AN ASBESTOS SURVEY WAS CONDUCTED FOR THE FOLLOWING STRUCTURES LOCATED IN CLINTON COUNTY: CLI-71-0.278 (SFN: 1401416) THAT CARRIES DOSTER ROAD OVER I-71, CLI-71-2.914 (SFN: 1402102) THAT CARRIES SR-73 OVER I-71, CLI-71-4.077 (SFN: 1401505) THAT CARRIES SMITH ROAD OVER I-71, & CLI-143-11.069 (SFN: 1402978) THAT CROSSES COWAN CREEK. THE SURVEYS WERE CONDUCTED BY A LICENSED ASBESTOS HAZARD EVALUATION SPECIALIST. THE ASBESTOS SURVEYS DID NOT IDENTIFY THE PRESENCE OF ASBESTOS CONTAINING MATERIALS AT ANY OF THE BRIDGE LOCATIONS.

ELECTRONIC SUBMISSION:

THE CONTRACTOR SHALL SUBMIT ELECTRONICALLY TO OEPA A COMPLETED NOTIFICATION OF DEMOLITION & RENOVATION FORM (NDRF) AND APPLICABLE FEES ALONG WITH THE ASBESTOS SURVEY REPORT. THE COMPLETED NDRF MUST BE SUBMITTED TO OEPA AT LEAST 10 DAYS PRIOR TO ANY DEMOLITION AND RENOVATION ACTIVITY. THE CONTRACTOR IS RESPONSIBLE FOR RETAINING AN ELECTRONIC COPY OF THE NDRF (IN PDF FORM) FOR SUBMISSION TO THE DISTRICT ENVIRONMENTAL STAFF AND A ONE HARD COPY TO THE PROJECT ENGINEER.

(GO TO THE OEPA EBUSINESS CENTER AND SUBMIT THE NDRF AND PAYMENT ALONG WITH THE ASBESTOS SURVEY REPORT)

HARD COPY SUBMISSION:

THE CONTRACTOR MAY ELECT TO SUBMIT A HARD COPY OF THE COMPLETED NDRF AND PAYMENT ALONG WITH THE ASBESTOS SURVEY REPORT TO THE FOLLOWING:

ASBESTOS PROGRAM OHIO EPA, DAPC P.O. BOX 1049 COLUMBUS, OHIO 43216-1049	OR	ASBESTOS PROGRAM OHIO EPA, DAPC 50 W. TOWN ST., SUITE 700 COLUMBUS, OHIO 43215
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IF THE CONTRACTOR ELECTS TO SUBMIT A HARD COPY TO OEPA THEY ARE RESPONSIBLE FOR RETAINING A HARD COPY OF THE NDRF FOR SUBMISSION TO THE DISTRICT ENVIRONMENTAL STAFF AND A ONE HARD COPY TO THE PROJECT ENGINEER.

BASIS OF PAYMENT

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

690E98400 ITEM SPECIAL - MISC.: WORK INVOLVING ASBESTOS CONTAINING MATERIALS - LUMP SUM

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT WILL BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, COMPLY WITH ALL LOCAL NOISE ORDINANCE(S).

SOLE SOURCE AQUIFER CLE-50-0547

THIS PROJECT IS LOCATED WITHIN A SOLE SOURCE AQUIFER. IN ORDER TO MINIMIZE THE POTENTIAL FOR CONTAMINATION, THE CONTRACTOR SHALL UTILIZE PROPER CONTAINMENT AND DIKING IN REFUELING AREAS. FUELS, TOXIC/HAZARDOUS MATERIALS, AND CHEMICALS SHALL NOT BE STORED NEAR DRAINAGE WAYS, DITCHES, OR STREAMS. A SPILL KIT IS TO BE MAINTAINED ON-SITE THROUGHOUT CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL IMMEDIATELY TAKE STEPS TO MITIGATE ANY EVENT, SUCH AS A SPILL OF FUELS, OILS, OR CHEMICALS, THAT COULD THREATEN TO CONTAMINATE THE DRINKING WATER SUPPLY. IF THE SPILL IS A REPORTABLE AMOUNT (PER OHIO EPA'S RELEASE REPORTING REQUIREMENTS), THE CONTRACTOR SHALL CONTACT MIAMI TOWNSHIP FIRE DEPARTMENT, 513-248-3706 OR THE OHIO EPA'S SPILLS HOTLINE 1-800-282-9378 FOR CLEAN-UP OF THE SPILL.

DESIGN AGENCY



DESIGNER
CAH

REVIEWER
JTO MM-DD-YY

PROJECT ID
112980

SHEET TOTAL
04 42

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL OR BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

A QUANTITY OF 14 WORK ZONE IMPACT ATTENUATORS HAS BEEN CARRIED TO THE GENERAL SUMMARY.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 1 (BI-DIRECTIONAL) 28 EACH

ITEM 614, OBJECT MARKER, TWO-WAY 14 EACH

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE OFFICE OF COMMUNICATIONS. THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTICE TO OFFICE OF COMMUNICATIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO OFFICE OF COMMUNICATIONS
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION AND TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTICE TO OFFICE OF COMMUNICATIONS TIME TABLE.

PORTABLE BARRIER

THE CONTRACTOR SHALL ERECT PORTABLE BARRIER TO PROTECT STATIONARY CONSTRUCTION EQUIPMENT USED FOR PAINTING, CONCRETE SEALING, ETC. A QUANTITY OF 2,100 LIN. FEET OF PORTABLE BARRIER HAS BEEN CARRIED TO THE GENERAL SUMMARY. THIS QUANTITY SHALL BE USED FOR SIGNALIZED CLOSURE AT SR 727 AND SR 134; AND FOR INTERSTATE SHOULDER CLOSURES ON I-71

QUANTITIES

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO MAINTAIN ONE UN-SHIFTED LANE AT SR 727 AND SR 134:

ITEM 614 - WORKZONE EDGE LINE, CLASS 1, 6", 740.06, TYPE 1 = 0.8 MILES
ITEM 614 - WORKZONE STOPLINE, CLASS 1, 740.06, TYPE 1 = 48 FT

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) 7 AND 8 OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 2 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 6 SIGN MONTHS ASSUMING 2 PCMS SIGNS FOR 3 MONTHS FOR THE I-71 BRIDGES.

PERMITTED LANE CLOSURE TIMES

SHORT TERM LANE CLOSURES ARE THOSE WHICH ARE PERMITTED BY THE PERMITTED LANE CLOSURE NOTE. THESE TIMES SHALL NOT BE REVISED WITHOUT PRIOR APPROVAL FROM THE DISTRICT 8 WORK ZONE TRAFFIC CONTROL MANAGER. SHORT TERM LANE CLOSURES SHALL ONLY BE IMPLEMENTED WHEN WORK IS BEING CONTINUOUSLY PERFORMED IN THE LANE. THE CLOSURE SHALL BE REMOVED AS SOON AS POSSIBLE AFTER WORK HAS STOPPED. PERMITTED LANE CLOSURES SHALL ONLY BE ALLOWED DURING THE TIMES SPECIFIED IN THE LANE VALUE CONTRACT TABLE INCLUDED IN THESE PLANS. NO LANE OR SHOULDER CLOSURE SHALL BE IN PLACE WHEN NO WORK IS BEING PERFORMED.

LANE VALUE CONTRACT TABLE			
DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ TIME UNIT PER LANE
ON I71: ALL LANES OPEN	9 AM TO 8 PM	1 MINUTE	\$300
ON I-71 TRAFFIC MAINTAINED (15 MINUTE SHORT DURATION COMPLETE CLOSURE PER MT-99.60	5 AM TO 12 MIDNIGHT	1 MINUTE	\$300
US 50 ALL LANES OPEN TO TRAFFIC	6 AM TO 8:30 AM & 3 PM TO 6 PM	1 MINUTE	\$250
SR 727 & SR 134 ALL LANES OPEN TO TRAFFIC	NO RESTRICTIONS	1 MINUTE	\$200
SR 73 ALL LANES OPEN TO TRAFFIC	2 PM TO 6 PM	1 MINUTE	\$250

DESIGN AGENCY



DESIGNER

CAH

REVIEWER

SK MM-DD-YY

PROJECT ID

112980

SHEET TOTAL

06 42

REFER TO THE FOLLOWING SUPPLEMENTAL SPECIFICATIONS:

AR-1-57 4/2/62

DESIGN STRESSES

CLASS QC1 CONCRETE (ABUTMENT) - COMPRESSIVE STRENGTH = 4.0 KSI

ITEM 519 - PATCHING CONCRETE STRUCTURES, AS PER PLAN

PRIOR TO THE SURFACE CLEANING SPECIFIED IN 519.04 AND WITHIN 24 HOURS OF PLACING PATCHING MATERIAL, BLAST CLEAN ALL SURFACES TO BE PATCHED INCLUDING THE EXPOSED REINFORCING STEEL. ACCEPTABLE METHODS INCLUDE HIGH-PRESSURE WATER BLASTING WITH OR WITHOUT ABRASIVES IN THE WATER, ABRASIVE BLASTING WITH CONTAINMENT, OR VACUUM ABRASIVE BLASTING. MINIMUM WATER PRESSURE SHALL BE 1,500 PSI.

ITEM 202 - PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN

THIS WORK CONSISTS OF THE REMOVAL OF PORTIONS OF THE EXISTING STRUCTURES, ETC. AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. THE PROVISIONS OF ITEM 202 APPLY EXCEPT AS SPECIFIED BY THE FOLLOWING NOTES. PERFORM WORK CAREFULLY DURING SUBSTRUCTURE REMOVALS TO PROTECT PORTIONS OF SUCH SYSTEMS THAT ARE TO BE SALVAGED AND INCORPORATED INTO THE PROPOSED STRUCTURE. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE RAM TYPE OF EQUIPMENT IS PROHIBITED. SUBMIT CONSTRUCTION AND DEMOLITION PLANS TO THE ENGINEER FOR APPROVAL.

THE CONTRACTOR MUST REVIEW THE STRUCTURE WHEN PREPARING HIS BID. THE CONTRACTOR WILL REVIEW THE CONDITION OF THE STRUCTURE TO DETERMINE WHAT DEBRIS WILL FALL FROM THE STRUCTURE DURING REMOVAL. THE CONTRACTOR WILL DETERMINE THE CORRESPONDING COST TO CLEAN UP ANY AND ALL DEBRIS WHICH FALLS FROM THE STRUCTURE DURING ALL REMOVAL OPERATIONS. THE COST TO CLEAR AND CLEAN UP ALL DEBRIS DURING REMOVAL SHALL BE INCLUDED WITH THE BID FOR THIS ITEM OF WORK. NO ADDITIONAL COST WILL BE RECOGNIZED TO CLEAN DEBRIS RESULTING FROM THE STRUCTURE REMOVAL OPERATION.

CUT LINE CONSTRUCTION JOINT PREPARATION: SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 1 INCH DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL, IF REQUIRED IN THE PLANS, IN PLACE. INSTALL DOWEL BARS IF SPECIFIED. PRIOR TO CONCRETE PLACEMENT ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST, OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH, BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

THE CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE ANY PORTION OF THE STRUCTURE THAT WILL REMAIN IN SERVICE. ANY PORTION OF THE REMAINING STRUCTURE DAMAGED AS A RESULT OF CONTRACTOR ACTIONS SHALL BE REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.

MEASUREMENT & PAYMENT: THE DEPARTMENT WILL MEASURE THE QUANTITY OF REMOVALS ON A LUMP SUM BASIS. THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES OF REMOVALS AT THE CONTRACT PRICE FOR ITEM 202, PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN.

SEALING OF CONCRETE SURFACES (EPOXY-URETHANE), AS PER PLAN

DUE TO THE RECENT SUPPLY SHORTAGES, THE DEPARTMENT HAS BEEN MADE AWARE OF DIFFICULTIES THAT SUPPLIERS ARE HAVING IN OBTAINING THE NECESSARY MATERIALS FOR EPOXY. ON THIS PROJECT THE CONTRACTOR CAN USE TRADITIONAL EPOXY-URETHANE SEALERS APPROVED ON THE QPL OR ELECT TO SUBSTITUTE BRIDGE COTE XL-70 W/SILANE THAT IS LISTED ON THE APPROVED NOISE SUPPLIER LIST UNDER APPROVED SEALERS FOR NOISE BARRIERS.

APPROVEDNOISESUPPLIERSLIST.PDF (OHIO.GOV)

IF BRIDGE COTE XL-70 W/SILANE IS CHOSEN, MEET THE REQUIREMENTS OF THE BRIDGE COTE XL-70 W/SILANE TECHNICAL DATA SHEET WITH THE EXCEPTION OF THE SURFACE PREPARATION THAT WILL STILL FOLLOW THE REQUIREMENTS LISTED UNDER C&MS 512 FOR EPOXY URETHANE SEALERS.

APPLY NON-SKID SURFACE TREATMENT TO SEALED BRIDGE SIDEWALKS AND SAFETY CURBS.

PAINTING OF STRUCTURAL STEEL AND CONCRETE SELAING WORK

MODIFICATIONS TO THE VANDAL FENCE FABRIC TO ACCOMMODATE PAINTING TARPS AND SEALING OF THE TOP SURFACE OF THE BARRIER MAY BE REQUIRED, BUT SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PAINTING AND SEALING WORK. THE EXISTING CONCRETE SEALER TO REMAIN IN PLACE SHALL BE PROTECTED DURING BLASTING/PAINTING OPERATIONS OR REPAIRED AT THE CONTRACTOR'S EXPENSE.

NO WORK SHALL OCCUR AND NO EQUIPMENT SHALL BE PLACED BELOW THE ORDINARY HIGH WATER ELEVATION TO COMPLETE THE BRIDGE PAINTING WORK AND/OR CONCRETE SEALING WORK.

CLI-71-0407 PARAPET RECONSTRUCTION

TEMPORARY REMOVAL, STORAGE AND RECONNECTION OF SALVAGED GUARDRAIL WITH NEW ANCHOR BOLTS WILL BE REQUIRED, BUT SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE CONCRETE WORK. ANCHORS BOLTS SHALL MATCH EXISTING. ANY DAMAGE TO THE EXISTING GUARDRAIL SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02 AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD.

CONSTRUCTION PLANS

ARCHIVE BRIDGE DRAWINGS WERE USED TO PREPARE THESE CONSTRUCTION PLANS. SUCH DRAWINGS ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY.

PROPOSED REHABILITATION WORK

BRIDGE CLE-50-0546 (SFN 1300946):

1. PAINT THE EXPOSED PORTIONS OF EXISTING PILING ABOVE THE CONCRETE ENCASEMENT AT EACH PIER UP TO THE BOTTOM OF THE PIER CAP. INCLUDE PAINTING THE CONCRETE WITH THE SAME OZEU SYSTEM WITHIN 3" OF THE PIPE TO ENSURE THE INTERFACE BETWEEN THE CONCRETE AND STEEL IS ADEQUATELY BLASTED AND PAINTED. COLOR TO BE FEDERAL COLOR 14277 (GREEN).

BRIDGE CLE-727-0263 (SFN 1304747):

1. PAINT THE EXISTING STRUCTURAL STEEL USING OZEU 514 SPECIFICATIONS. COLOR TO BE FEDERAL COLOR 14277 (GREEN).

BRIDGE CLI-71-0031 (SFN 1401416):

1. PAINT THE EXISTING STRUCTURAL STEEL USING OZEU 514 SPECIFICATIONS. COLOR TO BE FEDERAL COLOR 14277 (GREEN).
2. PATCH THE EXISTING CONCRETE ABUTMENTS WITH 519 PATCHING.
3. EXCEPT FOR PIERS, REMOVE ALL EXISTING CONCRETE SEALER. EXCEPT FOR PIERS, SEAL THE EXISTING CONCRETE SURFACES WITH EPOXY-URETHANE SEALER, FEDERAL COLOR 17778. PIERS WILL BE REHABILITATED WITH A FUTURE PROJECT.

BRIDGE CLI-73-0283 (SFN 1402102) WHICH CARRIES SR 73 OVER I-71:

1. PAINT THE EXISTING STRUCTURAL STEEL USING OZEU 514 SPECIFICATIONS. COLOR TO BE FEDERAL COLOR 14277 (GREEN).
2. PERFORM CONCRETE PATCHING AND EPOXY INJECT HORIZONTAL CRACKS IN BARRIERS.
3. EXCEPT FOR PIERS, REMOVE ALL EXISTING CONCRETE SEALER. EXCEPT FOR PIERS, SEAL THE EXISTING CONCRETE SURFACES WITH EPOXY-URETHANE SEALER, FEDERAL COLOR 17778. PIERS WILL BE REHABILITATED WITH A FUTURE PROJECT.

BRIDGE CLI-71-0407 (SFN 1401505):

1. PAINT THE EXISTING STRUCTURAL STEEL USING OZEU 514 SPECIFICATIONS. COLOR TO BE FEDERAL COLOR 14277 (GREEN).
2. REPAIR DAMAGED RAILING/BARRIER ON NORTHWEST WINGWALL BY RECONSTRUCTING CONCRETE PER ARCHIVE STD. DWG. AR-1-57.
3. EXCEPT FOR PIERS, REMOVE ALL EXISTING CONCRETE SEALER. EXCEPT FOR PIERS, SEAL THE EXISTING CONCRETE SURFACES WITH EPOXY-URETHANE SEALER, FEDERAL COLOR 17778. PIERS WILL BE REHABILITATED WITH A FUTURE PROJECT.

BRIDGE CLI-134-1107 (SFN 1402978):

1. PAINT THE EXISTING STRUCTURAL STEEL USING OZEU 514 SPECIFICATIONS. COLOR TO BE FEDERAL COLOR 14277 (GREEN).

STRUCTURE NOTES
BRIDGE No: VARIES

SFN VARIES

DESIGN AGENCY



DESIGNER CHECKER

CAH XXX

REVIEWER

XXX MM-DD-YY

PROJECT ID

112980

SUBSET	TOTAL
1	1

SHEET	TOTAL
08	42

STRUCTURE REPAIR (CLE-50-0546)					(100% 02/S-2/13 FUNDING)				
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUTMENT	PIERS	SUPERSTRUCTURE	GENERAL	REFERENCE
503	11100	LS		COFFERDAMS AND EXCAVATION BRACING		LUMP			
514	00050	704	SF	SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL		704			
514	00056	704	SF	FIELD PAINTING EXISTING STRUCTURAL STEEL, PRIME COAT		704			
514	00060	704	SF	FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT		704			
514	00066	704	SF	FIELD PAINTING STRUCTURAL STEEL, FINISH COAT		704			
514	00504	2	MNHR	GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL		2			
514	10000	1	EACH	FINAL INSPECTION REPAIR		1			

STRUCTURE REPAIR (CLE-727-0263)					(100% 04/NFA/13 FUNDING)				
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUTMENT	PIERS	SUPERSTRUCTURE	GENERAL	REFERENCE
514	00050	5203	SF	SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL			5,203		
514	00056	5203	SF	FIELD PAINTING EXISTING STRUCTURAL STEEL, PRIME COAT			5,203		
514	00060	5203	SF	FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT			5,203		
514	00066	5203	SF	FIELD PAINTING STRUCTURAL STEEL, FINISH COAT			5,203		
514	00504	7	MNHR	GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL			7		
514	10000	5	EACH	FINAL INSPECTION REPAIR			5		

STRUCTURE REPAIR (CLI-71-0031)					(100% 01/IMS/13 FUNDING)				
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUTMENT	PIERS	SUPERSTRUCTURE	GENERAL	REFERENCE
512	10101	1208	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE), AS PER PLAN	262		946		8
512	74000	1205	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES	259		946		
514	00050	35075	SF	SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL			35,075		
514	00056	35075	SF	FIELD PAINTING EXISTING STRUCTURAL STEEL, PRIME COAT			35,075		
514	00060	35075	SF	FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT			35,075		
514	00066	35075	SF	FIELD PAINTING STRUCTURAL STEEL, FINISH COAT			35,075		
514	00504	28	MNHR	GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL			28		
514	10000	30	EACH	FINAL INSPECTION REPAIR			30		
519	11101	25	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN	25				8

STRUCTURE REPAIR (CLI-71-0407)					(100% 01/IMS/13 FUNDING)				
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION	ABUTMENT	PIERS	SUPERSTRUCTURE	GENERAL	REFERENCE
202	11203	LS	LUMP	PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	LUMP		LUMP		8
511	45710	0.2	CY	CLASS QC1 CONCRETE, ABUTMENT	0.2				
512	10101	903	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE), AS PER PLAN	140		763		8
512	74000	903	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES	140		763		
514	00050	15965	SF	SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL			15,965		
514	00056	15965	SF	FIELD PAINTING EXISTING STRUCTURAL STEEL, PRIME COAT			15,965		
514	00060	15965	SF	FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT			15,965		
514	00066	15965	SF	FIELD PAINTING STRUCTURAL STEEL, FINISH COAT			15,965		
514	00504	21	MNHR	GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL			21		
514	10000	14	EACH	FINAL INSPECTION REPAIR			14		
517	75001	14	FT	RAILING, ALUMINUM , AS PER PLAN	14				37

STRUCTURE QUANTITIES - 1
 BRIDGE No.: VARIES

SFN
 VARIES
 DESIGN AGENCY



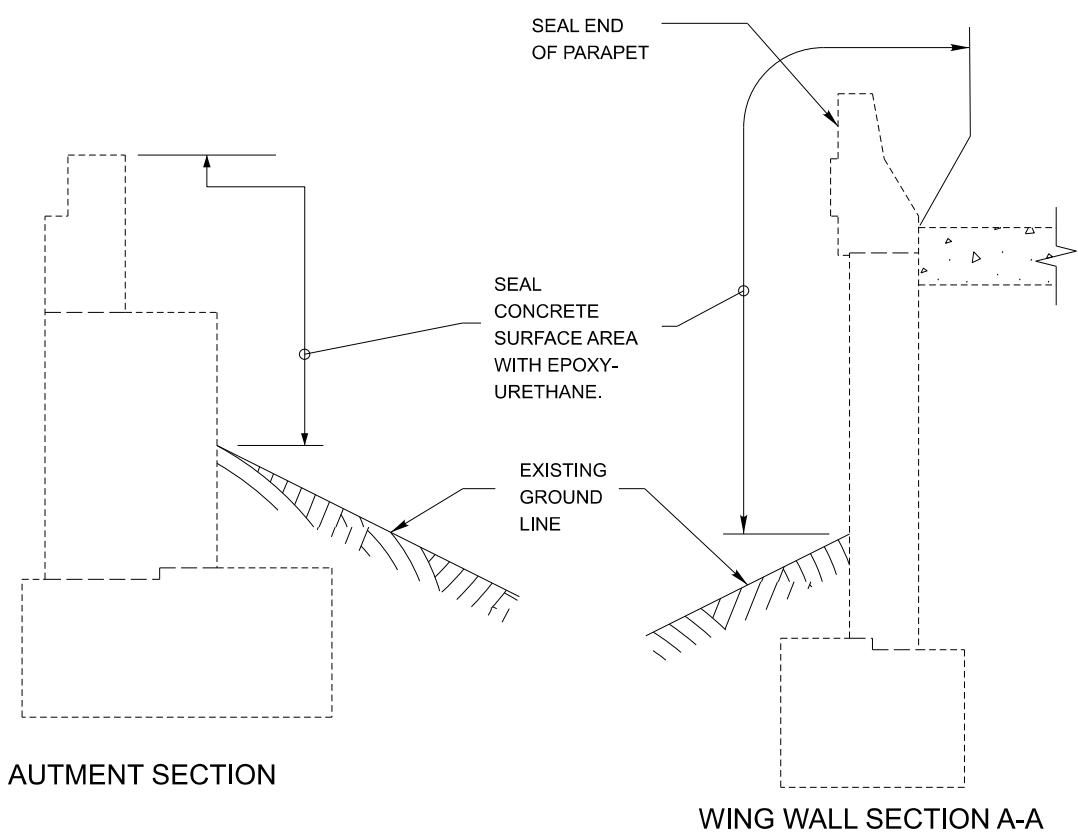
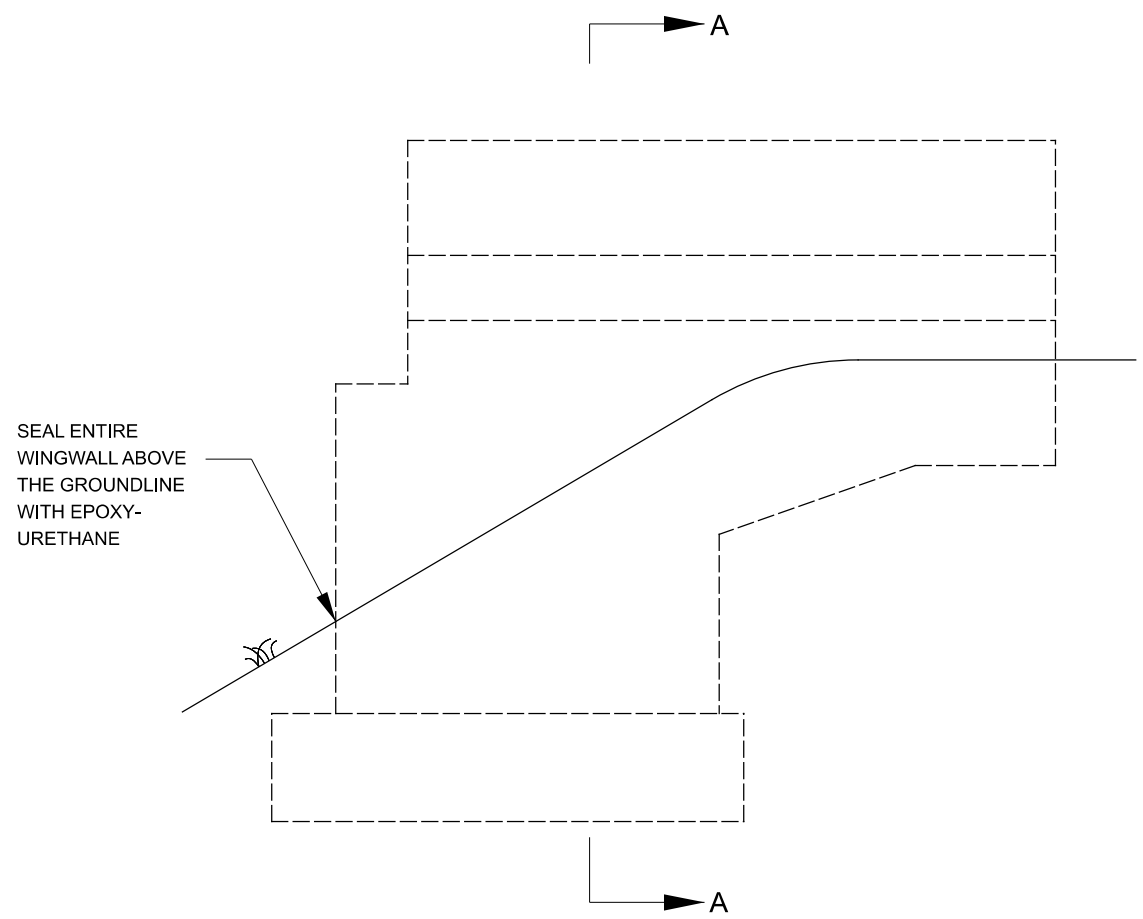
DESIGNER CHECKER
 CAH XXX

REVIEWER
 XXX MM-DD-YY

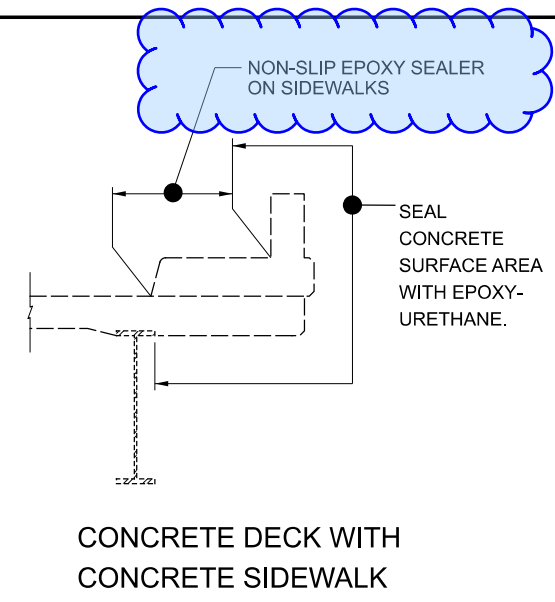
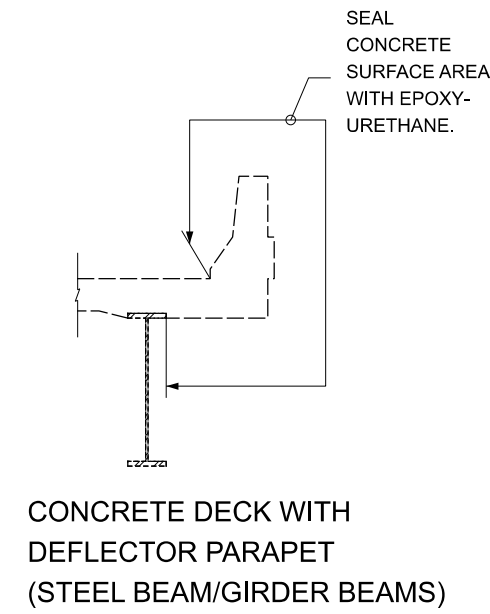
PROJECT ID
 112980

SUBSET TOTAL
 2 2

SHEET TOTAL
 09 42



ABUTMENT AND WINGWALL SEALING LIMITS



CONCRETE SEALING DETAILS
 BRIDGE No.: VARIES

DESIGN AGENCY	
DESIGNER	CAH
REVIEWER	AS
PROJECT ID	112980
SUBSET	TOTAL
1	1
SHEET	TOTAL
11	42

BENCHMARK DATA

BM #2 STA. 17+45.64, EL. 1020.192, OFFSET 15.22', LT
 BM #3 STA. 22+64.15, EL. 1019.789, OFFSET 15.08', RT

FOR ADDITIONAL BENCHMARK INFORMATION. SEE ROADWAY PLAN SHEETS 29/102 & 30/102

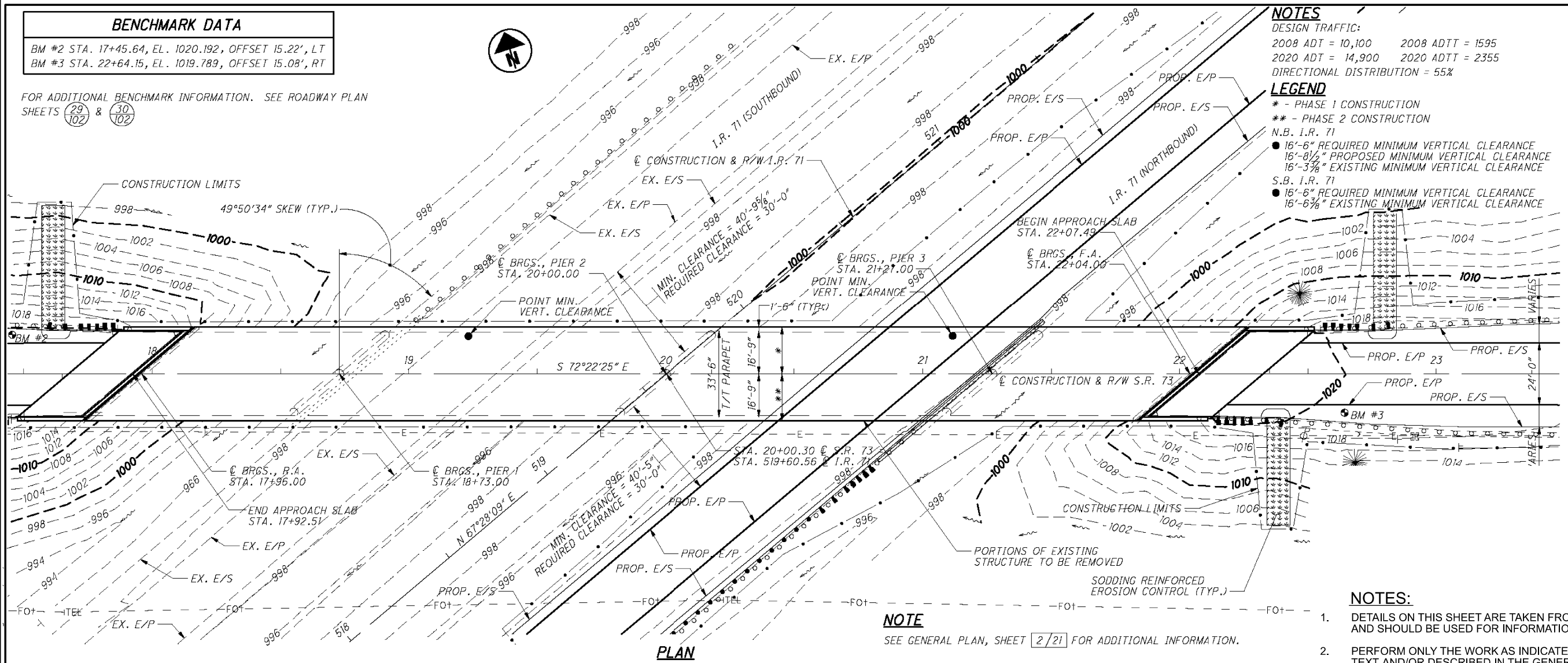


NOTES

DESIGN TRAFFIC:
 2008 ADT = 10,100 2008 ADTT = 1595
 2020 ADT = 14,900 2020 ADTT = 2355
 DIRECTIONAL DISTRIBUTION = 55%

LEGEND

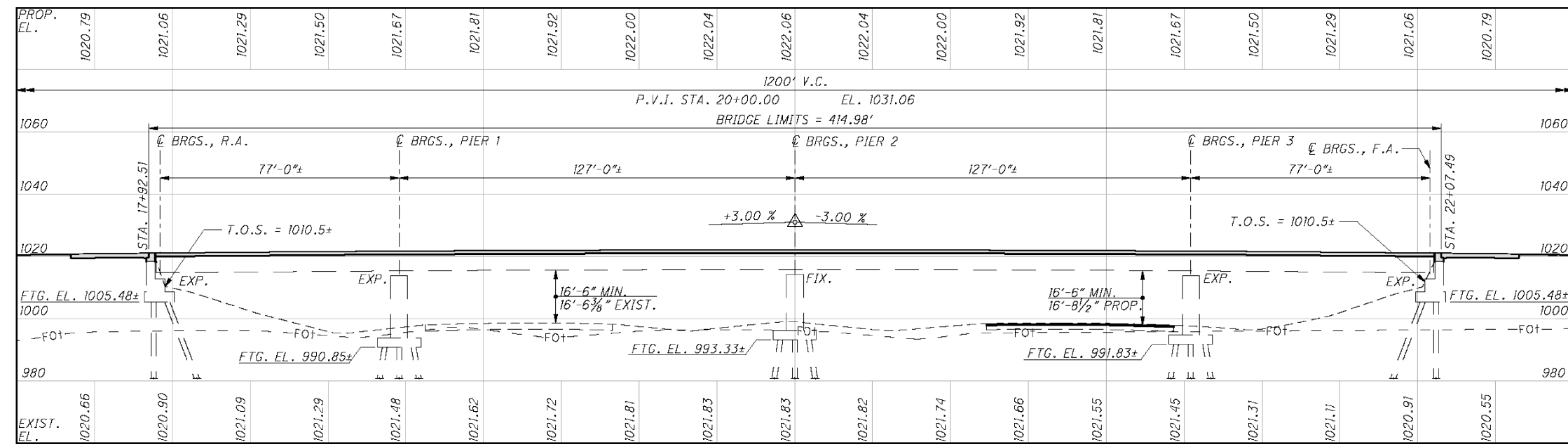
- * - PHASE 1 CONSTRUCTION
- ** - PHASE 2 CONSTRUCTION
- N.B. I.R. 71
- 16'-6" REQUIRED MINIMUM VERTICAL CLEARANCE
- 16'-8 1/2" PROPOSED MINIMUM VERTICAL CLEARANCE
- 16'-3 3/8" EXISTING MINIMUM VERTICAL CLEARANCE
- S.B. I.R. 71
- 16'-6" REQUIRED MINIMUM VERTICAL CLEARANCE
- 16'-6 3/8" EXISTING MINIMUM VERTICAL CLEARANCE



NOTE
 SEE GENERAL PLAN, SHEET 2/21 FOR ADDITIONAL INFORMATION.

- NOTES:**
1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
 2. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT AND/OR DESCRIBED IN THE GENERAL NOTES.

PROFILE ALONG C CONSTRUCTION S.R. 73



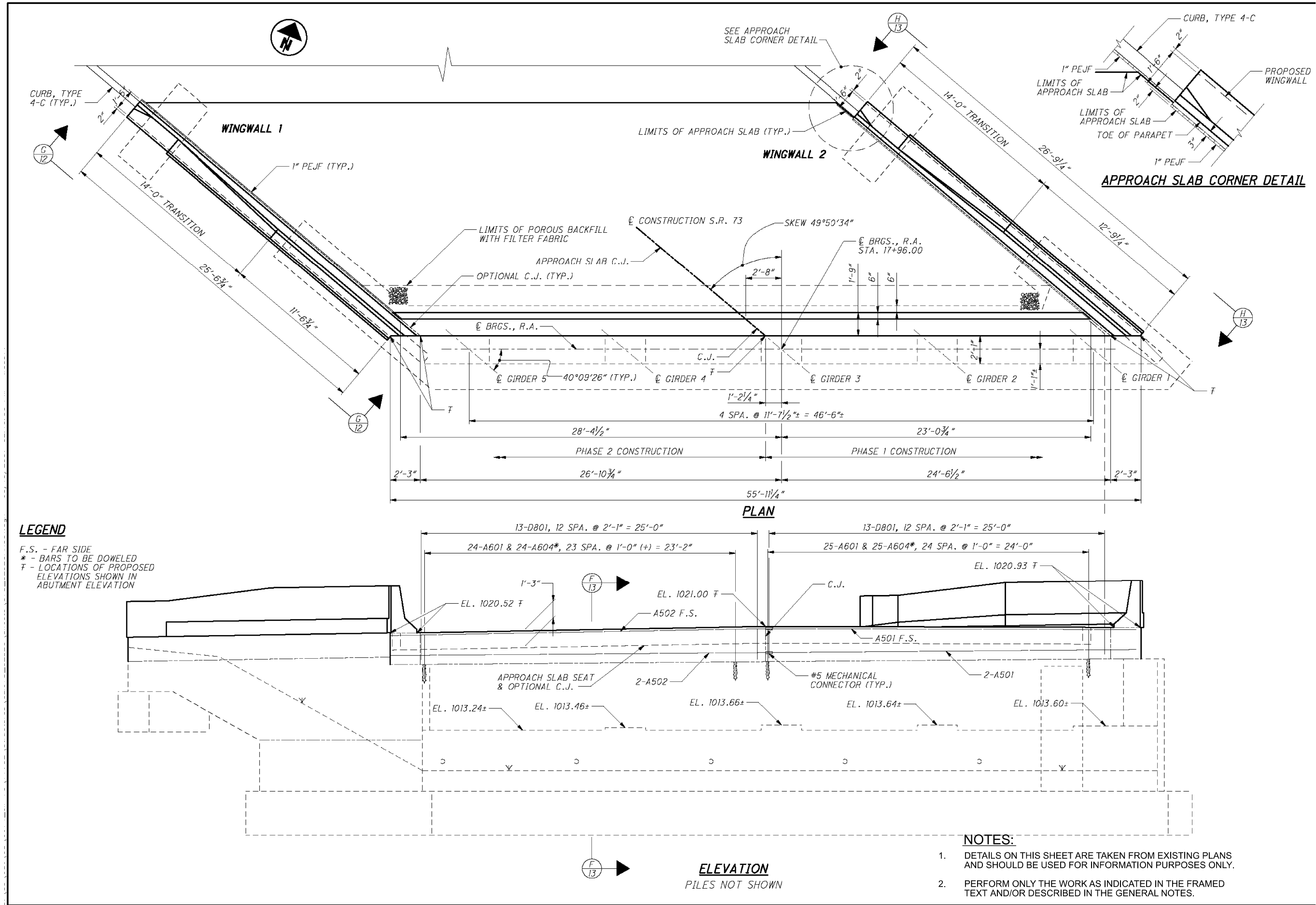
EXISTING STRUCTURE	
TYPE:	CONTINUOUS STEEL GIRDER WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURES
SPANS:	77'-0", 127'-0", 127'-0", 77'-0"
ROADWAY:	33'-6" TOE/TOE OF PARAPETS
LOADING:	HS20-44 & ALT. MILITARY LOADING (HS25 FOR DECK) 60 PSF FUTURE WEARING SURFACE
SKEW:	49°50'34" L.F.
WEARING SURFACE:	1" MONOLITHIC CONCRETE
APPROACH SLABS:	25'-0" LONG (AS-1-81)
ALIGNMENT:	TANGENT
CROWN:	NORMAL 0.016'
STRUCTURE FILE NUMBER:	1401505
DATE BUILT:	1962
DISPOSITION:	MINOR BRIDGE REHAB
DECK AREA:	14,977 SF
COORDINATES:	LATITUDE N 39°29'15" LONGITUDE W 83°56'30"

SITE PLAN
BRIDGE No.: CLI-73-0283
SR 73 OVER 71

CLE/CLI-BP VAR-2024

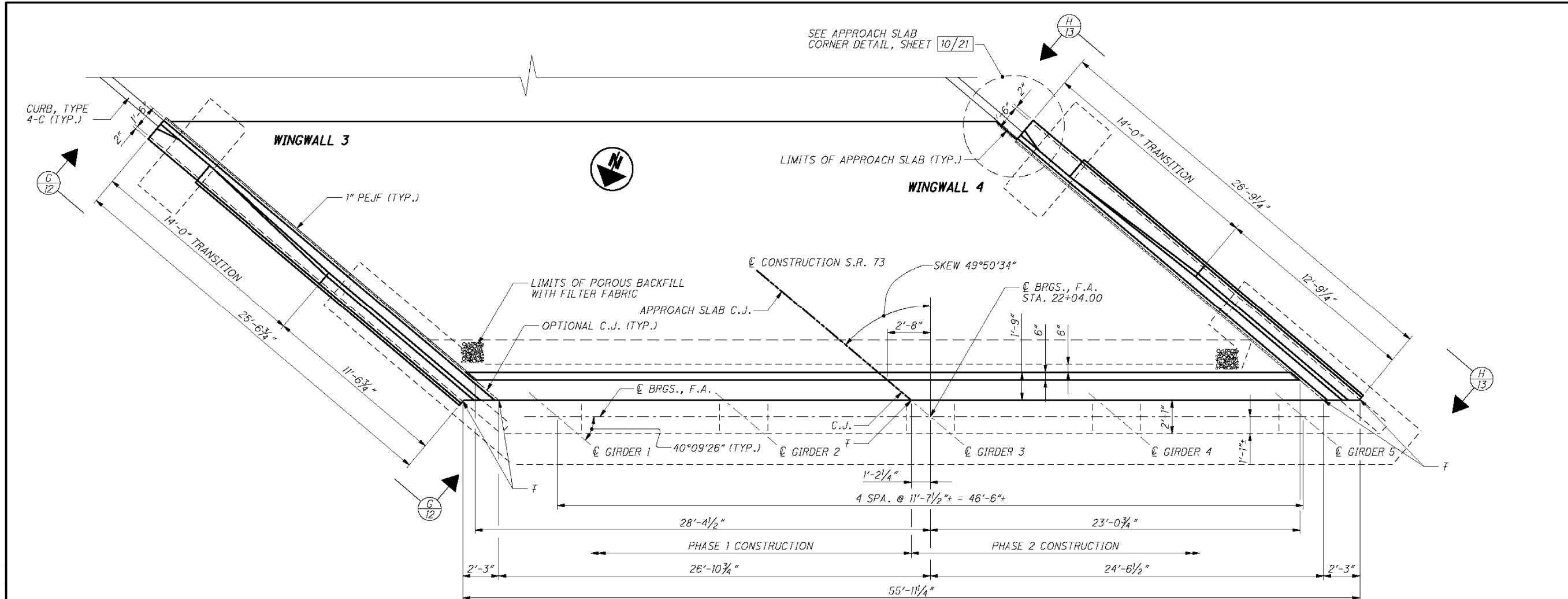
MODEL: Sheet PAPER SIZE: 17x11 (in.) DATE: 1/22/2024 TIME: 10:21:16 AM USER: cchoward4
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SFN	1402102
DESIGN AGENCY	
DESIGNER/CHECKER	CAH XXX
REVIEWER	XXX MM-DD-YY
PROJECT ID	112980
SUBSET	TOTAL
1	11
SHEET	TOTAL
24	42



REAR ABUTMENT PLAN
 BRIDGE No.: CLI-73-0283
 SR 73 OVER I-74

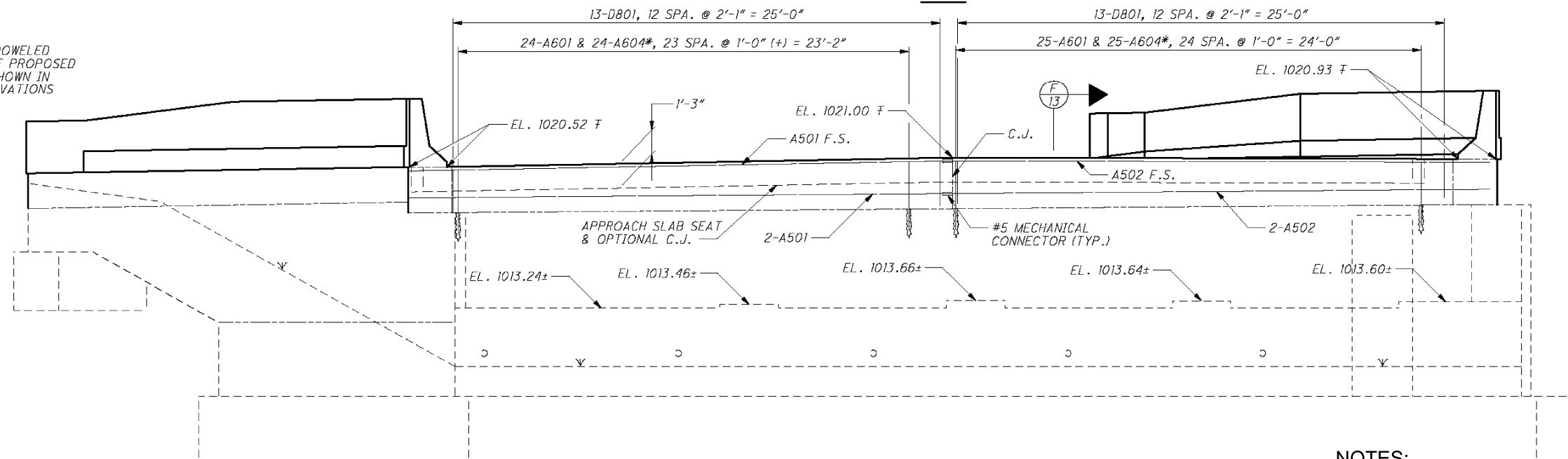
SFN	1402102
DESIGN AGENCY	
DESIGNER	CAH
CHECKER	XXX
REVIEWER	XXX
PROJECT ID	112980
SUBSET	2
TOTAL	11
SHEET	25
TOTAL	42



PLAN

LEGEND

F.S. - FAR SIDE
 * - BARS TO BE DOWELED
 † - LOCATIONS OF PROPOSED ELEVATIONS SHOWN IN ABUTMENT ELEVATIONS



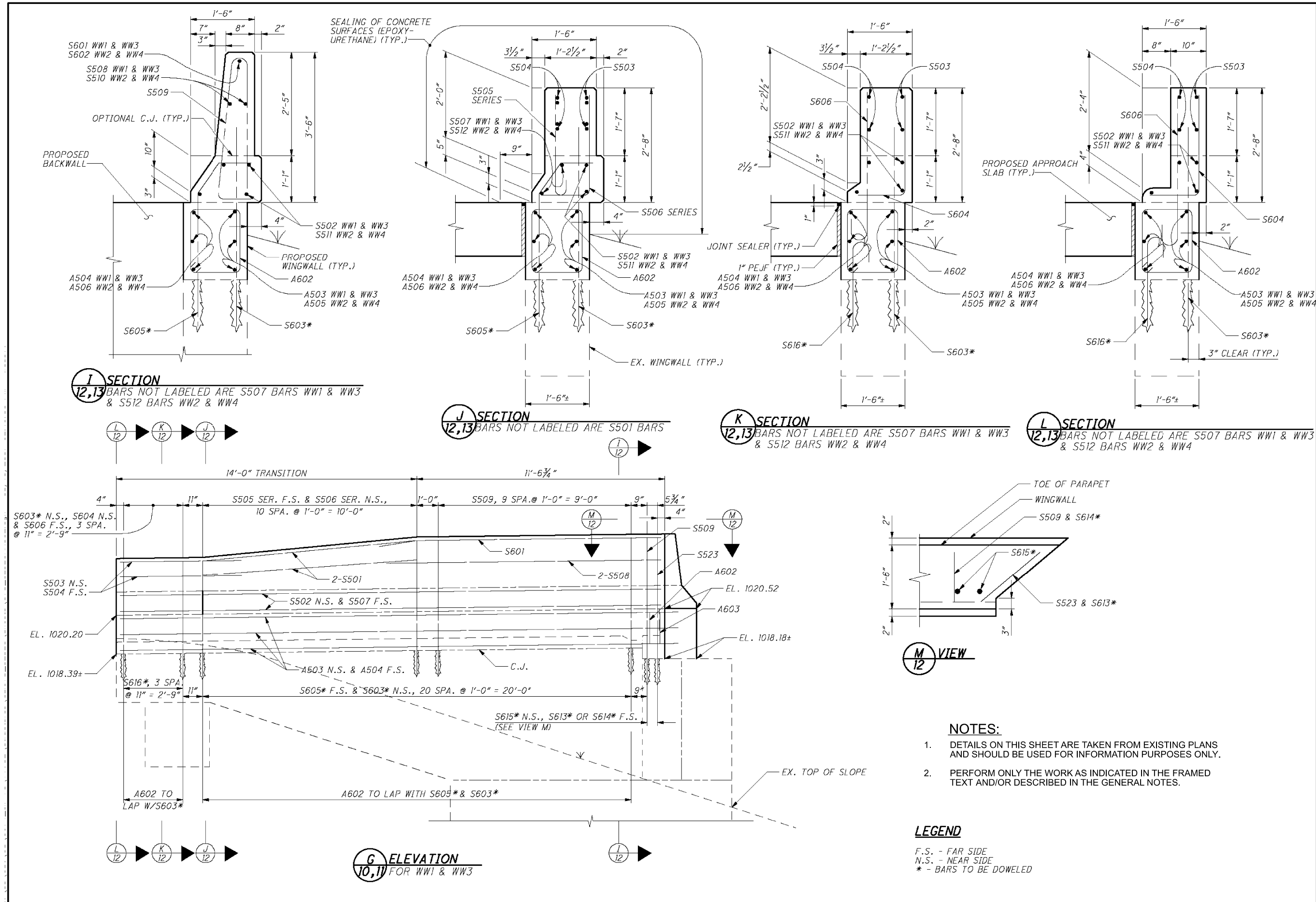
ELEVATION
 PILES NOT SHOWN

NOTES:

1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
2. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT AND/OR DESCRIBED IN THE GENERAL NOTES.

FORWARD ABUTMENT PLAN
 BRIDGE No.: CLI-73-0283
 SR 73 OVER I-77

SFN	
1402102	
DESIGN AGENCY	
DESIGNER	CHECKER
CAH	XXX
REVIEWER	
XXX MM-DD-YY	
PROJECT ID	
112980	
SUBSET	TOTAL
3	11
SHEET	TOTAL
26	42



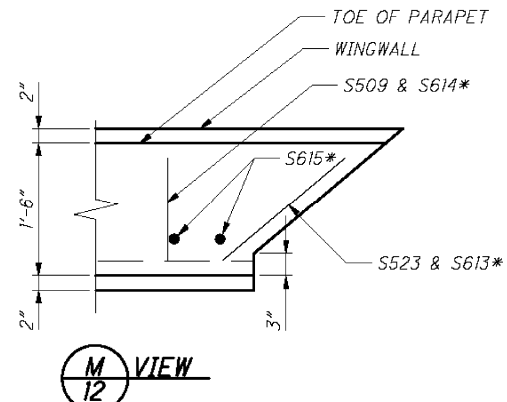
I SECTION
 12,13 BARS NOT LABELED ARE S507 BARS WW1 & WW3
 & S512 BARS WW2 & WW4

J SECTION
 12,13 BARS NOT LABELED ARE S501 BARS

K SECTION
 12,13 BARS NOT LABELED ARE S507 BARS WW1 & WW3
 & S512 BARS WW2 & WW4

L SECTION
 12,13 BARS NOT LABELED ARE S507 BARS WW1 & WW3
 & S512 BARS WW2 & WW4

G ELEVATION
 10,11 FOR WW1 & WW3



M VIEW
 12

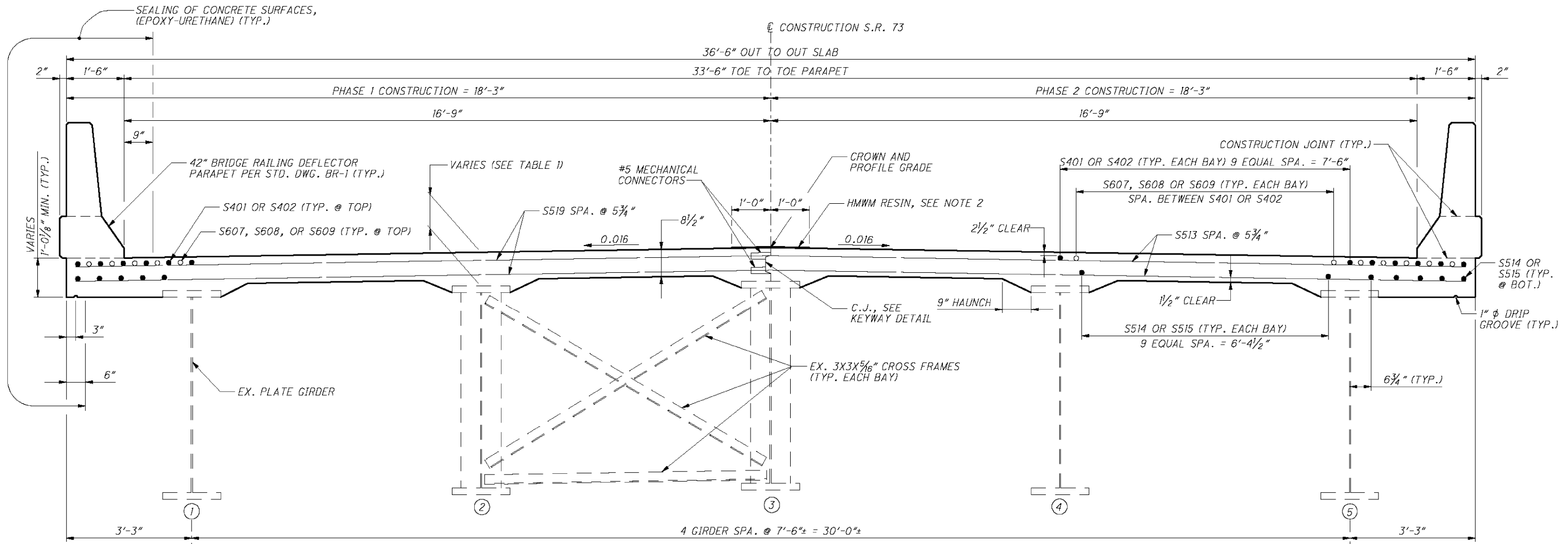
- NOTES:**
1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
 2. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT AND/OR DESCRIBED IN THE GENERAL NOTES.

LEGEND

F.S. - FAR SIDE
 N.S. - NEAR SIDE
 * - BARS TO BE DOWELED

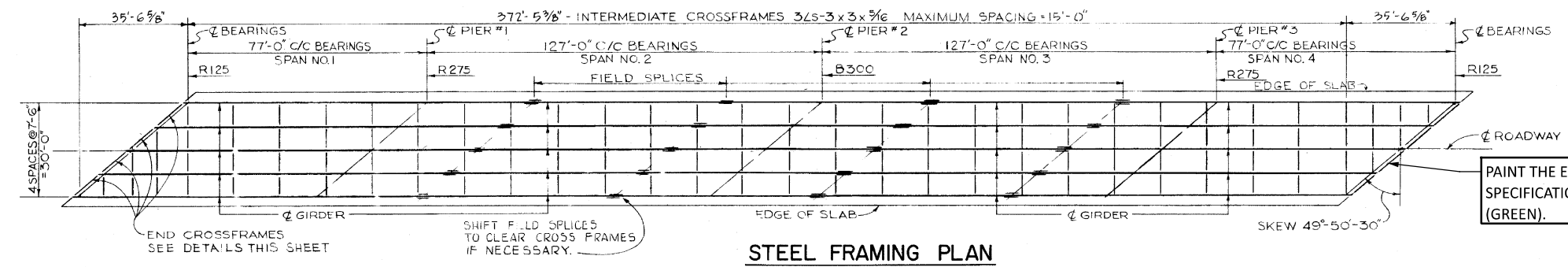
ABUTMENT DETAILS
 BRIDGE No.: CLI-73-0283
 SR 73 OVER I-71

SFN	1402102
DESIGN AGENCY	
DESIGNER/CHECKER	CAH / XXX
REVIEWER	XXX MM-DD-YY
PROJECT ID	112980
SUBSET	TOTAL
4	11
SHEET	TOTAL
27	42



TRANSVERSE SECTION

STUD SHEAR CONNECTORS NOT SHOWN
 VANDAL PROTECTION FENCE NOT SHOWN



STEEL FRAMING PLAN

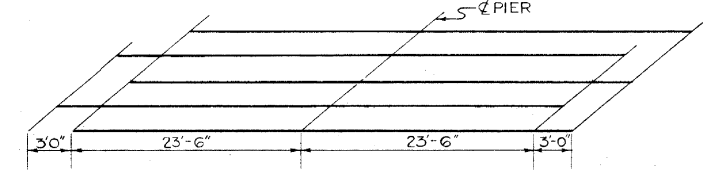
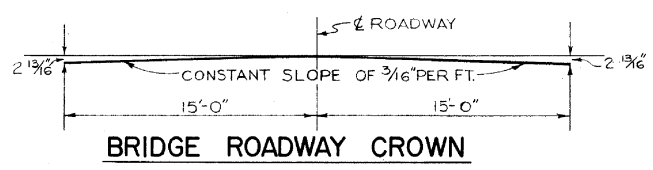


DIAGRAM SHOWING STAGGER OF S603 BARS OVER PIERS



BRIDGE ROADWAY CROWN

NOTES:

1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
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PAINT THE EXISTING STRUCTURAL STEEL USING OZEU 514 SPECIFICATIONS. COLOR TO BE FEDERAL COLOR 14277 (GREEN).

A. M. KINNEY, INC. CINCINNATI, OHIO					
DODSON, KINNEY & LINDBLOM COLUMBUS, OHIO					
SUPERSTRUCTURE DETAILS					
BRIDGE NO. CLI-1-0293 PROPOSED S.R. 1 UNDER STATE ROUTE 73					
CLINTON CO.			PROPOSED S.R. 1 STA. 519 + 60.27		
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE
J.K.	K.B.	K.B.	J.O.	<i>C.J.T.</i>	8-7-62

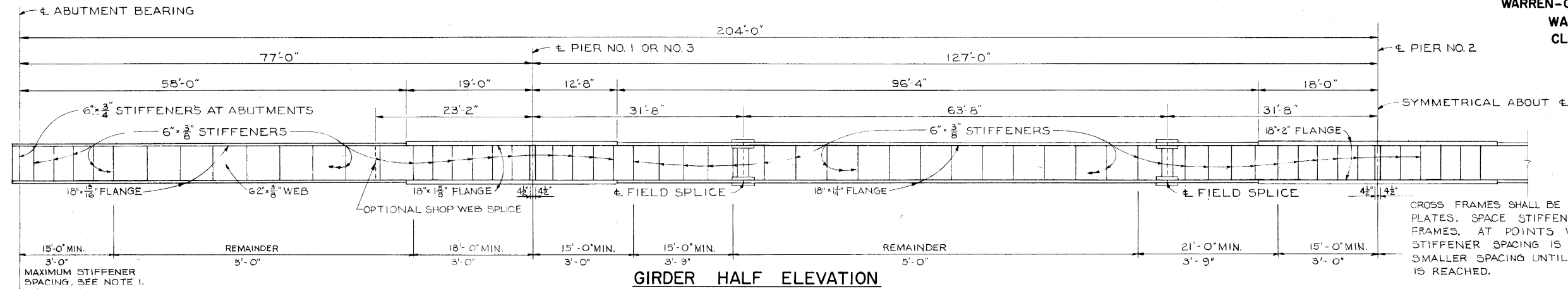
SUPERSTRUCTURE DETAILS
 BRIDGE No.: CLI-73-0283
 SR 73 OVER I-75

SFN	1402102
DESIGN AGENCY	
DESIGNER	CAH
CHECKER	XXX
REVIEWER	
PROJECT ID	112980
SUBSET	5
TOTAL	11
SHEET	28
TOTAL	42

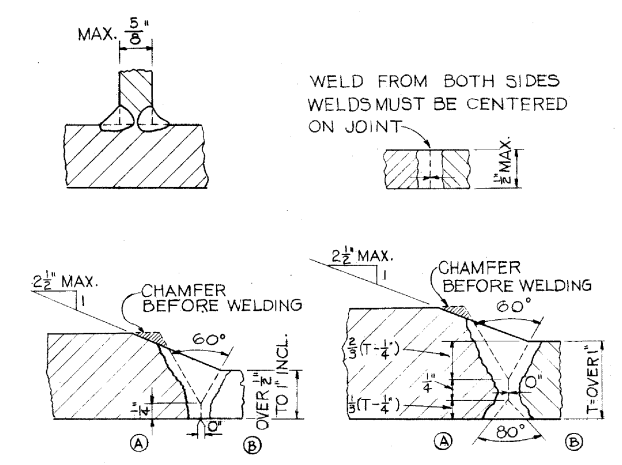
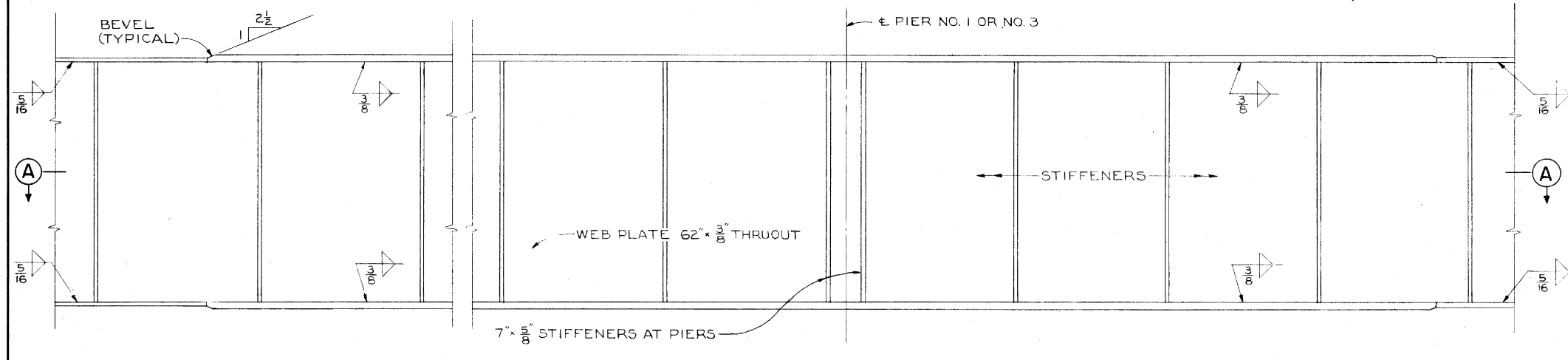
FED. RD. DIVISION	STATE	PROJECT
2	OHIO	

219
256

WARREN-CLINTON COUNTIES
WAR - I - 20.84
CLI - I - 0.00



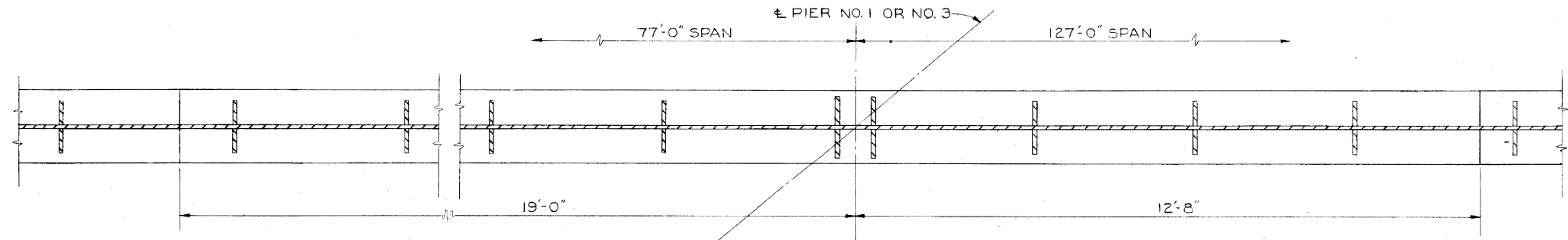
NOTE 1
CROSS FRAMES SHALL BE WELDED TO WEB STIFFENER PLATES. SPACE STIFFENERS EQUALLY BETWEEN CROSS FRAMES. AT POINTS WHERE AN INCREASE IN STIFFENER SPACING IS ALLOWED, MAINTAIN THE SMALLER SPACING UNTIL THE NEXT CROSS FRAME IS REACHED.



NOTE:
 (A): WELD AFTER PLACING AT LEAST ONE PASS ON OTHER SIDE.
 (B): ALL FLANGE BUTT-WELDS SHALL BE GROUND FLUSH.

WELDED JOINTS
AUTOMATIC SUBMERGED ARC PROCESS

GIRDER DETAIL AT PIER NO. 1



SECTION A-A

- NOTES:**
1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
 2. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT AND/OR DESCRIBED IN THE GENERAL NOTES.

A. M. KINNEY, INC. CINCINNATI, OHIO					
DODSON, KINNEY & LINDBLOM COLUMBUS, OHIO					
SUPERSTRUCTURE DETAILS					
BRIDGE NO. CLI-I-0293 PROPOSED S. R. 1 UNDER STATE ROUTE 73					
CLINTON CO.			PROPOSED S. R. 1 STA. 519 + 60.27		
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE
J.K.	W.S.	W.S.	J.O.		8.7.62

SUPERSTRUCTURE DETAILS 2
 BRIDGE No.: CLI-73-0283
 SR 73 OVER I-74

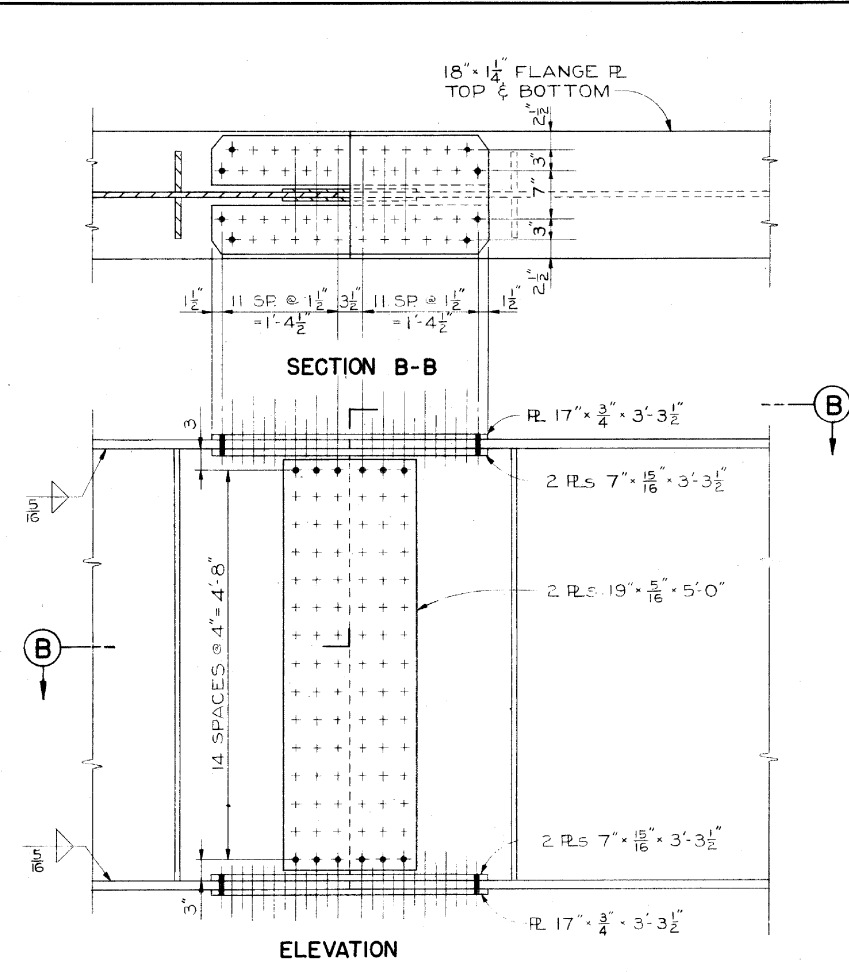
SFN	1402102
DESIGN AGENCY	
DESIGNER	CAH
CHECKER	XXX
REVIEWER	XXX
PROJECT ID	112980
SUBSET	6
TOTAL	11
SHEET	29
TOTAL	42

CLE/CLI-BP VAR-2024

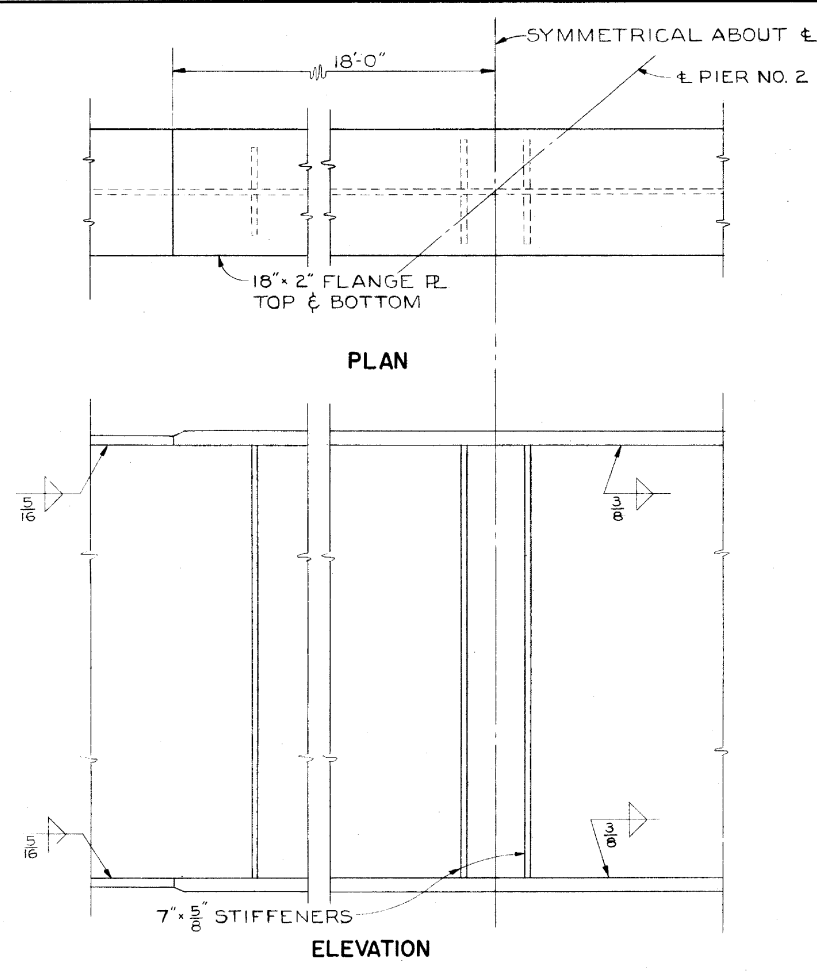
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FED. RD. DIVISION	STATE	PROJECT	220 258
2	OHIO		

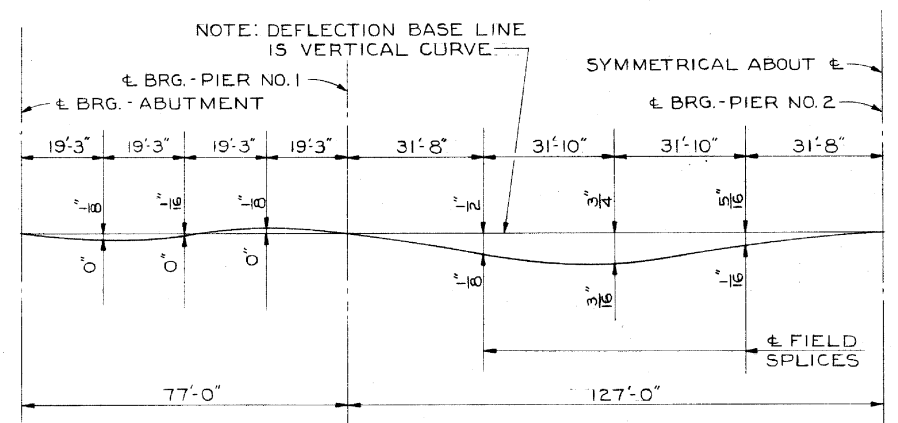
WARREN-CLINTON COUNTIES
WAR -I-20.84
CLI -I-0.00



TYPICAL FIELD SPLICE DETAIL

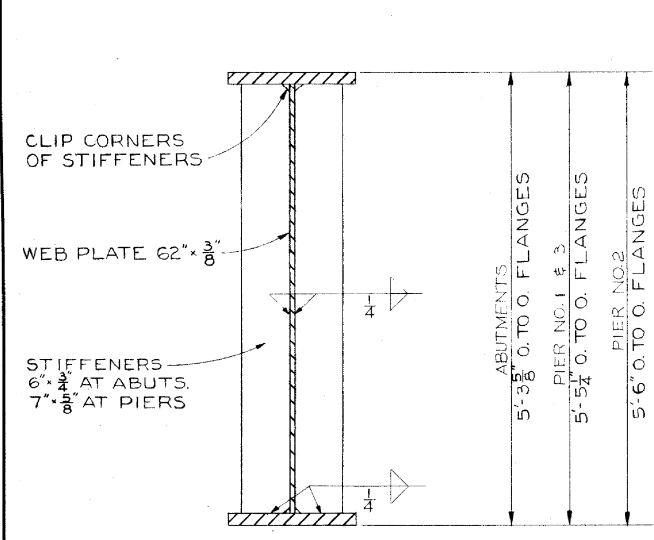


GIRDER DETAILS AT PIER NO. 2

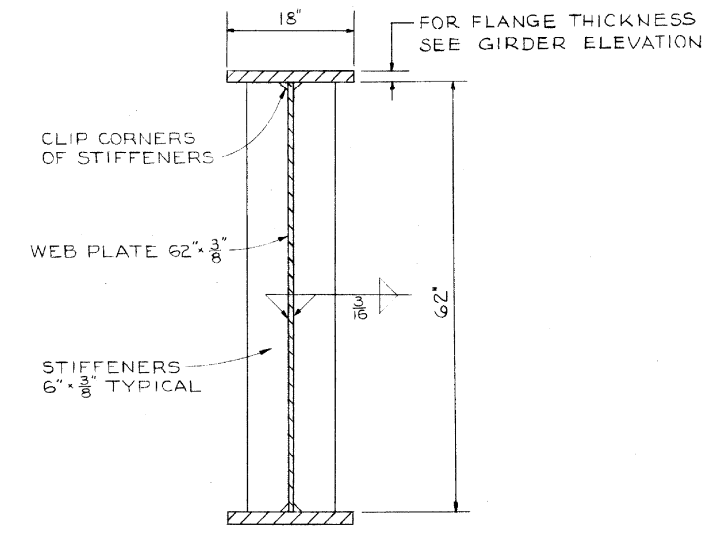


NOTE: DEFLECTION BASE LINE IS VERTICAL CURVE.
NOTE: VALUES SHOWN ABOVE DEFLECTION BASE LINE INDICATE THE TOTAL DEAD LOAD DEFLECTION. VALUES SHOWN BELOW INDICATE THE DEFLECTION DUE TO THE WEIGHT OF STRUCTURAL STEEL ONLY.

TOTAL D.L. DEFLECTION DIAGRAM



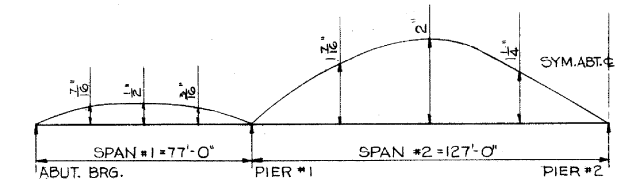
TYPICAL GIRDER SECTION AT ABUTMENTS AND PIERS



TYPICAL GIRDER INTERMEDIATE SECTION

NOTES:

1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
2. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT AND/OR DESCRIBED IN THE GENERAL NOTES.



NOTE: CAMBER ORDINATES SHOWN FOR 1/4 POINTS INCLUDE D.L. DEFLECTION PLUS CONVEXITY REQUIRED FOR VERTICAL CURVE MEASURED FROM CHORD OF VERTICAL CURVE BETWEEN BEARING POINTS.

GIRDER CAMBER DIAGRAM

A. M. KINNEY, INC.
CINCINNATI, OHIO
DODSON, KINNEY & LINDBLOM
COLUMBUS, OHIO

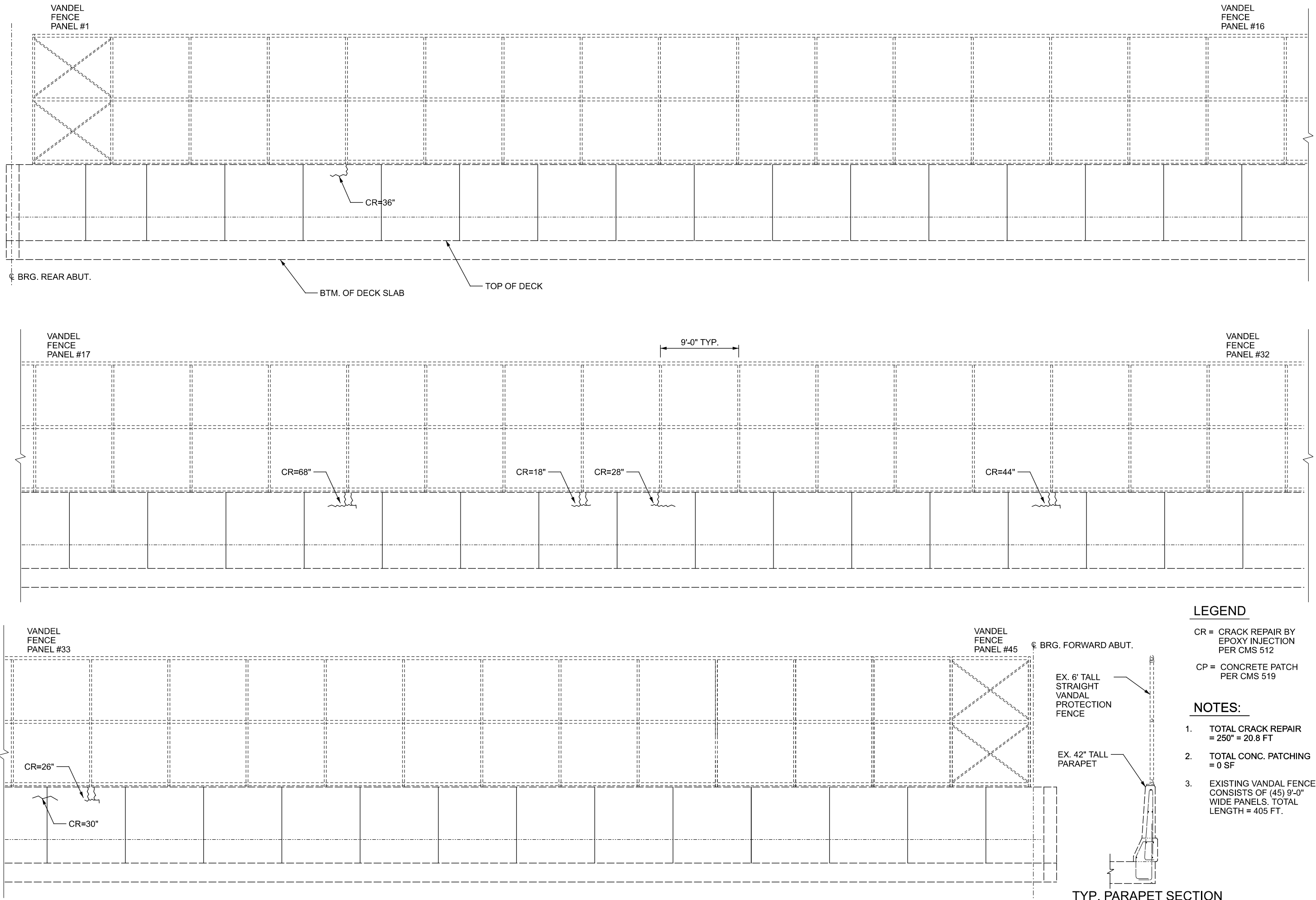
SUPERSTRUCTURE DETAILS

BRIDGE NO. CLI-I-0293
PROPOSED S.R. I UNDER
STATE ROUTE 73
CLINTON CO. PROPOSED S.R. I
STA. 519+60.27

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
J.K.	W.S.	W.S.	J.Q.		6/17/87	

SUPERSTRUCTURE DETAILS - 3
BRIDGE No.: CLI-78-0283
SR 73 OVER I-71

SFN	1402102
DESIGN AGENCY	
DESIGNER	CAH
CHECKER	XXX
REVIEWER	XXX MM-DD-YY
PROJECT ID	112980
SUBSET	7
TOTAL	11
SHEET	30
TOTAL	42

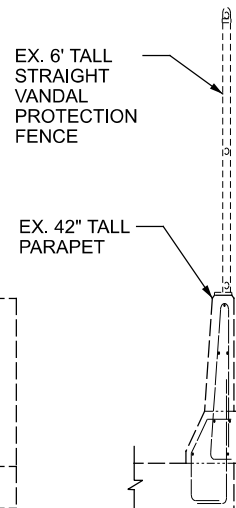


LEGEND

- CR = CRACK REPAIR BY EPOXY INJECTION PER CMS 512
- CP = CONCRETE PATCH PER CMS 519

NOTES:

1. TOTAL CRACK REPAIR = 250' = 20.8 FT
2. TOTAL CONC. PATCHING = 0 SF
3. EXISTING VANDAL FENCE CONSISTS OF (45) 9'-0" WIDE PANELS. TOTAL LENGTH = 405 FT.



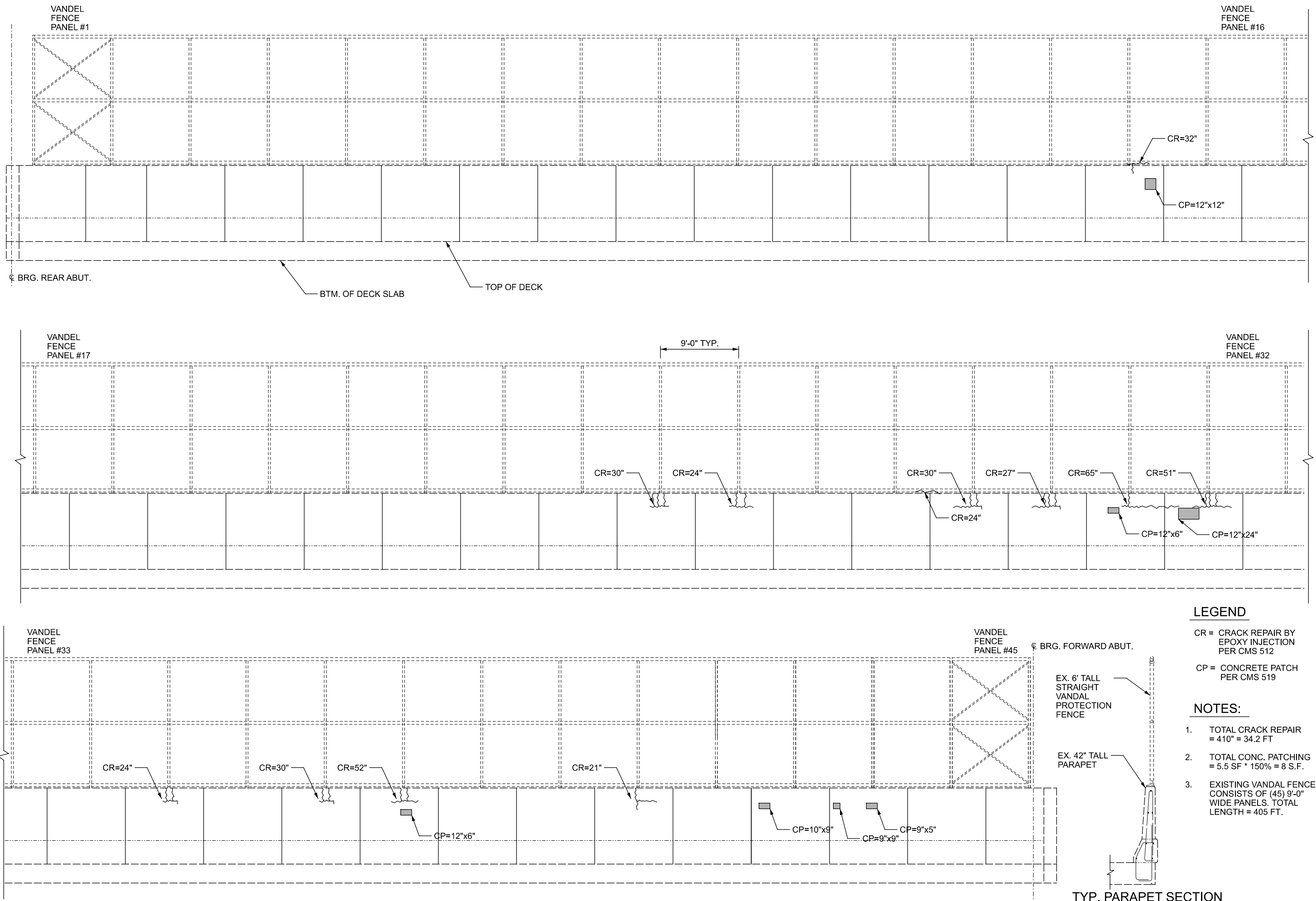
TYP. PARAPET SECTION

LEFT PARAPET (OUTSIDE FACE) REPAIR DETAILS
 BRIDGE No.: CLI-73-0283
 SR 73 OVER I-71

SFN 1402102
 DESIGN AGENCY



DESIGNER	CHECKER
CAH	XXX
REVIEWER	
XXX MM-DD-YY	
PROJECT ID	
112980	
SUBSET	TOTAL
8	11
SHEET	TOTAL
31	42



LEGEND

- CR = CRACK REPAIR BY EPOXY INJECTION PER CMS 512
- CP = CONCRETE PATCH PER CMS 519

NOTES:

1. TOTAL CRACK REPAIR = 410' = 34.2 FT
2. TOTAL CONC. PATCHING = 5.5 SF * 150% = 8 S.F.
3. EXISTING VANDAL FENCE CONSISTS OF (45) 9'-0" WIDE PANELS. TOTAL LENGTH = 405 FT.

LEFT PARAPET (INSIDE FACE) - REPAIR DETAILS

BRIDGE No.: CLI-73-0283

SR 73 OVER I-71

SFN 1402102
 DESIGN AGENCY



DESIGNER/CHECKER
 CAH XXX

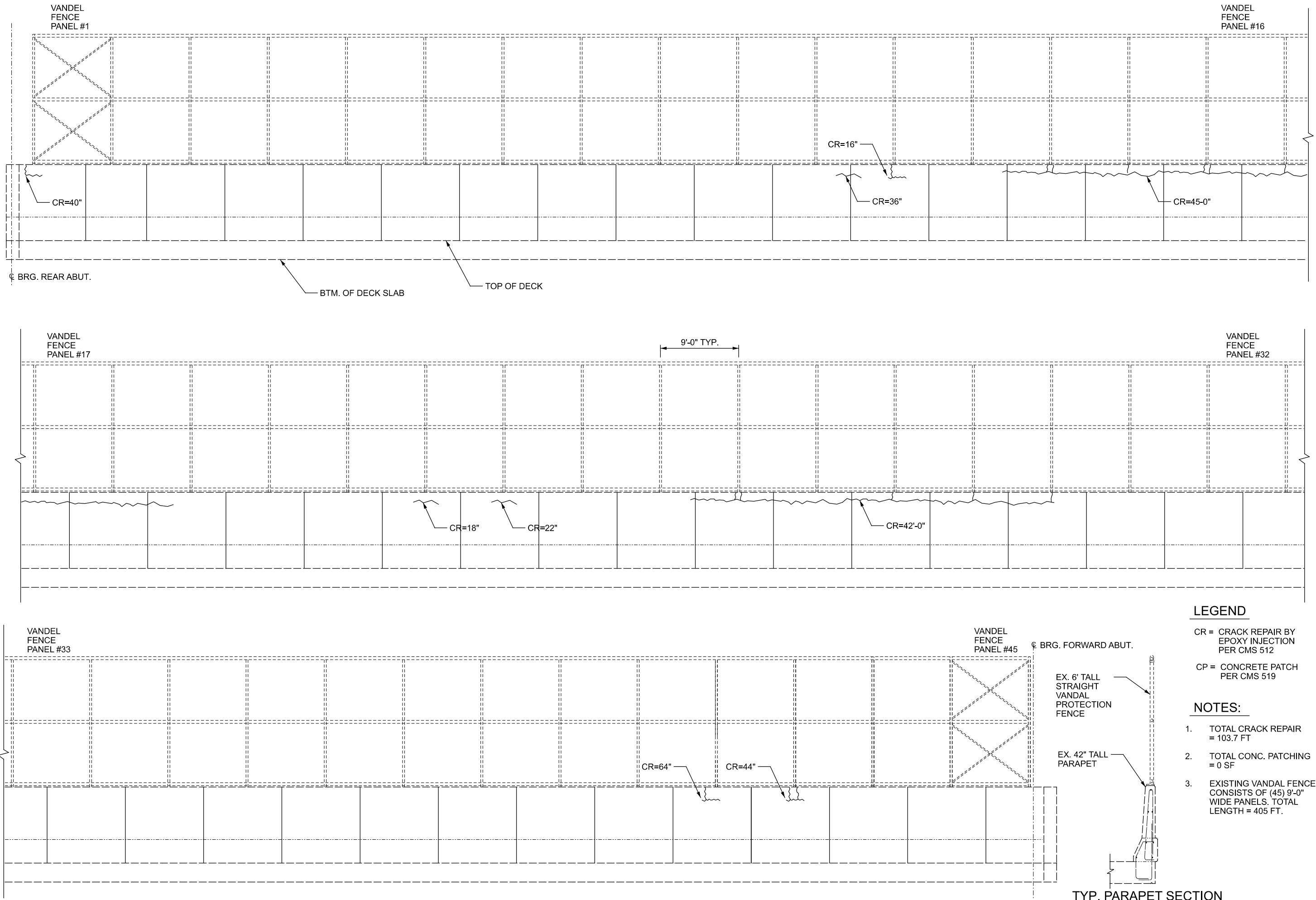
REVIEWER
 XXX MM-DD-YY

PROJECT ID
 112980

SUBSET	TOTAL
9	11

SHEET	TOTAL
32	42

TYP. PARAPET SECTION

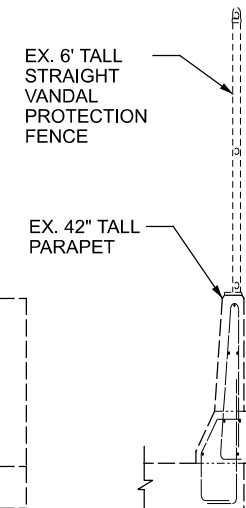


LEGEND

- CR = CRACK REPAIR BY EPOXY INJECTION PER CMS 512
- CP = CONCRETE PATCH PER CMS 519

NOTES:

1. TOTAL CRACK REPAIR = 103.7 FT
2. TOTAL CONC. PATCHING = 0 SF
3. EXISTING VANDAL FENCE CONSISTS OF (45) 9'-0" WIDE PANELS. TOTAL LENGTH = 405 FT.



TYP. PARAPET SECTION

RIGHT PARAPET (OUTSIDE FACE) - REPAIR DETAILS

BRIDGE No.: CLI-73-0283

SR 73 OVER I-71

SFN 1402102
 DESIGN AGENCY



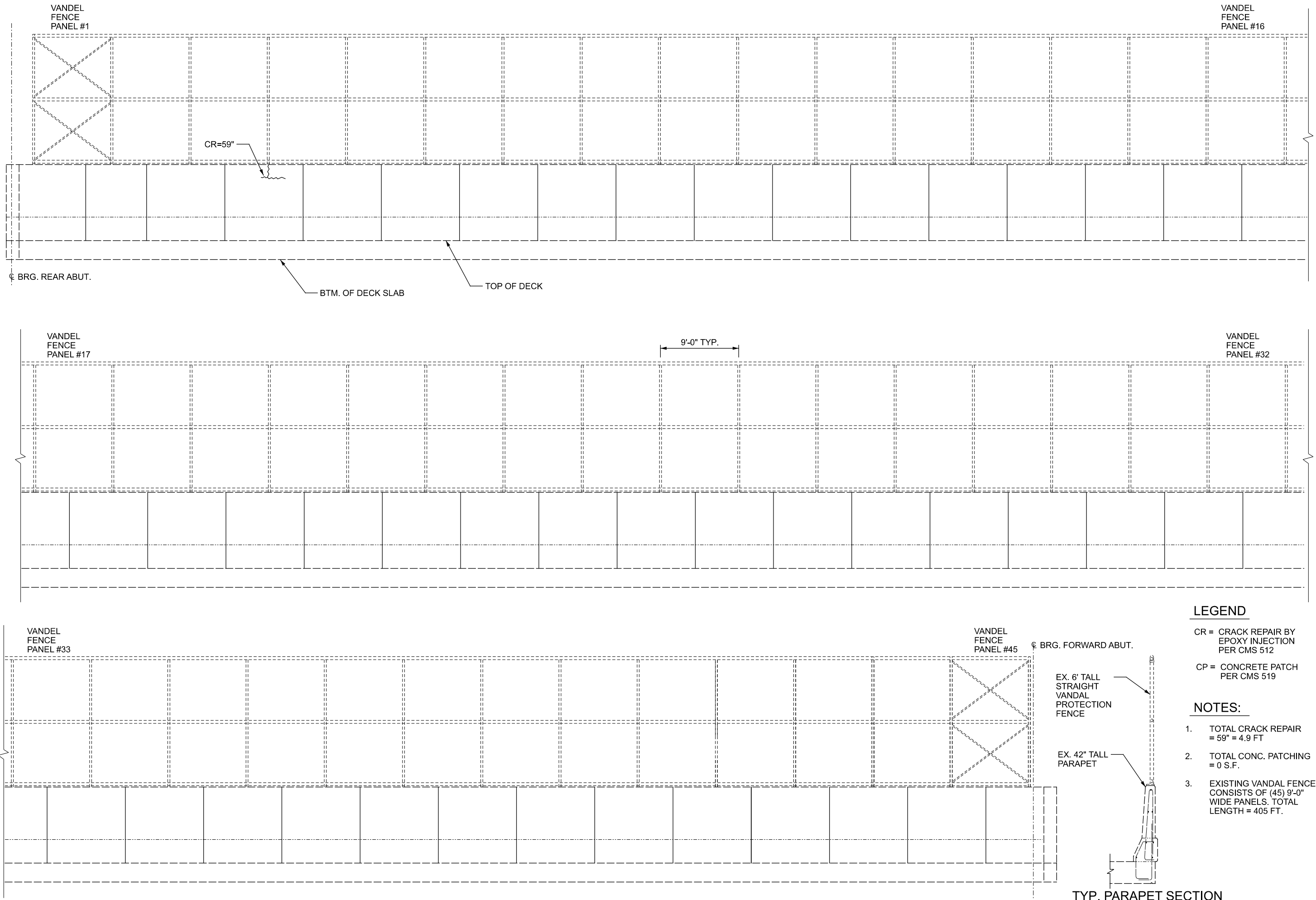
DESIGNER CHECKER
 CAH XXX

REVIEWER
 XXX MM-DD-YY

PROJECT ID
 112980

SUBSET	TOTAL
10	11

SHEET	TOTAL
33	42



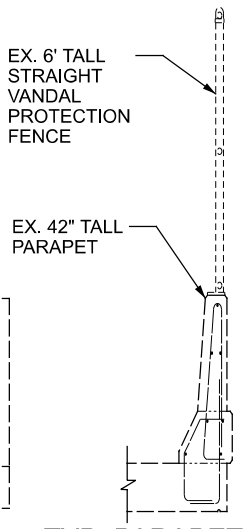
RIGHT PARAPET (INSIDE FACE) REPAIR DETAILS
 BRIDGE No.: CLI-73-0283
 SR 73 OVER I-71

LEGEND

- CR = CRACK REPAIR BY EPOXY INJECTION PER CMS 512
- CP = CONCRETE PATCH PER CMS 519

NOTES:

1. TOTAL CRACK REPAIR = 59" = 4.9 FT
2. TOTAL CONC. PATCHING = 0 S.F.
3. EXISTING VANDAL FENCE CONSISTS OF (45) 9'-0" WIDE PANELS. TOTAL LENGTH = 405 FT.



TYP. PARAPET SECTION

SFN		1402102
DESIGN AGENCY		
DESIGNER	CHECKER	
CAH	XXX	
REVIEWER		
XXX MM-DD-YY		
PROJECT ID		
112980		
SUBSET	TOTAL	
11	11	
SHEET		
TOTAL		
34	42	