

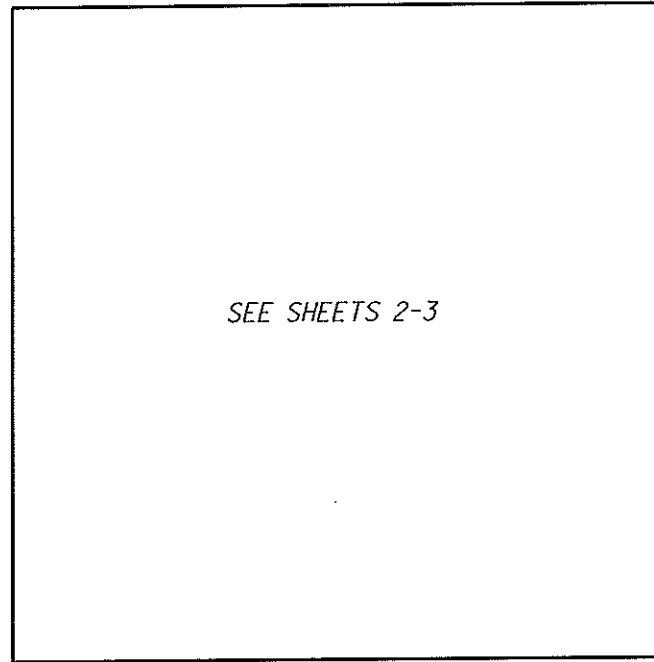
STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

D05-SP-FY2019

DISTRICT WIDE SPOT PAVING

PROJECT DESCRIPTION
DISTRICT WIDE SPOT PAVING AND BRIDGE REPAIR

PROJECT EARTH DISTURBED AREA = N/A (MAINT.)
ESTIMATED CONTRACTOR EARTH DISTURBED AREA = N/A (MAINT.)
NOTICE OF INTENT EARTH DISTURBED AREA = N/A (MAINT.)



SEE SHEETS 2-3

LOCATION MAP

LATITUDE: 39° 57' 06" LONGITUDE: 82° 24' 41"

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LOCALITY	PLAN SPLIT	COUNTY	ROUTE	BEGINNING SLM	ENDING SLM	LENGTH MILES	CITY/VILLAGE
SEE SHEET 2 FOR LOCATIONS							

2016 SPECIFICATIONS


THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

DESIGN DESIGNATION
SEE SHEET 2

DESIGN EXCEPTIONS
NONE

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

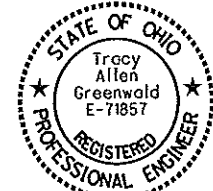


Call Before You Dig
1-800-362-2764

(Non-members must be called directly)
OIL & GAS PRODUCERS
UNDERGROUND PROTECTION SERVICE
1-800-925-0988

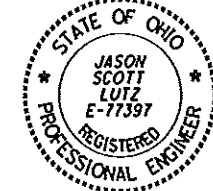
PLAN PREPARED BY:
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT 5 PLANNING & ENGINEERING

ENGINEERS SEAL
SHEETS 8-18



SIGNED: *Tracy Allen Greenwald*
DATE: 4/16/2018

ENGINEERS SEAL



SIGNED: *Jason Scott Lutz*
DATE: 4/16/2018

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	
MT-95.30	7/21/17					800	4/20/18
MT-98.11	1/20/17					821	4/20/12
MT-98.22	1/20/17					832	1/17/14
MT-98.29	1/20/17					921	4/20/12
						SPECIAL PROVISIONS	

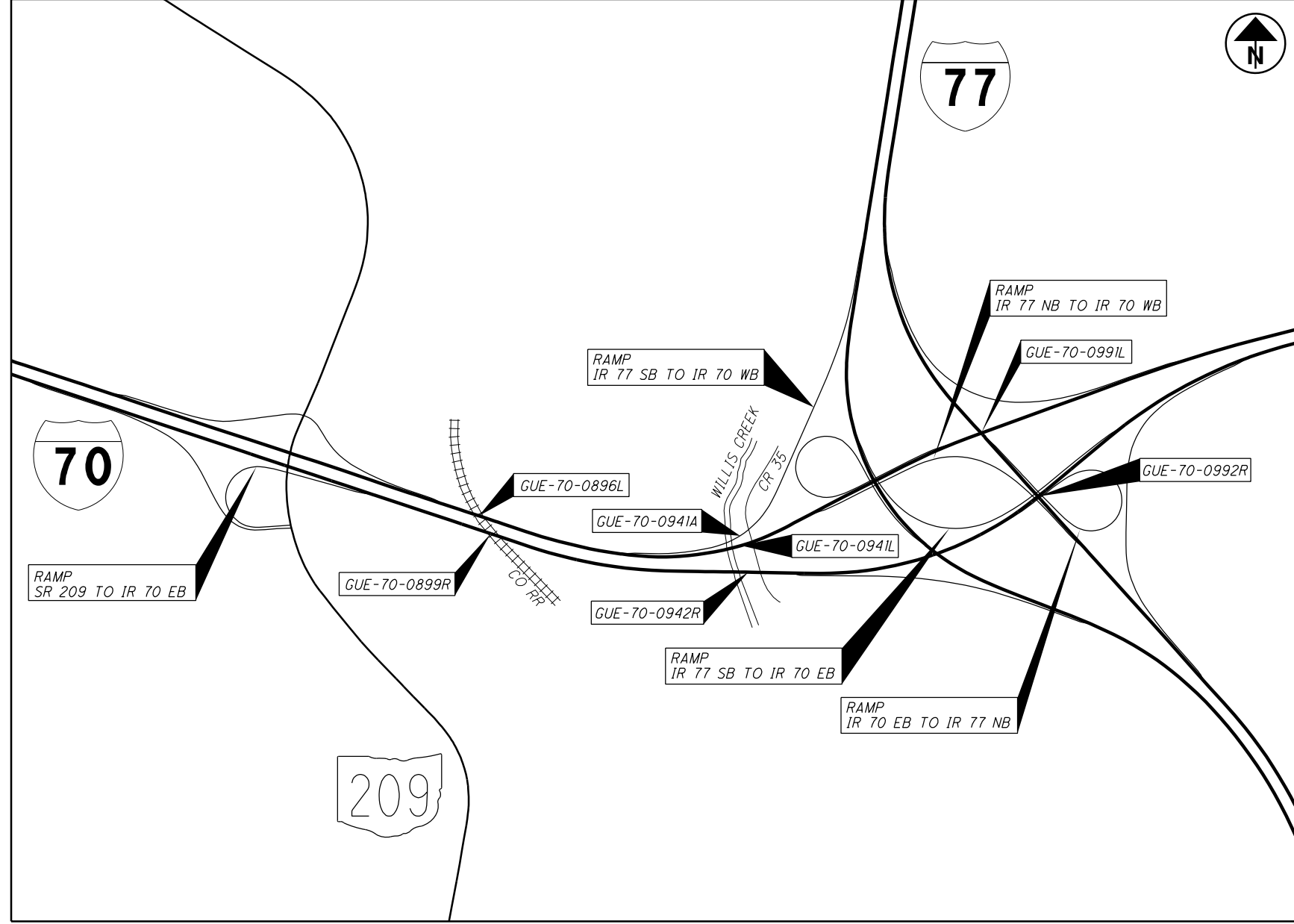
APPROVED: *Jan 2018*
DATE: 4/12/18 DISTRICT DEPUTY DIRECTOR

APPROVED: *Jan 2018*
DATE: 5-2-18 DIRECTOR DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. E170(465)
PID NO. 98343
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
D05-SP-FY2019
1/19

D05 - Spot Paving FY2019
180415 PID - 98343
Dist 5 7/12/2018
Contract Proposal Available @
www.contracts.dot.state.oh.us/home

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UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

NOTIFICATION OF ROAD CLOSURE OR RESTRICTION

THE CONTRACTOR WILL ADVISE THE PROJECT ENGINEER A MINIMUM OF TWENTY ONE (21) DAYS PRIOR TO THE FOLLOWING: THE START OF CONSTRUCTION ACTIVITIES, LANE RESTRICTIONS, LANE CLOSURES, AND OR ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THIS INFORMATION TO THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICER (PIO) BY FAX AT (614) 887-4510 OR EMAIL AT D05.PIO@DOT.STATE.OH.US

DISTRICT PERMIT SECTION BY FAX AT (614) 887-4525 OR EMAIL AT BRIAN.BOSCH@DOT.STATE.OH.US

CENTRAL OFFICE SPECIAL HAUL PERMITS SECTION BY FAX AT (614) 728-4099 OR EMAIL AT HAULING.PERMITS@DOT.STATE.OH.US

THE PIO WILL, IN TURN, NOTIFY THE PUBLIC, THE LOCAL EMERGENCY SERVICES, AFFECTED SCHOOLS AND BUSINESSES, AND ANY OTHER IMPACTED LOCAL PUBLIC AGENCY OF ANY OF THE ABOVE MENTIONED ITEMS, VIA MEDIA SOURCES.

AGGREGATE SHOULDER WORK

THE CONTRACTOR SHALL NOTIFY LANCE ZIMMERMAN AT (740) 404-7804, AT LEAST ONE WEEK PRIOR TO BEGINNING WORK AT EACH LOCATION.

AS THE CONTRACTOR FINISHES PAVEMENT REPAIRS, ANY WORK REQUIRED TO RESTORE THE AGGREGATE SHOULDERS WILL BE COMPLETED BY ODOT COUNTY FORCES. LANCE ZIMMERMAN WILL COORDINATE THE WORK TO REPAIR THE AGGREGATE SHOULDERS WITH EACH ODOT COUNTY MANAGER.

THE WORK BY ODOT FORCES MAY BE CONSTRUCTED CONCURRENTLY WITH THIS PROJECT. IT IS IMPERATIVE THAT THE ODOT COUNTY FORCES AND THE CONTRACTOR COOPERATE FULLY WITH EACH OTHER AS OUTLINED IN SECTION 105.08 OF THE CMS. ALL MAINTENANCE OF TRAFFIC SHALL BE COORDINATED BETWEEN BOTH PARTIES SO THAT THERE IS NO CONFLICTS WITH ONE ANOTHER OR THE WORK THEY ARE PERFORMING.

ITEM 251, PARTIAL DEPTH REPAIR, MISC.: CENTER LINE JOINT REPAIR

AN ESTIMATED QUANTITY FOR PARTIAL DEPTH PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED ALONG THE CENTER LINE JOINT ON S.R. 16 IN MUSKINGUM COUNTY BETWEEN SLM 11.57 AND SLM 14.86.

THE ROADWAY SHALL BE PLANED **1.5" IN DEPTH AND 2' WIDE** CENTERED ABOUT THE CENTER LINE CONSTRUCTION JOINT. AFTER THE 1.5" PLANING, THE FACE OF THE REPAIR SHALL BE COATED WITH ITEM 407, NON-TRACKING TACK COAT. THE CONTRACTOR SHALL THEN PLACE AND COMPACT 1.5" OF ITEM 441 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M TO BE FLUSH WITH EXISTING ROADWAY SURFACE.

THE PERIMETER OF ALL REPAIR AREAS SHALL BE SEALED IN ACCORDANCE WITH CMS 401.17.

ALL PLANING, MATERIALS, LABOR, EQUIPMENT, TOOLS, TRAFFIC CONTROL AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 251 PARTIAL DEPTH REPAIR, MISC.: CENTER LINE JOINT REPAIR.

ITEM 251, PARTIAL DEPTH REPAIR, MISC.: SURFACE EDGE REPAIR

AN ESTIMATED QUANTITY FOR PARTIAL DEPTH PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED FOR SURFACE EDGE CRACKING ON U.S. 22 AND S.R. 37 IN PERRY COUNTY AT THE LOCATIONS SHOWN IN THE PLANS, AS DIRECTED BY THE ENGINEER.

THE ROADWAY SHALL BE PLANED **1.25" IN DEPTH AND MINIMUM 5' WIDE.** AFTER THE 1.25" PLANING, THE FACE OF THE REPAIR SHALL BE COATED WITH ITEM 407, NON-TRACKING TACK COAT. THE CONTRACTOR SHALL THEN PLACE AND COMPACT 1.25" OF ITEM 441 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M TO BE FLUSH WITH EXISTING ROADWAY SURFACE. **REPAIRS SHALL BE COMPLETED IN CONJUNCTION WITH ITEM 253, PAVEMENT REPAIRS.**

THE PERIMETER OF ALL REPAIR AREAS SHALL BE SEALED IN ACCORDANCE WITH CMS 401.17.

ALL PLANING, MATERIALS, LABOR, EQUIPMENT, TOOLS, TRAFFIC CONTROL AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 251 PARTIAL DEPTH REPAIR, MISC.: SURFACE EDGE CRACKING REPAIR.

ITEM 253, PAVEMENT REPAIR

PAVEMENT REPAIR LOCATIONS AND QUANTITIES HAVE BEEN INCLUDED IN THE PLAN TO BE USED, "AS DIRECTED BY THE ENGINEER". THE INTENT OF THIS OPERATION IS TO REPAIR THOSE AREAS OF PAVEMENT WHICH HAVE COMPLETELY FAILED (PUMPING OF SUB-BASE MATERIAL) AND NOT TO CORRECT SURFACE IRREGULARITIES.

DEPTH OF EXCAVATION SHALL BE **7.25"**. THE MINIMUM WIDTH SHALL BE **5 FT** IN ORDER TO PROVIDE ENOUGH ROOM FOR PROPER COMPACTION EQUIPMENT. AFTER EXCAVATION HAS BEEN COMPLETED, THE FACE OF THE REPAIR SHALL BE COATED WITH ITEM 407, NON-TRACKING TACK COAT. REPLACEMENT MATERIAL WILL BE 6" OF ITEM 301, ASPHALT CONCRETE BASE, PG64-22 FOLLOWED BY 1.25" OF ITEM 441, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M. **SURFACE COURSE SHALL BE PLACED ON ALL REPAIR AREAS THE SAME DAY UNLESS DIRECTED OTHERWISE BY THE ENGINEER.**

THE PERIMETER OF ALL REPAIR AREAS SHALL BE SEALED IN ACCORDANCE WITH CMS 401.17.

REPAIR QUANTITIES MAY BE USED ON THE MAINLINE PAVEMENT OR ON PAVED SHOULDERS. ALL EXCAVATION, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 253, PAVEMENT REPAIR.

ITEM 621, RAISED PAVEMENT MARKER REMOVED

RPM REMOVAL SHALL NOT OCCUR SOONER THAN 10 DAYS PRIOR TO RESURFACING OF THE ROADWAY. ALL RPM'S REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR.

CALCULATED
PTB
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GENERAL NOTES

D05 - SP - FY 2019

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ITEM 614, MAINTAINING TRAFFIC

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES ON **MUS-16, PER-22, AND PER-37**. ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

WORK ON **MUS-16** SHALL BE PERFORMED USING 42" WEIGHTED CHANNELIZERS TO PROVIDE ADDITIONAL LANE WIDTH. TRAFFIC WILL NEED SHIFTED ONTO THE EXISTING SHOULDERS. NO TEMPORARY MARKINGS SHALL BE USED. ALL CHANNELIZING OF TRAFFIC SHALL BE COMPLETED USING THE WEIGHTED CHANNELIZERS. THE EXISTING EDGE LINES SHALL NOT BE DISTURBED.

A MINIMUM OF TWO LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES ON **I.R. 70**, EXCLUDING THE CLOSURE TIMES STATED IN THE LANE VALUE CONTRACT TABLE BELOW.

LANE VALUE CONTRACT TABLE

LOCATION	CRITICAL WORK: TIME WHEN ONE (1) LANE MAY BE CLOSED	TIME UNIT	DISINCENTIVE (\$ PER TIME UNIT)
I.R 70 EB & WB SLM 8.80-9.90	ODOT PERMITTED LANE CLOSURE TIMES WEBSITE: HTTP://PLCM.DOT.STATE.OH.US	15 MIN.	\$2,500

LANE CLOSURES ON **I.R. 70** SHALL BE PERFORMED USING DRUMS PER SCD **MT-95.30**. TRAFFIC WILL NEED SHIFTED ONTO THE EXISTING SHOULDERS. NO TEMPORARY MARKINGS SHALL BE USED. ALL CHANNELIZING OF TRAFFIC SHALL BE COMPLETED USING DRUMS. THE EXISTING EDGE LINES AND RUMBLE STRIPS SHALL NOT BE DISTURBED. IT IS RECOMMENDED THAT TRUCK MOUNTED ATTENUATORS BE USED IN ALL REPAIR LOCATIONS ON **I.R. 70** TO PROTECT WORKERS FROM TRAFFIC.

RAMP CLOSURES ON **I.R. 70** SHALL BE PERFORMED USING DRUMS PER SCD **MT-98.11, MT-98.22, AND MT-98.29** ONLY DURING THE PERMITTED LANE CLOSURE TIMES ABOVE WHEN PERFORMING WORK ON THE FOLLOWING BRIDGES:

- GUE-70-0899R, GUE-70-0942R (CLOSE SR 209 TO IR 70 EB RAMP, COINCIDE DRIVING LANE AND PASSING LANE REPAIRS)
- GUE-70-0896L DRIVING LANE, GUE-70-0941L DRIVING LANE, GUE-70-0941A (CLOSE IR 77 SB TO IR 70 WB RAMP, COINCIDE DRIVING LANE/RAMP REPAIRS)
- GUE-70-0896L PASSING LANE, GUE-70-0941L PASSING LANE (CLOSE IR 77 NB TO IR 70 WB RAMP, COINCIDE PASSING LANE REPAIRS)
- GUE-70-0992R (CLOSE IR 70 EB TO IR 77 NB RAMP, CLOSE IR 77 SB TO IR 70 EB RAMP)

RAMP CLOSURES SHALL NOT OCCUR CONCURRENTLY. RAMP DETOURS SHALL BE COMPLETED THROUGH THE USE OF PORTABLE CHANGEABLE MESSAGE SIGNS (USE S.R. 209 AS THE DETOUR ROUTE).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT, IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES ON **I.R. 70** SHALL BE OPENED TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

MEMORIAL DAY, FOURTH OF JULY, LABOR DAY, THANKSGIVING, CHRISTMAS, NEW YEARS, EASTER

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00AM FRIDAY
THURSDAY (THANKSGIVING)	12:00N WEDNESDAY THROUGH 6:00AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE OF \$75 FOR EACH MINUTE THE ABOVE DESCRIBED LANE AND/OR RAMP CLOSURE RESTRICTIONS ARE VIOLATED.

NOTIFICATION OF ROAD CLOSURE OR RESTRICTIONS

THE CONTRACTOR WILL ADVISE THE PROJECT ENGINEER A MINIMUM OF TWENTY ONE (21) DAYS PRIOR TO THE FOLLOWING: THE START OF CONSTRUCTION ACTIVITIES, LANE RESTRICTIONS, LANE CLOSURES, AND OR ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THIS INFORMATION TO THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICER (PIO) BY FAX AT (614) 887-4510 OR EMAIL AT D05.PIO@DOT.STATE.OH.US

DISTRICT PERMIT SECTION BY FAX AT (614) 887-4525 OR EMAIL AT BRIAN.BOSCH@DOT.STATE.OH.US

CENTRAL OFFICE SPECIAL HAUL PERMITS SECTION BY FAX AT (614) 728-4099 OR EMAIL AT HAULING.PERMITS@DOT.STATE.OH.US

THE PIO WILL, IN TURN, NOTIFY THE PUBLIC, THE LOCAL EMERGENCY SERVICES, AFFECTED SCHOOLS AND BUSINESSES, AND ANY OTHER IMPACTED LOCAL PUBLIC AGENCY OF ANY OF THE ABOVE MENTIONED ITEMS, VIA MEDIA SOURCES.

NOTIFICATION OF RAMP CLOSURE

THE CONTRACTOR SHALL PROVIDE NOTICE OF CLOSURE TO ALL RAMP TRAFFIC AT LEAST 7 CALENDAR DAYS IN ADVANCE THROUGH THE USE OF PORTABLE CHANGEABLE MESSAGE SIGNS. THE SIGNS MAY BE ERECTED ANYWHERE ON THE RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

DROP-OFFS IN WORK ZONES

DROP-OFFS THAT DEVELOP DURING CONSTRUCTION OPERATIONS AND THAT ARE NOT OTHERWISE PROVIDED FOR IN THE PLANS SHALL BE TREATED AS SHOWN ON STANDARD DRAWING MT-101.90. WHERE THE PLANS DO NOT PROVIDE SPECIFIC ITEMS FOR LABOR, EQUIPMENT, OR MATERIALS TO IMPLEMENT THE DROP-OFF TREATMENTS SPECIFIED, THEY SHALL BE INCLUDED FOR PAYMENT IN THE **LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC**.

ITEM 614, WORK ZONE MARKING SIGNS

IN ACCORDANCE WITH CMS SECTION 614.04, THE QUANTITIES OF WORK ZONE MARKING SIGN TO BE USED AS DIRECTED BY THE ENGINEER:

R4-1 (DO NOT PASS): LOCATION 3: 8 EACH
 R4-2 (PASS WITH CARE): LOCATION 3: 8 EACH

IN ADDITION, THE CONTRACTOR SHALL ERECT A "GROOVED PAVEMENT" SIGN 250 FEET IN ADVANCE OF ANY SECTION OF ROADWAY WHERE TRAFFIC MUST TRAVEL ON A PLANED SURFACE. "GROOVED PAVEMENT" SIGNS SHALL BE INCLUDED FOR PAYMENT WITH THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC AS PER CMS SECTION 614.055.

ITEM 614, WORK ZONE MARKING SIGN LOCATION 2: 16 EACH

ITEM 614, WORK ZONE PAVEMENT MARKINGS

THE CONTRACTOR SHALL PLACE ALL WORK ZONE PAVEMENT MARKINGS IN ACCORDANCE WITH **CMS 614.11** AND STANDARD DRAWING **MT-99.20** UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE QUANTITIES BELOW ARE FOR PLACEMENT OF TEMPORARY MARKINGS ON THE SURFACE COURSE.

ITEM 614, WORK ZONE CENTER LINE, CLASS II, 642 PAINT LOCATION 2: 3.29 MILE

ITEM 614, WORK ZONE CENTER LINE, CLASS III, 642 PAINT LOCATION 2: 3.29 MILE

**ITEM 614, WORK ZONE EDGE LINE, CLASS III, 4", 642 PAINT LOCATION 3: 4.00 MILE
 LOCATION 4: 2.00 MILE**

COOPERATION BETWEEN CONTRACTORS

THE STATE OF OHIO HAS CONTRACTED PROJECTS **D05-PM-FY2018 (A) PID 98196, D05-PM-FY2018 (B) PID 98197** WHICH MAY BE CONSTRUCTED CURRENTLY WITH THIS PROJECT. IT IS IMPERATIVE THAT THE CONTRACTORS COOPERATE FULLY WITH EACH OTHER AS OUTLINED IN SECTION 105.08 OF THE CMS MANUAL. ALL MAINTENANCE OF TRAFFIC SHALL BE COORDINATED BETWEEN PROJECTS AND NOT CONFLICT WITH ONE ANOTHER. **FINAL PAVEMENT MARKINGS SHALL BE PERFORMED ON THE ABOVE PROJECTS AFTER PAVEMENT REPAIRS HAVE BEEN COMPLETED.**

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHT TIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIAL AND INCIDENTALS TO PERFORM THIS WORK SHALL BE INCLUDED IN THE **LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC**.

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MAINTENANCE OF TRAFFIC NOTES

D05 - SP - FY 2019

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED IN THIS NOTE WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS, **AS DIRECTED BY THE ENGINEER:**

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE.

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

**ITEM 614 LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE
LOCATION 1: 150 HOUR**

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FEET AND 475 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 2 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN (CONT'D.)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

A TOTAL OF **10 PCMS** WILL BE REQUIRED FOR THIS PROJECT.
10 SIGNS X 1 SNMT = 10 SNMT (I.R. 70 BRIDGE REPAIRS)

**ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
LOCATION 1: 10 SNMT**

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S)

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE BEEN APPROVED BY THE MOT EXCEPTION COMMITTEE (MOTEC) OR THE PROJECT IMPACT ADVISORY COUNCIL (PIAC) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND THE STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTION(S) INCLUDE:

- CLOSURE OF I.R. 77 SOUTHBOUND TO I.R. 70 WESTBOUND SYSTEM RAMP
- CLOSURE OF I.R. 77 SOUTHBOUND TO I.R. 70 EASTBOUND SYSTEM RAMP
- CLOSURE OF I.R. 77 NORTHBOUND TO I.R. 70 WESTBOUND SYSTEM RAMP
- CLOSURE OF I.R. 70 EASTBOUND TO I.R. 77 NORTHBOUND SYSTEM RAMP

THE ABOVE SYSTEM RAMPS SHALL NOT BE CLOSED CONCURRENTLY

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF **30 CALENDAR DAYS** PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE DISTRICT WORK ZONE TRAFFIC MANAGER AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED **4/12/2018** FOR **PID 98343** IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE APPLICABLE ODOT CENTRAL OFFICE COMMITTEE (MOTEC OR PIAC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE APPLICABLE ODOT CENTRAL OFFICE COMMITTEE. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

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PAVEMENT REPAIR DATA																
LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	ESTIMATED REPAIR LENGTH	MINIMUM PROPOSED REPAIR WIDTH	REPAIR AREA	251		251		253		621		COMMENTS
								THICKNESS	PARTIAL DEPTH REPAIR, MISC.: CENTER LINE JOINT REPAIR	THICKNESS	PARTIAL DEPTH REPAIR, MISC.: EDGE LINE SURFACE REPAIR	THICKNESS	PAVEMENT REPAIR	RPM (YELLOW/YELLOW 80' SPACING)	RAISED PAVEMENT MARKER REMOVED	
					FEET	FEET	SQ. YD.	INCHES	CU. YD.	INCHES	CU. YD.	INCHES	CU. YD.	EACH	EACH	
1	GUE	I.R. 70	8.80	9.90												SEE SHEETS 8-18
LOCATION 1 TOTALS (CARRIED TO GENERAL SUMMARY)																
2	MUS	S.R. 16	11.57	14.86	17,371	2	3,860.2	1.75	187.6					218	218	
LOCATION 2 TOTALS (CARRIED TO GENERAL SUMMARY)																
									187.6					218	218	
3	PER	U.S. 22 E.B.	0.00	14.27	3,350	5	1,861.1			1.25	64.6					
3	PER	U.S. 22 E.B.	0.00	14.27	7,000	5	3,888.9					7.25	783.2			
3	PER	U.S. 22 W.B.	0.00	14.27	3,150	5	1,750.0			1.25	60.8					
3	PER	U.S. 22 W.B.	0.00	14.27	4,450	5	2,472.2					7.25	497.9			
LOCATION 3 TOTALS (CARRIED TO GENERAL SUMMARY)																
											125.4		1281.1			
4	PER	S.R. 37 E.B.	0.00	8.26	3,950	5	2,194.4			1.25	76.2					
4	PER	S.R. 37 E.B.	0.00	8.26	1,550	5	861.1					7.25	173.4			
4	PER	S.R. 37 W.B.	0.00	8.26	1,450	5	805.6			1.25	28.0					
4	PER	S.R. 37 W.B.	0.00	8.26	2,600	5	1,444.4					7.25	290.9			
LOCATION 4 TOTALS (CARRIED TO GENERAL SUMMARY)																
											104.2		464.3			

CALCULATED
PTB
CHECKED
JSL

PAVEMENT REPAIR DATA

D05 - SP - FY 2019

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SHEET NUM.										PART.		ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
											03/IMS/BR		EXT	TOTAL			
STRUCTURE REPAIR (GUE-70-8.96L or SFN 3001148)																	
										0.5		202	11301	0.5	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	11
										0.5		511	53012	0.5	CY	CLASS OC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE	11
										7		516	31011	7	FT	2" DEEP JOINT SEALER, AS PER PLAN	11
STRUCTURE REPAIR (GUE-70-8.99R or SFN 3001172)																	
										1		202	11301	1	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	11
										1		511	53012	1	CY	CLASS OC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE	11
										22		516	31011	22	FT	2" DEEP JOINT SEALER, AS PER PLAN	11
STRUCTURE REPAIR (GUE-70-9.41A or SFN 3001202)																	
										0.5		202	11301	0.5	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	11
										0.5		511	53012	0.5	CY	CLASS OC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE	11
										9		516	31011	9	FT	2" DEEP JOINT SEALER, AS PER PLAN	11
STRUCTURE REPAIR (GUE-70-9.41L or SFN 3001237)																	
										0.5		202	11301	0.5	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	11
										110		202	23500	110	SY	WEARING COURSE REMOVED	
										5		441	50100	5	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5" THICK)	
										0.5		511	53012	0.5	CY	CLASS OC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE	11
										43		516	31011	43	FT	2" DEEP JOINT SEALER, AS PER PLAN	11
STRUCTURE REPAIR (GUE-70-9.42R or SFN 3001261)																	
										1		202	11301	1	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	11
										93		202	23500	93	SY	WEARING COURSE REMOVED	
										5		253	02000	5	CY	PAVEMENT REPAIR	
										4		441	50100	4	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5" THICK)	
										1		511	53012	1	CY	CLASS OC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE	11
										79		516	31011	79	FT	2" DEEP JOINT SEALER, AS PER PLAN	11
STRUCTURE REPAIR (GUE-70-9.91L or SFN 3001350)																	
										2		251	98000	2	CY	PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT	11
STRUCTURE REPAIR (GUE-70-9.92R or SFN 3001385)																	
										3		202	11301	3	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	11
										225		202	23500	225	SY	WEARING COURSE REMOVED	
										9		441	50100	9	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5" THICK)	
										3		511	53012	3	CY	CLASS OC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE	11
										104		516	31011	104	FT	2" DEEP JOINT SEALER, AS PER PLAN	11

DESIGN AGENCY
OHIO DEPARTMENT OF
TRANSPORTATION DISTRICT 5

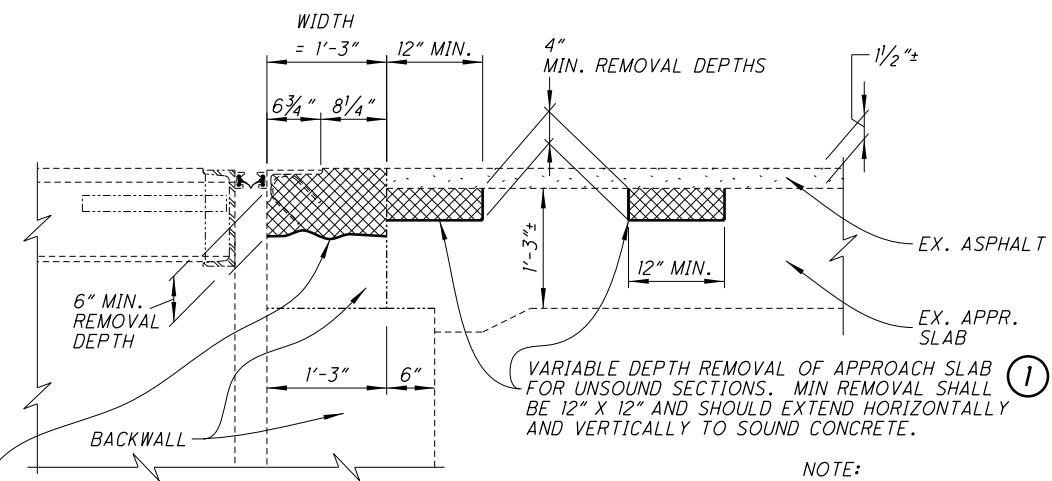
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4-12-18
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STRUCTURE FILE NUMBER

DRAWN TAG
REVISED
DESIGNED TAG
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JDR

GENERAL SUMMARY

D05-SP-FY2019
PID No. 98343

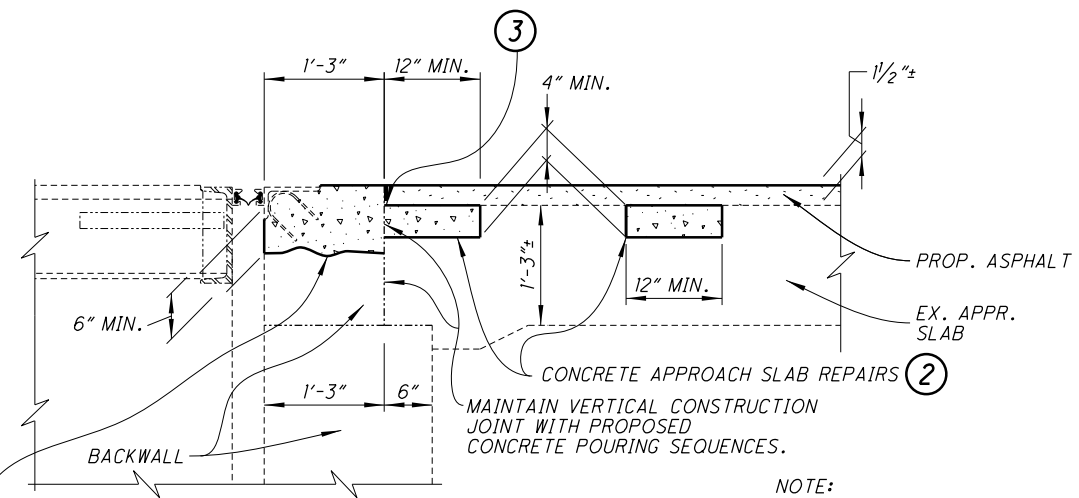
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BR. NO. GUE-70-0941L (FWD. A.S.)
 BR. NO. GUE-70-0942R (FWD. A.S.)
 BR. NO. GUE-70-0992R (FWD. A.S.)
 (PROPOSED TYP. ABUTMENT SECTION IN LANES)

VARIABLE DEPTH REMOVAL OF BACKWALL FOR UNSOUND SECTIONS OF BACKWALL. MIN REMOVAL SHALL BE NOT LESS THAN 12" IN LENGTH X FULL BACKWALL WIDTH. (FACE/FACE PARAPETS). EXTEND DOWN TO SOUND CONCRETE. ①

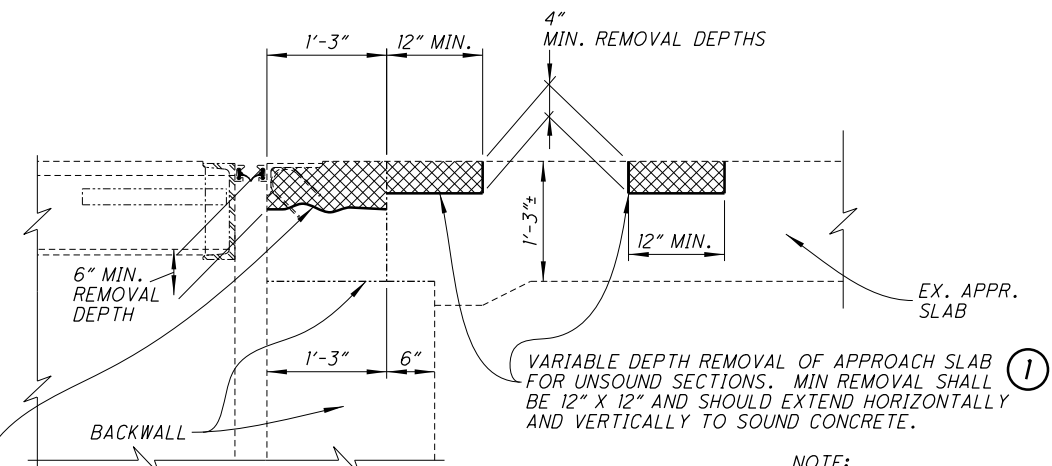
NOTE:
 ALL RESTEEL AND JOINT ARMORS SHALL BE PRESERVED IN PLACE.
 SUPPORT JOINT ARMOR AS NECESSARY.



BR. NO. GUE-70-0943L/R (FWD. A.S.)
 BR. NO. GUE-70-0994R (FWD. A.S.)
 (PROPOSED TYP. ABUTMENT SECTION IN LANES)

CONCRETE BACKWALL REPAIRS ②

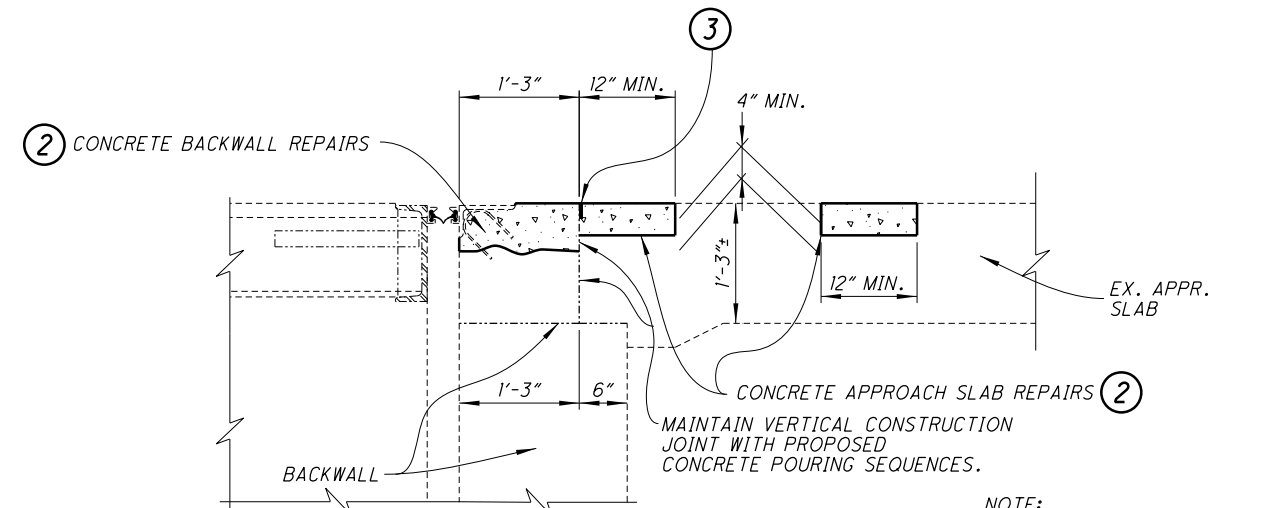
NOTE:
 ALL RESTEEL AND JOINT ARMORS SHALL BE PRESERVED IN PLACE.
 SUPPORT JOINT ARMOR AS NECESSARY.



BR. NO. GUE-70-0896L
 BR. NO. GUE-70-0899R
 BR. NO. GUE-70-0941A
 BR. NO. GUE-70-0941L (REAR A.S.)
 BR. NO. GUE-70-0941R (REAR A.S.)
 BR. NO. GUE-70-0992R (REAR A.S.)
 (PROPOSED TYP. ABUTMENT SECTION IN LANES)

VARIABLE DEPTH REMOVAL OF BACKWALL FOR UNSOUND SECTIONS OF BACKWALL. MIN REMOVAL SHALL BE NOT LESS THAN 12" IN LENGTH X FULL BACKWALL WIDTH. (FACE/FACE PARAPETS). EXTEND DOWN TO SOUND CONCRETE. ①

NOTE:
 ALL RESTEEL AND JOINT ARMORS SHALL BE PRESERVED IN PLACE.
 SUPPORT JOINT ARMOR AS NECESSARY.

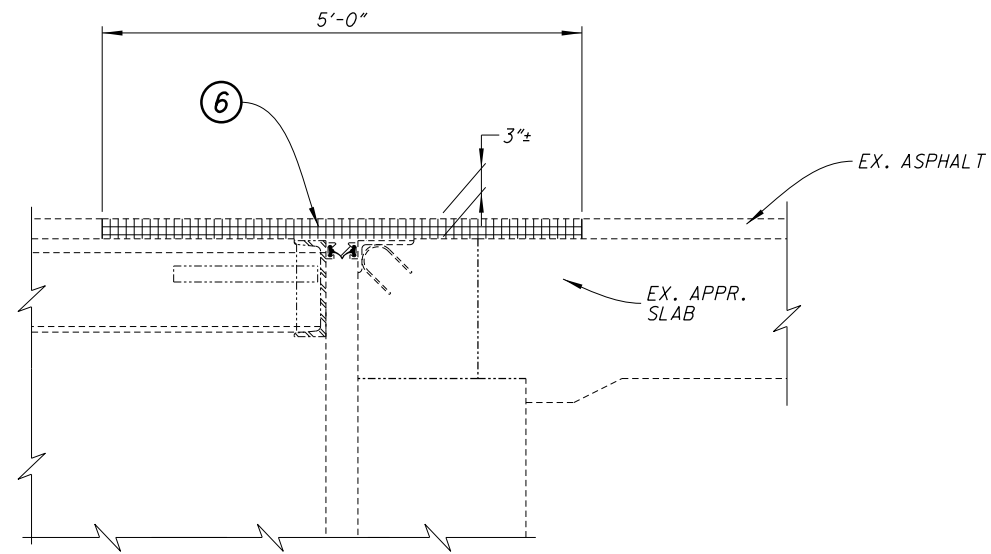


BR. NO. GUE-70-0896L
 BR. NO. GUE-70-0899R
 BR. NO. GUE-70-0941A
 BR. NO. GUE-70-0941L (REAR A.S.)
 BR. NO. GUE-70-0941R (REAR A.S.)
 BR. NO. GUE-70-0992R (REAR A.S.)
 (PROPOSED TYP. ABUTMENT SECTION IN LANES)

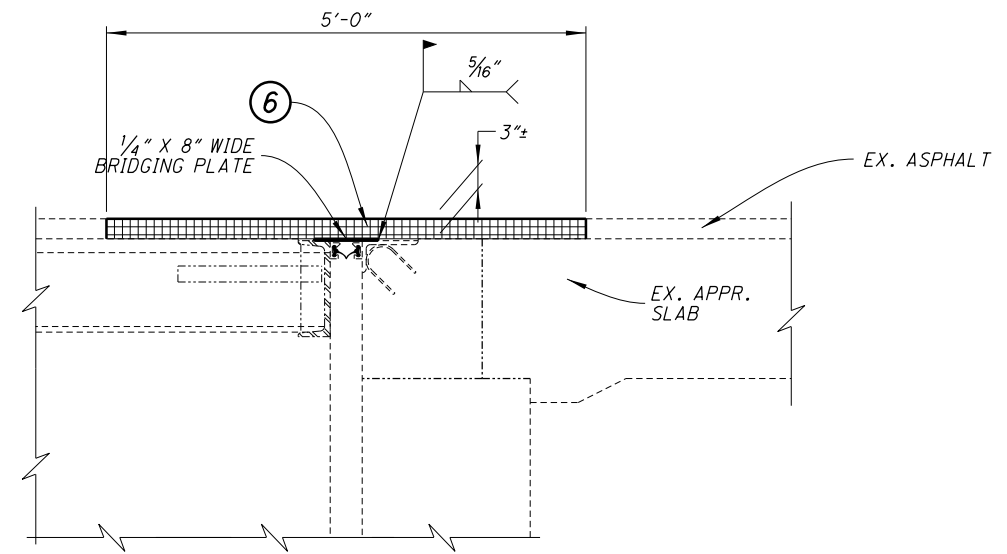
NOTE:
 ALL RESTEEL AND JOINT ARMORS SHALL BE PRESERVED IN PLACE.
 SUPPORT JOINT ARMOR AS NECESSARY.

- ① - [Cross-hatched pattern] ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ② - [Dotted pattern] ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③ - [Solid black] ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④ - [Horizontal lines] ITEM 202 - WEARING COURSE REMOVED
- ⑤ - [Vertical lines] ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥ - [Grid pattern] ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦ - [Diagonal lines] ITEM 253 - PAVEMENT REPAIR

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BR. NO. GUE-70-0991L
(EXISTING TYP. ABUTMENT SECTION IN LANES)



BR. NO. GUE-70-0991L
(EXISTING TYP. ABUTMENT SECTION IN LANES)

- ① - [Cross-hatch pattern] ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ② - [Dotted pattern] ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③ - [Solid black] ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④ - [Dotted pattern] ITEM 202 - WEARING COURSE REMOVED
- ⑤ - [Dotted pattern] ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥ - [Grid pattern] ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦ - [Diagonal lines] ITEM 253 - PAVEMENT REPAIR

DESIGN AGENCY
OHIO DEPARTMENT OF
TRANSPORTATION DISTRICT 5

REVIEWED DATE
TAG 4-12-18
STRUCTURE FILE NUMBER

DRAWN TAG
TAG REVISED

DESIGNED TAG
TAG CHECKED JDR

BRIDGE DETAILS

D05-SP-FY2019
PID No. 98343

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① ITEM 202 - PORTION OF STRUCTURE REMOVED, AS PER PLAN

UPON REMOVAL OF THE EXISTING ASPHALT, THE FIELD ENGINEER WILL SOUND THE ENTIRE ABUTMENT BACKWALL AND APPROACH SLAB SURFACE TO DETERMINE, AND INDICATE TO THE CONTRACTOR, THE PORTIONS OF STRUCTURE TO BE REMOVED AND REPLACED.

ALL CONCRETE REMOVED SHALL BE REMOVED BY MEANS OF APPROVED PNEUMATIC HAMMERS EMPLOYING POINTED AND BLUNT CHISEL TOOLS. HYDRAULIC HOE-RAM TYPE HAMMERS WILL NOT BE PERMITTED. THE WEIGHT OF THE HAMMER SHALL NOT BE MORE THAN 35 POUNDS FOR REMOVAL WITHIN 18 INCHES OF PORTIONS TO BE PRESERVED. OUTSIDE THE 18 INCH LIMIT, THE CONTRACTOR MAY USE HAMMERS NOT EXCEEDING 90 POUNDS UPON THE APPROVAL OF THE ENGINEER. DO NOT PLACE PNEUMATIC HAMMERS IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. FOLLOW C.M.S. SECTION 519 TO PROPERLY EXTEND THE LIMITS OF REMOVAL DIRECTED BY THE ENGINEER OR SHOWN IN THIS PLAN AND FOR PREPARING THE REMOVED AREAS FOR THE PLACEMENT OF ITEM #2 SHOWN ON THIS SHEET.

② ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE

TO EXPEDITE WORK, CLASS QC2 CONCRETE WITH AN ACCELERATING ADMIXTURE SIKA RAPID-1 OR ANY APPROVED EQUIVALENT ADMIXTURE SHALL BE USED TO ACHIEVE 3,000 PSI COMPRESSIVE STRENGTH IN 12 HRS. USE A NON-CHLORIDE ACCELERATING ADMIXTURE AND PROVIDE DOCUMENTATION THAT THE MIX WILL PROVIDE THE STRENGTH IN THE SPECIFIED TIME.

THIS ITEM SHALL CONFORM TO CMS 511 WITH THE FOLLOWING CONDITIONS AND REVISIONS:

AT LEAST 5 DAYS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR APPROVAL A SCHEDULE OF REPAIR WORK ITEMS TO BE COMPLETED. THE SCHEDULE SHALL INCLUDE A BREAKDOWN OF ALL MAJOR WORK ACTIVITIES ON AN HOURLY BASIS. REPAIR WORK SHALL NOT BEGIN UNTIL THE SCHEDULE IS APPROVED BY THE ENGINEER.

THE CONTRACTOR SHALL CONTINUE THE WET CURE FOR THE MAXIMUM NUMBER OF HOURS POSSIBLE DURING THE PERMITTED LANE CLOSURE. THE CLOCK STARTS FOR THE WET CURE WHEN THE CONCRETE PLACEMENT IS COMPLETE.

TRAFFIC WILL NOT BE PERMITTED ON THE FINISHED CONCRETE SURFACE UNTIL AFTER COMPLETION OF A 12 HOUR MINIMUM WET CURE AND AFTER TWO TEST BEAMS HAVE ATTAINED AN AVERAGE MODULUS OF RUPTURE OF 400 PSI.

PAYMENT FOR ALL OF THE ABOVE DESCRIBED LABOR, EQUIPMENT, AND MATERIALS WILL BE MADE AT THE CONTRACT PRICE BID FOR ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE

③ ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN

SAWCUT FINAL PROPOSED ASPHALT 1" WIDE X 2" DEEP AND SEAL WITH HOT APPLIED JOINT SEALER AS PER 705.04 AS DIRECTED BY THE ENGINEER. (SEE ROADWAY QUANTITIES FOR PAYMENT OF THIS ITEM.)


⑥ ITEM 251, PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT

AN ESTIMATED QUANTITY FOR PARTIAL DEPTH PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED ALONG THE CENTER LINE OF THE EXPANSION JOINT.








THE ROADWAY SHALL BE PLANED 3" +/- DEEP AND 5 FEET WIDE CENTERED ABOUT THE CENTER LINE OF THE EXPANSION JOINT. AFTER PLANING THE ROADWAY SURFACE PLACE A 1/4" THICK BY 8" WIDE STEEL PLATE THE ENTIRE LENGTH OF THE EXPANSION JOINT. THE STEEL PLATE SHALL BE WELDED TO THE EXISTING END DAM ON THE ABUTMENT. PRIOR TO PLACING AND COMPACTION OF 3.0" OF ITEM 441 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M, THE FACE OF THE REPAIR SHALL BE COATED WITH ITEM 407, NON-TRACKING TACK COAT ALONG WITH THE TOP OF THE STEEL BRIDGING PLATE.

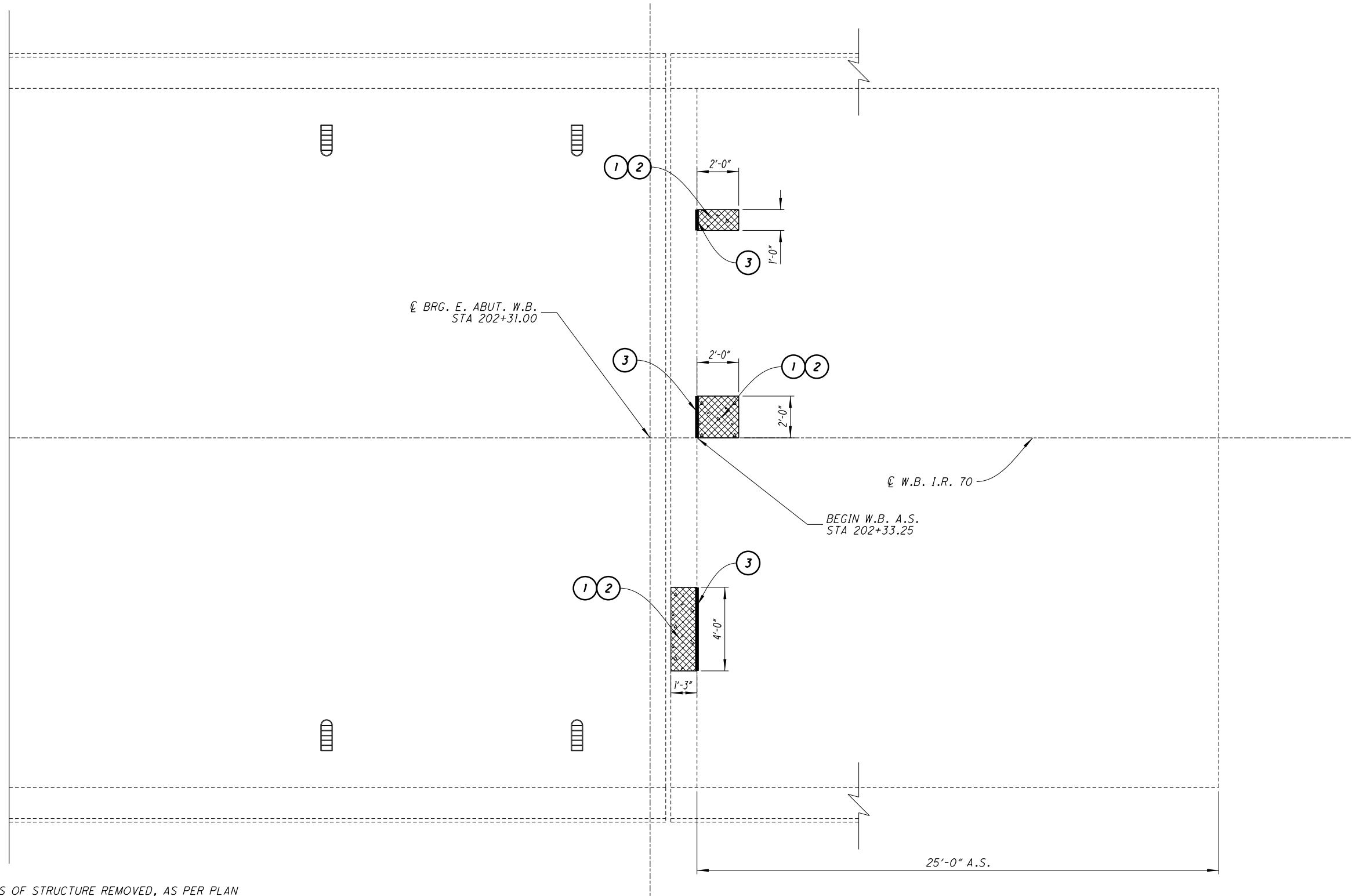
THE PERIMETER OF ALL REPAIR AREAS SHALL BE SEALED IN ACCORDANCE WITH CMS 401.17.

ALL PLANING, MATERIALS, LABOR, EQUIPMENT, TOOLS, TRAFFIC CONTROL AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 251 PARTIAL DEPTH REPAIR, MISC.: CENTER LINE JOINT REPAIR.

 11 19	D05-SP-FY2019 PID No. 98343	BRIDGE NOTES	DESIGNED TAG CHECKED JDR	DRAIN TAG REVISED	REVIEWED TAG STRUCTURE FILE NUMBER	DATE 4-12-18	DESIGN AGENCY OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 5

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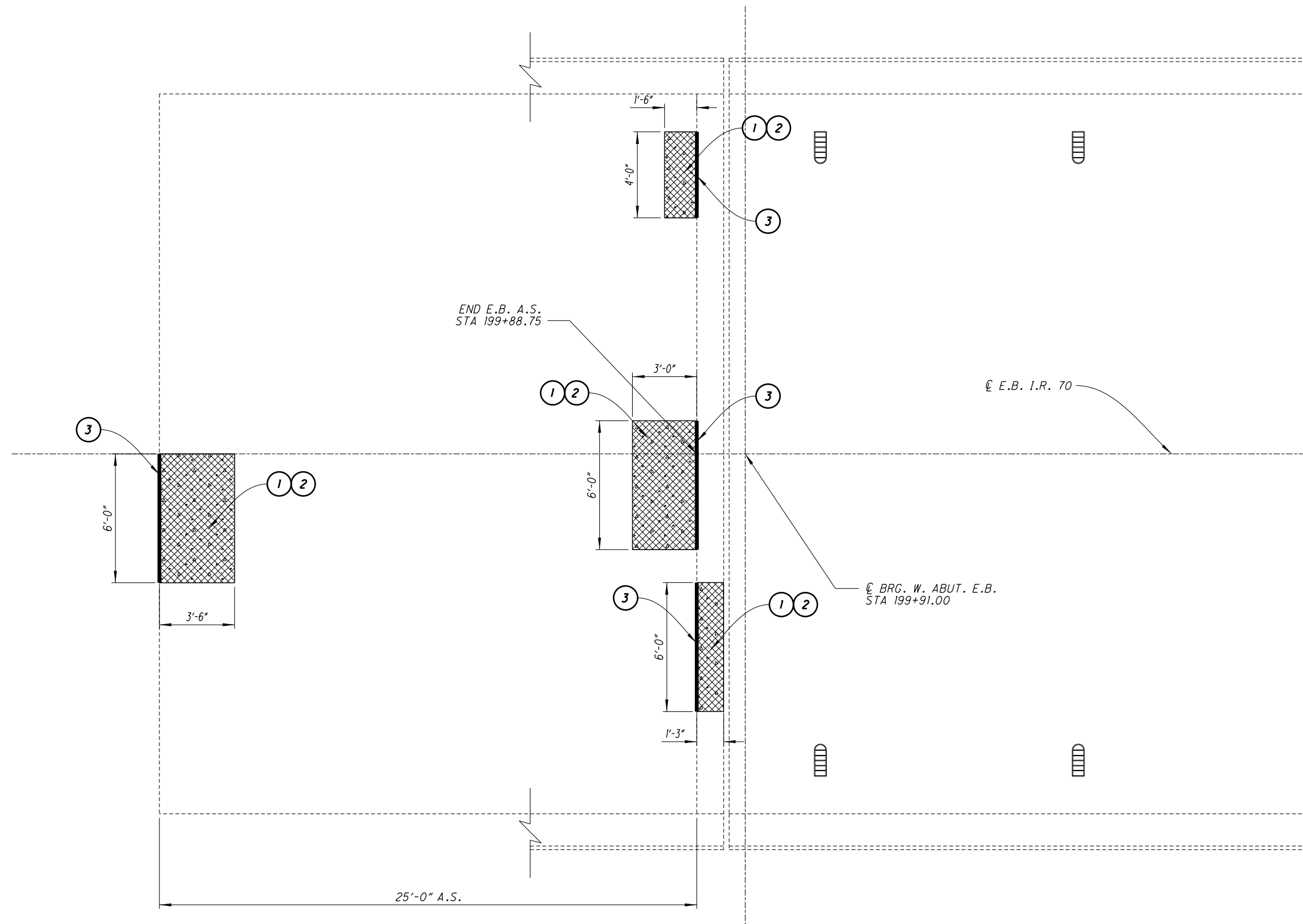
- ①  ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ②  ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③  ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④  ITEM 202 - WEARING COURSE REMOVED
- ⑤  ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥  ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦  ITEM 253 - PAVEMENT REPAIR



FORWARD APPROACH SLAB

	DESIGN AGENCY OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 5	DATE 4-12-18	REVIEWED TAG 3001148	STRUCTURE FILE NUMBER 3001148		
DESIGNED TAG JDR	CHECKED TAG JDR	DRAIN TAG REVISED	TAG REVISED			
GENERAL PLAN BRIDGE NO.: GUE-70-0896L I.R. 70 OVER PENNA RAILROAD						
D05-SP-FY2019 PID No. 98343						
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12						
19						

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REAR APPROACH SLAB

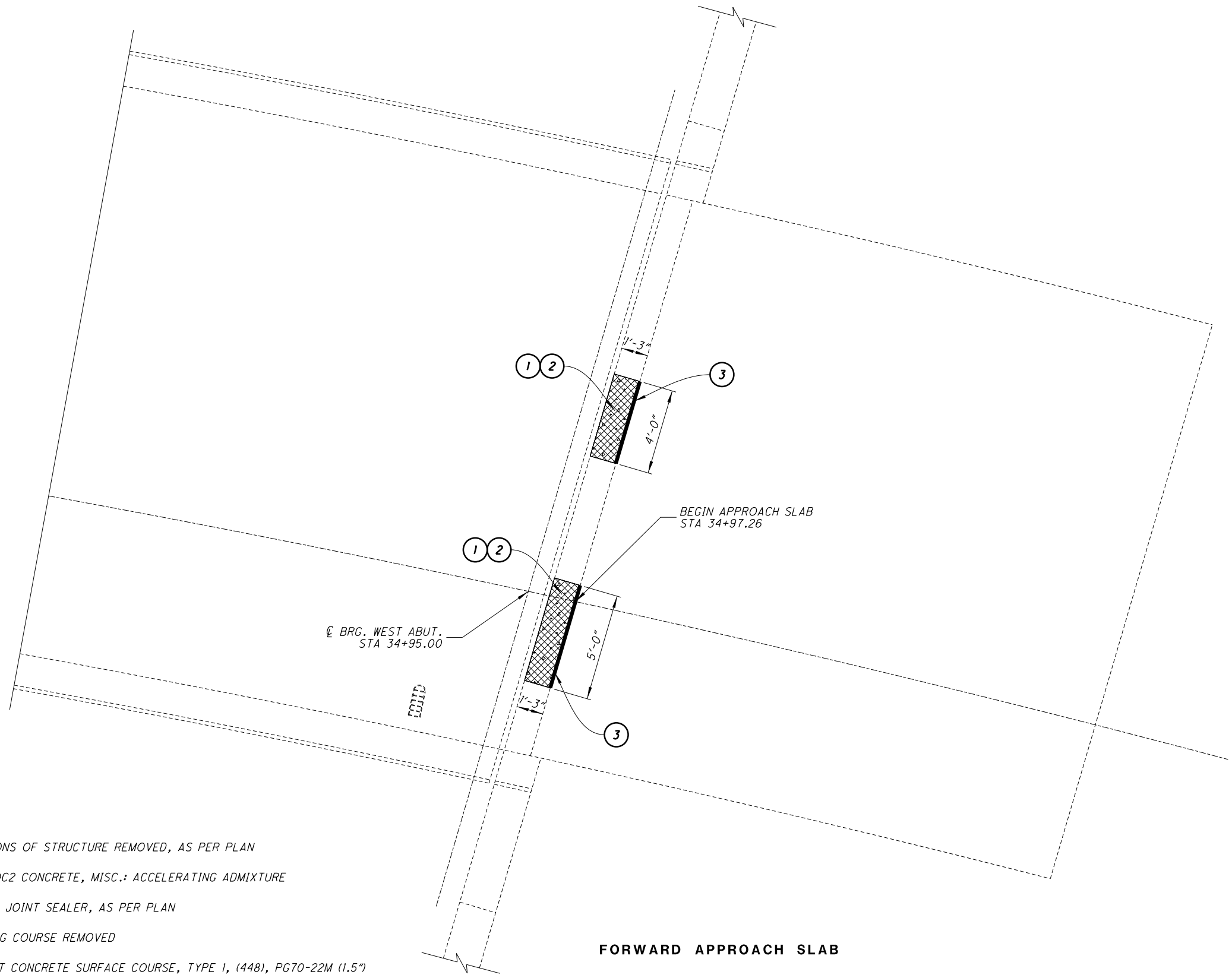
- ① ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ② ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③ ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④ ITEM 202 - WEARING COURSE REMOVED
- ⑤ ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥ ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦ ITEM 253 - PAVEMENT REPAIR

GENERAL PLAN BRIDGE NO.: GUE-70-0899R I.R. 70 OVER PENNA RAILROAD	DESIGN AGENCY OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 5	DATE 4-12-18 STRUCTURE FILE NUMBER 3001172
DESIGNED TAG CHECKED JDR	DRAWN TAG REVISED	REVIEWED TAG STRUCTURE FILE NUMBER 3001172
D05-SP-FY2019 PID No. 98343		

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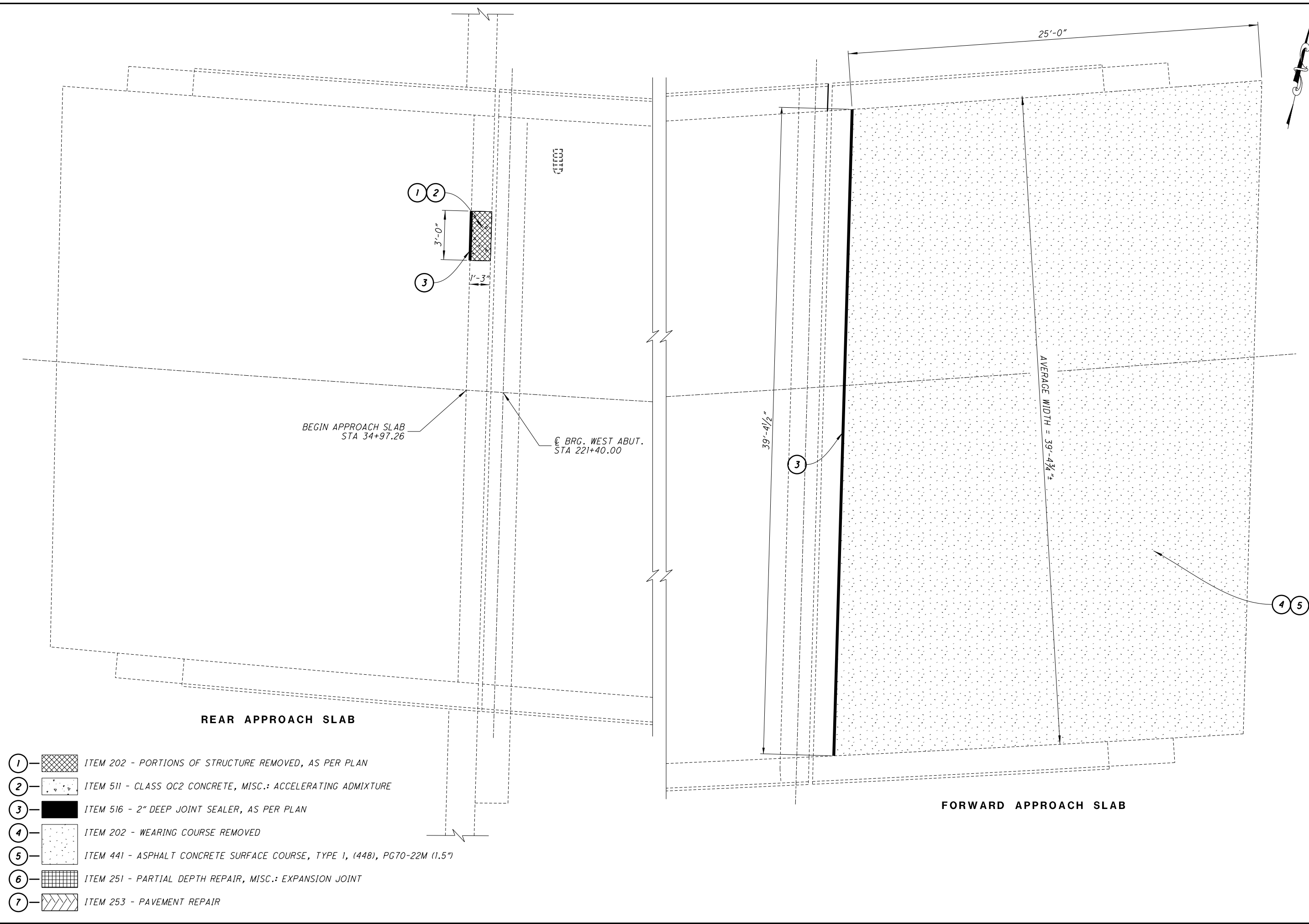
- ① ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ② ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③ ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④ ITEM 202 - WEARING COURSE REMOVED
- ⑤ ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥ ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦ ITEM 253 - PAVEMENT REPAIR



FORWARD APPROACH SLAB

	DESIGN AGENCY OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 5		
DESIGNED TAG CHECKED JDR	REVIEWED TAG STRUCTURE FILE NUMBER 3001202		
DRAIN TAG REVISED	DATE 4-12-18		
GENERAL PLAN BRIDGE NO. GUE-70-0941A (RAMP) I.R. 70 OVER WILLS CREEK & C.R. 35			
D05-SP-FY2019 PID No. 98343	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%; text-align: center;">14</td> <td style="width: 50%; text-align: center;">19</td> </tr> </table>	14	19
14	19		

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- ① ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ② ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③ ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④ ITEM 202 - WEARING COURSE REMOVED
- ⑤ ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥ ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦ ITEM 253 - PAVEMENT REPAIR








	DESIGN AGENCY OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 5	DATE 4-12-18	REVIEWED TAG 3001237	DRAIN TAG REVISED	DESIGNED TAG CHECKED JDR
GENERAL PLAN BRIDGE NO. GUE-70-0941L I.R. TO OVER WILLS CREEK & T.R. 435					
D05-SP-FY2019 PID No. 98343					

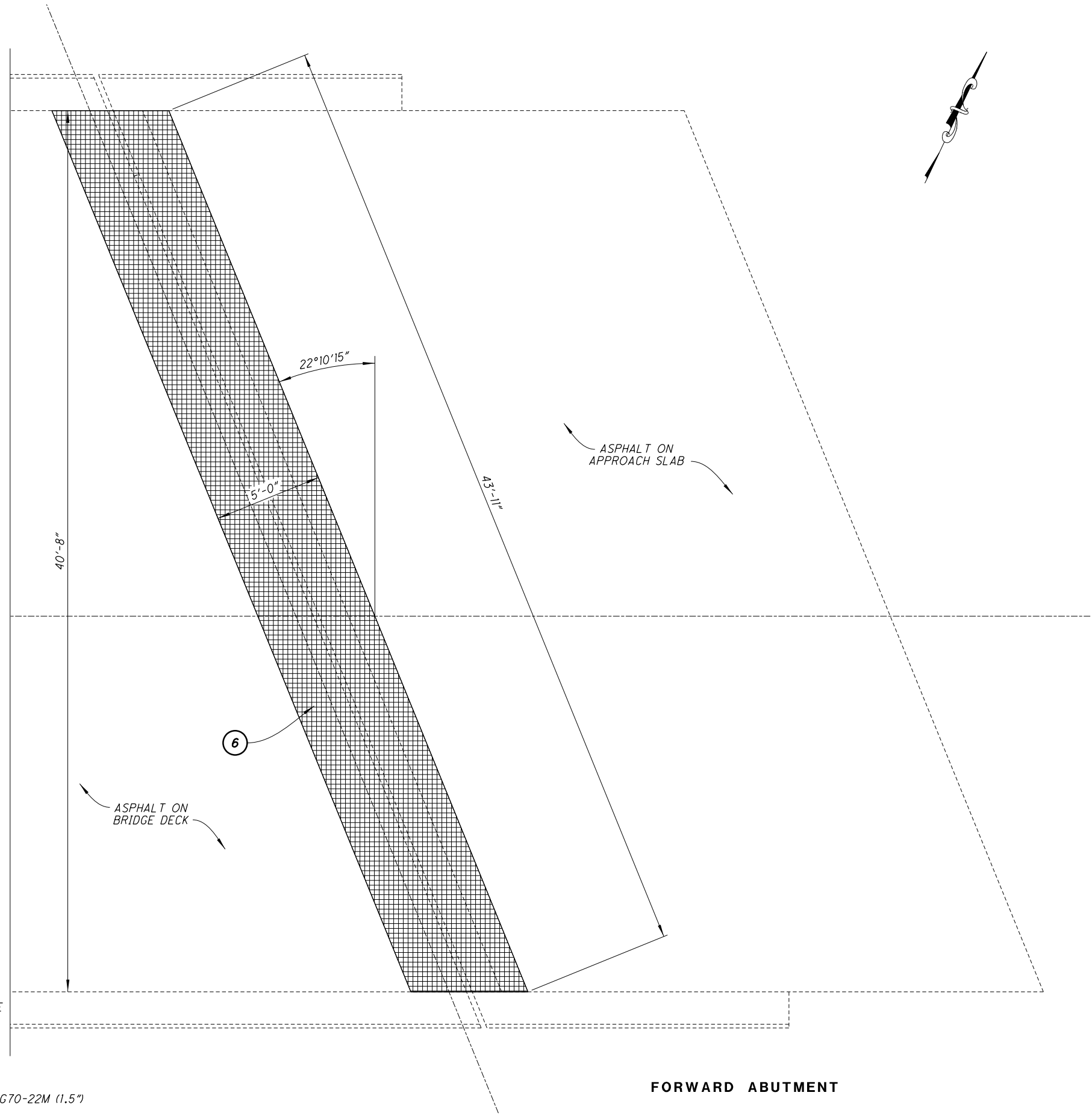
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


- ① ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ② ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③ ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④ ITEM 202 - WEARING COURSE REMOVED
- ⑤ ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥ ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦ ITEM 253 - PAVEMENT REPAIR

	DESIGN AGENCY OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 5
REVIEWED TAG STRUCTURE FILE NUMBER 3001261	DATE 4-12-18
DRAIN TAG REVISED	CHECKED JDR
GENERAL PLAN BRIDGE NO. GUE-70-0942R I.R. TO OVER WILLS CREEK & T.R. 435	
D05-SP-FY2019 PID No. 98343	16 19

- ①  ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
- ②  ITEM 511 - CLASS QC2 CONCRETE, MISC.: ACCELERATING ADMIXTURE
- ③  ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN
- ④  ITEM 202 - WEARING COURSE REMOVED
- ⑤  ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1.5")
- ⑥  ITEM 251 - PARTIAL DEPTH REPAIR, MISC.: EXPANSION JOINT
- ⑦  ITEM 253 - PAVEMENT REPAIR



	GENERAL PLAN BRIDGE NO. GUE-70-0991L I.R. 70 OVER I.R. 77	DESIGNED TAG CHECKED JDR	DRAIN TAG REVISED	REVIEWED TAG STRUCTURE FILE NUMBER 3001350	DESIGN AGENCY OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 5
D05-SP-FY2019 PID No. 98343					

