

STATE OF OHIO  
 DEPARTMENT OF TRANSPORTATION  
**CRA-96-3.65**  
**RIC-96-0.00**  
 SANDUSKY TWP.  
 VERNON TWP.  
 SHARON TWP.  
 CRAWFORD COUNTY  
 RICHLAND COUNTY

PROJECT DESCRIPTION

THIS PROJECT IS 8.91 MILES LONG AND WILL INCLUDE PAVEMENT PLANING, PAVEMENT REPAIRS, RESURFACING WITH ASPHALT CONCRETE AND PLACEMENT OF PAVEMENT MARKINGS.

PROJECT EARTH DISTURBED AREA: .N/A ACRES  
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA: .N/A ACRES  
 NOTICE OF INTENT EARTH DISTURBED AREA: .N/A ACRES

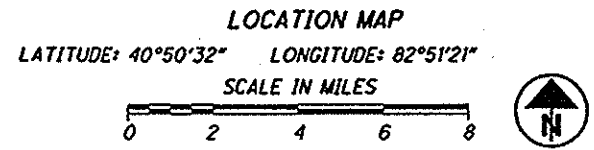
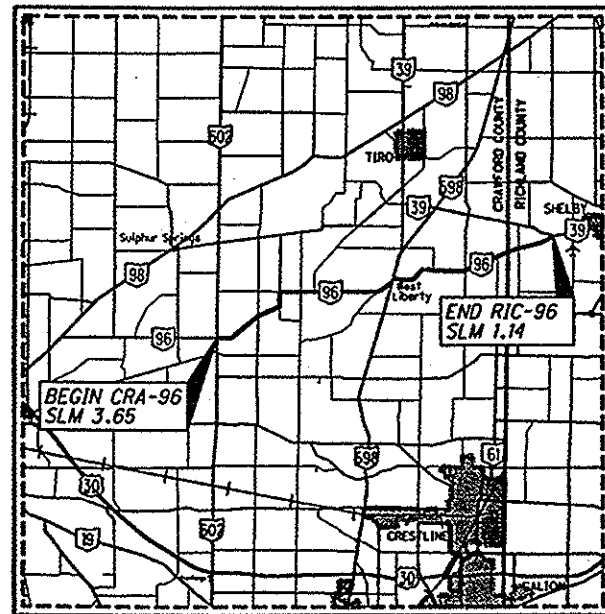
2008 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED *J. Hart, P.E.*  
 DATE 2/17/10 DISTRICT DEPUTY DIRECTOR

APPROVED *John M. Maltonia, Jr.*  
 DATE 2-24-10 DIRECTOR, DEPARTMENT OF TRANSPORTATION



PORTION TO BE IMPROVED: —————  
 UNDIVIDED STATE & FEDERAL ROUTES: —————  
 OTHER ROADS: —————

DESIGN DESIGNATION  
 SEE SHEET NO. 3.

DESIGN EXCEPTIONS  
 3R PROJECT

INDEX OF SHEETS:

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- PAVEMENT MARKINGS & RPM INFORMATION 9

CRA - SR-96-3.65;RIC-96-0.00  
 100251 PID - 83414  
 Dist 3 5/6/2010

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ROADWAY ENGINEERS SEAL:

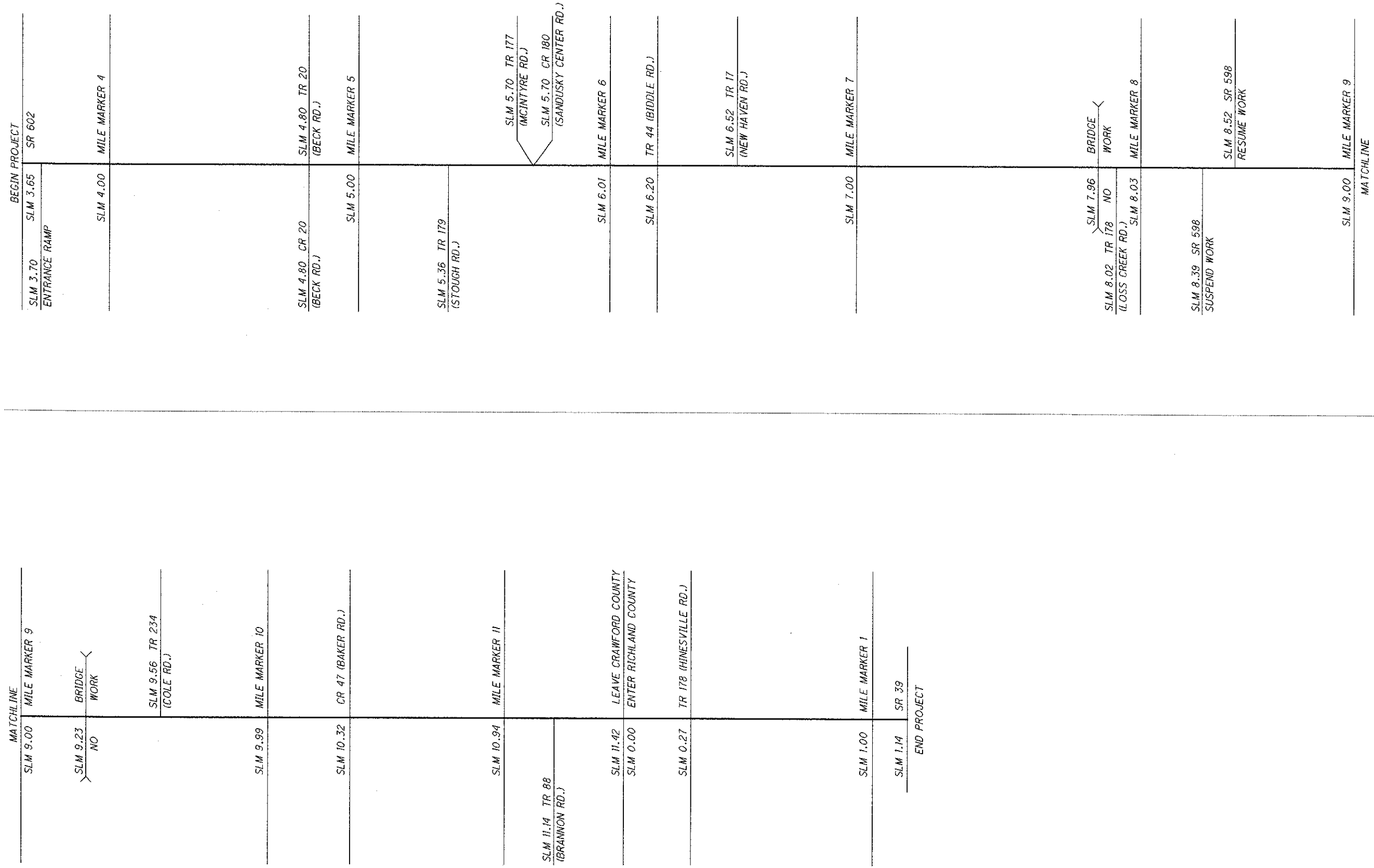
SIGNED: *Ahad D. Baraty*  
 DATE: 2/17/10

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	
BP-3.1	10/19/07	MT-97.10	4/17/09	TC-41.20	1/19/01	800	1/15/10
BP-4.1	7/16/04	MT-97.12	4/17/09	TC-42.20	7/16/04	832	5/5/09
		MT-99.20	1/16/09	TC-52.10	1/19/07		
RM-1.1	7/18/08	MT-101.90	1/16/09	TC-52.20	1/19/07		
		MT-105.10	1/16/09	TC-65.10	1/21/05		
DM-4.3	4/17/09			TC-65.11	1/21/05		
DM-4.4	4/17/09			TC-71.10	1/15/10		
				TC-73.10	1/19/01		

UNDERGROUND UTILITIES  
 TWO WORKING DAYS  
**BEFORE YOU DIG**  
 CALL 1-800-362-2764 (TOLL FREE)  
 OHIO UTILITIES PROTECTION SERVICE  
 NON-MEMBERS  
 MUST BE CALLED DIRECTLY  
 OIL & GAS PRODUCERS PROTECTIVE  
 SERVICE CALL: 1-800-925-0988



FEDERAL PROJECT NO. E080954  
 PID NO. 83414  
 CONSTRUCTION PROJECT NO. \_\_\_\_\_  
 RAILROAD INVOLVEMENT NONE  
 CRA-96-3.65  
 RIC-96-0.00  
 1/9



CRA-96-3.65  
 RIC-96-0.00

STRAIGHT LINE DIAGRAM

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 WORKSTATION: claughre DATE: 2/12/2010

**CRA SR 96**  
**DESIGN DESIGNATION**

SLM 3.65-8.37  
 CURRENT ADT (2010) 1000  
 DESIGN YEAR ADT (2022) 1150  
 DESIGN HOURLY VOLUME (2022) 130  
 DIRECTIONAL DISTRIBUTION 53%  
 TRUCKS (24 HOUR B&C) 0.09  
 DESIGN SPEED/LEGAL SPEED 55 MPH

DESIGN FUNCTIONAL CLASSIFICATION:  
 RURAL MAJOR COLLECTOR  
 NHS PROJECT - NO

SLM 8.37-8.49  
 CURRENT ADT (2010) 2720  
 DESIGN YEAR ADT (2022) 3200  
 DESIGN HOURLY VOLUME (2022) 320  
 DIRECTIONAL DISTRIBUTION 55%  
 TRUCKS (24 HOUR B&C) 0.11  
 DESIGN SPEED/LEGAL SPEED 55 MPH

DESIGN FUNCTIONAL CLASSIFICATION:  
 RURAL MAJOR COLLECTOR  
 NHS PROJECT - NO

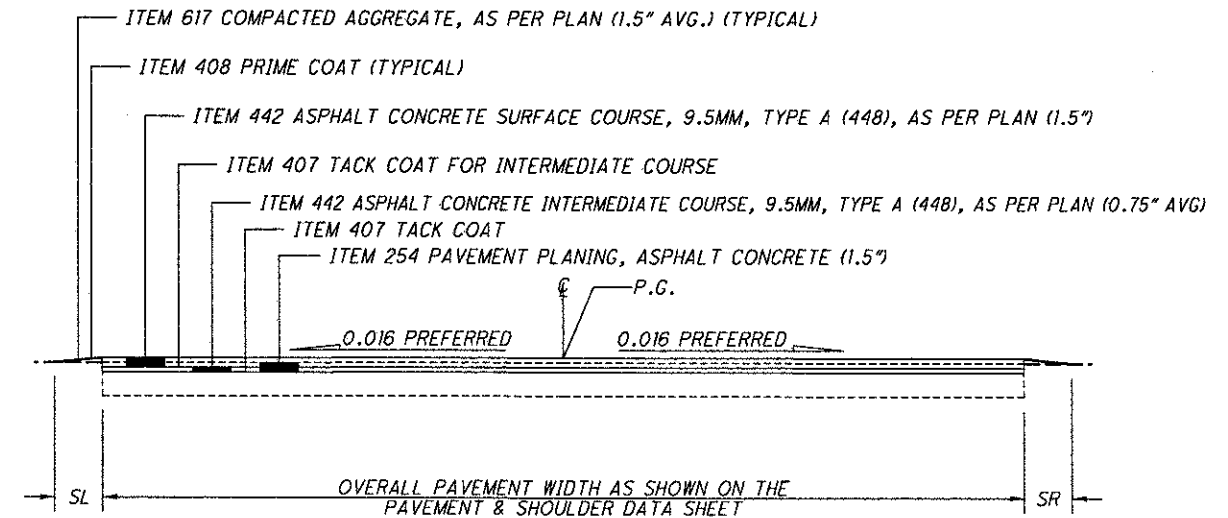
SLM 8.49-11.40  
 CURRENT ADT (2010) 1800  
 DESIGN YEAR ADT (2022) 2240  
 DESIGN HOURLY VOLUME (2022) 250  
 DIRECTIONAL DISTRIBUTION 51%  
 TRUCKS (24 HOUR B&C) .05  
 DESIGN SPEED/LEGAL SPEED 55 MPH

DESIGN FUNCTIONAL CLASSIFICATION:  
 RURAL MAJOR COLLECTOR  
 NHS PROJECT - NO

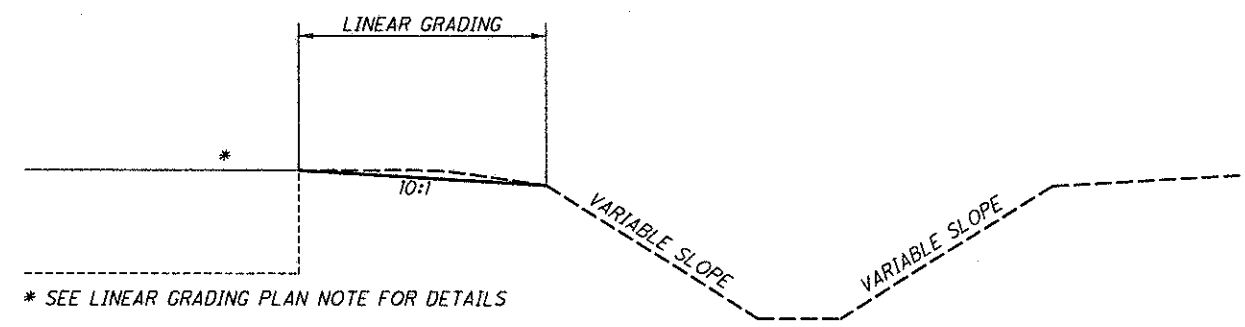
**RIC SR 96**  
**DESIGN DESIGNATION**

SLM 0.00-1.14  
 CURRENT ADT (2010) 2060  
 DESIGN YEAR ADT (2022) 2550  
 DESIGN HOURLY VOLUME (2022) 280  
 DIRECTIONAL DISTRIBUTION 60%  
 TRUCKS (24 HOUR B&C) 0.06  
 DESIGN SPEED/LEGAL SPEED 55 MPH

DESIGN FUNCTIONAL CLASSIFICATION:  
 RURAL MAJOR COLLECTOR  
 NHS PROJECT - NO



TYPICAL 1



LINEAR GRADING DETAIL

\* SEE LINEAR GRADING PLAN NOTE FOR DETAILS

CALCULATED CAL CHECKED SCJ
DESIGN DESIGNATION & TYPICAL SECTION
CRA-96-3.65 RIC-96-0.00

**GENERAL**

**UTILITIES**

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

CABLE T.V.  
TIME WARNER CABLE  
1575 LEXINGTON AVENUE  
MANSFIELD, OH 44901  
419-756-6091 ext 5136

GAS  
COLUMBIA GAS OF OHIO  
1120 W. 4TH ST.  
MANSFIELD, OH 44906  
419-528-1137

ELECTRIC  
AMERICAN ELECTRIC POWER  
2622 SOUTH S.R. 100  
TIFFIN, OH 44883  
419-443-4607

ELECTRIC  
NORTH-CENTRAL ELECTRIC CO-OP  
13978 EAST C.R. 56  
ATTICA, OH 44807  
419-426-3072

TELEPHONE  
CENTURYLINK  
175 ASHLAND RD.  
P.O. BOX 3555  
MANSFIELD, OH 44907  
419-755-7956

TELEPHONE  
VERIZON  
1534 S.R. 511 SOUTH  
ASHLAND, OH 44805  
419-282-6551

CRAWFORD COUNTY ENGINEERS  
815 WHETSTONE STREET  
BUCYRUS, OH 44820  
419-562-7731

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

**ROUTINE MAINTENANCE**

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

**WORK LIMITS**

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**COORDINATION OF WORK BETWEEN CONTRACTORS**

THE CONTRACTOR SHOULD BE AWARE THAT THERE MAY BE OTHER WORK BEING PERFORMED BY A SEPARATE CONTRACT. CRA-39-0.00 (PID 83410) AND CRA-598-9.09 (83415) ARE RESURFACING PROJECTS AND ARE SCHEDULED TO BEGIN WORK IN THE 2010 CONSTRUCTION SEASON. COORDINATION OF WORK IS THE RESPONSIBILITY OF THE CONTRACTOR.

**GENERAL**

**AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS**

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. THE CONTRACTOR IS ADVISED THAT NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 25 FEET. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, THE CONTRACTOR IS ADVISED THAT FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA) WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO SUBMIT FORM 7460-1 TO THE FAA. A COPY OF THE SUBMISSION AND TWO COPIES OF FORM 7460-1 SHALL BE FORWARDED TO THE ODOT OFFICE OF AVIATION. THE CONTRACTOR IS ADVISED THAT NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

THE CONTRACTOR IS FURTHER ADVISED THAT THE FAA APPROVAL MAY TAKE UP TO 45 DAYS. ALL SUBMISSIONS SHALL BE DIRECTED TO THESE OFFICES:

EXPRESS PROCESSING CENTER  
THE FEDERAL AVIATION ADMINISTRATION  
SOUTHWEST REGIONAL OFFICE  
OBSTRUCTION EVALUATION SERVICE, AJR-32  
2601 MEACHAM BLVD.  
FORT WORTH, TX 76137-0520

ODOT  
OFFICE OF AVIATION  
2829 WEST DUBLIN-GRANVILLE ROAD  
COLUMBUS, OHIO 43235  
(614) 793-5046

**ROADWAY**

**ITEM 209 - LINEAR GRADING**

THE CONTRACTOR IS REQUIRED TO PERFORM LINEAR GRADING ON THE GRADED SHOULDER. IT IS ANTICIPATED THAT THERE ARE AREAS WHERE THE GRADED SHOULDER IS AT A HIGHER ELEVATION THAN THE ADJACENT PROPOSED PAVEMENT. A 10:1 SLOPE SHALL BE ESTABLISHED, OR AS DIRECTED BY THE ENGINEER, WHEN PERFORMING ITEM 209 LINEAR GRADING. THE INTENT IS TO PROVIDE AN UNOBSTRUCTED AND POSITIVE FLOW OF STORM WATER FROM THE PAVEMENT TO THE DITCH. THE LINEAR GRADING SHALL BE PERFORMED AFTER THE INTERMEDIATE COURSE HAS BEEN COMPLETED AND BEFORE THE SURFACE COURSE IS PLACED. ALL LABOR AND EQUIPMENT NECESSARY TO PERFORM THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER MILE FOR ITEM 209 LINEAR GRADING.

**ITEM 604 - MONUMENT BOX ADJUSTED TO GRADE**

THE MONUMENT BOX TO BE ADJUSTED MAY OR MAY NOT HAVE AN EXISTING ADJUSTABLE FRAME. THE WORK SHALL CONSIST OF ADJUSTING THE EXISTING MONUMENT BOX TO THE SATISFACTION OF THE ENGINEER. THE CONTRACTOR IS REMINDED TO FIELD CHECK ALL ADJUSTMENT TO GRADE ITEMS PRIOR TO BIDDING, AS NO ADDITIONAL COMPENSATION WILL BE GRANTED FOR LABOR AND MATERIALS REQUIRED TO SATISFACTORILY ADJUST CASTINGS WITHOUT ADJUSTABLE FRAMES.

APPROXIMATE LOCATIONS OF KNOWN MONUMENT BOXES:

CRA-96, SLM 9.31

APPROXIMATE LOCATIONS OF BURIED MONUMENT BOXES:

- |                  |                  |
|------------------|------------------|
| CRA-96, SLM 7.10 | CRA-96, SLM 7.21 |
| CRA-96, SLM 7.39 | CRA-96, SLM 7.58 |
| CRA-96, SLM 7.70 | CRA-96, SLM 7.92 |
| CRA-96, SLM 7.94 | CRA-96, SLM 8.13 |
| CRA-96, SLM 8.15 | CRA-96, SLM 8.34 |
| CRA-96, SLM 8.38 | CRA-96, SLM 9.24 |

TOTAL = 13 EACH

**PAVEMENT**

**ITEM 253. PAVEMENT REPAIR, AS PER PLAN  
ITEM 253. PAVEMENT REPAIR, MISC.: PARTIAL DEPTH**

THESE ITEMS OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE.

THE ENGINEER SHALL DESIGNATE THE LOCATIONS AND LIMITS OF THE AREAS TO BE REPAIRED. PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE INTERMEDIATE AND/OR SURFACE COURSE. THE REPAIR AREAS SHALL BE SAW CUT AND EXCAVATED TO PROVIDE STRAIGHT AND VERTICAL SURFACES AROUND THE PERIMETER OF THE REPAIR AREA. PAVEMENT PLANING MAY BE USED AS AN ALTERNATIVE TO SAW CUTTING AND EXCAVATING. THE PAVEMENT SHALL BE REMOVED WITHIN THE DESIGNATED AREAS BY METHODS WHICH WILL NOT DAMAGE ADJACENT PAVEMENT. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH AN AVERAGE DEPTH OF 3" AND AN AVERAGE WIDTH OF 4 FT FOR ESTIMATING PURPOSES. THE MATERIALS REMOVED SHALL BE DISPOSED OF IN ACCORDANCE WITH 105.16 AND 105.17.

THE CONTRACTOR SHALL BE CAPABLE OF PERFORMING PAVEMENT REPAIRS 2 FEET WIDE.

REPLACEMENT MATERIAL SHALL BE ITEM 301 OR ITEM 448 TYPE 2 MATERIAL AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. ITEM 301 ASPHALT CONCRETE, PG64-22 CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 3" AND 12" WITH A MAXIMUM PAVEMENT LIFT OF 6". ITEM 448 TYPE 2 CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 0" AND 5" WITH A MAXIMUM PAVEMENT LIFT OF 3". THE CONTRACTOR HAS THE OPTION OF USING EITHER ITEM 301 OR ITEM 448 TYPE 2 MATERIAL WHEN THE PAVEMENT REPAIR IS BETWEEN 3" AND 5" DEEP. ITEM 448 TYPE 2 MATERIAL SHALL BE PG64-22 FOR MEDIUM MIX DESIGN PAVEMENTS AND PG64-28 FOR HEAVY MIX DESIGN PAVEMENTS. ALL EXISTING PAVEMENT AREAS WHICH WILL BE IN CONTACT WITH THE PAVEMENT REPAIR SHALL BE CLEANED AND COATED PER CMS 401.14, USING AN ASPHALT MATERIAL COMPLYING WITH 407.02. ALL COMPACTION SHALL BE ACHIEVED BY MECHANICAL METHODS TO THE SATISFACTION OF THE ENGINEER.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES ITEM 253 PAVEMENT REPAIR, MISC.: PARTIAL DEPTH IS TO BE A MAXIMUM OF 4" DEEP AND ITEM 253 PAVEMENT REPAIR, AS PER PLAN IS FOR DEPTHS GREATER THAN 4". PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, (BY TICKET WEIGHT CONVERSION), OF ITEM 253, PAVEMENT REPAIR, AS PER PLAN OR ITEM 253 PAVEMENT REPAIR, MISC.: PARTIAL DEPTH. THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ITEM 253 PAVEMENT REPAIR, MISC.: PARTIAL DEPTH 600 CU. YD.  
ITEM 253 PAVEMENT REPAIR, AS PER PLAN 200 CU. YD.

**ITEM 254. PAVEMENT PLANING, ASPHALT CONCRETE**

THE INTENT OF THE PLANING IS TO MILL 1.5 INCHES AT THE EDGE OF PAVEMENT AT NON-CURBED AREAS. THE PAVEMENT SLOPE SHALL BE 0.016 PREFERRED AND 0.010 MINIMUM, CONTINUOUS BETWEEN THE CROWN AND THE PROPOSED EDGELINE/SHOULDER. THE MILLING DEPTH SHALL BE CONTROLLED FROM THE EDGE OF PAVEMENT IN CONFORMANCE WITH THE ABOVE GUIDELINES.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER. IF THERE IS NO INFORMATION IN THE PLANS TO CHANGE THE SUPERELEVATION, THE INTENT IS TO MAINTAIN THE EXISTING SUPERELEVATION.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. THE 14 CALENDAR DAYS SHALL BE CONSIDERED AN INTERIM COMPLETION DATE (SECTION 108), AND FOR EACH CALENDAR DAY BEYOND THE 14 DAYS THAT THE ROADWAY REMAINS EXPOSED TO THE PLANED SURFACE, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES AS PER 108.07.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF PAVEMENT PLANING, ASPHALT CONCRETE. NO ADDITIONAL COMPENSATION WILL BE GRANTED FOR UNEXPECTED VOLUMES OF ASPHALT GRINDINGS.

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GENERAL NOTES

CRA-96-3.65  
RIC-96-0.00

**PAVEMENT**

**ITEM 254 PATCHING PLANED SURFACE**

AN ESTIMATED QUANTITY OF ITEM 254, PATCHING PLANED SURFACE HAS BEEN SET UP TO BE USED AS DIRECTED BY THE ENGINEER AS DESCRIBED IN CMS 254.04. THE LIMIT OF THE PATCHING DEPTH IS 0 TO 2 IN.

**ITEM 407. TACK COAT**

**ITEM 407. TACK COAT FOR INTERMEDIATE COURSE**

AS PER 407.06 THE APPLICATION RATES SHALL BE 0.08 GAL. PER SQ. YD. PRIOR TO THE INTERMEDIATE COURSE AND SHALL BE 0.03 GAL PER SQ. YD. PRIOR TO THE SURFACE COURSE FOR ESTIMATING PURPOSES ONLY. THE RATE OF APPLICATION SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. A COMPLETE PAVEMENT SURFACE COVERAGE SHALL BE REQUIRED. AREAS OF TACK STRIPPED BY CONSTRUCTION EQUIPMENT OR TRAFFIC SHALL BE RE-COATED PRIOR TO PLACING ASPHALT CONCRETE. ALL COSTS AS DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID PER GALLON FOR ITEM 407, TACK COAT AND ITEM 407 TACK COAT FOR INTERMEDIATE COURSE.

**ITEM 442. ASPHALT CONCRETE SURFACE COURSE, 9.5 MM. TYPE A (448). AS PER PLAN**

ALL OPEN TRANSVERSE JOINTS SHALL BE TAPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC. A "BUMP" SIGN (W8-1-36) SHALL BE ERCTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER JOINT HAS BEEN CLOSED. PLACEMENT OF SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

CARE SHALL BE TAKEN TO MATCH EXISTING PAVEMENT ELEVATIONS AT EXISTING PAVED BERMS, DRIVES, INTERSECTIONS, ETC.

REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:  
MIX DESIGN: FOR Ndes USE 50 GYRATIONS, FOR Nmax USE 75 GYRATIONS.  
MINIMUM TOTAL PG BINDER CONTENT IS 6.0 PERCENT.  
USE A PG 64-22 BINDER.  
MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT IS 20 PERCENT.  
WHEN AN AGGREGATE SOURCE IS SPECIALLY DESIGNATED WITH AN SR ON THE AGGREGATE SOURCE GROUP LIST DO NOT USE THE AGGREGATE EXCEPT AS ALLOWED FOR MEDIUM TRAFFIC IN THE GUIDELINES FOR MAINTAINING ADEQUATE PAVEMENT FRICTION IN SURFACE PAVEMENT.  
QUALITY CONTROL: DO NOT PERFORM Nmax IN QUALITY CONTROL TESTING. DO NOT TAKE EXTRA ASPHALT BINDER SAMPLES AS OUTLINED IN CMS 442.05.

**ITEM 442. ASPHALT CONCRETE INTERMEDIATE COURSE, 9.5MM. TYPE A (448). AS PER PLAN**

THIS ITEM SHALL BE USED FOR CORRECTION OF CROWN, PROFILE AND ANY OTHER IRREGULARITIES.

ALL OPEN TRANSVERSE JOINTS SHALL BE TAPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC. A "BUMP" SIGN (W8-1-36) SHALL BE ERCTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER JOINT HAS BEEN CLOSED. PLACEMENT OF SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:  
MIX DESIGN: FOR Ndes USE 50 GYRATIONS, FOR Nmax USE 75 GYRATIONS.  
MINIMUM TOTAL PG BINDER CONTENT IS 6.0 PERCENT.  
USE A PG 64-22 BINDER.  
MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT IS 20 PERCENT.  
QUALITY CONTROL: DO NOT PERFORM Nmax IN QUALITY CONTROL TESTING. DO NOT TAKE EXTRA ASPHALT BINDER SAMPLES AS OUTLINED IN CMS 442.05.

**PAVEMENT**

**ITEM 617. COMPACTED AGGREGATE, AS PER PLAN**

THIS ITEM OF WORK SHALL CONFORM TO ITEM 617 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS BOOK WITH EXCEPTION OF 617.02 (MATERIALS).

THE MATERIAL ON THIS PROJECT SHALL BE THE ASPHALT CONCRETE GRINDINGS RESULTING FROM ITEM 254. THE GRINDINGS USED FOR THIS WORK ARE TO BE PLACED AND COMPACTED AS DESCRIBED IN 617.05 WITH SPECIAL CARE TO CREATE PROPER COMPACTION. 100% OF THIS MATERIAL SHALL PASS A 1.5 INCH SIEVE. THE CONTRACTOR SHALL TAKE SPECIAL CARE TO MEET THE TYPICAL SECTIONS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS REQUIRED TO APPLY THE ITEM 408 PRIME COAT WITHIN 5 CALENDAR DAYS OF PLACING THE COMPACTED AGGREGATE, AS PER PLAN.

PAYMENT FOR ALL THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID PER CU. YD. OF ITEM 617 COMPACTED AGGREGATE, AS PER PLAN.

**INTERSECTIONS AND DRIVES**

RURAL-INTERSECTIONS SHALL BE PLANED AND PAVED TO THE END OF THE RADI) OR AS DIRECTED BY THE ENGINEER. (TO PROVIDE A SMOOTH TRANSITION BETWEEN THE TWO HIGHWAYS, AND TO ELIMINATE WATER POCKETS).

URBAN-INTERSECTIONS SHALL BE PLANED AND PAVED TO THE BACK OF CROSSWALKS OR AS DIRECTED BY THE ENGINEER. ( TO PROVIDE A SMOOTH TRANSITION BETWEEN THE TWO HIGHWAYS, AND TO ELIMINATE WATER POCKETS).

EXISTING PAVED DRIVES SHALL BE PAVED SO AS TO PROVIDE A SMOOTH TRANSITION BETWEEN THE HIGHWAY AND THE DRIVE, (DISTANCE FROM EDGE OF ROADWAY MAY VARY AT EACH DRIVE) AS DIRECTED BY THE ENGINEER.

EXISTING AGGREGATE DRIVES SHALL BE PAVED WITH AN APRON AN AVERAGE WIDTH OF 4 FT. THE SLOPE OF THIS APRON SHALL BE THE SAME AS THE ADJACENT PAVEMENT SLOPE OR AS DIRECTED BY THE ENGINEER. ANY GRADING NEEDED TO PAVE THE APRON SHALL BE INCLUDED IN THE RELATED ASPHALT ITEM FOR PAYMENT. ITEM 617 COMPACTED AGGREGATE, AS PER PLAN SHALL BE PLACED ADJACENT TO THIS APRON TO PROVIDE A SMOOTH TRANSITION FROM THE APRON TO THE EXISTING DRIVE, (WIDTH OF THIS 617 APPLICATION MAY VARY) AS DIRECTED BY THE ENGINEER. AN ADDITIONAL QUANTITY OF ITEM 617 HAS BEEN ESTIMATED TO COMPLETE THIS WORK AND IS SHOWN AS AN EXTRA AREA ON THE PAVEMENT & SHOULDER DATA SHEET.

ANY HAZARD OR UNSAFE CONDITION RESULTING FROM THE ABOVE WORK MUST BE CORRECTED IMMEDIATELY. THE CONTRACTOR IS REMINDED OF SECTIONS 105.01, 107.07 & 614.02A OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

**MAINTENANCE OF TRAFFIC**

**BUTT JOINTS**

BUTT JOINTS SHALL NOT BE CUT AND LEFT OPEN TO TRAFFIC. THEY SHALL BE FILLED IN WITH A TEMPORARY ASPHALT CONCRETE WEDGE USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

CONSTRUCTION "BUMP" (W8-1-36) AND "ADVISORY SPEED" (W13-1-24) SIGNS SHALL BE ERCTED AND MAINTAINED DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. THESE SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC.

**PLACEMENT OF ASPHALT CONCRETE**

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

**ITEM 614. ASPHALT CONCRETE FOR MAINTAINING TRAFFIC**

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO CONSTRUCT A TEMPORARY ASPHALT WEDGE FROM THE EXISTING PAVEMENT TO THE PLANED SURFACE AT BUTT JOINTS AND OTHER LOCATIONS THAT RESULT IN A DROP-OFF IN EXCESS OF 1.5 INCHES. THIS QUANTITY SHALL ALSO BE USED AT PLANED SURFACES WHERE A TEMPORARY ASPHALT WEDGE IS NEEDED AROUND CASTINGS. BEFORE RESURFACING OF THE PAVEMENT, THE TEMPORARY WEDGE SHALL BE REMOVED AND THE COST SHALL BE CONSIDERED INCIDENTAL TO ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 50 CU YD

**ITEM 614. MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)**

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS FOURTH OF JULY  
NEW YEARS LABOR DAY  
MEMORIAL DAY THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH CMS 108.07.

**ITEM 614. WORK ZONE MARKING SIGN**

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR TEMPORARY WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS, 614.04.

WORK ZONE MARKING SIGN: (W8-H13-36) NO EDGE LINE = 24 EACH  
WORK ZONE MARKING SIGN: (R4-1-24) DO NOT PASS = 19 EACH  
WORK ZONE MARKING SIGN: (R4-2-24) PASS WITH CARE = 18 EACH

TOTAL = 61 EACH

GENERAL NOTES

CRA-96-3.65  
RIC-96-0.00

5  
9

**ITEM SPECIAL, MAILBOX SUPPORT SYSTEM**

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF EXISTING NON-STANDARD MAILBOX SUPPORTS AND FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED HARDWARE IN ACCORDANCE WITH THE DETAILS SHOWN, AND ATTACHING AN OWNER SUPPLIED MAILBOX, AT LOCATIONS DETERMINED BY THE ENGINEER.

IN ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE BOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL SUPPLY ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION. SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO MAILBOXES MAY BE MOUNTED ON A SINGLE POST. [HARDWARE SHALL BE COMMERCIAL GRADE GALVANIZED STEEL.]

WOOD POSTS SHALL BE NOMINAL 4 IN. x 4 IN. (S4S) OR 4 1/2 IN. DIAMETER ROUND, AND CONFORM TO 710.14. STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2 IN. I.D., AND CONFORM TO AASHTO M 181.

POSTS SHALL BE SET AS PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

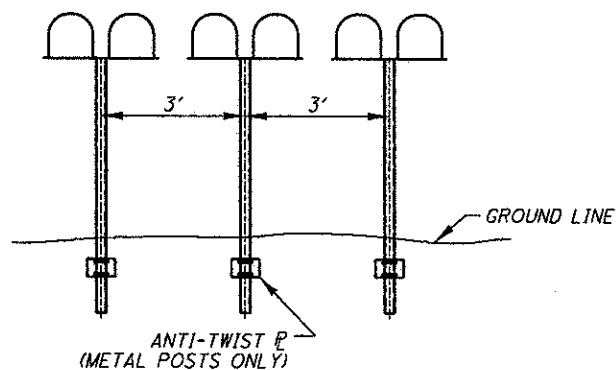
THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WORK WITH THE LOCAL POST MASTER AND NOTIFYING THE PROPERTY OWNERS PRIOR TO WORK.

GROUP MAILBOX SUPPORTS SHALL BE PLACED ON 3 FT. CENTERS AND THE TURNOUT LENGTHENED TO ACCOMMODATE THE GROUPING.

WHERE GUARDRAIL EXISTS, MAILBOXES AND THEIR SUPPORTS SHALL BE PLACED BEHIND THE GUARDRAIL. SUPPORTS MUST STILL MEET THE BREAKAWAY REQUIREMENTS LISTED ABOVE.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DESCRIBED ABOVE.

ITEM SPECIAL-MAILBOX SUPPORT SYSTEM, SINGLE  
S.R. 96 10 EACH



**GROUP MAILBOX INSTALLATION**

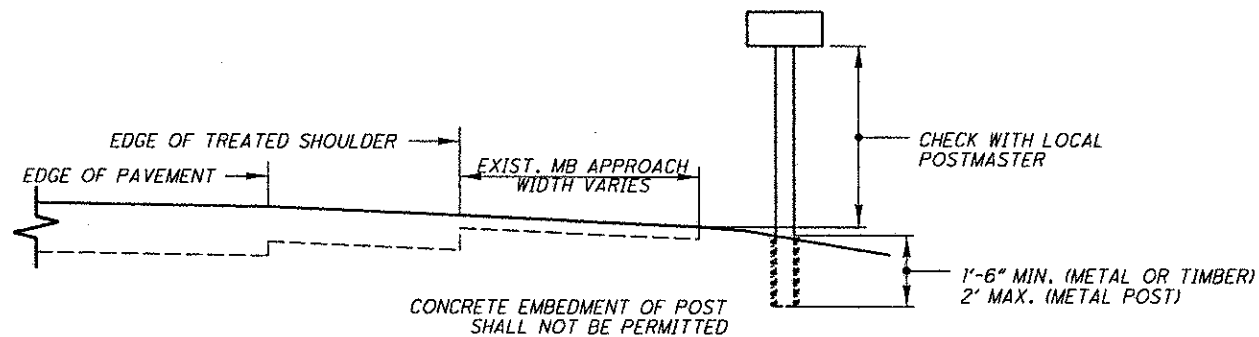
**MAILBOX APPROACHES**

THE EXISTING MAILBOX APPROACHES SHALL BE PAVED WITH PROPOSED PAVEMENT BUILDUP. NO PROPOSED MAILBOX APPROACHES ARE TO BE INCLUDED IN THIS PROJECT.

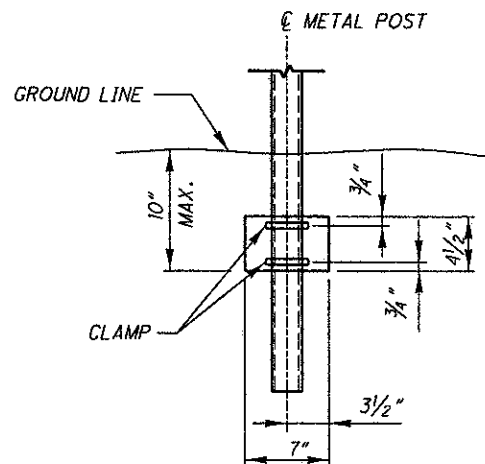
**LOCATIONS OF MAILBOX SUPPORT SYSTEM TO BE REPLACED**

ADDRESSES AND/OR LOCATIONS OF MAILBOX SUPPORT SYSTEM TO BE REPLACED:

CRA-96	#7040	slm 8.16
CRA-96	#7094	slm 8.30
CRA-96	#7180	slm 7.80
CRA-96	#7222	slm 8.74
CRA-96	#7250	slm 8.80
CRA-96	#7291	slm 8.95
CRA-96	#7634	slm 9.89
CRA-96	#7902	slm 10.60
RIC-96	#5522	slm 0.58
RIC-96	#5486	slm 0.64



**CROSS SECTION / ELEVATION VIEW**



**ANTI-TWIST PLATE**

FOR DETAILS NOT SHOWN SEE STANDARD DRAWING BP-4.1

DESIGN FILE: I:\projects\83414\roadway\sheets\83414GM001.dgn  
WORKSTATION: claughre DATE: 2/12/2010

CALCULATED  
CAL  
CHECKED  
SCJ

**MAILBOX FACILITIES**

**CRA-96-3.65  
RIC-96-0.00**

6  
9

SHEET NUMBER

PARTICIPATION

ITEM

ITEM EXT.

GRAND TOTAL

UNIT

DESCRIPTION

SEE SHEET NO.

CALCULATED  
 CAL  
 CHECKED  
 SCJ

4 5 6 8 9

ROADWAY

13 17.60 811

209 60500 17.60 MILE LINEAR GRADING  
 604 39500 13 EACH MONUMENT BOX ADJUSTED TO GRADE  
 621 54000 811 EACH RAISED PAVEMENT MARKER REMOVED  
 SPECIAL 69050100 10 EACH MAILBOX SUPPORT SYSTEM, SINGLE

6

PAVEMENT

200 600 122182 1218 9774 3688 8253 5123 2561 890 21373

253 02001 200 CU YD PAVEMENT REPAIR, AS PER PLAN  
 253 90000 600 CU YD PAVEMENT REPAIR, MISC.: PARTIAL DEPTH  
 254 01000 122182 SO YD PAVEMENT PLANING, ASPHALT CONCRETE (1.5")  
 254 01600 1218 SO YD PATCHING PLANED SURFACE  
 407 10000 9774 GALLON TACK COAT  
 407 14000 3688 GALLON TACK COAT FOR INTERMEDIATE COURSE  
 408 10000 8253 GALLON PRIME COAT  
 442 10501 5123 CU YD ASPHALT CONCRETE SURFACE COURSE, 9.5 MM, TYPE A (448), AS PER PLAN  
 442 20101 2561 CU YD ASPHALT CONCRETE INTERMEDIATE COURSE, 9.5 MM, TYPE A (448), AS PER PLAN  
 617 10101 890 CU YD COMPACTED AGGREGATE, AS PER PLAN  
 617 20000 21373 SO YD SHOULDER PREPARATION

5  
 5  
 5

TRAFFIC CONTROL

811 17.90 8.91 270

621 00100 811 EACH RPM  
 642 00100 17.90 MILE EDGE LINE, TYPE I  
 642 00300 8.91 MILE CENTER LINE, TYPE I  
 644 00500 270 FT STOP LINE

MAINTENANCE OF TRAFFIC

61 50 26.73 90 45

614 12460 61 EACH WORK ZONE MARKING SIGN  
 614 13000 50 CU YD ASPHALT CONCRETE FOR MAINTAINING TRAFFIC  
 614 21500 26.73 MILE WORK ZONE CENTER LINE, CLASS II, 642 PAINT  
 614 26200 90 FT WORK ZONE STOP LINE, CLASS I, 642 PAINT  
 614 26610 45 FT WORK ZONE STOP LINE, CLASS III, 642 PAINT

614 11000 LUMP MAINTAINING TRAFFIC  
 619 16000 2 MONTH FIELD OFFICE, TYPE A  
 624 10000 LUMP MOBILIZATION

GENERAL SUMMARY

CRA-96-3.65  
 RIC-96-0.00

\* - FOR TYPICALS, SEE SHEET 3

COUNTY	ROUTE	LOG POINT TO LOG POINT		LENGTH		WIDTH FEET AVG.	* TYPICAL	PAVEMENT AREA	254		407	407	442		442		AGGREGATE SHOULDER PROPOSED WIDTH		209	408	617	617							
				MILE	FEET				SQ YD	SQ.YD	SQ.YD	GALLON	GALLON	INCH	CU.YD.	INCH (AVG)	CU.YD.	SL	SR	SQ YD	MILE	GALLON	CU YD	SQ.YD					
																									AGGREGATE SHOULDER AREA	LINEAR GRADING	PRIME COAT @ 0.40 GAL/SY	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION
																									1.5 INCHES	AVG. THICKNESS			
STRAIGHT LINE MILEAGE	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET	FEET											
CRA	96	3.65	8.39	4.74	25027	23.0	1	63,958	63,958	640	5,117	1,919	1.5	2,665	0.75	1,332	2.0	2.0	11,123	9.48	4,449	463	11,123						
CRA	96	8.52	9.19	0.67	3538	23.0	1	9,042	9,042	90	723	271	1.5	377	0.75	188	2.0	2.0	1,572	1.34	629	66	1,572						
CRA	96	9.19	9.23	0.04	211	24.0	1	563	563	6	45	17	1.5	23	0.75	12	2.0	2.0	94	0.08	38	4	94						
CRA	96	9.23	9.33	0.10	528	25.0	1	1,467	1,467	15	117	44	1.5	61	0.75	31	2.0	2.0	235	0.20	94	10	235						
CRA	96	9.33	9.37	0.04	211	24.0	1	563	563	6	45	17	1.5	23	0.75	12	2.0	2.0	94	0.08	38	4	94						
CRA	96	9.37	11.42	2.05	10824	23.0	1	27,661	27,661	277	2,213	830	1.5	1,153	0.75	576	2.0	2.0	4,811	4.10	1,924	200	4,811						
RIC	96	0.00	1.14	1.14	6019	25.0	1	16,719	16,719	167	1,338	502	1.5	697	0.75	348	2.0	2.0	2,675	2.28	1,070	111	2,675						
CRA	96	SR 602 EB ENTRANCE RAMP			190	22.0		464	464	5	37	14	1.5	19	0.75	10	2.0	2.0	84	0.07	34	4	84						
EXTRA AREA FOR INTERSECTIONS								1572	1572	16	126	47	1.5	66	0.75	33													
EXTRA AREA FOR PAVED DRIVES								280	280		22	8	1.5	12	0.75	6													
EXTRA AREA FOR AGGREGATE DRIVES								745			22	1.5	31	0.75	16			745			31	745							
EXTRA AREA FOR EX. MAILBOX APPROACHES								254	254		20	8	1.5	11	0.75	5													
DEDUCT BRIDGE CRA-96-7.96				35	23.0		89	-89	-1	-7	-3	1.5	-4	0.75	-2	2.0	2.0	16	-0.01	-6	-1	-16							
DEDUCT BRIDGE CRA-96-9.23				98	25.0		272	-272	-3	-22	-8	1.5	-11	0.75	-6	2.0	2.0	44	-0.02	-17	-2	-44							
<b>TOTALS</b>				<b>8.78</b>	<b>46358</b>		<b>122,182</b>		<b>1,218</b>	<b>9,774</b>	<b>3,688</b>	<b>5,123</b>		<b>2,561</b>				<b>17.60</b>	<b>8,253</b>	<b>890</b>	<b>21,373</b>								

PAVEMENT & SHOULDER DATA

CRA-39-0.00/RIC-96-0.00

CALC BY  
CAL  
CHKD BY  
SCJ



### AUXILIARY & LONG LINE MARKINGS

ROUTE	COUNTY	STATION / SLM		HIGHWAY MILES	614				642, TYPE 1					644												SPECIAL											
					WORK ZONE LANE LINE, CLASS I, 642 PAINT	WORK ZONE CENTER LINE, CLASS II, 642 PAINT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	EDGE LINE		CENTER LINE			AUXILIARY MARKINGS (740.04)																							
									TOTAL (PAY QUANTITY) (WHITE)	TOTAL (PAY QUANTITY) (YELLOW)	LANE LINE	SOLID LINE EQUIVALENT	TOTAL (PAY QUANTITY)	CHANNELIZING LINE	STOP LINE	CROSSWALK LINE	TRANSVERSE/DIAGONAL LINE (WHITE)	TRANSVERSE/DIAGONAL LINE (YELLOW)	ISLAND MARKING	RAILROAD SYMBOL MARKING	SCHOOL SYMBOL MARKING		PARKING LOT STALL MARKING	LANE ARROW				WORD ON PAVEMENT "ONLY"		HANDICAP SYMBOL MARKING	AIR SPEED ZONE MARKING						
FROM	TO	MILE	MILE	MILE	FT	FT	MILE	MILE	MILE	MILE	MILE	FT	24"	12"	24"	24"	SQ FT	EACH	72 INCH	96 INCH	FT	EACH				EACH	FT	EACH	EACH								
SR 96	CRAWFORD	3.65	11.42	7.77		23.31	90	45			15.54			10.527	7.77																						
SR 96	RICHLAND	0.00	1.14	1.14		3.42					2.28			1.581	1.14																						
SR 96	SR 602 EB ENTRANCE RAMP			0.04							0.04	0.04																									
<b>TOTAL</b>				<b>8.95</b>	<b>0.00</b>	<b>26.73</b>	<b>90</b>	<b>45</b>	<b>17.86</b>	<b>0.04</b>		<b>12.108</b>	<b>8.91</b>	<b>0</b>	<b>270</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	

### RAISED PAVEMENT MARKERS

ROUTE	COUNTY	STATION/SLM		DETAIL	621					621					REMARKS	DETAIL	DESCRIPTION																		
					RAISED PAVEMENT MARKER REMOVED	RPM	PRISMATIC RETRO-REFLECTOR TYPES																												
							ONE-WAY	TWO-WAY																											
FROM	TO	EACH	EACH	WHITE	YELLOW / YELLOW	WHITE / RED	YELLOW / RED	BLUE / BLUE																											
SR 96	CRAWFORD	3.65	3.85	6	32	32	16	11						5	PLACE 5 YELLOW/RED RPM'S ON EASTBOUND ENTRANCE RAMP	1	MULTILANE UNDIVIDED TYPICAL SPACING																		
SR 96	CRAWFORD	3.85	4.10	15	51	51	0	51								2	TAPERED ACCEL. LANE																		
SR 96	CRAWFORD	4.10	5.24	GAP	75	75	0	75								3	DECELERATION LANE																		
SR 96	CRAWFORD	5.24	5.45	16	29	29	0	29								4	PARALLEL ACCEL LANE																		
SR 96	CRAWFORD	5.45	5.59	GAP	9	9	0	9								5	MULTILANE DIVIDED/EXPRESSWAY																		
SR 96	CRAWFORD	5.59	5.80	16	29	29	0	29								6	STOP APPROACH																		
SR 96	CRAWFORD	5.80	8.17	GAP	156	156	0	156								7	2 LANE APPR. WITH TURN LANE																		
SR 96	CRAWFORD	8.17	8.37	6	27	27	16	11								8	THROUGH APPROACH																		
SR 96	CRAWFORD	8.37	8.49	8	15	15	0	15								9	3 LANE APPR. WITH TURN LANE																		
SR 96	CRAWFORD	8.49	8.69	6	27	27	16	11								10	3 LANE DIVIDED TO 2 LANE TRANSITION																		
SR 96	CRAWFORD	8.69	8.85	GAP	10	10	0	10								11	3 LANE UNDIVIDED TO 2 LANE TRANSITION																		
SR 96	CRAWFORD	8.85	9.38	15/16	101	101	0	101								12	TWO LANE NARROW BRIDGE																		
SR 96	CRAWFORD	9.38	10.12	GAP	49	49	0	49								13	TWO WAY LEFT TURN LANE																		
SR 96	CRAWFORD	10.12	10.34	15	36	36	0	36								14	ONE LANE BRIDGE																		
SR 96	CRAWFORD	10.34	11.41	GAP	71	71	0	71								15	HORIZONTAL CURVE																		
SR 96	RICHLAND	0.00	0.91	GAP	60	60	0	60								16	HORIZONTAL CURVE ALT.																		
SR 96	RICHLAND	0.91	1.14	15/8	34	34	0	34								17	STOP APPROACH ALT.																		
																	18	FIRE HYDRANT																	
																	GAP	CENTER LINE AT 80 FT. TYP.																	
																	NOTES																		
																	1) THRU LANES SHALL BE STRIPED AT 11' WIDTHS.																		
																	2) FOR ALL WORK ZONE MARKINGS, THE 642 PAINT USED SHALL BE TYPE 1.																		
																	3) WORK ZONE STOP LINES SHALL BE INSTALLED AT THE FOLLOWING LOCATIONS: SR 96 & SR 602 SR 96 & SR 598																		
<b>TOTAL</b>				<b>811</b>	<b>811</b>	<b>48</b>	<b>758</b>																												

PAVEMENT MARKING / RPM SUB-SUMMARY

CRA-96-3.65/RIC-96-0.00

CALC BY  
CHKD BY

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