

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

EXISTING PLANS

EXISTING PLANS ENTITLED ALL/HAN-30-(20.31)(0.00) AND HAN/WYA-30-(30.560)(0.000) MAY BE INSPECTED IN THE ODOT DISTRICT 1 OFFICE IN LIMA, OHIO.

EROSION CONTROL

THE QUANTITY BELOW HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR EROSION CONTROL.

ITEM 832 EROSION CONTROL 2,000 EACH

CONTACT INFORMATION

THE CONTRACTOR SHALL NOT BEGIN WORK IN A COUNTY UNTIL CONTACTING THE COUNTY MANAGER & PROJECT ENGINEER. BELOW IS A CONTACT LIST FOR COUNTY MANAGERS:

ALLEN

CONTACT	TITLE	OFFICE NUMBER	CELL NUMBER
JASON HOSCHAK	ADMINISTRATOR	(419) 999-6711	(419) 438-4615
ANDY WITA	MANAGER	(419) 999-6712	(419) 234-5377
BRIAN RADER	MANAGER	(419) 999-6717	(419) 204-3683

HANCOCK

CONTACT	TITLE	OFFICE NUMBER	CELL NUMBER
DEIDRA NOEL	ADMINISTRATOR	(419) 999-6731	(419) 772-4420
JAMES HEACOCK	MANAGER	(419) 999-6738	(419) 306-1428
MATTHEW CLAY	MANAGER	(419) 999-6732	(419) 306-5199

PUTNAM

CONTACT	TITLE	OFFICE NUMBER	CELL NUMBER
PAUL LEHMAN	ADMINISTRATOR	(419) 999-6761	(419) 615-3449
LARRY SCHROEDER	MANAGER	(419) 999-6762	(419) 957-4999
KEN WILLIAMSON	MANAGER	(419) 999-6768	(419) 796-0127

WYANDOT

CONTACT	TITLE	OFFICE NUMBER	CELL NUMBER
KEVIN KLIESCH	ADMINISTRATOR	(419) 999-6781	(419) 348-5224
GEENA SNOW	MANAGER	(419) 999-6782	(419) 619-2745
APRIL NOEL	MANAGER	(419) 999-6788	(419) 294-7654

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (442)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION ADJACENT TO THE APPROACH SLABS BEING REPLACED AT BRIDGE NO. HAN-15-2090 L&R AND PLACING ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5 mm, TYPE A or B, (448). IN ADDITION, THIS ITEM SHALL BE USED TO PROVIDE A SMOOTH TRANSITION INTO THE NEW APPROACH SLABS AS DIRECTED BY THE ENGINEER. FOR PLACEMENT OF ITEM 442, A PG64-22 BINDER IS REQUIRED, AND IT SHALL BE PLACED IN AN ONE AND HALF INCH LIFT THICKNESS. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF THE ABUTTING APPROACH SLAB REPLACEMENT WORK, AND THIS ITEM SHALL COMMENCE WITHIN 7 DAYS OF THE COMPLETION OF THE APPROACH SLABS. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF SURFACE PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 251, PARTIAL DEPTH PAVEMENT REPAIR (442), 1,900 SQ. YD.

SEEDING AND MULCHING

CARE SHALL BE TAKEN TO LIMIT DAMAGE TO EXISTING GROUND COVER. ALL SEEDED AREAS THAT ARE DISTURBED SHALL BE SEEDED ACCORDING TO CMS SECTION 659. PAYMENT FOR REPAIRING THE DISTURBED AREAS SHALL BE INCIDENTAL TO ITEM 611 FIELD PAVING OF EXISTING PIPE.

ITEM 253 - PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH ADJACENT TO THE APPROACH SLABS BEING REPLACED AT BRIDGE NO. HAN-15-2090 L&R AND PLACING 12" 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. THE FULL DEPTH PAVEMENT REPAIRS SHALL HAVE A SURFACE COURSE APPLIED PER THE NOTE AND REQUIREMENTS FOR ITEM 251, PARTIAL DEPTH PAVEMENT REPAIR (442). PAYMENT FOR THE SURFACE COURSE SHALL BE INCLUDED WITH ITEM 251. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF THE ABUTTING APPROACH SLAB REPLACEMENT WORK, AND THIS ITEM SHALL COMMENCE WITHIN 7 DAYS OF THE COMPLETION OF THE APPROACH SLABS.

IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED.

PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF SURFACE PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 253, PAVEMENT REPAIR, 200 SQ. YD.

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

ITEM 503 - COFFERDAMS AND EXCAVATION BRACING

A LUMP SUM QUANTITY FOR EACH LOCATION TO BE FIELD PAVED HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 513 - COFFERDAMS AND EXCAVATION BRACING - LS

ITEM 642 - EDGE LINE, 6", TYPE 1 & ITEM 642 - LANE LINE, 6", TYE 1

THE QUANTITIES BELOW HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR PLACEMENT OF LANE AND EDGE LINES ON THE RECONSTRUCTED APPROACH SLABS AT BRIDGE NO. HAN-15-2090 LT & RT.

ITEM 642, EDGE LINE, 6", TYPE 1 0.04 MILE
ITEM 642, LANE LINE, 6", TYPE 1 0.02 MILE

GENERAL SUMMARY

SHEET	SHEET	SHEET	SHEET	SHEET	SHEET	PARTICIPATION (PLAN SPLIT)				ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
						01/STR/BR	02/IMS/BR	03/NHS/BR	04/NFP/BR						
2	3	4	5	5A	8										
LS								LS	LS	201	11000	LS		ROADWAY CLEARING AND GRUBBING	
														EROSION CONTROL	
2,000						250	250	1,000	500	832	30000	2,000	EACH	EROSION CONTROL	
														PAVEMENT	
1,900								1,900		251	01020	1,900	SY	PARTIAL DEPTH PAVEMENT REPAIR (442)	
200								200		253	01000	200	SY	PAVEMENT REPAIR	
	128							128		SPECIAL	4513000	128	FT	PRESSURE RELIEF JOINT, TYPE A	3
														TRAFFIC CONTROL	
0.04								0.04		642	00104	0.04	MILE	EDGE LINE, 6", TYPE 1	
0.02								0.02		642	00204	0.02	MILE	LANE LINE, 6", TYPE 1	
														STRUCTURE REPAIR (SEE SHHEET 8))	
														MAINTENANCE OF TRAFFIC	
			80					80		614	11100	80	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
				3,200				3,200		614	11630	3,200	FT	INCREASED BARRIER DELINEATION	
			2					2		614	12384	2	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL)	
			4					4		614	12460	4	EACH	WORK ZONE MARKING SIGN	
				20				20		614	13310	20	EACH	BARRIER REFLECTOR, TYPE 1, ONE WAY	
				20				20		614	13310	20	EACH	BARRIER REFLECTOR, TYPE 1, BIDIRECTIONAL	
				20				20		614	13350	20	EACH	OBJECT MARKER, ONE WAY	
				20				20		614	13360	20	EACH	OBJECT MARKER, TWO WAY	
			1.4					1.4		614	22210	1.4	MILE	WORK ZONE EDGE LINE, CLASS 1, 6", 740.06, TYPE 1	
			3,100					3,100		614	24402	3,100	FT	WORK ZONE DOTTED LINE, CLASS 1, 6", 740.06, TYPE 1	
				550				550		615	25001	550	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN	5
				5,400				5,400		618	40101	5,400	FT	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE), AS PER PLAN	5
				800				800		622	41100	800	FT	PORTABLE BARRIER, UNANCHORED	
														INCIDENTALS	
			LS	LS	LS			LS	LS	614	11001	LS		MAINTAINING TRAFFIC, AS PER PLAN	4 & 5A
								LS	LS	624	10000	LS		MOBILIZATION	

ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN

IR 75 RAMP @ BLUELICK RD. (ALL-75-11.81C)

ONE-WAY TRAFFIC ON IR 75 RAMP SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED SEVEN (7) CONSECUTIVE CALENDAR DAYS THAT RAMP D TRAFFIC FROM IR 75 SB TO BLUELICK RD. MAY BE DETOURED AS SHOWN ON THIS SHEET AND TO COMPLETE THE REPAIR WORK ON BRIDGE NO. ALL-75-1181C. THE CLOSURE SHALL NOT OCCUR ON AND BETWEEN JULY 1ST & JULY 15TH THE CLOSURE LIMITS SHALL BE RESTRICTED TO THESE WORK LIMITS. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$5,000 PER DAY FOR EACH CALENDAR DAY THE RAMP REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMITS. THE DETOUR SHALL BE ESTABLISHED, MAINTAINED & SUBSEQUENTLY REMOVED BY THE STATE OF OHIO.

NO ROAD CLOSURES, WORK OR EQUIPMENT WILL BE ALLOWED ON THE BLUELICK RD. RAMPS FOR THE PERIOD OF JULY 1ST THRU JULY 15TH.

THE FIRST DAY THAT THE DETOUR IS IN EFFECT SHALL BE CONSIDERED THE STARTING DATE OF THE 7 DAY DETOUR/ CLOSURE LIMITATION. THE 7TH DAY OF THE 7 DAY DETOUR/ CLOSURE LIMITATION SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE. ON OR BEFORE THE 7TH DAY, THE ROADWAY SHALL BE OPENED TO THE SAFE AND CONVENIENT USE OF THE TRAVELING PUBLIC. IF THE ROADWAY IS NOT OPENED BY THIS INTERIM COMPLETION DATE, DISINCENTIVES SHALL BE ASSESSED AS PER THE ABOVE SPECIFICATION.

US 30 RAMPS @ SR 696 (ALL-696-0.20)

US 30 RAMP TRAFFIC AT SR 696 SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FIFTY-SEVEN (57) CONSECUTIVE HOURS, THAT THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEET 6. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$200 PER FIFTEEN (15) MINUTES FOR EACH FIFTEEN (15) MINUTE PERIOD A RAMP REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIC LIMIT. THE DETOURS, AS SHOWN ON SHEET 6, SHALL BE ESTABLISHED, MAINTAINED AND SUBSEQUENTLY REMOVED BY THE STATE OF OHIO. RAMPS CAN BE CLOSED CONCURRENTLY AS LONG AS THE CLOSURES DO NOT INTERFERE WITH DETOUR ROUTES. THE EXIT RAMP CLOSURES AS PER STANDARD CONSTRUCTION DRAWING MT-98.29 AND THE BARRICADES AS PER STANDARD CONSTRUCTION DRAWING MT-101.60 FOR THE ENTRANCE RAMP CLOSURES, SHALL BE ESTABLISHED, MAINTAINED AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR.

EACH FIFTY-SEVEN (57) HOURS OF US 30 RAMP CLOSURE SHALL OCCUR OVER A WEEKEND AND PRIOR TO JUNE 15. THE CLOSURE SHALL START ON FRIDAY AT 9 PM AND SHALL END ON MONDAY AT 6 AM.

THE FIRST HOUR THAT THE DETOUR IS IN EFFECT SHALL BE CONSIDERED THE STARTING DATE OF THE 57 HOUR DETOUR/CLOSURE LIMITATION. THE 57TH HOUR OF THE 57 HOUR DETOUR/CLOSURE LIMITATION SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE. ON OR BEFORE THE 57TH HOUR, THE RAMP SHALL BE OPENED TO THE SAFE AND CONVENIENT USE OF THE TRAVELING PUBLIC. IF THE RAMP IS NOT OPENED BY THIS INTERIM COMPLETION DATE, DISINCENTIVES SHALL BE ASSESSED AS PER THE ABOVE SPECIFICATIONS.

A MINIMUM OF 2 LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES. THE SHOULDERS MAY BE CLOSED ON U.S. 30 WHILE WORK IS OCCURRING PER MT-95.45. USE OF BARRELS IN LIEU OF PORTABLE CONCRETE BARRIER SHALL BE PERMITTED IF WARRANTED BY SCD MT-101.90. ONCE WORK HAS BEEN COMPLETED FOR THE DAY, THE ZONE MUST BE COMPLETELY REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN. LENGTH AND DURATION OF SHOULDER CLOSURES SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE

ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)

TO IMPACT TO THE TRAVELING PUBLIC. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

EQUIPMENT CANNOT BE STORED UNPROTECTED IN THE MEDIAN OR SHOULDER AREA AS PER 614.035. IT MUST BE MOVED TO A PROTECTED AREA, WHENEVER NOT IN USE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ON STATE ROUTES, A MINIMUM OF ONE, TEN FOOT LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 502 STRUCTURE FOR MAINTAINING TRAFFIC, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, ITEM 615 ROADS FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614.

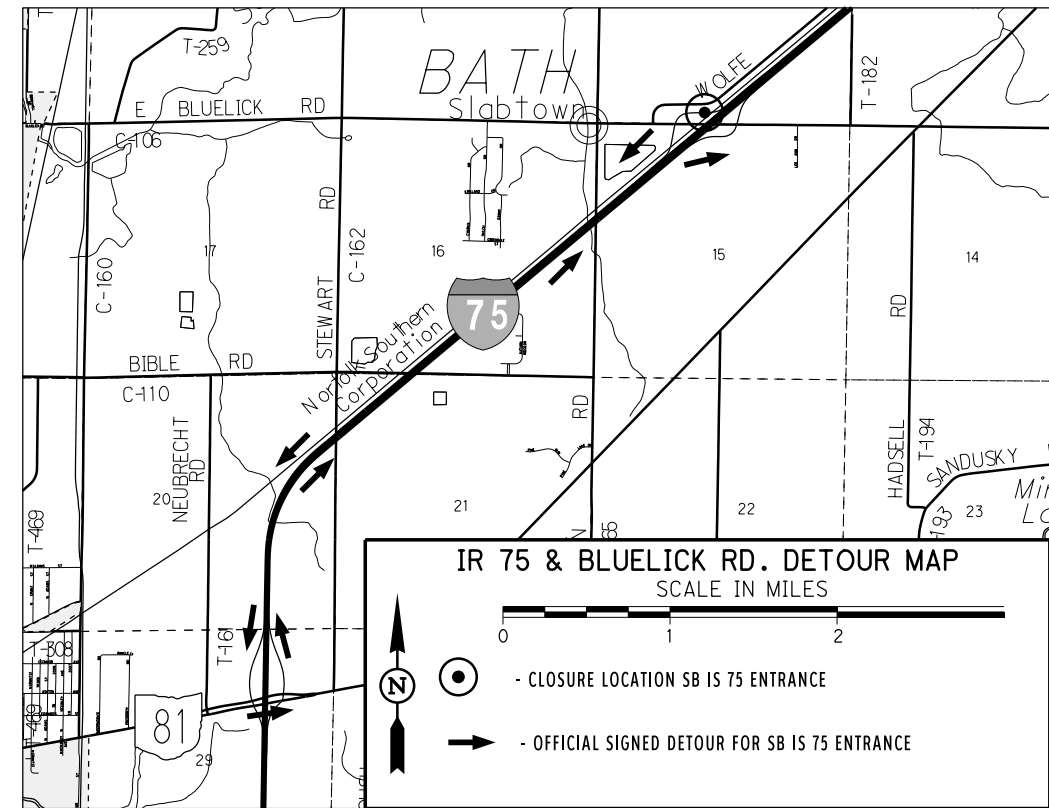
FOR WORK AT THE BRIDGES, A MINIMUM OF ONE AND HALF FEET LATERAL CLEARANCE IS REQUIRED FROM EDGE OF LANE TO BARRIERS AND CHANNELIZING DEVICES, AND AT THE HAN-15-2090 L&R BRIDGES, NO LANE SHIFTS OVER THE BRIDGES AND APPROACH SLABS ARE PERMITTED UNTIL THE REPAIRS TO THE ABUTMENTS (PATCHING AND CURTAIN WALL CONSTRUCTION) ARE COMPLETED.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES AND RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATED WITH THE WORK IN PROGRESS.

ACCESS TO ADJACENT PROPERTY WITHIN THE WORK LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR AT ALL TIMES, AS PER 614.02(a).

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.



ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)



NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
RAMP & ROAD CLOSURE	>= 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.

UPON COORDINATION WITH THE STATE OF OHIO, DETOURS SHALL BE ESTABLISHED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE STATE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

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CALCULATED
XXX
CHECKED
XXX

MAINTENANCE OF TRAFFIC

D01-BM-FY21

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STRUCTURE REPAIR (SFN 6900062) (PUT-115-01.32 OVER SUGAR CREEK)				
ITEM	EXT.	DESCRIPTION	UNIT	TOTAL
516	01301	ELASTOMERIC STRIP SEAL WITHOUT STEEL EXTRUSIONS, AS PER PLAN (SEE SHT, 7)	FT	70
STRUCTURE REPAIR (SFN 0200506) (ALL-696-0020 OVER WB US 30 & RAMP D)				
ITEM	EXT.	DESCRIPTION	UNIT	TOTAL
512	10100	SEALING OF CONCRETE STRUCTURE (EXPOXY-EURETHANE)	SY	31
519	11101	PATCHING CONCRETE STRUCTURE, AS PER PLAN (SEE SHT. 7)	SF	200
SPECIAL	51912510	PATCHING CONCRETE BRIDGE DECK (SEE THIS SHEET)	SY	27
842	10000	CORRECTING ELEVATION OF CONCRETE APPROACH SLABS W/ HIGH DENSITY POLYURETHANE	LB	70
843	50000	PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR	SF	75
STRUCTURE REPAIR (SFN 8801630) (WYA-30-1821 OVER INTERMITTENT WATERWAY)				
ITEM	EXT.	DESCRIPTION	UNIT	TOTAL
PIPE LINER OPTION A: SS 837 LINER PIPE (SEE PN 138)				
503	11100	COFFERDAMS AND EXCAVATION BRACING		LS
837	10000	LINER PIPE, 54" OD (707.34) (SEE DETAIL AND NOTE BELOW)	FT	472
837	21000	BACKFILL FOR LINER PIPE	FT	472
PIPE LINER OPTION B: SS 833 CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER (SEE PN 138)				
833	10000	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT (60" DIAMETER)	FT	472
PIPE LINER OPTION C: SS 899 CURED-IN-PLACE PIPE LINER (SEE PN 138)				
899	10000	CURED-IN-PLACE PIPE LINER (60" DIAMETER)	FT	472

WORK TO BE PERFORMED

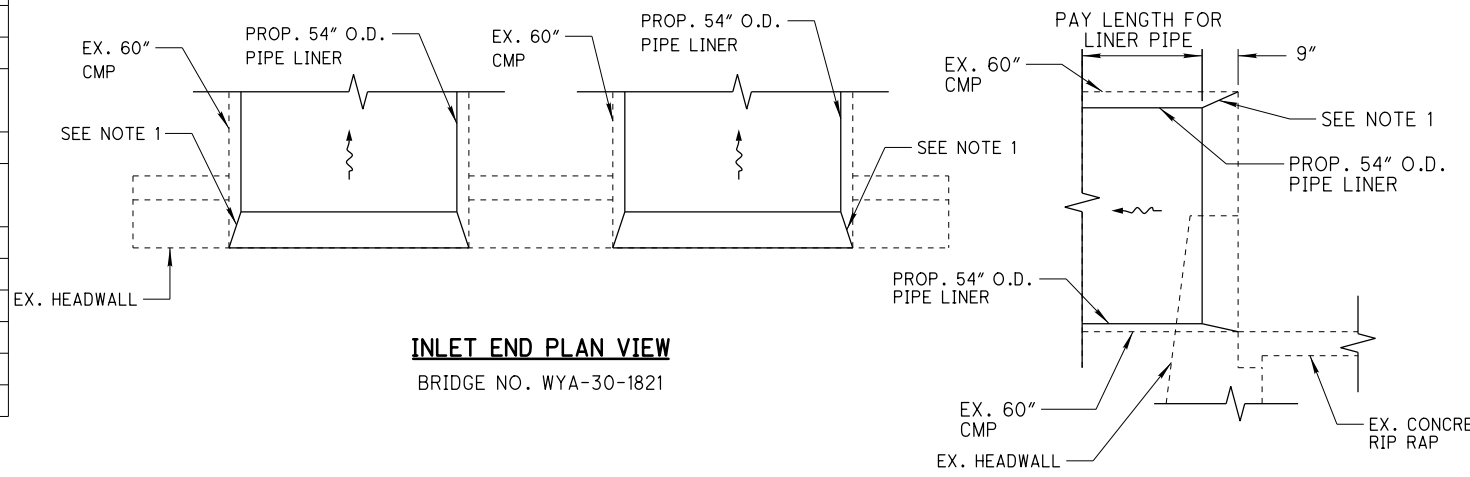
County	Route	SLM	Feature Intersected	SFN	Description of Work
Replace Approach Slabs					
HAN	SR 15	2090L	LYE CREEK	3200574	Replace Approach Slabs, patch deck & abutment/wingwalls, install curtain walls, and treat deck with GFR
HAN	SR 15	2090R	LYE CREEK	3200604	Replace Approach Slabs, patch deck & abutment/wingwalls, install curtain walls, and treat deck with GFR
Joint Replacement					
ALL	IR 75	1181C	RAMP C OVER NS RR	0202401	Replace damaged intermediate joint
ALL	SR 696	00200	US 30 WB & RAMP D	0200506	Replace pressure relied joint, repair median sections, lift parapet end sections & patch deck
PUT	SR 115	0132	SUGAR CREEK	6900062	Strip Seal Gland Replacement
Pipe Lining					
WYA	US 30	1821	INTERMITTENT WATERWAY	8801630	Line Twin 60" Alum.

ITEM SPECIAL, PATCHING CONCRETE BRIDGE DECK

THIS ITEM SHALL BE COMPLETED PER THE SPECIFICATIONS IN PROPOSAL NOTE 512. THE CONTRACTOR HAS THE OPTION TO USE PATCHING MATERIAL TYPE A, B OR C TO COMPLETE THE REPAIRS WITHIN THE TIME FRAMES PERMITTED IN THE PLANS.

THIS ITEM SHALL BE USED TO COMPLETE REPAIRS TO CONCRETE ON BRIDGE DECKS AND APPROACH SLABS AS PER THE "WORK TO BE PERFORMED" LIST ON THIS SHEET AND TO ADDRESS ANY POT HOLES OR CONCRETE SCALING THAT EXISTS.

STRUCTURE REPAIR (SFN 3200574) (HAN-15-2090 L OVER LYE CREEK)				
ITEM	EXT.	DESCRIPTION	UNIT	TOTAL
202	22900	APPROACH SLAB REMOVED	SY	134
509	10000	EPOXY COATED REINFORCING STEEL	LB	1182
510	10000	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	EACH	158
512	10600	CONCRETE REPAIR BY EPOXY INJECTION	FT	10
512	73500	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY-FED RESIN	SY	442
519	11101	PATCHING CONCRETE STRUCTURE, AS PER PLAN (SEE SHT. 7)	SF	175
SPECIAL	51911900	PATCHING CONCRETE STRUCTURES, MISC.: ABUTMENT BACKWALL (SEE SHT. 7)	CY	6
SPECIAL	51912510	PATCHING CONCRETE BRIDGE DECK (SEE THIS SHEET)	SY	40
526	25001	REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN (SEE SHT. 7)	SY	134
843	50000	PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR	SF	50
STRUCTURE REPAIR (SFN 3200604) (HAN-15-2090 R OVER LYE CREEK)				
ITEM	EXT.	DESCRIPTION	UNIT	TOTAL
202	22900	APPROACH SLAB REMOVED	SY	134
509	10000	EPOXY COATED REINFORCING STEEL	LB	1182
510	10000	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	EACH	158
512	10600	CONCRETE REPAIR BY EPOXY INJECTION	FT	15
512	73500	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY-FED RESIN	SY	442
519	11101	PATCHING CONCRETE STRUCTURE, AS PER PLAN (SEE SHT. 7)	SF	320
SPECIAL	51911900	PATCHING CONCRETE STRUCTURES, MISC.: ABUTMENT BACKWALL (SEE SHT. 7)	CY	6
SPECIAL	51912510	PATCHING CONCRETE BRIDGE DECK (SEE THIS SHEET)	SY	50
526	25001	REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN (SEE SHT. 7)	SY	134
843	50000	PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR	SF	160
STRUCTURE REPAIR (SFN 0202401) (ALL-75-1181C OVER NORFOLK AND SOUTHERN RR)				
ITEM	EXT.	DESCRIPTION	UNIT	TOTAL
202	11203	PORTIONS OF STRUCTURE REMOVED, OVER 20 FT SPAN, AS PER PLAN (SEE SHT. 7)		LS
511	34410	CLASS QC2 CONCRETE, SUPERSTRUCTURE	CY	1
512	10100	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	SY	1
516	12301	STRIP SEAL EXPANSION JOINT ANCHORED WITH ELASTOMERIC CONCRETE, AS PER PLAN (SEE SHTS 12-13)	FT	28



NOTE:

1. THE LINER PIPES ARE TO BE PLACED 9" SHORTER THAN THE EXISTING PIPES ON THE INLET ENDS. BACKFILL FOR THE LINER PIPES TO BE SLOPED TO TRANSITION FROM THE INLET ENDS OF THE LINER PIPES TO THE INLET ENDS OF THE EXISTING CORRUGATED METAL PIPES.

DESIGN AGENCY: ODOT DISTRICT ONE
 DATE: VARIOUS
 REVIEWED: VARIOUS
 DRAWN: EJS
 DESIGNED: EJS
 CHECKED: EJS
 STRUCTURE FILE NUMBER: VARIOUS
 PLANNING & ENGINEERING
 STRUCTURE GENERAL NOTES & ESTIMATED QUANTITIES
 D01-BM-FY21
 PID No. 101196
 2 / 7
 8 / 13