

MODEL: Sheet PAPER:SIZE: 34x22 (in.) DATE: 1/9/2026 TIME: 9:27:07 AM PLTDRV: OHDOT_PDF_PLOT_PDF.pltGIG PENTBL: OHDOT_Pen.tbl USER: Clark.Schlatter@doh.ohio.gov WORKSPACE: OHDOTCEv02 WORKSET: 119050 PRODUCT: OpenRoadsDesigner 24.00.00.205 pwc:\ohio\do-pw-bentley.com\ohio\do-pw-02\Documents\01 Active Projects\District 01\ D01\119050\400-Engineering\Roadway\Sheets\119050_GT001.dgn

FOR LOCATION MAPS, LATTITUDE AND LONGITUDE
SEE SHEET 2

LOCATION MAP

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

D01-BP-FY26

BATH, MILFORD, AND MONROE TOWNSHIPS

ALLEN AND DEFIANCE COUNTIES

INDEX OF SHEETS:

TITLE SHEET	1
LOCATION MAPS	2
GENERAL NOTES	3
MAINTENANCE OF TRAFFIC	4-8
GENERAL SUMMARY	9
STRUCTURE - ALL-75-13.87	10-12
STRUCTURE - ALL-75-15.05	13-15
STRUCTURE - DEF-249-0.10	16-18

FEDERAL PROJECT NUMBER

E250(153)

RAILROAD INVOLVEMENT

NORFOLK SOUTHERN RAILWAY GREAT LAKES DIVISION,
MILEPOST SP-81.25/SP-82.37

PROJECT DESCRIPTION

IMPROVEMENT OF VARIOUS STRUCTURES IN DISTRICT 1 BY PAINTING THE
STRUCTURAL STEEL.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.0 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.0 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A (NOI NOT REQUIRED)*
*ROUTINE MAINTENANCE PROJECT	

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND
HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION
OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION
5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF
TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN
THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL
SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN
THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS
IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY
AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEET 7 & 8.

Christopher A. Hughes

Christopher A. Hughes, P.E.
District 01 Deputy Director

Pamela Boratyn

Pamela Boratyn
Director, Department of Transportation

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES

Contact Two Working Days
Before You Dig

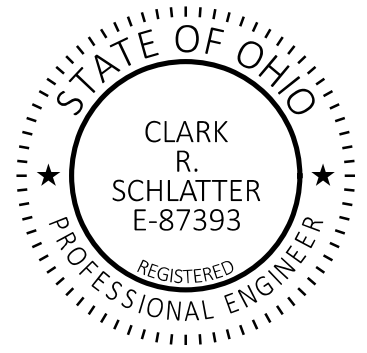

Before You Dig

OHIO 811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
OHIO DEPT. OF TRANSPORTATION, DISTRICT 1
1885 N. MCCULLOUGH ST.
LIMA, OHIO 45801

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
MT-95.30	7/19/19					800	1/17/25
MT-95.50	7/21/17					821	4/20/12
MT-97.10	4/19/19					832	7/19/24
MT-101.60	1/17/25					921	7/19/24
MT-102.20	4/19/19						
MT-105.10	1/17/20						
TC-41.20	10/18/13						
TC-42.20	10/18/13						
TC-52.10	10/18/13						
TC-52.20	1/15/21						

ENGINEER'S SEAL



DESIGN AGENCY



DESIGNER

KRH

REVIEWER

--- ---

PROJECT ID

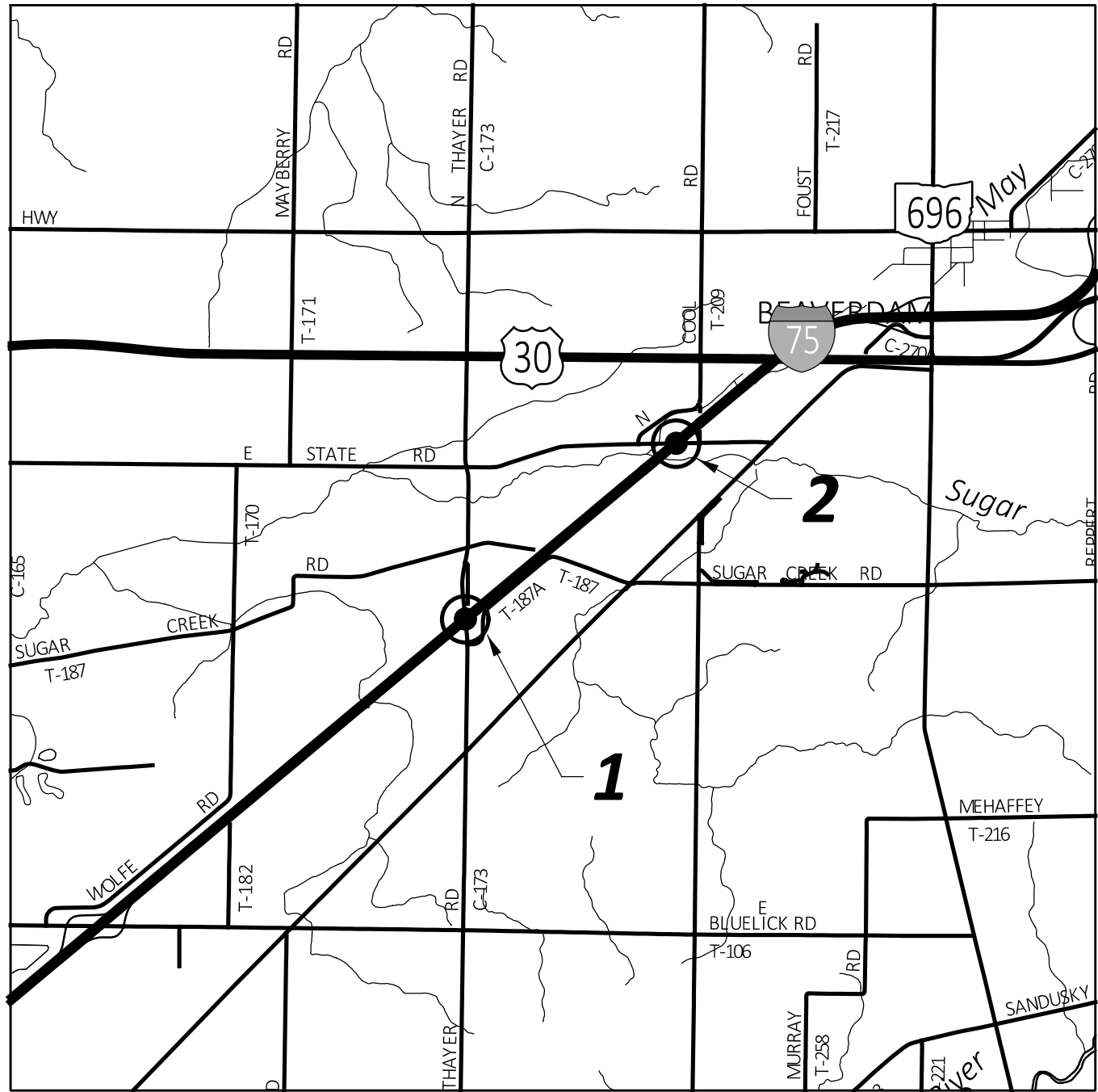
119050

SHEET

P.1

TOTAL

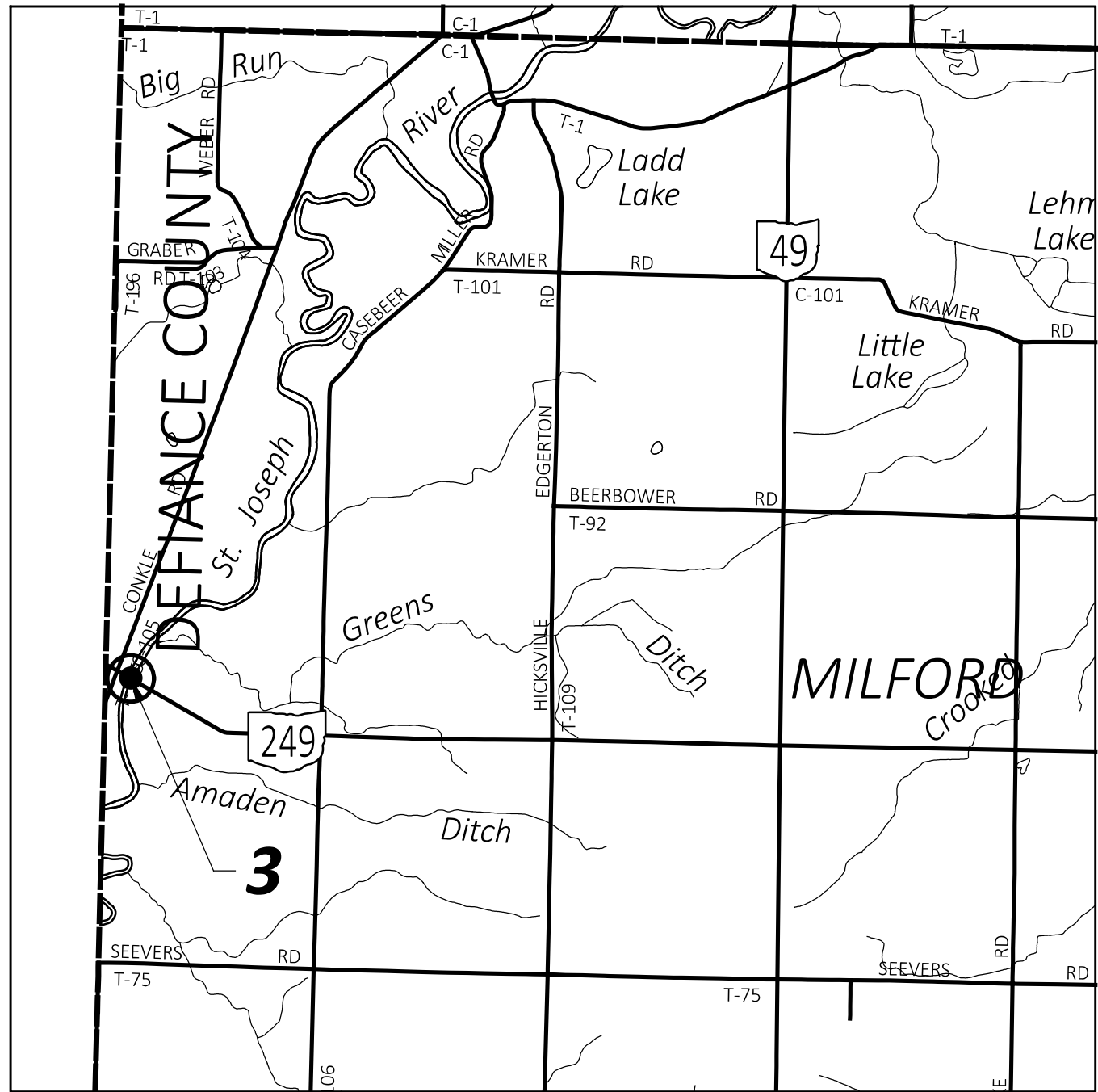
18



LOCATION MAP:

1. **All-75-13.87**
SFN: 0202495
LATITUDE: 40°48'31" LONGITUDE: 84°00'48"

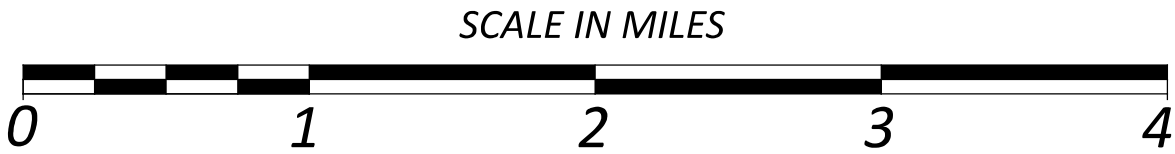
2. **All-75-15.05**
SFN: 0202614
LATITUDE: 40°49'11" LONGITUDE: 83°59'47"



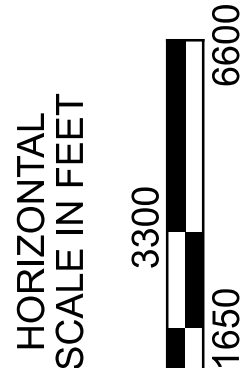
LOCATION MAP:

3. **DEF-249-0.10**
SFN: 2002280
LATITUDE: 41°23'09" LONGITUDE: 84°48'07"

INTERSTATE HIGHWAY
FEDERAL ROUTES
STATE ROUTES
COUNTY & TOWNSHIP ROADS
OTHER ROADS



NOTE:
● LOCATION OF STRUCTURE



LOCATION MAPS

DESIGN AGENCY



DESIGNER

KRH

REVIEWER

XXX MM-DD-YY

PROJECT ID

119050

SHEET

P.2

TOTAL

18

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS, SECTIONS 102.05, 105.02, AND 513.04*. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

EXISTING BRIDGE PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 1 OFFICE IN LIMA, OHIO DURING NORMAL BUSINESS HOURS.

PAINTING OF STRUCTURAL STEEL, AS PER PLAN

THE STRUCTURAL STEEL FOR EACH BRIDGE SHALL BE PAINTED THE FOLLOWING COLORS:

- ALL-75-13.87 - GREEN (AMS-595A, 14277)
- ALL-75-15.05 - GREEN (AMS-595A, 14277)
- DEF-249-0.10 - GREEN (AMS-595A, 14277)

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

SCUPPER, LENGTHENING, AS PER PLAN

PORTIONS OF SCUPPERS THAT ARE CORRODED THROUGH ARE TO BE CUT OFF AT A 90 DEGREE ANGLE TO THE SCUPPER BEFORE PAINTING. AFTER PAINTING SCUPPERS ARE TO BE LENGTHENED TO AT LEAST 8" BELOW THE BOTTOM OF THE BOTTOM FLANGE WITH CLAMP ON EXTENSIONS.

NOTIFICATION

THE CONTRACTOR SHALL NOTIFY THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 1 CONSTRUCTION ENGINEER TWO (2) WEEKS PRIOR TO BEGINNING WORK, ALONG WITH THE COUNTY ADMINISTRATORS USING THE LIST OF ODOT COUNTY PHONE NUMBERS BELOW:

ALLEN COUNTY	419-222-9905
DEFIANCE COUNTY	419-782-2826

PERSONAL PROTECTION EQUIPMENT (PPE)

THE CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF SECTIONS XXIV AND XXXIV OF THE OHIO DEPARTMENT OF TRANSPORTATION SAFETY & HEALTH STANDARD OPERATING PROCEDURE 220-006(SP) EFFECTIVE: NOVEMBER 1, 2018 (EXCEPT AS AMENDED BELOW) AND ALL SUBSEQUENT UPDATES POSTED AT THE FOLLOWING WEBSITE:

<https://www.transportation.ohio.gov/about-us/policies-and-procedures/procedures/220-006-sp>

AMENDMENTS TO THE REQUIREMENTS OF THIS DOCUMENT ARE:

XXIV. HEAD PROTECTION (HARD HATS)
ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR APPROPRIATE HEAD PROTECTION. ALL HARD HATS MUST MEET OR EXCEED ANSI Z89.1-2009 TYPE 1, CLASS E-G REQUIREMENTS.

XXXIV. SAFETY APPAREL AND VEST (HIGH VISIBILITY)
ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR A HIGH-VISIBILITY SAFETY VEST THAT MEETS THE PERFORMANCE CLASS II OR CLASS III REQUIREMENTS OF THE ANSI/ISEA 107-2015 PUBLICATION ENTITLED "AMERICAN NATIONAL STANDARD FOR HIGH-VISIBILITY SAFETY APPAREL AND ACCESSORIES.

WORKERS MAY WEAR AN ANSI CLASS II OR ANSI CLASS III APPROVED RAIN SUIT, JACKET OR OTHER APPAREL WITHOUT A SAFETY VEST OVER IT.

COORDINATION OF CONTRACTORS

SINCE THE MAINTENANCE OF TRAFFIC AND WORK ON THIS PROJECT MAY OVERLAP OTHER PROJECTS, IT IS ESSENTIAL THAT EACH CONTRACTOR CONDUCT THEIR WORK IN COOPERATION WITH EACH OTHER IN SUCH A MANNER AS NOT TO HINDER THE PROGRESS OR COMPLETION OF THE WORK BEING PERFORMED BY THE OTHER CONTRACTOR.

NORFOLK SOUTHERN RAILWAY COMPANY

ALL WORK TO BE PERFORMED ON, OVER, UNDER, OR ADJACENT TO THE RAILROAD RIGHT-OF-WAY SHALL COMPLY WITH THE NORFOLK SOUTHERN RAILWAY COMPANY ("RAILROAD", "NSR" or "NS") PUBLIC PROJECTS MANUAL (APPENDIX E, SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS, AND APPENDIX H1, OVERHEAD GRADE SEPARATION DESIGN CRITERIA). WHEN IN CONFLICT WITH OTHER PROJECT SPECIFICATIONS, THE MOST STRINGENT ONE SHALL APPLY.

"ONE CALL" SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL & COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD'S REPRESENTATIVE TWO DAYS IN ADVANCE OF WORK AT LOCATIONS WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE THE RAILROADS UNDERGROUND FACILITIES. UPON REQUEST FROM THE CONTRACTOR OR SPONSOR, RAILROAD PERSONNEL WILL LOCATE AND PAINT, MARK, OR FLAG THE RAILROAD'S UNDERGROUND FACILITIES. THE CONTRACTOR SHALL AVIOD EXCAVATION OR OTHER DISTURBANCE OF THESE FACILITIES. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD FACILITY, THE CONTRACTOR SHALL COORDINATE WITH THE RAILROAD TO HAVE THE FACILITY POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION. THE FACILITY SHALL BE PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF THE RAILROAD'S REPRESENTATIVE. (SEE NS PUBLIC PROJECTS MANUAL, APPENDIX E, SECTION 3.D)

THE CONTRACTOR SHALL NOT COMMENCE ANY WORK ON RAILROAD RIGHT-OF-WAY UNTIL COMPLIANCE IS MET WITH THE CONDITIONS PRESENTED ON NS PUBLIC PROJECTS MANUAL (SEE APPENDIX E, NORFOLK SOUTHERN - SPECIAL PROVISIONS FOR PROTECTION OF RAILWAY INTERESTS). THE CONTRACTOR SHALL SO ARRANGE AND CONDUCT HIS WORK THAT THERE WILL BENO INTERFERENCE WITH RAILROAD'S OPERATIONS. WHENEVER WORKIS LIABLE TO AFFECT THE OPERATIONS OR SAFETY OF TRAINS, THEMETHODS OF DOING SUCH WORK SHALL FIRST BE SUBMITTED TO THERAILROAD ENGINEER FOR APPROVAL, BUT SUCH APPROVAL SHALL NOT RELIEVE THE CONTRACTOR FROM ANY LIABILITY. RIGHT-OF-WAY AND/OR SECURITY FENCE SHALL BE PROVIDED AS DIRECTED BY THE NS PUBLIC PROJECT ENGINEER.

THE CONTRACTOR SHALL AT NO TIME ENTER THE RAILROAD RIGHT-OF-WAY WITH PERSONNEL OR EQUIPMENT UNTIL COMPLIANCE HAS BEEN MET WITH THE REQUIREMENTS PRESENTED IN THE NS PUBLIC PROJECTS MANUAL INCLUDING APPROVED RAILROAD PROTECTIVE INSURANCE, A CONTRACTOR RIGHT OF ENTRY, PROTECTIVE SERVICES ("FLAGGING"), AND RECIEVED FROM NS WRITTEN AUTHORIZATION TO PROCEED. (SEE APPENDIX E OF THE MANUAL, "NORFOLK SOUTHERN - SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS"). THE CONTRACTOR SHALL SO ARRANGE AND CONDUCT WORK SUCH THAT THERE WILL BE NO INTERFERENCE WITH THE RAILROAD'S OPERATIONS. WHENEVER WORK IS LIABLE TO AFFECT THE OPERATIONS OR SAFETY OF TRAINS, THE METHODS OF DOING SUCH WORK SHALL FIRST BE SUBMITTED TO THE RAILROAD ENGINEER FOR APPROVAL, BUT SUCH APPROVAL SHALL NOT RELIEVE THE CONTRACTOR FROM ANY LIABILITY. RAILROAD PROTECTIVE SERVICES ("FLAGGING") AND/OR SECURITY FENCE SHALL BE PROVIDED AS DIRECTED BY THE RAILROAD ENGINEER OR HIS FIELD REPRESENTATIVE.

THE CONTRACTOR IS REQUIRED TO SUBMIT A CONTAINMENT PLAN TO NORFOLK SOUTHERN DEPICTING THE PAINT PROCESS AND HOW CONTAMINATION OF NORFOLK SOUTHERN RIGHT-OF-WAY WITH PAINT/DEBRIS SHALL BE PREVENTED.

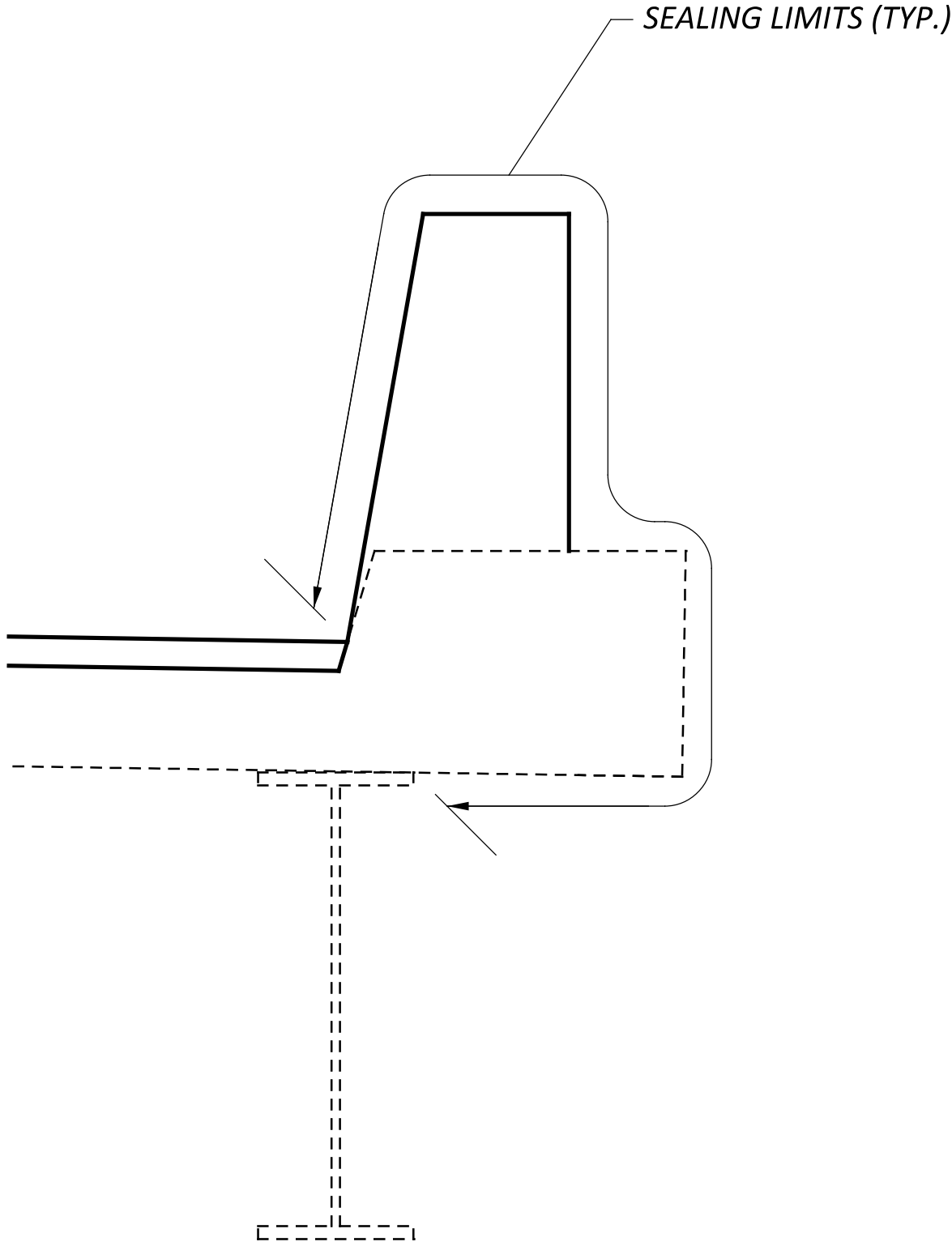
IF ANY UTILITIES MUST BE INSTALLED OR RELOCATED AS PART OF THIS PROJECT AT LOCATIONS: ALL-75-13.87 AND ALL-75-15.05 IN THE NORFOLK AND SOUTHERN RAILROAD RIGHT-OF-WAY THE FOLLOWING NOTE MUST BE COMPLETED PRIOR TO SUCH WORK OCCURRING: ALL UTILITY INSTALLATIONS OR RELOCATIONS THAT ARE REQUIRED IN CONJUNCTION WITH THIS PROJECT CAN BE INSTALLED OR RELOCATED AS PART OF THE PROJECT PROVIDED THE CONSTRUCTION IS PERFORMED BY THE PROJECT CONTRACTR OR PROJECT CONTRACTOR'S SUB-CONTRACTOR. HOWEVER, THE UTILITY MUST SUBMIT AN APPLICATION FOR THE INSTALLATION OR RELOCATION TO RAILPROS OR APPROPRIATE HANDLING FOR LICENSEAGREEMENT AND APPLICABLE FEES. FOR UTILITY APPLICATIONS GO TO: [HTTPS://WWW.NORFOLKSOUTHERN.COM/EN/RAIL-DEVELOPMENT-PROPERTY/NS-PROPERTY/PROJECTS-ON-NS-PROPERTY/PIPE-AND-WIRE](https://www.norfolksouthern.com/en/rail-development-property/ns-property/projects-on-ns-property/pipe-and-wire) OR WWW.NSCORP.COM > RAIL DEVELOPMENT & PROPERTY > REAL ESTATE > OVERVIEW > PIPE, WIRE & FIBER OPTICS

THE RAILROAD MAY BE CONTACTED USING THE FOLLOWING INFORMATION:

ELDRIDGE W. CHAMBERS
ENGINEER PUBLIC IMPROVEMENTS
NORFOLK SOUTHERN CORPORATION
650 PEACHTREE STREET, NW, BOX 45
ATLANTA, GA 30308
(470)-463-6307
Eldridge.Chambers@nscorp.com

SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)

THE DRAWING BELOW SHOWS THE EXTENTS OF THE SEALING OF CONCRETE SURFACES AT DEF-249-0.10. THIS IS BASED ON SEALING BOTH SIDES WITH AN EDGE DISTANCE OF 9.83' FOR 195'. A QUANTITY OF 426 SY HAS BEEN CARRIED TO THE GENERAL SUMMARY.



DESIGN AGENCY	
DESIGNER	KRH
REVIEWER	---
PROJECT ID	119050
SHEET	P.3
TOTAL	18

ITEM 614, MAINTAINING TRAFFIC

DEF-249-0.10, OVER ST. JOSEPH RIVER

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION OF SR 249 SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED THIRTY (30) CONSECUTIVE DAYS, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEET 8. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$4500 PER DAY FOR EACH CALENDAR DAY A ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE 30 DAY LIMITS.

THE FIRST DAY THAT THE DETOUR IS IN EFFECT SHALL BE CONSIDERED THE STARTING DATE OF THE 30 DAY DETOUR/CLOSURE LIMITATION. THE 30TH DAY OF THE 30 DAY DETOUR/CLOSURE LIMITATION SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE. ON OR BEFORE THE 30TH DAY, THE ROADWAY SHALL BE OPENED TO THE SAFE AND CONVENIENT USE OF THE TRAVELING PUBLIC. IF THE ROADWAY IS NOT OPENED BY THIS INTERIM COMPLETION DATE, DISINCENTIVES SHALL BE ASSESSED AS PER THE ABOVE SPECIFICATION.

TEMPORARY LANE CLOSURES PER STANDARD CONSTRUCTION DRAWING MT-97.10, OUTSIDE OF THE 30 DAY CLOSURES, ARE ACCEPTABLE AT THE APPROVAL OF THE ENGINEER.

ACCESS TO ADJACENT PROPERTY WITHIN THE WORK LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR AT ALL TIMES AS PER 614.02(a).

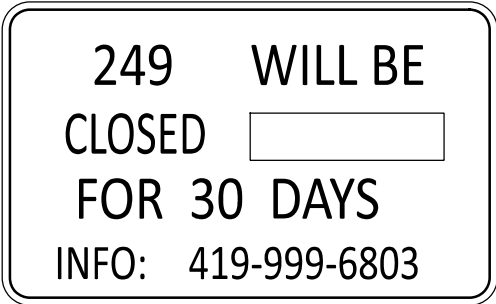
NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. AFTER THE ROAD IS OPENED TO THROUGH TRAFFIC AND THE "NOTICE OF CLOSURE" SIGNS ARE NO LONGER NEEDED, THE CONTRACTOR SHALL REMOVE THE SIGNS.

NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
ROAD CLOSURES	>= 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 CALENDAR DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.



W20-H13-60

NOTE: THE CONTRACTOR IS TO SUPPLY THE DATES

THE SR 249 ROADWAY CLOSURE SHALL BE ESTABLISHED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR AS PER STANDARD CONSTRUCTION DRAWING MT-101.60.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AND ADVANCE SIGNING AS DETAILED IN SCD MT-101.60 AS PER THE DETOUR MAP DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

THE CONTRACTOR MAY STORE EQUIPMENT ON THE BRIDGE ONLY DURING THE ROAD CLOSURE. ALL STAGING AND ACCESS TO UNDERSIDE OF BRIDGE MUST BE WITHIN THE RIGHT-OF-WAY.

ALL-75-13.87, THAYER RD OVER I.R. 75
ALL-75-15.05, STATE RD OVER I.R. 75

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION OF STATE RD AND THAYER RD SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FORTY-FIVE (45) CONSECUTIVE CALENDAR DAYS AT EACH BRIDGE. STATE RD AND THAYER RD SHALL NOT BE CLOSED CONCURRENTLY. FOR ALL-75-13.87 A DISINCENTIVE OF \$4,500 SHALL BE ASSESSED AND FOR ALL-75-15.05 A DISINCENTIVE SHALL BE ASSESSED PER THE SCHEDULE OF LIQUIDATED DAMAGES, TABLE 108.07-1, IN THE C&MS FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE 45 DAY LIMITS.

THE FIRST DAY THAT THE CLOSURE IS IN EFFECT SHALL BE CONSIDERED THE STARTING DATE OF THE 45 DAY CLOSURE LIMITATION. THE 45TH DAY OF THE 45 DAY CLOSURE LIMITATION SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE. ON OR BEFORE THE 45TH DAY, THE ROADWAY SHALL BE OPENED TO THE SAFE AND CONVENIENT USE OF THE TRAVELING PUBLIC. IF THE ROADWAY IS NOT OPENED BY THIS INTERIM COMPLETION DATE, DISINCENTIVES SHALL BE ASSESSED AS PER THE ABOVE SPECIFICATION.

THE INTENTION OF THE ROAD CLOSURES ON STATE RD AND THAYER RD IS TO ALLOW THE CONTRACTOR TO STORE EQUIPMENT ON THE BRIDGE. THE CONTRACTOR MAY STORE EQUIPMENT ON THE BRIDGE ONLY DURING THE ROADWAY CLOSURES. ALL STAGING AND ACCESS TO UNDERSIDE OF BRIDGE MUST BE WITHIN THE RIGHT-OF-WAY

ACCESS TO ADJACENT PROPERTY WITHIN THE WORK LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR AT ALL TIMES AS PER 614.02(a).

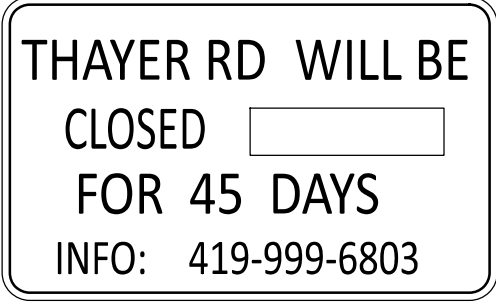
NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. AFTER THE ROAD IS OPENED TO THROUGH TRAFFIC AND THE "NOTICE OF CLOSURE" SIGNS ARE NO LONGER NEEDED, THE CONTRACTOR SHALL REMOVE THE SIGNS.

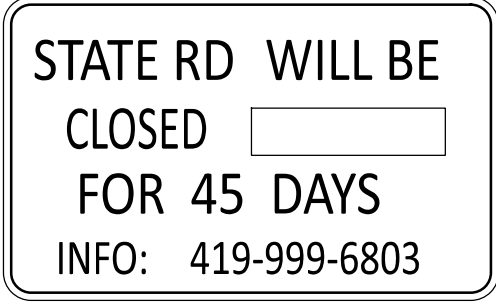
NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
ROAD CLOSURES	>= 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 CALENDAR DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.



W20-H13-60



W20-H13-60

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AND ADVANCE SIGNING AS DETAILED IN SCD MT-101.60 AS PER THE DETOUR MAP ON SHEET 7 DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

LANE CLOSURES ON I.R. 75

THE CONTRACTOR SHALL MAINTAIN TRAFFIC AT ALL TIMES ON MAINLINE I.R. 75 IN ACCORDANCE WITH THE REQUIREMENTS OF SPEC. 614, THESE MAINTENANCE OF TRAFFIC NOTES AND DETAILS, AND THE TRAFFIC CONTROL DETAILS DESCRIBED IN THESE PLANS.

LANE CLOSURES ON IR-75 ARE NOT PERMITTED BETWEEN 7:00 AM AND 7:00 PM. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE, PER THE LANE VALUE CONTRACT TABLE. REFER TO PN 127 AND THE LANE VALUE CONTRACT TABLE ON SHEET 6 FOR ADDITIONAL INFORMATION.

THE MINIMUM LANE WIDTH FOR TRAFFIC CONTROL SHALL BE 11 FT AT ALL TIMES ON I.R. 75. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ORGANIZE HIS/HER WORK IN SUCH A MANNER TO PROVIDE THE MOST SAFETY WITH THE LEAST INCONVENIENCE TO THE TRAVELING PUBLIC.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO DIRECTLY TRANSPORT OR OPERATE EQUIPMENT ACROSS THE OPEN LANES OF I.R. 75.

NO WORK WITHIN ACTIVE TRAVEL LANES OR WHICH SLOWS TRAFFIC IS PERMITTED AT ANY TIME.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED)	GENERAL/REGULAR ELECTION DAY (NOV)
MEMORIAL DAY	THANKSGIVING
FOURTH OF JULY (OBSERVED)	CHRISTMAS (OBSERVED)
LABOR DAY	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
---------------------------------	--

SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY (GEN./REG. ELECTION)	5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPERATELY ITEMIZED IN THE PLAN.

ALTERNATE METHODS

IF THE CONTRACTOR SO ELECTS, THEY MAY SUBMIT ALTERNATEMETHODS FOR MAINTENANCE OF TRAFFIC PROVIDED THE INTENT OF THE ABOVE PROVISIONS ARE FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THERE FROM. NO ALTERNATE PLAN SHALL BE PLACED INTO EFFECT UNTIL APPROVAL HAS BEEN GRANTED, IN WRITING, BY THE DISTRICT CONSTRUCTION ENGINEER. ALLOW 2 WEEKS FOR REVIEW WITH NO DELAY TO THE PROJECT. NO ADDITIONAL MONEY WILL BE PAID FOR ALTERNATE METHODS.

MAINTAINING TRAFFIC NEAR RUMBLE STRIPS

TRAFFIC IS NOT PERMITTED TO RUN ON OR CROSS OVER ANY RUMBLE STRIPS AT ANY TIME. RUMBLE STRIPS MUST BE FILLED WHEN THEY CONFLICT WITH THE MAINTENANCE OF TRAFFIC LANE CONFIGURATION. THIS INCLUDES LOCATIONS OF LANE SHIFTS ENTERING AND EXITING THE ZONE. THE RUMBLE STRIPS SHALL BE FILLED OR ELIMINATED BY PLANING AND PAVING TO PROVIDE A SMOOTH RIDE TO THE SATISFACTION OF THE PROJECT ENGINEER. ONCE TRAFFIC IS RETURNED TO IT FINAL LANE CONFIGURATION, THE RUMBLE STRIPS THAT WERE REMOVED IN ANY EXISTING PAVEMENT SHALL BE RESTORED TO THE PRE-CONSTRUCTION CONDITION TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL MATERIALS, LABOR, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

DESIGNATED LOCAL DETOUR ROUTE

IN ADDITION TO THE OFFICIAL, SIGNED DETOUR ROUTE, A LOCAL ROUTE HAS BEEN DETERMINED TO BE THE SECONDARY, UNSIGNED DETOUR ROUTE OR "DESIGNATED LOCAL DETOUR ROUTE." THESE ROUTES ARE SHOWN ON SHEETS NO. 7-8. DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THIS ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DESIGNATED LOCAL DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DETERMINED BY THE ENGINEER.

THE REPLACEMENT PAVEMENT FOR ITEM 253 PAVEMENT REPAIR SHALL CONSIST OF 1-1/4" ITEM 441, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 AND ITEM 407, NON-TRACKING TACK COAT PLACED ON 5" OF ITEM 301, ASPHALT CONCRETE BASE, PG64-22.

THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED FOR USE AS DETERMINED BY THE ENGINEER TO MAINTAIN AND SUBSEQUENTLY RESTORE THE DESIGNATED LOCAL DETOUR ROUTE.

ITEM 253 - PAVEMENT REPAIR	= 20 CY
ITEM 407 - NON-TRACKING TACK COAT	= 20 GAL
ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22	= 10 CY
ITEM 617 - COMPACTED AGGREGATE	= 50 CY

DESIGN AGENCY



DESIGNER

KRH

REVIEWER

PROJECT ID

119050

SHEET

P.4

TOTAL

18

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY. THE CONTRACTOR IS REQUIRED TO PROVIDE CLASS A PCMS ONLY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS SHOULD BE LOCATED IN ADVANCE OF THE LANE CLOSURES ON I.R. 75 BRIDGES PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT. THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN = 10 SNMT

ASSUMING 2 PCMS SIGNS EACH FOR 5 MONTHS

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:
ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION;
AND,
AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:
THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
THE ACTIVE WORK AREA Laterally CLOSEST TO THE OPEN TRAVELED LANE; OR
OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE = 800 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL

AN ESTIMATED QUANTITY OF 30 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.



DESIGNER
KRH

REVIEWER

PROJECT ID
119050

SHEET TOTAL
P.5 18

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

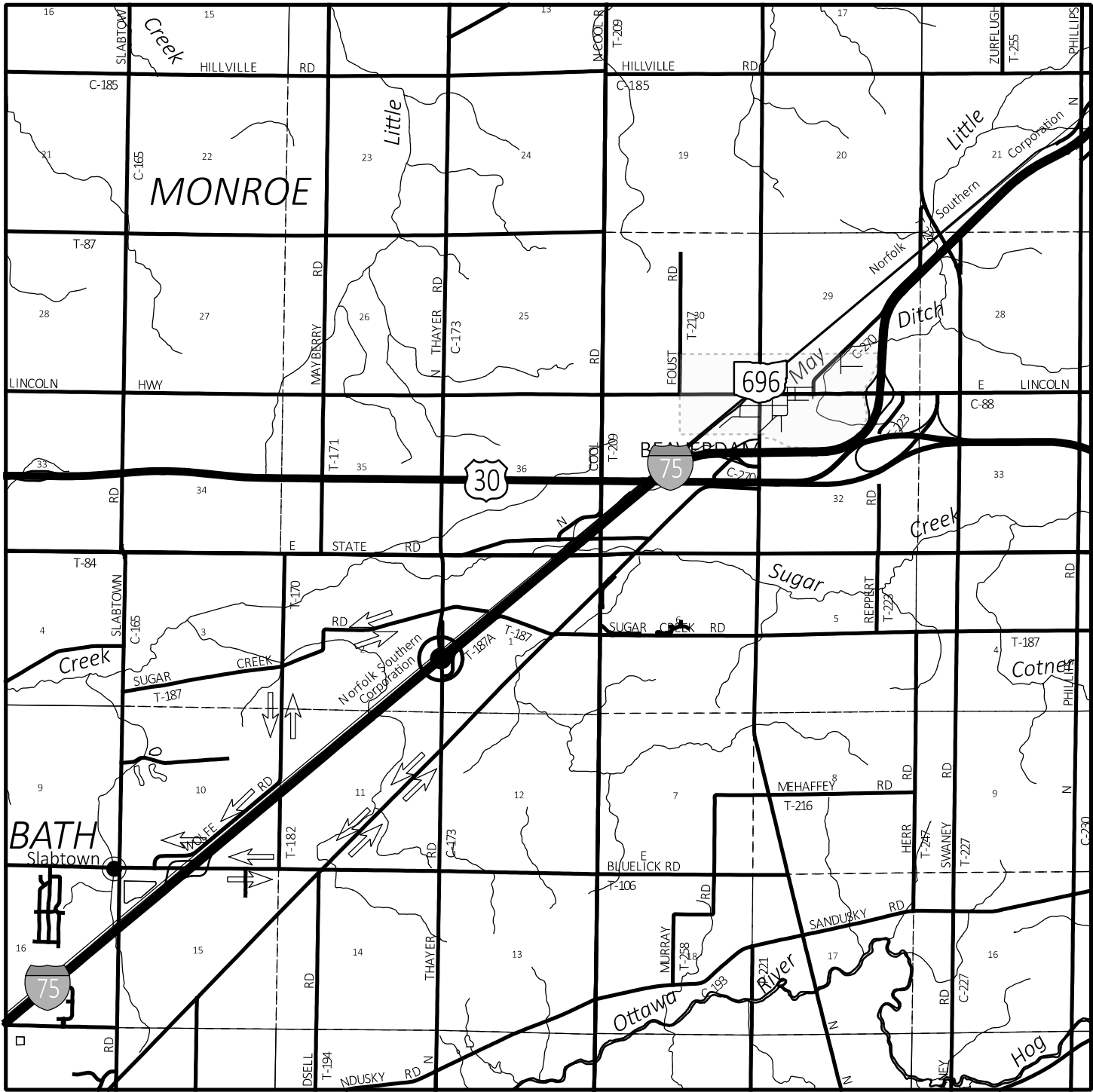
NOTIFICATION TIME TABLE

<u>ITEM</u>	<u>DURATION OF CLOSURE</u>	<u>NOTICE DUE TO PERMITS & PIO</u>
	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
ROAD CLOSURES	>= 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RE-STRICKIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES		14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

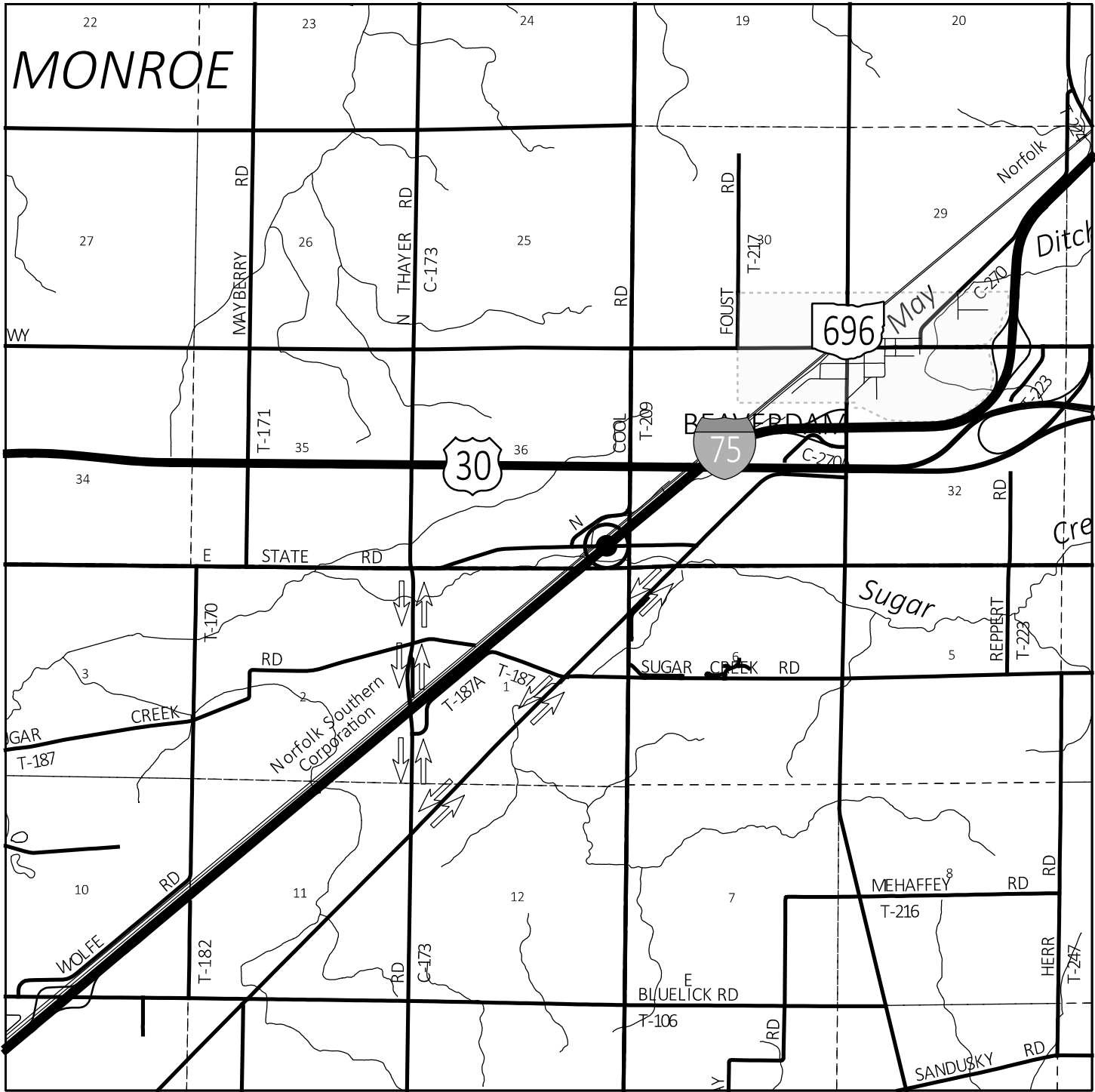
ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

LANE VALUE CONTRACT TABLE

LANE VALUE CONTRACT TABLE			
DESCRIPTION OF CRITICAL LANE TO BE MAINTAINED	RESTRICED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
2 LANES OF ALL IR-75 IN EACH DIRECTION	07:00 - 19:00	EACH MINUTE	\$150



STATE DETOUR MAP - ALL-75-13.87
THAYER RD OVER I.R. 75



STATE DETOUR MAP - ALL-75-15.05
STATE RD OVER I.R. 75

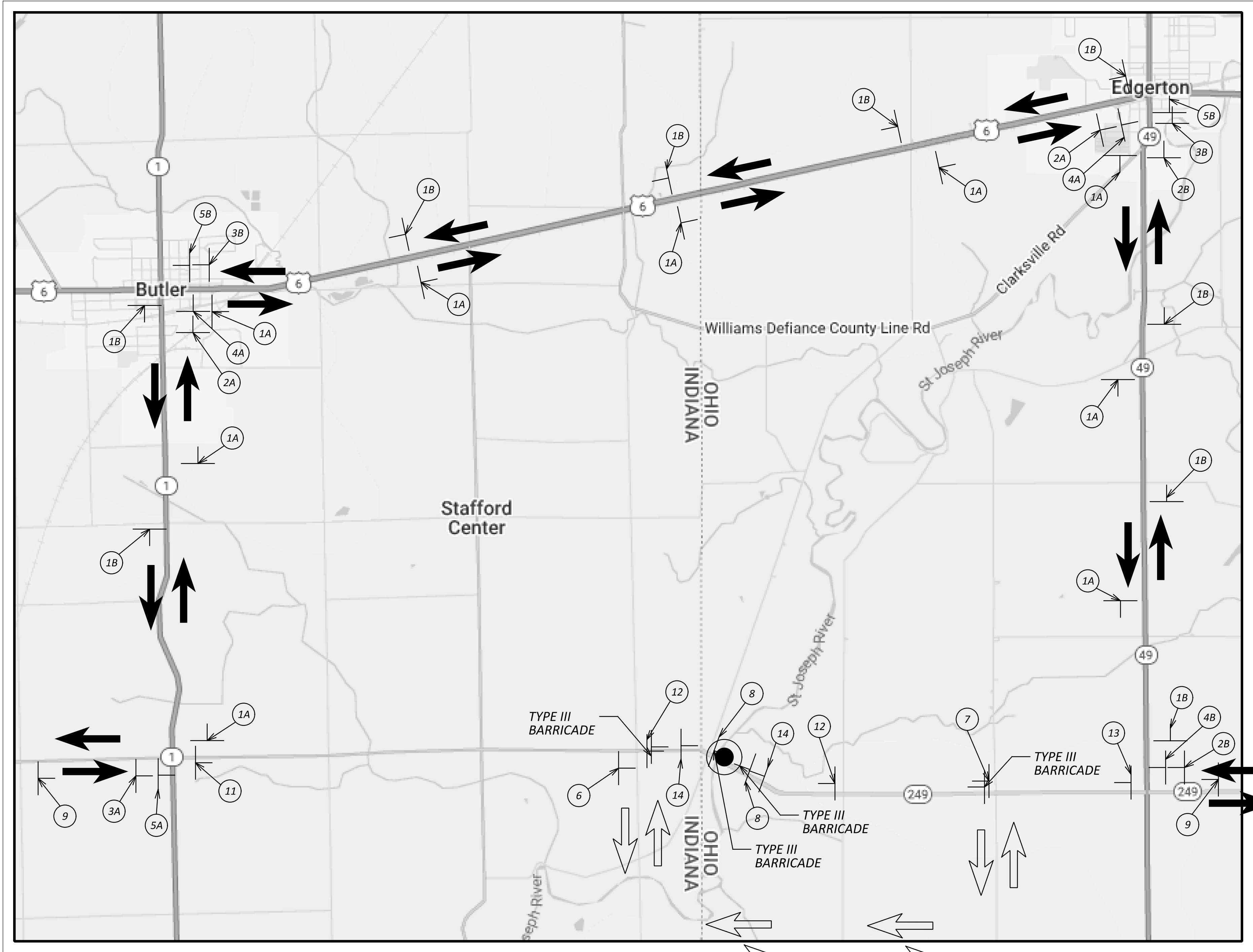


- NOTE:
- LOCATION OF STRUCTURE
 - DESIGNATED LOCAL DETOUR
 - OFFICIAL SIGNED DETOUR



- INTERSTATE HIGHWAY
- FEDERAL ROUTES
- STATE ROUTES
- COUNTY & TOWNSHIP ROADS
- OTHER ROADS





STATE DETOUR MAP

1
DETOUR
249
M1-5-24-3

2
DETOUR
249
M1-5-24-3

3
DETOUR
249
M1-5-24-3

4
DETOUR
249
M1-5-24-3

5
DETOUR
249
M1-5-24-3

6
ROAD CLOSED
LOCAL TRAFFIC ONLY
DETOUR
M4-10R-48

7
ROAD CLOSED
LOCAL TRAFFIC ONLY
DETOUR
M4-10L-48

8
ROAD CLOSED
R11-2-48

9
DETOUR AHEAD
W20-2-36

11
ROAD CLOSED
3.5 MILES AHEAD
LOCAL TRAFFIC ONLY
R11-3a-60

12
ROAD CLOSED
0.5 MILES AHEAD
LOCAL TRAFFIC ONLY
R11-3a-60

13
ROAD CLOSED
2.85 MILES AHEAD
LOCAL TRAFFIC ONLY
R11-3a-60

14
ROAD CLOSED
1000FT
W20-3-36

A
EAST
M3-2-24

B
WEST
M3-4-24

R11-2
ROAD CLOSED

R11-2
ROAD CLOSED


Type III Barricades Per MT-101.60

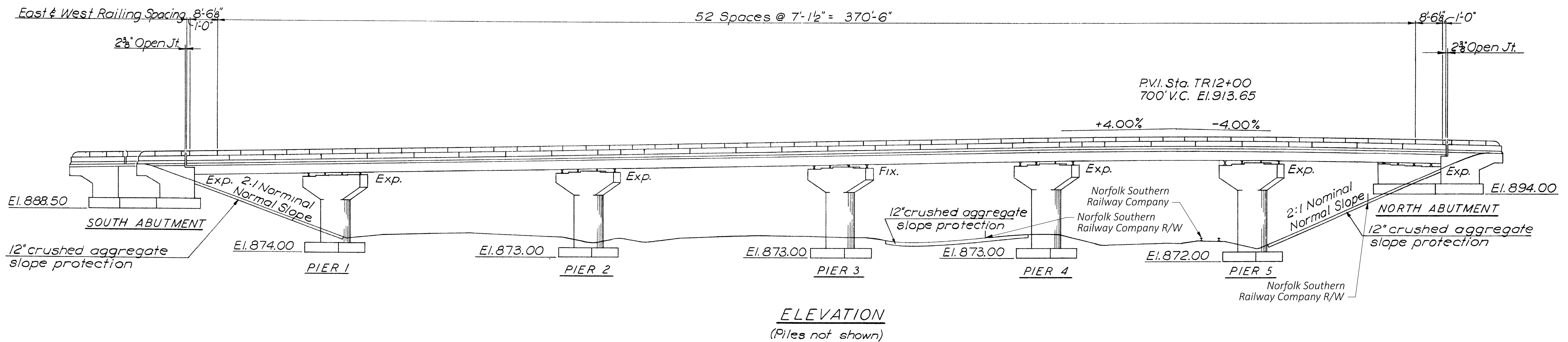
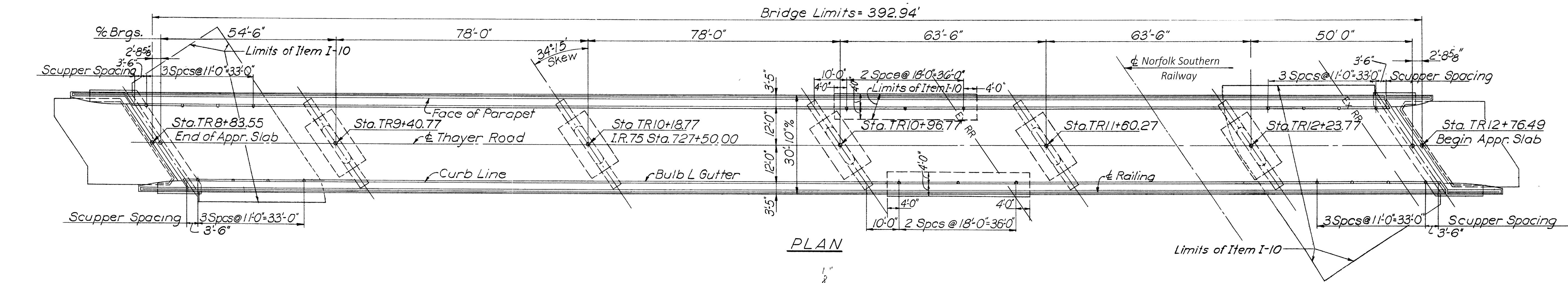
LOCAL DETOUR ROUTE

OFFICIAL SIGNED DETOUR ROUTE

PROJECT SITE LOCATION

LEGEND

GENERAL SUMMARY	
DESIGN AGENCY	
	
DESIGNER	
KRH	
REVIEWER	
XXX MM-DD-YY	
PROJECT ID	
119050	
SHEET	TOTAL
P.9	18



NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.

GENERAL PLAN
ALL-75-13.87
THAYER RD OVER IR-75

SFN
0202495

DESIGN AGENCY



DESIGNER CHECKER

KRH

REVIEWER

XXX XXX

PROJECT ID

119050

SUBSET TOTAL

1 3

SHEET TOTAL

P.10 18



DETAILS SHOWN ARE TAKEN FROM EXISTING
PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.

PROPOSED WORK

*PAINT STRUCTURAL STEEL BEAMS, CROSS FRAMES,
END FRAMES AND BEARINGS.*

EXISTING STRUCTURE

TYPE: CONT. STEEL BEAMS WITH REINF. CONC. DECK
AND REINF. CONC. SUBSTRUCTURE.

SPANS: 54'-6" / 78' / 78' / 63'-6" / 63'-6" / 50'
c/c BEARINGS

ROADWAY: 24' - 0" f/f OF 2'-3" SAFETY CURBS

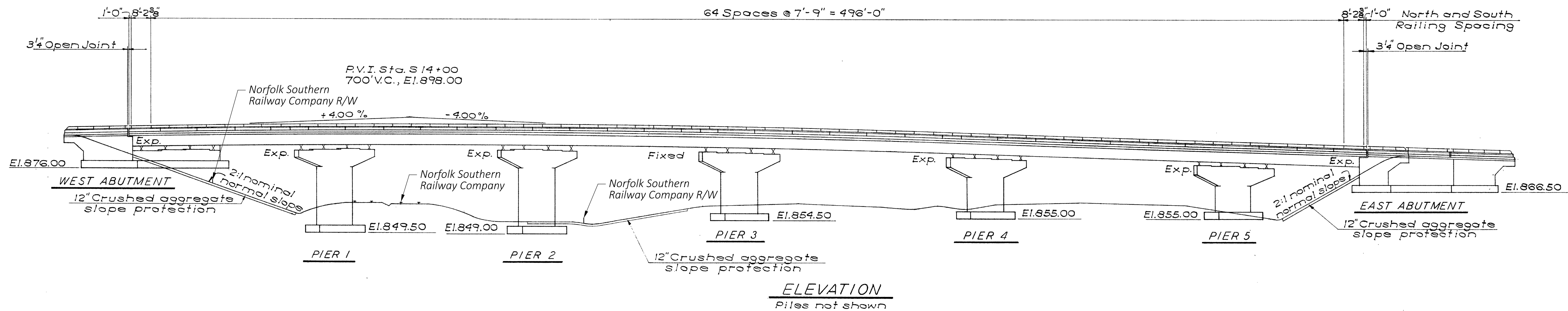
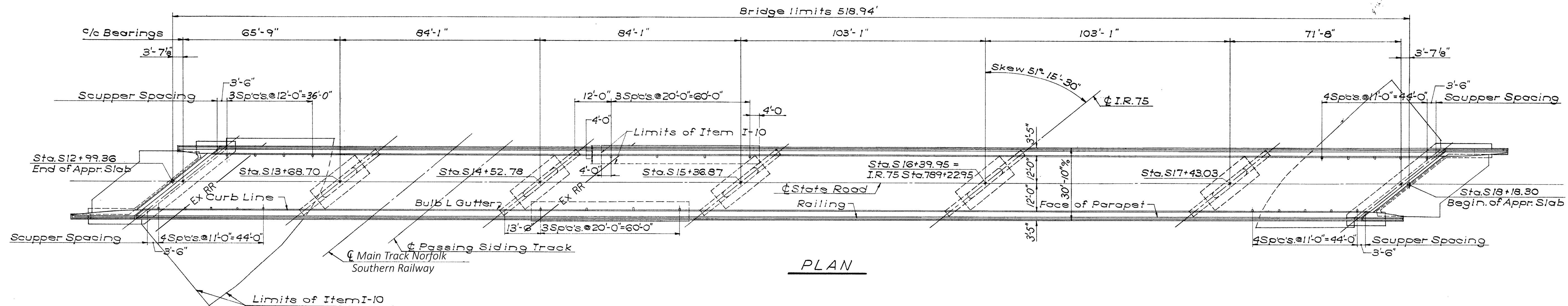
SKEW: 34°-15'-00" RE

APPROACH SLABS: AS-1-54 REVISED 7-5-62 (25' LONG)

ALIGNMENT: Tangent

DATE BUILT: 1964

DISPOSITION: TO BE REHABILITATED



NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

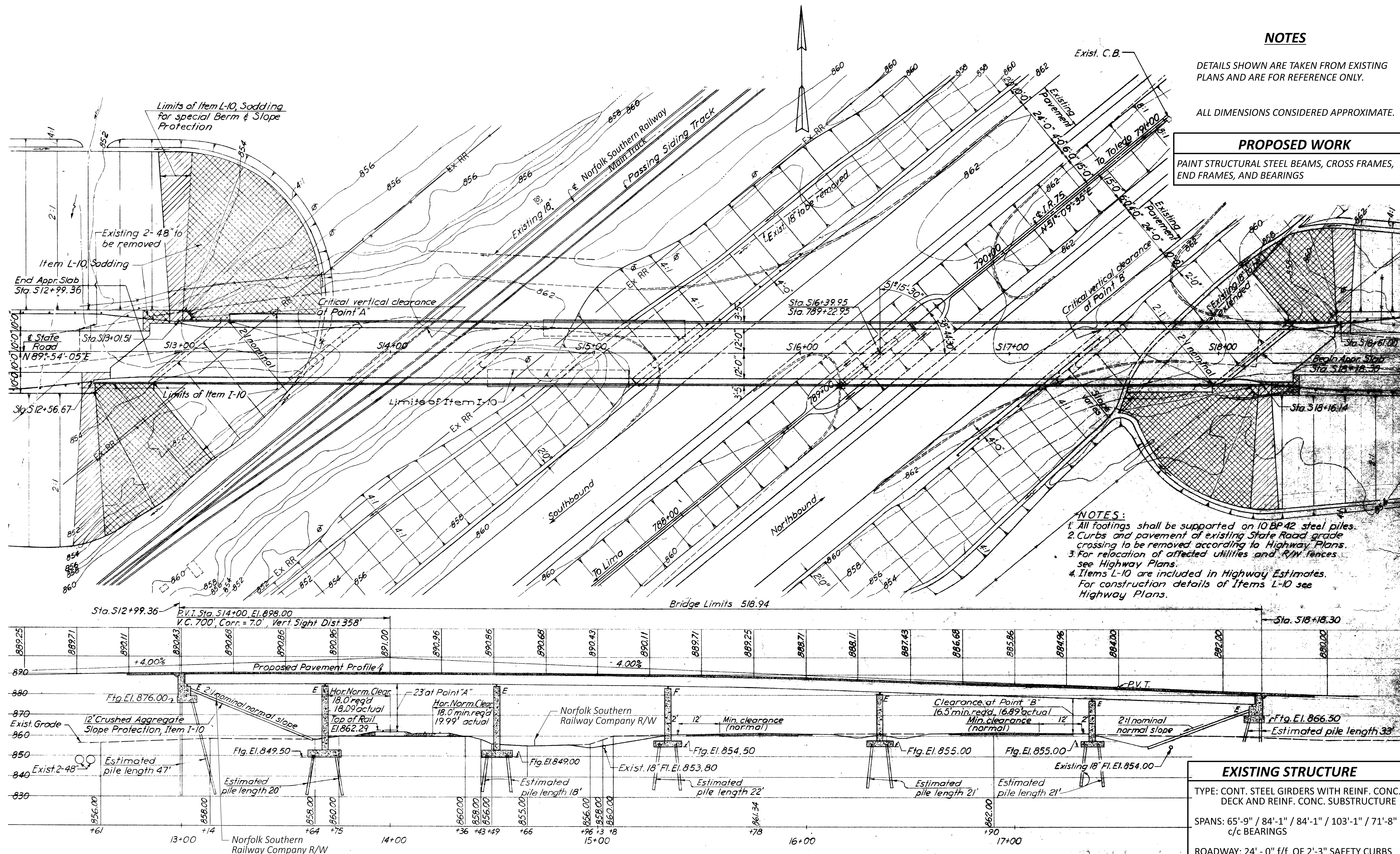
ALL DIMENSIONS CONSIDERED APPROXIMATE.

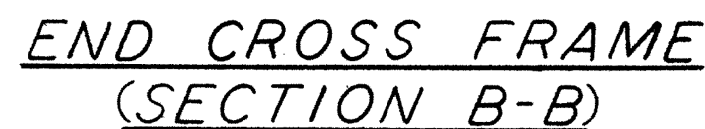
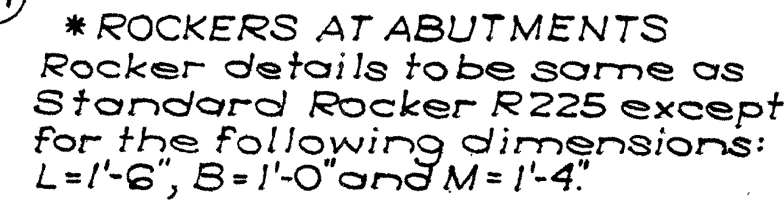
GENERAL PLAN
 ALL-75-15.05
 STATE RD OVR IR-75

SFN
 0202614
 DESIGN AGENCY



DESIGNER	CHECKER
KRH	
REVIEWER	
XXX	XXX
PROJECT ID	
119050	
SUBSET	TOTAL
1	3
SHEET	TOTAL
P.13	18





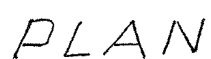
NOTE ON END DAM:
Beveled shim plate is required under
L8x4x $\frac{3}{4}$ at east abutment. For detail
see abutment details drawings.



NOTES

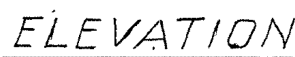
DETAILS SHOWN ARE TAKEN FROM EXISTING
PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.



DETAILS SHOWN ARE TAKEN FROM EXISTING
PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.



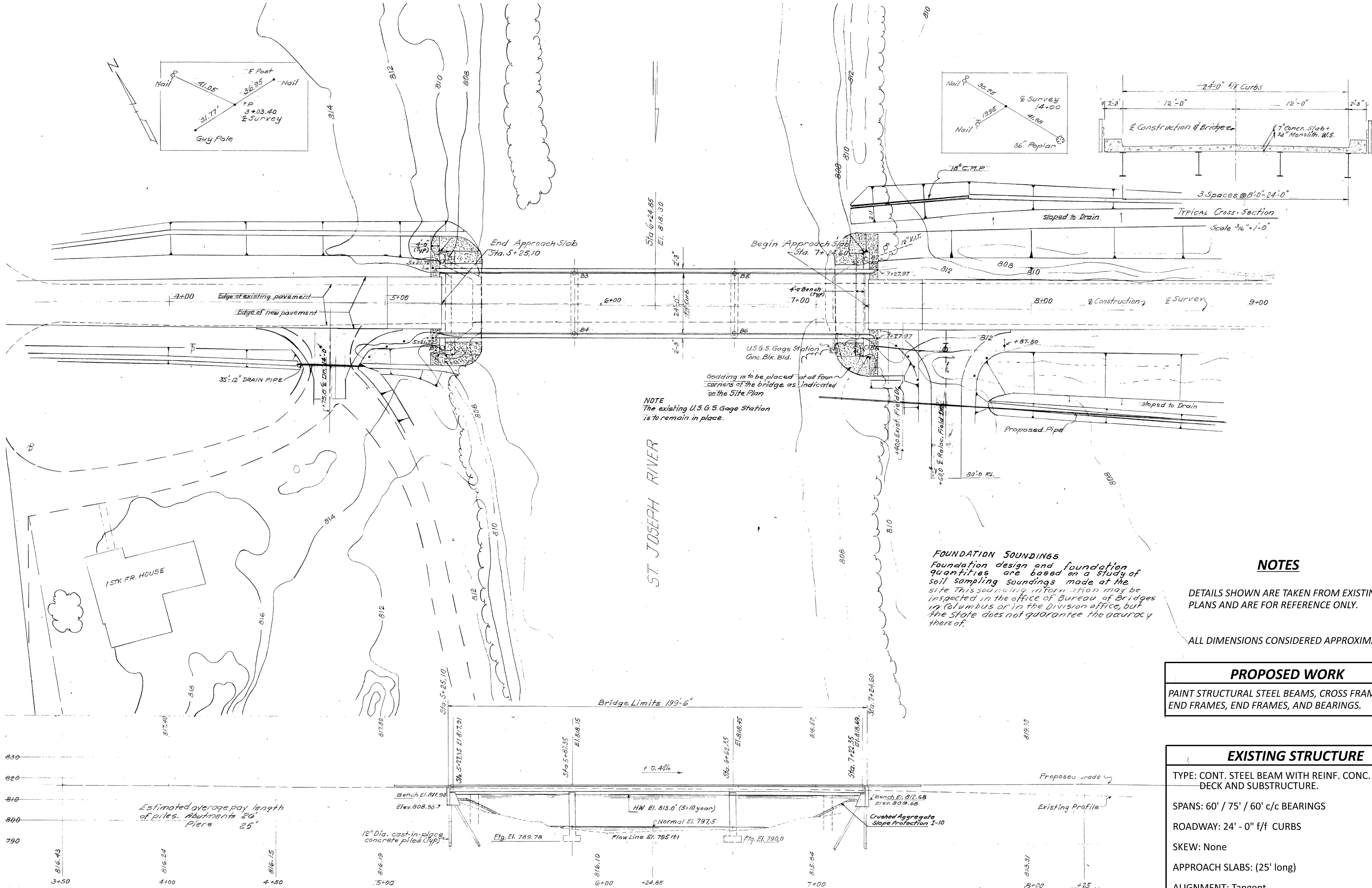
Design Specifications: This structure conforms to the requirements of "Design Specifications for Highway Structures" of the State of Ohio, Department of Highways, dated 9-1-1957 together with revisions thereof dated 2-21-59.

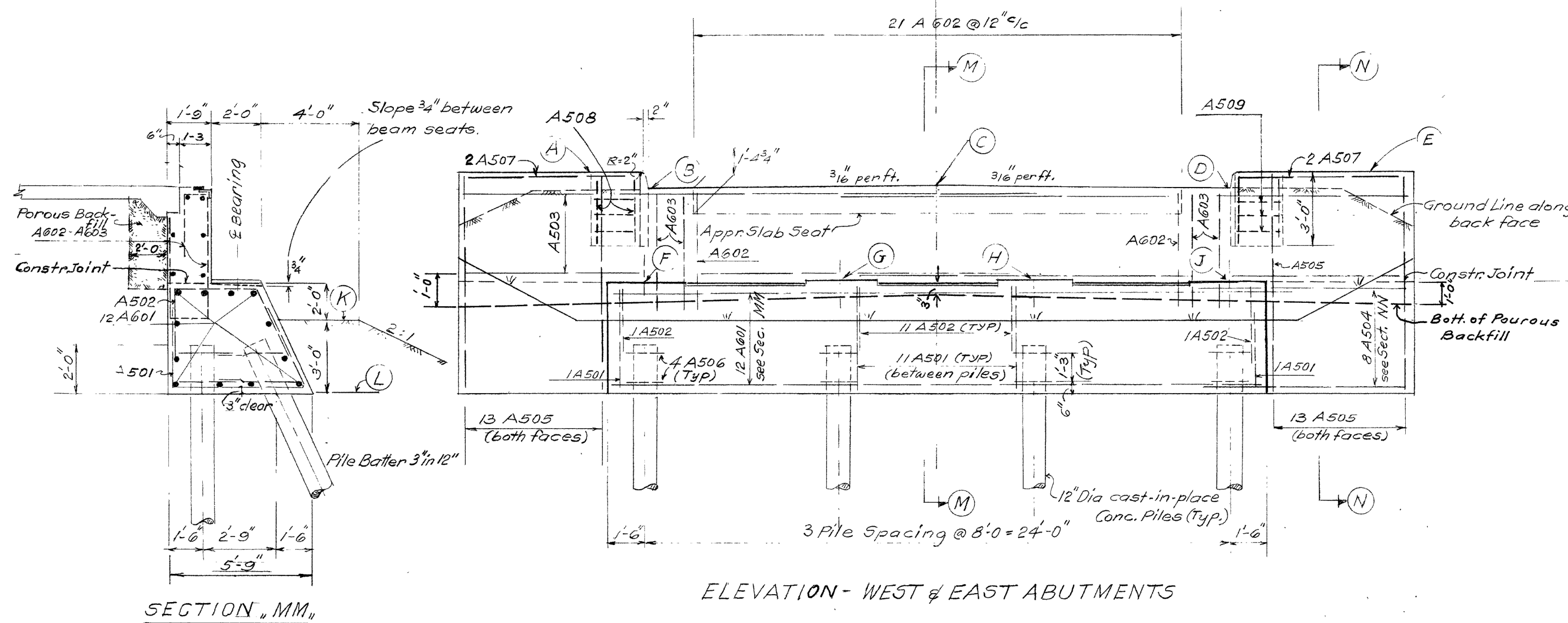
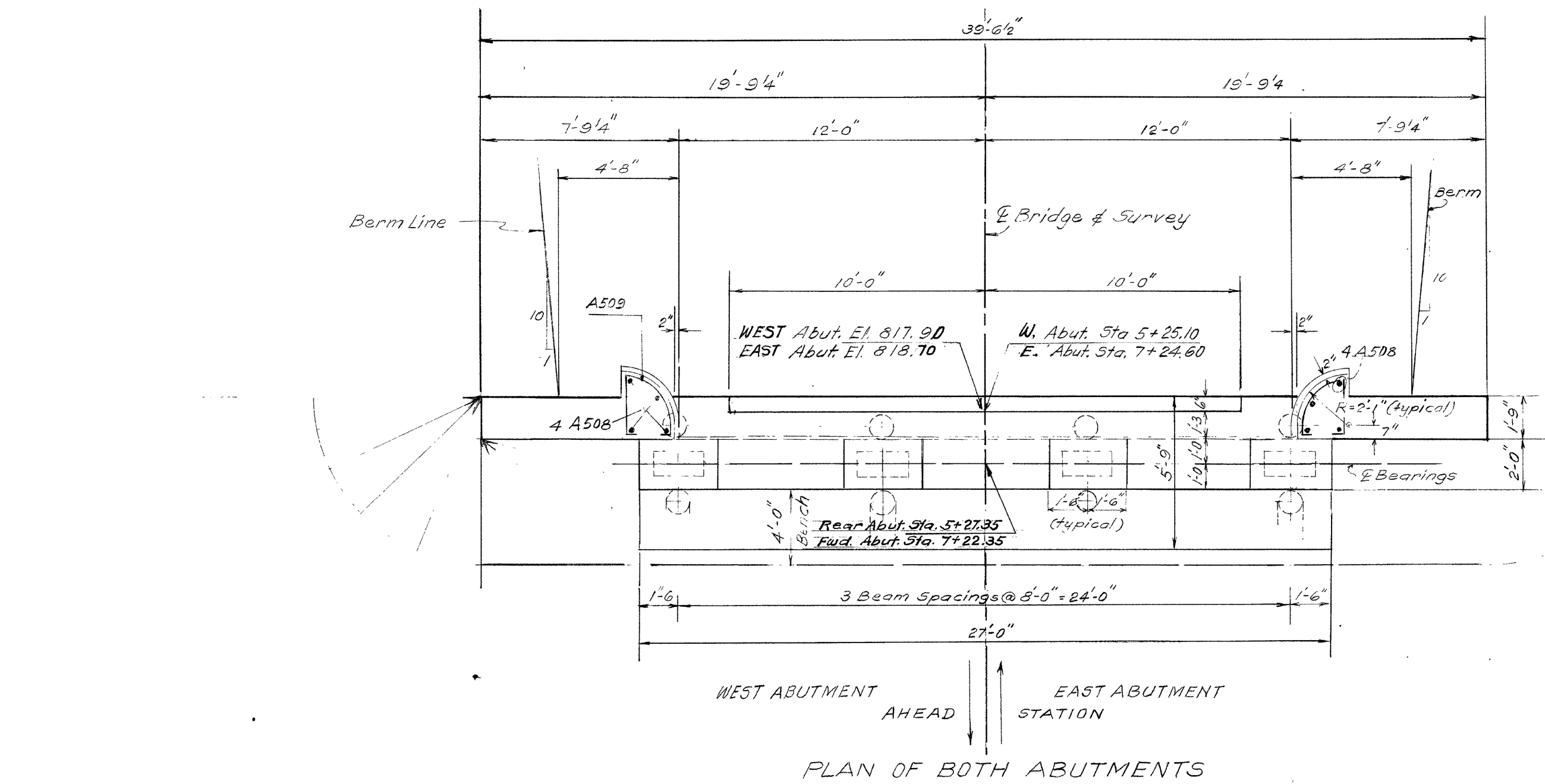
Reference shall be made to sheets 1,2,3 and 4 of Std. Dwg. CSB-2-56 revised 2-2-59 and supplemental specification S/01, dated 12-2-59.

Concrete: Superstructure & Pier walls shall be Class "C" concrete. Abutments and pier footings shall be Class "E" concrete.

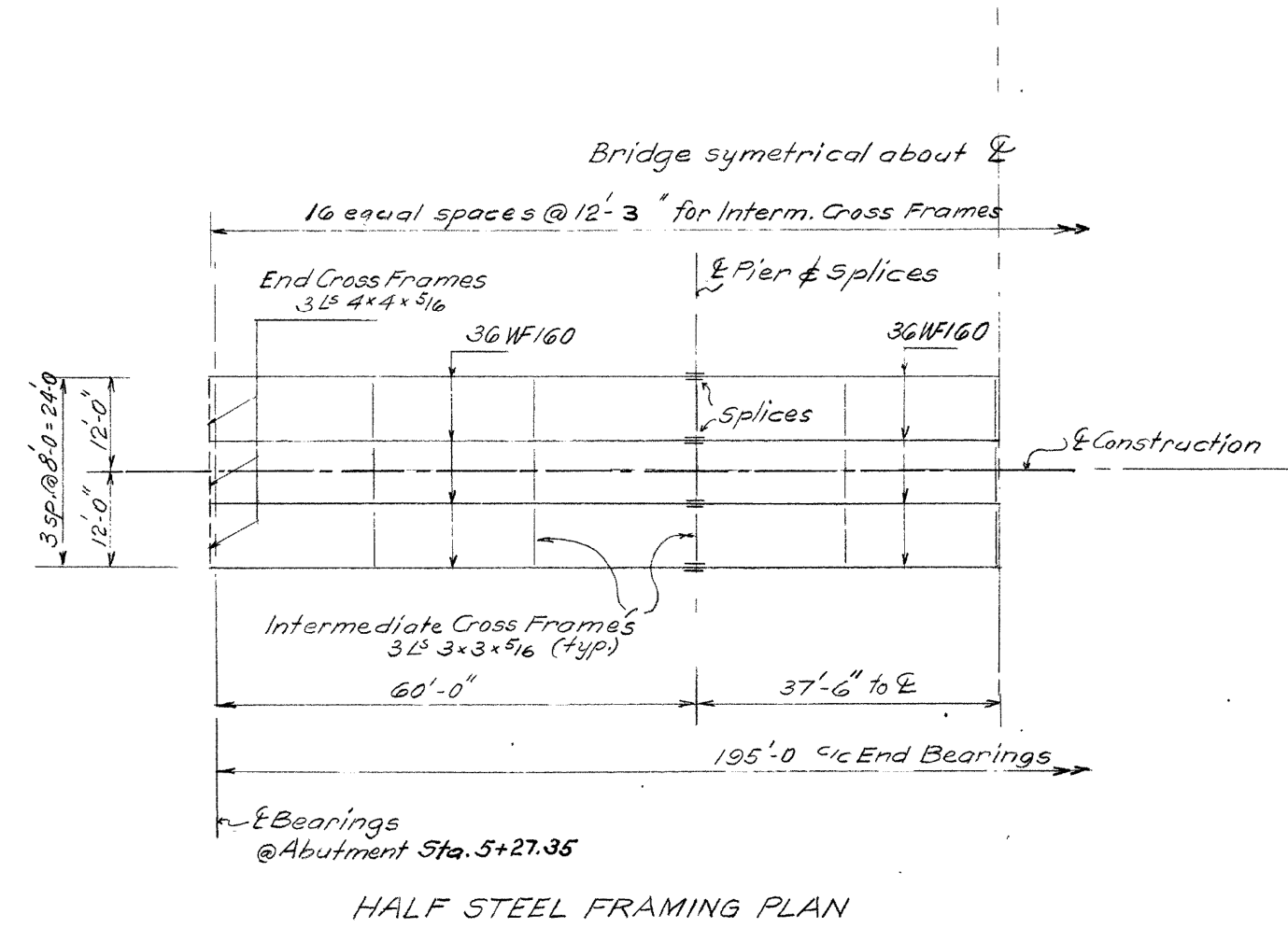
EXCAVATION QUANTITY for the abutments, in addition to that outlined in Sec. E-2.09, includes the removal of material bounded by the proposed bench, by the front vertical plane described in Sec. E-2.09 and by the finished slope of the cut.

A vertical strip on the left side of the page. At the top, it features a stylized sailboat logo inside a circle. Below the logo is a table with project details. The table has two columns: 'DESIGNER' and 'CHECKER' in the first row, 'REVIEWER' in the second row, and 'PROJECT ID' in the third row. The values are 'KRH' and 'XXX' for the first row, 'XXX' and 'XXX' for the second row, and '19050' for the third row. Below the table, there are two more rows: 'SUBSET' and 'TOTAL' with values '1' and '3', and 'SHEET' and 'TOTAL' with values 'P.16' and '18'.





ABUTMENT ELEVATION						
Elevation	A = E	B = D	C	F = J	G = H	K
WEST ABUTMENT (REAR)	813.57	817.71	817.90	813.90	814.02	811.90
EAST ABUTMENT (FWD)	813.35	818.51	818.70	814.68	814.80	812.68



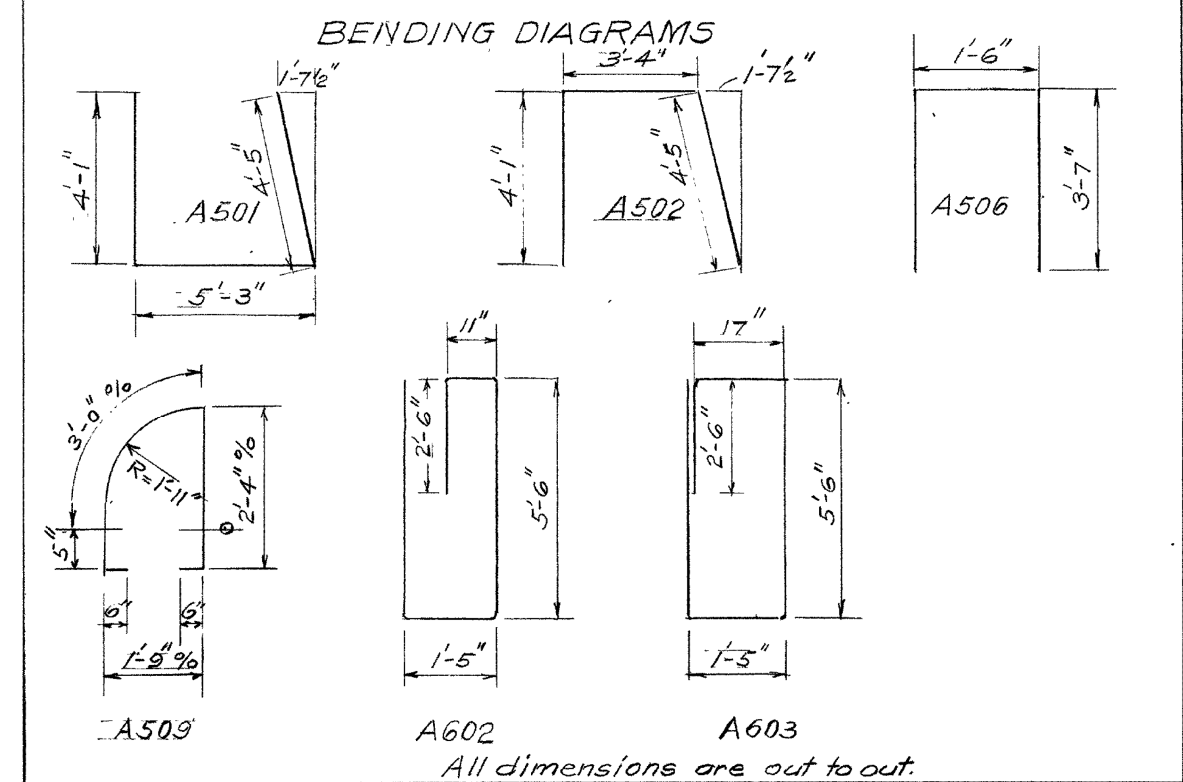
REPLACEMENT BARS				
MARK	NO	LENGTH	WT	SHR
RE 1001	1	7'-2"	-	S
RE 801	1	6'-6"	-	S
RE 601	2	5'-11"	-	S
RE 501	1	5'-7"	-	S
RE 401	1	5'-3"	-	S

Replacement Bars
If reinforcing bars are fabricated from stock which has been previously tested and approved by the Ohio Highway Testing Laboratory, test samples as provided in Sec. 5-402 need not be furnished and replacement bars will not be required.

DEFLECTIONS & CAMBER				
	Exten. Bms.		Inter. Bms.	
	End - Middle Span	Middle - End Span	End - Middle Span	Middle - End Span
Deflection due to Steel Weight	1/16	1/8	1/16	1/8
" " " to Remaining DL	3/8	1/2	5/16	7/16
Sum of Deflections	7/16	5/8	3/8	9/16
Required Camber	0	0	0	0

BAR SCHEDULE FOR SUPERSTRUCTURE				
MARK	NO	LENGTH	WT	SHR
S 501	336	28'-2"	9872	S
S 601	336	28'-2"	14,217	S
S 602	294	34'-3"	15,124	S
S 603	44	30'-0"	1,983	S
S 401	262	2'-9"	981	B
S 402	262	4'-11"	861	B

BAR SCHEDULE FOR ABUTMENTS					
MARK	NO	W. ABUT.	E. ABUT.	LENGTH	WEIGHT
A 501	35	35	13'-9"	1,004	B
A 502	35	35	11'-10"	864	B
A 503	12	12	21'-1"	528	S
A 504	16	16	8'-7"	286	S
A 505	52	52	9'-5"	1,022	S
A 506	16	16	8'-8"	289	B
A 601	12	12	26'-8"	961	S
A 602	21	21	15'-10"	999	B
A 603	4	4	16'-4"	196	B
A 507	4	4	7'-5"	62	S
A 508	8	8	2'-10"	47	S
A 509	6	6	6'-9"	85	B



NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.