

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS, SECTIONS 102.05, 105.02, AND 513.04*. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

EXISTING BRIDGE PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 1 OFFICE IN LIMA, OHIO DURING NORMAL BUSINESS HOURS.

PAINTING OF STRUCTURAL STEEL, AS PER PLAN

THE STRUCTURAL STEEL FOR EACH BRIDGE SHALL BE PAINTED THE FOLLOWING COLORS:

ALL-75-13.87 - GREEN (AMS-595A, 14277)
ALL-75-15.05 - GREEN (AMS-595A, 14277)
DEF-249-0.10 - GREEN (AMS-595A, 14277)

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

SCUPPER, LENGTHENING, AS PER PLAN

PORTIONS OF SCUPPERS THAT ARE CORRODED THROUGH ARE TO BE CUT OFF AT A 90 DEGREE ANGLE TO THE SCUPPER BEFORE PAINTING. AFTER PAINTING SCUPPERS ARE TO BE LENGTHENED TO AT LEAST 8" BELOW THE BOTTOM OF THE BOTTOM FLANGE WITH CLAMP ON EXTENSIONS.

NOTIFICATION

THE CONTRACTOR SHALL NOTIFY THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 1 CONSTRUCTION ENGINEER TWO (2) WEEKS PRIOR TO BEGINNING WORK, ALONG WITH THE COUNTY ADMINISTRATORS USING THE LIST OF ODOT COUNTY PHONE NUMBERS BELOW:

ALLEN COUNTY	419-222-9905
DEFIANCE COUNTY	419-782-2826

PERSONAL PROTECTION EQUIPMENT (PPE)

THE CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF SECTIONS XXIV AND XXXIV OF THE OHIO DEPARTMENT OF TRANSPORTATION SAFETY & HEALTH STANDARD OPERATING PROCEDURE 220-006(SP) EFFECTIVE: NOVEMBER 1, 2018 (EXCEPT AS AMENDED BELOW) AND ALL SUBSEQUENT UPDATES POSTED AT THE FOLLOWING WEBSITE:

<https://www.transportation.ohio.gov/about-us/policies-and-procedures/220-006-sp>

AMENDMENTS TO THE REQUIREMENTS OF THIS DOCUMENT ARE:

XXIV. HEAD PROTECTION (HARD HATS)

ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR APPROPRIATE HEAD PROTECTION. ALL HARD HATS MUST MEET OR EXCEED ANSI Z89.1-2009 TYPE 1, CLASS E-G REQUIREMENTS.

XXXIV. SAFETY APPAREL AND VEST (HIGH VISIBILITY)

ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR A HIGH-VISIBILITY SAFETY VEST THAT MEETS THE PERFORMANCE CLASS II OR CLASS III REQUIREMENTS OF THE ANSI/ISEA 107-2015 PUBLICATION ENTITLED "AMERICAN NATIONAL STANDARD FOR HIGH-VISIBILITY SAFETY APPAREL AND ACCESSORIES".

WORKERS MAY WEAR AN ANSI CLASS II OR ANSI CLASS III APPROVED RAIN SUIT, JACKET OR OTHER APPAREL WITHOUT A SAFETY VEST OVER IT.

COORDINATION OF CONTRACTORS

SINCE THE MAINTENANCE OF TRAFFIC AND WORK ON THIS PROJECT MAY OVERLAP OTHER PROJECTS, IT IS ESSENTIAL THAT EACH CONTRACTOR CONDUCT THEIR WORK IN COOPERATION WITH EACH OTHER IN SUCH A MANNER AS NOT TO HINDER THE PROGRESS OR COMPLETION OF THE WORK BEING PERFORMED BY THE OTHER CONTRACTOR.

NORFOLK SOUTHERN RAILWAY COMPANY

ALL WORK TO BE PERFORMED ON, OVER, UNDER, OR ADJACENT TO THE RAILROAD RIGHT-OF-WAY SHALL COMPLY WITH THE NORFOLK SOUTHERN RAILWAY COMPANY ("RAILROAD", "NSR" or "NS") PUBLIC PROJECTS MANUAL (APPENDIX E, SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS, AND APPENDIX H1, OVERHEAD GRADE SEPARATION DESIGN CRITERIA). WHEN IN CONFLICT WITH OTHER PROJECT SPECIFICATIONS, THE MOST STRINGENT ONE SHALL APPLY.

"ONE CALL" SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL & COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD'S REPRESENTATIVE TWO DAYS IN ADVANCE OF WORK AT LOCATIONS WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE THE RAILROAD'S UNDERGROUND FACILITIES. UPON REQUEST FROM THE CONTRACTOR OR SPONSOR, RAILROAD PERSONNEL WILL LOCATE AND PAINT, MARK, OR FLAG THE RAILROAD'S UNDERGROUND FACILITIES. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE FACILITIES. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD FACILITY, THE CONTRACTOR SHALL COORDINATE WITH THE RAILROAD TO HAVE THE FACILITY POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION. THE FACILITY SHALL BE PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF THE RAILROAD'S REPRESENTATIVE. (SEE NS PUBLIC PROJECTS MANUAL, APPENDIX E, SECTION 3.D)

THE CONTRACTOR SHALL NOT COMMENCE ANY WORK ON RAILROAD RIGHT-OF-WAY UNTIL COMPLIANCE IS MET WITH THE CONDITIONS PRESENTED ON NS PUBLIC PROJECTS MANUAL (SEE APPENDIX E, NORFOLK SOUTHERN - SPECIAL PROVISIONS FOR PROTECTION OF RAILWAY INTERESTS). THE CONTRACTOR SHALL SO ARRANGE AND CONDUCT HIS WORK THAT THERE WILL BE NO INTERFERENCE WITH RAILROAD'S OPERATIONS. WHENEVER WORK IS LIABLE TO AFFECT THE OPERATIONS OR SAFETY OF TRAINS, THE METHODS OF DOING SUCH WORK SHALL FIRST BE SUBMITTED TO THE RAILROAD ENGINEER FOR APPROVAL, BUT SUCH APPROVAL SHALL NOT RELIEVE THE CONTRACTOR FROM ANY LIABILITY. RIGHT-OF-WAY AND/OR SECURITY FENCE SHALL BE PROVIDED AS DIRECTED BY THE NS PUBLIC PROJECT ENGINEER.

THE CONTRACTOR SHALL AT NO TIME ENTER THE RAILROAD RIGHT-OF-WAY WITH PERSONNEL OR EQUIPMENT UNTIL COMPLIANCE HAS BEEN MET WITH THE REQUIREMENTS PRESENTED IN THE NS PUBLIC PROJECTS MANUAL INCLUDING APPROVED RAILROAD PROTECTIVE INSURANCE, A CONTRACTOR RIGHT OF ENTRY, PROTECTIVE SERVICES ("FLAGGING"), AND RECEIVED FROM NS WRITTEN AUTHORIZATION TO PROCEED. (SEE APPENDIX E OF THE MANUAL, "NORFOLK SOUTHERN - SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS"). THE CONTRACTOR SHALL SO ARRANGE AND CONDUCT WORK SUCH THAT THERE WILL BE NO INTERFERENCE WITH THE RAILROAD'S OPERATIONS. WHENEVER WORK IS LIABLE TO AFFECT THE OPERATIONS OR SAFETY OF TRAINS, THE METHODS OF DOING SUCH WORK SHALL FIRST BE SUBMITTED TO THE RAILROAD ENGINEER FOR APPROVAL, BUT SUCH APPROVAL SHALL NOT RELIEVE THE CONTRACTOR FROM ANY LIABILITY. RAILROAD PROTECTIVE SERVICES ("FLAGGING") AND/OR SECURITY FENCE SHALL BE PROVIDED AS DIRECTED BY THE RAILROAD ENGINEER OR HIS FIELD REPRESENTATIVE.

THE CONTRACTOR IS REQUIRED TO SUBMIT A CONTAINMENT PLAN TO NORFOLK SOUTHERN DEPICTING THE PAINT PROCESS AND HOW CONTAMINATION OF NORFOLK SOUTHERN RIGHT-OF-WAY WITH PAINT/DEBRIS SHALL BE PREVENTED.

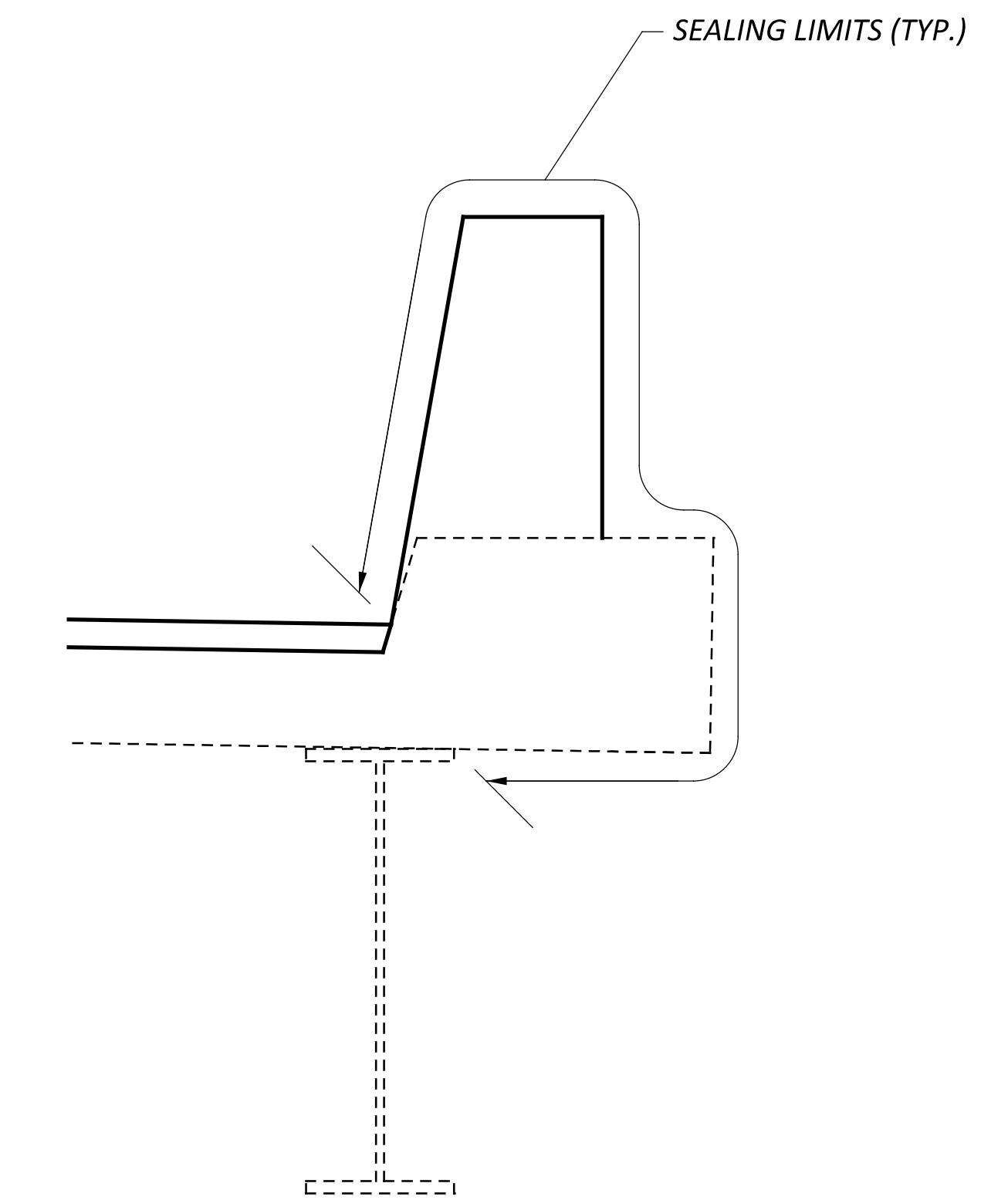
IF ANY UTILITIES MUST BE INSTALLED OR RELOCATED AS PART OF THIS PROJECT AT LOCATIONS: ALL-75-13.87 AND ALL-75-15.05 IN THE NORFOLK AND SOUTHERN RAILROAD RIGHT-OF-WAY THE FOLLOWING NOTE MUST BE COMPLETED PRIOR TO SUCH WORK OCCURRING: ALL UTILITY INSTALLATIONS OR RELOCATIONS THAT ARE REQUIRED IN CONJUNCTION WITH THIS PROJECT CAN BE INSTALLED OR RELOCATED AS PART OF THE PROJECT PROVIDED THE CONSTRUCTION IS PERFORMED BY THE PROJECT CONTRACTOR OR PROJECT CONTRACTOR'S SUB-CONTRACTOR. HOWEVER, THE UTILITY MUST SUBMIT AN APPLICATION FOR THE INSTALLATION OR RELOCATION TO RAILPROS OR APPROPRIATE HANDLING FOR LICENSE AGREEMENT AND APPLICABLE FEES. FOR UTILITY APPLICATIONS GO TO: [HTTPS://WWW.NORFOLKSOUTHERN.COM/EN/RAIL-DEVELOPMENT-PROPERTY/NS-PROPERTY/PROJECTS-ON-NS-PROPERTY/PIPE-AND-WIRE](https://www.norfolksouthern.com/en/rail-development-property/ns-property/projects-on-ns-property/pipe-and-wire) OR [WWW.NSCORP.COM](http://www.nscorp.com) > RAIL DEVELOPMENT & PROPERTY > REAL ESTATE > OVERVIEW > PIPE, WIRE & FIBER OPTICS

THE RAILROAD MAY BE CONTACTED USING THE FOLLOWING INFORMATION:

ELDRIDGE W. CHAMBERS
ENGINEER PUBLIC IMPROVEMENTS
NORFOLK SOUTHERN CORPORATION
650 PEACHTREE STREET, NW, BOX 45
ATLANTA, GA 30308
(470)-463-6307
Eldridge.Chambers@nscorp.com

SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)

THE DRAWING BELOW SHOWS THE EXTENTS OF THE SEALING OF CONCRETE SURFACES AT DEF-249-0.10. THIS IS BASED ON SEALING BOTH SIDES WITH AN EDGE DISTANCE OF 9.83' FOR 195'. A QUANTITY OF 426 SY HAS BEEN CARRIED TO THE GENERAL SUMMARY.



DESIGN AGENCY
KRH
REVIEWER

PROJECT ID
119050
SHEET TOTAL
P.3 18

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY. THE CONTRACTOR IS REQUIRED TO PROVIDE CLASS A PCMS ONLY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS SHOULD BE LOCATED IN ADVANCE OF THE LANE CLOSURES ON I.R. 75 BRIDGES PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT. THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN,
AS PER PLAN = 10 SNMT

ASSUMING 2 PCMS SIGNS EACH FOR 5 MONTHS

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA: ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND, AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:
THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
THE ACTIVE WORK AREA LATERALLY CLOSEST TO THE OPEN TRAVELED LANE; OR
OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE = 800 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL

AN ESTIMATED QUANTITY OF 30 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.



DESIGNER

KRH

REVIEWER

PROJECT ID

119050

SHEET

TOTAL

P.5

18

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

*AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED
IN THE GENERAL SUMMARY.*

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE

<u>ITEM</u>	<u>DURATION OF CLOSURE</u>	<u>NOTICE DUE TO PERMITS & PIO</u>
	≥ 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
ROAD CLOSURES	≥ 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RE-RESTRICTIONS	≥ 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES		14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

*ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS
REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO
THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.*

LANE VALUE CONTRACT TABLE

Lane Value Contract Table			
Description of Critical Lane to be Maintained	Restricted Time Period	Time Unit	Disincentive \$ per Time Unit
2 Lanes of All IR-75 in Each Direction	07:00 - 19:00	Each Minute	\$150

MAINTENANCE OF TRAFFIC NOTES

DESIGN AGENCY

1

DESIGNER
KBLI

KRH
REVIEW

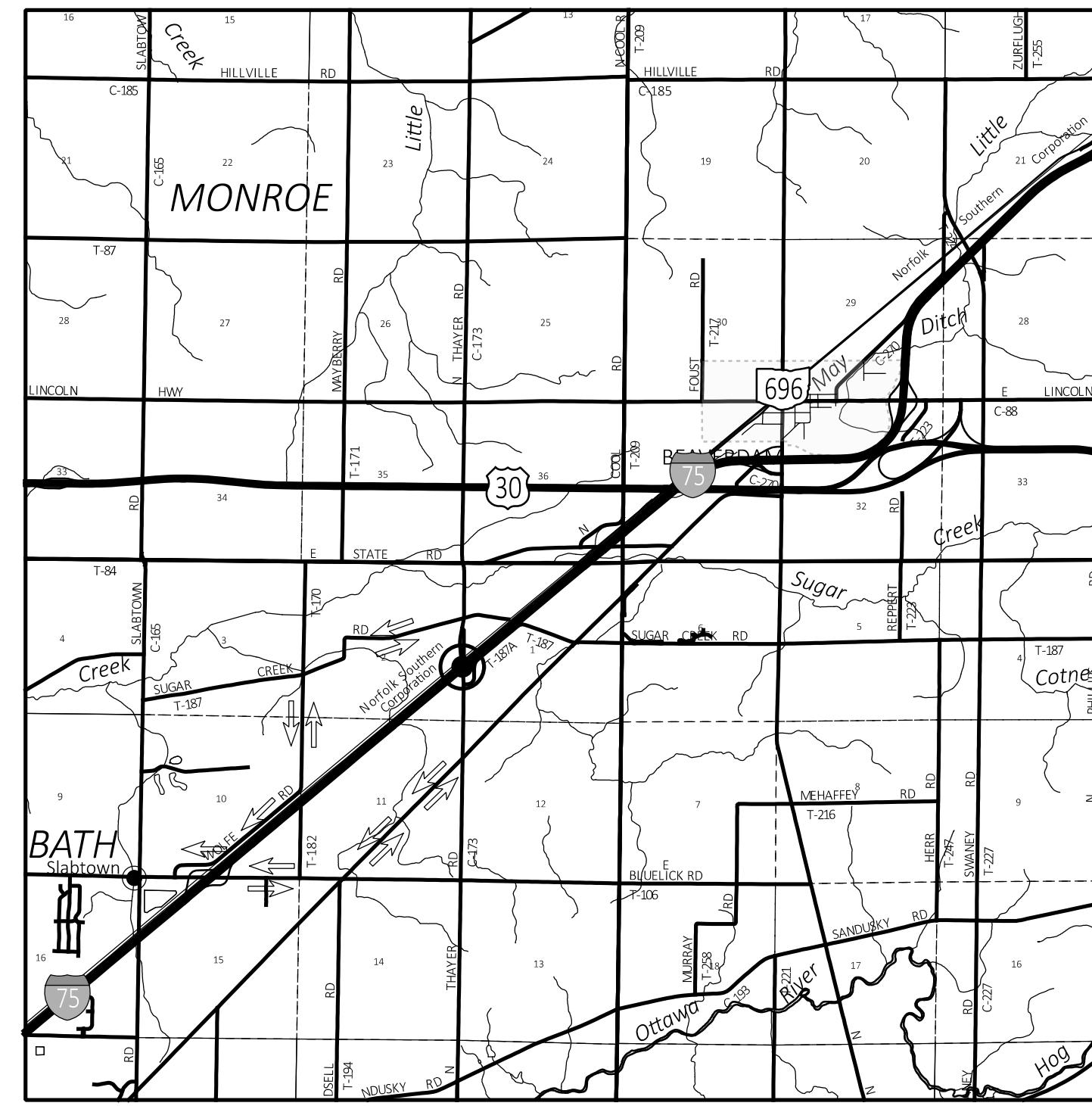
REVIEW

PROJECT ID

11905

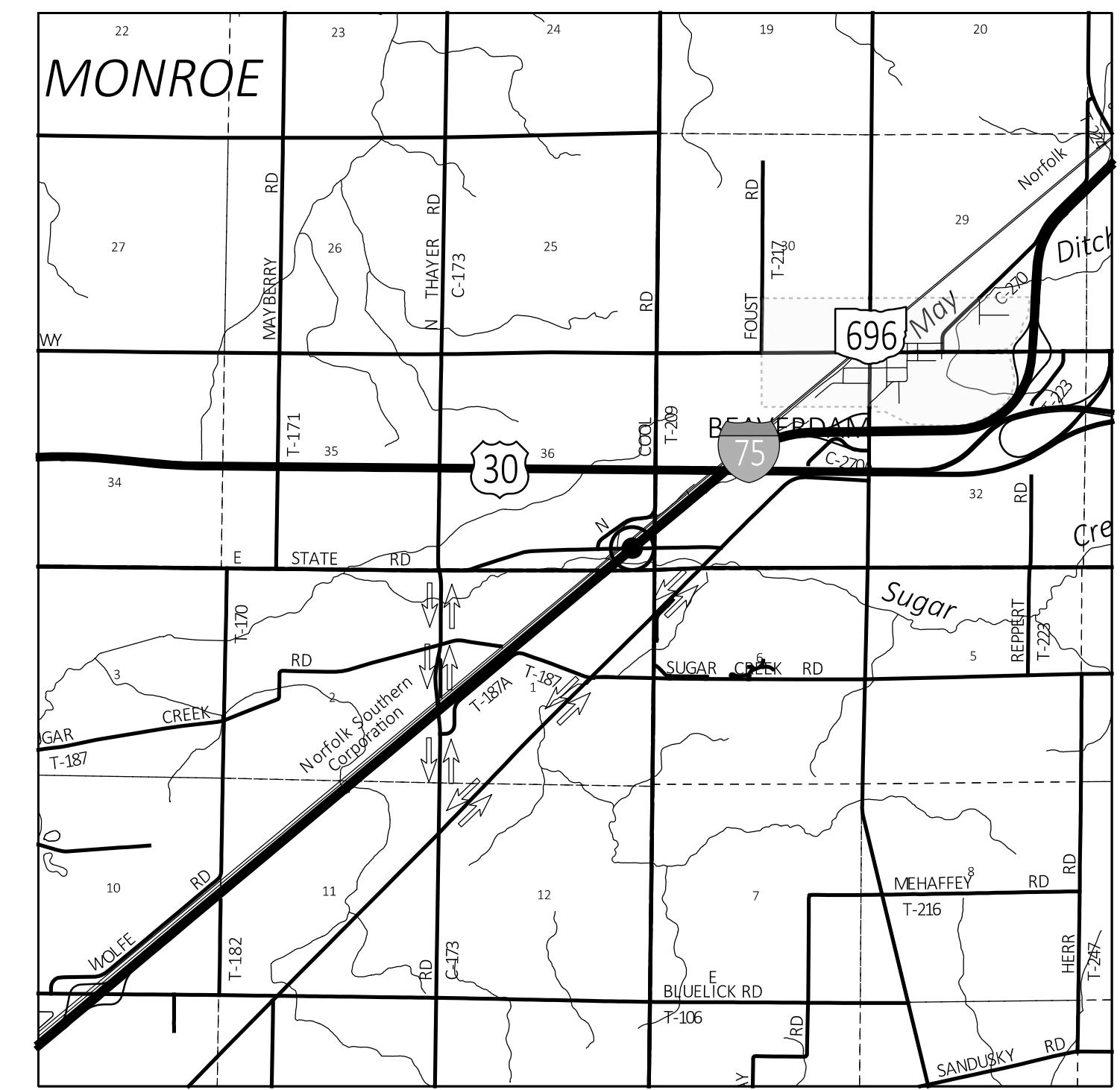
P.6 |

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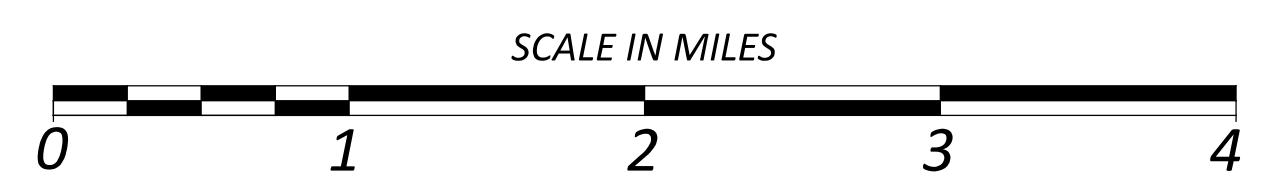
STATE DETOUR MAP - ALL-75-13.87

THAYER RD OVER I.R. 75



STATE DETOUR MAP - ALL-75-15.05

STATE RD OVER I.R. 75



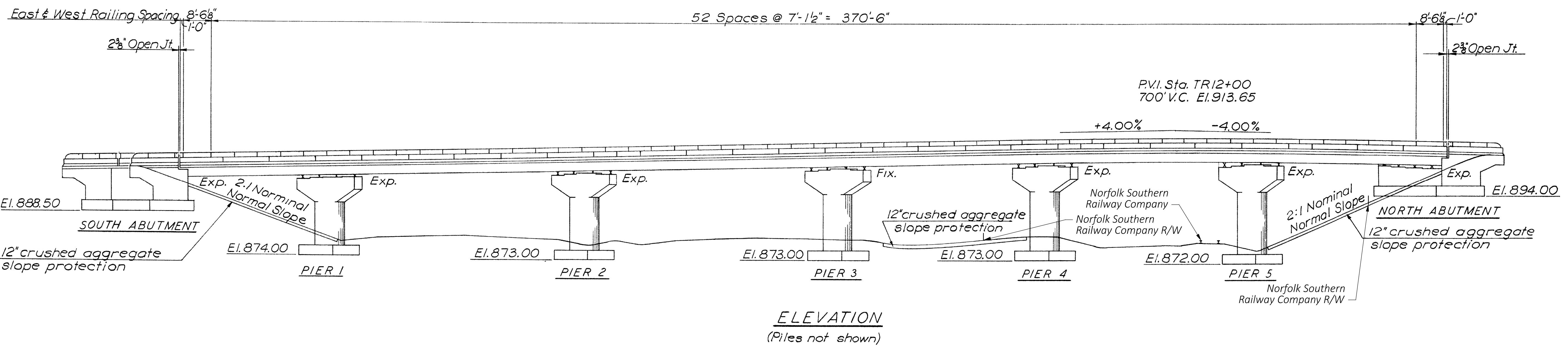
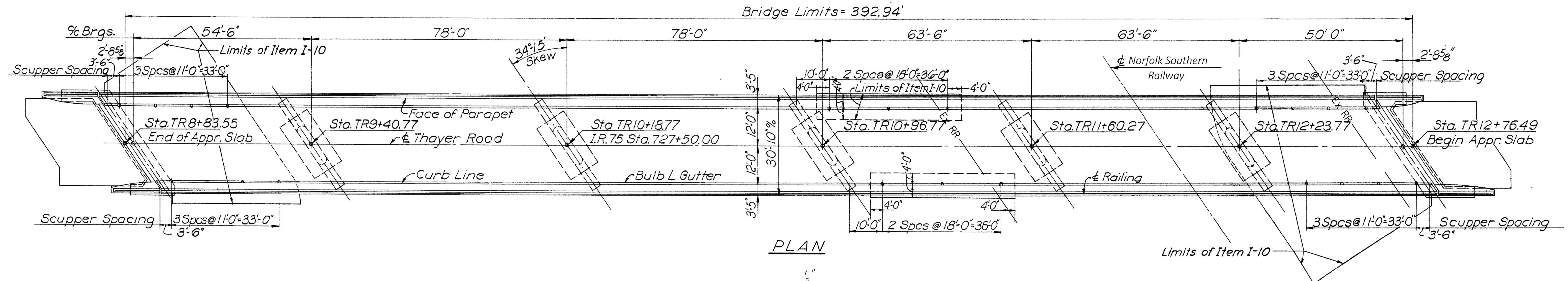
NOTE:

- LOCATION OF STRUCTURE
- ↔ DESIGNATED LOCAL DETOUR
- ↔ OFFICIAL SIGNED DETOUR

INTERSTATE HIGHWAY _____
 FEDERAL ROUTES _____
 STATE ROUTES _____
 COUNTY & TOWNSHIP ROADS _____
 OTHER ROADS _____



DESIGNER KRH
 REVIEWER ---
 PROJECT ID 119050
 SHEET TOTAL P.7 18



NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

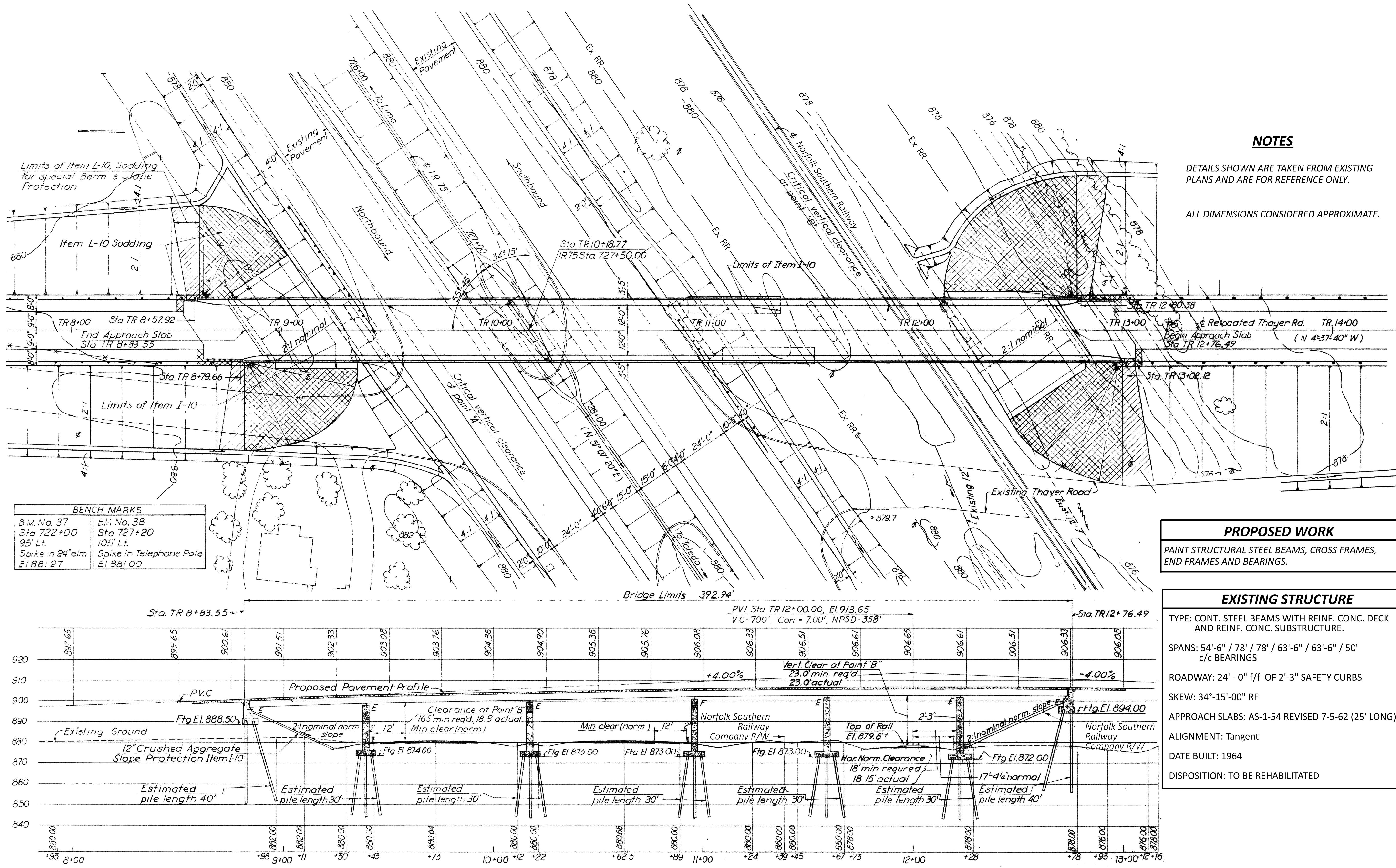
ALL DIMENSIONS CONSIDERED APPROXIMATE.

GENERAL PLAN
ALL-75-13.87
THAYER RD OVER IR-75

SFN
0202495
DESIGN AGENCY



DESIGNER	CHECKER
KRH	
REVIEWER	
XXX	XXX
PROJECT ID	
119050	
SUBSET	TOTAL
1	3
SHEET	TOTAL
P.10	18



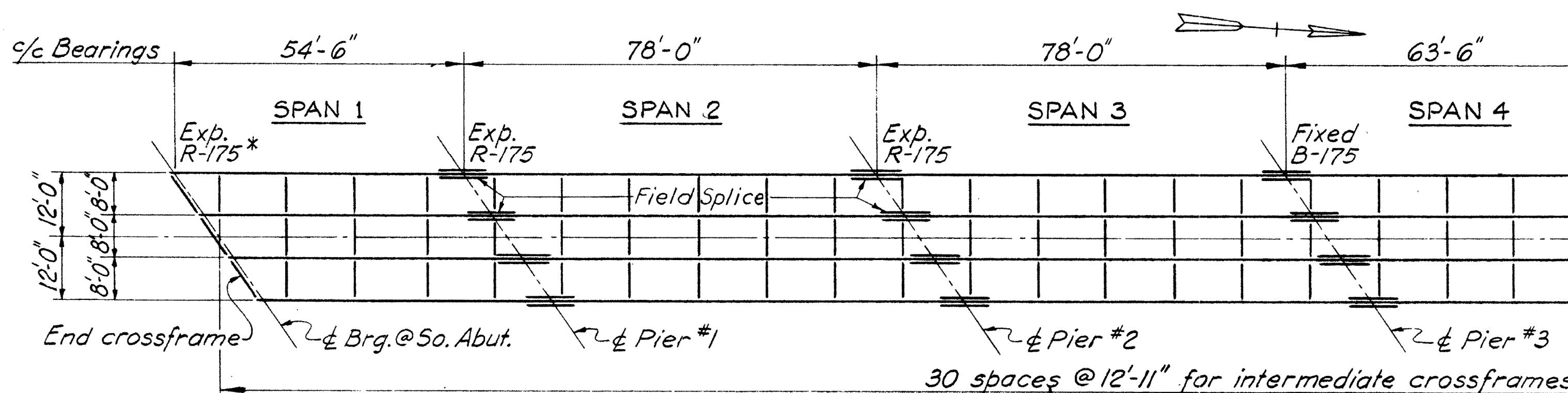
SITE PLAN
ALL-75-13.87
THAYER RD OVER IR-75



DESIGNER	CHECKER
KRH	
REVIEWER	XXX
PROJECT ID	119050
SUBSET	TOTAL
2	3
SHEET	TOTAL
P.11	18

HALF TRANSVERSE SECTION

(Slab thickness shown includes $\frac{3}{4}$ " monolithic wearing surface)



STEEL FRAMING PLAN

NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE

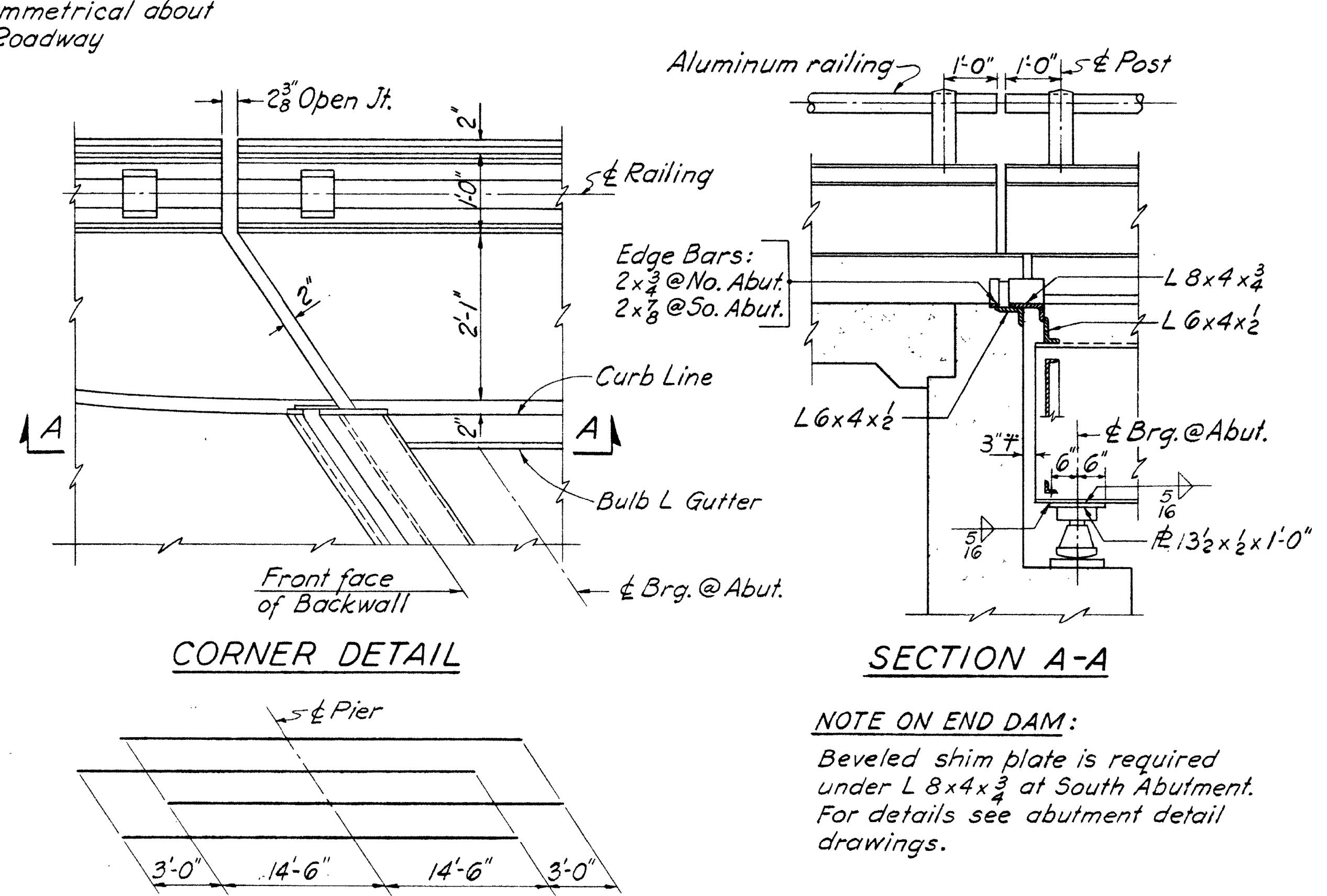
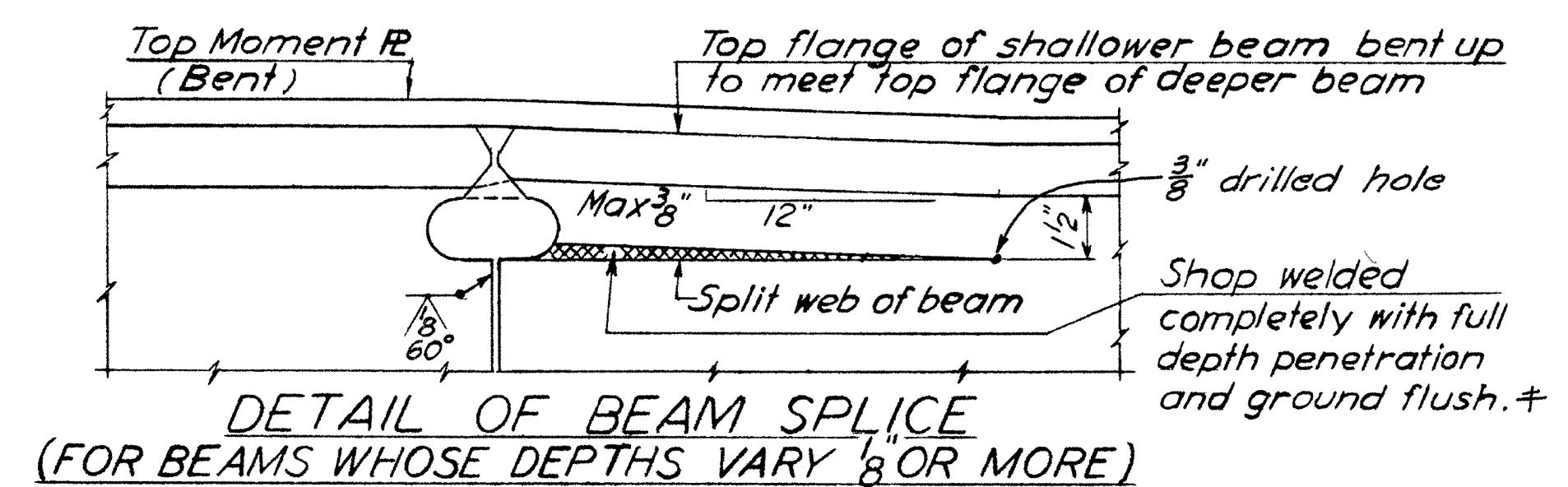


DIAGRAM SHOWING STAGGER OF
S 603 BARS OVER PIERS

Span	Beam Size
2 & 3	36 WF170
1, 4, 5 & 6	36 WF135

***ROCKERS AT ABUTMENTS:**
Rocker details to be same as Standard Rocker R-175 except for the following dimensions:
 $L=1'-8"$, $B=1'-0"$ and $M=1'-6"$.



DETAIL OF BEAM SPLICE
(FOR BEAMS WHOSE DEPTHS VARY 8" OR MORE)

SUPERSTRUCTURE DETAILS

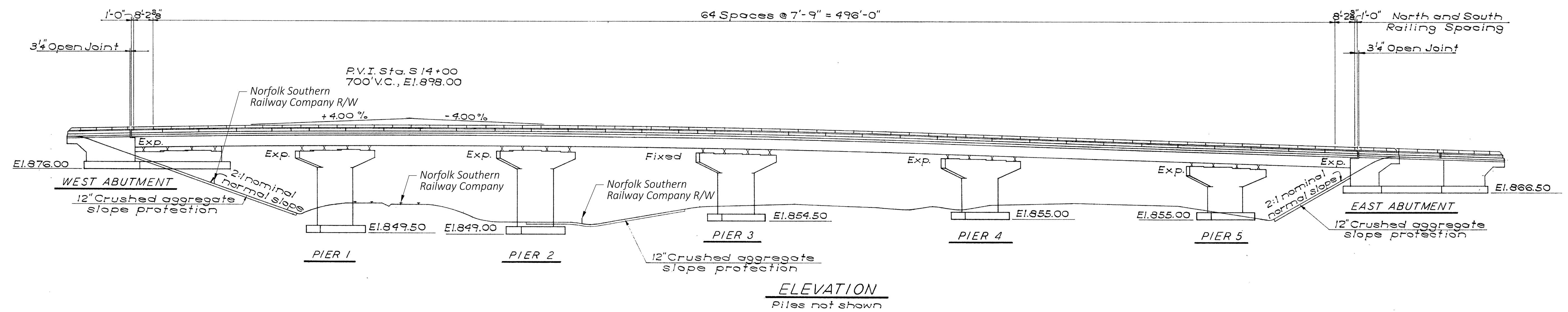
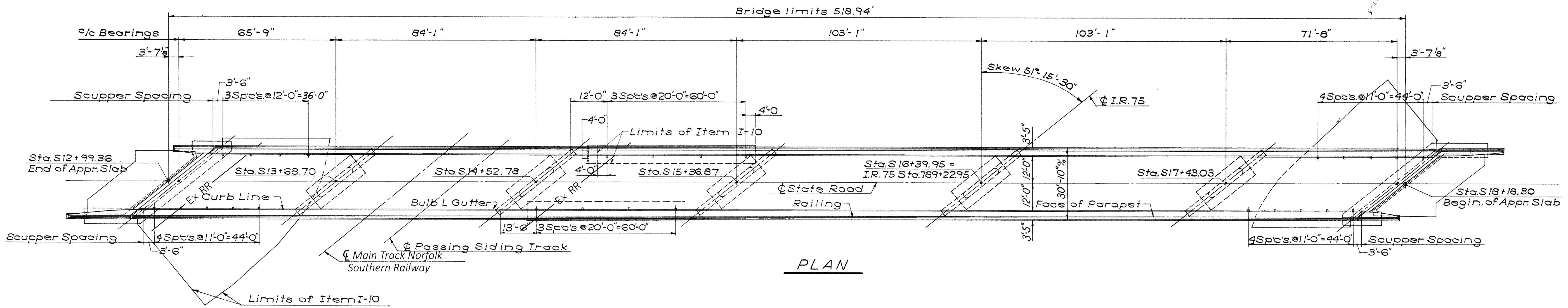
ALL-75-13.87

THAYER RD OVER IR-75

SFN
0202495



DESIGNER	CHECKER
KRH	
REVIEWER	
XXX	XXX
PROJECT ID	
119050	
SUBSET	TOTAL
3	3
SHEET	TOTAL
P.12	18

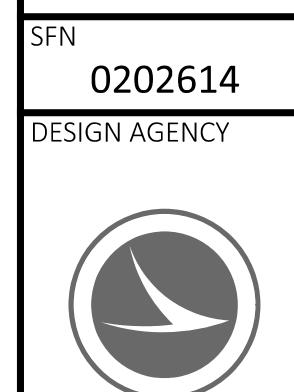


NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

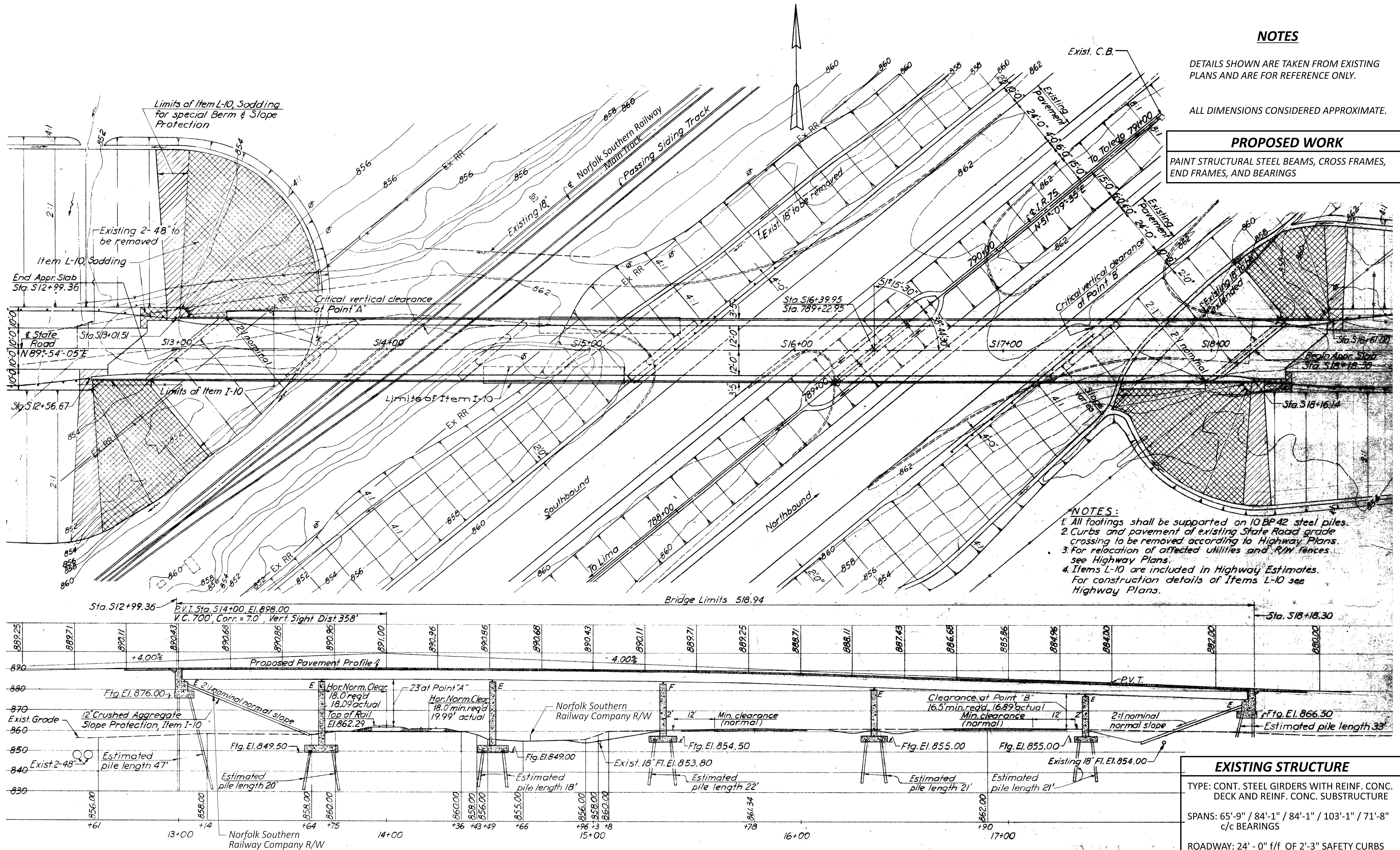
ALL DIMENSIONS CONSIDERED APPROXIMATE.

GENERAL PLAN
ALL-75-15.05
STATE RD OVR IR-75



SFN
0202614
DESIGN AGENCY

DESIGNER KRH	CHECKER
REVIEWER	XXX XXX
PROJECT ID	119050
SUBSET	TOTAL
1	3
SHEET	TOTAL
P.13	18



NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.

PROPOSED WORK

PAINT STRUCTURAL STEEL BEAMS, CROSS FRAMES, END FRAMES, AND BEARINGS

SITE PLAN
ALL-75-15.05

EXISTING STRUCTURE

TYPE: CONT. STEEL GIRDERS WITH REINF. CONC.

PANS: 65'-9" / 84'-1" / 84'-1" / 103'-1" / 71'-8"
c/c BEARINGS

ROADWAY: 24' - 0" f/f, OF 2'-3" SAFETY CURBS

KFW: 51°-15'-30" LE

APPROACH SI ABS: AS-1-54 REVISED 7-5-62

ALIGNMENT: Tangent

DATE BUILT: 1964

DISPOSITION: TO BE REHABILITATED

SFN
0202614



DESIGNER
KPH

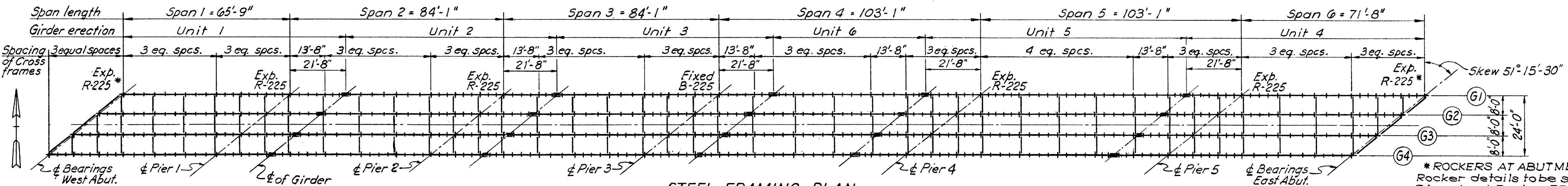
REVIEWER

PROJECT ID

119050

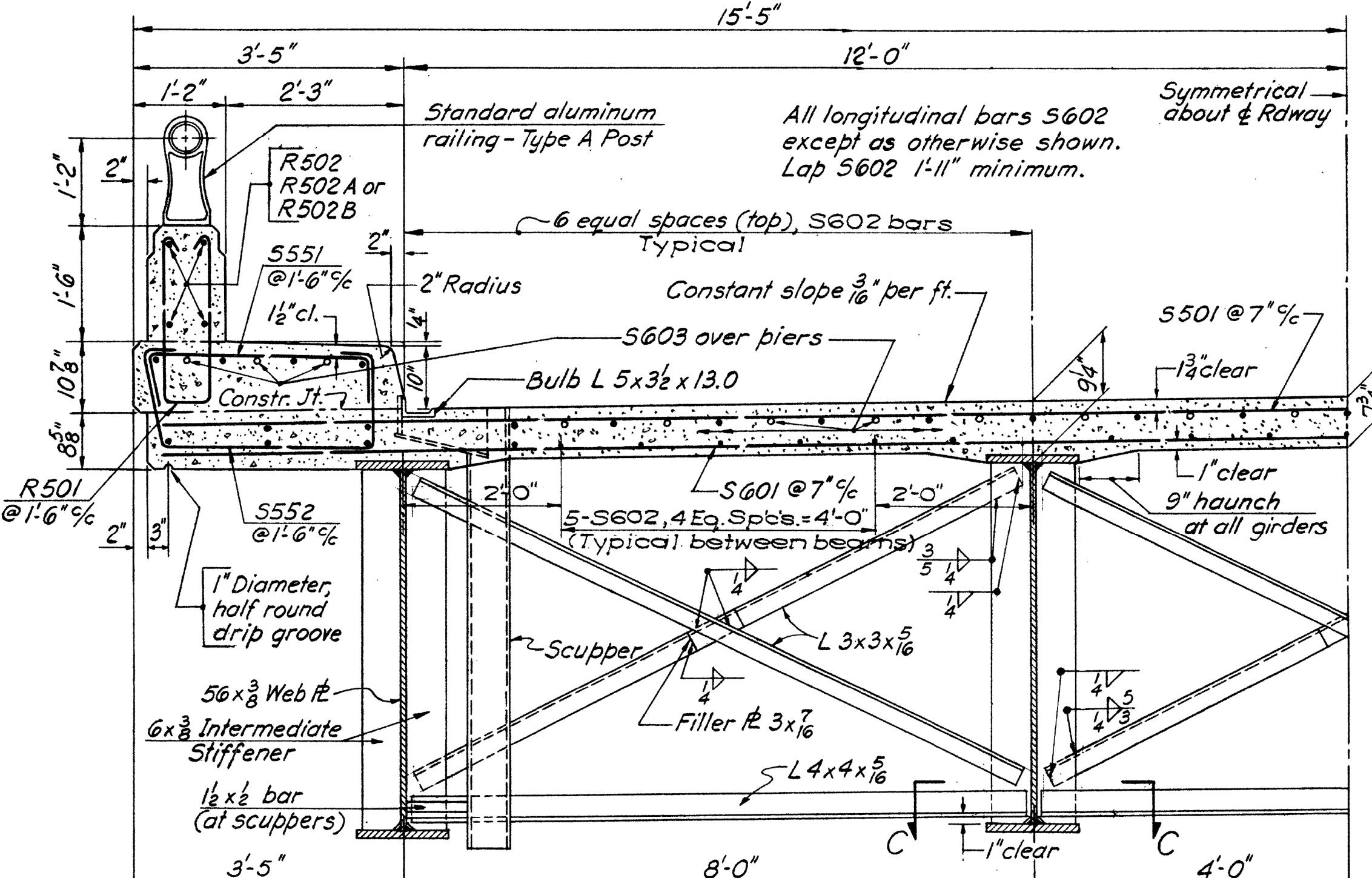
2 3

P.14 18



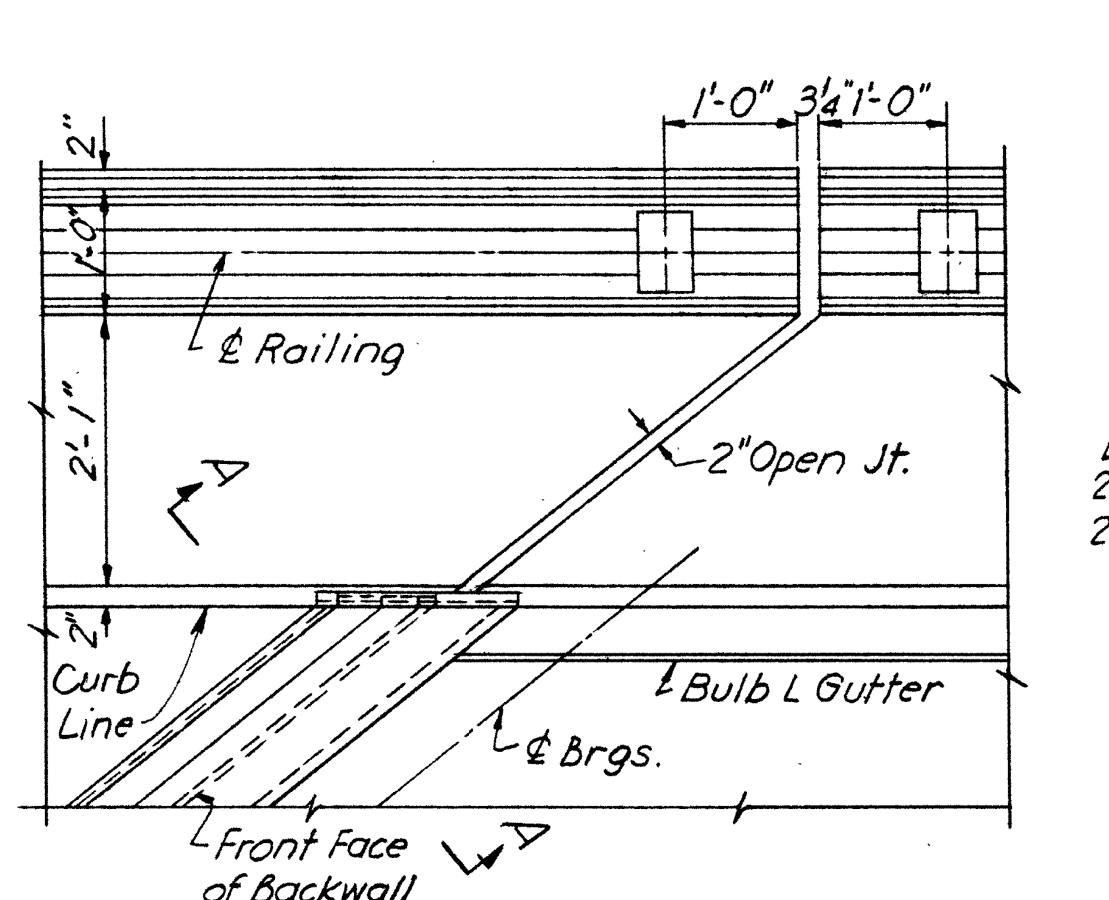
STEEL FRAMING PLATE

***ROCKERS AT ABUTMENTS**
Rockers details to be same as
standard Rocker R225 except
for the following dimensions:
 $A = 1'-6"$, $B = 1'-0"$ and $M = 1'-4"$.

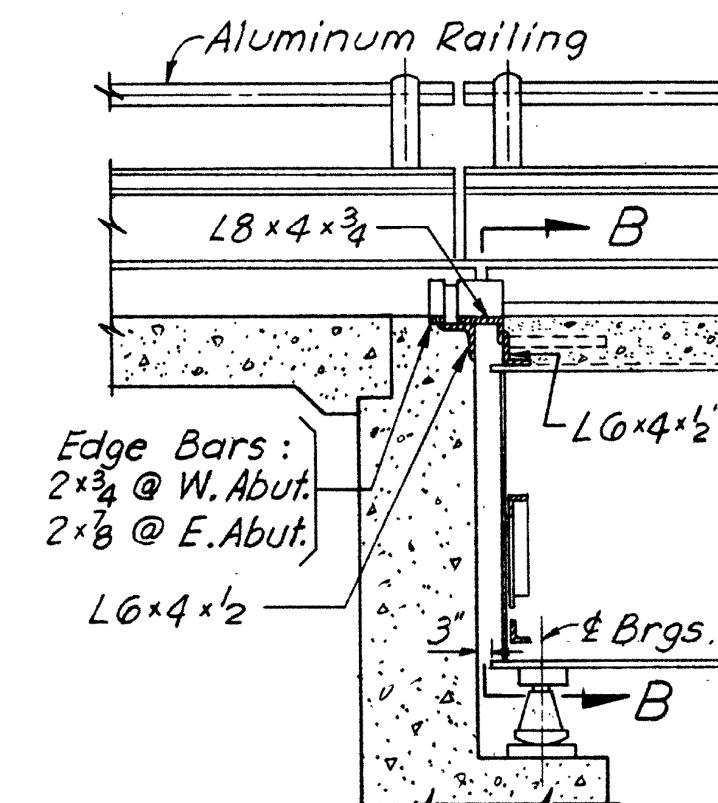


HALF TRANSVERSE SECTION
(Slab thickness shown includes
3" thick concrete)

TRANSVERSE SEC
Lab thickness shown includes
"monolithic" wearing surfaces



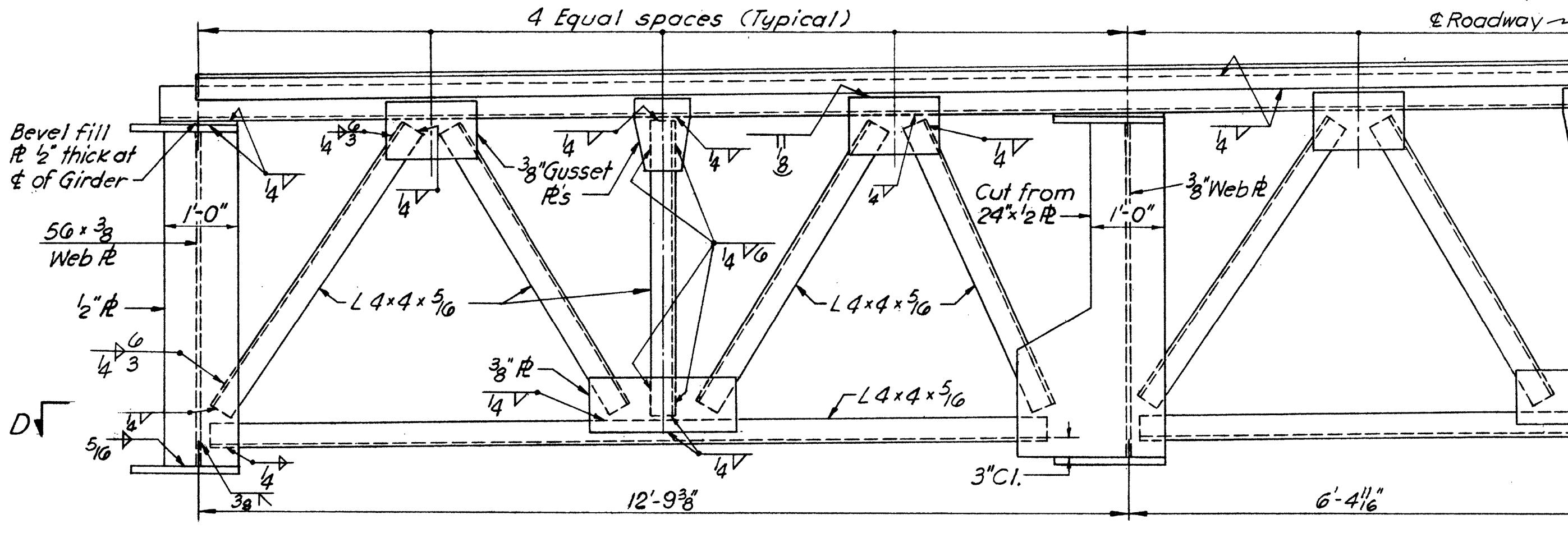
CORNER DETAIL



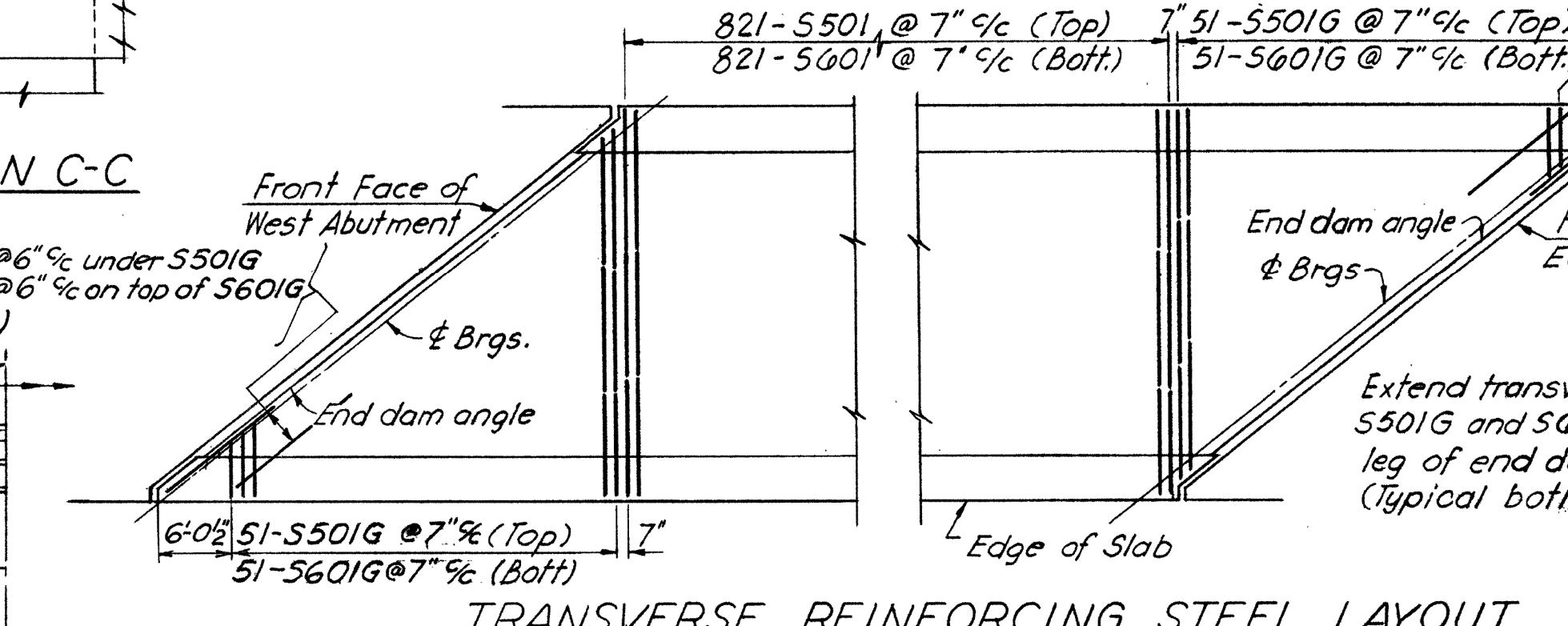
NOTES

**DETAILS SHOWN ARE TAKEN FROM EXISTING
PLANS AND ARE FOR REFERENCE ONLY.**

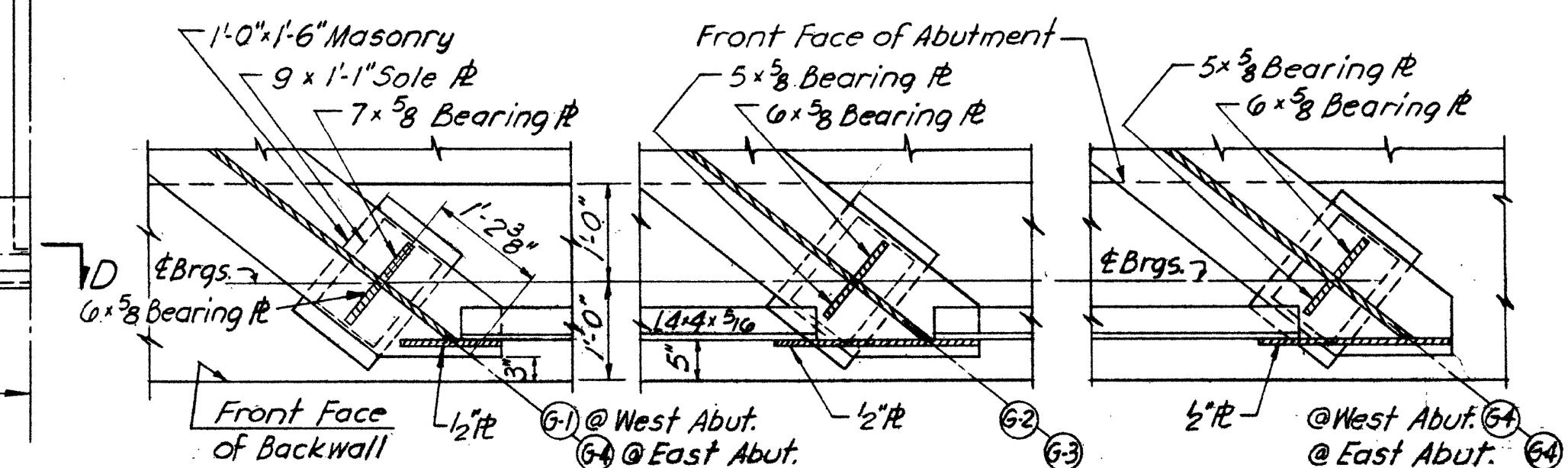
ALL DIMENSIONS CONSIDERED APPROXIMATE.



END CROSS FRAME
(SECTION B-B)



TRANSVERSE REINFORCING STEEL LAYOUT



SECTIONAL PLAN D-D

SUPERSTRUCTURE DETAILS

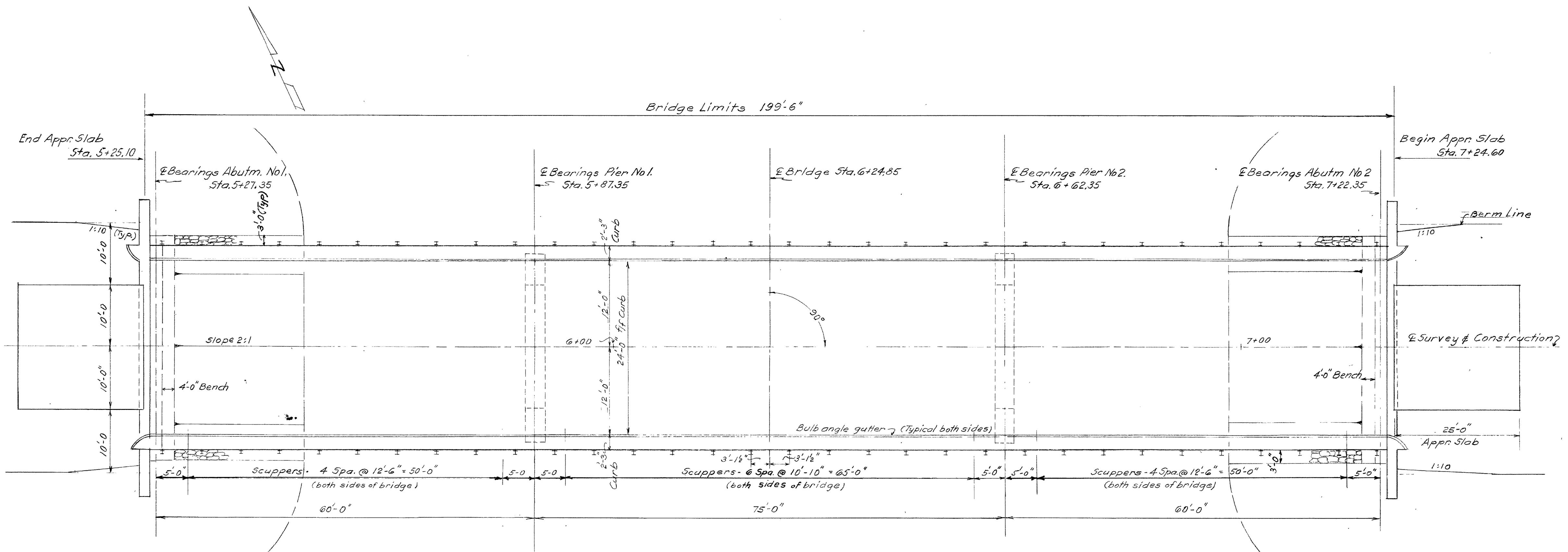
ALL-75-15.05

STATE RD OVER ID 3E

STRUCTURE DATA

STRUCTURE DETA L-75-15.05

DESIGNER KRH	CHECKED
REVIEWER	
XXX	XXX
PROJECT ID	
119050	
SUBSET	TOTAL
3	3
SHEET	TOTAL
P 15	1

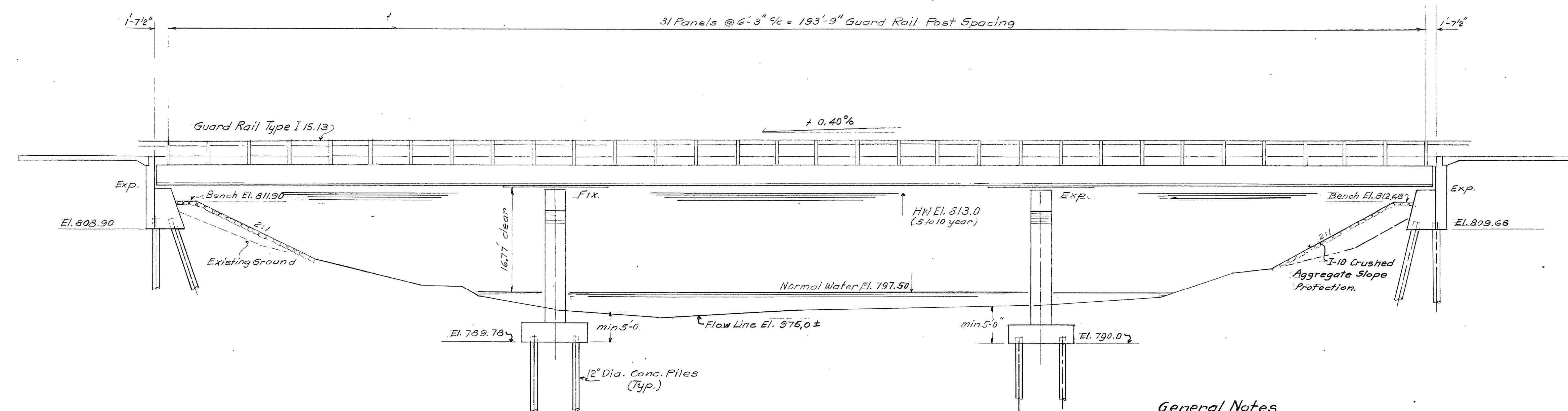


PLAN

NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.



ELEVATION

General Notes

Design Specifications: This structure conforms to the requirements of "Design Specifications for Highway Structures" of the State of Ohio, Department of Highways, dated 9-1-1957 together with revisions thereof dated 2-21-58.

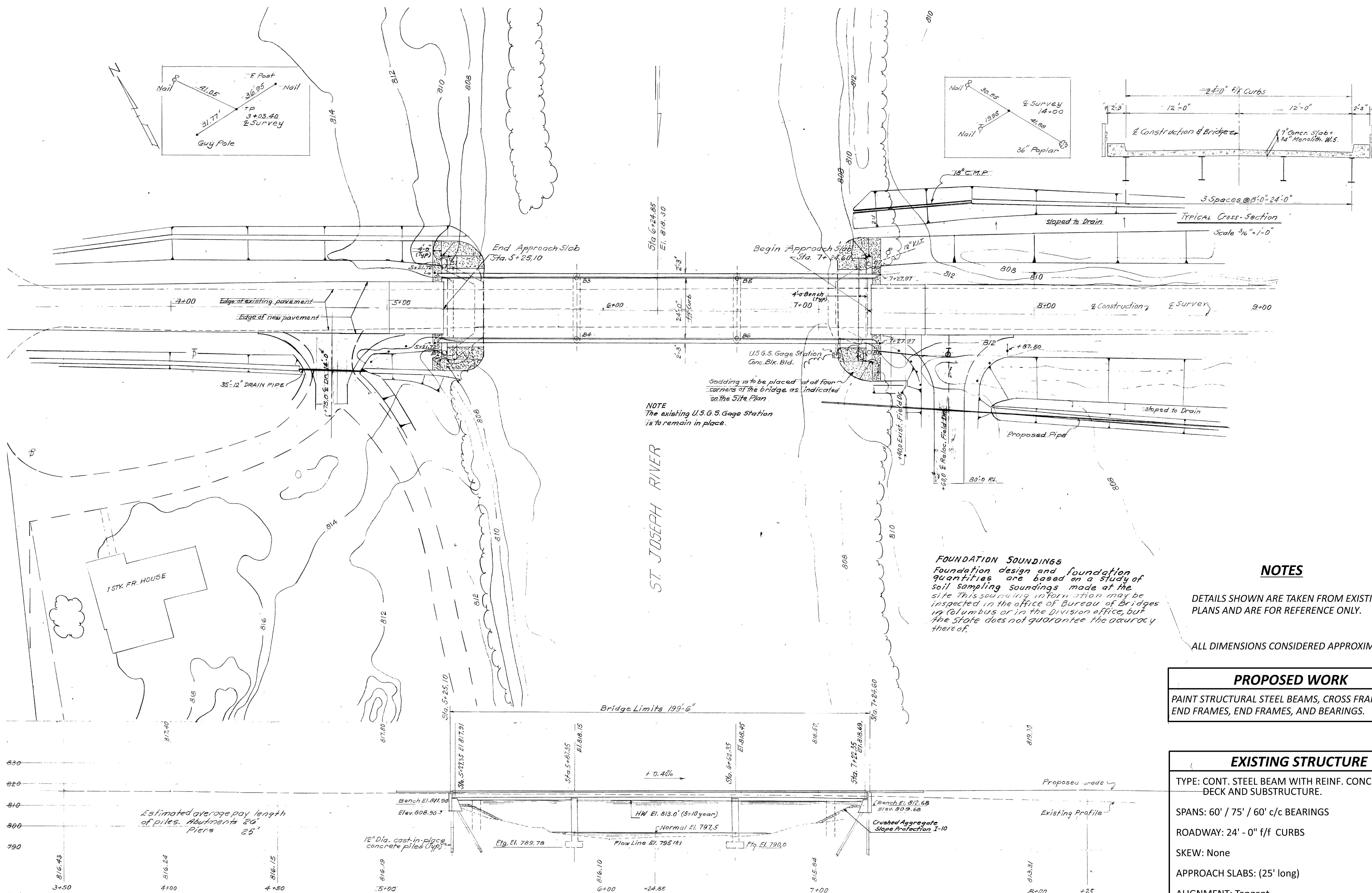
Reference shall be made to sheets 1, 2, 3 and 4 of Std. Dwg. CSB-2-56 revised 2-2-59 and supplemental specification S101, dated 12-2-59.

Concrete: Superstructure & Pier walls shall be Class "C" concrete. Abutments and pier footings shall be Class "E" concrete.

EXCAVATION QUANTITY for the abutments, in addition to that outlined in Sec. E-2.09, includes the removal of material bounded by the proposed bench, by the front vertical plane described in Sec. E-2.09 and by the finished slope of the cut.

DESIGNER KRH	CHECKER XXX
REVIEWER XXX XXX	
PROJECT ID 119050	
SUBSET 1	TOTAL 3
SHEET P.16	TOTAL 18

GENERAL PLAN
DEF-249-0.10
SR-249 OVER ST. JOSEPH RIVER



SITE PLAN
DEF-249-0.10
R-249 OVER ST. JOSEPH R

DEF-249-0.10
R-249 OVER ST: JOSEPH RIVER

NOTES

DETAILS SHOWN ARE TAKEN FROM EXISTING PLANS AND ARE FOR REFERENCE ONLY.

ALL DIMENSIONS CONSIDERED APPROXIMATE.

PROPOSED WORK

PAINT STRUCTURAL STEEL BEAMS, CROSS FRAMES, END FRAMES, END FRAMES, AND BEARINGS.

EXISTING STRUCTURE

TYPE: CONT. STEEL BEAM WITH REINF. CONC. DECK AND SUBSTRUCTURE.

SPANS: 60' / 75' / 60' c/c BEARINGS

ROADWAY: 24' - 0" f/f CURBS

SKEW: None

APPROACH SLABS: (25' long)

ALIGNMENT: Tangent

DATE BUILT: 196

DISPOSITION: TO BE REHABILITATED

SFN	2002280
DESIGN AGENCY	
	
DESIGNER	CHECKED
KRH	XXX
REVIEWER	
XXX	XXX
PROJECT ID	
119050	
SUBSET	TOTAL
2	3
SHEET	TOTAL
P 17	18

