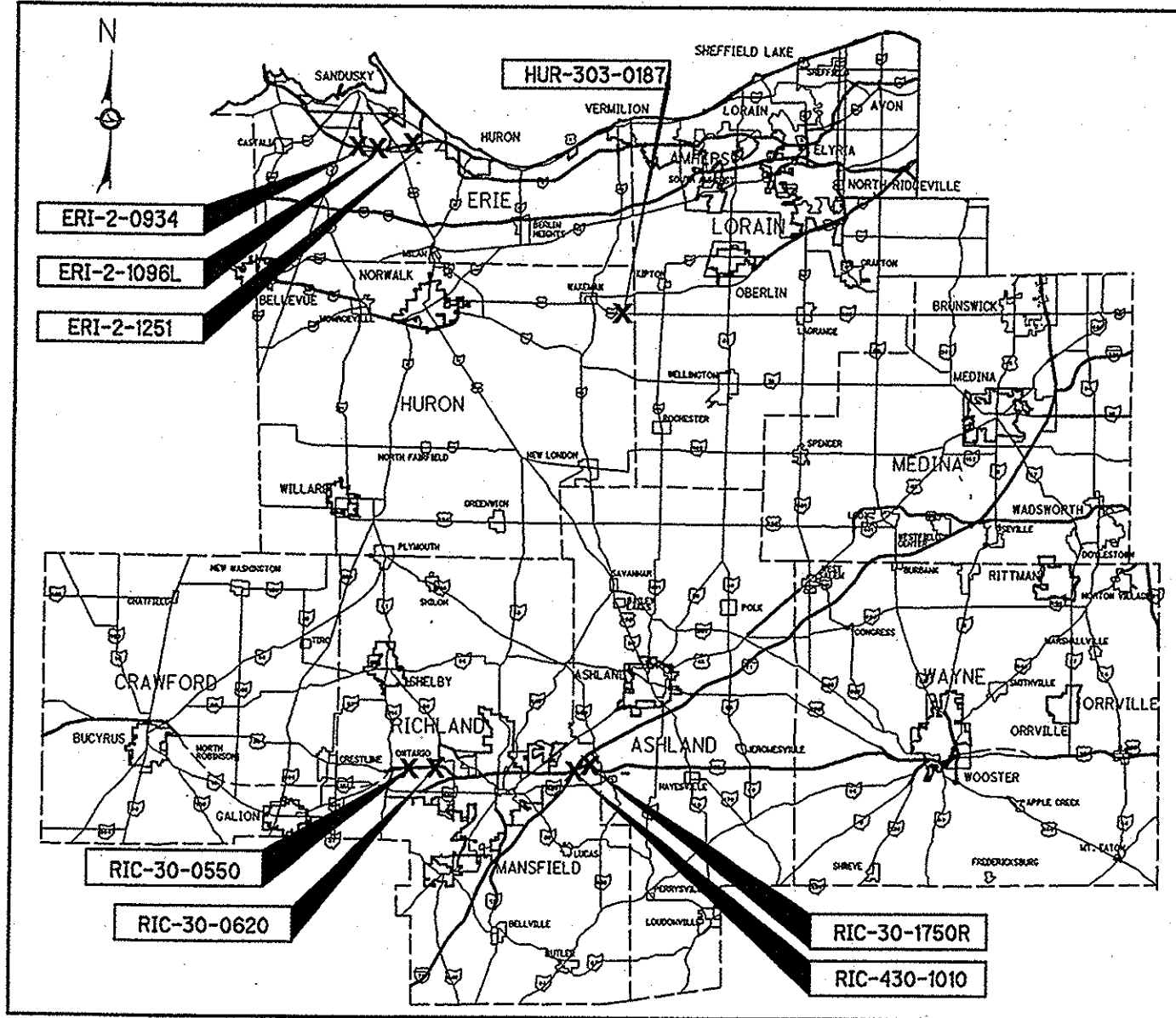


D03 - VA-BH-FY2009(A)
 080500 PID - 80111
 Dist 3 8/6/2008

OHIO DEPARTMENT OF TRANSPORTATION

LOCATION MAP

D03-BH-FY2009(A)



PROJECT DESCRIPTION:
 BRIDGE MAINTENANCE ITEMS INCLUDING OVERLAYING THE BRIDGE DECKS, MISCELLANEOUS CONCRETE REPAIR, SEALING CONSTRUCTION JOINTS, CORRECTING APPROACH SLAB'S ELEVATION AND RESURFACING APPROACHES.

PROJECT EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA = N/A
 NOTICE OF INTENT EARTH DISTURBED AREA = N/A

2008 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEETS 4 - 9.

LIMITED ACCESS: ERI-2-0934, ERI-2-1096L, ERI-2-1251, RIC-30-0550, RIC-30-0620, RIC-30-1750R & RIC-430-1010

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

INDEX OF SHEETS:

- 1 - TITLE SHEET
- 2-3 - GENERAL NOTES
- 4-9 - MAINTENANCE OF TRAFFIC NOTES
- 10-11 - GENERAL SUMMARY
- 12 - HUR-303-0187
- 13-14 - ERI-2-0934
- 15-16 - ERI-2-1251
- 17 - RIC-30-0550
- 18 - RIC-30-0620
- 19 - RIC-30-1750R
- 20 - ERI-2-1096L
- 21 - RIC-430-1010

3/26/08 APPROVED DATE
 John Hart P.E. DISTRICT DEPUTY DIRECTOR OF TRANSPORTATION

5-15-08 APPROVED DATE
 James A. Baselyll DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO.
 NON-FEDERAL

PID NO.
 80111

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT
 NONE

D03-BH-FY2009(A)

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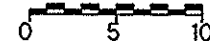
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TWO WORKING DAYS BEFORE YOU DIG
 call 800-362-2764
 TOLL FREE
 OHIO UTILITIES PROTECTION SERVICE
 NON-MEMBERS MUST BE CALLED DIRECTLY

PLAN PREPARED BY:



SCALE IN MILES



	LATITUDE	LONGITUDE
HUR-303-0187	N 41° 14' 25"	W 82° 22' 30"
ERI-2-0934	N 41° 23' 59"	W 82° 42' 24"
ERI-2-1096L	N 41° 23' 48"	W 82° 42' 30"
ERI-2-1251	N 41° 24' 06"	W 82° 38' 48"
RIC-30-0550	N 40° 47' 00"	W 82° 37' 48"
RIC-30-0620	N 40° 47' 01"	W 82° 37' 00"
RIC-30-1750R	N 40° 46' 57"	W 82° 24' 24"
RIC-430-1010	N 40° 46' 00"	W 82° 25' 30"

ENGINEER'S SEAL



SIGNED: *Charles A. Laughrey*
 DATE: 3/26/08

STANDARD CONSTRUCTION DRAWINGS

	MT-35.10	4-20-01			
BP-3.1	10-19-07	MT-95.30	9-05-06	800-2008	4/18/08
		MT-95.50	9-05-06	842	1-19-07
		MT-97.10	9-05-06	848	4-15-05
		MT-101.20	10-18-02		
		MT-101.60	9-20-06		
		MT-105.10	10-18-02		
		MT-105.11	10-18-02		

SUPPLEMENTAL SPECIFICATIONS

SPECIAL PROVISIONS
 WATERWAY PERMIT
 N/A

EXISTING STRUCTURE VERIFICATION:

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATION AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURES AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02 & 513.02

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURES. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

DESIGN SPECIFICATIONS:

THIS STRUCTURE CONFORMS TO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2002, INCLUDING THE 2003, 2004, 2005 AND 2006 SPECIFICATIONS AND THE ODOT BRIDGE DESIGN MANUAL.

DESIGN DATA:

CONCRETE CLASS C - COMPRESSIVE STRENGTH 4,000 PSI

EXISTING PLANS:

THE ORIGINAL CONSTRUCTION PLANS OF THE EXISTING BRIDGES ARE AVAILABLE UPON REQUEST AT THE DISTRICT 3 OFFICE OF THE OHIO DEPARTMENT OF TRANSPORTATION, ASHLAND, OH.

STRUCTURE #	PLAN NAME	DATE
HUR-303-0187	HUR-162/303/61-VARIOUS	1982
	HUR-303-1.72	1953
ERI-2-0934	ERI-6-7.31	1960
ERI-2-1096L	ERI-2-12.558	1999
ERI-2-1251	ERI-6-11.30	1960
RIC-30-0550	RIC-30-3.74	1976
RIC-30-0620	RIC-30-3.74	1976
RIC-30-1750R	RIC-30-12.37	1985
	RIC-30-9.28	1964
RIC-430-1010	RIC-71-13.66	2000

UTILITY LINES:

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

PLACING ASPHALT CONCRETE FEATHERING ON APPROACHES TO BRIDGES:

SPECIAL CARE SHALL BE TAKEN, WHEN PLACING THE ASPHALT CONCRETE FEATHERING TO CREATE A SMOOTH TRANSITION FROM THE EXISTING APPROACH PAVEMENT TO THE BRIDGE DECK. THE CONTRACTOR'S ATTENTION IS CALLED TO STANDARD DRAWING BP-3.1 FOR REQUIRED TOLERANCES.

CUT LINE CONSTRUCTION JOINT PREPARATION:

SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 1 INCH DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL IN PLACE. PRIOR TO CONCRETE PLACEMENT ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH, BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN:

THESE ITEMS SHALL BE USED AT LOCATIONS IN THE PLAN.

THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL SHALL BE APPROVED BY THE ENGINEER.

THE EXISTING REINFORCING STEEL SHALL BE PRESERVED AS INDICATED IN THE PLANS. EXISTING CONCRETE SHALL BE REMOVED IN A MANNER THAT WILL NOT CUT, ELONGATE, OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL BE NO HEAVIER THAN 90 POUND CLASS.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER CUBIC YARD FOR THE ABOVE ITEM, WHICH WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 202 - REMOVAL MISC.: EXPANSION JOINT EXTENSION

THIS ITEM SHALL BE USED TO REMOVE THE EXISTING WELDED STEEL VERTICAL EXTENSIONS AND THE EXISTING ASPHALT & BITUMINOUS ON THE EXPANSION JOINTS. THE EXISTING WELDS SHALL BE REMOVED AND GROUND SMOOTH WITH THE EXPANSION JOINT. CARE SHALL BE TAKEN TO AVOID DAMAGE TO THE EXISTING EXPANSION JOINT. IN CASE OF DAMAGE TO EXISTING EXPANSION JOINT, REPAIR OR REPLACE THE EXPANSION JOINT AT NO EXPENSE TO THE DEPARTMENT.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT FOR THE ABOVE ITEM, WHICH WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

DESIGN FILE: \$\$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$\$
 WORKSTATION: \$TERMINALS\$ DATE: \$\$\$DATE\$\$\$\$\$

DESIGN AGENCY	DISTRICT THREE	OFFICE OF PRODUCTION	
REVIEWED	DATE	DRAWN	STRUCTURAL FILE NUMBER
CAL	3/08	CAL	REVISED
DESIGNED	CAL	CHECKED	HYH
GENERAL NOTES			
D03-BH-FY2009(A)			
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21			

ITEM 407 - TACK COAT:

AS PER 407.06 THE APPLICATION RATE SHALL BE 0.08 GAL. PER SQ. YD. FOR ESTIMATING PURPOSES ONLY. THE RATE OF APPLICATION SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. A COMPLETE PAVEMENT SURFACE COVERAGE SHALL BE REQUIRED. AREAS OF TACK STRIPPED BY CONSTRUCTION EQUIPMENT OR TRAFFIC SHALL BE RE-COATED PRIOR TO PLACING ASPHALT CONCRETE. ALL COSTS AS DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID PER GALLON FOR ITEM 407, TACK COAT.

ITEM 511 - CONCRETE MISC.: APPROACH SLAB REPAIR:

THE CONCRETE SHALL BE CLASS C AND MEET CMS 511 EXCEPT THAT THE THE COARSE AGGREGATE SHALL BE LIMESTONE.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER CUBIC YARD FOR THE ABOVE ITEM WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 511 - CLASS C CONCRETE, ABUTMENT, AS PER PLAN: (REPAIR)

THE COARSE AGGREGATE SHALL BE LIMESTONE.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER CUBIC YARD FOR THE ABOVE ITEM WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 516 - JOINT SEALER, AS PER PLAN

THE JOINT SHALL BE CLEANED, ALL DEBRIS REMOVED AND MUST BE FREE OF OILS, GREASE, CURING COMPOUND RESIDUES AND ANY OTHER FOREIGN MATTER THAT MIGHT PREVENT BONDING. THE JOINT SHALL BE SANDBLASTED TO REMOVE ALL PACK RUST AND DEBRIS. AFTER CLEANED, THE JOINT SHALL BE FILLED WITH "SIKAFLEX-2C NS/NL" POLYURETHANE ELASTOMERIC SEALANT OR AN APPROVED EQUAL. THE CONTRACTOR SHALL ADHERE TO THE MANUFACTURES INSTRUCTIONS FOR THE COVERAGE, SURFACE PREPARATION, MIXING, APPLICATION & LIMITATIONS.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT FOR THE ABOVE ITEM WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

- ITEM 646 - EDGE LINE, AS PER PLAN:
- ITEM 646 - LANE LINE, AS PER PLAN:
- ITEM 646 - CENTER LINE, AS PER PLAN:

THE EPOXY PAVEMENT MARKING MATERIAL FURNISHED UNDER THESE ITEMS SHALL BE EPOPLEX LS-60 AS FURNISHED BY EPOPLEX, MAPLE SHADE, NEW JERSEY.

- ITEM 646 - EDGE LINE: (ALTERNATE BID)
- ITEM 646 - LANE LINE: (ALTERNATE BID)
- ITEM 646 - CENTER LINE: (ALTERNATE BID)

THE EPOXY PAVEMENT MARKING MATERIAL SHALL BE MARK 55.4 AS FURNISHED BY POLYCARB, CLEVELAND, OHIO.

ITEM 848 - MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN:

THE COARSE AGGREGATE SHALL BE LIMESTONE.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER CUBIC YARD FOR THE ABOVE ITEM WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 848 - EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN (1 1/2" NOMINAL THICKNESS):

THIS ITEM SHALL BE USED TO REMOVE THE EXISTING OVERLAY AS PER DETAILS IN THE PLAN.

THE EXISTING OVERLAY SHALL BE SAW CUT 1 1/2" DEEP AT THE LOCATIONS SHOWN IN THE PLAN.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER SQUARE YARD FOR THE ABOVE ITEM WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 848 - EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN (2 " NOMINAL THICKNESS):

THIS ITEM SHALL BE USED TO REMOVE THE EXISTING OVERLAY AS PER DETAILS IN THE PLAN.

THE EXISTING OVERLAY SHALL BE SAW CUT 2" DEEP AT THE LOCATIONS SHOWN IN THE PLAN.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER SQUARE YARD FOR THE ABOVE ITEM WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 848 - MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (1 1/4" THICK):

ITEM 848 - MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (2 1/2" THICK):

ITEM 848 - MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (3" THICK):

ITEM 848 - MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (3 1/2" THICK):

THE COARSE AGGREGATE SHALL BE LIMESTONE.

THE SURFACE FINISH REQUIREMENTS SHALL BE AS PER CMS 511.19 AND 511.20 IN LIEU OF THAT WHICH IS SPECIFIED IN SUPPLEMENTAL SPECIFICATION 848.

SEE THE SUPPLEMENTAL SPECIFICATION FOR DETAILS.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER SQUARE YARD FOR THE ABOVE ITEMS WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

DESIGN FILE: SSSSS.DGNFILESPECIFICATIONS88888
WORKSTATION: STERMINALS DATE: SSS88DATE88888

DESIGN AGENCY
DISTRICT THREE
OFFICE OF PRODUCTION

DATE
REVISED RDN 3/08
STRUCTURAL FILE NUMBER

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GENERAL NOTES

D03-BH-FY2009(A)

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ITEM 614, MAINTENANCE OF TRAFFIC

LANE CLOSURE/REDUCTION REQUIRED:

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

TWO LANES OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT THROUGH TRAFFIC AT STRUCTURE ERI-2-1096L SHALL HAVE A LANE CLOSURE AS PER STANDARD CONSTRUCTION DRAWING MT-95.30. NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

- CHRISTMAS
- NEW YEARS
- MEMORIAL DAY
- FOURTH OF JULY
- LABOR DAY
- THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH CMS 108.07.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

MAINTAINING TRAFFIC FOR STRUCTURE RIC-30-1750R:

TWO LANES OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT THROUGH TRAFFIC ON THIS STRUCTURE SHALL HAVE A LANE CLOSURE AS PER STANDARD CONSTRUCTION DRAWING MT-95.30, FOR A MAXIMUM OF 10 CONSECUTIVE CALENDAR DAYS. THE 10 CONSECUTIVE DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE SECTION 108) AND FOR EACH CALENDAR DAY BEYOND THE 10 CALENDAR DAYS THAT THE HIGHWAY REMAINS IN A SINGLE LANE, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES AS PER 108.07.

NO LANE CLOSURES SHALL OCCUR FROM AUGUST 6, 2008 THRU SEPTEMBER 2, 2008 & FROM MAY 22, 2009 THRU SEPTEMBER 8, 2009.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS AS WELL AS IN ACCORDANCE WITH PART 7 OF THE OMUTCD. PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS (INCLUDING WORK ZONE PAVEMENT MARKINGS AND REMOVING & RESTORING CONFLICTING RAISED PAVEMENT MARKER REFLECTORS) NECESSARY TO PROVIDE THIS METHOD OF TRAFFIC CONTROL SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

MAINTAINING TRAFFIC FOR STRUCTURE RIC-430-1010:

DETOUR LIMITATION AND INTERIM COMPLETION DATE:

TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED SEVEN (7) CONSECUTIVE CALENDAR DAYS, WHEN THROUGH TRAFFIC WILL BE DETOURED AS SHOWN THIS SHEET.

THE CONTRACTOR SHALL NOTIFY THE O.D.O.T. DISTRICT THREE ROADWAY SERVICES MANAGER, IN WRITING, A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE DATE THE DETOUR IS NEEDED. THE STATE OF OHIO WILL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE THE DETOUR SIGNING.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE GATES AND BARRICADES AT THE APPROXIMATE WORK LIMITS OF THE PROJECT, AND THE ADVANCE WARNING SIGNS AS SHOWN ON STANDARD CONSTRUCTION DRAWING MT-101.60.

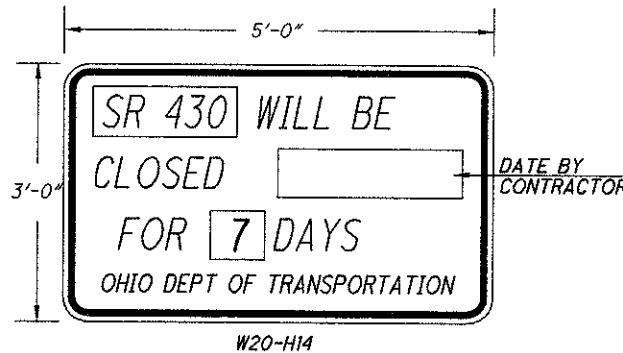
THE SEVEN (7) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE (SECTION 108) AND FOR EACH CALENDAR DAY BEYOND THE SEVEN (7) DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES, AS PER SECTION 108.07 OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES AS PER 614.02 (a).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTICE OF CLOSURE SIGNS:

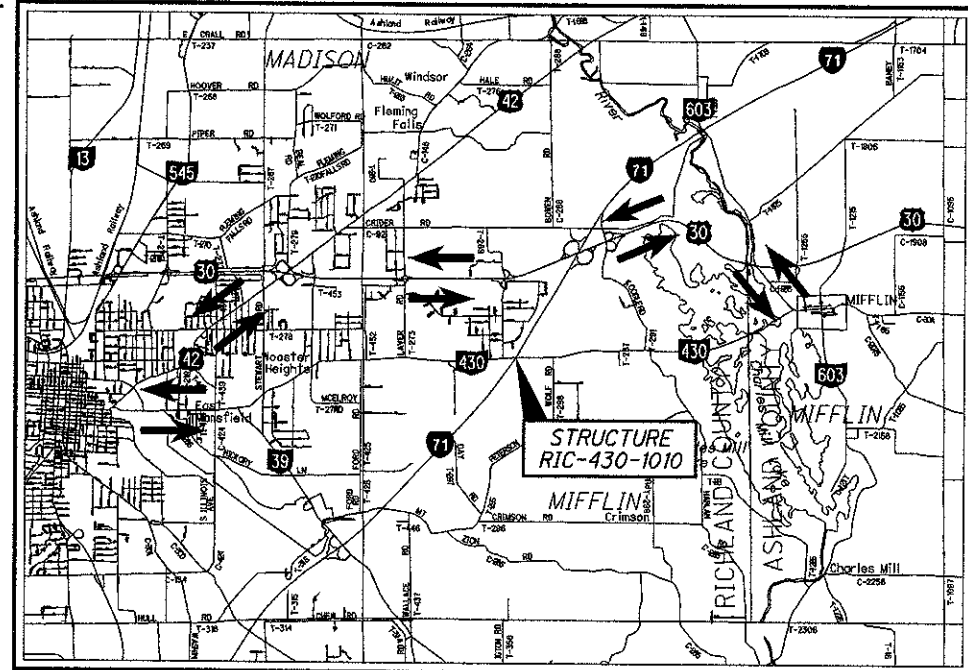
THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD CLOSURE FOR S.R. 430. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE LOCATED IN THE FIELD SO AS NOT TO INTERFERE WITH ANY PERMANENT SIGNS. ON THIS PROJECT THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC AND SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING AND REMOVING THE SIGNS INCLUDING SUPPORTS.



MAINTAINING OF LOCAL DETOUR ROUTE

A LOCAL DETOUR ROUTE, OTHER THAN THE OFFICIAL SIGNED ODOT DETOUR ROUTE, AS NOTED ON THIS SHEET, WILL BE SELECTED BY AGREEMENT BETWEEN ODOT AND LOCAL GOVERNMENTAL AGENCIES PRIOR TO THE HIGHWAY CLOSURE. DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THIS ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST, AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DESIGNATED LOCAL DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DIRECTED BY THE ENGINEER. THE DESIGNATED LOCAL DETOUR ROUTE IS TO BE REVIEWED AND REPAIRED PRIOR TO THE ASPHALT CONTRACTOR OR SUBCONTRACTOR LEAVING THE PROJECT. PAYMENT FOR THE WORK NECESSARY TO REPAIR THESE LOCAL ROADS WILL BE PERFORMED BY EITHER CHANGE ORDER OR FORCE ACCOUNT.

STRUCTURE RIC-430-1010



DESIGN FILE: 8888.DGN FILE SPECIFICATIONS 888888 WORKSTATION: 8TERMINAL8 DATE: 888888DATE888888

DISTRICT THREE
OFFICE OF PRODUCTION
 DATE: 3/08
 RDN: 3/08
 STRUCTURAL FILE NUMBER:
 DRAWN: CAL
 CHECKED: HYH
MAINTENANCE OF TRAFFIC NOTES
D03-BH-FY2009(A)
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 21

614 - MAINTAINING TRAFFIC FOR STRUCTURE HUR-303-0187

DETOUR LIMITATION AND INTERIM COMPLETION DATE:

TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FOURTEEN (14) CONSECUTIVE CALENDAR DAYS, WHEN THROUGH TRAFFIC WILL BE DETOURED AS SHOWN THIS SHEET.

THE CONTRACTOR SHALL NOTIFY THE O.D.O.T. DISTRICT THREE ROADWAY SERVICES MANAGER, IN WRITING, A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE DATE THE DETOUR IS NEEDED. THE STATE OF OHIO WILL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE THE DETOUR SIGNING.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE GATES AND BARRICADES AT THE APPROXIMATE WORK LIMITS OF THE PROJECT, AND THE ADVANCE WARNING SIGNS AS SHOWN ON STANDARD CONSTRUCTION DRAWING MT-101.60.

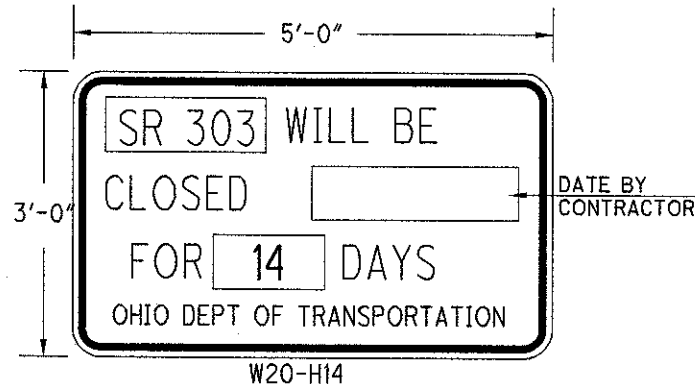
THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE (SECTION 108) AND FOR EACH CALENDAR DAY BEYOND THE FOURTEEN (14) DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES, AS PER SECTION 108.07 OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES AS PER 614.02 (a).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTICE OF CLOSURE SIGNS

THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE LOCATED IN THE FIELD SO AS NOT TO INTERFERE WITH ANY PERMANENT SIGNS. ON THIS PROJECT THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC AND SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING AND REMOVING THE SIGNS INCLUDING SUPPORTS.

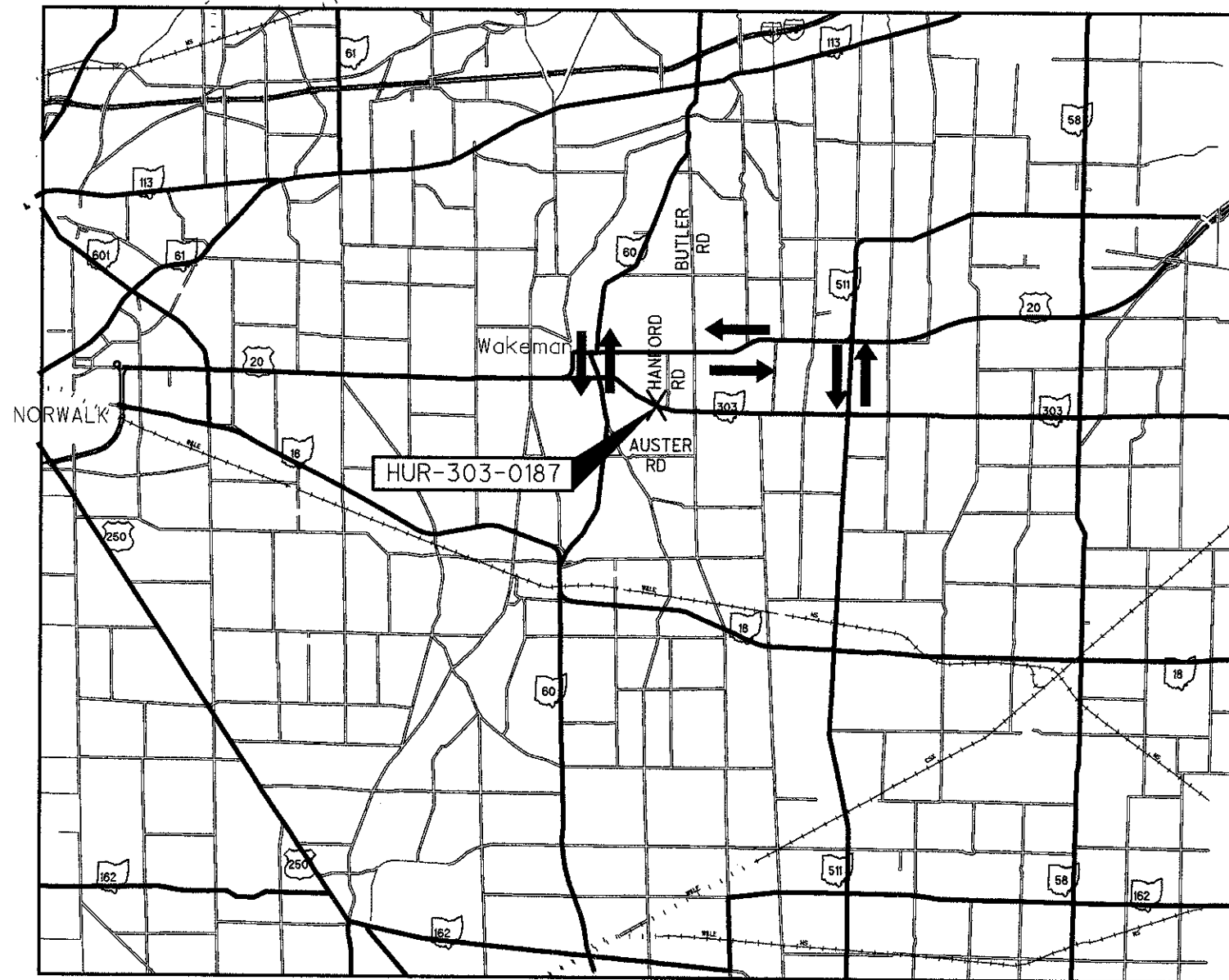


MAINTENANCE OF LOCAL DETOUR ROUTE

A LOCAL MAINTENANCE ROUTE, OTHER THAN THE OFFICIAL SIGNED ODOT DETOUR ROUTE, WILL BE DESIGNATED BY AGREEMENT BETWEEN ODOT AND LOCAL GOVERNMENTAL AGENCIES PRIOR TO THE HIGHWAY CLOSURE.

DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THIS ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DESIGNATED LOCAL MAINTENANCE ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DIRECTED BY THE ENGINEER. THE DESIGNATED LOCAL DETOUR ROUTE IS TO BE REVIEWED AND REPAIRED PRIOR TO THE ASPHALT CONTRACTOR OR SUBCONTRACTOR LEAVING THE PROJECT.

PAYMENT FOR THE WORK NECESSARY TO REPAIR THESE LOCAL ROADS WILL BE PERFORMED BY EITHER CHANGE ORDER OR FORCE ACCOUNT.



DETOUR MAP



DESIGN FILE: \$\$\$\$.DGNFILESPECIFICATIONS\$\$\$
 WORKSTATION: \$TERMINAL\$ DATE: \$\$\$DATE\$\$\$

MAINTENANCE OF TRAFFIC NOTES

D03-BH-FY2009(A)

DESIGNED
CAL
CHECKED
HYH

ITEM 614 - MAINTAINING TRAFFIC

ERIE CAMPBELL STREET (COUNTY ROAD 110) DETOUR LIMITATION AND INTERIM COMPLETION DATE:

ROAD CLOSURES SHALL ONLY OCCUR FROM JUNE 15, 2008 THRU AUGUST 15, 2008 & FROM JUNE 15, 2009 THRU AUGUST 15, 2009.

TWO WAY TRAFFIC ALONG CAMPBELL STREET (COUNTY ROAD 110) SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FOURTEEN (14) CALENDAR DAYS, WHEN THROUGH TRAFFIC WILL BE DETOURED AS SHOWN ON THIS SHEET.

THE CONTRACTOR SHALL NOTIFY THE ERIE COUNTY ENGINEER IN WRITING, A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE DATE THE COUNTY ROAD 110 DETOUR WILL BE IMPLEMENTED.

THE CONTRACTOR SHALL ALSO NOTIFY, IN WRITING, THE FOLLOWING AGENCIES AT LEAST TEN (10) DAYS PRIOR TO THE TIME THE COUNTY ROAD 110 DETOUR WILL BE IMPLEMENTED:

- TOWNSHIP(S)
- LOCAL FIRE DEPARTMENT(S)
- LOCAL SCHOOL DISTRICT(S)
- COUNTY SHERIFF(S)

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE THE COUNTY ROAD 110 DETOUR SIGNING AS DETAILED ON THIS SHEET. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE GATES AND BARRICADES AT THE APPROXIMATE WORK LIMITS OF THE PROJECT AND THE ADVANCE WARNING SIGNS AS SHOWN ON STANDARD CONSTRUCTION DRAWING MT-101.60.

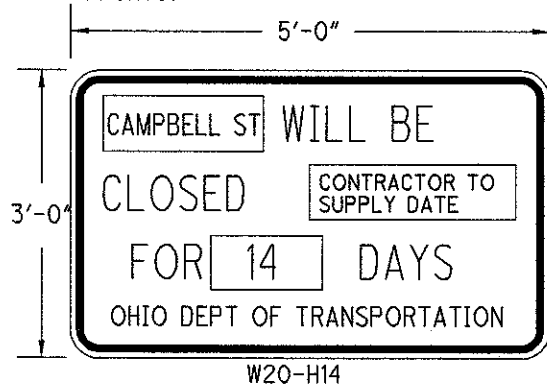
THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE (SECTION 108), AND FOR EACH CALENDAR DAY BEYOND THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES, AS PER SECTION 108.07 OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, AS PER SECTION 614.02 (A).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATION, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTICE OF CLOSURE SIGNS

THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE LOCATED IN THE FIELD SO AS NOT TO INTERFERE WITH ANY PERMANENT SIGNS. ON THIS PROJECT, THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC AND SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE SIGNS INCLUDING SUPPORTS.

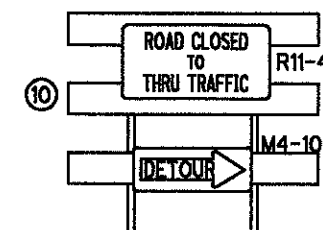
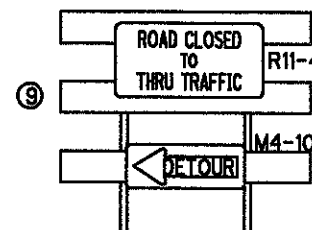
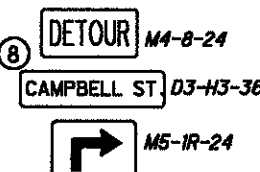
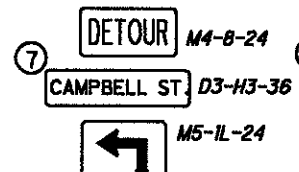
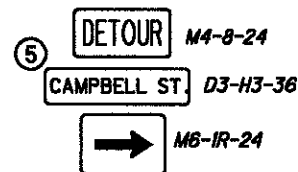
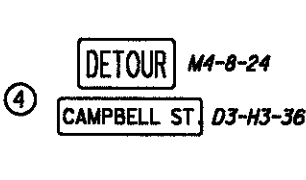
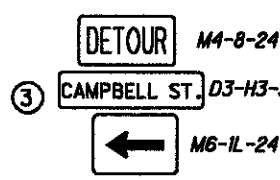
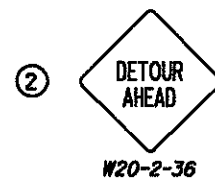
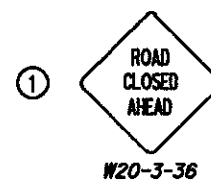


DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614, DETOUR SIGNING

LUMP



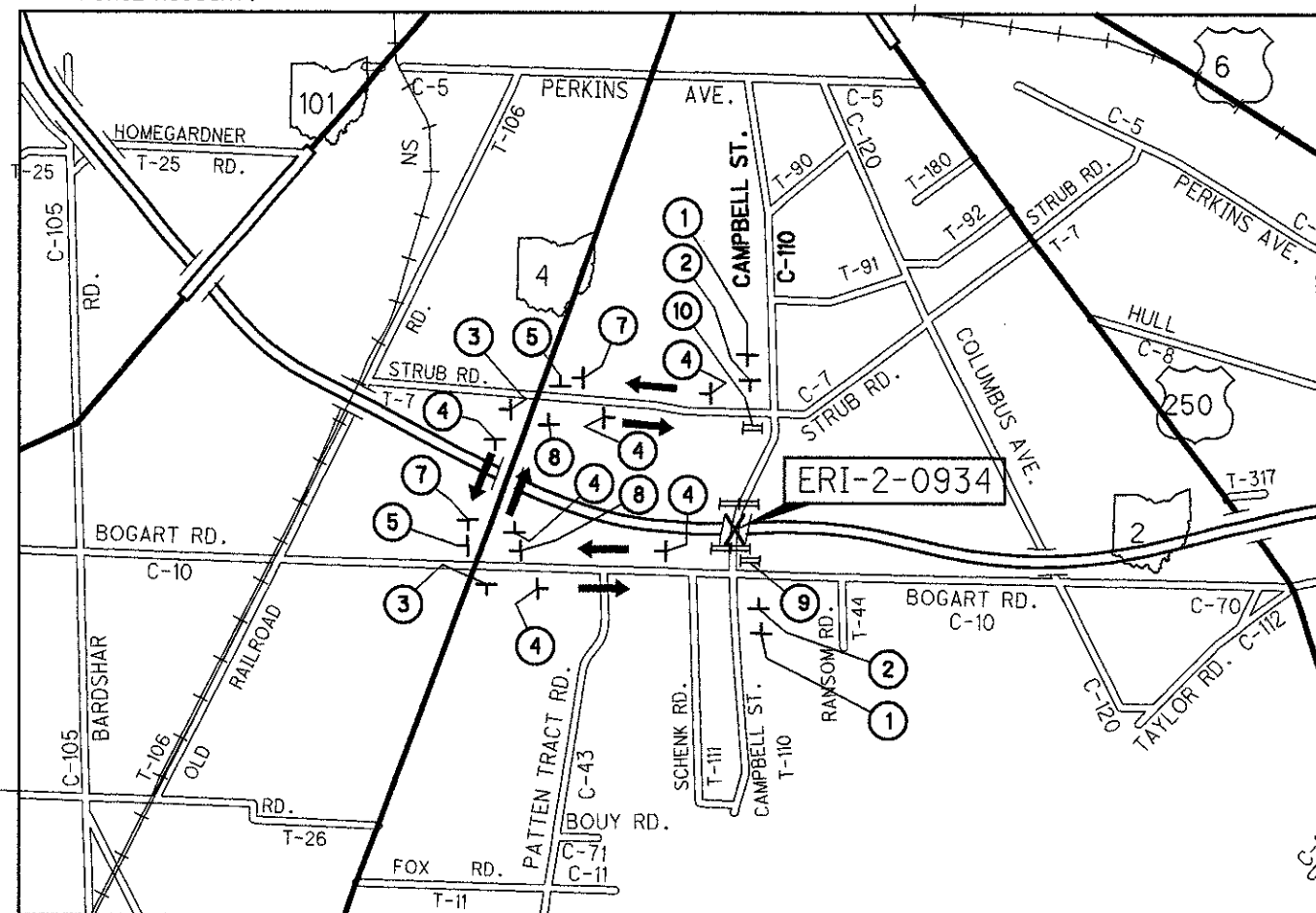
TYPE III BARRICADES

TYPE III BARRICADES

MAINTENANCE OF DETOUR ROUTE

DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THE ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DIRECTED BY THE ENGINEER. THE DETOUR ROUTE IS TO BE REVIEWED AND REPAIRED PRIOR TO THE ASPHALT CONTRACTOR OR SUBCONTRACTOR LEAVING THE PROJECT.

PAYMENT FOR THE WORK NECESSARY TO REPAIR THE DETOUR ROUTE WILL BE PERFORMED BY EITHER CHANGE ORDER OR FORCE ACCOUNT.



ERIE COUNTY ROAD 110 DETOUR MAP

MAP LEGEND

- X - PROJECT LOCATION
- ⇄ - OFFICIAL SIGNED DETOUR
- || - GATES AND BARRICADES, AS PER MT-101.60



SIGN LEGEND

DESIGN FILE: \$\$\$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$\$ WORKSTATION: \$TERMINALS\$ DATE: \$\$\$\$\$\$DATE\$\$\$\$\$

MAINTENANCE OF TRAFFIC NOTES - ERI-2-0934 (CAMPBELL ST.)

D03-BH-FY2009(A)

ITEM 614 - MAINTAINING TRAFFIC

ERIE COUNTY'S GALLOWAY RD. (TOWNSHIP ROAD 118) DETOUR LIMITATION AND INTERIM COMPLETION DATE:

ROAD CLOSURES SHALL ONLY OCCUR FROM JUNE 15, 2008 THRU AUGUST 15, 2008 & FROM JUNE 15, 2009 THRU AUGUST 15, 2009.

TWO WAY TRAFFIC ALONG TOWNSHIP ROAD 118 SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FOURTEEN (14) CALENDAR DAYS, WHEN THROUGH TRAFFIC WILL BE DETOURED AS SHOWN ON THIS SHEET.

THE CONTRACTOR SHALL NOTIFY THE ERIE COUNTY ENGINEER IN WRITING, A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE DATE THE TOWNSHIP ROAD 118 DETOUR WILL BE IMPLEMENTED.

THE CONTRACTOR SHALL ALSO NOTIFY, IN WRITING, THE FOLLOWING AGENCIES AT LEAST TEN (10) DAYS PRIOR TO THE TIME THE TOWNSHIP ROAD 118 DETOUR WILL BE IMPLEMENTED:

- TOWNSHIP(S)
- LOCAL FIRE DEPARTMENT(S)
- LOCAL SCHOOL DISTRICT(S)
- COUNTY SHERIFF(S)

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE THE TOWNSHIP ROAD 118 DETOUR SIGNING AS DETAILED ON THIS SHEET. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE GATES AND BARRICADES AT THE APPROXIMATE WORK LIMITS OF THE PROJECT AND THE ADVANCE WARNING SIGNS AS SHOWN ON STANDARD CONSTRUCTION DRAWING MT-101.80.

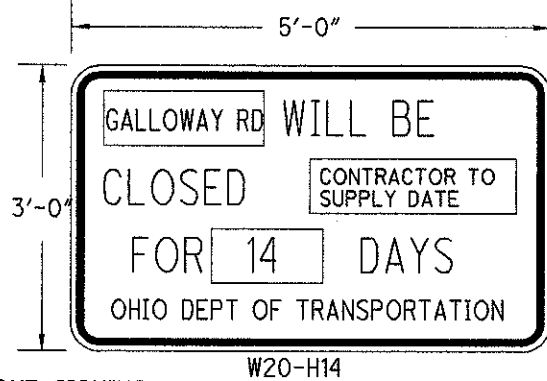
THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE (SECTION 108), AND FOR EACH CALENDAR DAY BEYOND THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES, AS PER SECTION 108.07 OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, AS PER SECTION 614.02 (A).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATION, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTICE OF CLOSURE SIGNS

THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE LOCATED IN THE FIELD SO AS NOT TO INTERFERE WITH ANY PERMANENT SIGNS. ON THIS PROJECT, THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC AND SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE SIGNS INCLUDING SUPPORTS.

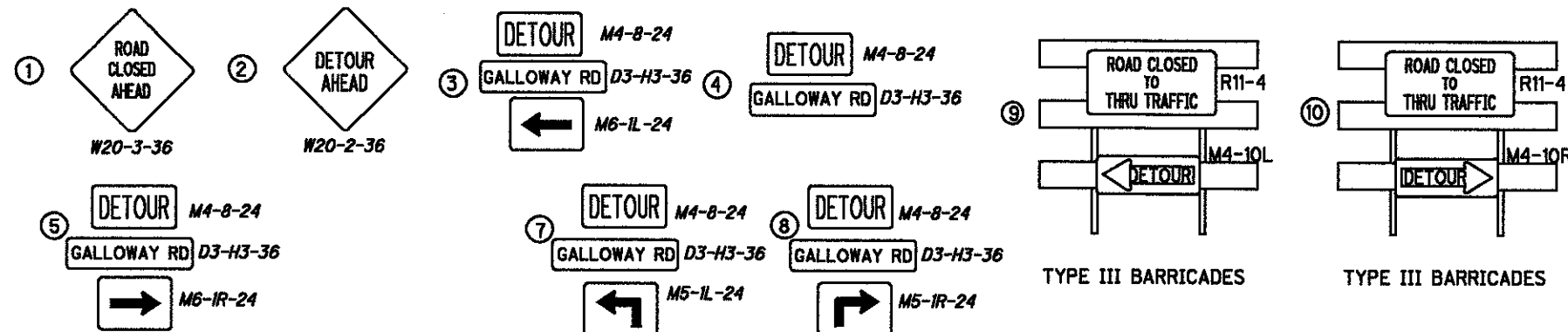


DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614, DETOUR SIGNING

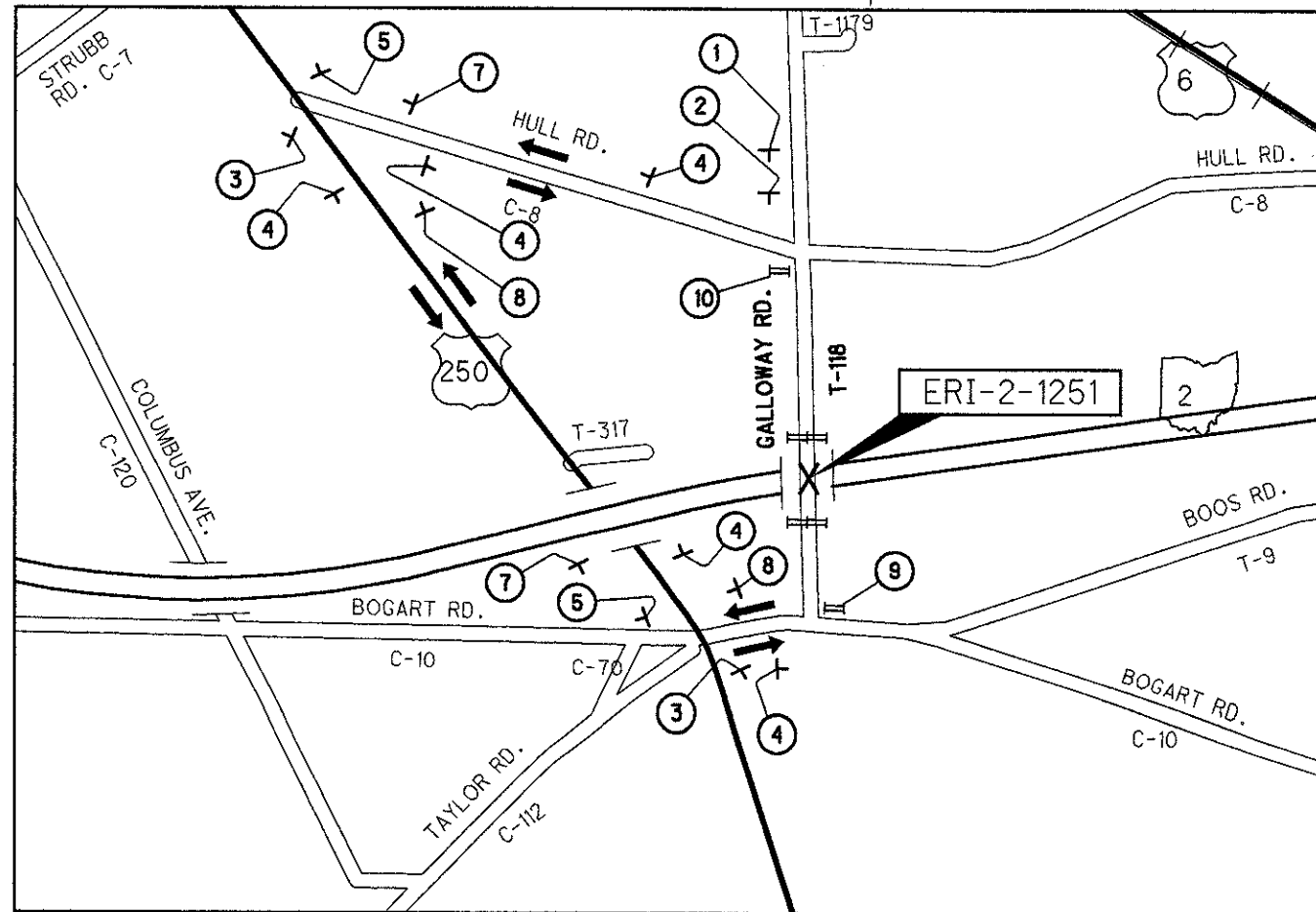
LUMP



MAINTENANCE OF DETOUR ROUTE

DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THE ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DIRECTED BY THE ENGINEER. THE DETOUR ROUTE IS TO BE REVIEWED AND REPAIRED PRIOR TO THE ASPHALT CONTRACTOR OR SUBCONTRACTOR LEAVING THE PROJECT.

PAYMENT FOR THE WORK NECESSARY TO REPAIR THE DETOUR ROUTE WILL BE PERFORMED BY EITHER CHANGE ORDER OR FORCE ACCOUNT.

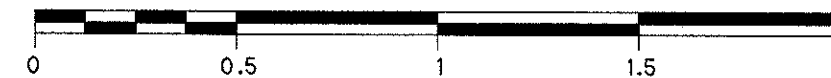


ERIE COUNTY TOWNSHIP ROAD 118 DETOUR MAP

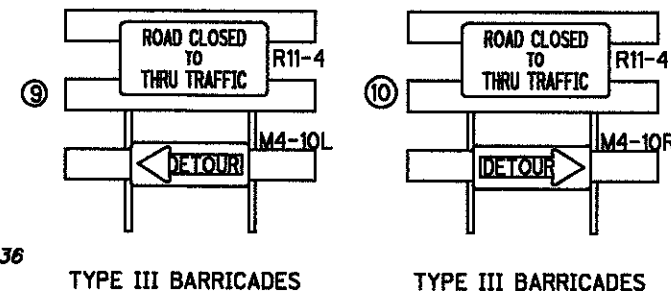
MAP LEGEND

- X - PROJECT LOCATION
- ⇄ - OFFICIAL SIGNED DETOUR
- || - GATES AND BARRICADES, AS PER MT-101.80

SCALE IN MILES



SIGN LEGEND



DESIGN FILE: \$\$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$\$
WORKSTATION: \$TERMINAL\$ DATE: \$\$\$\$DATE\$\$\$\$\$

MAINTENANCE OF TRAFFIC NOTES - ERI-2-1251 (GALLOWAY RD.)

D03-BH-FY2009(A)

ITEM 614 - MAINTAINING TRAFFIC

RICHLAND ROCK RD. (COUNTY ROAD 150) DETOUR LIMITATION AND INTERIM COMPLETION DATE:

ROCK RD. (COUNTY ROAD 150) SHALL NOT BE CLOSED AT THE SAME TIME AS TOWNSHIP ROAD 161 (LEWIS ROAD).

TWO WAY TRAFFIC ALONG COUNTY ROAD 150 SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FOURTEEN (14) CALENDAR DAYS, WHEN THROUGH TRAFFIC WILL BE DETOURED AS SHOWN ON THIS SHEET.

THE CONTRACTOR SHALL NOTIFY THE RICHLAND COUNTY ENGINEER IN WRITING, A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE DATE THE COUNTY ROAD 150 DETOUR WILL BE IMPLEMENTED.

THE CONTRACTOR SHALL ALSO NOTIFY, IN WRITING, THE FOLLOWING AGENCIES AT LEAST TEN (10) DAYS PRIOR TO THE TIME THE COUNTY ROAD 150 DETOUR WILL BE IMPLEMENTED:

- CITY OF ONTARIO
- TOWNSHIP(S)
- LOCAL FIRE DEPARTMENT(S)
- LOCAL SCHOOL DISTRICT(S)
- COUNTY SHERIFF(S)

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE THE COUNTY ROAD 150 DETOUR SIGNING AS DETAILED ON THIS SHEET. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE GATES AND BARRICADES AT THE APPROXIMATE WORK LIMITS OF THE PROJECT AND THE ADVANCE WARNING SIGNS AS SHOWN ON STANDARD CONSTRUCTION DRAWING MT-101.60.

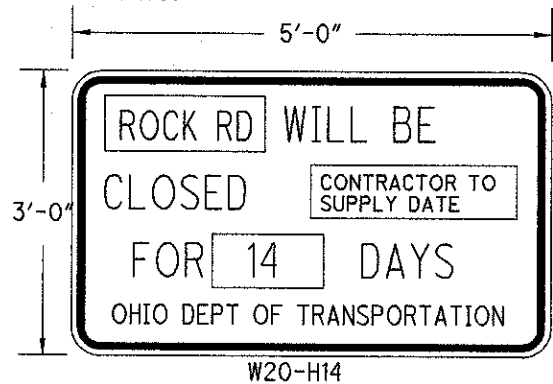
THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE (SECTION 108), AND FOR EACH CALENDAR DAY BEYOND THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES, AS PER SECTION 108.07 OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, AS PER SECTION 614.02 (A).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATION, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTICE OF CLOSURE SIGNS

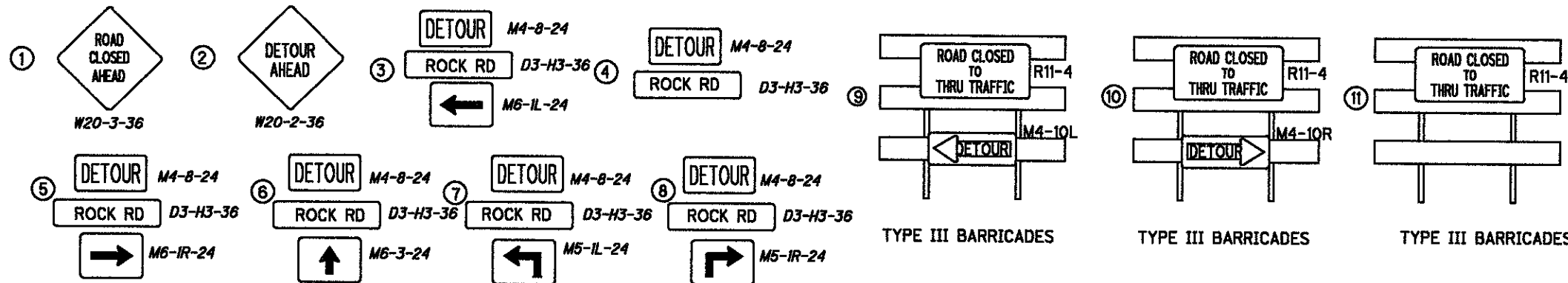
THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE LOCATED IN THE FIELD SO AS NOT TO INTERFERE WITH ANY PERMANENT SIGNS. ON THIS PROJECT, THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC AND SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE SIGNS INCLUDING SUPPORTS.



DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

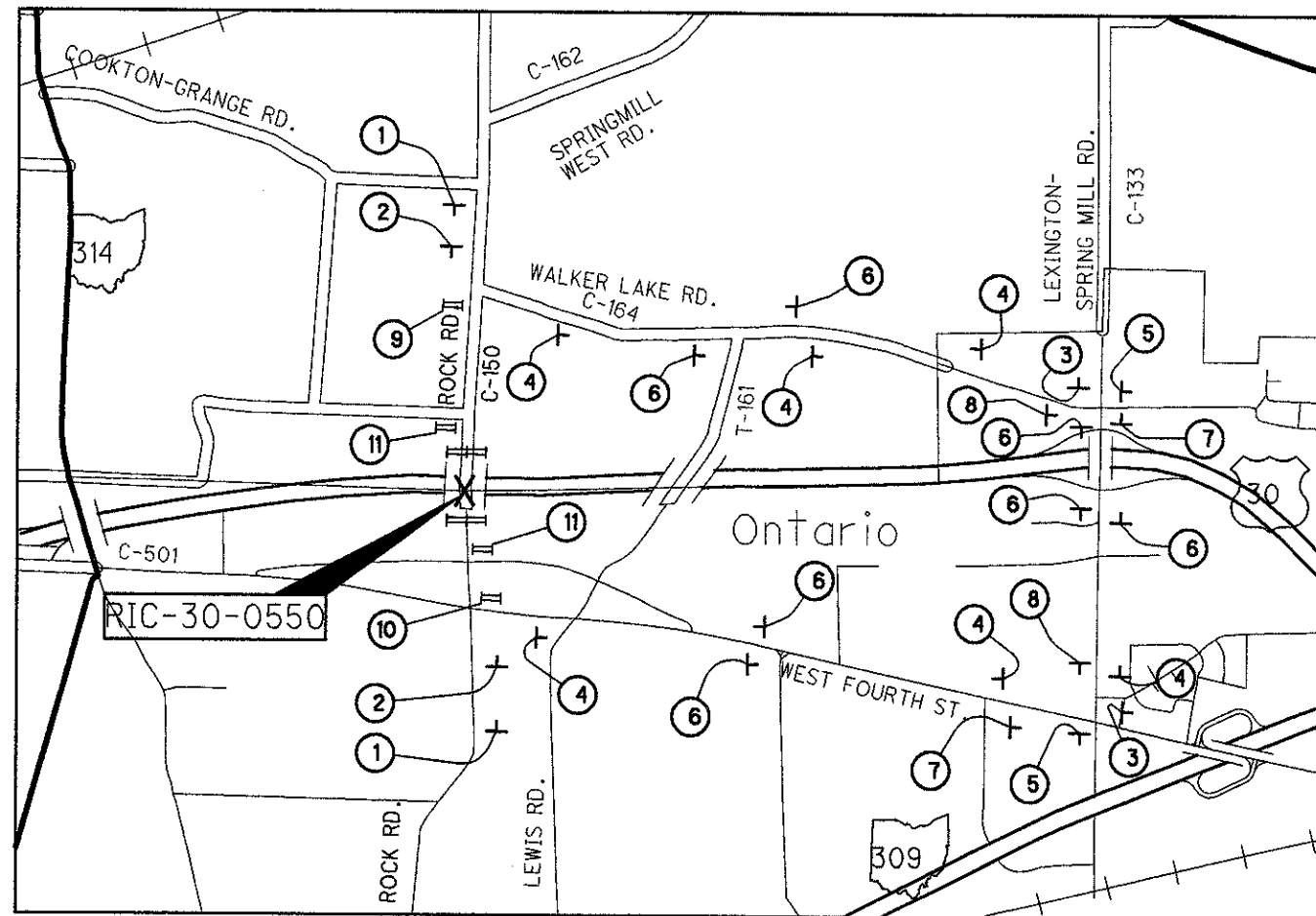
ITEM 614, DETOUR SIGNING LUMP



MAINTENANCE OF DETOUR ROUTE

DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THE ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DIRECTED BY THE ENGINEER. THE DETOUR ROUTE IS TO BE REVIEWED AND REPAIRED PRIOR TO THE ASPHALT CONTRACTOR OR SUBCONTRACTOR LEAVING THE PROJECT.

PAYMENT FOR THE WORK NECESSARY TO REPAIR THE DETOUR ROUTE WILL BE PERFORMED BY EITHER CHANGE ORDER OR FORCE ACCOUNT.



RICHLAND COUNTY ROAD 150 (ROCK ROAD) DETOUR MAP

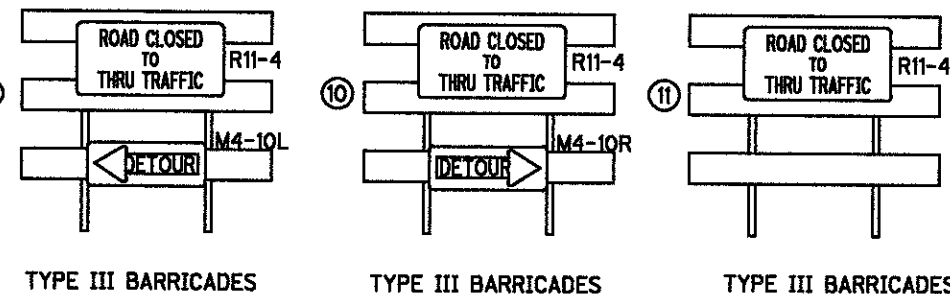
MAP LEGEND

- X - PROJECT LOCATION
- ← - OFFICIAL SIGNED DETOUR
- || - GATES AND BARRICADES, AS PER MT-101.60

SCALE IN MILES



SIGN LEGEND



DESIGN FILE: \$\$\$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$\$ WORKSTATION: \$TERMINAL\$ DATE: \$\$\$\$\$DATE\$\$\$\$\$

CALCULATED: CAL CHECKED: HYH
 MAINTENANCE OF TRAFFIC NOTES - RIC-30-0550 (ROCK RD.)
 D03-BH-FY2009(A)
 8/21

ITEM 614 - MAINTAINING TRAFFIC

RICHLAND COUNTY LEWIS RD. (TOWNSHIP ROAD 161) DETOUR LIMITATION AND INTERIM COMPLETION DATE:

LEWIS RD. (TOWNSHIP ROAD 161) SHALL NOT BE CLOSED AT THE SAME TIME AS COUNTY ROAD 150 (ROCK ROAD).

TWO WAY TRAFFIC ALONG TOWNSHIP ROAD 161 SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FOURTEEN (14) CALENDAR DAYS, WHEN THROUGH TRAFFIC WILL BE DETOURED AS SHOWN ON THIS SHEET.

THE CONTRACTOR SHALL NOTIFY THE RICHLAND COUNTY ENGINEER IN WRITING, A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE DATE THE TOWNSHIP ROAD 161 DETOUR WILL BE IMPLEMENTED.

THE CONTRACTOR SHALL ALSO NOTIFY, IN WRITING, THE FOLLOWING AGENCIES AT LEAST TEN (10) DAYS PRIOR TO THE TIME THE TOWNSHIP ROAD 161 DETOUR WILL BE IMPLEMENTED:

- CITY OF ONTARIO
- TOWNSHIP(S)
- LOCAL FIRE DEPARTMENT(S)
- LOCAL SCHOOL DISTRICT(S)
- COUNTY SHERIFF(S)

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE THE TOWNSHIP ROAD 161 DETOUR SIGNING AS DETAILED ON THIS SHEET. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE GATES AND BARRICADES AT THE APPROXIMATE WORK LIMITS OF THE PROJECT AND THE ADVANCE WARNING SIGNS AS SHOWN ON STANDARD CONSTRUCTION DRAWING MT-101.60.

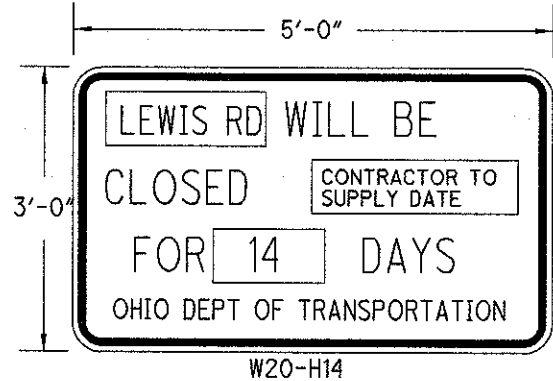
THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE (SECTION 108), AND FOR EACH CALENDAR DAY BEYOND THE FOURTEEN (14) CONSECUTIVE CALENDAR DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES, AS PER SECTION 108.07 OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, AS PER SECTION 614.02 (A).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATION, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTICE OF CLOSURE SIGNS

THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE LOCATED IN THE FIELD SO AS NOT TO INTERFERE WITH ANY PERMANENT SIGNS. ON THIS PROJECT, THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC AND SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE SIGNS INCLUDING SUPPORTS.

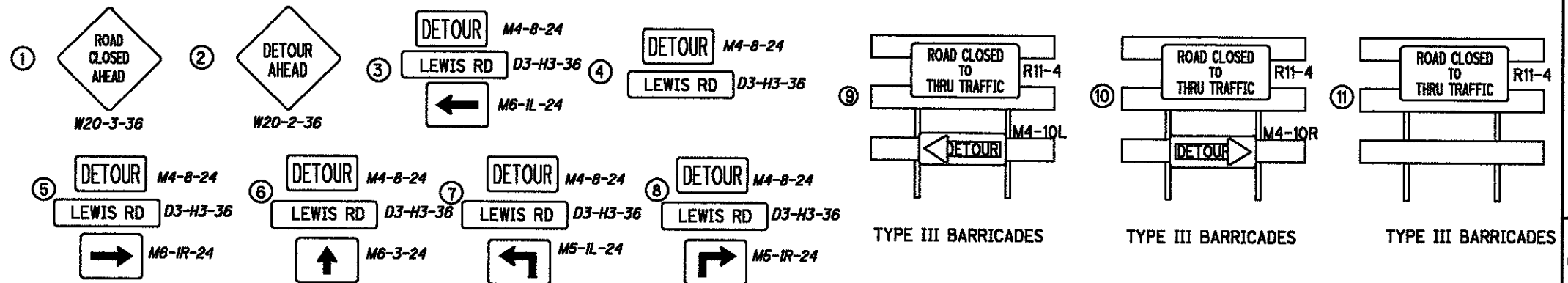


DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614, DETOUR SIGNING

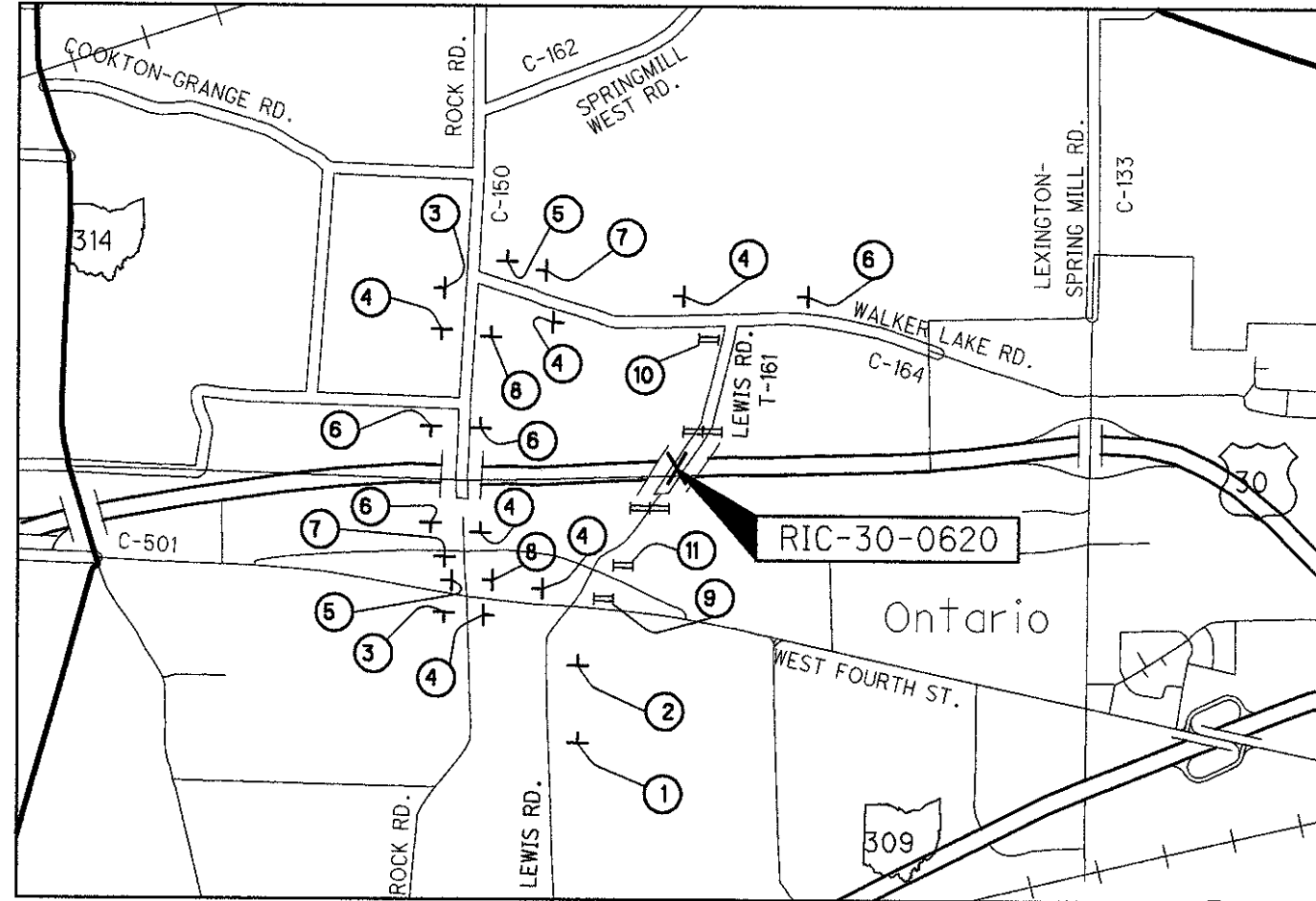
LUMP



MAINTENANCE OF DETOUR ROUTE

DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THE ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DIRECTED BY THE ENGINEER. THE DETOUR ROUTE IS TO BE REVIEWED AND REPAIRED PRIOR TO THE ASPHALT CONTRACTOR OR SUBCONTRACTOR LEAVING THE PROJECT.

PAYMENT FOR THE WORK NECESSARY TO REPAIR THE DETOUR ROUTE WILL BE PERFORMED BY EITHER CHANGE ORDER OR FORCE ACCOUNT.



RICHLAND COUNTY TOWNSHIP ROAD 161 (LEWIS ROAD) DETOUR MAP

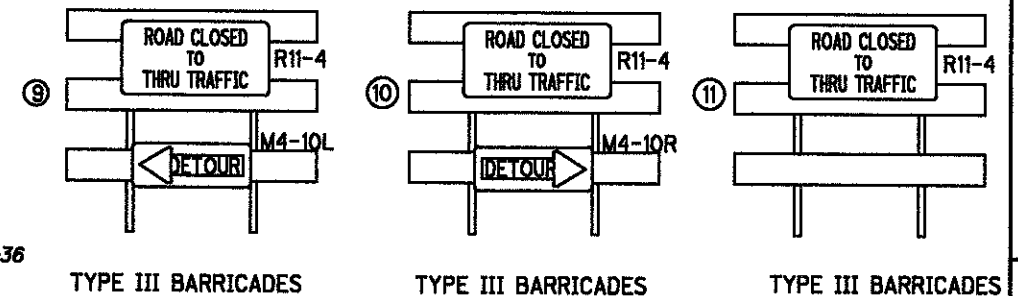
MAP LEGEND

- X - PROJECT LOCATION
- ↔ - OFFICIAL SIGNED DETOUR
- I - GATES AND BARRICADES, AS PER MT-101.60

SCALE IN MILES



SIGN LEGEND



DESIGN FILE: \$\$\$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$\$ WORKSTATION: \$TERMINAL\$ DATE: \$\$\$\$\$\$DATE\$\$\$\$\$

CALCULATED
CREATED
HYH

MAINTENANCE OF TRAFFIC NOTES - RIC-30-0620 (LEWIS RD.)

D03-BH-FY2009(A)

DESIGN F. 2008-03-05 08:00 AM
 WORKSHEET: D:\PROJECTS\2008\2008-03-05\2008-03-05.DAT

SHEET NUMBER												ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.				
12	13	14	15	16	17	18	19	20	21												
PAVEMENT																					
						2	2					209	60200	4	STA	LINEAR GRADING					
						378	356			480		254	01000	1214	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE					
												30	28		38	407	10000	96	GALLON	TACK COAT	3
												16	15		25	448	47020	56	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22	
TRAFFIC CONTROL																					
																621	00100	1	EACH	RPM	
																621	54000	1	EACH	RAISED PAVEMENT MARKER REMOVED	
	0.08	0.13		0.13		0.13	0.15		0.03		0.05					646	10001	0.7	MILE	EDGE LINE, AS PER PLAN	3
									0.30							646	10101	0.30	MILE	LANE LINE, AS PER PLAN	3
	0.04	0.06		0.06		0.06	0.07				0.02					646	10201	0.31	MILE	CENTER LINE, AS PER PLAN	3
ALTERNATE BID																					
	0.08	0.13		0.13		0.13	0.15		0.03		0.05					646	10000	0.7	MILE	EDGE LINE	3
									0.30							646	10100	0.30	MILE	LANE LINE	3
	0.04	0.06		0.06		0.06	0.07				0.02					646	10200	0.31	MILE	CENTER LINE	3
STRUCTURES (OVER 20')																					
STRUCTURE HUR-303-0187 (SFN 3904083)																					
												550				848	10001	550	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (2 1/2" THICK)	3
												550				848	20000	550	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION	
												15				848	30001	15	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN	3
												17				848	50000	17	SQ YD	HAND CHIPPING	
												LUMP				848	50100	LUMP		TEST SLAB	
												1				848	50200	1	CU YD	FULL DEPTH REPAIR	
												550				848	50321	550	SQ YD	EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN (1 1/2" NOMINAL THICKNESS)	3
												28				848	50340	28	SQ YD	REMOVAL OF DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY	
STRUCTURE ERI-2-0934 (SFN 2200724)																					
						2										202	11301	2	CU YD	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	2
						1										511	45701	1	CU YD	CLASS C CONCRETE, ABUTMENT, AS PER PLAN (REPAIR)	3
						1										511	71100	1	CU YD	CONCRETE, MISC: APPROACH SLAB REPAIR	3
		300														512	10050	300	SQ YD	SEALING OF CONCRETE SURFACES (NON-EPOXY)	
																516	31000	60	FT	JOINT SEALER	
																516	31001	60	FT	JOINT SEALER, AS PER PLAN	3
		927														848	10001	927	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (1 1/4" THICK)	3
		927														848	20000	927	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION	
																848	30001	13	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN	3
																848	50000	28	SQ YD	HAND CHIPPING	
																848	50100	LUMP		TEST SLAB	
																848	50200	1	CU YD	FULL DEPTH REPAIR	
STRUCTURE ERI-2-1251 (SFN 2200996)																					
						2										202	11301	2	CU YD	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	2
						1										511	45701	1	CU YD	CLASS C CONCRETE, ABUTMENT, AS PER PLAN (REPAIR)	3
						1										511	71100	1	CU YD	CONCRETE, MISC: APPROACH SLAB REPAIR	3
																512	10050	297	SQ YD	SEALING OF CONCRETE SURFACES (NON-EPOXY)	
																516	31000	48	FT	JOINT SEALER	
																516	31001	48	FT	JOINT SEALER, AS PER PLAN	3
																848	10001	729	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (1 1/4" THICK)	3
																848	20000	729	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION	
																848	30001	10	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN	3
																848	50000	22	SQ YD	HAND CHIPPING	
																848	50100	LUMP		TEST SLAB	
																848	50200	1	CU YD	FULL DEPTH REPAIR	

GENERAL SUMMARY

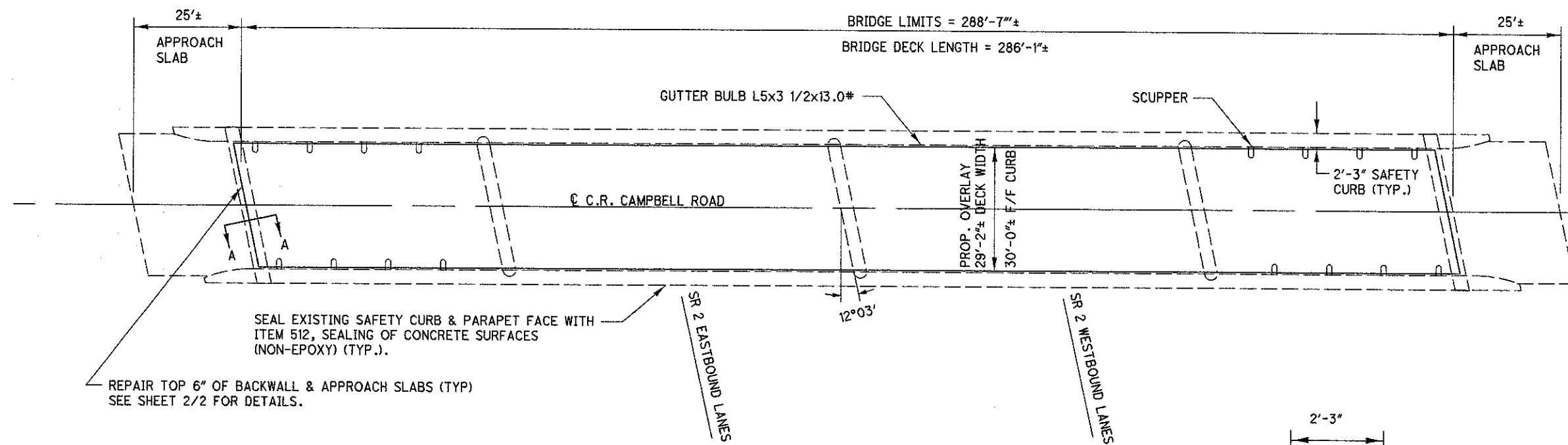
D03-BH-FY2009(A)

SHEET NUMBER														ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.				
6	7	8	9	12	13	14	15	16	17	18	19	20	21										
																		STRUCTURE RIC-30-0550 (SFN 7001010)					
														85	202	98200	85	FT	REMOVAL MISC.: EXPANSION JOINT EXTENSION	2			
														201	512	10050	201	SQ YD	SEALING OF CONCRETE SURFACES (NON-EPOXY)				
														85	516	31001	85	FT	JOINT SEALER, AS PER PLAN	3			
														1098	848	10001	1098	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (3½" THICK)	3			
														1098	848	20000	1098	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION				
														43	848	30001	43	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN	3			
														40	848	50000	40	SQ YD	HAND CHIPPING				
														LUMP	848	50100	LUMP		TEST SLAB				
														2	848	50200	2	CU YD	FULL DEPTH REPAIR				
														1098	848	50300	1098	SQ YD	WEARING COURSE REMOVED, ASPHALT				
																			STRUCTURE RIC-30-0620 (SFN 7001029)				
														85	202	98200	85	FT	REMOVAL MISC.: EXPANSION JOINT EXTENSION	2			
														245	512	10050	245	SQ YD	SEALING OF CONCRETE SURFACES (NON-EPOXY)				
														85	516	31001	85	FT	JOINT SEALER, AS PER PLAN	3			
														1351	848	10001	1351	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (3½" THICK)	3			
														1351	848	20000	1351	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION				
														53	848	30001	53	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN	3			
														45	848	50000	45	SQ YD	HAND CHIPPING				
														LUMP	848	50100	LUMP		TEST SLAB				
														1	848	50200	1	CU YD	FULL DEPTH REPAIR				
														1351	848	50300	1351	SQ YD	WEARING COURSE REMOVED, ASPHALT				
																			STRUCTURE RIC-30-1750R (SFN 7001592)				
														202	848	10001	202	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (3" THICK)	3			
														202	848	20000	202	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION				
														7	848	30001	7	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN	3			
														6	848	50000	6	SQ YD	HAND CHIPPING				
														LUMP	848	50100	LUMP		TEST SLAB				
														1	848	50200	1	CU YD	FULL DEPTH REPAIR				
														202	848	50321	202	SQ YD	EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN	3			
														10	848	50340	10	SQ YD	REMOVAL OF DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY				
																			STRUCTURE ERI-2-1096L (SFN 2200848)				
															202	11301	1	CU YD	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	2			
															512	10100	1	SQ YD	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)				
															395	842	10000	395	POUND	CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE			
																			STRUCTURE RIC-430-1010 (SFN 7006284)				
																	1729	842	10000	1729	POUND	CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE	
																			MAINTENANCE OF TRAFFIC				
LUMP	LUMP	LUMP	LUMP												614	12420	LUMP	DETOUR SIGNING					
															614	11000	LUMP	MAINTAINING TRAFFIC					
															624	10000	LUMP	MOBILIZATION					

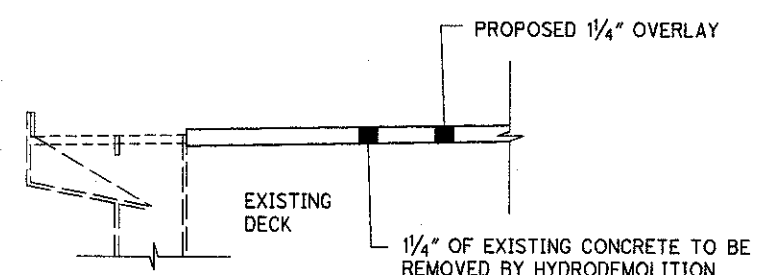
GENERAL SUMMARY

D03-BH-FY2009(A)

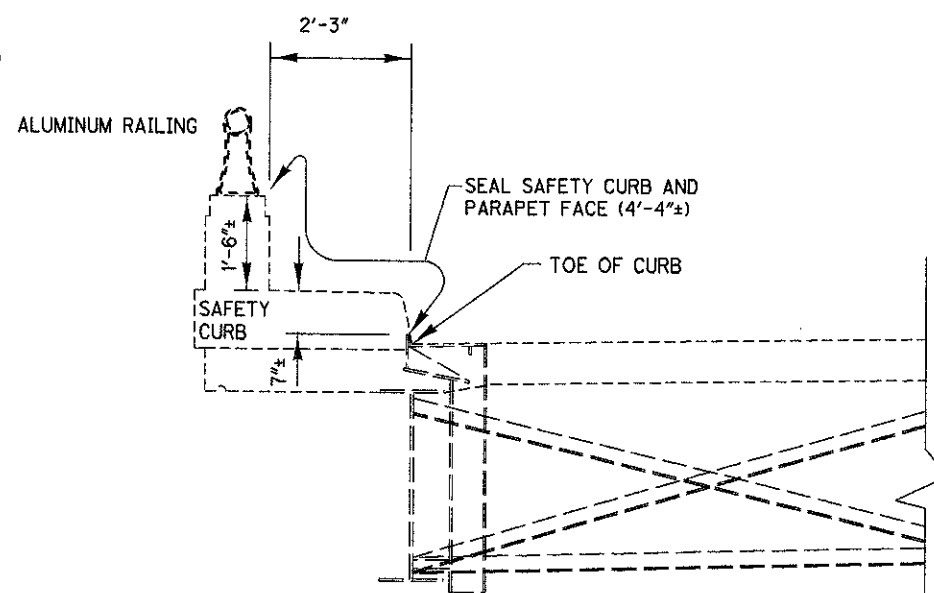
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WORKSTAT 11/11/08 DGNF ILESPECIFICATIONS *****



PLAN VIEW



ELEVATION VIEW AT SCUPPER



SEALING OF CONCRETE SURFACES

SEALING SAFETY CURB AND PARAPET FACE ON BRIDGE DECK & WINGWALLS (AVG. LENGTH= 311'-6"±).

ITEM	QUANTITY	UNIT	DESCRIPTION
512	300	SQ YD	SEALING CONCRETE SURFACES (NON-EPOXY)
646	0.13	MILE	EDGE LINE, AS PER PLAN
646	0.06	MILE	CENTER LINE, AS PER PLAN
848	927	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (1/4" THICK)
848	927	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION
848	13	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN
848	28	SQ YD	HAND CHIPPING
848	LUMP		TEST SLAB
848	1	CU YD	FULL-DEPTH REPAIR

QUANTITIES CARRIED TO GENERAL SUMMARY

NOTES:

- 1) THE EXISTING APPROACH GUARDRAIL AND BRIDGE RAIL IS NOT SHOWN.
- 2) THE PROPOSED OVERLAY PROFILE ELEVATIONS SHALL MATCH THE EXISTING BRIDGE DECK PROFILE ELEVATIONS.
- 3) THE PROPOSED OVERLAY SHALL BE SLOPED TO DRAIN TO THE EXISTING SCUPPERS; HOWEVER, THE EXISTING SCUPPERS SHALL NOT BE DISTURBED.
- 4) FOR BACKWALL & APPROACH SLAB REPAIR DETAILS, SEE SHEET 2/2.

DESIGN FILE: \$\$\$\$.DGNFILESPECIFICATIONS\$\$\$
 WORKSTATION: \$TERMINALS DATE: \$\$\$DATE\$\$\$

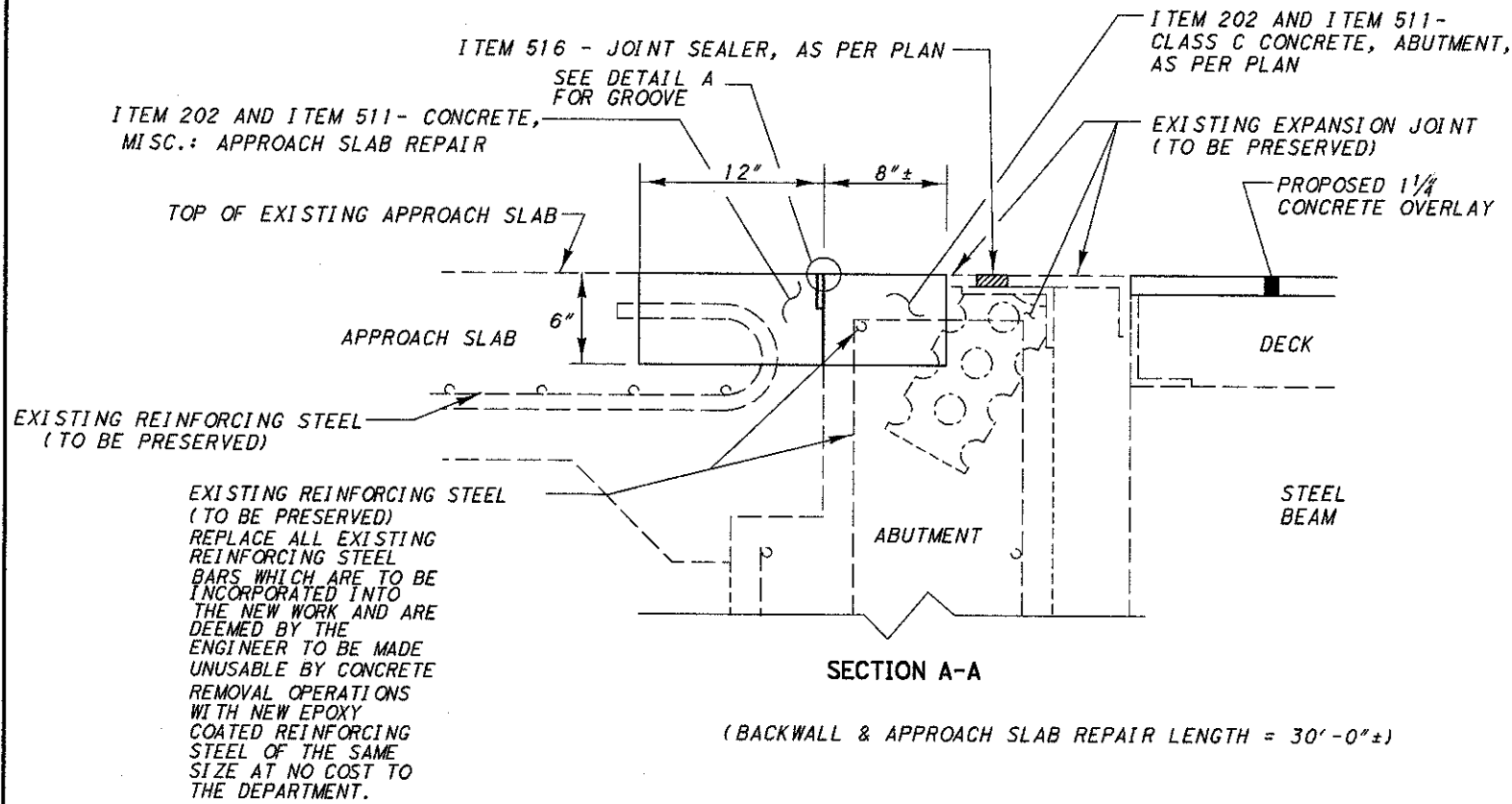
DESIGN AGENCY
 DISTRICT THREE
 OFFICE OF PRODUCTION

DESIGNED	DATE	REVISED	DATE
CAL	3/08	RDN	3/08
CHECKED		STRUCTURE FILE NUMBER	2200724
HYH			

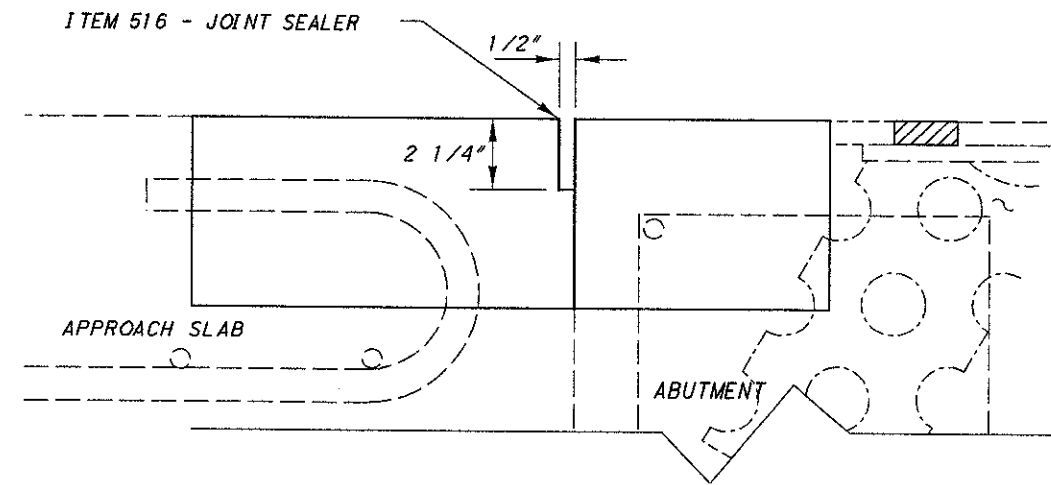
PLAN VIEW
 ERI-2-0934
 UNDER CAMPBELL STREET (C.R. 110)

D03-BH-FY2009(A)

DESIGN FILE: \$\$\$\$.DGNFILESPECIFICATIONS\$\$\$
 WORKSTATION: \$TERMINAL\$ DATE: \$\$\$SDATE\$\$\$



SECTION A-A
 (BACKWALL & APPROACH SLAB REPAIR LENGTH = 30'-0"±)



DEATIL A
 APPROACH SLAB GROOVE LENGTH = 30'-0"±

ITEM	QUANTITY	UNIT	DESCRIPTION
202	2	CU YD	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
511	1	CU YD	CLASS C CONCRETE, ABUTMENT, AS PER PLAN (REPAIR)
511	1	CU YD	CONCRETE, MISC.: APPROACH SLAB REPAIR
516	60	FT	JOINT SEALER
516	60	FT	JOINT SEALER, AS PER PLAN

QUANTITIES CARRIED TO GENERAL SUMMARY

DESIGN AGENCY
 DISTRICT THREE
 OFFICE OF PRODUCTION

DATE
 3/08
 REVISION
 RDN
 STRUCTURE FILE NUMBER
 2200724

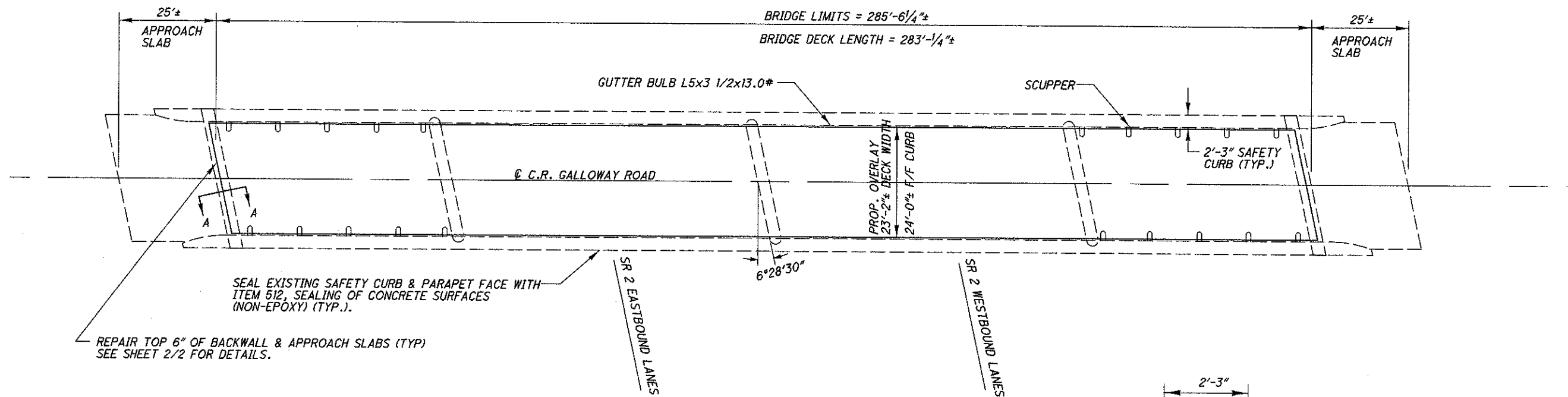
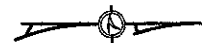
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PLAN VIEW
 ERI-2-0934
 UNDER CAMPBELL STREET (C.R. 110)

D03-BH-FY2009(A)

2 / 2

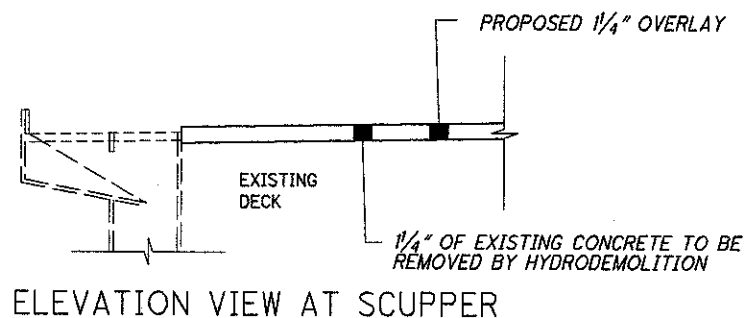
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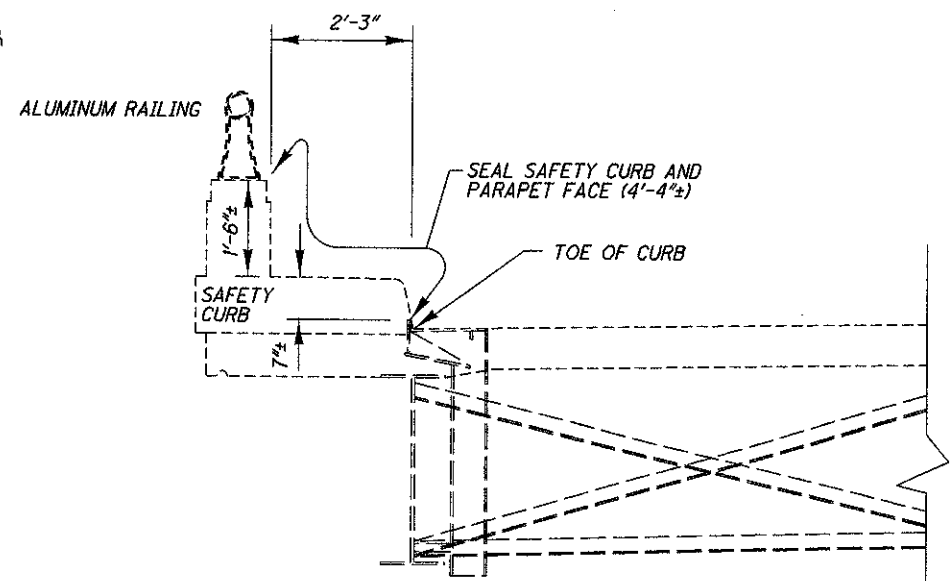
SEAL EXISTING SAFETY CURB & PARAPET FACE WITH ITEM 512, SEALING OF CONCRETE SURFACES (NON-EPOXY) (TYP.).

REPAIR TOP 6" OF BACKWALL & APPROACH SLABS (TYP) SEE SHEET 2/2 FOR DETAILS.

PLAN VIEW



ELEVATION VIEW AT SCUPPER



SEALING OF CONCRETE SURFACES

SEALING SAFETY CURB AND PARAPET FACE ON BRIDGE DECK & WINGWALLS (AVG. LENGTH= 309'±).

ITEM	QUANTITY	UNIT	DESCRIPTION
512	297	SQ YD	SEALING CONCRETE SURFACES (NON-EPOXY)
646	0.13	MILE	EDGE LINE, AS PER PLAN
646	0.06	MILE	CENTER LINE, AS PER PLAN
848	729	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (1/4" THICK)
848	729	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION
848	10	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN
848	22	SQ YD	HAND CHIPPING
848	LUMP		TEST SLAB
848	1	CU YD	FULL-DEPTH REPAIR

QUANTITIES CARRIED TO GENERAL SUMMARY

NOTES:

- 1) THE EXISTING APPROACH GUARDRAIL AND BRIDGE RAIL IS NOT SHOWN.
- 2) THE PROPOSED OVERLAY PROFILE ELEVATIONS SHALL MATCH THE EXISTING BRIDGE DECK PROFILE ELEVATIONS.
- 3) THE PROPOSED OVERLAY SHALL BE SLOPED TO DRAIN TO THE EXISTING SCUPPERS; HOWEVER, THE EXISTING SCUPPERS SHALL NOT BE DISTURBED.
- 4) FOR BACKWALL & APPROACH SLAB REPAIR DETAILS, SEE SHEET 2/2.

DESIGN FILE: \$\$\$\$.DGNFILESPECIFICATIONS\$\$\$
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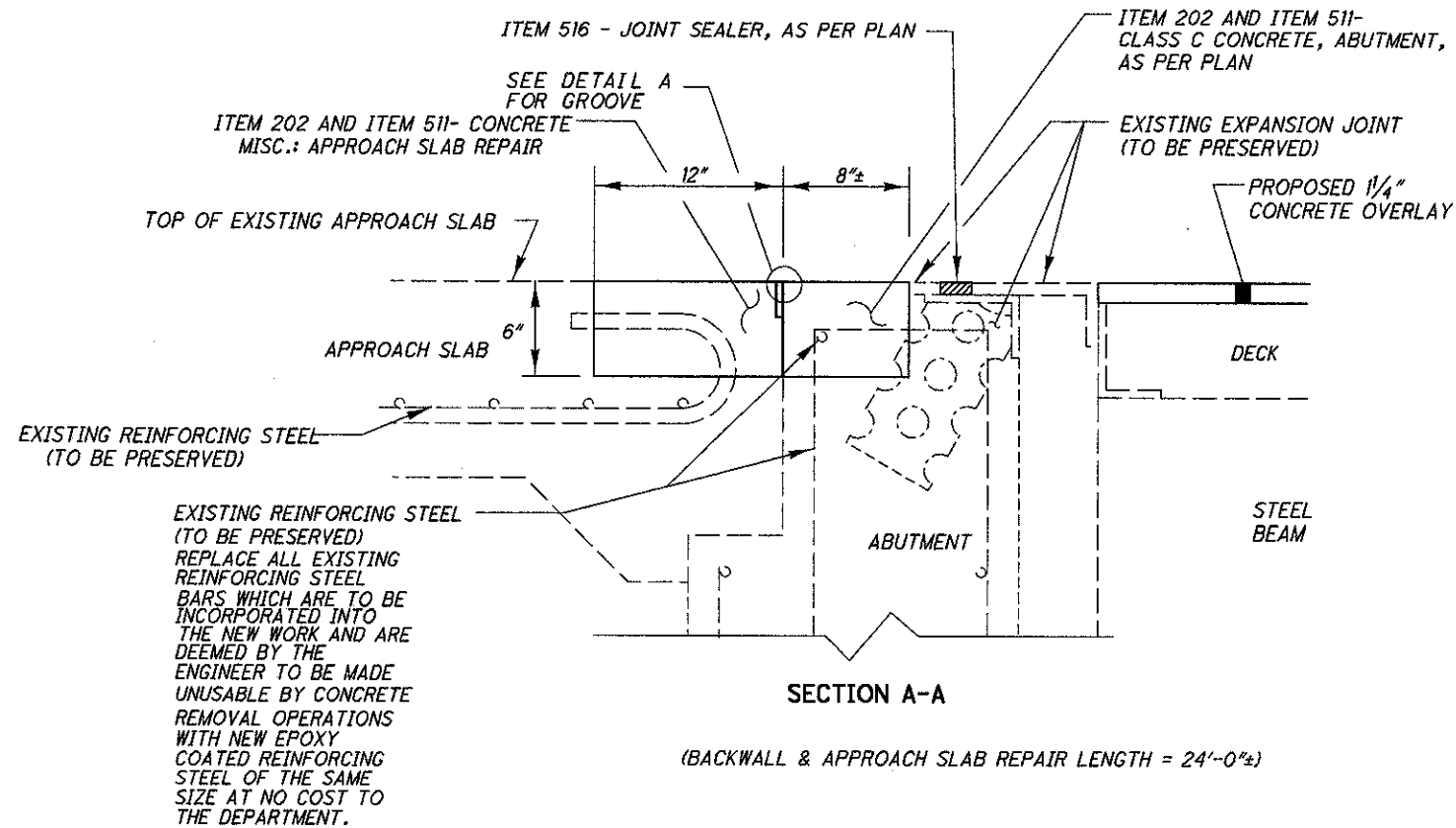
DESIGN AGENCY
 DISTRICT THREE
 OFFICE OF PRODUCTION

DATE
 3/08
 REVISION
 RDN
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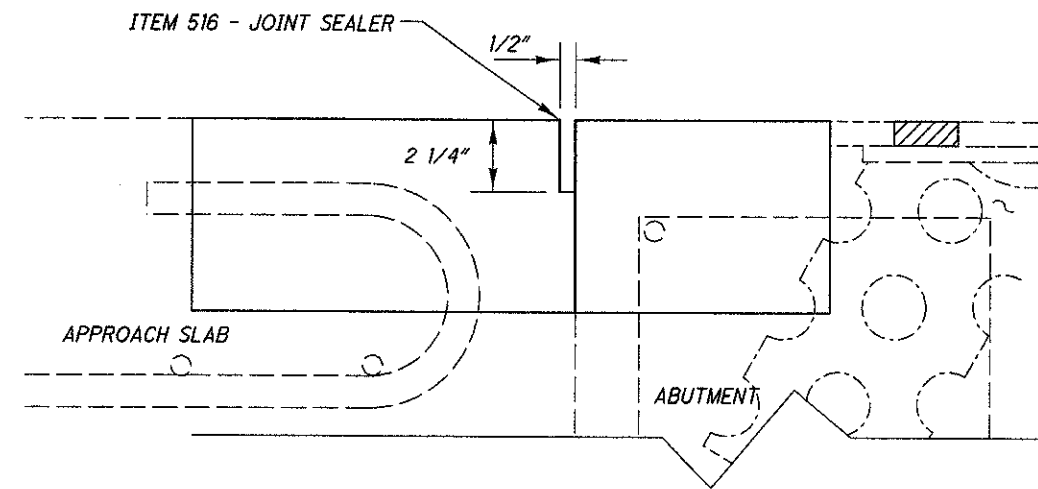
PLAN VIEW
 ERI-2-1251
 UNDER GALLOWAY ROAD (T.R. 118)

D03-BH-FY2009(A)

DESIGN FILE: \$\$\$\$.DGNFILESPECIFICATIONS\$\$\$
 WORKSTATION: \$TERMINALS\$ DATE: \$\$\$DATE\$\$\$



(BACKWALL & APPROACH SLAB REPAIR LENGTH = 24'-0"±)



DEATIL A
 APPROACH SLAB GROOVE LENGTH = 24'-0"±

ITEM	QUANTITY	UNIT	DESCRIPTION
202	2	CU YD	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
511	1	CU YD	CLASS C CONCRETE, ABUTMENT, AS PER PLAN (REPAIR)
511	1	CU YD	CONCRETE, MISC: APPROACH SLAB REPAIR
516	48	FT	JOINT SEALER
516	48	FT	JOINT SEALER, AS PER PLAN

QUANTITIES CARRIED TO GENERAL SUMMARY

DESIGN AGENCY
 DISTRICT THREE
 OFFICE OF PRODUCTION

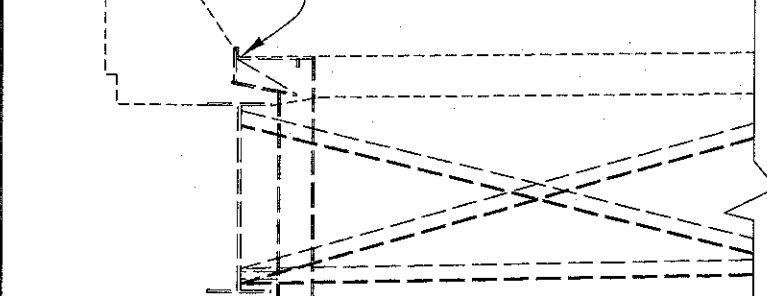
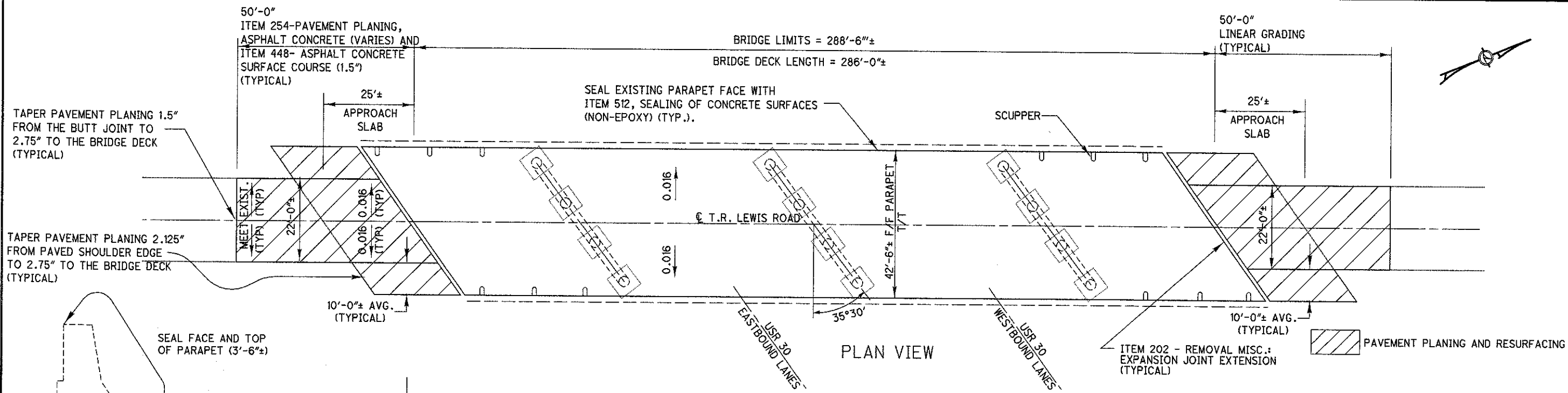
DATE
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 REVISIONS
 RDN
 STRUCTURE FILE NUMBER
 2200896

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 ERI-2-1251
 UNDER GALLOWAY ROAD (T.R. 118)

D03-BH-FY2009(A)

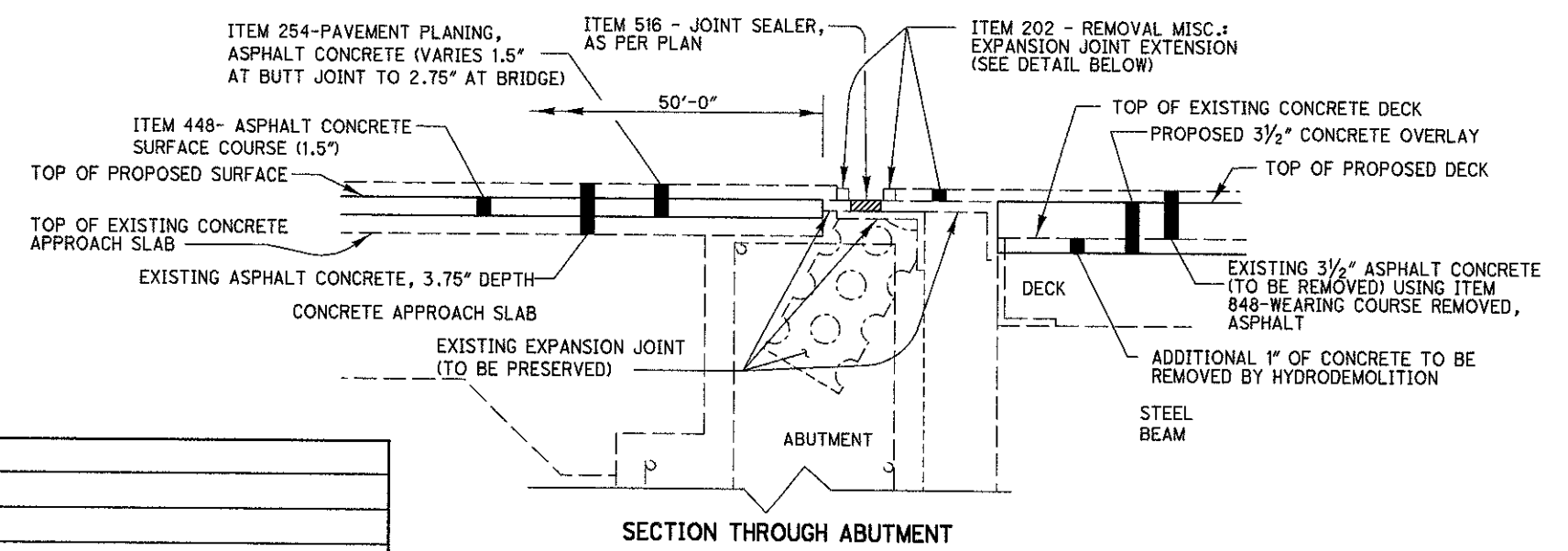
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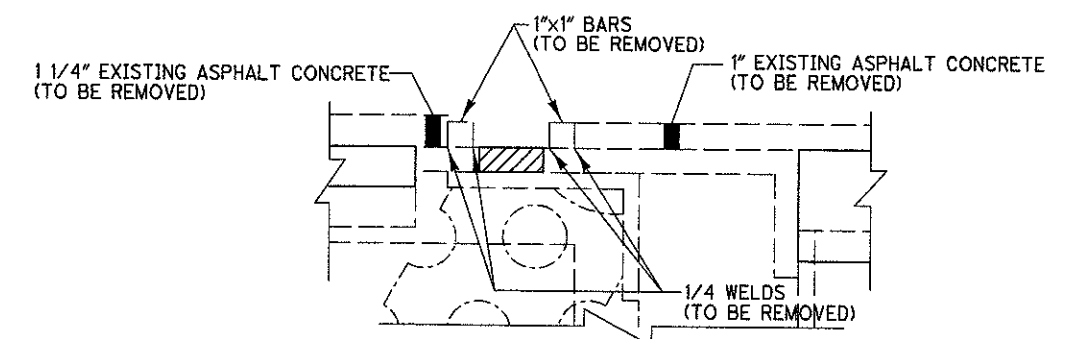


SEALING OF CONCRETE SURFACES

SEALING THE FACE AND TOP OF THE PARAPET (LENGTH= 284'-11"± BRIDGE DECK & 30'-3"± ROADWAY).



SECTION THROUGH ABUTMENT



DETAIL FOR ITEM 202 - REMOVAL MISC.: EXPANSION JOINT EXTENSION

ITEM	QUANTITY	UNIT	DESCRIPTION
202	85	FT	REMOVAL MISC.: EXPANSION JOINT EXTENSION
209	2	STA	LINEAR GRADING
254	356	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE (1.5")
407	28	GALLON	TACK COAT
448	15	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22
512	245	SQ YD	SEALING CONCRETE SURFACES (NON-EPOXY)
516	85	FT	JOINT SEALER, AS PER PLAN
646	0.15	MILE	EDGE LINE, AS PER PLAN
646	0.07	MILE	CENTER LINE, AS PER PLAN
848	1351	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (3 1/2" THICK)
848	1351	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION
848	53	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN
848	45	SQ YD	HAND CHIPPING
848	LUMP		TEST SLAB
848	1	CU YD	FULL-DEPTH REPAIR
848	1351	SQ YD	WEARING COURSE REMOVED, ASPHALT

QUANTITIES CARRIED TO GENERAL SUMMARY

NOTES:

- 1) THE EXISTING APPROACH GUARDRAIL AND BRIDGE PARAPETS ARE NOT SHOWN.
- 2) THE PROPOSED OVERLAY SHALL BE SLOPED TO DRAIN TO THE EXISTING SCUPPERS; HOWEVER, THE EXISTING SCUPPERS SHALL NOT BE DISTURBED.

DESIGN FILE: \$\$\$\$.DGNFILESPECIFICATION\$\$\$\$\$
 WORKSTATION: \$TERMINAL\$ DATE: \$\$\$DATE\$\$\$\$\$

DESIGN AGENCY: DISTRICT THREE
 OFFICE OF PRODUCTION

DATE: 3/08
 RDN: STRUCTURE FILE NUMBER 7001029

DRAIN: GTS
 CHECKED: HYH

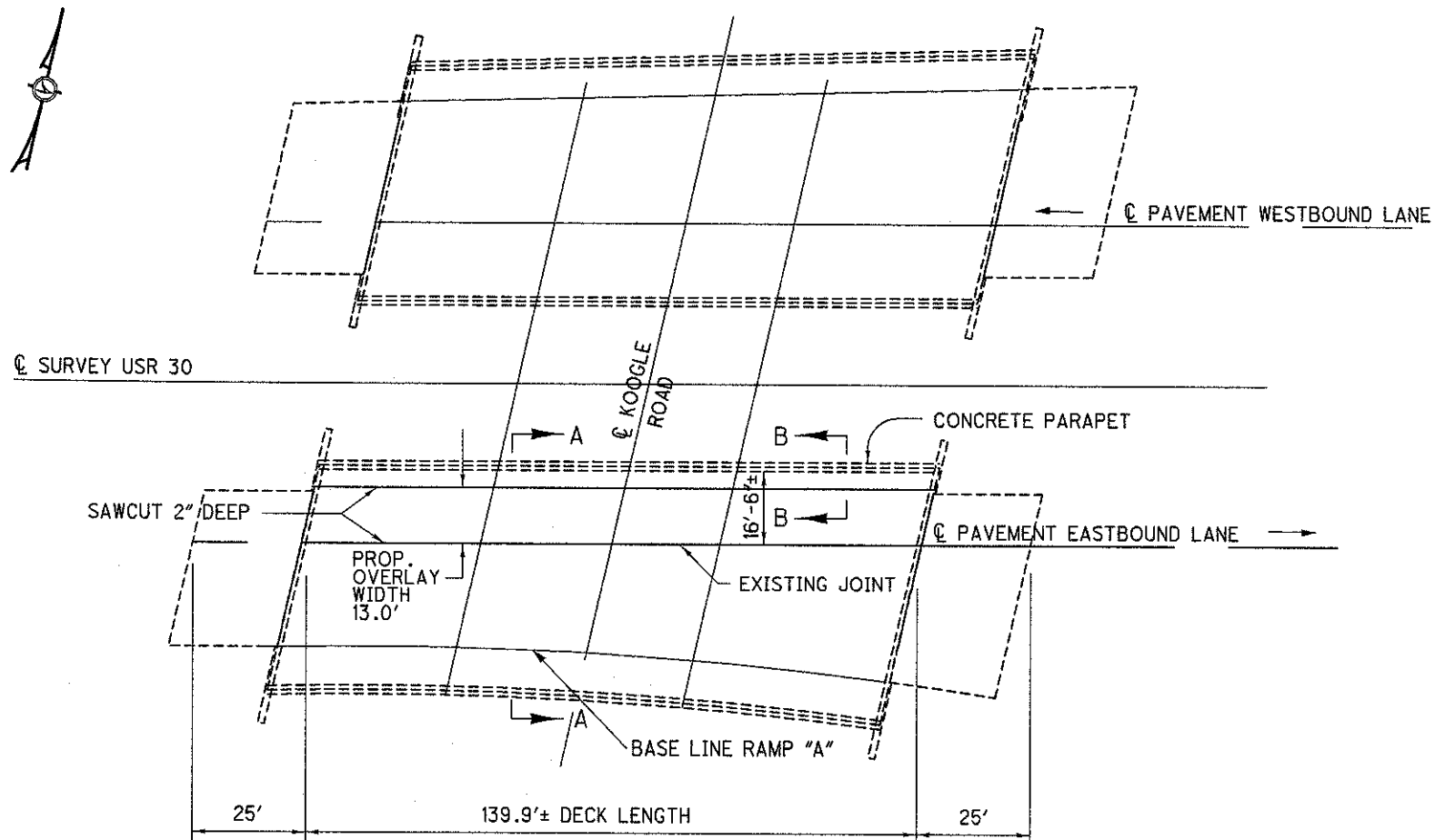
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PLAN VIEW
 RIC-30-0620
 UNDER LEWIS ROAD (T.R. 161)

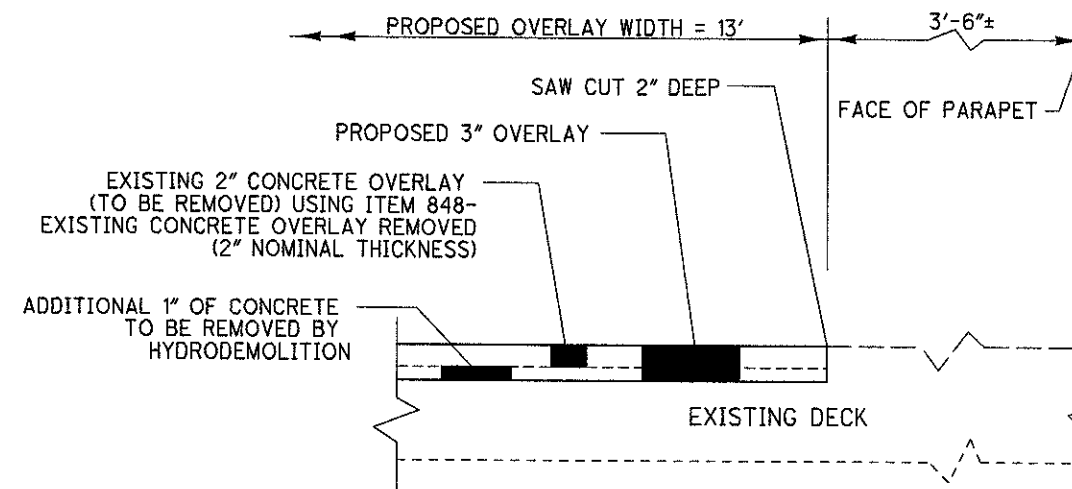
D03 BH FY2009(A)

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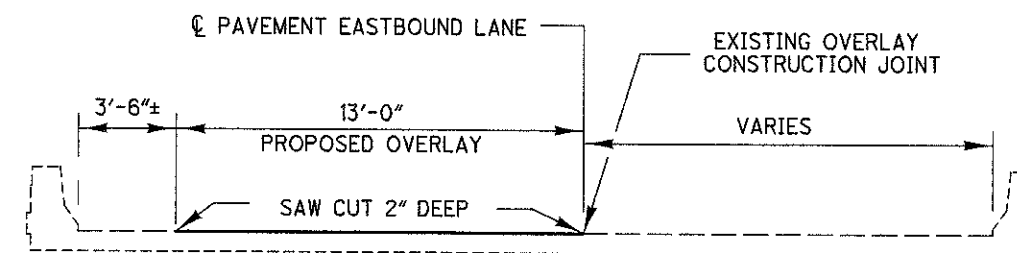
18
 21



PLAN VIEW



OVERLAY DETAIL B-B



SECTION A-A
(RIGHT STRUCTURE ONLY)

ITEM	QUANTITY	UNIT	DESCRIPTION
621	1	EACH	RAISED PAVEMENT MARKER REMOVED
621	1	EACH	RPM
646	0.03	MILE	EDGE LINE, AS PER PLAN
646	0.30	MILE	LANE LINE, AS PER PLAN
848	202	SQ YD	MICRO SILICA MODIFIED CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN (3" THICK)
848	202	SQ YD	SURFACE PREPARATION USING HYDRODEMOLITION
848	7	CU YD	MICRO SILICA MODIFIED CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN
848	6	SQ YD	HAND CHIPPING
848	lump		TEST SLAB
848	1	CU YD	FULL-DEPTH REPAIR
848	202	SQ YD	EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN (2" NOMINAL THICKNESS)
848	10	SQ YD	REMOVAL OF DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY

QUANTITIES CARRIED TO GENERAL SUMMARY

NOTES:

- 1) THE EXISTING APPROACH GUARDRAIL IS NOT SHOWN.
- 2) THE PROPOSED OVERLAY ELEVATION SHALL MATCH THE EXISTING OVERLAY ELEVATION.
- 3) SAW CUT EXISTING DECK 2" DEEP AS SHOWN IN SECTION A-A. COST INCLUDED IN ITEM 848- EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN (2" NOMINAL THICKNESS). NEW OVERLAY SHALL BE PLACED BETWEEN THE SAW CUTS.
- 4) OVERLAY LENGTH SHALL BE THE LENGTH OF THE DECK.
- 5) THE PROPOSED RPM SHALL BE PLACED AT THE EXISTING RAISED PAVEMENT MARKER REMOVED LOCATION.

DESIGN FILE: \$\$\$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$\$
WORKSTATION: \$TERMINALS\$ DATE: \$\$\$\$\$\$DATE\$\$\$\$\$

DESIGN AGENCY
DISTRICT THREE

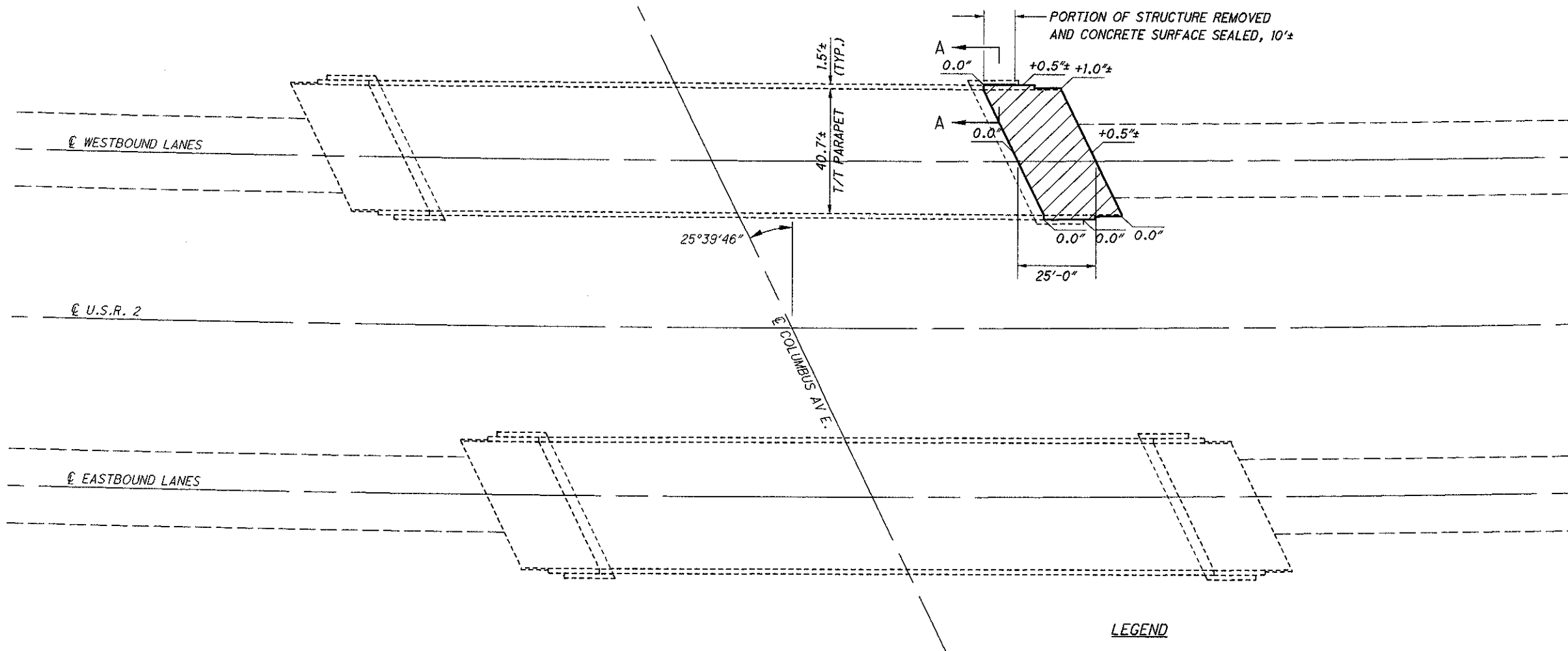
DATE 3/08
REVIEWED RDN
STRUCTURE FILE NUMBER 7001592

DRAWN CAL
CHECKED
DESIGNED CAL
CHECKED HYH

PLAN VIEW
RIC-30-1750R
OVER KOOGLE ROAD

D03-BH-2009(A)

DESIGN FILE: \$\$\$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$\$\$
 WORKSTATION: \$TERMINAL\$ DATE: \$\$\$\$\$\$DATE\$\$\$\$\$\$



LEGEND

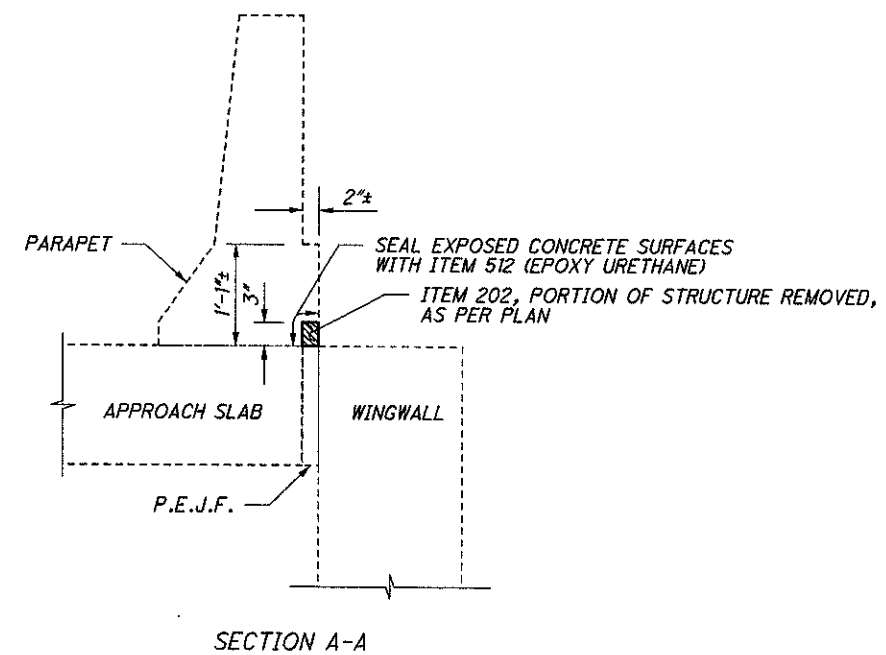
ITEM SPECIAL, CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE

ITEM	QUANTITY	UNIT	DESCRIPTION
202	1	CU YD	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
512	1	SQ YD	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)
842	395	LB	CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE

QUANTITIES CARRIED TO GENERAL SUMMARY SHEET

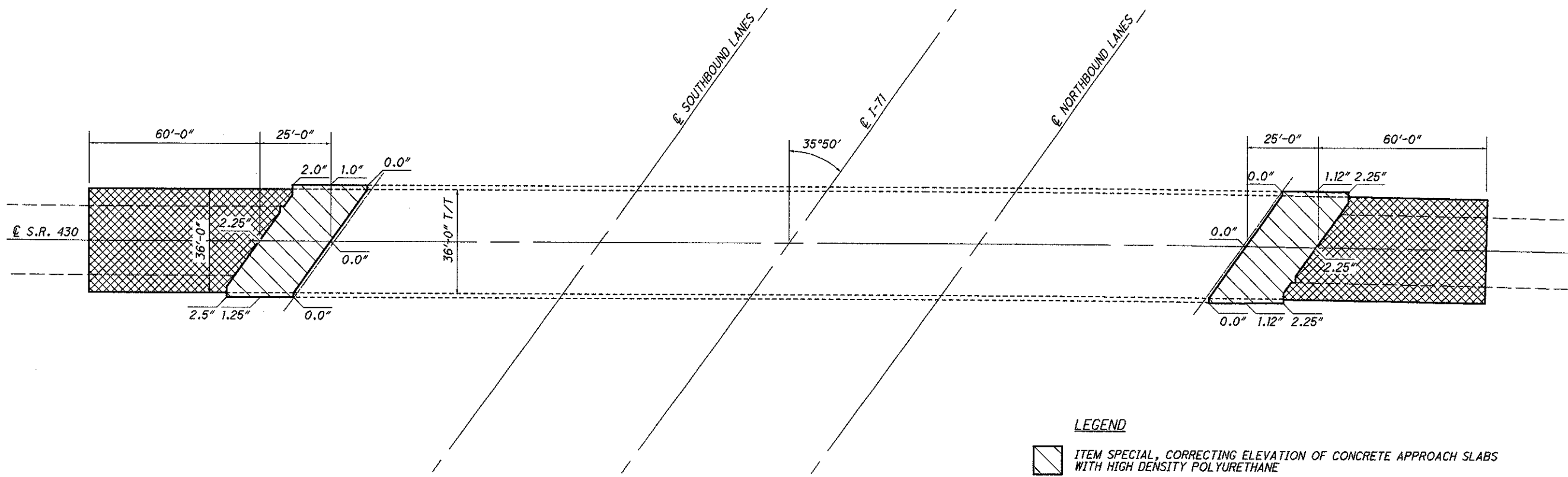
NOTES



- CORRECT ELEVATION OF THE APPROACH SLAB AT THE LOCATION SHOWN ON THIS SHEET. THE APPROACH SLAB IS TO BE EVEN WITH THE APPROACH PAVEMENT. THE ESTIMATED QUANTITY OF ITEM 842 AND THE ELEVATION CORRECTIONS SHOWN ARE APPROXIMATE.
- REMOVE PORTION OF PARAPET AT THE LOCATION AND PER THE DETAILS SHOWN ON THIS SHEET. SEAL THE EXPOSED CONCRETE SURFACE.
- APPROACH RAIL NOT SHOWN.



DESIGN AGENCY DISTRICT THREE OFFICE OF PRODUCTION	DATE 3/08	REVIEWED RDN STRUCTURE FILE NUMBER 2200848
DRAWN HYH REVISED		DESIGNED HYH CHECKED CAL
PLAN VIEW ERI-2-1096 L U.S.R. 2 OVER COLUMBUS AVE.		
D03-BH-FY2009(A)		
1 / 1		
<div style="border: 1px solid black; border-radius: 50%; width: 30px; height: 30px; display: flex; align-items: center; justify-content: center; margin: 0 auto;"> 20 21 </div>		

DESIGN FILE: \$\$\$\$.DGNFILESPECIFICATIONS\$\$\$\$
 WORKSTATION: \$TERMINAL\$ DATE: \$\$\$\$.DATE\$\$\$\$



- LEGEND**
-  ITEM SPECIAL, CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE
 -  RESURFACING

ITEM	QUANTITY	UNIT	DESCRIPTION
254	480	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE (1.5" MAX.)
407	38	GALLON	TACK COAT
448	25	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-22
646	0.05	MILE	EDGE LINE, AS PER PLAN
646	0.02	MILE	CENTER LINE, AS PER PLAN
842	1729	LB	CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE

QUANTITIES CARRIED TO GENERAL SUMMARY SHEET

- NOTES**
- CORRECT ELEVATION OF BOTH APPROACH SLABS. THE TOP OF CURB IS TO BE FLUSH AT THE APPROACH PAVEMENT JOINT. THE ESTIMATED QUANTITY OF ITEM 842 AND THE ELEVATION CORRECTIONS SHOWN ARE APPROXIMATE.
 - CREATE BUTT JOINT IN APPROACH PAVEMENT PER BP-3.1. PAVEMENT PLANING VARIES FROM 1.5" AT BUTT JOINT TO 0" AT THE APPROACH SLAB JOINT. ASPHALT CONCRETE SURFACE COURSE DEPTH VARIES FROM 1.5" AT THE BUTT JOINT TO APPROXIMATELY 2.25" (AVG.) AT THE APPROACH SLAB JOINT. THE APPROACH PAVEMENT IS TO BE EVEN WITH THE APPROACH SLAB AT THE JOINT.
 - APPROACH RAIL NOT SHOWN.

DESIGN AGENCY DISTRICT THREE OFFICE OF PRODUCTION	
DATE 3/08	STRUCTURE FILE NUMBER T006284
DRAWN HYH	REVIEWED RDN
DESIGNED HYH	CHECKED CAL
PLAN VIEW RIC-430-1010 S.R. 430 OVER I-71	
D03-BH-FY2009(A)	
1 / 1	
21 21	