

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

AUG-33-13.44

LIMITED ACCESS

This improvement is especially designed for through traffic and has been declared a limited access highway or freeway by action of the Director in accordance with the provisions of Section 5511.02 of the Revised Code of the State of Ohio.

| DESIGN DESIGNATION | | MICROFILMED |
|---------------------------|--------------------|-------------|
| CURRENT ADT(1988) | 6690 | MAR 9 1994 |
| DESIGN YEAR ADT(2008) | 9360 | |
| D.H.V. | 656 | |
| D. | 52% | |
| T. | 22% | |
| DESIGN SPEED | 60 MPH | |
| LEGAL SPEED | 55 MPH | |
| FUNCTIONAL CLASSIFICATION | PRINCIPAL ARTERIAL | |

WIDENING OF EXISTING AND CONSTRUCTION OF PROPOSED
GRADE SEPERATION STRUCTURE OVER CSX TRANSPORTATION, INC.

CITY OF WAPAKONETA
DUCHOQUET, MOULTON AND
PUSHETA TOWNSHIPS
AUGLAIZE COUNTY

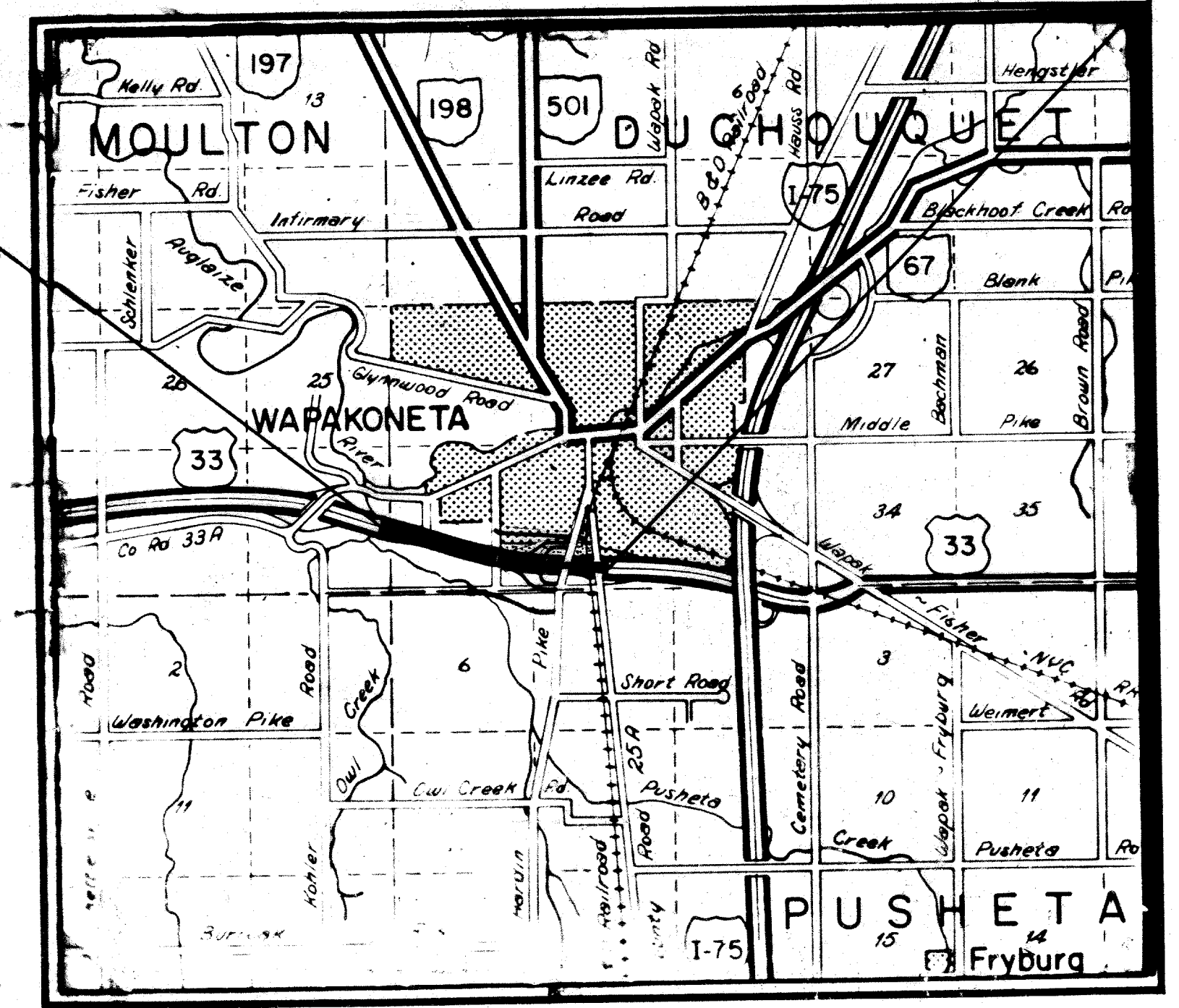
END PROJECT
AUG-33-13.44
STA. 781+72.97

--- CONVENTIONAL SIGNS

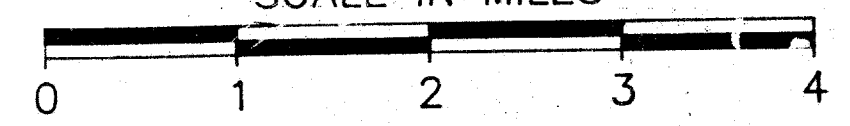
| | | | |
|--------------------------|--------------------------|-------------------------------|-----------------------------------|
| County Line | ----- | Limited Access (only) | -----LA----- |
| Township Line | ----- | Right of Way (only) | -----RW----- |
| Section Line | ----- | Limited Access & Right of Way | -----LA&RW----- |
| Corporation Line | ----- or ----- | Existing Right of Way | ----- |
| Fence Line(existing) | ---x---(proposed)---x--- | Property Line | ---P---(in existing fence)---P--- |
| Center Line | ----- | Railroad | ----- or ----- |
| Trees | ⊙ Stumps | ⊗ (to be removed) | ⊗ |
| Utility Poles: Telephone | ⊕ | Power | ⊕ |
| | | Light | ⊕ |
| | | Guardrail (existing) | o-o-o (proposed) o-o-o |

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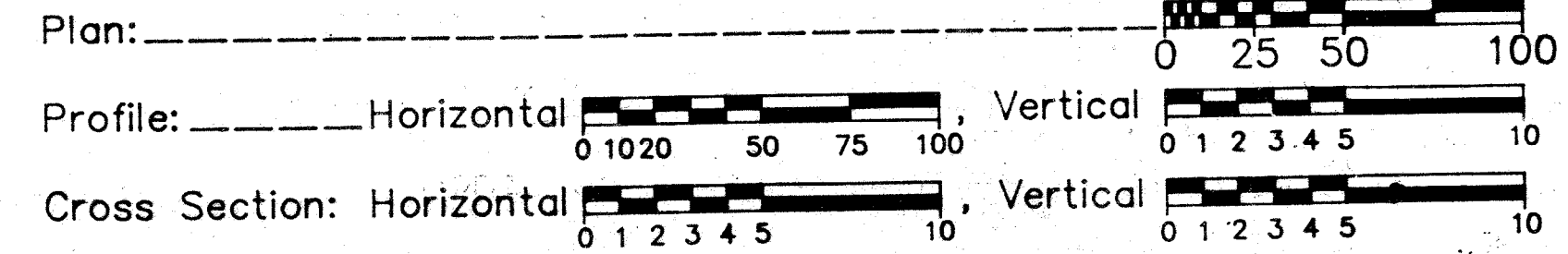


LOCATION MAP
SCALE IN MILES



Portion to be Improved: _____
State & Federal Routes: _____
Other Roads: _____

SCALES



UNDERGROUND UTILITIES

TWO WORKING DAYS
BEFORE YOU DIG

Call... 800-362-2764 (Toll Free)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY

| SUPPLEMENTAL SPECIFICATIONS | | | |
|-----------------------------|----------|-----|----------|
| 802 | 5-4-88 | 841 | 5-16-84 |
| 836 | 11-12-85 | 843 | 7-29-88 |
| 839 | 12-21-87 | 942 | 11-27-89 |
| 840 | 5-16-84 | | |
| 940 | 6-10-87 | | |
| 847 | 10-17-83 | | |
| 947 | 10-17-83 | | |
| 931 | 6-18-85 | | |
| 944 | 6-24-89 | | |

1989 SPECIFICATIONS

The standard specifications of the State of Ohio, Department of Transportation, including changes and supplemental specifications listed in the proposal shall govern this improvement.

I hereby approve these plans and declare that the making of this improvement will not require the closing to traffic of the highway, except as noted on sheets 22-23, and that provisions for the maintenance and safety of traffic will be as set forth on the plans and estimates.

Approved *Kenneth J. Kelly, P.E.*
Date 8/2/89 District Deputy Director of Transportation

Approved *B.D. Hachmann, W.T.E.*
Date _____ Engineer, Bureau of Bridges and Structural Design

Approved *Chadwick J. Smith*
Date 2/16/90 Chief Engineer, Planning & Design

Approved *Russell B. Hunt*
Date 2/16/90 Director, Department of Transportation

LINE DATA

BEGIN PROJECT = STA. 680+00.00
END PROJECT = STA. 781+72.97
LENGTH OF PROJECT = 10,172.97 LIN. FT. = 1.927 MILES

BEGIN WORK = STA. 590+00.00
END WORK = STA. 799+37.19
LENGTH OF WORK = 20,937.19 LIN. FT.

ADD WORK LENGTH ASHLAND AVE. = 2783.16 LIN. FT.
ADD WORK LENGTH HARDIN PIKE = 3245.41 LIN. FT.
ADD WORK LENGTH CANNING FACTORY RD. = 2168.14 LIN. FT.
ADD WORK LENGTH NEW STREET = 302.97 LIN. FT.

LENGTH OF WORK = 29,436.87 LIN. FT. = 5.575 MILES

| SUPPLEMENTAL PRINTS OF STANDARD CONSTRUCTION DRAWINGS | | | | | |
|---|---------------|-------------------|------------------|----------------------|--|
| BP-1 6-1-65 | F-1 11-10-83 | MC-1 6-13-69 | TC-35.10 8-29-84 | BP-2 1-11-85 | |
| BP-3 12-6-76 | F-2 5-1-76 | MC-4 7-26-76 | TC-41.10 8-29-84 | HL-50.11 5-1-87 | |
| BP-4 10-1-87 | F-3 5-1-76 | MC-6 1-30-84 | TC-41.20 3-26-79 | MC-10 5-1-76 | |
| BP-5 10-1-87 | F-5 5-1-76 | MC-7 10-15-76 | TC-41.50 3-26-79 | MH-5 6-12-75 | |
| BP-6 10-1-87 | F-6 5-1-76 | MC-9A 1-11-85 | TC-42.10 8-19-77 | BP-10 1-30-84 | |
| BP-7 10-1-87 | GR-1 1-11-85 | MC-11 8-1-78 | TC-42.20 3-26-79 | | |
| BP-9 12-6-76 | GR-2B 2-5-82 | MH-1 12-18-84 | TC-51.10 1-20-84 | | |
| CB-2-2-B 5-1-79 | GR-3 1-21-85 | MH-3 12-18-84 | TC-51.11 1-20-84 | | |
| CB-2-3 5-1-79 | GR-4 2-5-82 | MT-95.30 4-1-87 | TC-52.10 4-3-79 | EX-T-4-87 1-5-89 | |
| CB-3 5-1-79 | GR-4A 1-30-84 | LA-1 6-1-79 | TC-52.20 4-3-79 | AS-1-81 11-27-81 | |
| CB-4 11-10-83 | GR-5 2-5-82 | | | BR-1 5-29-79 | |
| CB-5 11-10-83 | GR-6 2-5-82 | MT-97.10 4-29-88 | TC-61.10 4-5-82 | FB-1-82 5-10-82 | |
| CB-8 11-10-83 | HW-4A 4-1-80 | MT-99.10 11-14-86 | | RB-1-55 2-2-59(REV.) | |
| CB-458A 5-1-79 | HW-4B 4-1-80 | MT-99.20 4-29-88 | TC-72.20 2-26-82 | SD-1-69 6-12-69 | |

Project: AUG-33-13.44
Date of Letting _____ 19 _____ Contract No. _____

Plan Prepared By:
DISTRICT NO. 7
OHIO DEPARTMENT OF
TRANSPORTATION

SEAL

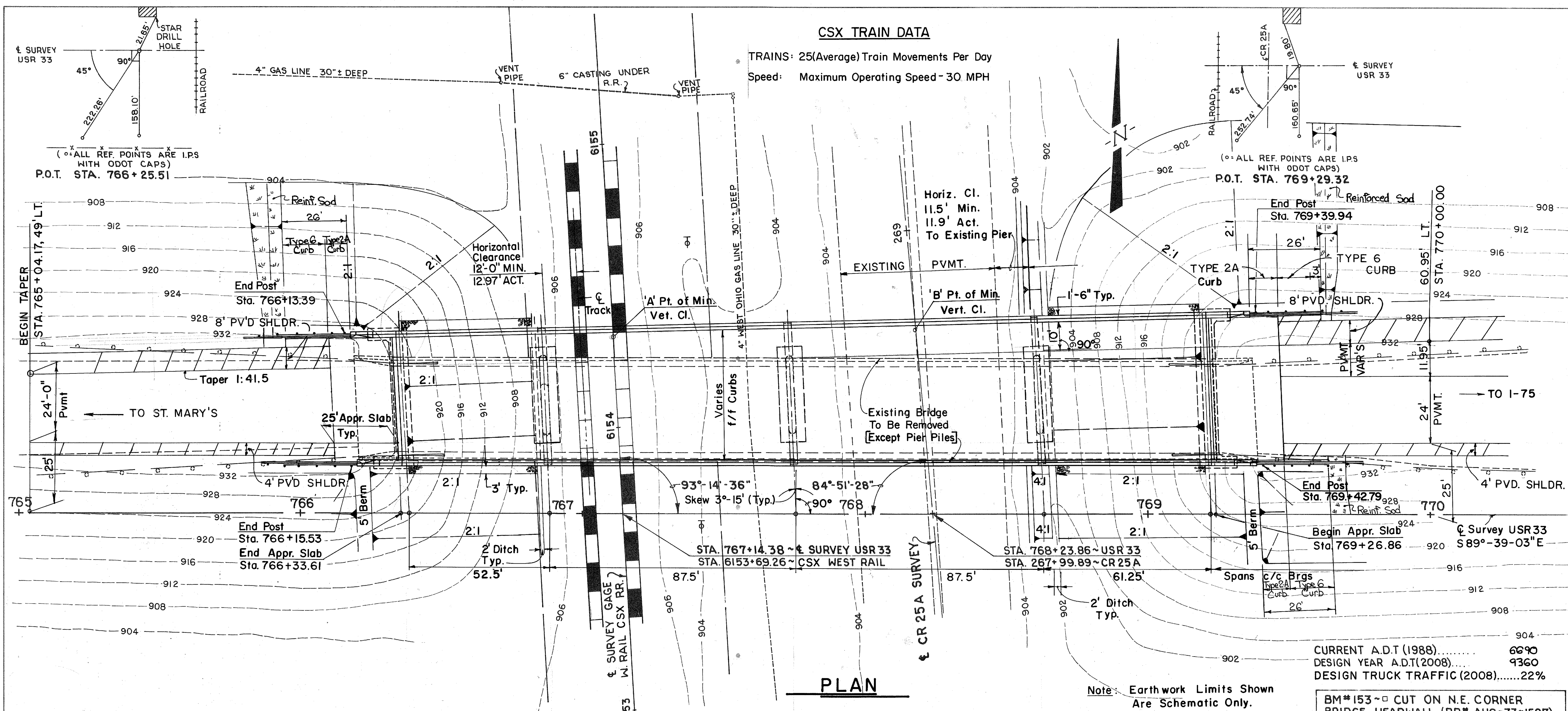
CSX TRAIN DATA

TRAINS: 25(Average) Train Movements Per Day
 Speed: Maximum Operating Speed - 30 MPH

| | | | |
|-------------|-------|---------|------------|
| FHWA REGION | STATE | PROJECT | 301 345 |
| 5 | OHIO | | |

AUGLAIZE COUNTY
 AUG-33-13.44

| CURVE DATA | | |
|----------------|------------|--------------------------------|
| ROAD | HORIZONTAL | VERTICAL |
| U.S. 33 | TANGENT | 0.69% ∇ -3.0% L=875' |
| COUNTY RD. 25A | TANGENT | EXISTING |

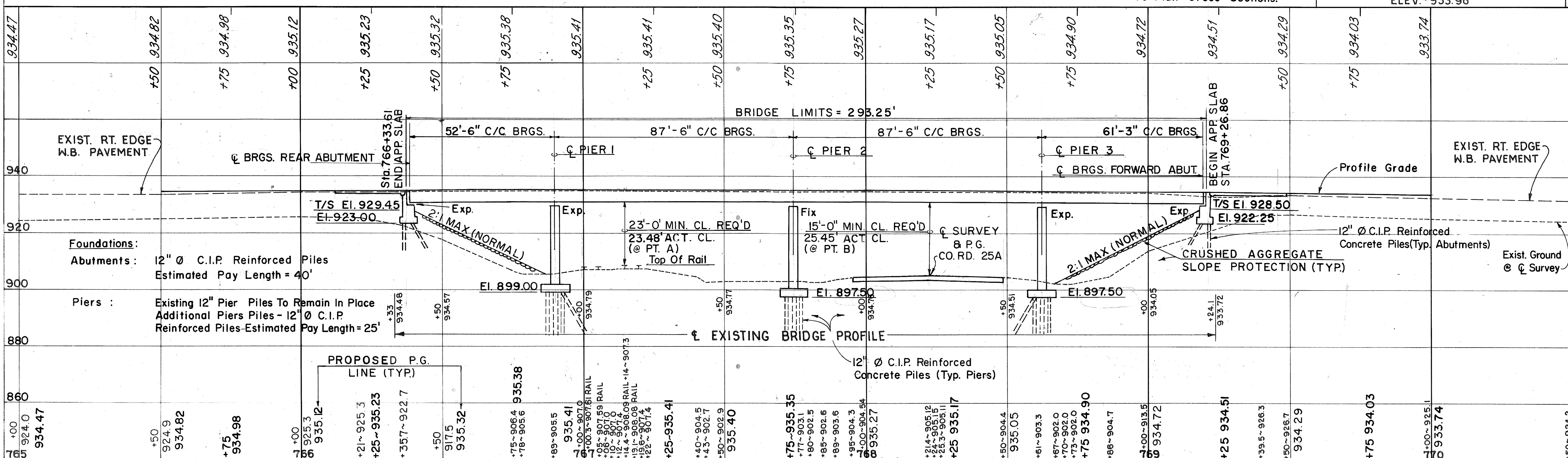


PLAN

Note: Earthwork Limits Shown Are Schematic Only. Actual Slopes Shall Conform To Plan Cross-Sections.

BM#153 - CUT ON N.E. CORNER BRIDGE HEADWALL (BR# AUG-33-1507)
 STA. 769+36.40 ~ 55' LT
 ELEV. - 933.96

CURRENT A.D.T (1988)..... 6690
 DESIGN YEAR A.D.T(2008)..... 9360
 DESIGN TRUCK TRAFFIC (2008)..... 22%



PROPOSED STRUCTURE
 TYPE: 4 SPAN CONTINUOUS COMP. STEEL BEAM WITH REINF. CONC. DECK AND SUBSTR. UNITS.
 SPAN: 52.5'-87.5' - 87.5'-61.25' C/C BRGS.
 ROADWAY WIDTH: VARIES (AVG. 46.67' F/CURB)
 LOAD CAPACITY: HS20-44, CASE II, 8 ALT. MILITARY LOADING.
 SKEW: 3°-15' RT. FWD.
 WEARING SURFACE: 1" MONOLITHIC CONCRETE
 APPROACH SLABS: AS-1-81 (25' LONG)
 ALIGNMENT: TANGENT
 SUPERELEVATION: N/A

EXISTING STRUCTURE
 TYPE: 4 SPAN CONT. STEEL BEAM WITH REINF. DECK AND SUBSTRUCTURE.
 SPAN: 52.5', 87.5', 87.5', 61.25' C/C BRIDGES.
 ROADWAY: 30'-0" F/F 2'-0" SAFETY CURBS.
 SKEW: 3°-15'-00"
 ALIGNMENT: TANGENT
 WEARING COURSE: 1" MONOLITHIC CONCRETE.
 LOADING: CF = 2000 (SI), *
 APPROACH SLABS: (SPECIAL) 25' LONG

*ADEQUATE FOR AASHTO ALT. LOADING
 PARSONS BRINCKERHOFF OHIO, INC.
 COLUMBUS, OHIO 1/13

SITE PLAN
 BRIDGE NO: AUG-1507L (W.B.)
 Over C.R. 25A & C.S.X. Railroad
 STA. 766+33.61 TO STA. 769+26.86
 AUGLAIZE COUNTY U.S.R. 33

| | | | |
|---------------|---------------|---------|----------|
| EXISTING TOPO | PROPOSED TOPO | WORK | REVIEWED |
| SURVEYED | DRAWN | CHECKED | |
| ODOT | ODOT | RS/MS | RS |
| Dist. 7 | Dist. 7 | JKS | RS |

SUBMITTED TO DIST. 7 7/19/09

GENERAL NOTES

AUGLAIZE COUNTY
AUG-33-13.44

| | | | | |
|-------------------|-------|---------|--|--|
| FED. RD. DIVISION | STATE | PROJECT | | |
| | OHIO | | | |

302
345

REFERENCES:

- STANDARD DRAWINGS:**
- SUPERSTRUCTURE DETAILS SD-1-69 (SHT. 1,2&3) DATED 6-12-89
 - ROCKERS & BOLSTERS RB-1-55 DATED 2-2-59(REV)
 - APPROACH SLAB AS-1-81 DATED 11-27-81
 - EXPANSION JOINTS EXJ-4-87 (SHT. 1,2&3) DATED 1-5-89
- SUPPLEMENTAL SPECIFICATIONS:**
- NO. 802 DATED 5-4-88
 - NO. 836 DATED 11-12-85

DESIGN SPECIFICATIONS:
THIS STRUCTURE CONFORMS TO 'STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES' ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 1983 SPECIFICATIONS, 1984 THRU 1988 INTERIM SPECIFICATIONS, AND THE OHIO 'SUPPLEMENT' TO THEM, DATED 1982.

- DESIGN DATA:**
- DESIGN LOADING HS20-44 CASE II AND ALTERNATE MILITARY LOADING
 - CONCRETE CLASS S, SUPERSTRUCTURE COMPRESSIVE STRENGTH F'c = 4500 PSI
 - CONCRETE CLASS C, SUBSTRUCTURE COMPRESSIVE STRENGTH F'c = 4000 PSI
 - STRUCTURAL STEEL ASTM A588, UNIT STRESS = 27000 PSI
ASTM A36, UNIT STRESS = 20000 PSI
 - REINFORCING STEEL ASTM A615, A616, OR A617, GRADE 60
MINIMUM YIELD STRENGTH = 60000 PSI
 - SPIRAL REINFORCEMENT MAY BE PLAIN BARS ASTM A802 OR A615
 - DECK PROTECTION METHOD EPOXY COATED REINFORCING STEEL, TOP & BOTTOM MATS
 - MONOLITHIC WEARING SURFACE IS ASSUMED, FOR DESIGN PURPOSES TO BE 1 INCH THICK

PILES

PILE DESIGN LOADS: THE DESIGN LOAD FOR THE 12" DIA. CAST-IN-PLACE REINFORCED CONCRETE PILES AS THE ABUTMENTS IS 44 TONS PER PILE. THE DESIGN LOAD FOR THE 12" DIA. CAST-IN-PLACE REINFORCED CONCRETE PILES AT THE PIERS IS 34 TONS PER PILE. THE ESTIMATED PAY LENGTH FOR THE ABUTMENT PILES IS 40' AND THE ESTIMATED PAY LENGTH FOR THE PIER PILES IS 25'.

12 INCH PRECAST PRESTRESSED CONCRETE PILES MAY BE SUBSTITUTED FOR THE 12" DIA. CAST-IN-PLACE REINFORCED CONCRETE PILES SHOWN ON THESE PLANS. DRAWINGS SHOWING DETAILS OF AND SPECIFICATIONS FOR PRESTRESSED CONCRETE PILES ARE AVAILABLE FROM THE DIRECTOR (BUREAU OF BRIDGES). IF THE PRESTRESSED PILE ALTERNATIVE IS CHOSEN, THE METHOD OF MEASUREMENT AND BASIS OF PAYMENT SHALL BE THE SAME FOR CAST-IN-PLACE REINFORCED CONCRETE PILES PER ITEM 507.

PILE HAMMER: THE PILE HAMMER USED TO INSTALL THE CAST-IN-PLACE REINFORCED CONCRETE PILES SHALL HAVE A STATE'S ENERGY RATING OF NOT LESS THAN 16,500 FOOT-POUNDS. THIS REQUIREMENT DOES NOT RELIEVE THE CONTRACTOR FROM CMS, ITEM 108.05, WHICH STATES THAT THE CONTRACTOR IS TO PROVIDE SUFFICIENT EQUIPMENT FOR PROSECUTING THE REQUIRED WORK. REFER TO ODOT'S MANUAL OF PROCEDURES FOR STRUCTURES TO OBTAIN THE STATE'S ENERGY RATING.

EXISTING PILES: THE EXISTING PILES AT PIERS 1, 2 & 3 SHALL BE INCORPORATED INTO THE PROPOSED PIERS. THE EXISTING PILES ARE 12" DIA. REINFORCED CONCRETE WITH A DESIGN LOADING OF 35 TONS PER PILE. THE CONTRACTOR SHALL EXERCISE CARE WHILE DEMOLISHING THE EXISTING PIER FOOTINGS SO AS NOT TO DAMAGE THE EXISTING PILES. DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING PILES HAVE BEEN OBTAINED FROM THE PLANS OF THE EXISTING STRUCTURE. CONSEQUENTLY, THEY INDICATE THE LOCATION AND PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05 AND 105.02 THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF THE EXISTING PILES IN THE FIELD AFTER REMOVAL OF THE EXISTING PIER FOOTINGS. IF ANY OF THE EXISTING PILES ARE DAMAGED DURING THE REMOVAL OF THE EXISTING FOOTINGS THEY MAY BE REQUIRED TO BE REPLACED BY EQUIVALENT NEW PILES AT THE DISCRETION OF AND AS APPROVED BY THE ENGINEER. FOR MORE NOTES REGARDING THE EXISTING PILES SEE SHEET 7/13.

"ITEM 507 12 INCH CAST-IN-PLACE REINFORCED CONCRETE PILES, AS PER PLAN", THE RESPONSIBILITY OF CHOOSING AND PROVIDING A SATISFACTORY PILE WALL THICKNESS FOR THIS PROJECT SHALL BE BORNE BY THE CONTRACTOR EXCEPT THAT THE PILE WALL THICKNESS SHALL NOT BE LESS THAN 0.250 INCHES IS NECESSARY TO RESIST THE PILE INSTALLATION DRIVING STRESS, THE CONTRACTOR SHALL MAKE THIS DETERMINATION AND SHALL FURNISH A PILE WITH AN ACCEPTABLE WALL THICKNESS. MONOTUBE PILES ARE NOT REQUIRED TO COMPLY WITH THE ABOVE SPECIFIED MINIMUM PILE WALL THICKNESS. IF MONOTUBE PILES ARE USED, THE PILE WALL THICKNESS SHALL NOT BE LESS THAN 0.200 INCHES AND THE PILES SHALL SATISFY ALL THE REQUIREMENTS OF ITEM 507.

REMOVAL OF EXISTING STRUCTURE: WHEN NO LONGER NEEDED TO MAINTAINING TRAFFIC THE EXISTING STRUCTURE SHALL BE REMOVED EXCEPT FOR THE PILES AT PIERS 1, 2 & 3 PAYMENT FOR THE ABOVE MENTIONED WORK INCLUDING THE SAVING OF THE PIER PILES SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 202 - STRUCTURE REMOVED, AS PER PLAN. THE CONTRACTOR SHALL PREPARE AND SUBMIT DETAILED PLANS TO THE STATE FOR REVIEW AND APPROVAL PRIOR TO THE START OF REMOVAL.

SPECIAL RAILROAD REQUIREMENTS

GENERAL: THE CONTRACTOR SHALL COOPERATE AT ALL TIMES WITH THE OFFICIALS OF THE RAILROAD COMPANY. HE SHALL USE ALL REASONABLE CARE AND DILIGENCE IN THE WORK IN ORDER TO AVOID ACCIDENTS, DAMAGE OR INTERFERENCE WITH THE TRAINS OR THE PROPERTY OF THE RAILROAD. THE CONTRACTOR SHALL NOTIFY THE RAILROAD PRIOR TO STARTING WORK THAT MAY AFFECT RAILROAD PROPERTY AND FACILITIES AND SHALL PAY THE RAILROAD COMPANY THE COST OF FLAGMAN FURNISHED BY THE RAILROAD COMPANY AND MADE NECESSARY BECAUSE OF ANY OF THE CONTRACTOR'S OPERATIONS OVER OR ADJACENT TO THE TRACKS.

NO SCAFFOLD, PLANKS OR OTHER EQUIPMENT SHALL BE SUSPENDED OR ERECTED ABOVE OR WITHIN 10 FEET HORIZONTALLY OF A RAIL OVER WHICH TRAINS ARE OPERATING WITHOUT PRIOR WRITTEN APPROVAL OF THE CHIEF ENGINEER, OR HIS AUTHORIZED REPRESENTATIVE, OF THE RAILROAD COMPANY.

CONSTRUCTION CLEARANCE: A CONSTRUCTION CLEARANCE OF 8 FEET, MEASURED HORIZONTALLY FROM THE CENTER OF TRACKS, AND 21 FEET VERTICALLY FROM A POINT LEVEL WITH THE TOP OF THE HIGHER RAIL AND 6 FEET FROM THE CENTER OF TRACKS, SHALL BE MAINTAINED AT ALL TIMES.

THE CONTRACTOR SHALL SUBMIT TO THE RAILROAD COMPANY FOR THEIR REVIEW DETAILED PLANS OF THE PROPOSED SHORING FOR THE EXCAVATION OF THE FOOTING AT PIER-1, AND OF PROTECTIVE SHIELD FOR CONSTRUCTION AND DEMOLITION ABOVE TRACKS, INCLUDING ERECTION PROCEDURE OF THE SPAN OVER THE TRACKS.

FAILURE TO COMPLY WITH THE RAILROAD COMPANY AS NOTED ABOVE SHALL BE CAUSE FOR THE STOPPING OF WORK UNTIL ALL PROVISIONS FOR PROTECTION OF THE RAILROAD PROPERTY HAVE BEEN UNDERTAKEN. BLASTING SHALL NOT BE USED OVER OR NEAR THE TRACKS.

RAILROAD AERIAL LINES: WILL BE RELOCATED BY THE RAILROAD. THE CONTRACTOR SHALL USE ALL PRECAUTIONS NECESSARY TO SEE THAT THE LINES ARE NOT DISTURBED DURING THE CONSTRUCTION STAGE AND SHALL COOPERATE WITH THE RAILROAD IN THE RELOCATION OF THESE LINES. THE COST OF THE RELOCATION SHALL BE INCLUDED IN THE RAILROAD FORCE ACCOUNT WORK.

MAINTENANCE OF TRAFFIC: FOR MAINTENANCE OF TRAFFIC AND SCHEDULE OF OPERATIONS, SEE PLAN SHEETS NO. 23/345

WATERPROOF MEMBRANE: CURING AS PER 511.14, METHOD B, SHALL BE APPLIED ON PARAPET, BACKWALLS AND ABUTMENT SEATS.

UTILITY LINES: ALL EXPENSE INVOLVED IN RELOCATING THE AFFECTED UTILITY LINES, SHALL BE BORNE BY THE OWNERS. THE CONTRACTOR AND OWNERS ARE REQUESTED TO COOPERATE BY ARRANGING THEIR WORK IN SUCH A MANNER THAN INCONVENIENCE TO EITHER WILL BE HELD TO A MINIMUM. UTILITY OWNERS ARE LISTED IN THE GENERAL NOTES ON SHEET NO. 20/345

ITEM 503 - UNCLASSIFIED EXCAVATION, AS PER PLAN:

THIS ITEM SHALL CONFORM TO CMS 503, EXCEPT THAT ONLY GRANULAR MATERIALS AS DEFINED IN CMS 203.02 SHALL BE USED FOR THE BACKFILL BEHIND THE ABUTMENTS.

ITEM 503 - COFFERDAMS, CRIBS AND SHEETING, AS PER PLAN:

ALL BACKFILL BEHIND THE ABUTMENTS NECESSITATED BY THE PERFORMANCE OF THIS ITEM SHALL CONSIST OF GRANULAR MATERIALS AS DEFINED IN CMS 203.02, EXCEPT THAT BACKFILL IN AREAS MORE THAN 30 FEET BEHIND ABUTMENTS MAY BE ANY MATERIAL MEETING THE REQUIREMENTS OF CMS 203.08.

ITEM 518 - POROUS BACKFILL, AS PER PLAN

TO ENSURE THAT THE FINE SOIL PARTICLES INCLUDED WITHIN THE EMBANKMENT BEHIND THE REAR ABUTMENT DO NOT MIGRATE INTO AND THROUGH THE VOIDS OF THE POROUS BACKFILL MATERIAL AT THE REAR ABUTMENT, FILTER FABRIC, 712.09 TYPE A, SHALL BE PLACED BETWEEN THE 518 POROUS BACKFILL MATERIAL AND THE 203 EMBANKMENT MATERIAL TO THE LIMITS AS SHOWN IN THE PLANS. PAYMENT FOR THE FILTER FABRIC SHALL BE INCLUDED IN THE UNIT PRICE BID PER CUBIC YARD FOR THE ITEM 518 POROUS BACKFILL, AS PER PLAN, WHICH SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK.

*** ITEM SPECIAL: SEALING OF CONCRETE SURFACES**

A CONCRETE SEALER, AN EPOXY TYPE, SHALL BE APPLIED TO THE FOLLOWING CONCRETE SURFACES: PARAPETS AND CURBS, SEE SHEET 8/13, BRIDGE SEAT BACKWALLS, AND FRONT FACES OF ABUTMENT BREASTWALLS ONE FOOT DOWN FROM THE LEVEL OF THE BRIDGE SEAT. TOTAL SURFACE OF PIERS ABOVE GROUND LINE TO BE COVERED WITH NON-EPOXY SEALER. SEE PROPOSAL NOTE FOR SURFACE PREPARATION REQUIREMENTS AND APPLICATION PROCEDURES. SUBSTRUCTURE UNITS SHALL BE SEALED PRIOR TO ERECTION OF STRUCTURAL STEEL.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS NEEDED TO PERFORM THIS ITEM SHALL BE INCLUDED IN THE SQUARE YARD UNIT PRICE BID FOR ITEM SPECIAL, SEALING OF CONCRETE SURFACES.

****** FOR PARTIAL PAINTING OF A588 STEEL NOTE, SEE SHEET 9/13 .

******* FOR SPECIAL NOTES REGARDING ITEM 516, STRUCTURAL EXPANSION JOINTS, INCLUDING ELASTOMERIC STRIP SEALS, SEE SHEET 11/13 .

Δ Item 511, Class 5 Concrete superstructure using shrink-age compensating cement. (See Proposal Note).

| CALCULATED BY: TDW 12-20-88 | | ESTIMATED QUANTITIES | | | | CHECKED BY: JKS 1-31-89 | | | |
|-----------------------------|----------|----------------------|----------|---|---------|-------------------------|-------|------|---|
| ITEM | ITEM EXT | TOTAL | UNIT | DESCRIPTION | SUPER. | ABUT. | PIERS | GEN. | |
| 202 | | LUMP | SUM | STRUCTURE REMOVED, AS PER PLAN | | | | LUMP | |
| 503 | | LUMP | SUM | COFFERDAMS, CRIBS AND SHEETING | | | | LUMP | |
| 503 | | LUMP | SUM | COFFERDAMS, CRIBS AND SHEETING, AS PER PLAN | | | | | |
| 503 * | | 510 | CU. YD. | UNCLASSIFIED EXCAVATION | | 260 | 250 | | |
| 503 | | LUMP | SUM | UNCLASSIFIED EXCAVATION AS PER PLAN | | LUMP | | | |
| 505 | | LUMP | SUM | PILE DRIVING EQUIPMENT MOBILIZATION | | | | LUMP | |
| 507 | | 2020 | LIN. FT. | 12" CAST-IN-PLACE REINFORCED CONCRETE PILES, AS PER PLAN | | 1120 | 900 | | |
| 509 | | 12,880 | LBS. | REINFORCING STEEL, GRADE 60 | | 8743 | 4137 | | |
| 509 | | 149,077 | LBS. | EPOXY COATED REINFORCING STEEL, GRADE 60 | 116,828 | 6384 | 25865 | | |
| 511 | | 457 | CU. YD. | CLASS S CONCRETE, SUPERSTRUCTURE | 457 | | | | |
| 511 | | 265 | CU. YD. | CLASS C CONCRETE, PIERS ABOVE FOOTINGS | | | 265 | | |
| 511 | | 101 | CU. YD. | CLASS C CONCRETE, ABUTMENTS ABOVE FOOTINGS | | 101 | | | |
| 511 | | 292 | CU. YD. | CLASS C CONCRETE, FOOTINGS | | 190 | 102 | | |
| 513 | | 352860 | LBS. | STRUCTURAL STEEL (AISC CATEGORY I) (ASTM A588) | 352860 | | | | |
| 513 | | 4102 | EACH | WELDED STUD SHEAR CONNECTORS | 4102 | | | | |
| 514 | | LUMP | SUM | PARTIAL PAINTING OF A588 STEEL, SYSTEM (A) | | | | LUMP | |
| 516 * | | 100 | LIN. FT. | STRUCTURAL EXPANSION JOINT INCLUDING ELASTOMERIC STRIP SEALS (B) | | 100 | | | |
| 516 * | | 80 | SQ. FT. | 1" PREFORMED EXPANSION JOINT FILLER | | 80 | | | |
| 518 | | 85 | LIN. FT. | 6" PERFORATED HELICAL CORRUGATED STEEL PIPE, 707.01 | | 85 | | | |
| 518 | | 90 | LIN. FT. | 6" NON-PERFORATED HELICAL CORRUGATED STEEL PIPE, INCLUDING SPECIALS, 707.01 | | 90 | | | |
| 518 | | 52 | CU. YD. | POROUS BACKFILL, AS PER PLAN | | 52 | | | |
| 518 | | 6 | EACH | SCUPPERS, INCLUDING SUPPORTS | 6 | | | | |
| 523 | | 3 | HOURS | DYNAMIC LOAD TEST | | | | | 3 |
| 601 | | 705 | SQ. YD. | CRUSHED AGGREGATE SLOPE PROTECTION | | 705 | | | |
| * SPECIAL | | 1071 | SQ. YD. | SEALING OF CONCRETE SURFACES | 663 | 137 | 271 | | |

PARSONS BRINCKERHOFF OHIO, INC.
COLUMBUS, OHIO

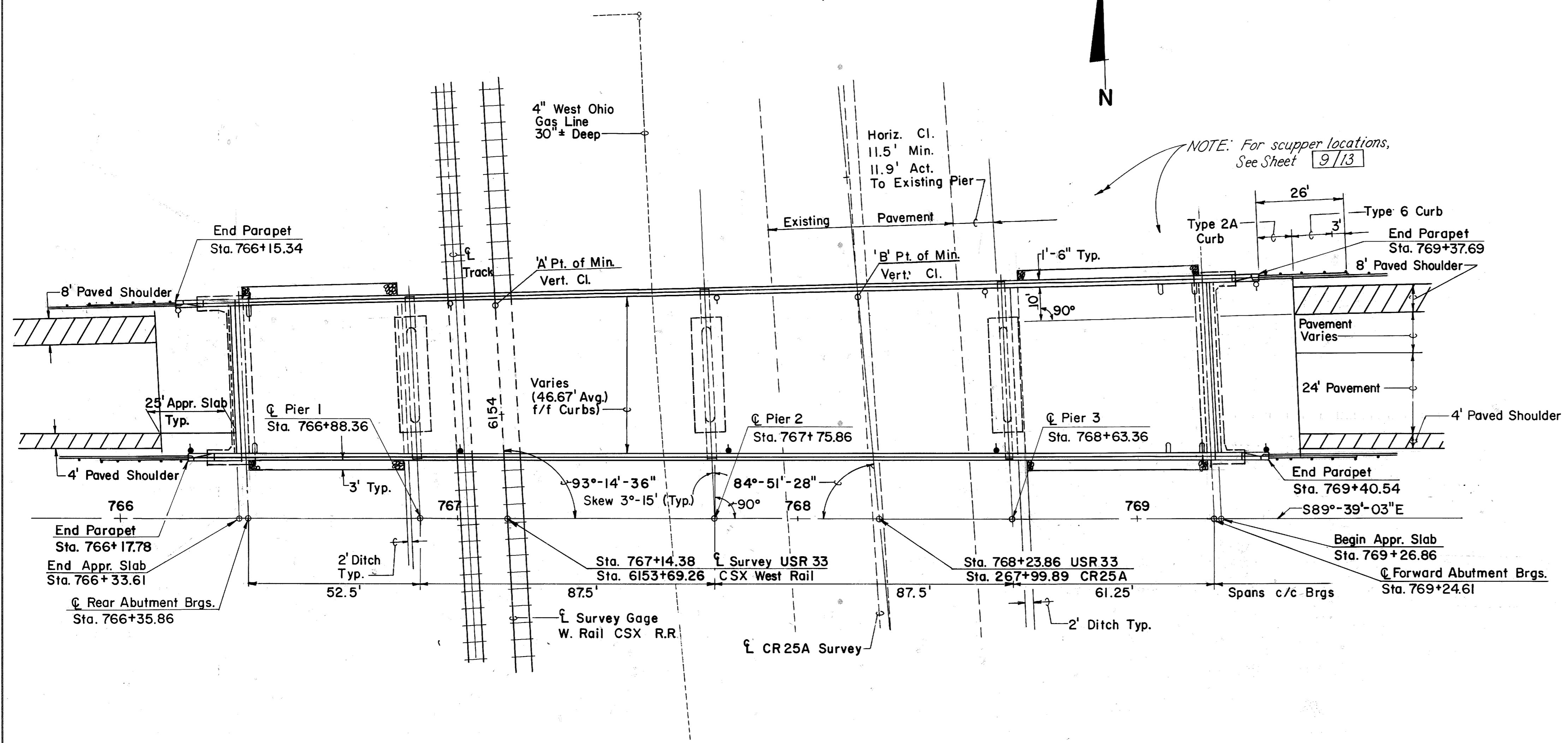
2/13

**GENERAL NOTES &
ESTIMATED QUANTITIES**

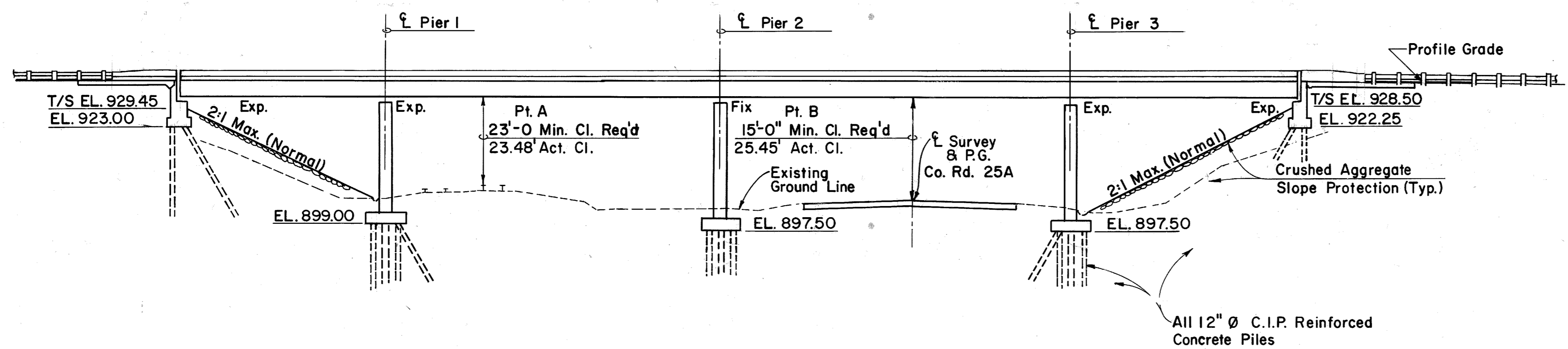
BRIDGE NO. AUG-33-1507L (W.B.)
OVER C.R.25A & C.S.X.R.R.

AUGLAIZE COUNTY U.S.R.33

| | | | | | | |
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| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
| TDW | TDW | CAD | BKU | JKS | | |



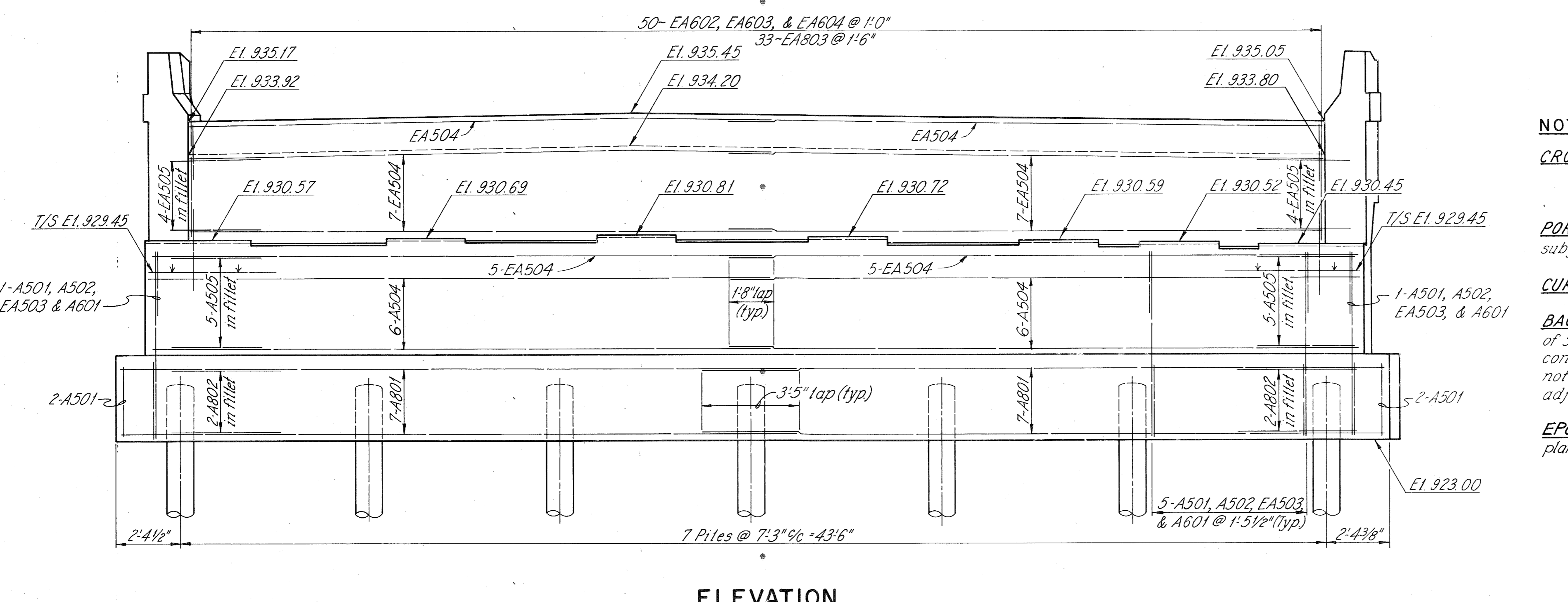
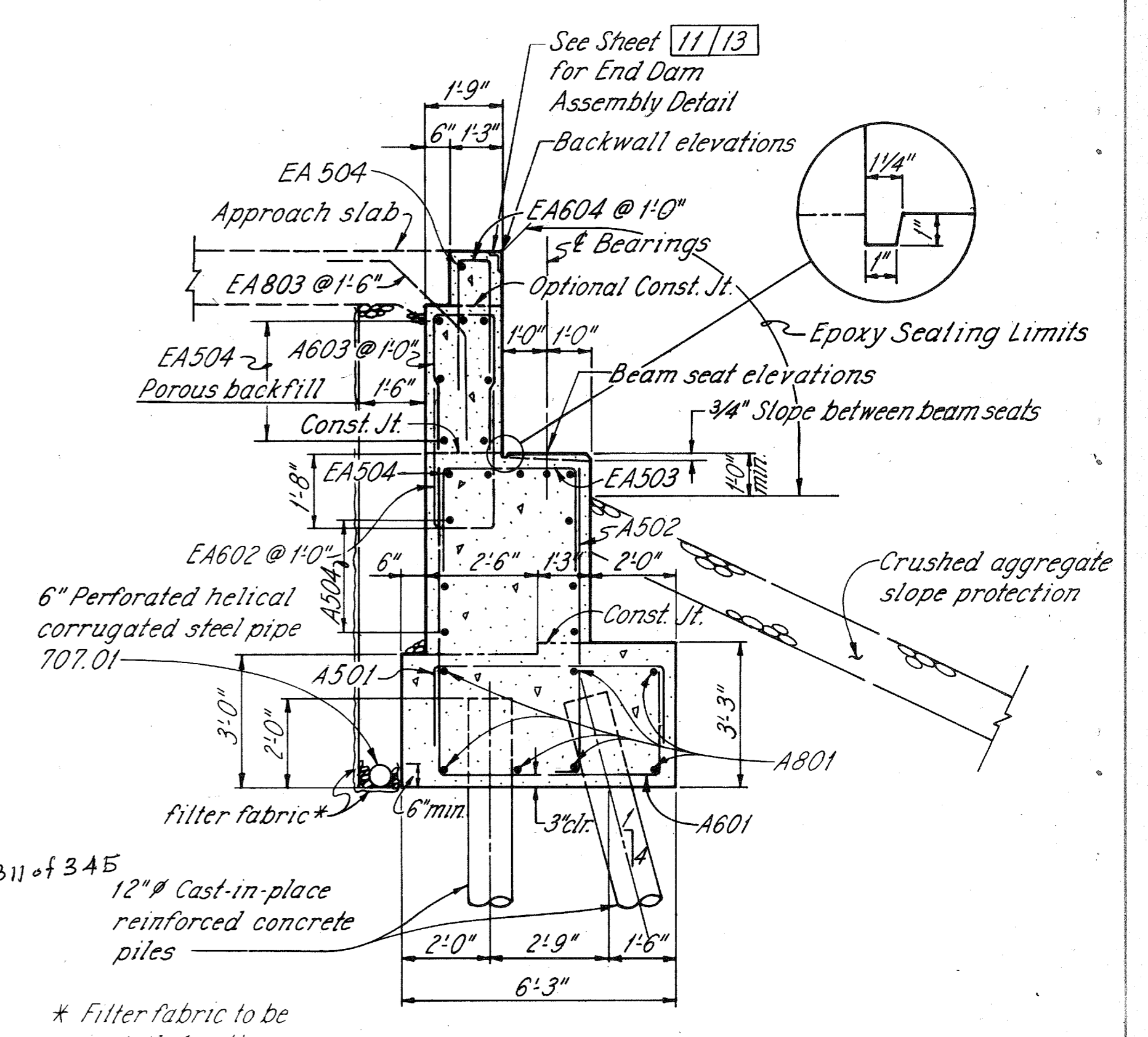
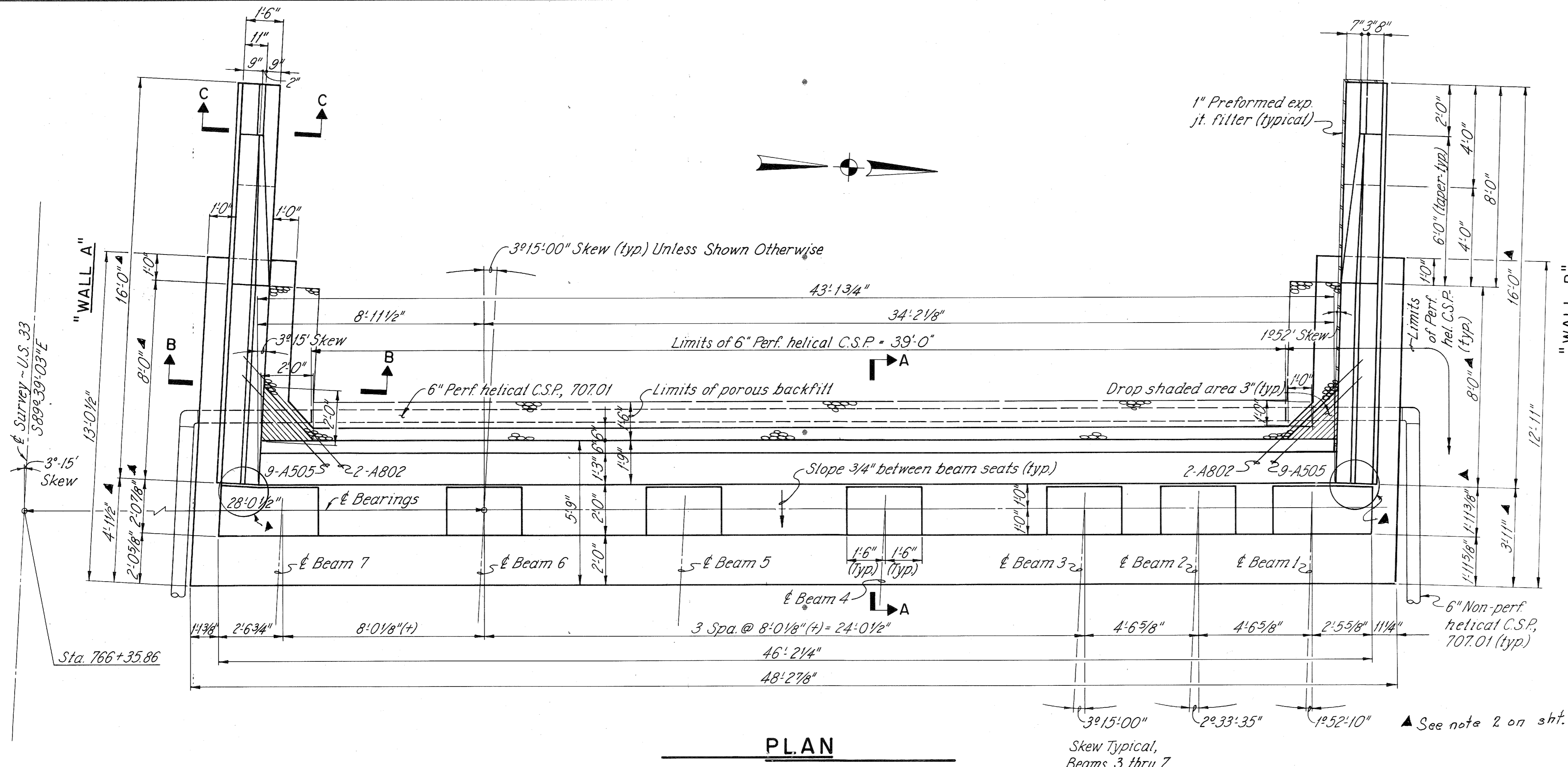
PLAN



ELEVATION-PROFILE GRADE LINE U.S. 33

| | | | | | |
|--|-------|--------|---------|-----------|--------|
| PARSONS & BRINCKERHOFF OHIO INC. COLUMBUS, OHIO | | | | | |
| | | | | | 3 / 13 |
| GENERAL PLAN | | | | | |
| BRIDGE NO. AUG-33-1507 L (W.B.) OVER C.R. 25A & C.S.X. R.R. | | | | | |
| AUGLAIZE COUNTY | | | | U.S.R. 33 | |
| Designed | Drawn | Traced | Checked | Reviewed | Date |
| JKS | | MDS | BKU | JKS | |

AUGLAIZE COUNTY
AUG-33-13.44



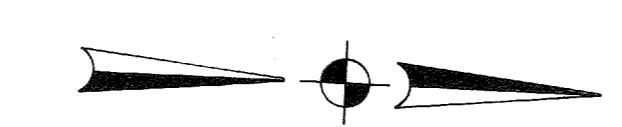
* Filter fabric to be included with porous backfill for payment.

▲ See note 2 on sh. 311 of 345

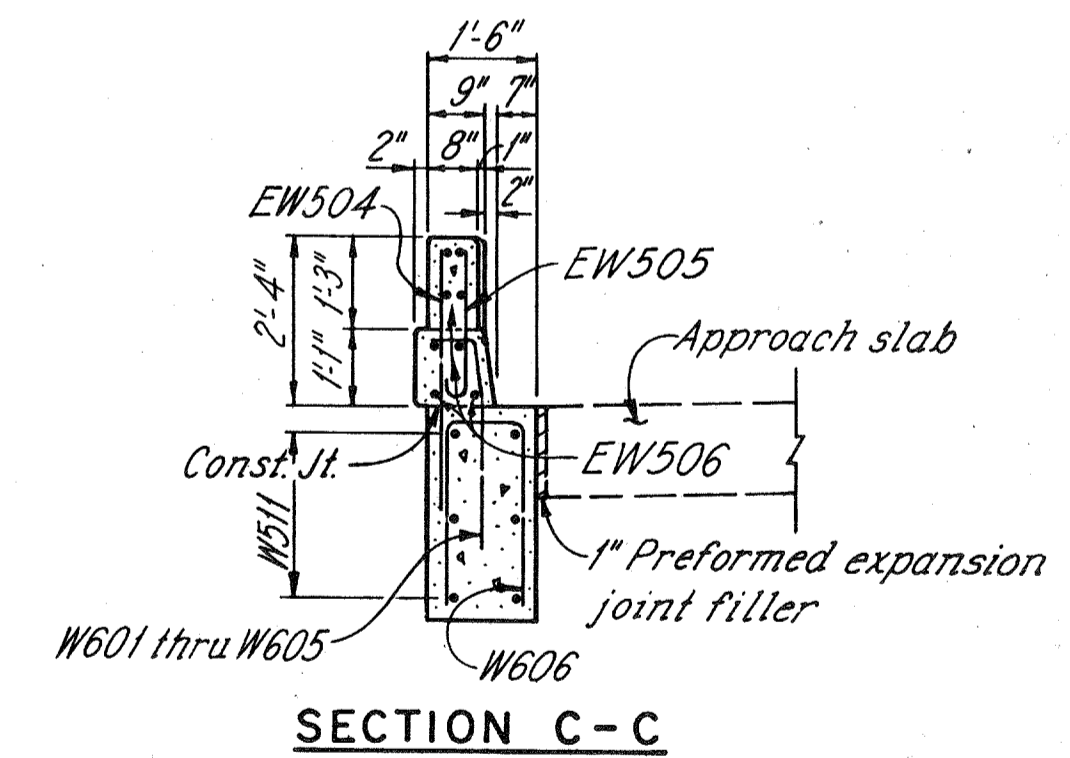
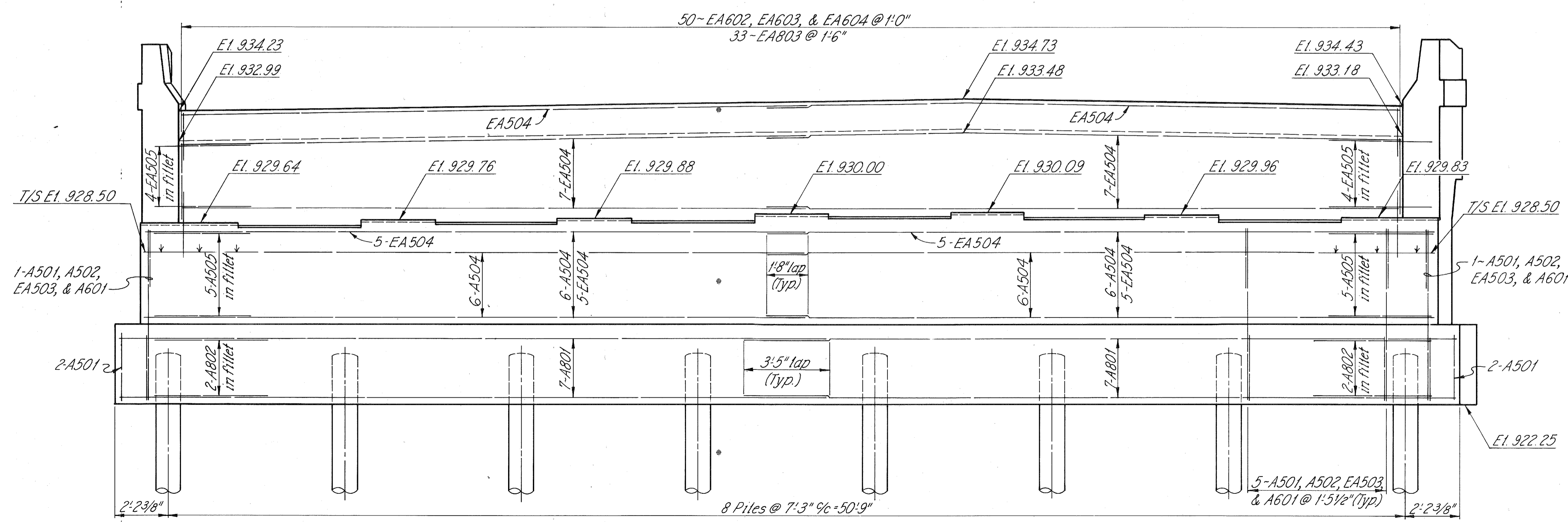
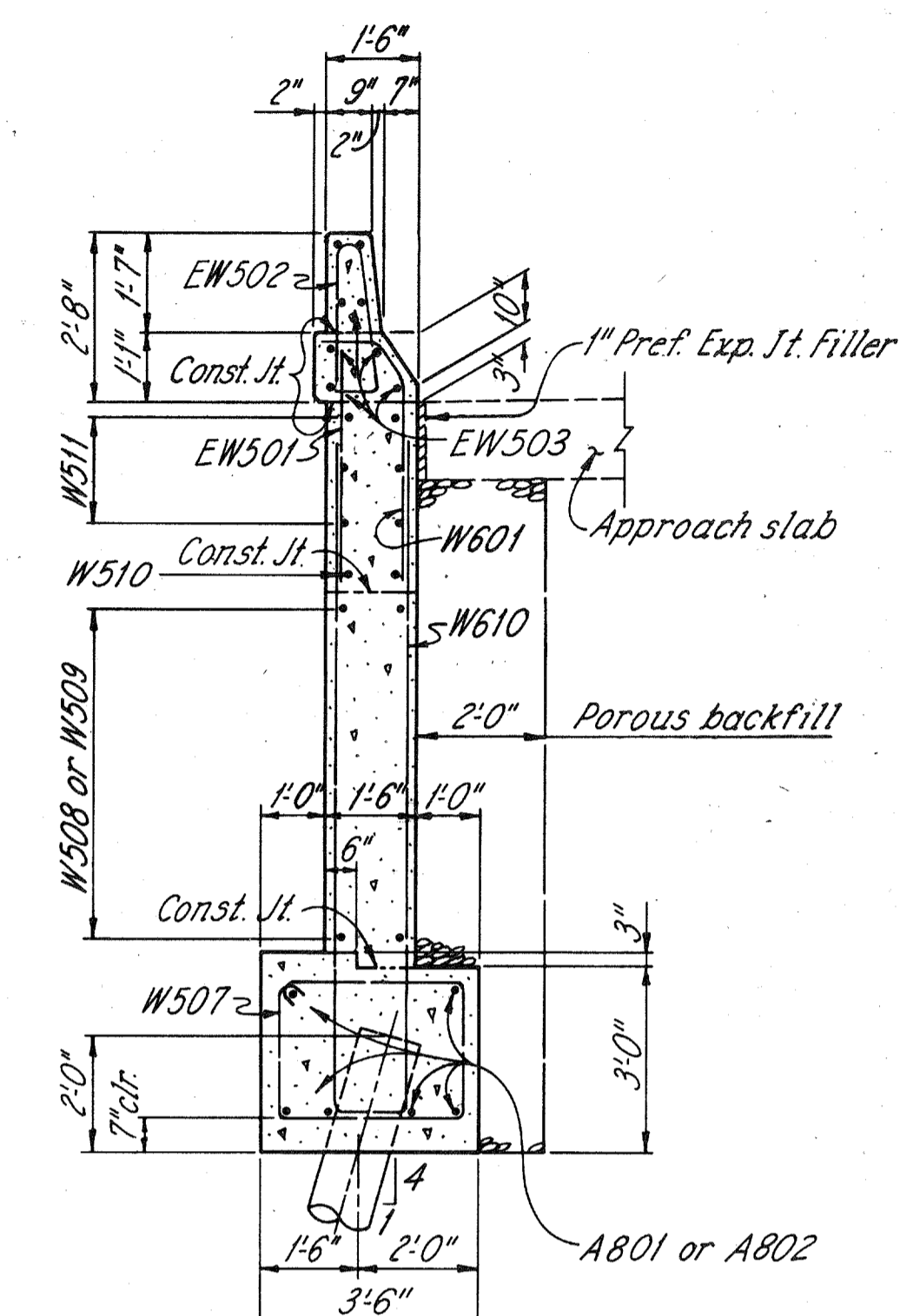
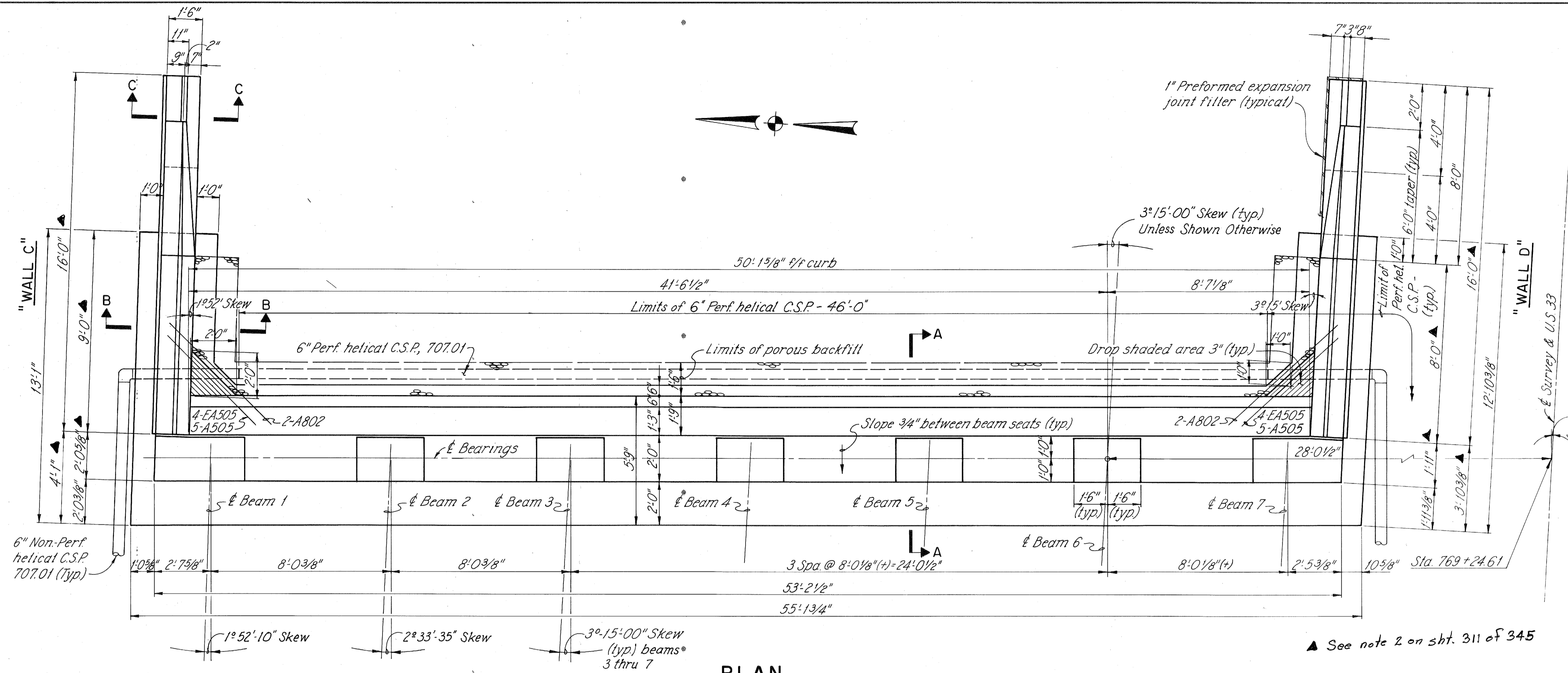
- NOTES:**
- CROSS REFERENCE SHEETS:** For Wingwall Sections, see Sheet 5/13. For Wingwall Details, see Sheet 6/13. For General Notes, see Sheet 2/13.
- POROUS BACKFILL:** 1'6" thick, shall extend from bottom of footing up to the plane of the subgrade, & laterally to the ends of the wingwalls.
- CURB & PARAPET DETAILS ON WINGWALLS:** See Standard Drawing BR-1.
- BACKWALL CONCRETE:** In addition to the provisions of 511.08, backwall concrete above the optional construction joint at the approach slab seat shall not be placed until the deck concrete in the span adjacent to the abutment has been placed.
- EPOXY COATED BARS** are denoted on the plan sheets with the prefix of the letter "E".

| | | |
|--|----------|-----------|
| PARSONS BRINCKERHOFF OHIO, INC. COLUMBUS, OHIO | | 4/13 |
| REAR ABUTMENT BRIDGE NO. AUG-33-1507L (W.B.) OVER C.R. 25A & C.S.X.R.R. | | |
| AUGLAIZE COUNTY | | U.S.R. 33 |
| Designed | Drawn | Traced |
| Checked | Reviewed | Date |
| Revised | | |
| TDW | BKU | BKU |
| BKU | BKU | JKS |

Survey - U.S. 33
3892 39-033"E
Sta. 766+35.86



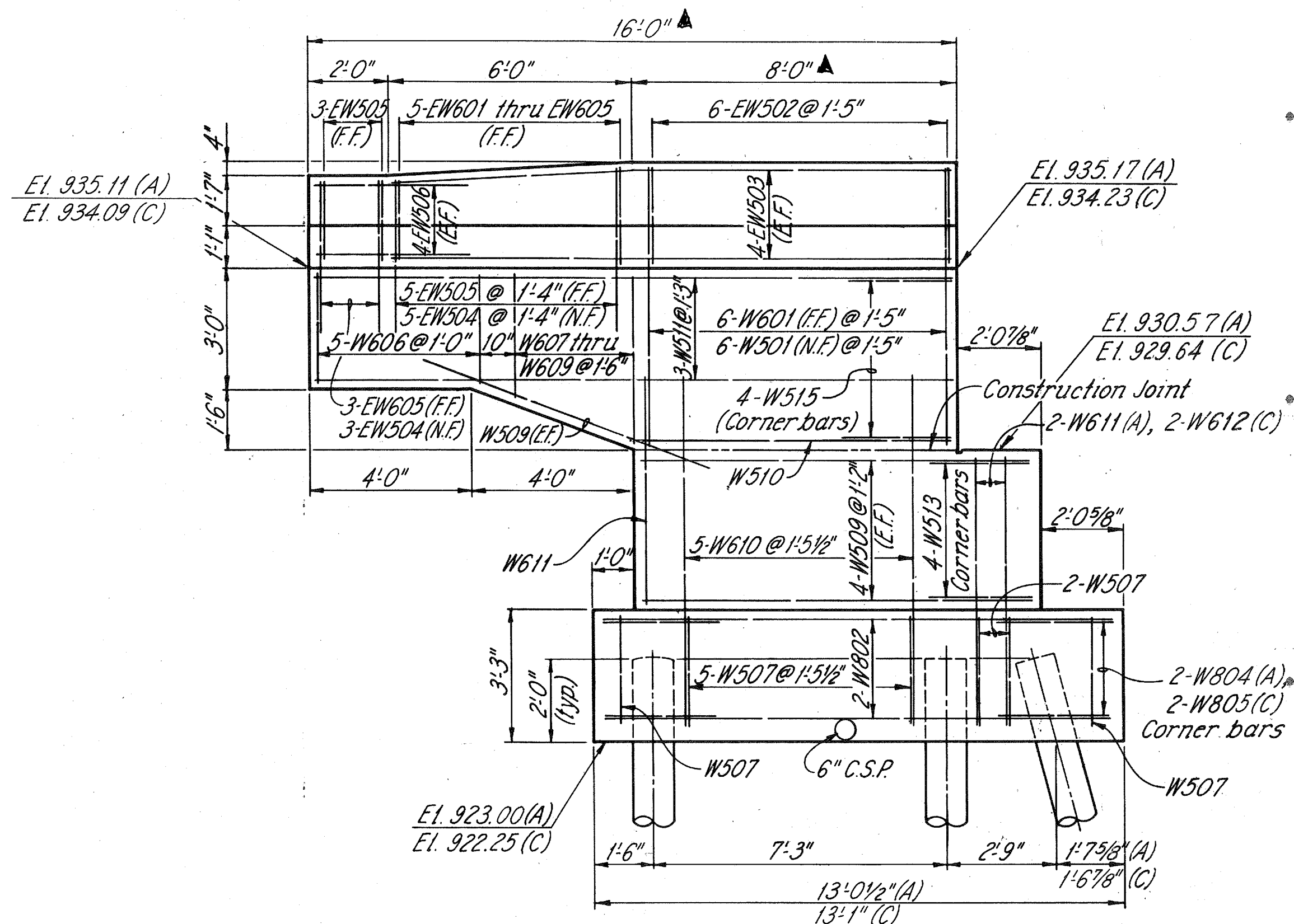
AUGLAIZE COUNTY
AUG-33-13.44



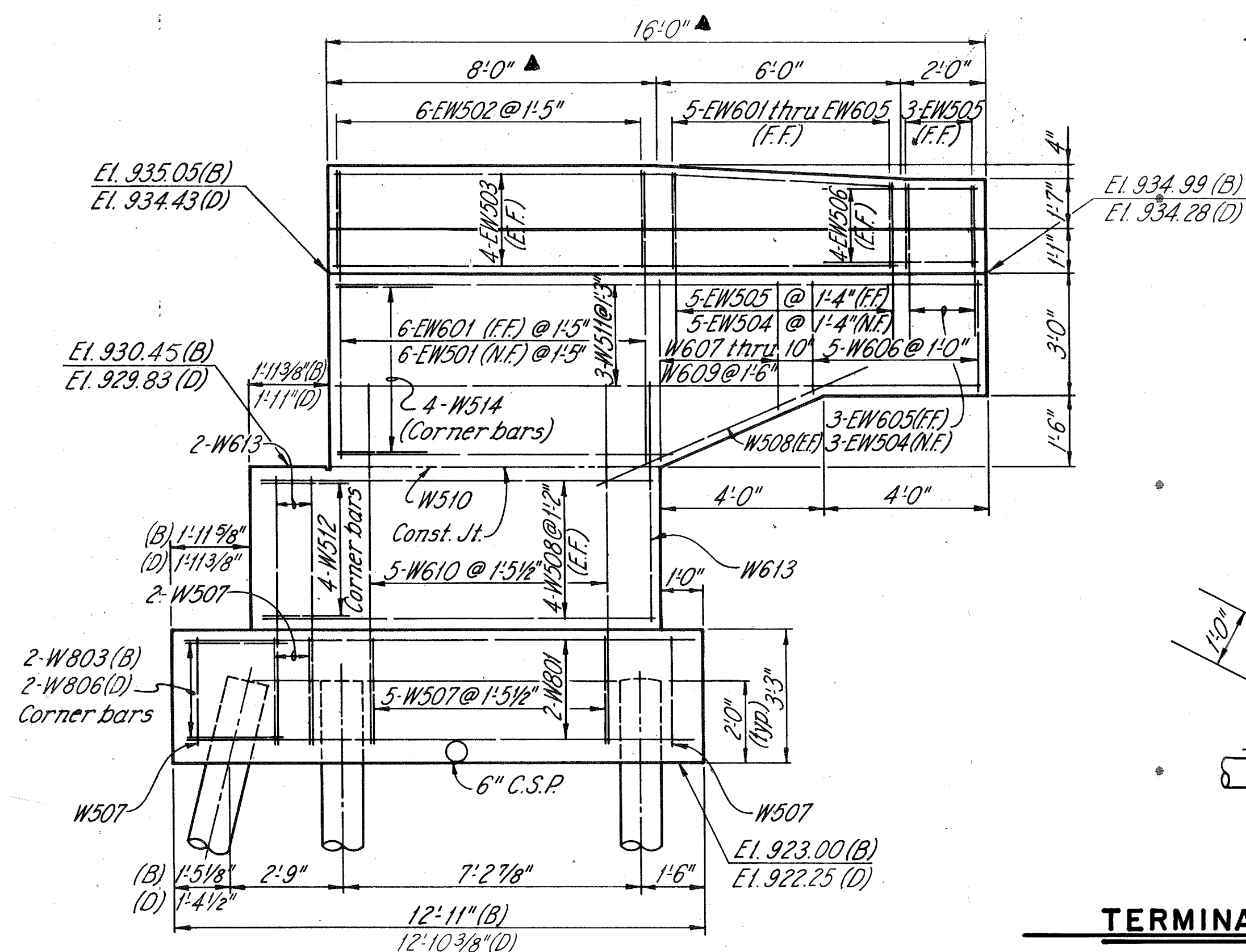
NOTE: For Section A-A (Abutment detail) & other notes, see Sheet 4/13.

| | | | | | | |
|--|-------|--------|---------|----------|------|-----------|
| PARSONS BRINCKERHOFF QUADE & DOUGLAS COLUMBUS, OHIO | | | | | | 5/13 |
| FORWARD ABUTMENT | | | | | | |
| BRIDGE NO. AUG-33-1507L (W.B.) OVER C.R. 25A & C.S.X.R.R. | | | | | | |
| AUGLAIZE COUNTY | | | | | | U.S.R. 33 |
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
| TDW | BKU | BKU | BKU | JKS | | |

**AUGLAIZE COUNTY
AUG-33-13.44**

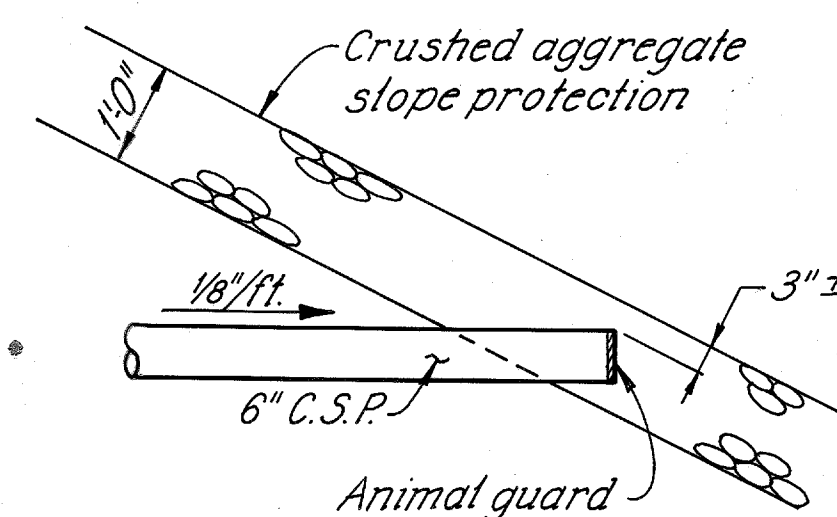


WINGWALL A (& C)

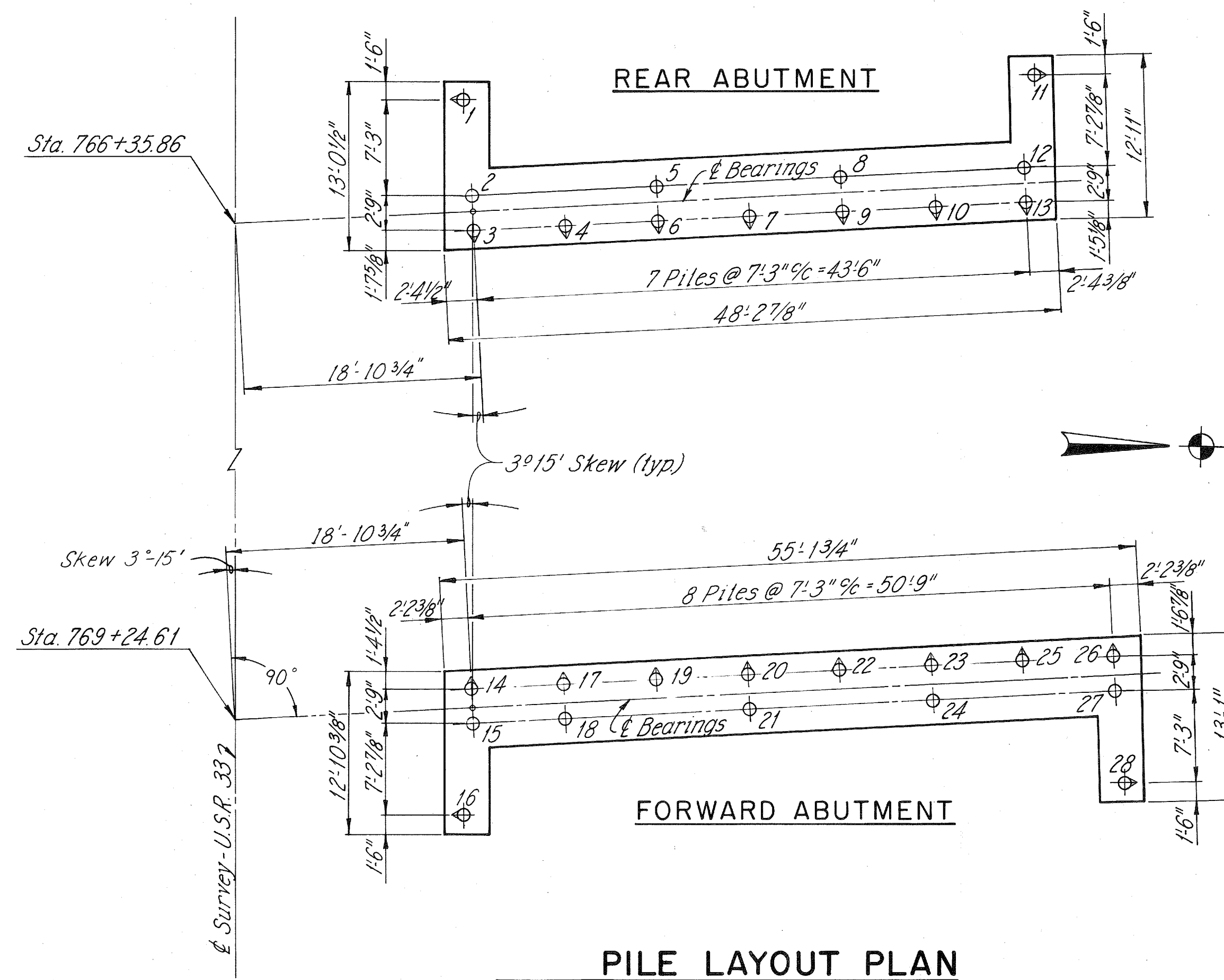


WINGWALL B (& D)

▲ See note 2 on sht. 311 of 345



**TERMINATION OF 6" NON-PERF. C.S.P.
(TYPICAL)**



PILE LAYOUT PLAN

LEGEND:

⊕ ~ Indicates 1:4 Battered Piles (in direction of the arrow')

E.F. - Each Face
F.F. - Far Face
N.F. - Near Face

NOTES:

CROSS REFERENCE SHEETS: For notes, see Sheet 4/13.
For Wingwall Sections, see Sheet 5/13.
For Wingwall locations & Abutment Details, see Sheets 4/13 & 5/13.

PILES: All piles are to be 12" ∅ Cast-in-place reinforced concrete.

| | | | | | | |
|---|-------|--------|---------|----------|------|---------|
| PARSONS BRINCKERHOFF OHIO, INC. COLUMBUS, OHIO | | | | | | |
| 6/13 | | | | | | |
| ABUTMENT DETAILS | | | | | | |
| BRIDGE NO. AUG-33-1507L (W.B.) OVER C.R. 25A & C.S.X. R.R. | | | | | | |
| AUGLAIZE COUNTY | | | | | | |
| U.S.R. 33 | | | | | | |
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
| TDW | BKU | BKU | BKU | JKS | | |

**AUGLAIZE COUNTY
AUG-33-13.44**

TABLE OF DIMENSIONS

| LOCATION | SKEW | (1) | (2) | (3) | (4) |
|-----------|----------|----------|---------|----------|----------|
| BEAMS 3-7 | 3°15'00" | 10 7/8" | 5 1/16" | 10 1/8" | 7 1/8" |
| BEAM 2 | 2°33'35" | 10 3/4" | 6" | 10 3/16" | 6 15/16" |
| BEAM 1 | 1°52'10" | 10 1/16" | 6 1/8" | 10 1/4" | 6 13/16" |

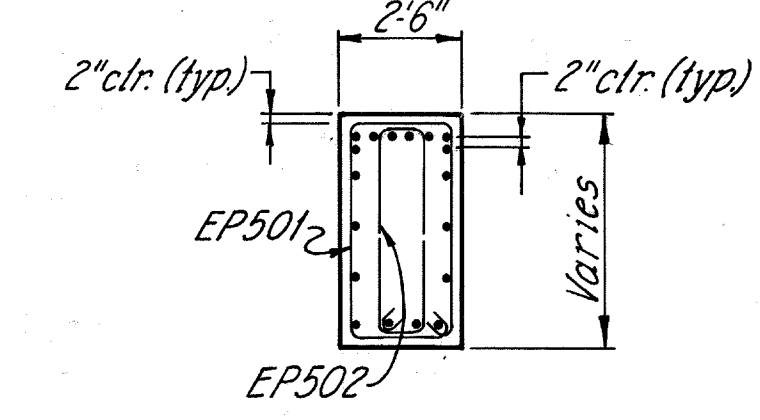
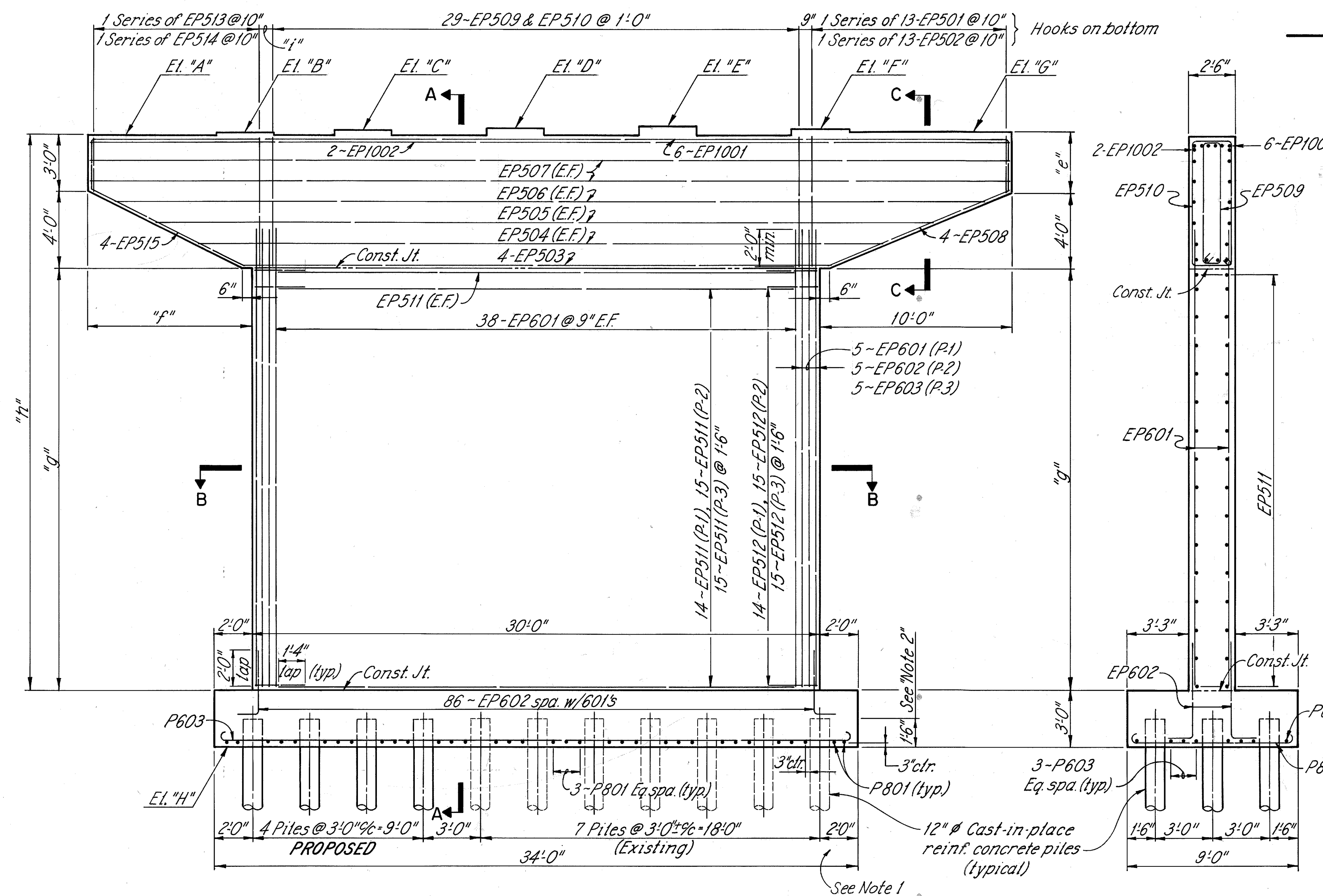
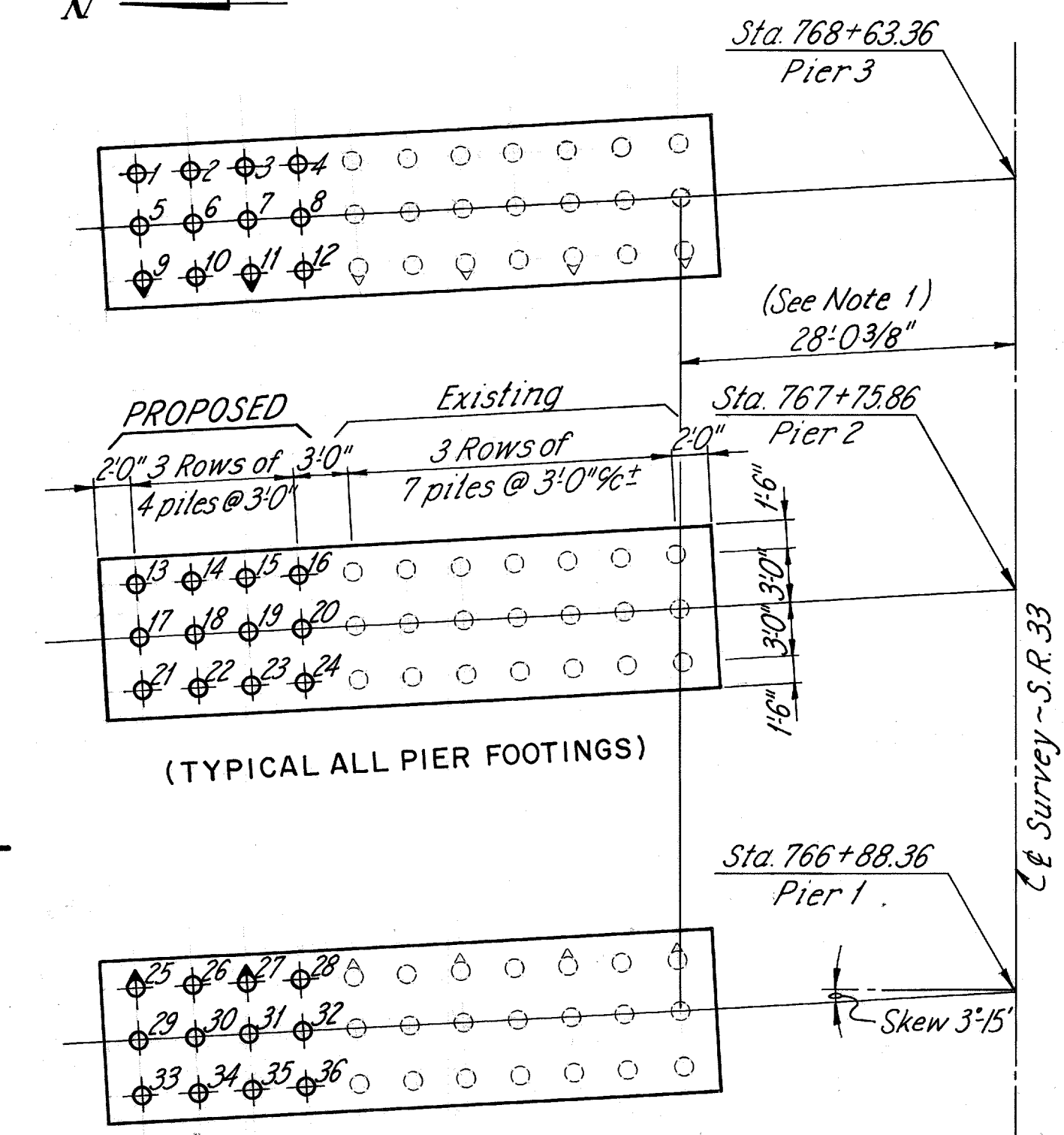
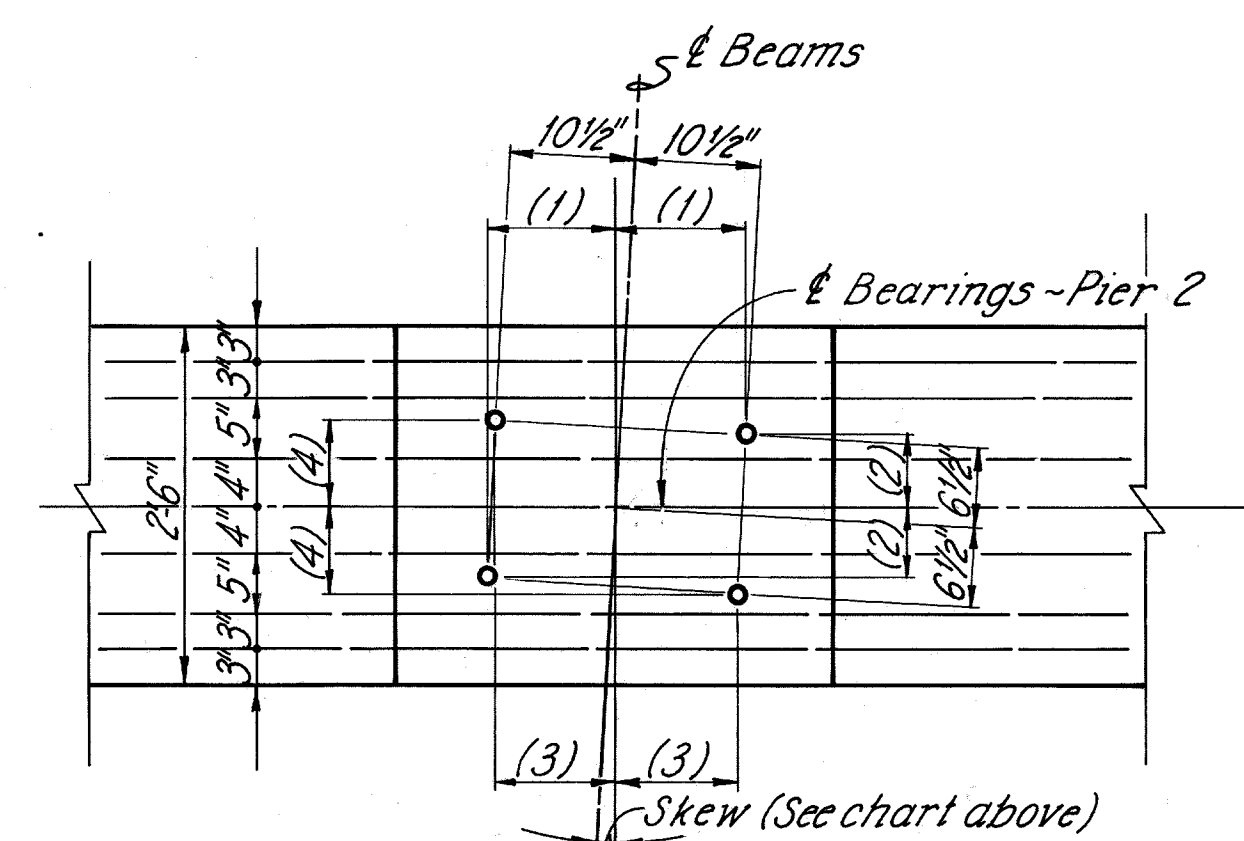
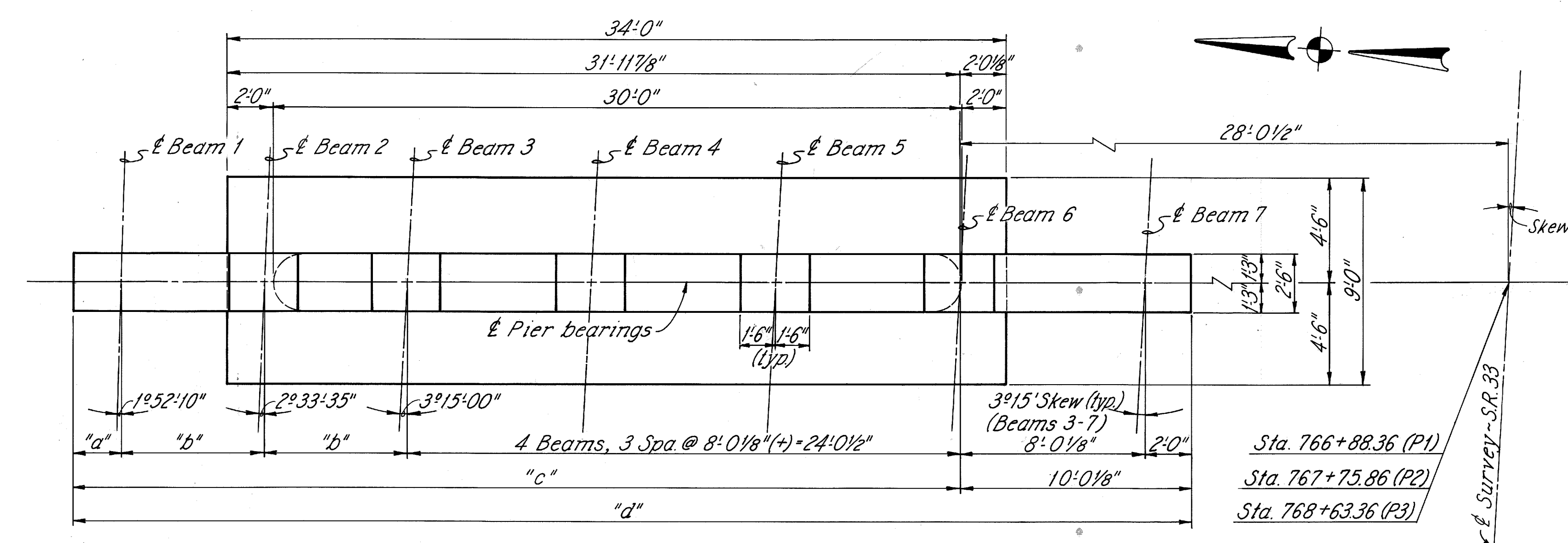
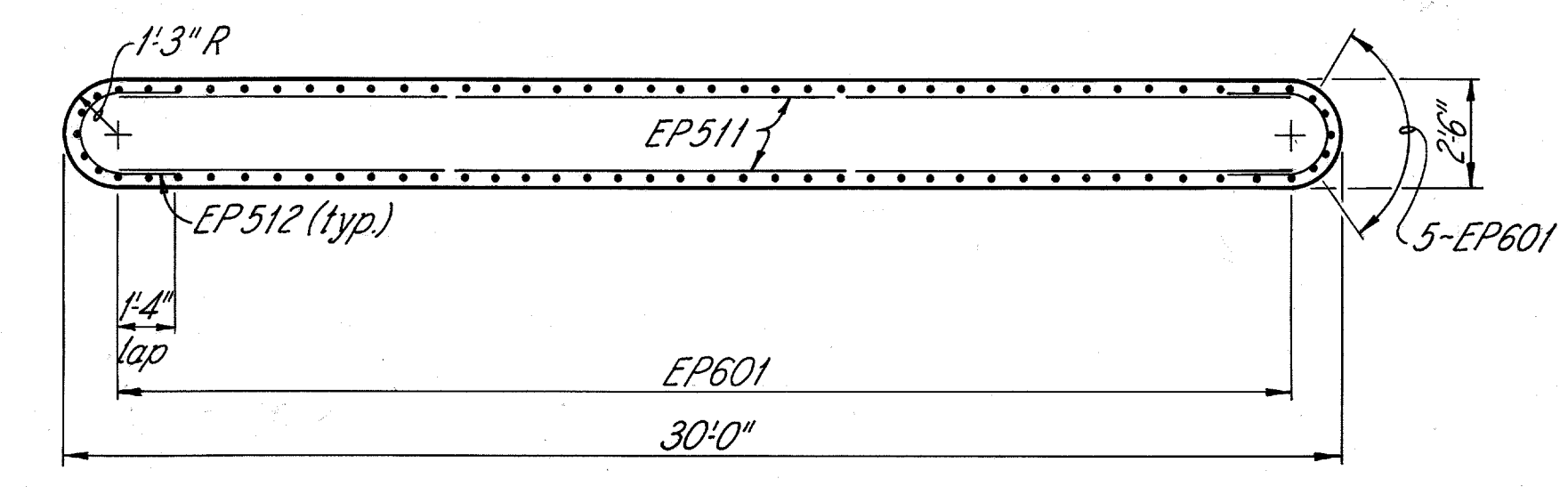


TABLE OF ELEVATIONS & DIMENSIONS

| LOCATION | EL. "A" | EL. "B" | EL. "C" | EL. "D" | EL. "E" | EL. "F" | EL. "G" | EL. "H" |
|----------|---------|---------|---------|---------|---------|---------|---------|---------|
| PIER 1 | 930.04 | 930.12 | 930.20 | 930.33 | 930.42 | 930.30 | 930.17 | 899.00 |
| PIER 2 | 929.90 | 930.00 | 930.10 | 930.22 | 930.31 | 930.19 | 930.06 | 897.50 |
| PIER 3 | 929.58 | 929.69 | 929.80 | 929.92 | 930.02 | 929.89 | 929.76 | 897.50 |

| LOCATION | DIM. "a" | DIM. "b" | DIM. "c" | DIM. "d" | DIM. "e" | DIM. "f" | DIM. "g" | DIM. "h" | DIM. "i" |
|----------|-----------|------------|------------|----------|-----------|----------|------------|------------|----------|
| PIER 1 | 2'-0 7/8" | 5'-2 1/4" | 36'-5 7/8" | 46'-6" | 3'-1 5/8" | 6'-6" | 21'-0 1/2" | 28'-0 1/2" | 7" |
| PIER 2 | 2'-2 5/8" | 6'-2 7/8" | 38'-8 7/8" | 48'-9" | 3'-1 7/8" | 8'-9" | 22'-4 3/4" | 29'-4 3/4" | 4" |
| PIER 3 | 2'-1 1/4" | 7'-3 9/16" | 40'-8 7/8" | 50'-9" | 3'-2 1/8" | 10'-9" | 22'-1" | 29'-1" | 8" |



ELEVATION
PIER 2 SHOWN
(PIER 1 & PIER 3 SIMILAR)

SECTION A-A

SECTION B-B

NOTE 1: This dimension may vary depending on the actual location of the existing piles. The contractor shall verify this dimension once the demolition of the exist piers has been completed and the exist piles are exposed. The footing dimensions shall be increased if necessary to accommodate the 2'-0" dimension from the edge of the 9'-0" side of the footing & the E of the exist pile & or the 1'-6" dimension from the edge of the 34'-0" side of the footing & the E of the exist pile. In no case shall the footing dimensions be decreased.

NOTE 2: The elevation at the bottom of the pier footings is approximate. The actual elevation shall be determined by the 1'-6" min. dimension from the top of the exist piles to the bottom of the footing. The footing depth of 3'-0" shall be increased if necessary to accommodate this 1'-6" dim. & in no case shall the pier stem height be changed.

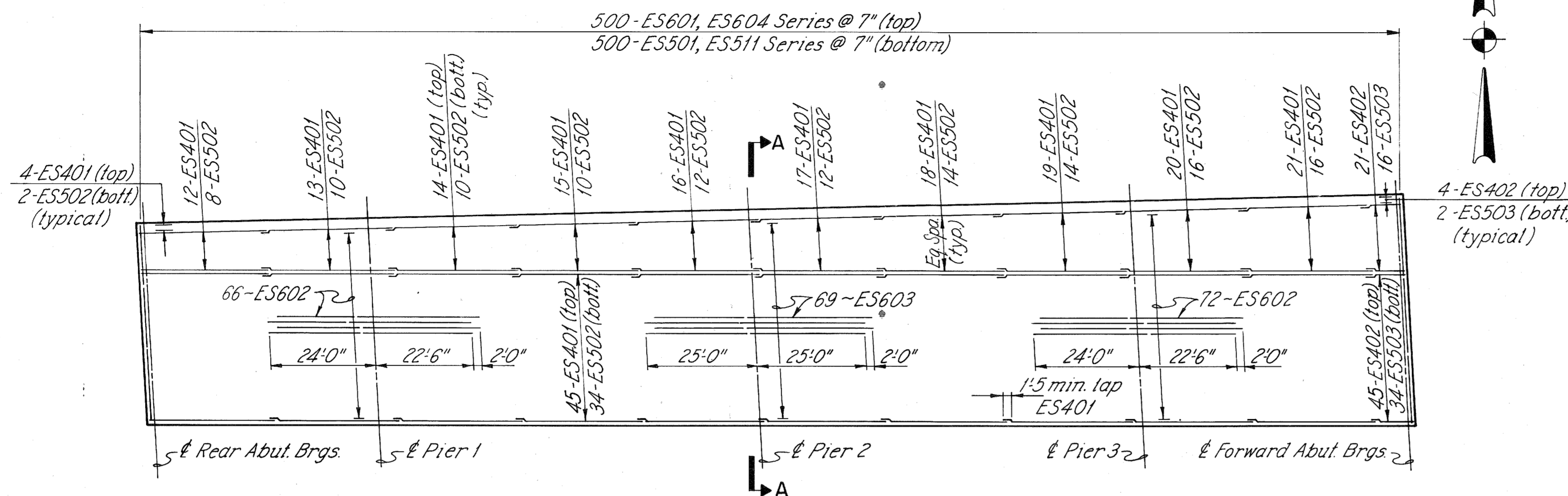
PARSONS BRINCKERHOFF OHIO, INC.
COLUMBUS, OHIO 7/13

PIER DETAILS
BRIDGE NO. AUG-33-1507L (W.B.)
OVER C.R. 25A & C.S.X.R.R.
AUGLAIZE COUNTY U.S.R. 33

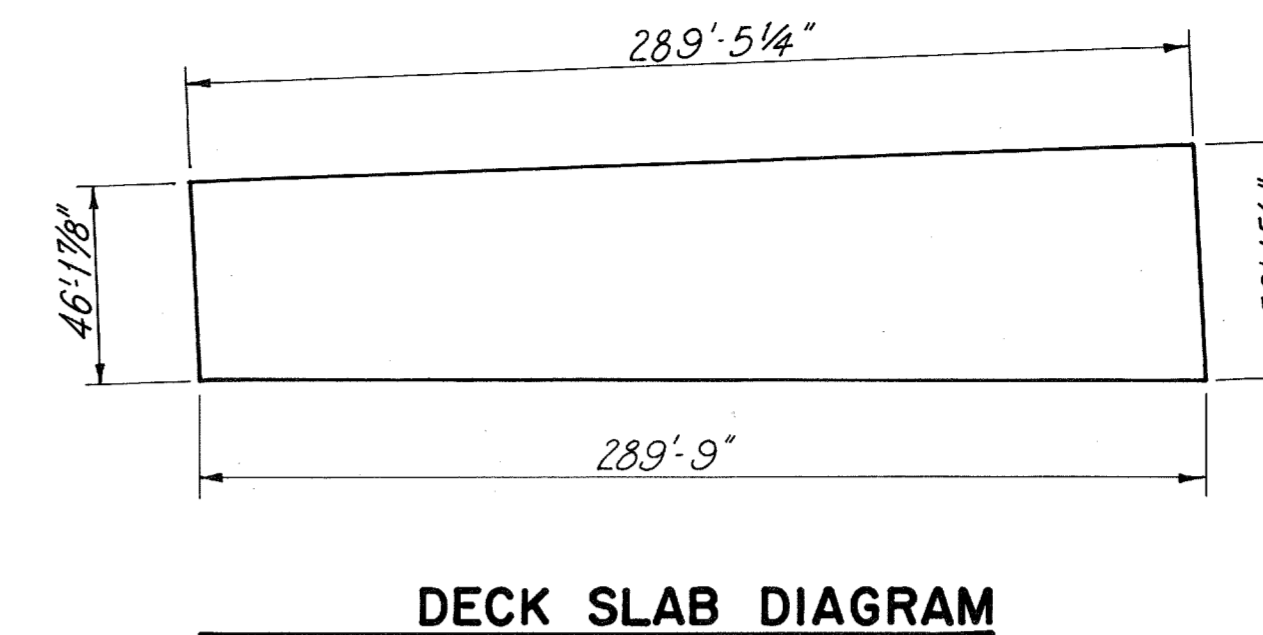
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
|----------|-------|--------|---------|----------|------|---------|
| TDW | BKU | BKU | BKU | JKS | | |

EPOXY COATED BARS are denoted on the plan sheets with the prefix of the letter "E."

AUGLAIZE COUNTY
AUG-33-13.44



DECK SLAB PLAN



DECK SLAB DIAGRAM

NOTES

DECK SLAB DEPTH: THE DISTANCE SHOWN FROM THE DECK SLAB TO THE TOP OF STEEL BEAM IS THE DESIGN DIMENSION. THE QUANTITY OF DECK CONCRETE TO BE PAID FOR SHALL BE BASED ON THIS DIMENSION, EVEN THOUGH DEVIATION FROM IT MAY BE NECESSARY BECAUSE THE TOP FLANGE OF THE BEAM MAY NOT HAVE THE EXACT CAMBER OR CONFORMATION REQUIRED TO PLACE IT PARALLEL TO THE FINISHED GRADE.

A HAUNCH WIDTH OF 9" SHALL BE USED FOR COMPUTING QUANTITY OF CONCRETE. HOWEVER, THE HAUNCH WIDTH MAY VARY BETWEEN 6" AND 12" (PROVIDED THAT THE SLOPE SHALL BE NOT MORE THAN 1:4 FOR A HAUNCH LESS THAN 9" IN WIDTH).

FIELD BEND TRANSVERSE BARS TO FIT CROWN. ALL TRANSVERSE BARS SHALL BE PARALLEL TO THE SUBSTRUCTURE. FIELD BENDING SHALL BE INCLUDED IN ITEM 509 FOR PAYMENT.

LONGITUDINAL STEEL SHALL BE FIELD CUT AS NECESSARY TO AVOID INTERFERENCE WITH SCUPPERS.

FOR MORE RAILING AND PARAPET DETAILS SEE STD. DWG. BR-1.

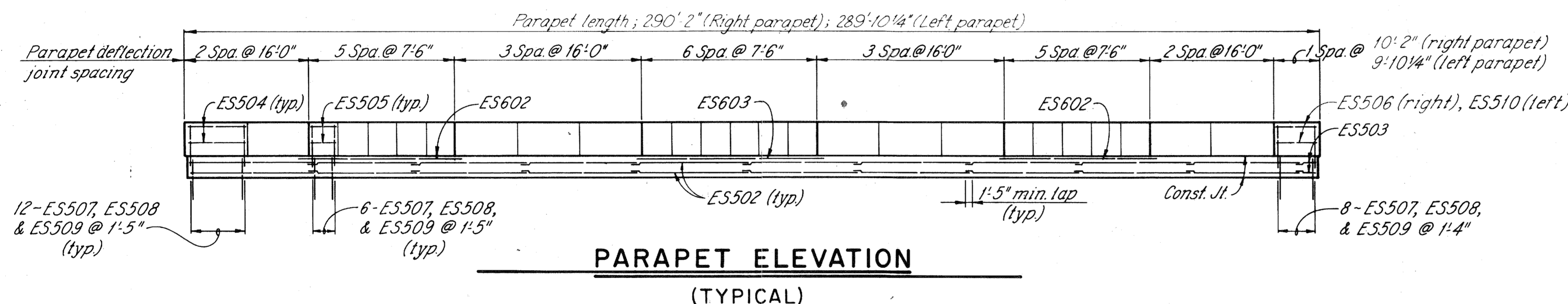
EPOXY-COATED BARS THAT ARE FIELD CUT AND BARS WITH THE EPOXY-COATING DAMAGED BY FIELD BENDING SHALL BE RECOATED AS PER APPROVED MANUFACTURER'S RECOMMENDATIONS.

FOR ADDITIONAL SUPERSTRUCTURE DETAILS SEE SHEETS **9/13** & **10/13**.

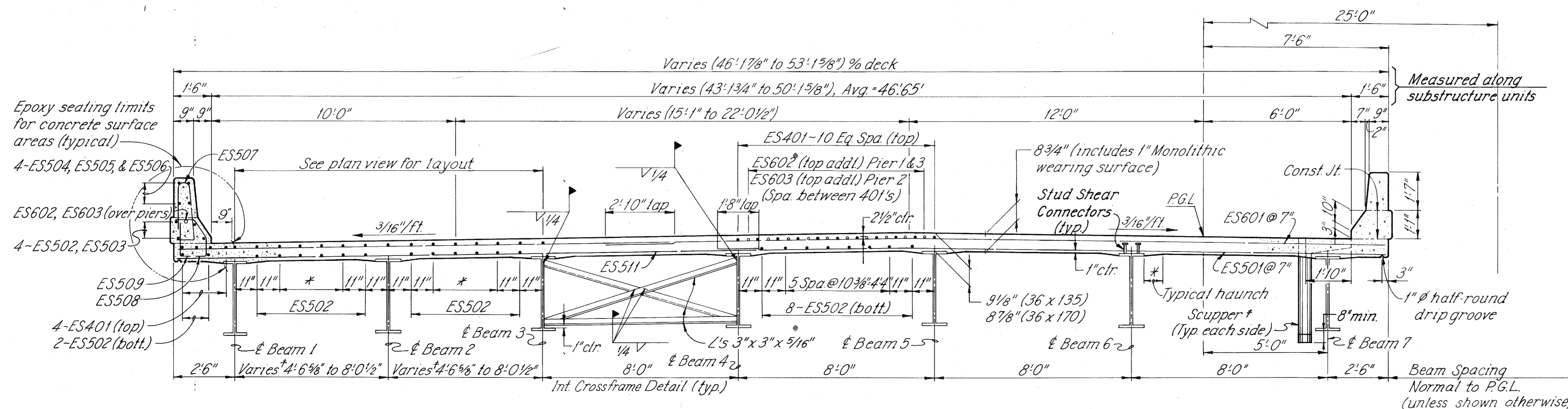
STAGGER REINFORCING BARS - ES602- OVER PIERS 1 & 3 AND ES603- OVER PIER 2

SCUPPERS SHALL BE IN ACCORDANCE WITH STD. DWG. SD-1-69 SHEET 3, EXCEPT THAT SCUPPER PIPES SHALL EXTEND 8" BELOW THE BOTTOM OF THE BEAMS INSTEAD OF 2". FOR SCUPPER LOCATIONS SEE SHEET **9/13**.

"E" - IN THE PREFIX OF A BAR MARK INDICATES EPOXY COATED REINFORCING STEEL.



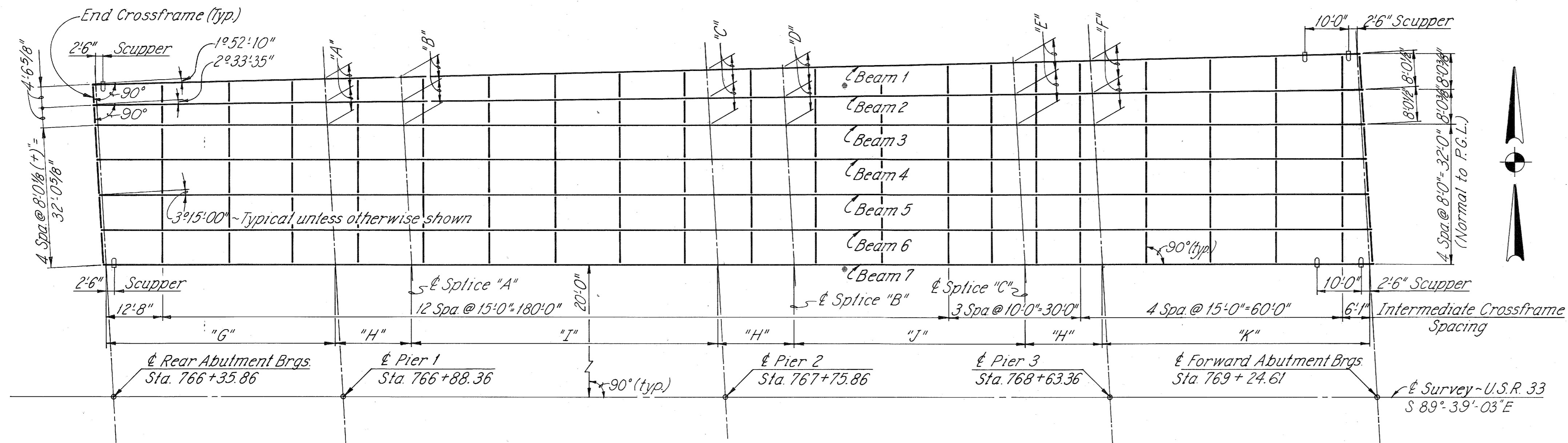
PARAPET ELEVATION
(TYPICAL)



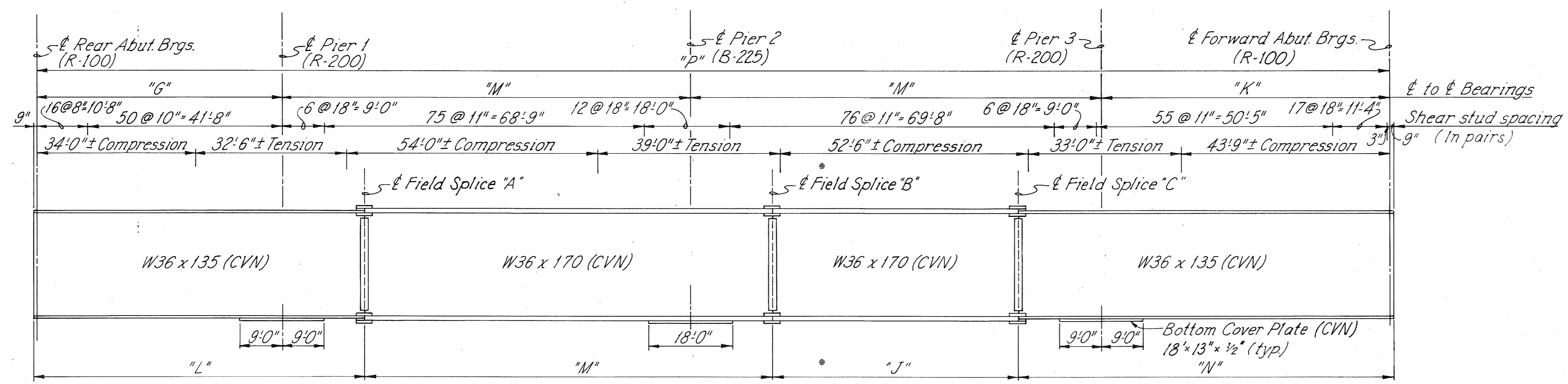
SECTION A-A

Note: + Dimensions parallel to substructure units

| | | | | | | |
|---------------------------------|-------|--------|---------|----------|------|-----------|
| PARSONS BRINCKERHOFF OHIO, INC. | | | | | | |
| COLUMBUS, OHIO | | | | | | |
| | | | | | | 8/13 |
| SUPERSTRUCTURE DETAILS-I | | | | | | |
| BRIDGE NO. AUG-33-1507L (W.B.) | | | | | | |
| OVER C.R. 25A & C.S.X.R.R. | | | | | | |
| AUGLAIZE COUNTY | | | | | | U.S.R. 33 |
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
| TDW | BKU | BKU | BKU | JKS | | |



FRAMING PLAN

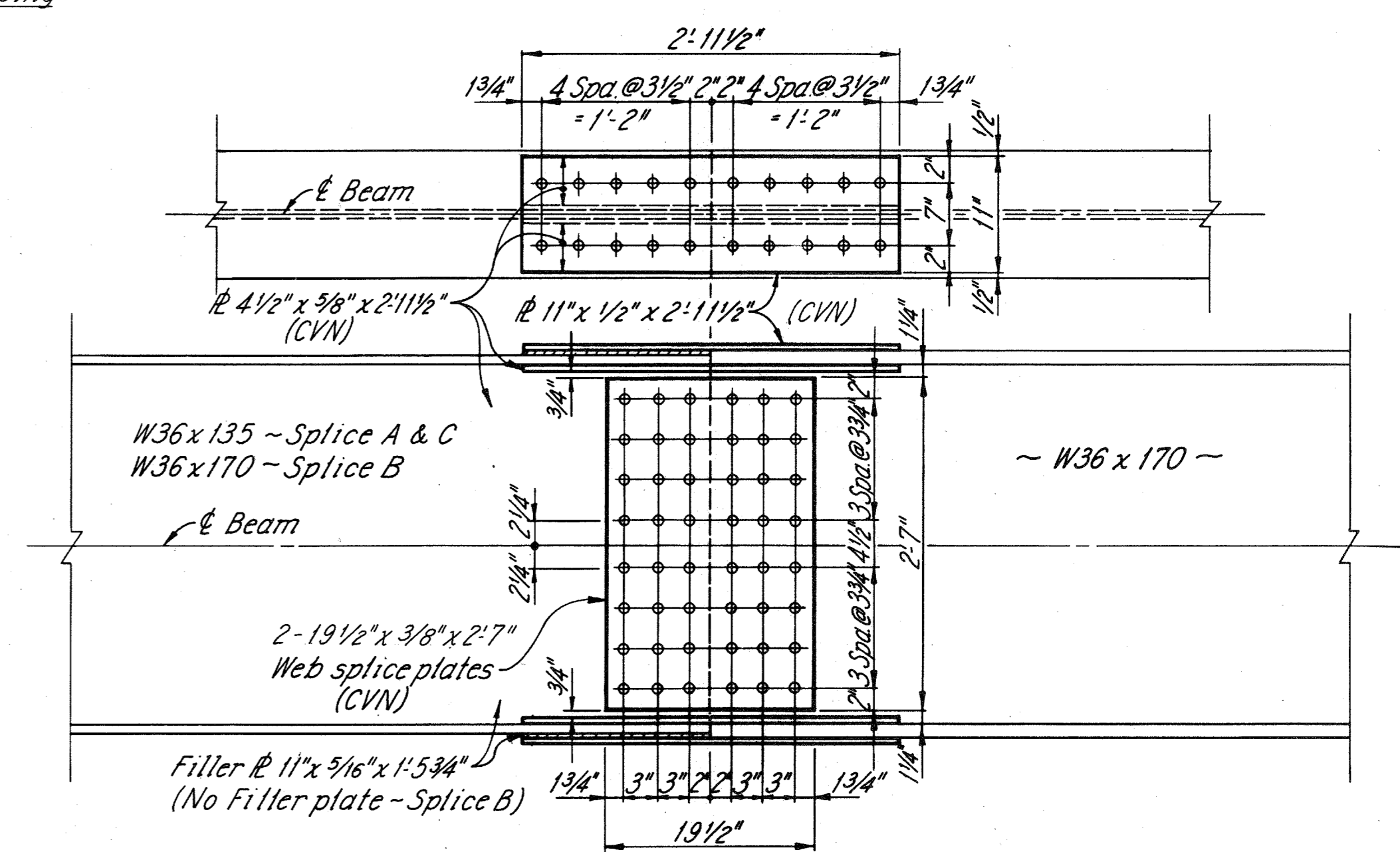


BEAM ELEVATION
BEAMS 1 THRU 7

| LOCATION | "A" | "B" | "C" | "D" | "E" | "F" | "G" | "H" | "I" | "J" | "K" | "L" | "M" | "N" | "P" | LOCATION |
|----------|-----------|-----------|-----------|------------|-------|------------|------------|------------|-------------|-------------|-------------|-------------|------------|-------------|---------------|----------|
| BEAM 1-2 | 5'-2 1/4" | 5'-4 3/4" | 6'-2 7/8" | 6'-5 7/16" | 7'-1" | 7'-3 9/16" | 52'-5 5/8" | 17'-5 3/4" | 69'-11 1/8" | 52'-5 9/16" | 61'-2 1/4" | 69'-11 1/8" | 87'-4 7/8" | 78'-8" | 288'-5 3/8" | BEAM 1 |
| BEAM 2-3 | 5'-2 1/4" | 5'-4 3/4" | 6'-2 7/8" | 6'-5 7/16" | 7'-1" | 7'-3 9/16" | 52'-5 5/8" | 17'-5 7/8" | 69'-11 1/2" | 52'-5 5/8" | 61'-2 9/16" | 69'-11 1/2" | 87'-5 3/8" | 78'-8 7/16" | 288'-6 13/16" | BEAM 2 |
| | | | | | | | 52'-6" | 17'-6" | 70'-0" | 52'-6" | 61'-3" | 70'-0" | 87'-6" | 78'-9" | 288'-9" | BEAM 3-7 |

Dimensions Between Adjacent Beams 1, 2 & 3

Dimensions Along Beams



BEAM SPLICE DETAIL
FIELD SPLICE "A" SHOWN
(FIELD SPLICE C - OPPOSITE HAND)
(FIELD SPLICE B - SIMILAR, EXCEPT AS NOTED)

PARTIAL PAINTING OF A588 STEEL: PARTIAL PAINTING OF A588 STEEL SHALL INCLUDE, AN 8' LENGTH OF THE BEAM ENDS ADJACENT TO THE ABUTMENTS, ALL CROSSFRAMES AND OTHER A588 STEEL WITHIN THESE LIMITS. PAINT SHALL BE 514, SYSTEM A, THE PRIME COAT SHALL BE 708.17. THE TOP COAT SHALL BE 708.18 EXCEPT THAT THE COLOR SHALL CLOSELY APPROACH FEDERAL STANDARD NO. 595A-20045 OR 20059 (RUSTY STEEL COLOR). THE REMAINING A588 STEEL IS TO BE LEFT UNPAINTED. SEE CMS 513.221 FOR CLEANING REQUIREMENTS.

SCUPPERS SHALL BE IN ACCORDANCE WITH STD. DWG. SD-1-69 SHEET 3 OF 4, EXCEPT THAT SCUPPER PIPES SHALL EXTEND 8" BELOW THE BOTTOM OF THE BEAM INSTEAD OF 2".

WHERE A SHAPE OR PLATE IS DESIGNATED (CVN) THE MATERIAL SHALL MEET SPECIFIED MINIMUM NOTCH TOUGHNESS REQUIREMENTS AS SPECIFIED IN 711.01 OF CMS.

ALL BEAM LENGTH DIMENSIONS ARE AT 60 DEG. F.

WELDING ATTACHMENT OF SUPPORTS FOR CONCRETE DECK FINISHING MACHINE MAY BE MADE TO AREAS OF THE FASCIA STRINGER FLANGES DESIGNATED 'COMPRESSION'. ATTACHMENTS SHALL NOT BE MADE TO AREAS DESIGNATED 'TENSION'. FILLET WELDS TO COMPRESSION FLANGES SHALL NOT BE CLOSER THAN 1" FROM EDGE OF FLANGE, NOT MORE THAN 2" LONG, AND NOT SMALLER THAN THE MINIMUM SIZE REQUIRED BY AASHTO.

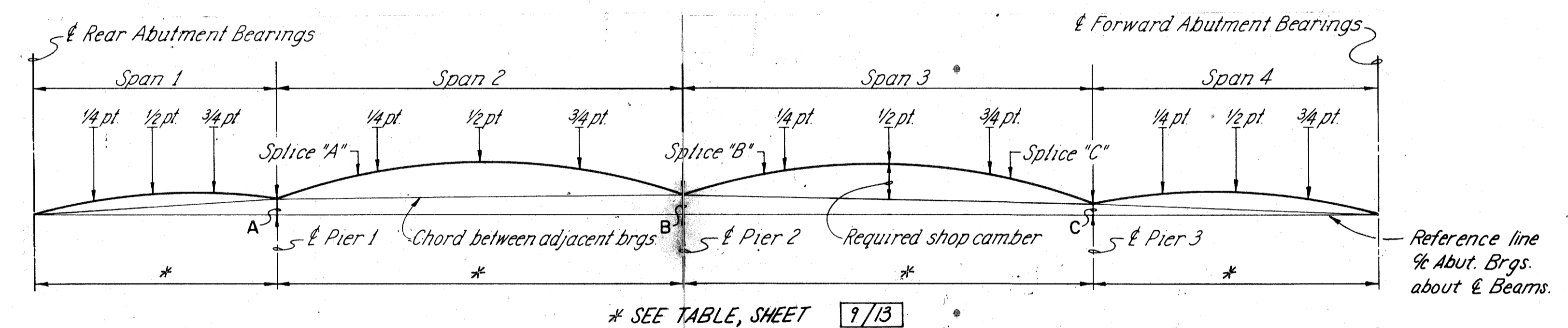
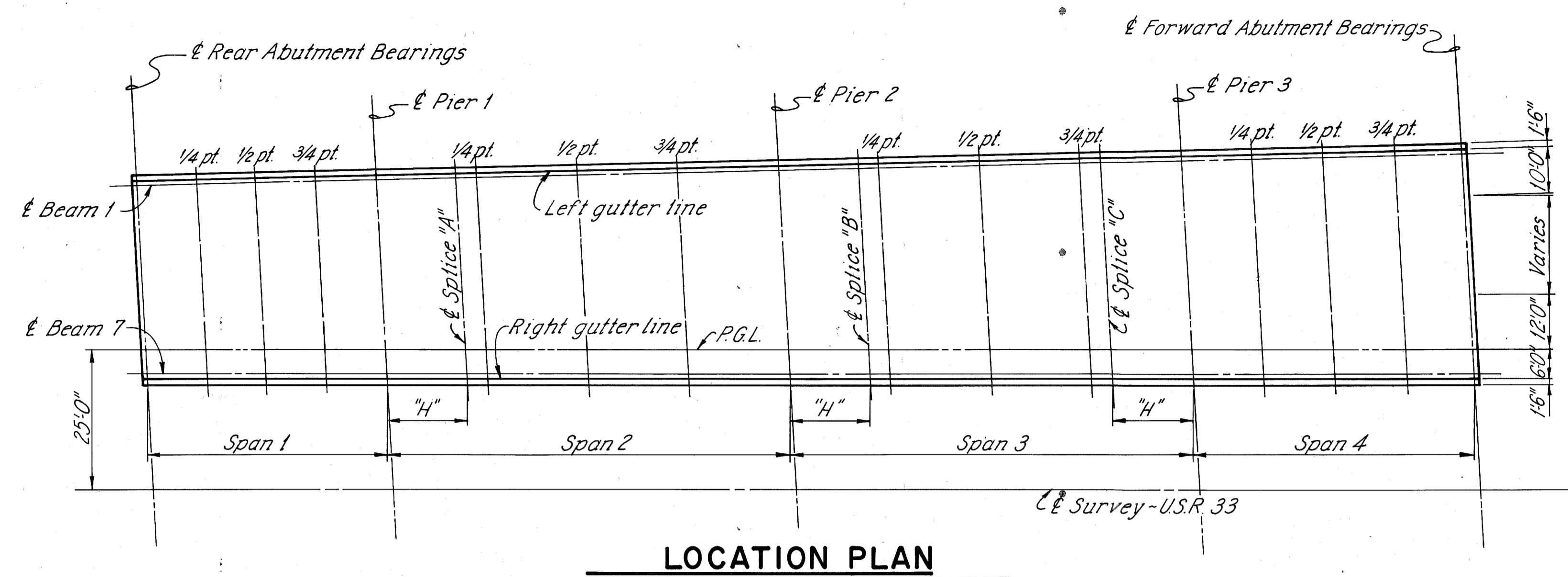
BEARINGS: IN LIEU OF A588 STEEL, A36 STEEL, GALVANIZED, MAY BE FURNISHED FOR THE BEARINGS, EXCEPTING THE UPPER PLATE ELEMENT OF THE BEARING. THIS A36 STEEL SHALL BE INCLUDED WITH THE A588 STEEL QUANTITY FOR PAYMENT.

FOR END CROSSFRAME DETAILS, SEE STD. DWG. SD-1-69 SHEET 1 OF 4.

FOR WELDING OF COVER PLATES, SEE STD. DWG. SD-1-69 SHEET 3 OF 4.

FOR BOLTED BEAM SPLICE DETAILS, SEE STD. DWG. SD-1-69 SHEET 4 OF 4. HIGH STRENGTH BOLTS SHALL BE 1" DIA. A325 UNLESS OTHERWISE NOTED.

| | | | | | | |
|---|-------|--------|---------|----------|------|---------|
| PARSONS BRINCKERHOFF OHIO, INC. COLUMBUS, OHIO | | | | | | |
| 9/13 | | | | | | |
| SUPERSTRUCTURE DETAILS-2 | | | | | | |
| BRIDGE NO. AUG-33-1507L (W.B.) OVER C.R. 25A & C.S.X. R.R. | | | | | | |
| AUGLAIZE COUNTY U.S.R. 33 | | | | | | |
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
| TDW | BKU | BKU | BKU | JKS | | |



BLOCKING & CAMBER DIAGRAM
(TYPICAL BEAMS 1 THRU 7)

| SPAN NO. → | 1 | | | 2 | | | 3 | | | 4 | | | | | |
|-----------------------------------|------|-------|------|------------|--------|--------|-------|------------|-------|--------|--------|------------|-------|--------|--------|
| | 1/4 | 1/2 | 3/4 | SPLICE "A" | 1/4 | 1/2 | 3/4 | SPLICE "B" | 1/4 | 1/2 | 3/4 | SPLICE "C" | 1/4 | 1/2 | 3/4 |
| Deflection due to weight of steel | 0 | 0 | 0 | 1/8" | 1/8" | 3/16" | 1/8" | 1/16" | 1/16" | 3/16" | 1/8" | 1/16" | 0 | 1/16" | 1/16" |
| Deflection due to remaining D.L. | 1/8" | 1/8" | 0 | 5/8" | 13/16" | 13/16" | 5/8" | 3/8" | 9/16" | 11/16" | 11/16" | 1/2" | 3/16" | 1/2" | 7/16" |
| Correction for vertical curve | 1/8" | 3/16" | 1/8" | 5/16" | 3/8" | 1/2" | 3/8" | 5/16" | 3/8" | 1/2" | 3/8" | 5/16" | 3/16" | 1/4" | 3/16" |
| Required shop camber | 1/4" | 5/16" | 1/8" | 11/16" | 15/16" | 17/8" | 11/8" | 3/4" | 1" | 13/4" | 11/8" | 7/8" | 3/8" | 13/16" | 11/16" |

| A | B | C |
|--------|--------|--------|
| 3 1/8" | 5 1/4" | 3 1/2" |

| LOCATIONS | LEFT GUTTER LINE | | RIGHT GUTTER LINE | | |
|--------------------|--------------------|-----------|-------------------|-----------|--------|
| | STATION | ELEV | STATION | ELEV | |
| SPAN 1 | BRG. REAR ABUTMENT | 766+32.34 | 935.05 | 766+34.78 | 935.17 |
| | 1/4 POINT | 766+45.44 | 935.10 | 766+47.91 | 935.23 |
| | 1/2 POINT | 766+58.55 | 935.14 | 766+61.04 | 935.26 |
| | 3/4 POINT | 766+71.66 | 935.15 | 766+74.16 | 935.28 |
| SPAN 2 | BRG. PIER 1 | 766+84.77 | 935.17 | 766+87.28 | 935.30 |
| | 1/4 POINT | 767+06.61 | 935.25 | 767+09.16 | 935.39 |
| | 1/2 POINT | 767+28.46 | 935.27 | 767+31.04 | 935.42 |
| | 3/4 POINT | 767+50.30 | 935.20 | 767+52.91 | 935.35 |
| SPAN 3 | BRG. PIER 2 | 767+72.15 | 935.10 | 767+74.78 | 935.26 |
| | 1/4 POINT | 767+93.99 | 935.08 | 767+96.66 | 935.24 |
| | 1/2 POINT | 768+15.84 | 935.03 | 768+18.54 | 935.20 |
| | 3/4 POINT | 768+37.68 | 934.89 | 768+40.41 | 935.06 |
| SPAN 4 | BRG. PIER 3 | 768+59.53 | 934.71 | 768+62.28 | 934.89 |
| | 1/4 POINT | 768+74.82 | 934.62 | 768+77.60 | 934.80 |
| | 1/2 POINT | 768+90.11 | 935.54 | 768+92.91 | 934.72 |
| | 3/4 POINT | 769+05.40 | 934.41 | 769+08.22 | 934.60 |
| BRG. FORWARD ABUT. | 769+20.69 | 934.24 | 769+23.54 | 934.44 | |

NOTE: THE CONSTRUCTION ELEVATIONS SHOWN ARE THOSE ON TOP OF THE CONCRETE, BEFORE THE DECK IS PLACED. PROPER ALLOWANCE FOR THE DEAD LOAD DEFLECTION OF THE WEIGHT OF THE CONCRETE DECK HAS BEEN MADE.

PARSONS BRINCKERHOFF OHIO, INC.
COLUMBUS, OHIO 10/13

SUPERSTRUCTURE DETAILS-3
BRIDGE NO. AUG-33-1507L (W.B.)
OVER C.R. 25A & C.S.X. R.R.
AUGLAIZE COUNTY U.S.R. 33

| | | | | | | |
|----------|-------|--------|---------|----------|------|---------|
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
| TDW | BKU | BKU | BKU | JKS | | |

ELASTOMERIC STRIP SEAL SYSTEM

SPECIAL NOTES:

ALL WELDING SHALL BE IN ACCORDANCE WITH THE WELDING SPECIFICATIONS FOR BRIDGE STRUCTURES, AASHTO M 587. ALL WELDING SHALL BE DONE BY A LICENSED WELDER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES AND STRUCTURES.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AND HIGHWAYS AT ALL TIMES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES AND STRUCTURES.

1. Seal shall have a movement rating of 3".
2. IT IS NECESSARY TO MODIFY THE DESIGN DRAWING TO ACCOMMODATE THE EXPANSION JOINT DEVICE. The drawings shall be modified by the Contractor and approved by the Engineer. Modified drawings shall be forwarded to the Bureau of Bridges, Structural Steel Section for use in checking shop drawings.
3. DURING INSTALLATION OF THE SUPPORT/ARMOR FOR THE SUPERSTRUCTURE SIDE OF THE EXPANSION JOINT SEAL, THE SEATING OF BEAMS ON BEARINGS SHALL BE CAREFULLY OBSERVED TO ASSURE THAT POSITIVE BEARING IS MAINTAINED. PROPER VERTICAL FIT OF THE SUPPORT/ARMOR OR THE BEAMS SHALL BE ACHIEVED BY POSITIONING OF THE BEVEL FILL PLATES RATHER THAN BY CLAMPING FORCE.
4. SEE STANDARD DRAWING EXJ-4-87 FOR ALL REMAINING DETAILS AND NOTES.
5. The above shall be included with item 516 for payment.

| STRIP SEAL JOINT AT ABUTMENT | | |
|------------------------------|--------------------------|------------------|
| JOINT SETTING TABLE | | |
| TEMP. (F°) | Joint Opening - Dim. "A" | |
| | Rear Abutment | Forward Abutment |
| 30° | 1 7/8" | 1 7/8" |
| 40° | 1 3/4" | 1 3/4" |
| 50° | 1 3/4" | 1 3/4" |
| 60° | 1 5/8" | 1 5/8" |
| 70° | 1 1/2" | 1 1/2" |
| 80° | * | * |
| 90° | * | * |

* MINIMUM JOINT OPENING (DIMENSION "A") AT TIME OF SEAL GLAND INSTALLATION SHALL NOT BE LESS THAN 1-1/2". IF THE JOINT OPENING IS LESS, THE INSTALLATION SHALL BE POSTPONED UNTIL THE TEMPERATURE DROPS A SUFFICIENT AMOUNT TO ALLOW THE MINIMUM 1-1/2" OPENING.

| | | | | | | | |
|--|-------|--------|---------|----------|------|---------|-----------|
| PARSONS BRINCKERHOFF OHIO, INC. COLUMBUS, OHIO | | | | | | | 11/13 |
| STRUCTURAL EXPANSION JOINT DETAIL | | | | | | | |
| BRIDGE NO. AUG-33-1507 L (W.B.) OVER C.R. 25A AND C.S.X. R.R. | | | | | | | |
| AUGLAIZE COUNTY | | | | | | | U.S.R. 33 |
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised | |
| | | | | | | | |

REINFORCING STEEL LIST

| MARK | NO. | LENGTH | WEIGHT | TYPE | A | B | C | D | E | F | G | H | J | K | O | RAD | INCRE. |
|--------------------------------------|-----|---------|--------|------|--------|--------|--------|-------|---------|--------|---|-------|-------|-------|---|-----|--------|
| REAR ABUTMENT * | | | | | | | | | | | | | | | | | |
| A501 | 36 | 8'-3" | 310 | 2 | 1'-7" | 5'-4" | 1'-7" | | | | | | | | | | |
| A502 | 32 | 8'-0" | 267 | 8 | 1'-3" | 6'-8" | | | | | | | | | | | |
| A504 | 12 | 23'-9" | 298 | STR | | | | | | | | | | | | | |
| A505 | 10 | 5'-0" | 52 | STR | | | | | | | | | | | | | |
| A601 | 32 | 14'-3" | 685 | 2 | 6'-9" | 5'-4" | 2'-6" | | | | | | | | | | |
| A801 | 14 | 25'-7" | 957 | STR | | | | | | | | | | | | | |
| A802 | 4 | 6'-0" | 64 | STR | | | | | | | | | | | | | |
| TOTAL = 2633 LB | | | | | | | | | | | | | | | | | |
| EA503 | 32 | 7'-2" | 240 | 2 | 2'-0" | 3'-5" | 2'-0" | | | | | | | | | | |
| EA504 | 16 | 23'-9" | 397 | STR | | | | | | | | | | | | | |
| EA505 | 8 | 5'-0" | 42 | STR | | | | | | | | | | | | | |
| EA602 | 43 | 8'-9" | 566 | 2 | 3'-10" | 1'-5" | 3'-10" | | | | | | | | | | |
| EA603 | 43 | 7'-1" | 458 | 2 | 3'-0" | 1'-5" | 3'-0" | | | | | | | | | | |
| EA604 | 43 | 5'-0" | 323 | 2 | 2'-3" | 10" | 2'-3" | | | | | | | | | | |
| EA803 | 28 | 4'-5" | 331 | 5 | | 0 | 1'-5" | 1'-9" | 1'-5" | 0 | | 1'-0" | | 1'-0" | | | |
| TOTAL, EPOXY COATED = 2387 LB | | | | | | | | | | | | | | | | | |
| FORWARD ABUTMENT * | | | | | | | | | | | | | | | | | |
| A501 | 41 | 8'-3" | 353 | 2 | 1'-7" | 5'-4" | 1'-7" | | | | | | | | | | |
| A502 | 37 | 8'-0" | 309 | 8 | 1'-3" | 6'-8" | | | | | | | | | | | |
| A504 | 12 | 27'-2" | 340 | STR | | | | | | | | | | | | | |
| A505 | 10 | 5'-0" | 52 | STR | | | | | | | | | | | | | |
| A601 | 37 | 14'-3" | 792 | 2 | 6'-9" | 5'-4" | 2'-6" | | | | | | | | | | |
| A801 | 14 | 29'-0" | 1084 | STR | | | | | | | | | | | | | |
| A802 | 4 | 6'-0" | 64 | STR | | | | | | | | | | | | | |
| TOTAL = 2994 LB | | | | | | | | | | | | | | | | | |
| EA503 | 37 | 7'-2" | 277 | 2 | 2'-0" | 3'-5" | 2'-0" | | | | | | | | | | |
| EA504 | 16 | 27'-2" | 454 | STR | | | | | | | | | | | | | |
| EA505 | 8 | 5'-0" | 42 | STR | | | | | | | | | | | | | |
| EA602 | 50 | 8'-9" | 658 | 2 | 3'-10" | 1'-5" | 3'-10" | | | | | | | | | | |
| EA603 | 50 | 7'-1" | 532 | 2 | 3'-0" | 1'-5" | 3'-0" | | | | | | | | | | |
| EA604 | 50 | 5'-0" | 376 | 2 | 2'-3" | 10" | 2'-3" | | | | | | | | | | |
| EA803 | 33 | 4'-5" | 390 | 5 | | 0 | 1'-5" | 1'-9" | 1'-5" | 0 | | 1'-0" | | 1'-0" | | | |
| TOTAL COATED = 2729 LB | | | | | | | | | | | | | | | | | |
| WINGWALLS * | | | | | | | | | | | | | | | | | |
| EW501 | 24 | 3'-0" | 76 | STR | | | | | | | | | | | | | |
| EW502 | 24 | 5'-3" | 132 | 9 | | | | | | | | | | | | | |
| EW503 | 32 | 13'-8" | 457 | STR | | | | | | | | | | | | | |
| EW504 | 32 | 4'-7" | 153 | STR | | | | | | | | | | | | | |
| EW505 | 32 | 2'-8" | 89 | 7 | 7" | 2'-1" | | | | | | | | | | | |
| EW506 | 32 | 4'-4" | 145 | STR | | | | | | | | | | | | | |
| W507 | 36 | 11'-5" | 429 | 10 | 5 1/2" | 3'-0" | 2'-3" | 3'-0" | 2'-3" | 5 1/2" | | | | | | | |
| W508 | 20 | 9'-5" | 197 | STR | | | | | | | | | | | | | |
| W509 | 20 | 9'-7" | 200 | STR | | | | | | | | | | | | | |
| W510 | 8 | 7'-6" | 63 | STR | | | | | | | | | | | | | |
| W511 | 24 | 15'-7" | 391 | STR | | | | | | | | | | | | | |
| W512 | 8 | 3'-5" | 30 | 8 | | 1'-0" | 1'-2" | 1'-6" | | | | | 1'-2" | | | | |
| W513 | 8 | 3'-5" | 30 | 8 | | 1'-6" | 1'-2" | 1'-0" | | | | | 1'-2" | | | | |
| W514 | 8 | 3'-5" | 30 | 8 | | 1'-0" | 1'-2" | 1'-6" | | | | | 1'-2" | | | | |
| W515 | 8 | 3'-5" | 30 | 8 | | 1'-6" | 1'-2" | 1'-0" | | | | | 1'-2" | | | | |
| EW601 | 28 | 3'-9" | 158 | 6 | | 8 1/2" | 2'-5" | 0 | 10 1/2" | 9" | | | | 6" | | | |
| EW602 | 4 | 3'-8" | 22 | 6 | | 8 1/2" | 2'-5" | 0 | 10" | 8" | | | | 5" | | | |
| EW603 | 4 | 3'-8" | 22 | 6 | | 8 1/2" | 2'-5" | 0 | 9 1/2" | 8" | | | | 4" | | | |
| EW604 | 4 | 3'-7" | 22 | 6 | | 8 1/2" | 2'-5" | 0 | 9" | 7" | | | | 3" | | | |
| EW605 | 4 | 3'-7" | 22 | 6 | | 8 1/2" | 2'-5" | 0 | 8 1/2" | 7" | | | | 2" | | | |
| W606 | 20 | 5'-10" | 176 | 2 | 2'-6" | 1'-2" | 2'-6" | | | | | | | | | | |
| W607 | 4 | 6'-6" | 40 | 2 | 2'-10" | 1'-2" | 2'-10" | | | | | | | | | | |
| W608 | 4 | 7'-8" | 47 | 2 | 3'-5" | 1'-2" | 3'-5" | | | | | | | | | | |
| W609 | 4 | 8'-10" | 54 | 2 | 4'-0" | 1'-2" | 4'-0" | | | | | | | | | | |
| W610 | 20 | 20'-2" | 606 | 2 | 9'-8" | 1'-2" | 9'-8" | | | | | | | | | | |
| W611 | 12 | 13'-10" | 250 | 2 | 6'-6" | 1'-2" | 6'-6" | | | | | | | | | | |

REINFORCING STEEL LIST

| MARK | NO. | LENGTH | WEIGHT | TYPE | A | B | C | D | E | F | G | H | J | K | O | RAD | INCRE. |
|-------------------------------|--------------|------------------|--------|------|-------|--------|----------------|-------|----------------|----------|---|---|---|-------|-------|-----|--------|
| W801 | 6 | 12'-4" | 198 | STR | | | | | | | | | | | | | |
| W802 | 6 | 12'-6" | 201 | STR | | | | | | | | | | | | | |
| W803 | 2 | 6'-7" | 36 | 8 | | 1'-0" | 3'-0" | 3'-0" | | | | | | 3'-0" | | | 2" |
| W804 | 2 | 6'-7" | 36 | 8 | | 3'-0" | 3'-0" | 1'-0" | | | | | | 3'-0" | | | 2" |
| W805 | 2 | 6'-7" | 36 | 8 | | 1'-0" | 3'-0" | 3'-0" | | | | | | 3'-0" | | | 1" |
| W806 | 2 | 6'-7" | 36 | 8 | | 3'-0" | 3'-0" | 1'-0" | | | | | | 3'-0" | | | 1" |
| TOTAL COATED = 1298 LB | | | | | | | | | | | | | | | | | |
| PIER #1 * | | | | | | | | | | | | | | | | | |
| EP501 | 1-SER. OF 13 | 10'-3" TO 18'-3" | 194 | 10 | | 2'-2" | 2'-8" TO 6'-8" | 2'-2" | 2'-8" TO 6'-8" | | | | | | | | |
| EP502 | 1-SER. OF 13 | 7'-7" TO 15'-7" | 157 | 10 | | 10" | 2'-8" TO 6'-8" | 10" | 2'-8" TO 6'-8" | | | | | | | | |
| EP503 | 4 | 31'-0" | 130 | STR | | | | | | | | | | | | | |
| EP504 | 2 | 35'-8" | 75 | STR | | | | | | | | | | | | | |
| EP505 | 2 | 40'-0" | 84 | STR | | | | | | | | | | | | | |
| EP506 | 2 | 44'-0" | 92 | STR | | | | | | | | | | | | | |
| EP507 | 4 | 46'-0" | 192 | STR | | | | | | | | | | | | | |
| EP508 | 4 | 10'-6" | 44 | STR | | | | | | | | | | | | | |
| EP509 | 29 | 18'-3" | 552 | 10 | | 2'-2" | 6'-8" | 2'-2" | 6'-8" | 5 1/2" x | | | | | | | |
| EP510 | 29 | 15'-7" | 472 | 10 | | 10" | 6'-8" | 10" | 6'-8" | 5 1/2" x | | | | | | | |
| EP511 | 30 | 27'-6" | 861 | STR | | | | | | | | | | | | | |
| EP512 | 30 | 5'-9" | 180 | 11 | | 1'-4" | 3'-3" | 1'-4" | | | | | | | 2'-0" | | 1'-0" |
| EP513 | 1-SER. OF 9 | 10'-3" TO 18'-3" | 134 | 10 | | 2'-2" | 2'-8" TO 6'-8" | 2'-2" | 2'-8" TO 6'-8" | 5 1/2" | | | | | | | |
| EP514 | 1-SER. OF 9 | 7'-7" TO 15'-7" | 109 | 10 | | 10" | 2'-8" TO 6'-8" | 10" | 2'-8" TO 6'-8" | 5 1/2" | | | | | | | |
| EP515 | 4 | 7'-0" | 30 | STR | | | | | | | | | | | | | |
| EP601 | 76 | 23'-0" | 2626 | STR | | | | | | | | | | | | | |
| EP602 | 86 | 6'-6" | 840 | 3 | 2'-0" | 4'-8" | | | | | | | | | | | |
| EP1001 | 6 | 48'-2" | 1243 | 3 | 2'-6" | 46'-0" | | | | | | | | | | | |
| EP1002 | 2 | 46'-0" | 313 | STR | | | | | | | | | | | | | |
| P603 | 10 | 34'-10" | 524 | 1 | 8" x | 33'-6" | | | | | | | | | | | |
| P801 | 34 | 9'-5" | 855 | 1 | 11" x | 8'-6" | | | | | | | | | | | |
| PIER #2 * | | | | | | | | | | | | | | | | | |
| EP501 | 1-SER. OF 13 | 10'-3" TO 18'-3" | 194 | 10 | | 2'-2" | 2'-8" TO 6'-8" | 2'-2" | 2'-8" TO 6'-8" | | | | | | | | |
| EP502 | 1-SER. OF 13 | 7'-7" TO 15'-7" | 157 | 10 | | 10" | 2'-8" TO 6'-8" | 10" | 2'-8" TO 6'-8" | | | | | | | | |
| EP503 | 4 | 31'-0" | 130 | STR | | | | | | | | | | | | | |
| EP504 | 2 | 35'-9" | 75 | STR | | | | | | | | | | | | | |
| EP505 | 2 | 40'-6" | 85 | STR | | | | | | | | | | | | | |
| EP506 | 2 | 45'-6" | 95 | STR | | | | | | | | | | | | | |
| EP507 | 4 | 48'-3" | 202 | STR | | | | | | | | | | | | | |
| EP508 | 4 | 10'-6" | 44 | STR | | | | | | | | | | | | | |
| EP509 | 29 | 18'-3" | 552 | 10 | | 2'-2" | 6'-8" | 2'-2" | 6'-8" | | | | | | | | |
| EP510 | 29 | 15'-7" | 472 | 10 | | 10" | 6'-8" | 10" | 6'-8" | | | | | | | | |
| EP511 | 30 | 27'-6" | 861 | STR | | | | | | | | | | | | | |
| EP512 | 30 | 5'-9" | 180 | 11 | | 1'-4" | 3'-3" | 1'-4" | | | | | | | 2'-0" | | 1'-0" |
| EP513 | 1-SER. OF 12 | 10'-3" TO 18'-3" | 179 | 10 | | 2'-2" | 2'-8" TO 6'-8" | 2'-2" | 2'-8" TO 6'-8" | | | | | | | | |
| EP514 | 1-SER. OF 12 | 7'-7" TO 15'-7" | 145 | 10 | | 10" | 2'-8" TO 6'-8" | 10" | 2'-8" TO 6'-8" | | | | | | | | |
| EP515 | 4 | 9'-0" | 38 | STR | | | | | | | | | | | | | |
| EP601 | 76 | 24'-4" | 2778 | STR | | | | | | | | | | | | | |
| EP602 | 86 | 6'-6" | 840 | 3 | 2'-0" | 4'-8" | | | | | | | | | | | |

***NOTE:**

THE REAR ABUTMENT BAR MARKS SHALL HAVE THE PREFIX "R"

THE FORWARD ABUTMENT BAR MARKS SHALL HAVE THE PREFIX "F"

THE LETTER 'E' BEFORE A BAR MARK DENOTES AN EPOXY COATED REINFORCING BAR.

THE PREFIX IN THE BAR MARKS FOR THE PIERS SHALL BE AS FOLLOWS:
 PIER-1: 1P OR 1EP
 PIER-2: 2P OR 2EP
 PIER-3: 3P OR 3EP

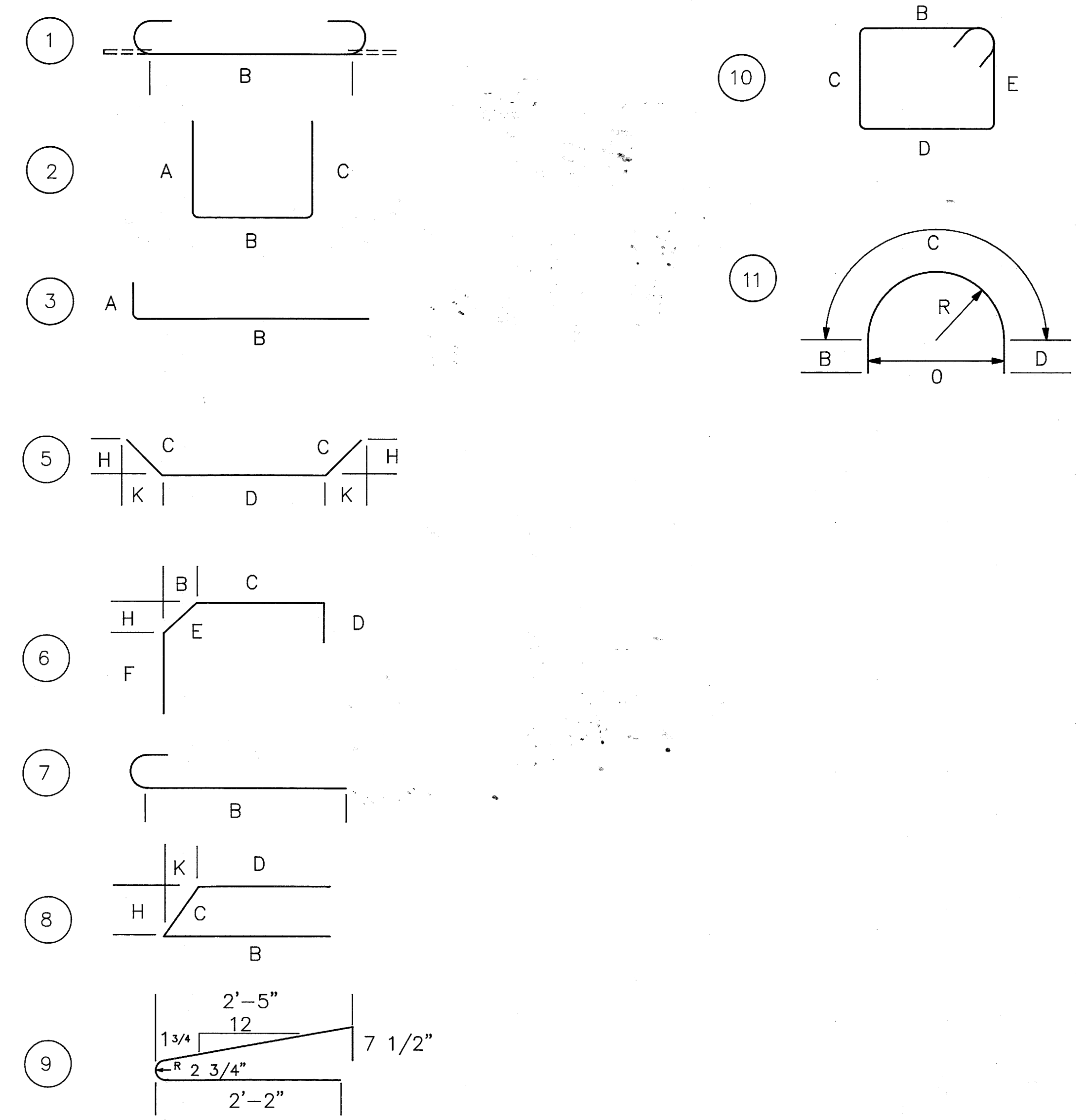
| | | | | | | | |
|---|-------|--------|---------|----------|------|---------|--|
| PARSONS BRINCKERHOFF OHIO, INC. COLUMBUS, OHIO | | | | | | | |
| REINFORCING STEEL LIST | | | | | | | |
| BRIDGE NO. AUG-33-1507L OVER HARDIN PIKE | | | | | | | |
| AUGLAIZE COUNTY | | | | OHIO | | | |
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised | |
| TDW | MDS | | MDS | JKS | | | |

AUGLAIZE COUNTY
AUG-33-13. 44

BAR BENDING SCHEDULE

REINFORCING STEEL LIST

| MARK | NO. | LENGTH | WEIGHT | TYPE | A | B | C | D | E | F | G | H | J | K | O | RAD | INCR. |
|-----------------------------------|--------------------------|-------------------|--------|------|---------|--------|-------|---------|---------|--------|---|---|---|---|-------|-------|-------|
| PIER #2 * | | | | | | | | | | | | | | | | | |
| EP1001 | 6 | 50'-5" | 1301 | 3 | 2'-6" | 46'-0" | | | | | | | | | | | |
| EP1002 | 2 | 48'-3" | 415 | STR | | | | | | | | | | | | | |
| P603 | 10 | 34'-10" | 524 | 1 | | 33'-6" | | | | | | | | | | | |
| P801 | 34 | 9'-5" | 855 | 1 | | 8'-6" | | | | | | | | | | | |
| PIER #3 * | | | | | | | | | | | | | | | | | |
| EP501 | 1-SER. OF 13 | 10'-3" TO 18'-3" | 194 | 10 | 5 1/2" | 2'-2" | 2'-8" | 2'-2" | 2'-8" | 5 1/2" | | | | | | 4" | |
| EP502 | 1-SER. OF 13 | 7'-7" TO 15'-7" | 157 | 10 | 5 1/2" | 10" | 6'-8" | 10" | 6'-8" | 5 1/2" | | | | | | 6" | |
| EP503 | 4 | 31'-0" | 130 | STR | | | | | | | | | | | | | |
| EP504 | 2 | 36'-4" | 76 | STR | | | | | | | | | | | | | |
| EP505 | 2 | 41'-8" | 85 | STR | | | | | | | | | | | | | |
| EP506 | 2 | 45'-6" | 95 | STR | | | | | | | | | | | | | |
| EP507 | 4 | 48'-3" | 202 | STR | | | | | | | | | | | | | |
| EP508 | 4 | 10'-6" | 44 | STR | | | | | | | | | | | | | |
| EP509 | 29 | 18'-3" | 552 | 10 | 5 1/2" | 2'-2" | 6'-8" | 2'-2" | 6'-8" | 5 1/2" | | | | | | | |
| EP510 | 29 | 15'-7" | 472 | 10 | 5 1/2" | 10" | 6'-8" | 10" | 6'-8" | 5 1/2" | | | | | | | |
| EP511 | 30 | 27'-6" | 861 | STR | | | | | | | | | | | | | |
| EP512 | 30 | 5'-9" | 180 | 11 | | 1'-4" | 3'-3" | 1'-4" | | | | | | | 2'-0" | 1'-0" | |
| EP513 | 1-SER. OF 14 | 10'-3" TO 18'-3" | 179 | 10 | 5 1/2" | 2'-2" | 2'-8" | 2'-2" | 2'-8" | 5 1/2" | | | | | | 4" | |
| EP514 | 1-SER. OF 14 | 7'-7" TO 15'-7" | 145 | 10 | 5 1/2" | 10" | 2'-8" | 10" | 2'-8" | 5 1/2" | | | | | | 4" | |
| EP515 | 4 | 11'-0" | 46 | STR | | | | | | | | | | | | | |
| EP601 | 76 | 24'-1" | 2750 | STR | | | | | | | | | | | | | |
| EP602 | 86 | 6'-6" | 840 | 3 | 2'-0" | 4'-8" | | | | | | | | | | | |
| EP1001 | 6 | 52'-5" | 1353 | 3 | 2'-6" | 50'-3" | | | | | | | | | | | |
| EP1002 | 2 | 50'-3" | 433 | STR | | | | | | | | | | | | | |
| P603 | 10 | 34'-10" | 524 | 1 | | 33'-6" | | | | | | | | | | | |
| P801 | 34 | 9'-5" | 855 | 1 | | 8'-6" | | | | | | | | | | | |
| TOTAL, EPOXY COATED = 25,865 | | | | | | | | | | | | | | | | | |
| TOTAL, NON-EPOXY COATED = 4137 LB | | | | | | | | | | | | | | | | | |
| SUPERSTRUCTURE | | | | | | | | | | | | | | | | | |
| ES401 | 655 | 30'-0" | 13134 | STR | | | | | | | | | | | | | |
| ES402 | 70 | 3'-5" | 160 | STR | | | | | | | | | | | | | |
| ES501 | 500 | 27'-4" | 14255 | STR | | | | | | | | | | | | | |
| ES502 | 562 | 30'-0" | 17585 | STR | | | | | | | | | | | | | |
| ES503 | 60 | 6'-0" | 376 | STR | | | | | | | | | | | | | |
| ES504 | 80 | 15'-6" | 1294 | STR | | | | | | | | | | | | | |
| ES505 | 128 | 7'-0" | 935 | STR | | | | | | | | | | | | | |
| ES506 | 4 | 9'-8" | 41 | STR | | | | | | | | | | | | | |
| ES507 | 240 | 5'-3" | 1315 | 9 | | | | | | | | | | | | | |
| ES508 | 240 | 3'-0" | 751 | 6 | | 8 1/2" | 9" | 10 1/2" | 10 1/2" | 9" | | | | | 6" | | |
| ES509 | 240 | 2'-2" | 543 | 3 | 10 1/2" | 1'-5" | | | | | | | | | | | |
| ES510 | 4 | 9'-4" | 39 | STR | | | | | | | | | | | | | |
| ES511 | 1 SER 10 SETS OF 50 BARS | 20'-0" TO 27'-6" | 12386 | STR | | | | | | | | | | | | | |
| ES601 | 500 | 31'-4" | 23531 | STR | | | | | | | | | | | | | |
| ES602 | 142 | 44'-6" | 9492 | STR | | | | | | | | | | | | | |
| ES603 | 67 | 50'-0" | 5032 | STR | | | | | | | | | | | | | |
| ES604 | 1 SER 10 SETS OF 50 BARS | 17'-7" TO 24'-11" | 15959 | STR | | | | | | | | | | | | | |
| TOTAL, EPOXY COATED = 116,828 LB | | | | | | | | | | | | | | | | | |



REINFORCING STEEL SAMPLES:
REFER TO CMS SECTIONS 106.03,700,709.01 THRU 709.05 AND 709.08. SUFFICIENT ADDITIONAL REINFORCING STEEL SHALL BE PROVIDED FOR SAMPLING. RANDOM SAMPLES SHALL BE REPLACED IN THE STRUCTURES BY THE ADDITIONAL STEEL, SPLICED IN ACCORDANCE WITH 509.08.

FOR ADDITIONAL NOTES SEE 12/13

* ~ SERIES BAR
"E" IN THE PREFIX OF A BAR MARK INDICATES EPOXY COATED REBAR

PARSONS BRINCKERHOFF OHIO, INC.
COLUMBUS, OHIO 13/13

REINFORCING STEEL LIST
BRIDGE NO. AUG-33-1507L
OVER HARDIN PIKE

AUGLAIZE COUNTY OHIO

| | | | | | | |
|----------|-------|--------|---------|----------|------|---------|
| Designed | Drawn | Traced | Checked | Reviewed | Date | Revised |
| TDW | MDS | | MDS | JKS | | |