

MAINTAINING TRAFFIC

IT IS THE INTENTION TO PERFORM THE REQUIRED WORK WITHIN THESE PLANS WITH THE LEAST INCONVENIENCE TO AND THE MAXIMUM SAFETY OF THE CONTRACTOR AND THE TRAVELING PUBLIC. THE REQUIREMENTS FOR MAINTAINING TRAFFIC AS SPECIFIED IN THE "OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREET AND HIGHWAYS" (CURRENT EDITION, LATEST REVISION), PERTINENT PROVISIONS OF THE "OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS" (INCLUDING SUPPLEMENTAL SPECIFICATION) AND APPLICABLE STANDARD CONSTRUCTION DRAWINGS SHALL APPLY TO THIS PROJECT IN ADDITION TO THE FOLLOWING NOTES AND DETAILS.

AUG-75-0674:

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION FOR IR-75 SHALL BE MAINTAINED AT ALL TIMES FOLLOWING THE PERMITTED LANE CLOSURE SCHEDULE. LANE OR RAMP CLOSURES AT THIS LOCATION (EITHER INLET OR OUTLET) MUST FOLLOW SCD. MT-95.30 OR MT-95.45 AND THE ALLOWABLE TIMES IN THE PERMITTED LANE CLOSURE SCHEDULE.

IN COORDINATION WITH THE MAINTENANCE OF TRAFFIC SCHEME FOR PID 97794, AUG-75-7.54, WORK AT THE OUTLET (WEST SIDE OF IR-75) SHALL BE COMPLETED DURING THE 2024 CONSTRUCTION SEASON. ALL WORK AND STAGING AT THE INLET (EAST SIDE OF IR-75) FOR THE STRUCTURAL SPRAY LINER AND PIPE EXTENTIONS SHALL BE CONSTRUCTED DURING THE 2025 SEASON.

LANE OR SHOULDER CLOSURES FOR TH OUTLET WORK IS INTENDED TO BE COMPLETED WITH DRUMS. THE INLET WORK (EAST SIDE) PERFORMED IN 2025 IS EXPECTED TO USE DRUMS FOR LANE CLOSURES AND SHOULD CLOSURES WITH PORTABLE BARRIER TO PROTECT THE WORK AND EQUIPMENT. AN ESTIMATED QUANTITY FOR PORTABLE BARRIER AND RELATED ITEMS ARE INCLUDED IN THE SUBSUMMARY FOR THIS LOCATION (SHEET 14).

DAR-121-1280:

A MINIMUM OF ONE LANE OF TRAFFIC IN EITHER DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT. THE CONTRACTOR SHALL BE PERMITTED TO CLOSE ONE LANE BY USING FLAGGERS TO CONTROL TRAFFIC. THE PERMITTED LANE CLOSURE SHALL BE DURING WORK HOURS AND USE OF THE LANE IS TO PROVIDED SPACE FOR DELIVERING MATERIALS TO AND FROM THE JOB SITE. THE CONTRACTOR SHALL WORK WITH THE PROJECT ENGINEER TO DETERMINE WHEN LANE CLOSURE IS WARRENTED. CLOSURE SHALL BE AS PER SCD. MT-97.10.

DAR-121-18.71:

A MINIMUM OF ONE LANE OF TRAFFIC IN EITHER DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT. THE CONTRACTOR SHALL BE PERMITTED TO CLOSE ONE LANE BY USING FLAGGERS TO CONTROL TRAFFIC. THE PERMITTED LANE CLOSURE SHALL BE DURING WORK HOURS AND USE OF THE LANE IS TO PROVIDED SPACE FOR DELIVERING MATERIALS TO AND FROM THE JOB SITE. THE CONTRACTOR SHALL WORK WITH THE PROJECT ENGINEER TO DETERMINE WHEN LANE CLOSURE IS WARRENTED. CLOSURE SHALL BE AS PER SCD. MT-97.10.

DAR-127-13.67:

A MINIMUM OF ONE LANE OF TRAFFIC IN EITHER DIRECTION BOTH NORTH AND SOUTH SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT. THE CONTRACTOR SHALL BE PERMITTED TO CLOSE ONE LANE AND SHOULDER BY USE OF PORTABLE BARRIER TO CONTROL TRAFFIC. THE PERMITTED LANE CLOSURE SHALL BE AS PER THE PERMITTED LANE CLOSURE WEBSITE LOCATED AT:

<http://plcm.dot.state.oh.us>

THIS CLOSURE SHALL BE IN ACCORDANCE WITH SCD. MT-95.40

DAR-127-14.90:

A MINIMUM OF ONE LANE OF TRAFFIC IN THE SOUTHBOUND DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT. THE CONTRACTOR SHALL BE PERMITTED TO CLOSE ONE LANE AND SHOULDER BY USE OF DRUMS TO CONTROL TRAFFIC. THE INTENT FOR THIS CLOSURE IS TO ALLOW FOR REMOVAL AND DELIVERY OF MATERIALS

NECESSARY FOR THIS PROJECT. THIS CLOSURE SHALL BE AS PER THE PERMITTED LANE CLOSURE LOCATED AT:

<http://plcm.dot.state.oh.us>

THIS CLOSURE SHALL BE IN ACCORDANCE WITH SCD. MT-95.30

SHE-75-1224:

A MINIMUM OF TWO LANES OF TRAFFIC IN EITHER DIRECTION BOTH NORTH AND SOUTH SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT. THE CONTRACTOR SHALL BE PERMITTED TO CLOSE THE OUTSIDE SHOULDER BY USE OF DRUMS TO CONTROL TRAFFIC. THE CONTRACTOR SHALL BE PERMITTED TO CLOSE THE INSIDE SHOULDERS BY USE OF PORTABLE BARRIER TO CONTROL TRAFFIC. BOTH INSIDE AND OUTSIDE SHOULDERS SHALL NOT BE PERMITTED TO BE CLOSED AT THE SAME TIME. THIS CLOSURE SHALL BE IN ACCORDANCE WITH SCD. MT-95.45.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

- CHRISTMAS FOURTH OF JULY
- NEW YEARS LABOR DAY
- MEMORIAL DAY THANKSGIVING
- GREAT DARKE COUNTY FAIR
- US 127 YARD SALE WEEKEND

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY TIME ALL LANES
OR EVENT MUST BE OPEN TO TRAFFIC

- SUNDAY 12:00N FRIDAY THROUGH 6:00AM MONDAY
- MONDAY 12:00N FRIDAY THROUGH 6:00AM TUESDAY
- TUESDAY 12:00N MONDAY THROUGH 6:00AM WEDNESDAY
- WEDNESDAY 12:00N TUESDAY THROUGH 6:00AM THURSDAY
- THURSDAY 12:00N WEDNESDAY THROUGH 6:00AM FRIDAY
- THURSDAY (THANKSGIVING ONLY)
- 6:00AM WEDNESDAY THROUGH 6:00AM MONDAY
- FRIDAY 12:00N THURSDAY THROUGH 6:00AM MONDAY
- SATURDAY 12:00N FRIDAY THROUGH 6:00AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

ITEM 614, MAINTAINING TRAFFIC (LANES CLOSURE/REDUCTION REQUIRED).

THE LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK

IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

LANE CLOSURE AND LANE RESTRICTIONS NOT SPECIFIED IN THE PLANS SHALL ONLY BE IMPLEMENTED DURING THE TIME PERIODS LISTED ON THE OHIO DEPARTMENT OF TRANSPORTATION'S PERMITTED LANE CLOSURE WEB SITE WHICH IS LOCATED AT:

<http://plcm.dot.state.oh.us/>

THE PERMITTED CLOSURE TIMES LISTED ON THE WEBSITE, 14 CALENDAR DAYS PRIOR TO THE BID LETTING DATE, SHALL BE IN EFFECT FOR THIS PROJECT.

NO WORK WITHIN ACTIVE TRAVEL LANES OR WHICH WILL SLOW TRAFFIC IS PERMITTED AT ANY OTHER TIMES.

NOTIFICATION OF RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTICE TO D7 PIO TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO D7 PERMITS AND PIO
RAMP & ROAD CLOSURES	>= 2 WKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WKS	2 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NONGATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY, AS REQUIRED BY THE MANUFACTURER.

ITEM 614, FLAGGING OPERATION

A STATIONARY FLAGGING OPERATION SHALL BE PERMITTED AT THE DAR-121-12.80 AND DAR-121-18.71 LOCATIONS IF DEEMED NECESSARY BY THE ENGINEER FOR THE STAGING OF TRUCKS DELIVERING CONCRETE MATERIAL OR OTHER SUPPLIES TO THE PROJECT.

ITEM 614, MAINTAINING TRAFFIC

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

COORDINATION OF WORK:

THE CONTRACTOR IS ADVISED THAT ADJACENT CONSTRUCTION PROJECT AUG-75-7.54, PID 97794, MAY BE WITHIN OR NEAR THE WORK LIMITS OF THIS PLAN, AND MAY IMPACT THE PROJECT SCHEDULE. SEQUENCE OF CONSTRUCTION AND OR TRAFFIC CONTROL BETWEEN THE ADJACENT ZONES. THE CONTRACTOR IS REQUIRED TO COORDINATE ALL MAINTENANCE OF TRAFFIC OPERATIONS WITH ADJACENT CONSTRUCTION PROJECT. CO-OPERATION WITH THE ENGINEER, INSPECTORS, AND ALL OTHER CONTRACTORS ON OR ADJACENT TO THE PROJECT IS REQUIRED PER CMS 105.08.

DESIGN AGENCY



DISTRICT 7 ENGINEERING

DESIGNER

REB

REVIEWER

NKH 08-10-23

PROJECT ID

110172

SUBSET TOTAL

1 2

SHEET TOTAL

P.05 35

INTERIM COMPLETION DATE

ALL WORK LOCATIONS MUST BE COMPLETED DURING THE 2024 CONSTRUCTION SEASON EXCEPT FOR AUG-75-0674 INLET & LINER WORK.

THE INTERIM COMPLETION DATE SHALL BE 9/2/24

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

[INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND PERMANENT CONCRETE BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE UNDER EITHER OF THE FOLLOWING CONDITIONS: ALONG TAPERS AND TRANSITION AREAS; OR ALONG CURVES (OUTSIDE ONLY) WITH DEGREE OF CURVATURE GREATER THAN OR EQUAL TO 3 DEGREES.]

[THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.]

[DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT-101.70.]

[TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT-101.70.]

THE ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY: (SHEET 9)

ITEM 614, BARRIER REFLECTOR, TYPE 1 (ONE-WAY)

ITEM 614, OBJECT MARKER, ONE-WAY

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

[ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.]

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:

- ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
- AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
- AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

- THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
- THE ACTIVE WORK AREA LATERALLY CLOSEST TO THE OPEN TRAVELED LANE; OR
- OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 40 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

DESIGN AGENCY



DISTRICT 7 ENGINEERING

DESIGNER
REB

REVIEWER
NKH 08-10-23

PROJECT ID
110172


SUBSET	TOTAL
2	2

SHEET	TOTAL
P.06	35

SHEET NUM.							PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
6	14	15	18	20	23	24	01/S<2/04	02/IMS/04	03/STR/04						
													ROADWAY		
	LS						LS	LS	LS	201	11000	LS	CLEARING AND GRUBBING		
	1,308						736	572		SPECIAL	20270120	1,308	FT	PIPE CLEANOUT, 27" TO 48"	4
	252						132		120	SPECIAL	20270130	252	FT	PIPE CLEANOUT OVER 48"	4
	52						8	44		209	98300	52	SY	LINEAR GRADING, MISC.:RESHAPE DITCH AREAS TO PROVIDE POSITIVE DRAINAGE	4
														EROSION CONTROL	
	7,248						7,248			659	00510	7,248	SY	SEEDING AND MULCHING, CLASS 2	
	363						363			659	14000	363	SY	REPAIR SEEDING AND MULCHING	
	363						363			659	15000	363	SY	INTER-SEEDING	
	2						2			659	20000	2	TON	COMMERCIAL FERTILIZER	
	2						2			659	31000	2	ACRE	LIME	
	40						40			659	35000	40	MGAL	WATER	
							1,000	5,000	1,000	832	30000	7,000	EACH	EROSION CONTROL	
														STRUCTURE REPAIR (AUG-75-0674)	
		2						2		202	20011	2	EACH	HEADWALL REMOVED, AS PER PLAN	4
		6						6		202	35100	6	FT	PIPE REMOVED, 24" AND UNDER	
		20						20		202	35200	20	FT	PIPE REMOVED, OVER 24"	
		LS						LS		503	11100	LS		COFFERDAMS AND EXCAVATION BRACING	
		15						15		601	32100	15	CY	ROCK CHANNEL PROTECTION, TYPE B WITH FILTER	
		4						4		602	20000	4	CY	CONCRETE MASONRY, HEADWALLS	
		6						6		611	05900	6	FT	15" CONDUIT, TYPE B, 706.01	
		20						20		611	20900	20	FT	48" CONDUIT, TYPE B, 707.12	
		376						376		833	10000	376	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT, 48"	
		14						14		833	10001	14	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT, AS PER PLAN, EXPOSED AREAS OF EXISTING HEADWALLS, AND ENDS OF PIPES	4
														STRUCTURE REPAIR (DAR-121-1280)	
		LS						LS		503	11100	LS		COFFERDAMS AND EXCAVATION BRACING	
		17.8						17.8		SPECIAL	51911710	17.8	SY	PATCHING CONCRETE STRUCTURE, TYPE B	4
		9						9		601	32104	9	CY	ROCK CHANNEL PROTECTION, TYPE B WITH GEOTEXTILE FABRIC	
		52						52		833	10000	52	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT, 72"	
		84						84		833	10001	84	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT, AS PER PLAN, 72"	4
														STRUCTURE REPAIR (DAR-121-1871)	
		LS					LS			503	11100	LS		COFFERDAMS AND EXCAVATION BRACING	
		132					132			833	10000	132	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT, 60"	
		10.5					10.5			833	10001	10.5	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT, AS PER PLAN, 60"	4
														STRUCTURE REPAIR (DAR-127-1367)	
					1		1			202	20010	1	EACH	HEADWALL REMOVED	
	64						64			202	35200	64	FT	PIPE REMOVED, OVER 24"	
	312						312			SPECIAL	20270000	312	FT	FILL AND PLUG EXISTING CONDUIT, TWIN 42"	4
					60		60			203	10000	60	CY	EXCAVATION	
					60		60			203	20000	60	CY	EMBANKMENT	
					LS		LS			503	11100	LS		COFFERDAMS AND EXCAVATION BRACING	
					16		16			601	32104	16	CY	ROCK CHANNEL PROTECTION, TYPE B WITH GEOTEXTILE FABRIC	
					5		5			602	20000	5	CY	CONCRETE MASONRY, (HEADWALLS)	
					176		176			611	96600	176	FT	CONDUIT, BORED OR JACKED, TYPE A, 66"	

GENERAL SUMMARY

DESIGN AGENCY



DISTRICT 7 ENGINEERING

DESIGNER
REB

REVIEWER
CWW 08/09/23


PROJECT ID
110172

SUBSET	TOTAL
1	2

SHEET	TOTAL
P.12	35


SHEET NUM.					PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
6	14	23	24	25	01/S<2/04	02/IMS/04	03/STR/04						
												STRUCTURE REPAIR (DAR-127-1490)	
			1		1			202	20010	1	EACH	HEADWALL REMOVED	
			17		17			202	32800	17	SY	CONCRETE SLOPE PROTECTION REMOVED	
	12				12			202	35200	12	FT	PIPE REMOVED, OVER 24"	
			84		84			202	98300	84	SY	REMOVAL MISC.; CONCRETE GUTTER REMOVED	3
			LS		LS			503	11100	LS		COFFERDAMS AND EXCAVATION BRACING	
			81.2		81.2			601	11001	81.2	SY	RIPRAP, TYPE D, AS PER PLAN	4
			17		17			601	21000	17	SY	CONCRETE SLOPE PROTECTION	
			2.75		2.75			602	20000	2.75	CY	CONCRETE MASONRY, (HEADWALLS)	
			12		12			611	52900	12	FT	34" X 53" CONDUIT, TYPE A, 706.04, 706.04	
			20		20			833	10001	20	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ROUND CONDUIT, AS PER PLAN, 34"x53"	4
			424		424			833	11000	424	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ELLIPTICAL CONDUIT, 34"x53"	
												STRUCTURE REPAIR (SHE-75-1224)	
				LS	LS			503	11100	LS		COFFERDAMS AND EXCAVATION BRACING	
				12.5	12.5			601	32104	12.5	CY	ROCK CHANNEL PROTECTION, TYPE B WITH GEOTEXTILE FABRIC	
				1	1			611	98635	1	EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN	3
				190	190			833	11000	190	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ELLIPTICAL CONDUIT, 38"x60"	
				10	10			833	11001	10	FT	CONDUIT RENEWAL USING SPRAY APPLIED STRUCTURAL LINER, ELLIPTICAL CONDUIT, AS PER PLAN, 38"x60"	4
												MAINTENANCE OF TRAFFIC	
40				40				614	11110	40	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
	5			5				614	12380	5	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL)	
	49			49				614	13310	49	EACH	BARRIER REFLECTOR, TYPE 1, ONE WAY	
	49			49				614	13350	49	EACH	OBJECT MARKER, ONE WAY	
	0.29			0.29				614	22200	0.29	MILE	WORK ZONE EDGE LINE, CLASS 1, 4", 740.06, TYPE 1	
	2,390			2,390				622	41100	2,390	FT	PORTABLE BARRIER, UNANCHORED	
												INCIDENTALS	
				LS	LS	LS		614	11000	LS		MAINTAINING TRAFFIC	
				LS	LS	LS		624	10000	LS		MOBILIZATION	

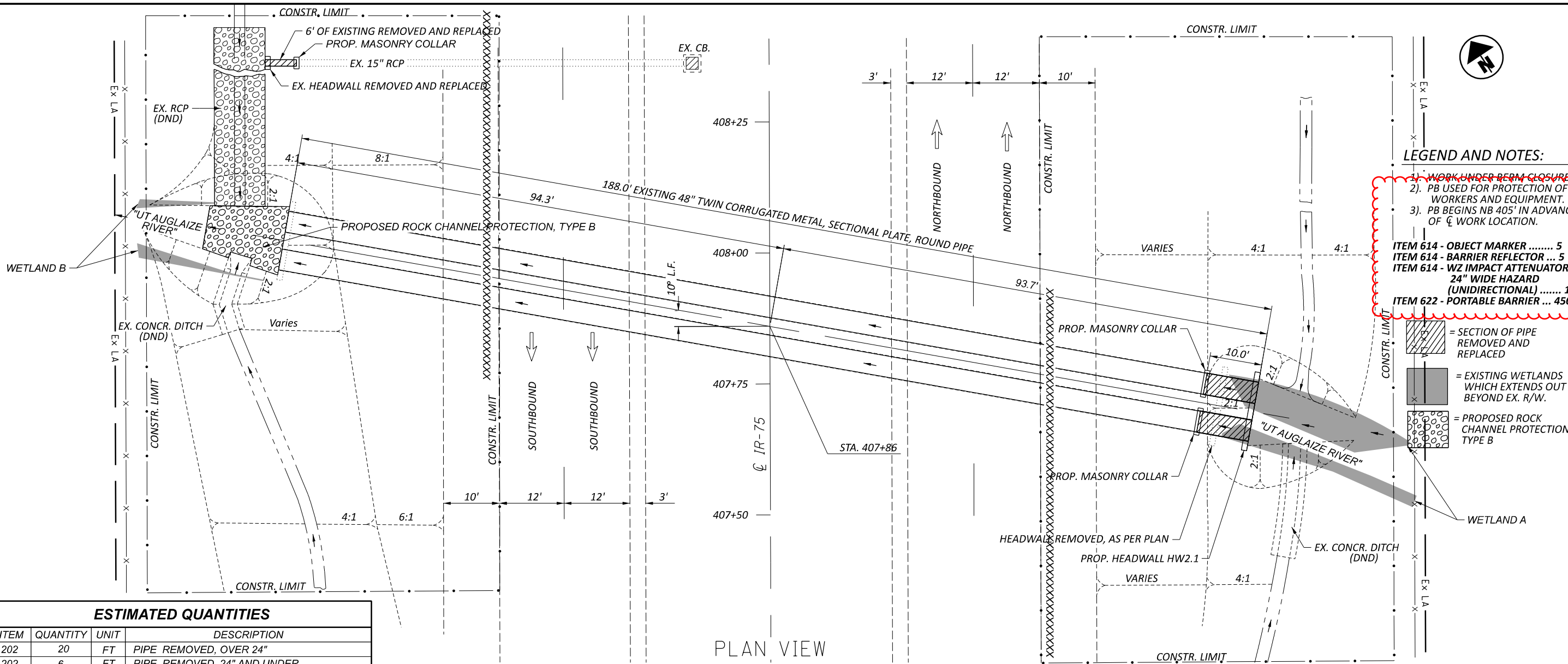
GENERAL SUMMARY

DESIGN AGENCY

DISTRICT 7 ENGINEERING
 DESIGNER
REB
 REVIEWER
CWW 08/09/23
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110172
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 2 2
 SHEET TOTAL
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SHEET NO.	LOCATION	201	202	SPECIAL	SPECIAL	SPECIAL		209		614	614	614	614	622	659	659	659	659	659	659						
		CLEARING AND GRUBBING LS	PIPE REMOVED, OVER 24" FT	PIPE CLEANOUT, 27" TO 48" FT	PIPE CLEANOUT OVER 48" FT	FILL AND PLUG EXISTING CONDUIT FT		LINEAR GRADING, MISC.: RESHAPE DITCH AREAS TO PROVIDE POSITIVE DRAINAGE SY		WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL) EACH	WORK ZONE EDGE LINE, CLASS I, 4", 740.06, TYPE I MILE	BARRIER REFLECTOR, TYPE 1, ONE WAY EACH	OBJECT MARKER, ONE WAY EACH	PORTABLE BARRIER, UNANCHORED FT	SEEDING AND MULCHING, CLASS 2 SY	REPAIR SEEDING AND MULCHING SY	INTER-SEEDING SY	COMMERCIAL FERTILIZER TON	LIME ACRE	WATER MGAL						
	AUG-75-6.74																									
3		LS																								
4				376											112	5.6	5.6	0.02	0.02	0.62						
15										1	5	5	450													
	DAR-121-12.80																									
3		LS																								
4					120										508	25	25	0.07	0.11	2.8						
	DAR-121-18.71																									
3		LS																								
4					132										172	8.6	8.6	0.02	0.04	0.95						
	DAR-127-13.67																									
3		LS																								
4																										
9																										
21			64	312		312				2	0.29	20	20	880	2864	143	143	0.4	0.59	15.9						
	DAR-127-14.90																									
3		LS																								
4																										
24			12	424				8							1560	78	78	0.22	0.32	8.6						
	SHE-75-12.24																									
3		LS																								
4																										
11																										
25				195.3				44		2	24	24	1060	2032	102	102	0.28	0.42	11							
	TOTALS CARRIED TO GENERAL SUMMARY	LS	76	1308	252	312		52		5	0.29	49	49	2390	7248	363	363	2	2	40						

ESTIMATED QUANTITIES

DESIGN AGENCY

 DISTRICT 7
 ENGINEERING
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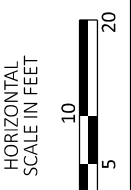


PLAN VIEW

LEGEND AND NOTES:

- 1. WORK UNDER BERM CLOSURE
 - 2. PB USED FOR PROTECTION OF WORKERS AND EQUIPMENT.
 - 3. PB BEGINS NB 405' IN ADVANCE OF CL WORK LOCATION.
- ITEM 614 - OBJECT MARKER 5
 ITEM 614 - BARRIER REFLECTOR ... 5
 ITEM 614 - WZ IMPACT ATTENUATOR, 24" WIDE HAZARD (UNIDIRECTIONAL) 1
 ITEM 622 - PORTABLE BARRIER ... 450'

- [Symbol] = SECTION OF PIPE REMOVED AND REPLACED
- [Symbol] = EXISTING WETLANDS WHICH EXTENDS OUT BEYOND EX. R/W.
- [Symbol] = PROPOSED ROCK CHANNEL PROTECTION, TYPE B



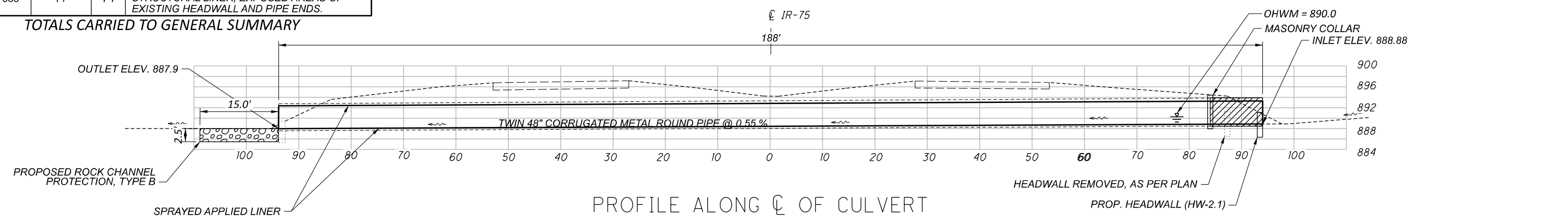
ESTIMATED QUANTITIES			
ITEM	QUANTITY	UNIT	DESCRIPTION
202	20	FT	PIPE REMOVED, OVER 24"
202	6	FT	PIPE REMOVED, 24" AND UNDER
202	2	EA	HEADWALL REMOVED, AS PER PLAN
503	LUMP		COFFERDAMS AND EXCAVATION BRACING
601	15	CY.	ROCK CHANNEL PROTECTION, TYPE B WITH FILTER
602	4.0	CY.	CONCRETE MASONRY (HEADWALL)
611	20	FT	48" CONDUIT, TYPE A, 707.03
611	6	FT	15" CONDUIT, TYPE B, 706.02
833	376	FT	CONDUIT RENEWAL USING SPRAYED APPLIED STRUCTURAL LINER, ROUND CONDUIT, 48"
833	14	FT	CONDUIT RENEWAL USING SPRAYED APPLIED STRUCTURAL LINER, EXPOSED AREAS OF EXISTING HEADWALL AND PIPE ENDS.

TOTALS CARRIED TO GENERAL SUMMARY

HYDRAULIC DATA			
DRAINAGE AREA = 588.8 ACRES			
Q (50) = 238 CFS	V (50) = 10.56 FT/S	HW (50) = 895.56	
Q (100) = 280 CFS	V (100) = 11.10 FT/S	HW (100) = 897.56	
ORDINARY HIGH WATER MARK: 890.63			
DESIGN SERVICE LIFE: 75 YEARS			
ABRASION LEVEL: 1			
pH: 7.6			

EXISTING STRUCTURE	
TYPE:	TWIN 48" CORRUGATED METAL ROUND PIPE
SIZE:	48" x 188'
SKEW:	10° L.F.
ALIGNMENT:	Tangent
CFN:	1860219

PROPOSED STRUCTURE	
TYPE:	TWIN 48" CORRUGATED METAL ROUND PIPE
SKEW:	10° L.F.
ALIGNMENT:	Tangent
CFN:	1860219



PROFILE ALONG CL OF CULVERT

CULVERT DETAILS - AUG-75-6.74
 CFN: 1860219 OVER TRIBUTARY OF AUGLAIZE RIVER

DESIGN AGENCY

 DISTRICT 7 ENGINEERING
 DESIGNER
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 PROJECT ID
 110172
 SUBSET TOTAL
 1 2
 SHEET TOTAL
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