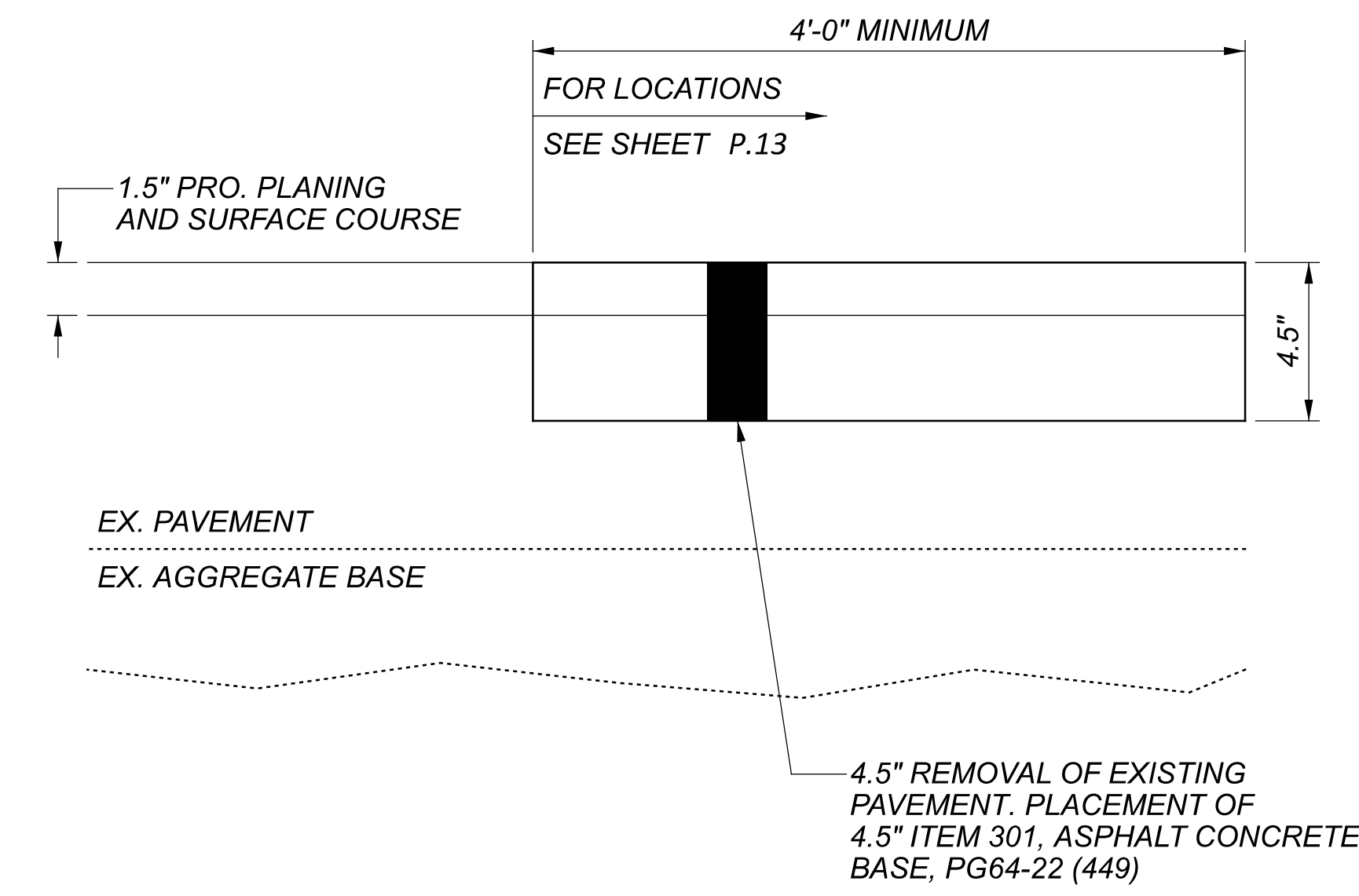


**TYPICAL DETAIL #1**

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 3.0"**

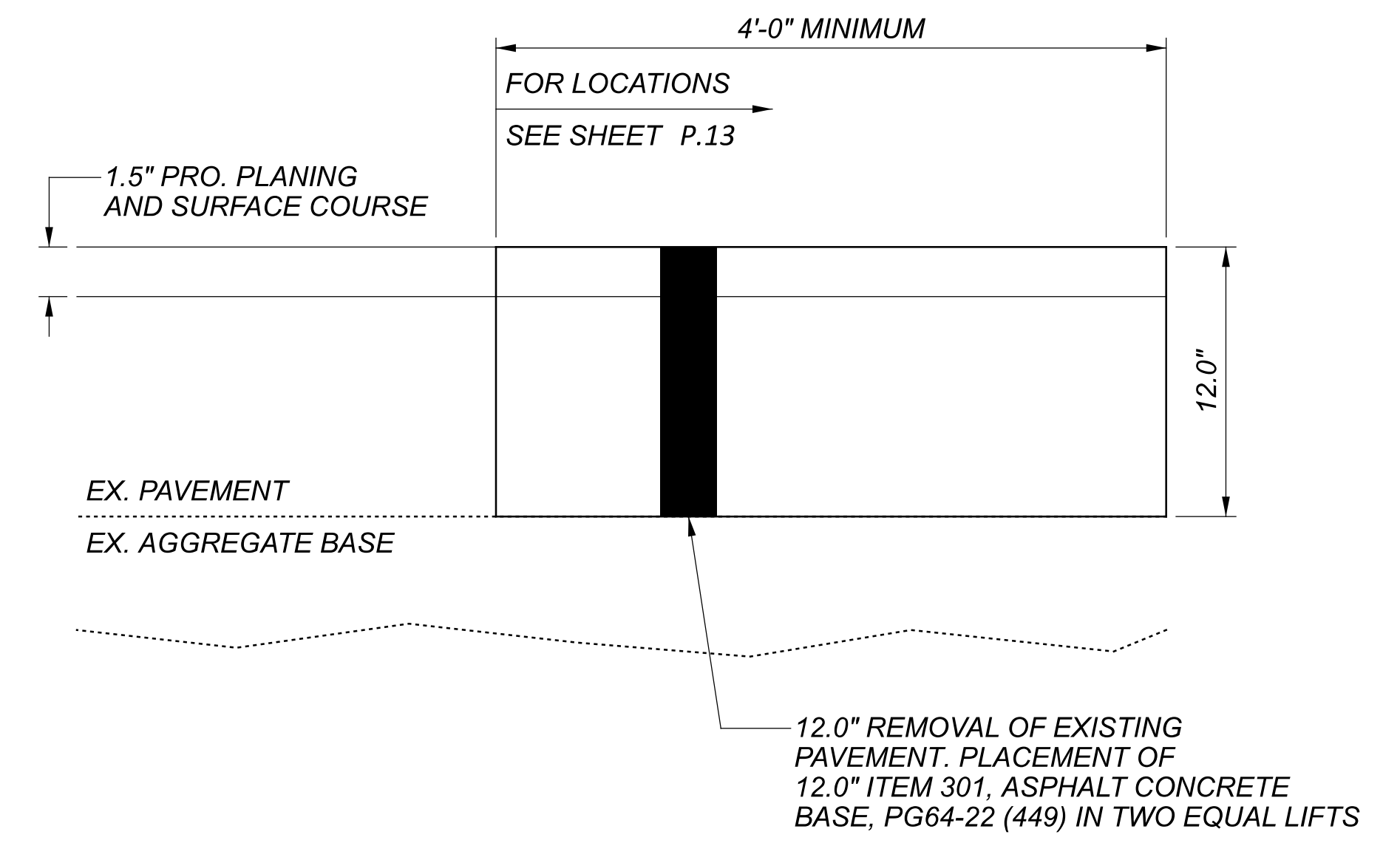
FOR MORE INFORMATION REGARDING ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 3.0" SEE GENERAL NOTES



**TYPICAL DETAIL #2**

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 4.5"**

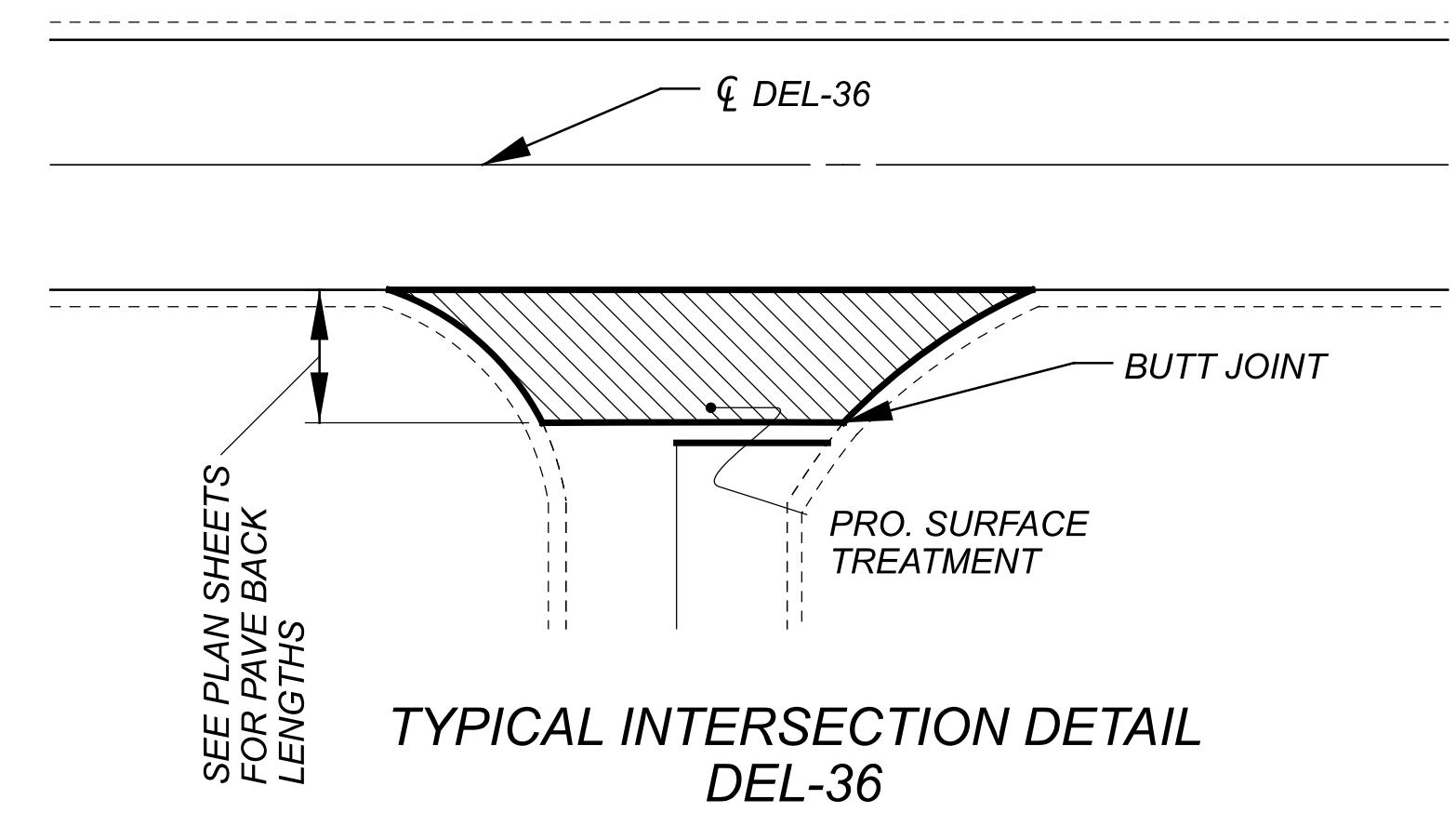
FOR MORE INFORMATION REGARDING ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 4.5" SEE GENERAL NOTES



**TYPICAL DETAIL #3**

**ITEM 253 - PAVEMENT REPAIR, AS PER PLAN, 12.0"**

FOR MORE INFORMATION REGARDING ITEM 253 - PAVEMENT REPAIR, AS PER PLAN, 12.0" SEE GENERAL NOTES



1.5" PAVEMENT PLANING AND RESURFACING (449)

ROUTE	SLM	TO	SLM	BUILD-UP BASED ON EXISTING PLANS*
US 36	7.28	-	7.46	1" ASPHALT ON MACADAM
US 36	7.46	-	7.70	1.25" ASPHALT ON MACADAM
US 36	7.70	-	8.02	3" ASPHALT ON 7" ASPHALT BASE
US 36	8.02	-	8.59	4.5" ASPHALT ON 7" BASE
US 36	8.59	-	9.03	3" ASPHALT ON 7" ASPHALT BASE
US 36	9.03	-	9.06	4.25" ASPHALT ON 8" MACADAM
US 36*	9.06	-	9.96	2.25" ASPHALT ON 12" BRICK
US 36*	9.96	-	10.18	1" ASPHALT ON 12" BRICK
US 36	10.18	-	10.40	4.25" ASPHALT ON 6" CONCRETE
US 36	10.40	-	10.60	3" ASPHALT ON 6" CONCRETE

BUILD-UPS ARE PROVIDED FOR REFERENCE ONLY AND MAY NOT REFLECT ACTUAL FIELD CONDITIONS.

\*TROLLEY RAILS MAY BE PRESENT UNDER ASPHALT PAVEMENT. DO NOT DISTURB.

NOTES:  
 1. MATCH EXISTING CROSS SLOPE  
 2. TYPICAL DETAILS ARE NOT TO SCALE

DESIGN AGENCY



DESIGNER  
 KLM

REVIEWER

XXX MM-DD-YY

PROJECT ID  
 111608

SHEET TOTAL  
 P.4 P.38



**GENERAL:**  
THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 108.02) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

**ALIGNMENT AND PROFILE:**  
THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED, AND THE PROFILE OF THE PROPOSED SURFACE WILL BE THE SAME AS EXISTING.

**CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:**  
THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

**CONTINGENCY QUANTITIES:**  
THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

**REMOVAL ITEMS:**  
ASPHALT AND MISCELLANEOUS HARDWARE DESIGNATED FOR REMOVAL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE REMOVED ITEM.

**WORK LIMITS:**  
THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**PART-WIDTH / FULL-WIDTH CONSTRUCTION:**  
BECAUSE OF THE NECESSITY TO BUILD PORTIONS OF THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXERCISE CARE TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN THE BASE COURSES. LAP LONGITUDINAL JOINTS AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

**CENTERLINE CONSTRUCTION / RIGHT OF WAY:**  
THE INTENT OF THIS PROJECT IS THAT ALL WORK IS TO BE COMPLETED WITHIN THE EXISTING RIGHT OF WAY. THE CENTERLINE SHOWN IN THIS PLAN IS TO BE CONSIDERED A CENTERLINE OF CONSTRUCTION ONLY AND NOT TO BE CONSTRUED AS THE ACTUAL GEOMETRIC ALIGNMENT OF THE ROADWAY. THIS CENTERLINE PROVIDED IS TO BE USED AS A REFERENCE OF PROJECT LENGTH ONLY AND SHALL NOT BE USED TO ESTABLISH PRECISE LOCATIONS OF ANY OTHER FEATURES SUCH AS/NOT LIMITED TO THE EXISTING RIGHTS OF WAY. ANY RIGHT OF WAY LOCATION SHOWN IN THE PLAN IS A GRAPHICAL REPRESENTATION (OF SAID RIGHT OF WAY) CONFIRMING THAT THE PLANNED WORK HAS BEEN DETERMINED TO BE IN ODOT RIGHT OF WAY. IN THE EVENT THAT ANY ACTIVITIES DEVIATE FROM THE PLAN, THE CONTRACTOR MAY BE REQUIRED, PER THE ENGINEER, TO VERIFY THE RIGHT OF WAY LIMITS IN THE FIELD. PAYMENT FOR ANY RIGHT OF WAY VERIFICATION WILL BE INCLUDED UNDER THE LUMP SUM BID ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN.

**UTILITIES:**  
NO UTILITY IMPACT IS ANTICIPATED DUE TO THE SCOPE OF WORK. THE ODOT CONTRACTOR IS REQUIRED TO CONTACT OHIO811 A MINIMUM OF 48 HOURS EXCLUDING WEEKENDS AND HOLIDAYS TO PERMIT ALL UNDERGROUND UTILITIES AN OPPORTUNITY TO MARK THEIR LINES AND TO ENSURE ALL UTILITIES ARE MARKED PRIOR TO BEGINNING WORK. IT IS ALSO THE ODOT CONTRACTOR'S RESPONSIBILITY TO CONTACT ALL NON-MEMBERS OF OHIO811 DIRECTLY A MINIMUM OF 48 HOURS NOTICE EXCLUDING WEEKENDS AND HOLIDAYS PRIOR TO EXCAVATION OCCURRING AT ANY LOCATIONS TO PROVIDE THEM WITH THE SAME OPPORTUNITY.

**MANHOLES AND OTHER CASTING STRUCTURES:**  
THE ITEM PROVIDED BELOW IS A CONTINGENCY QUANTITY TO BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS. THE ITEM SHALL INCLUDE THE COST OF ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO ADJUST CASTINGS TO GRADE TO THE PROPOSED ASPHALT ELEVATION AS DIRECTED.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND CARRIED TO THE GENERAL SUMMARY:

- ITEM 611 - MANHOLE ADJUSTED TO GRADE = 4 EACH
- ITEM 638 - VALVE BOX ADJUSTED TO GRADE = 4 EACH

**SEEDING AND MULCHING:**  
THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS AND HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

- ITEM 659 - TOPSOIL = 289 CY
- ITEM 659 - SEEDING AND MULCHING = 2644 SY
- ITEM 659 - REPAIR SEEDING AND MULCHING = 132 SY
- ITEM 659 - INTER-SEEDING = 132 SY
- ITEM 659 - COMMERCIAL FERTILIZER = 0.36 TON
- ITEM 659 - LIME = 0.55 ACRE
- ITEM 659 - WATER = 14 MGAL

APPLY SEEDING AND MULCHING TO ALL AREAS OF EXPOSED SOIL WITHIN THE LINEAR GRADING WORK. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THE AVERAGE 2' LINEAR GRADING WIDTH.

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 3.0" :**  
ALL REPAIR AREAS DETAILED IN THE PLAN SHALL BE VERIFIED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. THE REPAIR AREAS SHALL CONSIST OF REMOVING 3.0 INCHES OF PAVEMENT AND PLACING 3.0 INCHES OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, (449). THE AVERAGE WIDTH SHALL NOT BE LESS THAN 4 FEET. FOR MORE INFORMATION SEE DETAIL ON SHEET P.4. WORK SHALL BE PERFORMED PRIOR TO PLANING. NO MORE PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY.

FOR LOCATIONS AND QUANTITIES, SEE SHEET P.13.

IN ADDITION TO THE QUANTITIES PROVIDED ON SHEET P.13, THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED CARRIED TO THE GENERAL SUMMARY:

- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 3.0" = 17 SY

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 4.5" :**  
ALL REPAIR AREAS DETAILED IN THE PLAN SHALL BE VERIFIED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. THE REPAIR AREAS SHALL CONSIST OF REMOVING 4.5 INCHES OF PAVEMENT AND PLACING 4.5 INCHES OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, (449). EXISTING PAVEMENT BUILDUPS ARE SHOWN ON SHEET P.4 FOR REFERENCE. IF BRICK, CONCRETE, OR TROLLEY RAILS ARE ENCOUNTERED, STOP THE DEPTH OF REPAIR AT TOP OF BRICK, CONCRETE, OR RAILS. DO NOT DISLodge ANY BRICK OR REMOVE CONCRETE OR RAILS. THE AVERAGE WIDTH SHALL NOT BE LESS THAN 4 FEET. FOR MORE INFORMATION SEE DETAIL ON SHEET P.4. WORK SHALL BE PERFORMED PRIOR TO PLANING. NO MORE PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY.

FOR LOCATIONS AND QUANTITIES, SEE SHEET P.13.

IN ADDITION TO THE QUANTITIES PROVIDED ON SHEET P.13, THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED CARRIED TO THE GENERAL SUMMARY:

- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 4.5" = 125 SY

**ITEM 253 - PAVEMENT REPAIR, AS PER PLAN, 12.0" :**  
ALL REPAIR AREAS DETAILED IN THE PLAN SHALL BE VERIFIED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. THE REPAIR AREAS SHALL CONSIST OF REMOVING 12.0 INCHES OF PAVEMENT AND PLACING 12.0 INCHES OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, (449). THE AVERAGE WIDTH SHALL NOT BE LESS THAN 4 FEET. FOR MORE INFORMATION SEE DETAIL ON SHEET P.4. WORK SHALL BE PERFORMED PRIOR TO PLANING. NO MORE PAVEMENT REPAIR SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY.

FOR LOCATIONS AND QUANTITIES, SEE SHEET P.13.

IN ADDITION TO THE QUANTITIES PROVIDED ON SHEET P.13, THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED CARRIED TO THE GENERAL SUMMARY:

- ITEM 253 - PAVEMENT REPAIR, AS PER PLAN, 12.0" = 24 SY

**ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE:**  
THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE TO THE CONTRACTORS EQUIPMENT THAT MAY RESULT FROM THE PLANING OPERATION, INCLUDING DAMAGE CAUSED BY CASTINGS AND LOOP DETECTORS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED; TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT. GREAT CARE SHALL BE TAKEN TO PREVENT THE REMOVAL OF THE EXISTING PAVEMENT CROSS-SLOPE (CROWN) DURING THE PLANING OPERATIONS.

THE CONTRACTOR SHALL LIMIT THE PLANING OPERATION TO ONE LANE AT A TIME AS TO ENSURE THAT THE PROPOSED SURFACE COURSE IS BUTTING UP TO EITHER PROPOSED OR EXISTING ASPHALT.

PLANED PAVEMENT SHALL NOT BE EXPOSED TO TRAFFIC AT ANY TIME. FAILURE TO MEET THIS REQUIREMENT WILL SUBJECT THE CONTRACTOR TO A DISINCENTIVE OF \$900/DAY FOR EACH DAY THE PLANED SURFACE IS NOT RESURFACED.

**ITEM 617 - WATER:**  
THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND CARRIED TO THE GENERAL SUMMARY:

- ITEM 617 - WATER = 1 MGAL

**PROPOSED PAVEMENT MARKINGS:**  
IT IS THE INTENT OF THE PROPOSED PAVEMENT MARKINGS TO BE THE SAME AS EXISTING. ANY DEVIATION FROM EXISTING WILL BE IDENTIFIED WITHIN THIS PLAN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THESE EXISTING PAVEMENT MARKINGS BEFORE THE WORK OBLITERATES THEM. ANY PAVEMENT MARKING WHICH IS PLACED AT THE WRONG LOCATION SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

**ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN:**  
THIS ITEM SHALL CONSIST OF STATIONING USING 3 FT LATH STAKES. THE STAKES SHALL BE SPACED AT 200 FT INTERVALS AND SHALL EXTEND THROUGHOUT THE LENGTH OF THE PROJECT LOCATION.

PLACEMENT OF THE STAKES SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED OR MISSING STAKES.

THIS ITEM SHALL ALSO INCLUDE ANY WORK NECESSARY FOR THE CONTRACTOR TO VERIFY EXISTING RAILROAD RIGHT OF WAY, AS DIRECTED.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

