

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

FAI-204-0.00
FAI-204A-0.00

CITY OF PICKERINGTON
VIOLET AND LIBERTY TOWNSHIPS
FAIRFIELD COUNTY

PROJECT DESCRIPTION:

ASPHALT CONCRETE RESURFACING, AND RELATED WORK, ON S.R. 204 AND S.R. 204A IN FAIRFIELD COUNTY.

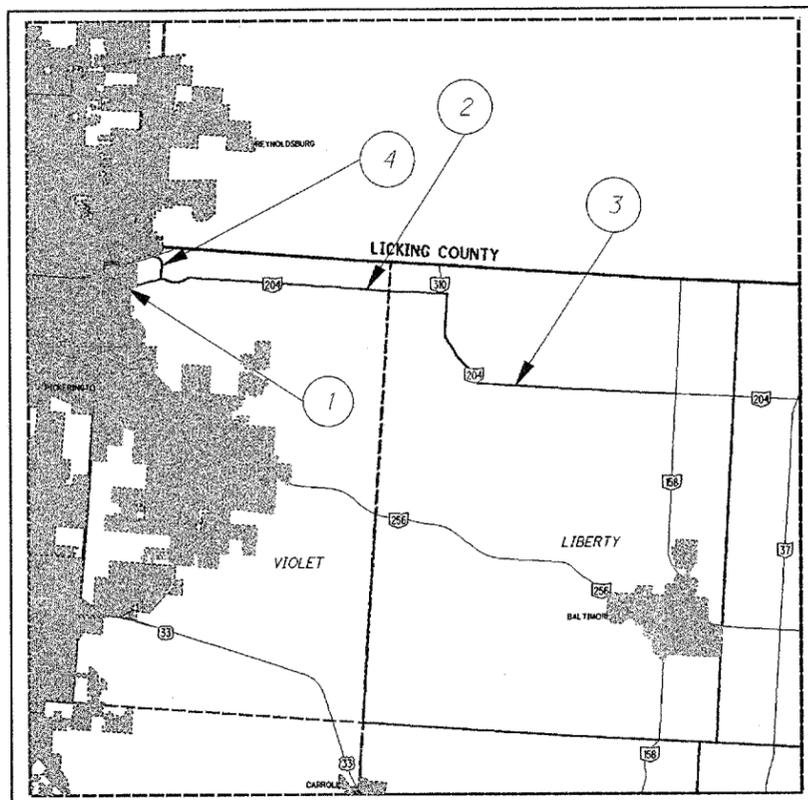
Project Earth Disturbed Area = N/A
(Maintenance Project)
Estimated Contractor Earth Disturbed Area = N/A
(Maintenance Project)
Notice of Intent Earth Disturbed Area = N/A
(Maintenance Project)

LOCATION	COUNTY	ROUTE	BEGIN SLM	END SLM	LENGTH MILES	CITY/VILLAGE
1	FAI	204	0.00	0.36	0.36	PICKERINGTON
2	FAI	204	0.36	4.37	4.01	
3	FAI	204	4.37	11.16	6.79	
4	FAI	204A	0.00	0.49	0.49	

2008 SPECIFICATIONS

THE STANDARD 2008 SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND THE PROPOSAL SHALL GOVERN THESE IMPROVEMENTS.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING OF THE HIGHWAY AND PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS INDICATED IN THE PROPOSAL.



LOCATION MAP

LON/LAT: 82° 41' 08" / 39° 55' 38"

PORTION TO BE IMPROVED

DESIGN DESIGNATION	LOCATIONS			
	1	2	3	4
Functional Classification	UMC	UMC	RMC	UMC
Opening Year ADT (2010)	15300	9900	3100	5400
Design Year ADT (2022)	18600	12100	3800	6600
Design Hourly Volume (2022)	1700	1200	400	660
Directional Distribution	50%	50%	50%	50%
Trucks (24 Hour B&C)	3%	3%	4%	2%
Design Speed	35mph	55mph	55mph	55mph
Legal Speed	35mph	45/55mph	55mph	55mph

UMC = URBAN MAJOR COLLECTOR
RMC = RURAL MAJOR COLLECTOR

DESIGN EXCEPTIONS: NONE

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES
CALL TWO WORKING DAYS
BEFORE YOU DIG
CALL
1-800-362-2764
(TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY
OIL & GAS PRODUCERS PROTECTIVE
SERVICE CALL: **1-800-925-0988**

PLAN PREPARED BY:
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT 5 PRODUCTION OFFICE

ENGINEER'S SEAL
STATE OF OHIO
DOUGLAS N. MORGAN
E-63839
REGISTERED PROFESSIONAL ENGINEER
SIGNED: *Douglas N. Morgan*
DATE: 8/11/2009

INDEX OF SHEETS:

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STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	
BP-3.1	10-19-07	TC-65.10	1-21-05	800	10-16-09
BP-4.1	7-16-04	TC-65.11	1-21-05	832	5-5-09
		TC-71.10	1-16-09	843	4-18-03
		TC-73.10	1-19-01		
MT-35.10	4-20-01	TC-82.10	4-19-02		
MT-97.10	4-17-09				
MT-97.12	4-17-09 (Corrected)				
MT-99.20	1-16-09				
MT-101.90	1-16-09				
MT-105.10	1-16-09				

APPROVED: *[Signature]*
DATE: 8/11/09 DISTRICT DEPUTY DIRECTOR
APPROVED: _____
DATE: _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT SHOULD NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

NOTIFICATION OF ROAD CLOSURE OR RESTRICTION

IN ORDER FOR ODOT TO PROPERLY PERMIT OVERSIZE LOADS, PREPARE PROPER SIGNING WHEN REQUIRED AND FURTHER TO NOTIFY THE GENERAL MOTORING PUBLIC, THE CONTRACTOR SHALL NOTIFY (IN WRITING) THE DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR WITH COPIES FOR THE DISTRICT 5 ROADWAY SERVICES MANAGER AND PROJECT ENGINEER NOT LESS THAN 21 DAYS BEFORE SUCH CLOSURE OR LANE RESTRICTIONS.

SEND NOTIFICATION TO:

DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR
P.O. BOX 306
JACKSONSTOWN, OH 43030
PHONE: (740) 323-4400 EXT. 5241

ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN

DEPTH OF PLANING SHALL BE 2.0" FULL WIDTH OF PAVEMENT IN LOCATION 1, 1.25" IN LOCATIONS 2 AND 3 AND 2.0" IN LOCATION 4 UNLESS OTHERWISE SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. THE ROADWAY SHALL BE PLANED SUCH THAT POSITIVE DRAINAGE IS CREATED FROM THE CENTER LINE TO THE EDGE OF PAVEMENT IN TANGENT SECTIONS AND SHALL FOLLOW EXISTING SUPERELEVATIONS WHERE APPLICABLE. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT THE PAVEMENT IS PLANED TO A DEPTH THAT DOES NOT LEAVE A "SLIVER" OF THIN ASPHALT (LESS THAN OR EQUAL TO ONE HALF INCH). IF SUCH "SLIVERS" ARE OBVIOUS AFTER THE PAVEMENT IS MILLED AND BROOMED, ADDITIONAL MILLING SHALL BE PERFORMED TO REMOVE SAID "SLIVERS" AT THE CONTRACTOR'S OWN EXPENSE. ALL REQUIREMENTS OF ITEM 254 SHALL APPLY.

4000 TONS OF GRINDINGS (RACP) SHALL BE DELIVERED TO THE OHIO DEPARTMENT OF TRANSPORTION-FAIRFIELD COUNTY BALTIMORE OUTPOST, 1894 WEST MARKET ST., BALTIMORE OHIO 43105. THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN.

ITEM 516 2" DEEP JOINT SEALER, AS PER PLAN

THE CONTRACTOR SHALL PLACE A 1" X 2.0" DEEP BEAD OF JOINT SEALER (AS PER 705.04) AT THE LOCATIONS SHOWN IN PLANS. THE CONTRACTOR SHALL SAW CUT A CHANNEL FOR THE JOINT SEALER. THE COST FOR SAW CUTTING THE CHANNEL FOR THE JOINT SEALER SHALL BE INCLUDED FOR PAYMENT WITH ITEM 516 2" DEEP JOINT SEALER, AS PER PLAN.

FEATHERING

FEATHERING OF THE ASPHALT CONCRETE SHALL BE DONE IN ACCORDANCE WITH SCD DRAWING BP-3.1, 10-19-07.

ITEM 617 COMPACTED AGGREGATE, AS PER PLAN

ALL AGGREGATE SHALL BE 100% CRUSHED LIMESTONE. ALL QUALITY REQUIREMENTS EXCEPT SHALE SHALL BE WAIVED. OTHER GRADATION REQUIREMENTS SHALL BE AS SPECIFIED EXCEPT THE PLASTICITY INDEX SHALL BE WAIVED. IF PERMITTED, THE CONTRACTOR MAY USE RECYCLED ASPHALT CONCRETE PAVEMENT (RACP MEETING REQUIREMENTS OF 617.02) IN LIEU OF CRUSHED LIMESTONE.

TACK COAT

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.075 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

TACK COAT FOR INTERMEDIATE COURSE

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.05 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

PAVEMENT MARKING

STOP LINES, CROSSWALK LINES, CHANNELIZING LINES, ETC., SHOWN IN THE PLANS ARE TAKEN FROM EXISTING MARKINGS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DOCUMENT EXISTING MARKING LOCATIONS (i.e. BY USE OF VIDEO, SURVEY OR PICTURES) AND PLACE NEW PAVEMENT MARKINGS AS NEAR AS POSSIBLE TO THE EXISTING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. DOCUMENTATION OF PAVEMENT MARKING SHALL BE SUPPLIED TO THE ENGINEER BEFORE COMMENCEMENT OF ANY OPERATION WHICH WILL REMOVE/OBLITERATE MARKINGS.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 35 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO SUBMIT FORM 7460-1 TO THE FAA. A COPY OF THE SUBMISSION AND TWO COPIES OF FORM 7460-1 SHALL BE FORWARDED TO THE ODOT OFFICE OF AVIATION. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

Express Processing Center
The Federal Aviation Administration
Southwest Regional Office
Air Traffic Airspace Branch ASW-520
2601 Meachan Blvd.
Fort Worth, TX 76137-4298

Ohio Department of Transportation
Office of Aviation
2829 West Dublin-Granville Road
Columbus, Ohio 43235
614-387-2346

ITEM 209 LINEAR GRADING

IN ORDER TO PROVIDE POSITIVE DRAINAGE FROM THE ROADWAY SURFACE TO THE SHOULDER BREAK, THE EXISTING ROADWAY SHOULDERS SHALL BE GRADED AND SHAPED USING A GRADER OF ADEQUATE SIZE TO PERFORM THE WORK TO THE SATISFACTION OF THE ENGINEER. ALL EXCESS MATERIAL REMAINING AROUND GUARDRAIL AND OTHER AREAS AFTER THE GRADER WORK IS COMPLETED SHALL BE DISPOSED OF ON THE SITE OR REMOVED AND DISPOSED OF BY THE CONTRACTOR.

THIS WORK MAY BE INTERMITTENT AND SPREAD THROUGHOUT THE PROJECT LIMITS, AS DIRECTED BY THE ENGINEER. ALL LINEAR GRADING WORK SHALL BE DONE BEFORE PLACING THE ASPHALT SURFACE COURSE.

ALL EQUIPMENT, LABOR, OR INCIDENTALS REQUIRED TO COMPLETE THIS ITEM SHALL BE INCLUDED FOR PAYMENT IN THE UNIT PRICE BID FOR ITEM 209 LINEAR GRADING. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE SUB-SUMMARIES FOR THE ABOVE DESCRIBED PURPOSES.

ITEM 209 LINEAR GRADING
LOCATION 2 - 2 MILE, LOCATION 3 - 4 MILE

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN

AN ESTIMATED QUANTITY FOR PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED AS DIRECTED BY THE ENGINEER TO REPAIR EXISTING PAVEMENT. ALL REPAIRS SHALL TAKE PLACE PRIOR TO THE PAVEMENT PLANING AND PAVING OPERATIONS. THE INTENT OF THIS OPERATION IS TO REPAIR THOSE AREAS OF PAVEMENT WHICH HAVE COMPLETELY FAILED (PUMPING OF SUB-BASE MATERIAL) AND NOT TO CORRECT SURFACE IRREGULARITIES. DEPTH OF EXCAVATION SHALL BE APPROXIMATELY 7". AFTER EXCAVATION HAS BEEN COMPLETED, THE FACE OF THE REPAIR SHALL BE COATED WITH 407 TACK COAT. REPLACEMENT MATERIAL WILL BE 7" OF ITEM 301 ASPHALT CONCRETE BASE, PG 64-22 (PLACED AND COMPACTED IN 2 LIFTS AS DIRECTED). ALL EXCAVATION, MATERIALS, LABOR, EQUIPMENT, TOOLS, TRAFFIC CONTROL AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 253 PAVEMENT REPAIR, AS PER PLAN.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE SUB-SUMMARIES FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 253 PAVEMENT REPAIR, AS PER PLAN

LOCATION 2 - 50 CU.YD., LOCATION 3 - 100 CU.YD.

ITEM 408 PRIME COAT, AS PER PLAN

THE CONTRACTOR SHALL APPLY ONE COAT OF MC-70 (AS PER SECTION 702) AT A RATE OF 0.40 GALLON PER SQUARE YARD TO THE COMPLETED AGGREGATE SHOULDER (ITEM 617) AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF PAVEMENT OR EDGE LINE. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS. ITEM 408 PRIME COAT, AS PER PLAN SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT TO PERFORM THE ABOVE MENTIONED WORK.

LOCATION 2 - 4038 SQ.YD. X 0.40 GAL/SQ.YD. = 1616 GALLON
LOCATION 3 - 15934 SQ.YD. X 0.40 GAL/SQ.YD. = 6374 GALLON
LOCATION 4 - 282 SQ.YD. X 0.40 GAL/SQ.YD. = 113 GALLON

ITEM 621 RAISED PAVEMENT MARKER REMOVED

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE PLANS TO REMOVE RAISED PAVEMENT MARKERS FOR DISPOSAL BY THE CONTRACTOR. RPM REMOVAL SHALL NOT OCCUR SOONER THAN 10 DAYS PRIOR TO RESURFACING OF THE ROADWAY. ALL RPM'S REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 621 RAISED PAVEMENT MARKER REMOVED
LOCATION 1 - 74 EACH, LOCATION 2 - 331 EACH, LOCATION 3 - 476 EACH
LOCATION 4 - 57 EACH

GENERAL NOTES

FAI-204-0.00
FAI-204A-0.00

RESIDENCE AND COMMERCIAL DRIVES

AN ESTIMATED QUANTITY OF ITEM 448 ASPHALT CONCRETE HAS BEEN INCLUDED IN THE PLAN TO BE USED AS DIRECTED BY THE ENGINEER TO PAVE APPROACH AREAS TO EXISTING DRIVEWAYS. PAVING SHALL TYPICALLY EXTEND 4' INTO THE DRIVEWAY (MEASURED FROM THE EDGE OF PAVEMENT OR PAVED SHOULDER IF PRESENT). THERE ARE 5 TYPES OF DRIVES: CONCRETE, ASPHALT, GRAVEL, GRAVEL WITH ASPHALT APRON, AND FIELD/OIL WELL DRIVES. FIELD DRIVES AND OIL WELL DRIVES SHALL NOT BE PAVED. GRAVEL DRIVES SHALL BE PAVED BACK 4' INTO THE DRIVEWAY UNLESS OTHERWISE DIRECTED BY THE ENGINEER. CONCRETE AND ASPHALT DRIVES SHALL HAVE BUTT JOINTS OR AS DIRECTED BY THE ENGINEER SO AS TO PROVIDE A SMOOTH TRANSITION. GRAVEL DRIVES WITH ASPHALT APRONS SHALL ALSO HAVE BUTT JOINTS BUT ONLY IF THE EXISTING ASPHALT APRON IS IN AN ACCEPTABLE CONDITION TO BE PAVED OVER AS DIRECTED BY THE ENGINEER. IF THE ASPHALT APRON CANNOT BE PAVED OVER (FOR EXAMPLE, BROKEN INTO SMALL PIECES) AS DETERMINED BY THE ENGINEER, IT SHALL BE REMOVED BEFORE BEING PAVED BACK 4' INTO THE DRIVEWAY. ALL GRADING, PRIME OR TACK COAT, MATERIALS, LABOR, EQUIPMENT TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE DRIVES SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEMS LISTED BELOW.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE SUB-SUMMARIES FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 448 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG 64-22
LOCATION 2 - 23 CU. YD., LOCATION 3 - 24 CU.YD.

ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 70-22M
LOCATION 2 - 39 CU. YD., LOCATION 3 - 40 CU. YD.

ITEM 202 WEARING COURSE REMOVED
LOCATION 2 - 1100 SQ. YD., LOCATION 3 - 1130 SQ. YD.

MAIL BOX TURN OUTS

A QUANTITY OF ASPHALT CONCRETE HAS BEEN PROVIDED IN THE PLAN TO COVER MAIL BOX TURN OUTS. TURN OUTS SHALL BE PAVED AS SHOWN IN THE DETAIL IN DRAWING BP-4.1. ANY EXTRA GRADING OF THE SHOULDERS, PRIME OR TACK COAT, MATERIALS, LABOR, EQUIPMENT TOOLS AND INCIDENTALS NECESSARY TO COMPLETE MAIL BOX TURN OUTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEMS LISTED BELOW. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE SUB-SUMMARIES FOR THE ABOVE PURPOSES.

ITEM 448 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG 64-22
LOCATION 2 - 20 CU.YD., LOCATION 3 - 20 CU.YD.

ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 70-22M
LOCATION 2 - 33 CU.YD., LOCATION 3 - 33 CU.YD.

ITEM 202 WEARING COURSE REMOVED
LOCATION 2 - 930 SQ.YD., LOCATION 3 - 930 SQ.YD.

**ITEM 604 MANHOLE ADJUSTED TO GRADE
ITEM 638 VALVE BOX ADJUSTED TO GRADE**

THESE ITEMS SHALL BE USED TO ADJUST CATCH BASINS, MANHOLES AND WATER VALVE BOXES LOCATED THROUGHOUT THE PROJECT LIMITS AS DIRECTED BY THE ENGINEER. ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK DESCRIBED SHALL BE INCLUDED FOR PAYMENT WITH THE ITEMS LISTED BELOW.

ANY GAS VALVE BOXES AND TELEPHONE COMPANY MANHOLES ON THIS PROJECT SHALL BE ADJUSTED TO GRADE BY THE RESPECTIVE OWNERS.

ITEM 604 MANHOLE ADJUSTED TO GRADE
LOCATION 1 - 4 EACH, LOCATION 3 - 1 EACH

ITEM 638 VALVE BOX ADJUSTED TO GRADE
LOCATION 2 - 5 EACH

DETECTOR LOOP, AS PER PLAN

ALL DETECTOR LOOPS SHALL BE CUT INTO THE PLANED SURFACE OR THE PROPOSED INTERMEDIATE COURSE AT A DEPTH OF 4" FROM THE PROPOSED SURFACE ELEVATION. IF THE CONTRACTOR SO CHOOSES, THEY MAY CUT THE DETECTOR LOOPS INTO THE EXISTING ASPHALT BEFORE PLANING BUT SHALL MAKE SURE THE MATERIAL USED TO FILL THE SAW CUT IS LEFT FAR ENOUGH BELOW THE SURFACE COURSE THAT IT WILL NOT BE DISTURBED DURING THE PLANING OPERATION. THE CONTRACTOR SHALL TEST ALL LEAD-IN CABLES PRIOR TO MAKING THE FINAL SPLICE. PLACEMENT SHALL BE AS PER SPECIFICATION 632.10. FINAL LOCATIONS, SIZE AND ORIENTATION SHALL BE PROVIDED TO THE CONTRACTOR AT THE PRE-CONSTRUCTION MEETING. ALL MATERIALS, LABOR, TOOLS, EQUIPMENT, TRAFFIC CONTROL AND INCIDENTALS NECESSARY TO PERFORM THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 632 DETECTOR LOOP, AS PER PLAN.

LOCATION 1: PICKERINGTON - 17 EACH
S.R. 204 AT S.R. 256 - 7 EACH
S.R. 204 AND BUSINESS DRIVE - 9 EACH
S.R. 204 AND YARMOUTH DR. - 1 EACH

LOCATION 2: 18 EACH
S.R. 204 AT TAYLOR RD. - 6 EACH
S.R. 204 AND HARMON RD. - 8 EACH
S.R. 204 AND MILNOR RD. - 4 EACH

LOCATION 4: 4 EACH
TAYLOR RD. AT S.R. 204 - 4 EACH

ITEM 614 MAINTAINING TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING AND PROPOSED PAVEMENT AS PER STANDARD DRAWINGS MT-97.10 AND MT-97.12.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

TRAFFIC SHALL NOT BE MAINTAINED ON A PLANED SURFACE. ALL PLANED SURFACES SHALL BE OVERLAYED WITH AT LEAST ASPHALT CONCRETE INTERMEDIATE COURSE BEFORE OPENING TO TRAFFIC.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 MAINTAINING TRAFFIC (NIGHT WORK)

IN ORDER TO MINIMIZE THE IMPACTS TO THE TRAVELING PUBLIC, THE FOLLOWING PROPOSED WORK SHALL BE COMPLETED DURING NIGHT TIME HOURS BEGINNING AT 10:00 P.M. AND ENDING AT 6:00 A.M.: ALL OF THE PROPOSED WORK ON S.R. 204 BEGINNING AT THE INTERSECTION OF S.R. 256 (SLM 0.00) AND ENDING JUST EAST OF THE INTERSECTION OF HARMON ROAD (SLM 1.49); AND ALL OF THE PROPOSED WORK ON S.R. 204A.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS NECESSARY TO MAINTAIN TRAFFIC AT NIGHT SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 WORK ZONE MARKING SIGNS

A QUANTITY OF WORK ZONE MARKING SIGNS HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

WORK ZONE MARKING SIGNS	LOCATIONS		
	2	3	4
W8-HI2a (NO EDGE LINES)	4	6	2
R4-1 (DO NOT PASS)	7	26	
R4-2 (PASS WITH CARE)	4	22	
TOTAL	15	54	2

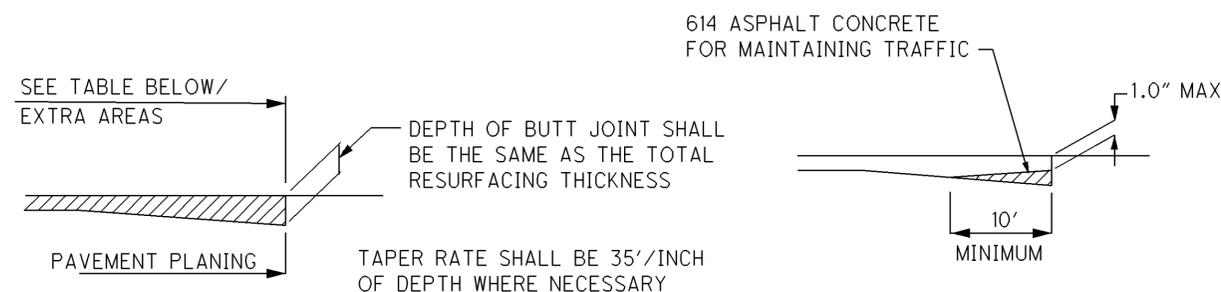
ITEM 614 WORK ZONE PAVEMENT MARKING

THE FOLLOWING QUANTITY OF ITEM 614 WORK ZONE CENTER LINE, CLASS II HAS BEEN CARRIED TO THE GENERAL SUMMARY AND SHALL BE PLACED AS PER THE SPECIFICATIONS FOR ITEM 614.

ITEM 614 WORK ZONE CENTER LINE, CLASS II
LOCATION 1 - 0.80 MILE, LOCATION 2 - 8.74 MILE,
LOCATION 3 - 13.58 MILE, LOCATION 4 - 0.28 MILE

BUTT JOINT

A BUTT JOINT WILL BE REQUIRED AT LOCATIONS SPECIFIED BELOW AND AT EXTRA AREAS WITH WEARING COURSE REMOVED. AFTER THE JOINT IS CONSTRUCTED, THE DROP OFF CREATED SHALL BE MINIMIZED BY IMMEDIATELY PLACING THE PROPOSED INTERMEDIATE COURSE TO WITHIN 1.0" OF EXISTING ROADWAY SURFACE OR BY PLACING A WEDGE AS SHOWN BELOW USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC. BUTT JOINTS SHALL BE AS PER SCD BP-3.1, 10-19-07 UNLESS OTHERWISE SHOWN IN THE PLANS.



LOCATION	ROUTE	DESCRIPTION	SLM	202 WEARING COURSE REMOVED	614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC
				SQ. YD.	CU.YD.
1	S.R. 204	BEGIN WORK	0.00	*	2.2
2	S.R. 204	FAI-204-0386	3.46	*	2.2
2	S.R. 204	FAI-204-0432	4.32	*	2.2
2	S.R. 204	TOTALS			4.4
3	S.R. 204	END WORK	11.16	*	1.1
4	S.R. 204A	BEGIN WORK	0.00	*	0.8
4	S.R. 204A	END WORK	0.49	*	1.4
4	S.R. 204A	TOTALS			2.2

* INCLUDED FOR PAYMENT WITH PAVEMENT PLANING

ITEM 614 AMERICAN RECOVERY AND REINVESTMENT ACT (ARRA) SIGN, CONVENTIONAL ROAD

THIS ITEM SHALL CONSIST OF THE FURNISHING, INSTALLING, MAINTAINING AND SUBSEQUENT REMOVAL, OF ONE AMERICAN RECOVERY AND REINVESTMENT ACT (ARRA) SIGN ON YIELDING POST SUPPORTS.

ON THE FIRST DAY OF WORK, INSTALL ONE ARRA SIGN NEAR THE BEGINNING OF THE PROJECT IN EACH ROUTE DIRECTION IN A LOCATION AS APPROVED BY THE ENGINEER. THE ARRA SIGN CONSISTS OF ONE 84" X 60" WHITE ON GREEN EXTRUSHEET SIGN WITH PICTOGRAPHS, ONE 84" X 18" BLACK ON ORANGE EXTRUSHEET SIGN, AND ONE 18" X 18" DIAMOND FLAT SHEET SIGN. THE SIGN FABRICATION DETAILS ARE FOUND ON THE OFFICE OF TRAFFIC ENGINEERING WEB PAGE AT:

[HTTP://WWW.DOT.STATE.OH.US/DIVISIONS/HIGHWAYOPS/TRAFFIC/PAGES/OTEHOME PAGE.ASPX](http://www.dot.state.oh.us/divisions/highwayops/traffic/pages/otehomepage.aspx)

INSTALL THE SIGN ON TWO NO. 3 YIELDING POSTS AS PER STANDARD DRAWING TC-41.20, WITH ONE POST 12" FROM EACH END. SIGNS IN PROTECTED LOCATIONS MAY BE INSTALLED ON OTHER SUPPORTS AS APPROVED BY THE ENGINEER. USED SIGNS ARE ALLOWED PROVIDED THEY ARE IN A CONDITION ACCEPTABLE TO THE ENGINEER. REMOVE THE ARRA SIGN AND SUPPORTS AT THE END OF THE PROJECT. REMOVED ARRA SIGN AND SUPPORTS ARE THE PROPERTY OF THE CONTRACTOR.

THE BASIS OF PAYMENT SHALL BE AT THE CONTRACT UNIT BID PRICE PER EACH FOR ONE INSTALLED AND SUBSEQUENTLY REMOVED ARRA SIGN, WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, TOOLS AND OTHER INCIDENTALS TO PROVIDE FOR A COMPLETE AND ACCEPTED ITEM OF WORK.

LOCATION 1 1 EACH
LOCATION 3 1 EACH
LOCATION 4 1 EACH

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

IN ADDITION TO THE REQUIREMENT OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP). IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.
- WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.
- DURING ANY WORK THAT IS REQUIRED TO BE PERFORMED DURING NIGHT TIME HOURS.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR (cont'd)

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING.

SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR

LOCATION 1	20 HOURS
LOCATION 2	40 HOURS
LOCATION 4	40 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

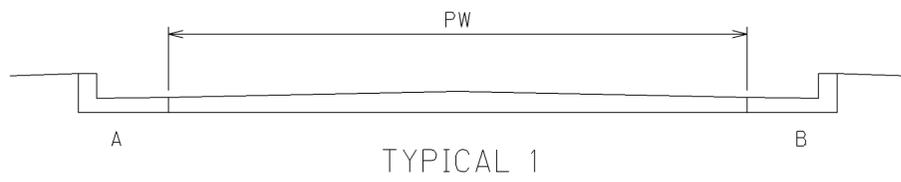
ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

PAVEMENT DATA																		
LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	LENGTH		PAVEMENT WIDTH (FEET)	TYPICAL	EXISTING PAVEMENT TYPE	PAVEMENT AREA	254		407		448 ASPHALT CONCRETE			
					MILES	LIN. FT.					PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN	TACK COAT @ 0.075 GAL./S.Y.	TACK COAT FOR INTERMEDIATE COURSE @ 0.05 GAL./S.Y.	THICKNESS	INTERMEDIATE COURSE, TYPE 1, PG 64-22	THICKNESS	SURFACE COURSE, TYPE 1, PG 70-22M	
																		SQ. YD.
1	FAI	S.R. 204	0.00	0.09	0.09	475.20	72.0	1	448	3,801.6	3,801.6	285.2	190.1	1.00	105.6	1.00	105.6	
1	FAI	S.R. 204	0.09	0.36	0.27	1,425.60	58.0 (AVG.)	1	448	9,187.2	9,187.2	689.1	459.4	1.00	255.2	1.00	255.2	
LOCATION 1 (TOTALS CARRIED TO SUB-SUMMARY)											12,989	974	650		360.8		360.8	
2	FAI	S.R. 204	0.36	0.40	0.04	211.20	55.0 (AVG.)	1	448	1,290.7	1,290.7	96.9	64.6	1.00	35.9	1.00	35.9	
2	FAI	S.R. 204	0.40	0.75	0.35	1,848.00	20.0	2	448	4,106.7	4,106.7	308.1	205.4	1.00	114.1	1.00	114.1	
2	FAI	S.R. 204	0.75	1.24	0.49	2,587.20	24.0	3	448	6,899.2	6,899.2	517.5	345.0	1.00	191.7	1.00	191.7	
2	FAI	S.R. 204	1.24	1.29	0.05	264.00	30.0 (AVG.)	3	448	880.0	880.0	66.0	44.0	1.00	24.5	1.00	24.5	
2	FAI	S.R. 204	1.29	1.40	0.11	580.80	36.0	3	448	2,323.2	2,323.2	174.3	116.2	1.00	64.6	1.00	64.6	
2	FAI	S.R. 204	1.40	1.49	0.09	475.20	30.0 (AVG.)	3	448	1,584.0	1,584.0	118.8	79.2	1.00	44.0	1.00	44.0	
2	FAI	S.R. 204	1.49	1.70	0.21	1,108.80	20.0	2	448	2,464.0	2,464.0	184.8	123.2	1.00	68.5	1.00	68.5	
2	FAI	S.R. 204	1.70	1.75	0.05	264.00	30.0 (AVG.)	3	448	880.0	880.0	66.0	44.0	1.00	24.5	1.00	24.5	
2	FAI	S.R. 204	1.75	1.80	0.05	264.00	36.0	3	448	1,056.0	1,056.0	79.2	52.8	1.00	29.4	1.00	29.4	
2	FAI	S.R. 204	1.80	1.85	0.05	264.00	30.0 (AVG.)	3	448	880.0	880.0	66.0	44.0	1.00	24.5	1.00	24.5	
2	FAI	S.R. 204	1.85	3.90	2.05	10,824.00	20.0	2	448	24,053.3	24,053.3	1,804.0	1,202.7	1.00	668.2	1.00	668.2	
2	FAI	S.R. 204	3.90	3.96	0.06	316.80	30.0 (AVG.)	3	448	1,056.0	1,056.0	79.2	52.8	1.00	29.4	1.00	29.4	
2	FAI	S.R. 204	3.96	4.00	0.04	211.20	36.0	3	448	844.8	844.8	63.4	42.3	1.00	23.5	1.00	23.5	
2	FAI	S.R. 204	4.00	4.10	0.10	528.00	30.0 (AVG.)	3	448	1,760.0	1,760.0	132.0	88.0	1.00	48.9	1.00	48.9	
2	FAI	S.R. 204	4.10	4.37	0.27	1,425.60	20.0	2	448	3,168.0	3,168.0	237.6	158.4	1.00	88.0	1.00	88.0	
BRIDGE DEDUCTIONS (FROM SHEET 11)										(635.6)	(635.6)	(47.7)	(31.8)	1.00	(17.7)	1.00	(17.7)	
LOCATION 2 (TOTALS CARRIED TO SUB-SUMMARY)											52,610	3,946	2,631		1,462.0		1,462.0	
3	FAI	S.R. 204	4.37	11.16	6.79	35,851.20	20.0	2	448	79,669.3	79,669.3	5,975.2	3,983.5	1.00	2,213.1	1.00	2,213.1	
BRIDGE DEDUCTIONS (FROM SHEET 11)										(486.7)	(486.7)	(36.6)	(24.4)	1.00	(13.6)	1.00	(13.6)	
LOCATION 3 (TOTALS CARRIED TO SUB-SUMMARY)											79,183	5,939	3,959		2,199.5		2,199.5	
4	FAI	S.R. 204A RAMP	0.00	0.24	0.24	1,267.20	16.0	6	448	2,252.8	2,252.8	169.0	112.7	1.00	62.6	1.00	62.6	
4	FAI	S.R. 204A S.B.	0.24	0.35	0.11	580.80	12.0	4	448	774.4	774.4	58.1	38.8	1.00	21.6	1.00	21.6	
4	FAI	S.R. 204A N.B.	0.24	0.35	0.11	580.80	12.0	4	448	774.4	774.4	58.1	38.8	1.00	21.6	1.00	21.6	
4	FAI	S.R. 204A	0.35	0.49	0.14	739.20	36.0	5	448	2,956.8	2,956.8	221.8	147.9	1.00	82.2	1.00	82.2	
LOCATION 4 (TOTALS CARRIED TO SUB-SUMMARY)											6,758	507	338		188.0		188.0	

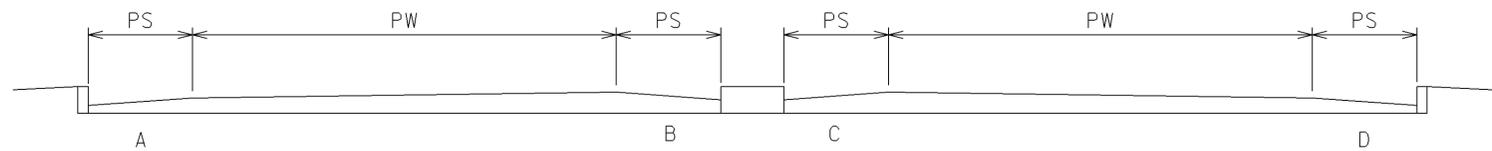
ASPHALT CONCRETE DATA

FAI-204-0.00
FAI-204A-0.00

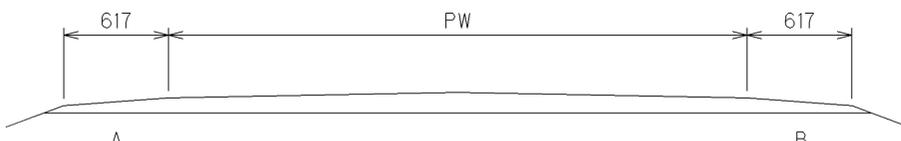
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TYPICAL 1

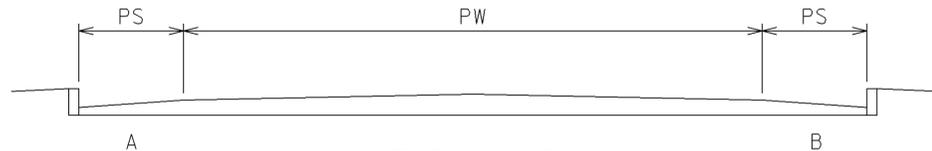


TYPICAL 4

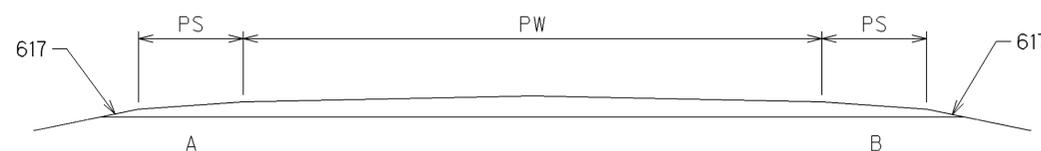


TYPICAL 2

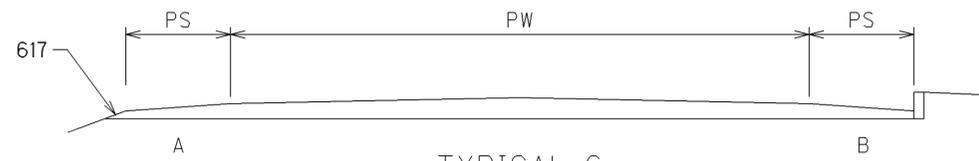
PW = PAVEMENT WIDTH
PS = PAVED SHOULDER
617 = AGGREGATE SHOULDER



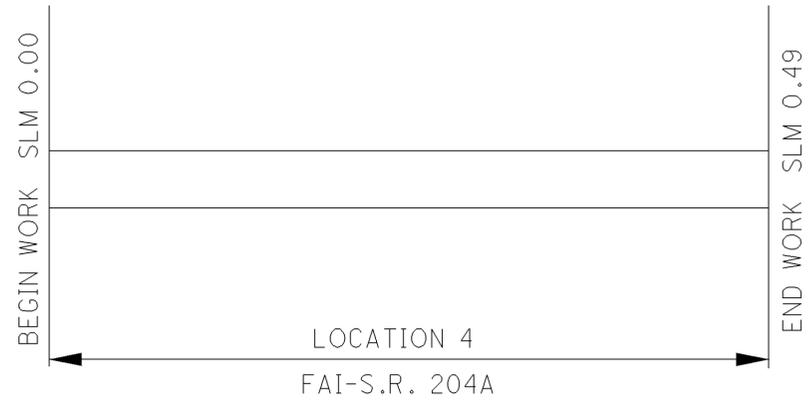
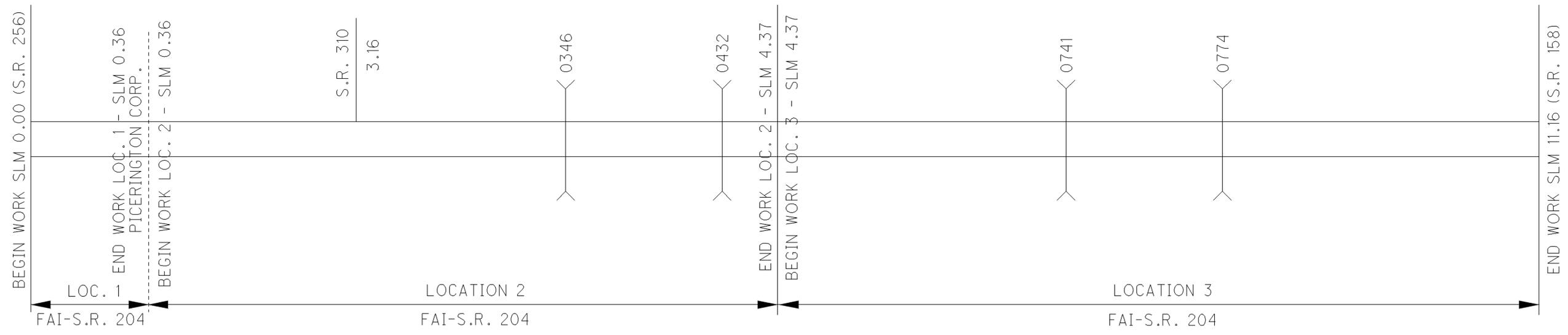
TYPICAL 5



TYPICAL 3



TYPICAL 6



ASPHALT CONCRETE DATA

FAI-204-0.00
FAI-204A-0.00

SHOULDER DATA

LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	LENGTH		TYPICAL	PROPOSED WIDTH (FT.)				SHOULDER AREA	254		407		448 ASPHALT CONCRETE				THICKNESS	COMPACTED AGGREGATE, AS PER PLAN	
								MILES	LIN. FT.	A	B		C	D	SQ. YD.	SQ. YD.	GALLON	GALLON	INCHES	CU. YD.			INCHES
					SQ. YD.	SQ. YD.																	
2	FAI	S.R. 204	0.40	0.75	0.35	1,848.0	2	2	2			821.3									2.00	45.6	
2	FAI	S.R. 204	0.75	1.49	0.74	3,907.2	3	4	4			3,473.1	3,473.1	260.5	173.7	1.00	96.5	1.00	96.5		2.00	96.5	
2	FAI	S.R. 204	1.49	1.70	0.21	1,108.8	2	2	2			492.8									2.00	27.4	
2	FAI	S.R. 204	1.70	1.85	0.15	792.0	3	2	2			352.0	352.0	26.4	17.6	1.00	9.8	1.00	9.8		2.00	19.6	
2	FAI	S.R. 204	1.85	3.90	2.05	10,824.0	2	2	2			4,810.7									2.00	267.3	
2	FAI	S.R. 204	3.90	4.10	0.20	1,056.0	3	2	0			234.7	234.7	17.6	11.7	1.00	6.5	1.00	6.5		2.00	26.1	
2	FAI	S.R. 204	4.10	4.37	0.27	1,425.6	2	2	2			633.6									2.00	35.2	
BRIDGE DEDUCTIONS (FROM SHEET 11)												(127.2)	(127.2)	(9.5)	(6.4)	1.00	(3.5)	1.00	(3.5)		2.00	(8.1)	
LOCATION 2 (TOTALS CARRIED TO SUB-SUMMARY)													3,933	295	197		109.3		109.3			509.6	
3	FAI	S.R. 204	4.37	11.16	6.79	35,851.2	2	2	2			15,933.9									2.00	885.2	
BRIDGE DEDUCTIONS (FROM SHEET 11)												(97.3)										2.00	(8.1)
LOCATION 3 (TOTALS CARRIED TO SUB-SUMMARY)																							877.1
4	FAI	S.R. 204A	0.00	0.24	0.24	1,267.2	6	3	6			1,267.2	1,267.2	95.0	63.4	1.00	35.2	1.00	35.2		2.00	15.6	
4	FAI	S.R. 204A	0.24	0.35	0.11	580.8	4	4	4	4	4	1,032.5	1,032.5	77.4	51.6	1.00	28.7	1.00	28.7				
4	FAI	S.R. 204A	0.35	0.49	0.14	739.2	5	4	4			657.1	657.1	49.3	32.9	1.00	18.3	1.00	18.3				
LOCATION 4 (TOTALS CARRIED TO SUB-SUMMARY)													2,957	222	148		82.2		82.2				15.6

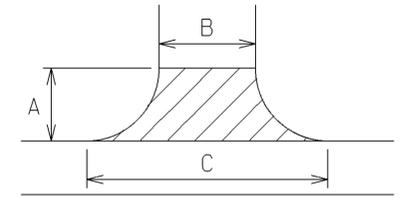
SEE SHEET 8 FOR TYPICALS AND STRAIGHT LINE DIAGRAM

SHOULDER TREATMENT DATA

FAI-204-0.00
FAI-204A-0.00

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EXTRA AREAS															
LOCATION	COUNTY	ROUTE	SIDE	DESCRIPTION	INTERSECTIONS			AREA SQ. YD.	202	407		448 ASPHALT CONCRETE			
					WEARING COURSE REMOVED SQ. YD.	TACK COAT @ 0.075 GAL./SQ. YD. GAL.	TACK COAT FOR INTERMEDIATE COURSE @ 0.05 GAL./SQ. YD. GAL.		THICKNESS INCHES	INTERMEDIATE COURSE, TYPE 1, PG 64-22 CU. YD.	THICKNESS INCHES	SURFACE COURSE, TYPE 1, PG 70-22M CU. YD.			
													DETAIL DIMENSION		
													A FT.	B FT.	C FT.
1	FAI	S.R. 204	LT	YARMOUTH DR.	35	23	75	190.6	190.6	14.3	9.6	0.75	4.0	1.25	6.7
LOCATION 1 (TOTALS CARRIED TO SUB-SUMMARY)								191	15	10		4.0		6.7	
2	FAI	S.R. 204	RT	SUMMERFIELD WAY	35	34	78	217.8	217.8	16.4	10.9	0.75	4.6	1.25	7.6
2	FAI	S.R. 204	LT	CAPETOWN AVE.	30	35	93	213.4	213.4	16.1	10.7	0.75	4.5	1.25	7.5
2	FAI	S.R. 204	LT	ECHO MANOR	34	21	58	149.3	149.3	11.2	7.5	0.75	3.2	1.25	5.2
2	FAI	S.R. 204	RT	RUSTIC DR.	25	38	86	172.3	172.3	13.0	8.7	0.75	3.6	1.25	6.0
2	FAI	S.R. 204	RT	WELLESLEY DR.	25	32	66	136.2	136.2	10.3	6.9	0.75	2.9	1.25	4.8
2	FAI	S.R. 204	RT	WATERTON DR.	25	25	59	116.7	116.7	8.8	5.9	0.75	2.5	1.25	4.1
2	FAI	S.R. 204	RT	HARMON RD.	50	36	120	433.4	433.4	32.6	21.7	0.75	9.1	1.25	15.1
2	FAI	S.R. 204	LT	HAFF FARM DR.	53	34	168	594.8	594.8	44.7	29.8	0.75	12.4	1.25	20.7
2	FAI	S.R. 204	LT	DAYSPRINGS WAY	25	36	116	211.2	211.2	15.9	10.6	0.75	4.4	1.25	7.4
2	FAI	S.R. 204	RT	ASHLEY CREEK DR.	25	44	85	179.2	179.2	13.5	9.0	0.75	3.8	1.25	6.3
2	FAI	S.R. 204	LT	CARRIAGE LN.	40	33	100	295.6	295.6	22.2	14.8	0.75	6.2	1.25	10.3
2	FAI	S.R. 204	RT	CHESINGTON DR.	24	26	72	130.7	130.7	9.9	6.6	0.75	2.8	1.25	4.6
2	FAI	S.R. 204	RT	HARDWICK CT.	30	16	68	140.0	140.0	10.5	7.0	0.75	3.0	1.25	4.9
2	FAI	S.R. 204	RT	CHEVINGTON DR.	35	20	75	184.8	184.8	13.9	9.3	0.75	3.9	1.25	6.5
2	FAI	S.R. 204	LT	EASTWAY DR.	40	20	80	222.3	222.3	16.7	11.2	0.75	4.7	1.25	7.8
2	FAI	S.R. 204	LT	NEW ENGLAND ACRES	20	60	117	196.7	196.7	14.8	9.9	0.75	4.1	1.25	6.9
2	FAI	S.R. 204	RT	MILNOR RD. N.W.	30	26	84	183.4	183.4	13.8	9.2	0.75	3.9	1.25	6.4
2	FAI	S.R. 204	LT	NANTUCKET AVE.	38	30	84	240.7	240.7	18.1	12.1	0.75	5.1	1.25	8.4
2	FAI	S.R. 204	LT	SUDBURY DR.	26	22	74	138.7	138.7	10.5	7.0	0.75	2.9	1.25	4.9
2	FAI	S.R. 204	LT	WAGRAM RD.	30	20	60	133.4	133.4	10.1	6.7	0.75	2.8	1.25	4.7
2	FAI	S.R. 204	RT	AULT RD.	22	18	52	85.6	85.6	6.5	4.3	0.75	1.8	1.25	3.0
2	FAI	S.R. 204	RT	CALHOUN CT.	12	40	91	87.4	87.4	6.6	4.4	0.75	1.9	1.25	3.1
2	FAI	S.R. 204	LT	VIOLET MEADOWS BLVD.	31	76	137	366.9	366.9	27.6	18.4	0.75	7.7	1.25	12.8
2	FAI	S.R. 204	RT	PICKERINGTON RD. N.W.	40	18	90	240.0	240.0	18.0	12.0	0.75	5.0	1.25	8.4
2	FAI	S.R. 204	LT	OPTIMARA DR.	20	50	82	146.7	146.7	11.1	7.4	0.75	3.1	1.25	5.1
2	FAI	S.R. 204	RT	TOLLGATE RD.	20	32	68	111.2	111.2	8.4	5.6	0.75	2.4	1.25	3.9
2	FAI	S.R. 204	RT	TOLLGATE RD.	25	18	53	98.7	98.7	7.5	5.0	0.75	2.1	1.25	3.5
LOCATION 2 (TOTALS CARRIED TO SUB-SUMMARY)								5,428	409	273		114.4		189.9	
3	FAI	S.R. 204	LT	SAYLOR RD.	25	21	55	105.6	105.6	8.0	5.3	0.75	2.2	1.25	3.7
3	FAI	S.R. 204	LT	S.R. 310	42	32	95	296.4	296.4	22.3	14.9	0.75	6.2	1.25	10.3
3	FAI	S.R. 204	LT	BLACKLICK RD.	50	19	75	261.2	261.2	19.6	13.1	0.75	5.5	1.25	9.1
3	FAI	S.R. 204	RT	MAUGER RD.	35	18	65	161.4	161.4	12.2	8.1	0.75	3.4	1.25	5.7
3	FAI	S.R. 204	RT	REFUGEE RD.	40	29	57	191.2	191.2	14.4	9.6	0.75	4.0	1.25	6.7
3	FAI	S.R. 204	RT	POPLAR CREEK RD.	39	27	104	283.9	283.9	21.3	14.2	0.75	6.0	1.25	9.9
3	FAI	S.R. 204	LT	HEIMBERGER RD.	22	18	50	83.2	83.2	6.3	4.2	0.75	1.8	1.25	2.9
3	FAI	S.R. 204	RT	HEIMBERGER RD.	25	22	58	111.2	111.2	8.4	5.6	0.75	2.4	1.25	3.9
3	FAI	S.R. 204	LT	SNYDER CHURCH RD. N.W.	25	22	84	147.3	147.3	11.1	7.4	0.75	3.1	1.25	5.2
3	FAI	S.R. 204	RT	SNYDER CHURCH RD. N.W.	35	22	85	208.1	208.1	15.7	10.5	0.75	4.4	1.25	7.3
3	FAI	S.R. 204	RT	BASIL RD.	25	20	70	125.0	125.0	9.4	6.3	0.75	2.7	1.25	4.4
3	FAI	S.R. 204	LT	BASIL RD.	33	19	80	181.5	181.5	13.7	9.1	0.75	3.8	1.25	6.4
3	FAI	S.R. 204	CL	@ S.R. 158	33	22	72	172.4	172.4	13.0	8.7	0.75	3.6	1.25	6.0
LOCATION 3 (TOTALS CARRIED TO SUB-SUMMARY)								2,329	176	117		49.1		81.5	



TYPICAL INTERSECTION

$$AREA = A \frac{(B+C)}{2} / 9$$

CALCULATED
LME
CHECKED
DNM

EXTRA AREA DATA

FAI-204-0.00
FAI-204A-0.00

BRIDGE TREATMENT

LOCATION 2

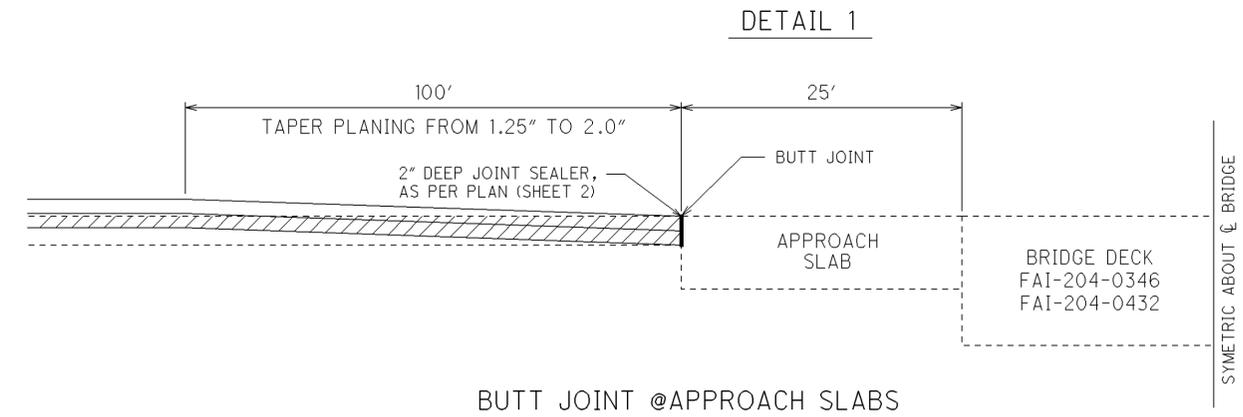
FAI-204-0346: BUTT JOINT @ APPROACH SLABS.
 FAI-204-0432: BUTT JOINT @ APPROACH SLABS.

LOCATION 3

FAI-204-0741: REMOVE ASPHALT, DRIP STRIP AND WATERPROOFING.
 PLACE TYPE 3 WATERPROOFING, DRIP STRIP AND 2.5" ASPHALT CONCRETE.

FAI-204-0774: REMOVE ASPHALT, DRIP STRIP AND WATERPROOFING.
 PLACE TYPE 3 WATERPROOFING, DRIP STRIP AND 2.5" ASPHALT CONCRETE.

THE AVERAGE DEPTH OF ITEM 448 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22 IS A RESULT OF THE INCREASED ASPHALT THICKNESS AT THE ABUTMENTS DUE TO THE CAMBER IN THE CONCRETE BOX BEAMS. SEE THE BRIDGE DECK DETAILS ON SHEETS 12 AND 13 FOR EXISTING ASPHALT CONCRETE THICKNESS.



BUTT JOINT @ APPROACH SLABS

BRIDGE DATA

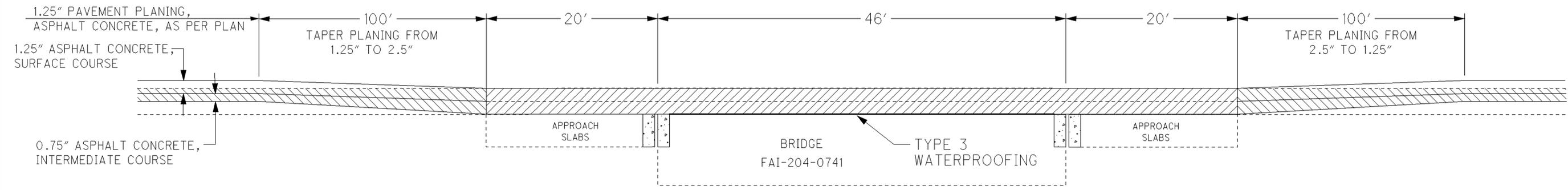
NO	COUNT	COUNT	COUNT	COUNT	COUNT	COUNT	COUNT	COUNT	COUNT	COUNT	COUNT	448 ASPHALT CONCRETE				COUNT	
												THICKNESS	INTERMEDIATE COURSE, TYPE 1, PG 64-22	THICKNESS	SURFACE COURSE, TYPE 1, PG 70-22M		
	COUNTY, ROUTE, BRIDGE NO.	LENGTH (BRIDGE LIMITS)	WIDTH (BRIDGE WIDTH)	BRIDGE DECK AREA	APPROACH SLAB LENGTH	APPROACH SLAB WIDTH	APPROACH SLAB AREA (INCLUDES BOTH APPROACH SLABS)	DETAIL (SEE SHEETS 12 & 13 FOR DETAILS 2 & 3)	MAINLINE DEDUCTIONS (CARRIED TO SHEET 7)	SHOULDER DEDUCTIONS (CARRIED TO SHEET 9)	WEARING COURSE REMOVED	TACK COAT FOR INTERMEDIATE COURSE @ 0.05 GAL./S.Y.	INCHES	CU. YD.	INCHES	CU. YD.	2" DEEP JOINT SEALER, AS PER PLAN
		LIN. FT.		SQ. YD.	LIN. FT.	LIN. FT.	SQ. YD.		SQ. YD.	SQ. YD.	SQ. YD.	GAL.					FEET
2	FAI-204-0346	93	27.5	284.2	25	27.5	152.8	1	317.8	63.6							55
2	FAI-204-0432	93	27.5	284.2	25	27.5	152.8	1	317.8	63.6							55
SUB-TOTALS									635.6	127.2							
LOCATION 2 (TOTALS CARRIED TO SUB-SUMMARY)																	110
3	FAI-204-0741	46	34.0	173.8	20	34	151.2	2	191.1	38.2	325.0	9	1.5 (AVG.)	7.2	1.25	6.0	
3	FAI-204-0774	93	40.0	413.4	20	40	177.8	3	295.6	59.1	591.2	21	1.5 (AVG.)	17.2	1.25	14.4	
SUB-TOTALS									486.7	97.3	916.2	30		24.4		20.4	
LOCATION 3 (TOTALS CARRIED TO SUB-SUMMARY)																	
											916.2	30		24.4		20.4	

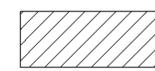
CALCULATED
LME
CHECKED
DNM

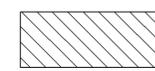
BRIDGE TREATMENT DATA

FAI-204-0.00
FAI-204A-0.00

DESCRIPTION OF WORK:
WEARING COURSE REMOVED SHALL BE AS SHOWN ON DETAIL BELOW,
REMOVE ASPHALT, DRIP STRIP AND WATERPROOFING COMPLETELY.
PLACE STEEL DRIP STRIP, TYPE 3 WATERPROOFING AND RESURFACE
WITH 1.25" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG 64-22
AND 1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 70-22M



 202 WEARING COURSE
REMOVED

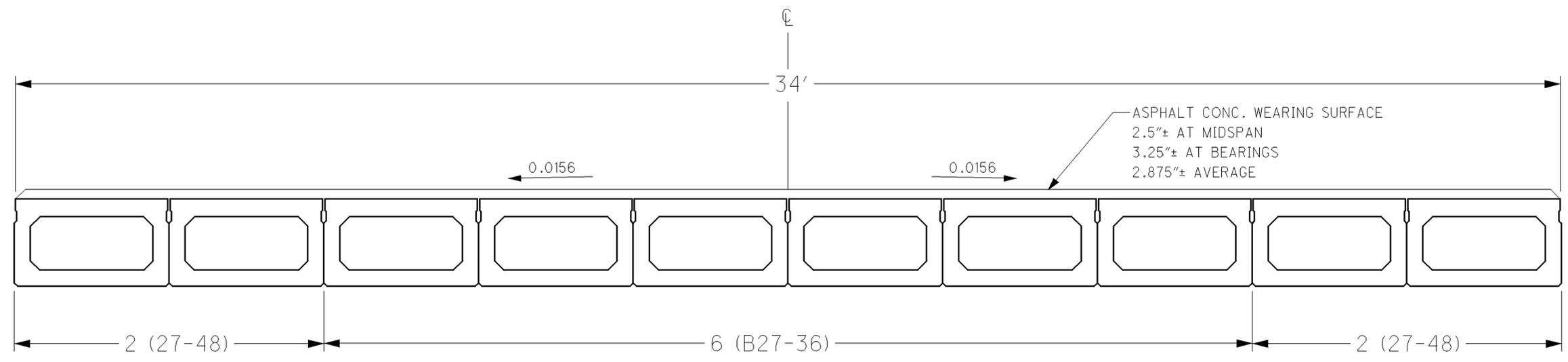
 254 PAVEMENT PLANING,
ASPHALT CONCRETE, AS PER PLAN

ITEM 512 TYPE 3 WATERPROOFING
46' X 34' / 9 = 174 SQ.YD.

ITEM 518 SPECIAL - STEEL DRIP STRIP
92 FEET

QUANTITIES CARRIED TO LOCATION 3 SUB-SUMMARY

NOTE: CARE SHALL BE TAKEN WHEN REMOVING
ASPHALT CONCRETE SO AS NOT TO DAMAGE
BOX BEAMS. ASPHALT DEPTH ON DECK VARIES
FROM 2.5" TO 3.25" IN DEPTH. ANY DAMAGE THAT
OCCURS TO THE EXISTING BOX BEAMS SHALL BE
CORRECTED AT THE CONTRACTOR'S EXPENSE AS
DIRECTED BY THE ENGINEER, AS PER CMS 519 OR
SS 843.



TRANSVERSE SECTION

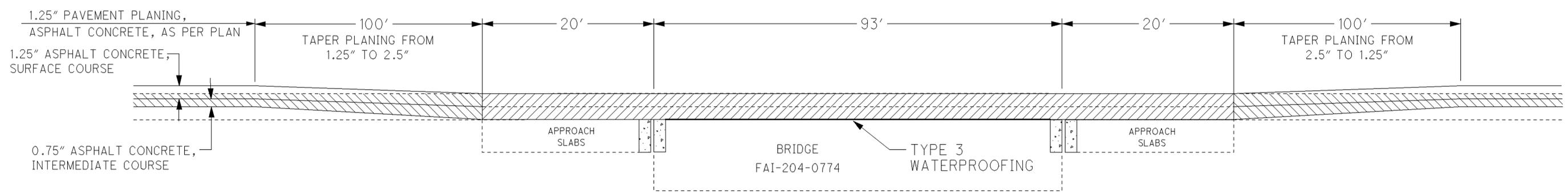
DRAWINGS NOT TO SCALE

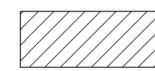
F204_MBT_002.DGN 5-26-09

BRIDGE DECK DETAILS

FAI-204-0.00
FAI-204A-0.00

DESCRIPTION OF WORK:
WEARING COURSE REMOVED SHALL BE AS SHOWN ON DETAIL BELOW,
REMOVE ASPHALT, DRIP STRIP AND WATERPROOFING COMPLETELY.
PLACE STEEL DRIP STRIP, TYPE 3 WATERPROOFING, AND RESURFACE
WITH 1.25" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG 64-22
AND 1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 70-22M



 202 WEARING COURSE
REMOVED

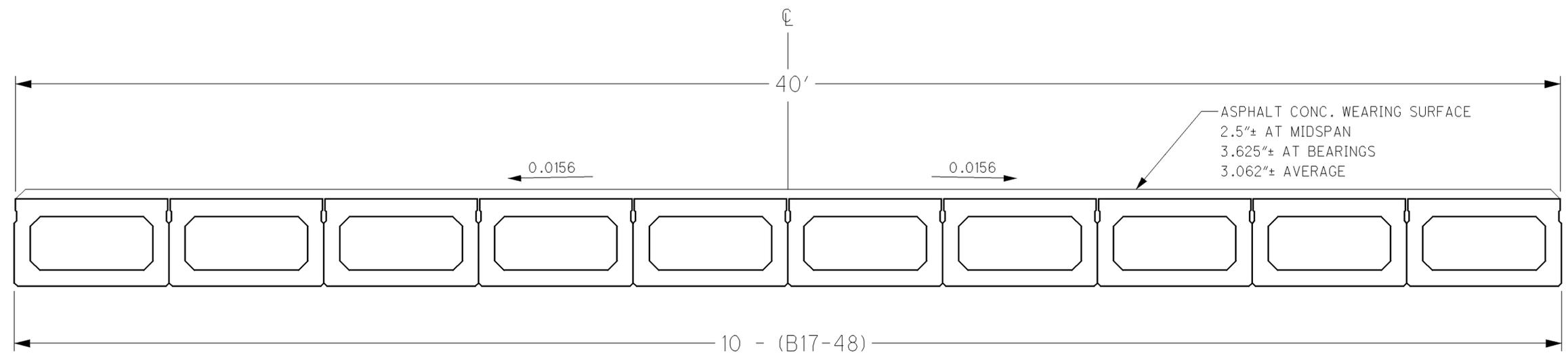
 254 PAVEMENT PLANING,
ASPHALT CONCRETE, AS PER PLAN

ITEM 512 TYPE 3 WATERPROOFING
93' X 40' / 9 = 414 SQ.YD.

ITEM 518 SPECIAL - STEEL DRIP STRIP
186 FEET

QUANTITIES CARRIED TO LOCATION 3 SUB-SUMMARY

NOTE: CARE SHALL BE TAKEN WHEN REMOVING
ASPHALT CONCRETE SO AS NOT TO DAMAGE
BOX BEAMS. ASPHALT DEPTH ON DECK VARIES
FROM 2.5" TO 3.625" IN DEPTH. ANY DAMAGE THAT
OCCURS TO THE EXISTING BOX BEAMS SHALL BE
CORRECTED AT THE CONTRACTOR'S EXPENSE AS
DIRECTED BY THE ENGINEER, AS PER CMS 519 OR
SS 843.



TRANSVERSE SECTION

DRAWINGS NOT TO SCALE

F204_MBT_003.DGN 5-26-09

BRIDGE DECK DETAILS

FAI-204-0.00
FAI-204A-0.00

ITEM 642 EDGE LINE SUB-SUMMARY													
LOCATION	COUNTY	ROUTE	S.L.M.		TOTAL LENGTH (MILES)	INFORMATION ONLY						TOTAL EDGE LINE MILES	REMARKS
						WHITE EDGE LINE QUANTITIES			YELLOW EDGE LINE QUANTITIES				
			FROM	TO		TOTAL MILES	HIGHWAY MILES	RAMP MILES	TOTAL MILES	HIGHWAY MILES	RAMP MILES		
2	FAI	S.R. 204	0.40	4.37	3.97	7.94	7.94					7.94	
LOCATION 2 (TOTAL CARRIED TO SUB-SUMMARY)												7.94	
3	FAI	S.R. 204	4.37	11.16	6.79	13.58	13.58					13.58	
LOCATION 3 (TOTAL CARRIED TO SUB-SUMMARY)												13.58	
ITEM 644 EDGE LINE SUB-SUMMARY													
4	FAI	S.R. 204A	0.00	0.24	0.24	0.24		0.24	0.24		0.24	0.48	
4	FAI	S.R. 204A	0.24	0.35	0.11	0.22	0.22		0.22	0.22		0.44	
4	FAI	S.R. 204A	0.35	0.49	0.14	0.28	0.28					0.28	
LOCATION 4 (TOTAL CARRIED TO SUB-SUMMARY)												1.20	

ITEM 644 LANE LINE SUB-SUMMARY													
LOCATION	COUNTY	ROUTE	S.L.M.		INFORMATION ONLY						TOTAL LANE LINE MILES	REMARKS	
					LANE LINE QUANTITIES								
			FROM	TO	TOTAL MILES	DASHED	SOLID						
1	FAI	S.R. 204 E.B.	0.00	0.34	0.34	0.34						0.34	PICKERINGTON
1	FAI	S.R. 204 W.B.	0.09	0.25	0.16	0.16						0.16	PICKERINGTON
LOCATION 1 (TOTAL CARRIED TO SUB-SUMMARY)					0.50	0.50						0.50	

ITEM 642 CENTER LINE SUB-SUMMARY

LOCATION	COUNTY	ROUTE	S.L.M.		TOTAL LENGTH (MILES)	INFORMATION ONLY		TOTAL CENTER LINE MILES	REMARKS
			FROM	TO		CENTER LINE QUANTITIES			
						TOTAL MILES	EQUIVALENT SOLID LINE		
1	FAI	S.R. 204	0.00	0.36	0.36	0.56	1.120	0.56	0.20 MILE EXTRA CENTER LINE FOR TRANSVERSE AREAS
LOCATION 1 (TOTAL CARRIED TO SUB-SUMMARY)								0.56	LOCATION 1 SHALL BE ITEM 644 CENTER LINE
2	FAI	S.R. 204	0.36	4.37	4.01	4.43	7.032	4.43	0.42 MILE EXTRA CENTER LINE FOR TRANSVERSE AREAS
LOCATION 2 (TOTAL CARRIED TO SUB-SUMMARY)								4.43	
3	FAI	S.R. 204	4.37	11.16	6.79	7.21	9.520	7.21	
LOCATION 3 (TOTAL CARRIED TO SUB-SUMMARY)								7.21	

ITEM 644 CENTER LINE SUB-SUMMARY

LOCATION	COUNTY	ROUTE	S.L.M.		TOTAL LENGTH (MILES)	INFORMATION ONLY		TOTAL CENTER LINE MILES	REMARKS
			FROM	TO		CENTER LINE QUANTITIES			
						TOTAL MILES	EQUIVALENT SOLID LINE		
4	FAI	S.R. 204A	0.35	0.49	0.14	0.14	0.280	0.14	
LOCATION 4 (TOTAL CARRIED TO SUB-SUMMARY)								0.14	

644 THERMOPLASTIC (AUXILIARY PAVEMENT MARKINGS)

LOCATION	COUNTY	ROUTE	SIDE	DESCRIPTION	TRANSVERSE/ DIAGONAL LINES (24")		STOP LINE (24")	12" CROSSWALK LINE	WORD ON PAVEMENT		SCHOOL SYMBOL MARKING		LANE ARROW				8" CHANNELIZING LINE	ISLAND MARKING	RAILROAD MARKING SYMBOL	REMARKS		
					WHITE FT.	YELLOW FT.			"ONLY"		72" EACH	96" EACH	72" EACH	96" EACH	RT/TH EACH	THRU EACH					LT. EACH	RT. EACH
									72"	96"												
									FT.	FT.												
1	FAI	S.R. 204		AT S.R. 256			62		4				2	2	4	1051			PLACE AT EXISTING LOCATIONS			
1	FAI	S.R. 204		AT INTERSECTION EAST OF S.R. 256		151	62		3				2	4	1	80	57		PLACE AT EXISTING LOCATIONS			
1	FAI	S.R. 204		AT YARMOUTH DR. INTERSECTION		158	53		3				2	4	1	75	57		PLACE AT EXISTING LOCATIONS			
1	FAI	S.R. 204		BEFORE SUMMERFIELD WAY	170	176			1							155			PLACE AT EXISTING LOCATIONS			
SUBTOTALS					170	485							4	10	4	6						
LOCATION 1 (TOTALS CARRIED TO SUB-SUMMARY)					655		177		11					24		1361	114					
2	FAI	S.R. 204	RT	SUMMERFIELD WAY			24												PLACE 10' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204		AFTER SUMMERFIELD WAY	226														PLACE AT EXISTING LOCATIONS			
2	FAI	S.R. 204	LT	CAPETOWN AVE.			31												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	ECHO MANOR			17												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	RUSTIC DR.			32												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	WELLESLEY DR.															PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	WATERTON DR.			23												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204		TURN LANES AT HARMON RD.		256	48			1					3	240			PLACE AT EXISTING LOCATIONS			
2	FAI	S.R. 204	RT	HARMON RD.			16												PLACE 30' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	HAFF FARM DR.			30												PLACE 13' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	DAYSPRINGS WAY			28												PLACE 14' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204		TURN LANE AT ASHLEY CREEK DR.		266				1					1	110	57		PLACE AT EXISTING LOCATIONS			
2	FAI	S.R. 204	RT	ASHLEY CREEK DR.			31								1				PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	CARRIAGE LN.			37												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	CHESINGTON DR.			22	40											PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	HARDWICK CT.			18												PLACE 10' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	CHEVINGTON DR.			22												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	EASTWAY DR.			30												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	NEW ENGLAND ACRES			32												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	MILNOR RD. N.W.			27												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	NANTUCKET AVE.			30												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	SUDBURY DR.			16												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	WAGRAM RD.			17												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	AULT RD.			15												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	CALHOUN CT.																		
2	FAI	S.R. 204	LT	VIOLET MEADOWS BLVD.																		
2	FAI	S.R. 204	RT	PICKERINGTON RD. N.W.			22												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204		TURN LANE AT OPTIMARA DR.		304				1					1	100	57		PLACE AT EXISTING LOCATIONS			
2	FAI	S.R. 204	LT	OPTIMARA DR.			25												PLACE 17' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	LT	TOLLGATE RD.			20												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
2	FAI	S.R. 204	RT	TOLLGATE RD.			20												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
SUBTOTALS					226	826								6								
LOCATION 2 (TOTALS CARRIED TO SUB-SUMMARY)					1,052		633	40		3				6		450	114					

644 THERMOPLASTIC (AUXILIARY PAVEMENT MARKINGS)

LOCATION	COUNTY	ROUTE	SIDE	DESCRIPTION	TRANSVERSE/DIAGONAL LINES (24")		STOP LINE (24")	12" CROSSWALK LINE	WORD ON PAVEMENT		SCHOOL SYMBOL MARKING		LANE ARROW				8" CHANNELIZING LINE	ISLAND MARKING	RAILROAD MARKING SYMBOL	REMARKS		
					WHITE	YELLOW			"ONLY"		72"	96"	72"	96"	RT/TH	THRU					LT.	RT.
					FT.	FT.			FT.	FT.	EACH	EACH	EACH	EACH	EACH	EACH					EACH	EACH
3	FAI	S.R. 204	LT	SAYLOR RD.			20												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	LT	S.R. 310			23												PLACE 12' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204		ON S.R. 204 @ BLACKLICK RD.			27												PLACE 8' FROM EDGE OF PAVEMENT BLACKLICK RD.			
3	FAI	S.R. 204	RT	MAUGER RD.			18												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	RT	REFUGEE RD.			18												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	RT	POPLAR CREEK RD.			20												PLACE 20' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	LT	HEIMBERGER RD.			18												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	RT	HEIMBERGER RD.			18												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	LT	SNYDER CHURCH RD. N.W.			25												PLACE 8' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	RT	SNYDER CHURCH RD. N.W.			25												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	RT	BASIL RD.			25												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	LT	BASIL RD.			30												PLACE 6' FROM EDGE OF PAVEMENT S.R. 204			
3	FAI	S.R. 204	CL	ON S.R. 204 @ S.R. 158			27												PLACE 12' FROM EDGE OF PAVEMENT S.R. 158			
SUBTOTALS																						
LOCATION 3 (TOTALS CARRIED TO SUB-SUMMARY)							294															
4	FAI	S.R. 204A		AT S.R. 204			28		2					3	700				PLACE AT EXISTING LOCATIONS			
SUBTOTALS														3								
LOCATION 4 (TOTALS CARRIED TO SUB-SUMMARY)							28		2					3	700							

CALCULATED
LME
CHECKED
DNM

PAVEMENT MARKING DATA

FAI-204-0.00
FAI-204A-0.00

DETAIL	
1	TAPERED ACCELERATION LANE
2	DECELERATION LANE
3	MULTILANE DIVIDED/ CONTROLLED ACCESS

DETAIL	
4	4 LANE DIVIDED TO 2 LANE TRANSITION
5	4 LANE UNDIVIDED TO 2 LANE TRANSITION
6	ONE LANE BRIDGE
7	STOP APPROACH
8	THRU APPROACH
9	TWO WAY LEFT TURN LANE

DETAIL	
10	APPROACH W/LT. TURN LANE
11	HORIZONTAL CURVE 40' *
12	HORIZONTAL CURVE ALT. *
GAP	CENTERLINE AT 80' TYP.

* SEE NOTE 6, SCD TC-65.11 SHEET 2/2

REM = REMARKS

CALCULATED
LME
CHECKED
DNM

ITEM 621 RPM SUB-SUMMARY

LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	LENGTH		DETAIL	621 PRISMATIC RETRO-REFLECTOR COLORS					REMARKS	
								RPM	INFORMATION ONLY					
									ONE-WAY		TWO-WAY			
									WHITE	YELLOW	YELLOW YELLOW	WHITE RED		YELLOW RED
EACH														
1	FAI	S.R. 204	0.00	0.24	0.24	1,267.2	GAP	16			16			
1	FAI	S.R. 204	0.24	0.27	0.03	158.4	11	4			4			PC 0.24 PT 0.27 L=158' DEG=9
1	FAI	S.R. 204	0.27	0.36	0.09	475.2	GAP	6			6			
1	FAI	S.R. 204					REM	48			14	34		EXTRA QUANTITY FOR TURN LANES
LOCATION 1 (TOTAL CARRIED TO SUB-SUMMARY)								74						
2	FAI	S.R. 204	0.36	0.77	0.41	2,164.8	GAP	27			27			
2	FAI	S.R. 204	0.77	0.90	0.13	686.4	11	17			17			PC 0.77 PT 0.90 L=370' DEG=6
2	FAI	S.R. 204	0.90	0.97	0.07	369.6	GAP	5			5			
2	FAI	S.R. 204	0.97	1.06	0.09	475.2	11	12			12			PC 0.97 PT 1.06 L=475' DEG=6
2	FAI	S.R. 204	1.06	1.13	0.07	369.6	GAP	5			5			
2	FAI	S.R. 204	1.13	1.19	0.06	316.8	11	8			8			PC 1.13 PT 1.19 L=317' DEG=6
2	FAI	S.R. 204	1.19	1.28	0.09	475.2	GAP	6			6			
2	FAI	S.R. 204	1.28	1.38	0.10	528.0	11	13			13			PC 1.28 PT 1.38 L=528' DEG=6
2	FAI	S.R. 204	1.38	4.37	2.99	15,787.2	GAP	197			197			
2	FAI	S.R. 204	1.24	1.45			10	16			8	8		LEFT TURN LANES AT HARMON RD. (EXTRA RPM'S)
2	FAI	S.R. 204	1.70	1.85			10	11			8	3		LEFT TURN LANE AT ASHLEY CREEK DR. (EXTRA RPM'S)
2	FAI	S.R. 204	3.90	4.10			10	14			11	3		LEFT TURN LANE AT OPTIMARA DR. (EXTRA RPM'S)
LOCATION 2 (TOTAL CARRIED TO SUB-SUMMARY)								331						
3	FAI	S.R. 204	4.37	6.54	2.17	11,457.6	GAP	143			143			
3	FAI	S.R. 204	6.54	6.76	0.22	1,161.6	12	29			29			PC 6.63 PT 6.67 L=211' DEG=19
3	FAI	S.R. 204	6.76	7.54	0.78	4,118.4	GAP	51			51			
3	FAI	S.R. 204	7.54	7.74	0.20	1,056.0	12	27			27			PC 7.63 PT 7.65 L=106' DEG=13
3	FAI	S.R. 204	7.74	11.16	3.42	18,057.6	GAP	226			226			
LOCATION 3 (TOTAL CARRIED TO SUB-SUMMARY)								476						
4	FAI	S.R. 204A	0.00	0.24	0.24	1,267.2	GAP	16				16		RAMP G TO S.R. 204A (TAYLOR RD.)
4	FAI	S.R. 204A	0.35	0.49	0.14	739.2	GAP	41	14		9	18		STOP AT S.R. 204, 40' SPACING ON CHANNELIZING LINE
LOCATION 4 (TOTAL CARRIED TO SUB-SUMMARY)								57						

RAISED PAVEMENT MARKER DATA

FAI-204-0.00
FAI-204A-0.00

LOCATION 1 (SHEET TOTALS)											ITEM	ITEM EXT.	LOCATION 1 TOTALS	UNIT	DESCRIPTION	SEE SHEET
3	4	5	6	7	10	14	15	16	18							
					191						202	23500	191	SQ.YD.	WEARING COURSE REMOVED	
				12,989							254	01001	12,989	SQ.YD.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN	2
				974	15						407	10000	989	GALLON	TACK COAT	
				650	10						407	14000	660	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
				361	4						448	46020	365	CU.YD.	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22	
				361	7						448	46904	368	CU.YD.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M	
	4										604	34500	4	EACH	MANHOLE ADJUSTED TO GRADE	
			20								614	11100	20	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR	
		3									614	13000	3	CU.YD.	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
		0.80									614	21400	0.80	MILE	WORK ZONE CENTER LINE, CLASS II	
			1								614	50010	1	EACH	AMERICAN RECOVERY AND REINVESTMENT ACT (ARRA) SIGN, CONVENTIONAL ROAD	
									74		621	00100	74	EACH	RPM	
74											621	54000	74	EACH	RAISED PAVEMENT MARKER REMOVED	
	17										632	26501	17	EACH	DETECTOR LOOP, AS PER PLAN	4
						0.50					644	00200	0.50	MILE	LANE LINE	
							0.56				644	00300	0.56	MILE	CENTER LINE	
								1,361			644	00400	1,361	FEET	CHANNELIZING LINE	
								177			644	00500	177	FEET	STOP LINE	
								655			644	00700	655	FEET	TRANSVERSE/DIAGONAL LINE	
								114			644	00900	114	SQ.FT.	ISLAND MARKING	
								24			644	01300	24	EACH	LANE ARROW	
								11			644	01400	11	EACH	WORD ON PAVEMENT, 72"	

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LOCATION 1 SUB-SUMMARY

FAI-204-0.00
FAI-204A-0.00

LOCATION 2 (SHEET TOTALS)												ITEM	ITEM EXT.	LOCATION 2 TOTALS	UNIT	DESCRIPTION	SEE SHEET	
3	4	5	6	7	9	10	11	14	15	16	18							
	2,030					5,428							202	23500	7,458	SQ.YD.	WEARING COURSE REMOVED	
2													209	60500	2	MILE	LINEAR GRADING	
50													253	02001	50	CU. YD.	PAVEMENT REPAIR, AS PER PLAN	3
					52,610	3,933							254	01001	56,543	SQ.YD.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN	2
					3,946	295	409						407	10000	4,650	GALLON	TACK COAT	
					2,631	197	273						407	14000	3,101	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
1,616													408	10001	1,616	GALLON	PRIME COAT, AS PER PLAN	3
	43				1,462	110	115						448	46020	1,730	CU.YD.	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22	
	72				1,462	110	190						448	46904	1,834	CU.YD.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M	
								110					516	31011	110	FEET	2" DEEP JOINT SEALER, AS PER PLAN	2
			40										614	11100	40	HOURL	LAW ENFORCEMENT OFFICER WITH PATROL CAR	
		15											614	12460	15	EACH	WORK ZONE MARKING SIGN	
		5											614	13000	5	CU.YD.	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
		8.74											614	21400	8.74	MILE	WORK ZONE CENTER LINE, CLASS II	
						510							617	10101	510	CU.YD.	COMPACTED AGGREGATE, AS PER PLAN	2
											331		621	00100	331	EACH	RPM	
331													621	54000	331	EACH	RAISED PAVEMENT MARKER REMOVED	
	18												632	26501	18	EACH	DETECTOR LOOP, AS PER PLAN	4
	5												638	10800	5	EACH	VALVE BOX ADJUSTED TO GRADE	
								7.94					642	00100	7.94	MILE	EDGE LINE, TYPE 1	
									4.43				642	00300	4.43	MILE	CENTER LINE, TYPE 1	
										450			644	00400	450	FEET	CHANNELIZING LINE	
										633			644	00500	633	FEET	STOP LINE	
										40			644	00600	40	FEET	CROSSWALK LINE	
										1,052			644	00700	1,052	FEET	TRANSVERSE/DIAGONAL LINE	
										114			644	00900	114	SQ.FT.	ISLAND MARKING	
										6			644	01300	6	EACH	LANE ARROW	
										3			644	01410	3	EACH	WORD ON PAVEMENT, 96"	

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LOCATION 2 SUB-SUMMARY

FAI-204-0.00
FAI-204A-0.00

LOCATION 3 (SHEET TOTALS)														ITEM	ITEM EXT.	LOCATION 3 TOTALS	UNIT	DESCRIPTION	SEE SHEET
3	4	5	6	7	9	10	11	12	13	14	15	17	18						
	2,060					2,329	917							202	23500	5,306	SQ.YD.	WEARING COURSE REMOVED	
4														209	60500	4	MILE	LINEAR GRADING	
100														253	02001	100	CU. YD.	PAVEMENT REPAIR, AS PER PLAN	3
				79,183										254	01001	79,183	SQ.YD.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN	2
				5,939		176								407	10000	6,115	GALLON	TACK COAT	
				3,959		117	30							407	14000	4,106	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
6,374														408	10001	6,374	GALLON	PRIME COAT, AS PER PLAN	3
	44			2,200		50	25							448	46020	2,319	CU.YD.	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22	
	73			2,200		82	21							448	46904	2,376	CU.YD.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M	
								174	414					512	33010	588	SQ.YD.	TYPE 3 WATERPROOFING	
								92	186					518	22300	278	FEET	SPECIAL - STEEL DRIP STRIP	
	1													604	34500	1	EACH	MANHOLE ADJUSTED TO GRADE	
		54												614	12460	54	EACH	WORK ZONE MARKING SIGN	
		2												614	13000	2	CU.YD.	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
		13.58												614	21400	13.58	MILE	WORK ZONE CENTER LINE, CLASS II	
			1											614	50010	1	EACH	AMERICAN RECOVERY AND REINVESTMENT ACT (ARRA) SIGN, CONVENTIONAL ROAD	
					878									617	10101	878	CU.YD.	COMPACTED AGGREGATE, AS PER PLAN	2
													476	621	00100	476	EACH	RPM	
476														621	54000	476	EACH	RAISED PAVEMENT MARKER REMOVED	
										13.58				642	00100	13.58	MILE	EDGE LINE, TYPE 1	
											7.21			642	00300	7.21	MILE	CENTER LINE, TYPE 1	
												294		644	00500	294	FEET	STOP LINE	

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LOCATION 3 SUB-SUMMARY

FAI-204-0.00
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LOCATION 4 (SHEET TOTALS)											ITEM	ITEM EXT.	LOCATION 4 TOTALS	UNIT	DESCRIPTION	SEE SHEET
3	4	5	6	7	9	14	15	17	18							
				6,758	2,957						254	01001	9,715	SQ.YD.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN	2
				507	222						407	10000	729	GALLON	TACK COAT	
				338	148						407	14000	486	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
113											408	10001	113	GALLON	PRIME COAT, AS PER PLAN	3
				188	83						448	46020	271	CU.YD.	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22	
				188	83						448	46904	271	CU.YD.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M	
			40								614	11100	40	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR	
		2									614	12460	2	EACH	WORK ZONE MARKING SIGN	
		3									614	13000	3	CU.YD.	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
		0.28									614	21400	0.28	MILE	WORK ZONE CENTER LINE, CLASS II	
			1								614	50010	1	EACH	AMERICAN RECOVERY AND REINVESTMENT ACT (ARRA) SIGN, CONVENTIONAL ROAD	
					16						617	10101	16	CU.YD.	COMPACTED AGGREGATE, AS PER PLAN	2
									57		621	00100	57	EACH	RPM	
57											621	54000	57	EACH	RAISED PAVEMENT MARKER REMOVED	
	4										632	26501	4	EACH	DETECTOR LOOP, AS PER PLAN	4
						1.20					644	00100	1.20	MILE	EDGE LINE	
							0.14				644	00300	0.14	MILE	CENTER LINE	
								700			644	00400	700	FEET	CHANNELIZING LINE	
								28			644	00500	28	FEET	STOP LINE	
								3			644	01300	3	EACH	LANE ARROW	
								2			644	01410	2	EACH	WORD ON PAVEMENT, 96"	

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LOCATION 4 SUB-SUMMARY

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LOCATION TOTALS					80% FEDERAL 20% LOCAL	80% FEDERAL 20% STATE URBAN	80% FEDERAL 20% STATE RURAL	ITEM	ITEM EXT.	GRAND TOTALS	UNIT	DESCRIPTION
1	2	3	4									
191	7,458	5,306			191	7,458	5,306	202	23500	12,955	SQ.YD.	WEARING COURSE REMOVED
	2	4				2	4	209	60500	6	MILE	LINEAR GRADING
	50	100				50	100	253	02001	150	CU. YD.	PAVEMENT REPAIR, AS PER PLAN
12,989	56,543	79,183	9,715		12,989	66,258	79,183	254	01001	158,430	SQ.YD.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN
989	4,650	6,115	729		989	5,379	6,115	407	10000	12,483	GALLON	TACK COAT
660	3,101	4,106	486		660	3,587	4,106	407	14000	8,353	GALLON	TACK COAT FOR INTERMEDIATE COURSE
	1,616	6,374	113			1,729	6,374	408	10001	8,103	GALLON	PRIME COAT, AS PER PLAN
365	1,730	2,319	271		365	2,001	2,319	448	46020	4,685	CU.YD.	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22
368	1,834	2,376	271		368	2,105	2,376	448	46904	4,849	CU.YD.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M
		588					588	512	33010	588	SQ.YD.	TYPE 3 WATERPROOFING
	110					110		516	31011	110	FEET	2" DEEP JOINT SEALER, AS PER PLAN
		278					278	518	22300	278	FEET	SPECIAL - STEEL DRIP STRIP
4		1			4		1	604	34500	5	EACH	MANHOLE ADJUSTED TO GRADE
20	40		40		20	80		614	11100	100	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR
	15	54	2			17	54	614	12460	71	EACH	WORK ZONE MARKING SIGN
3	5	2	3		3	8	2	614	13000	13	CU.YD.	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC
0.80	8.74	13.58	0.28		0.80	9.02	13.58	614	21400	23.40	MILE	WORK ZONE CENTER LINE, CLASS II
1		1	1		1	1	1	614	50010	3	EACH	AMERICAN RECOVERY AND REINVESTMENT ACT (ARRA) SIGN, CONVENTIONAL ROAD
	510	878	16			526	878	617	10101	1,404	CU.YD.	COMPACTED AGGREGATE, AS PER PLAN
74	331	476	57		74	388	476	621	00100	938	EACH	RPM
74	331	476	57		74	388	476	621	54000	938	EACH	RAISED PAVEMENT MARKER REMOVED
17	18		4		17	22		632	26501	39	EACH	DETECTOR LOOP, AS PER PLAN
	5					5		638	10800	5	EACH	VALVE BOX ADJUSTED TO GRADE
	7.94	13.58				7.94	13.58	642	00100	21.52	MILE	EDGE LINE, TYPE 1
	4.43	7.21				4.43	7.21	642	00300	11.64	MILE	CENTER LINE, TYPE 1
			1.20			1.20		644	00100	1.20	MILE	EDGE LINE
0.50					0.50			644	00200	0.50	MILE	LANE LINE
0.56			0.14		0.56	0.14		644	00300	0.70	MILE	CENTER LINE
1,361	450		700		1,361	1,150		644	00400	2,511	FEET	CHANNELIZING LINE
177	633	294	28		177	661	294	644	00500	1,132	FEET	STOP LINE
	40					40		644	00600	40	FEET	CROSSWALK LINE
655	1,052				655	1,052		644	00700	1,707	FEET	TRANSVERSE/DIAGONAL LINE
114	114				114	114		644	00900	228	SQ.FT.	ISLAND MARKING
24	6		3		24	9		644	01300	33	EACH	LANE ARROW
11					11			644	01400	11	EACH	WORD ON PAVEMENT, 72"
	3		2			5		644	01410	5	EACH	WORD ON PAVEMENT, 96"
								103	05000	LUMP		PREMIUM FOR CONTRACT PERFORMANCE BOND AND FOR PAYMENT BOND
								614	11000	LUMP		MAINTAINING TRAFFIC
								619	16000	2	MONTH	FIELD OFFICE, TYPE A
								623	10000	LUMP		CONSTRUCTION LAYOUT STAKES
								624	10000	LUMP		MOBILIZATION

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GENERAL SUMMARY

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