

LOCATION MAP

LATITUDE: 40°3'48"N LONGITUDE: 82°59'48"W



PORTION TO BE IMPROVED	-----	=====
INTERSTATE HIGHWAY	-----	=====
FEDERAL ROUTES	-----	=====
STATE ROUTES	-----	=====
COUNTY & TOWNSHIP ROADS	-----	=====
OTHER ROADS	-----	=====

DESIGN DESIGNATION

	FRA-71	FRA-23
CURRENT ADT (2023)	147,000	84,500
DESIGN YEAR ADT (2030)	149,000	112,000
DESIGN HOURLY VOLUME (2030)	14,900	10,080
DIRECTIONAL DISTRIBUTION	53%	55%
TRUCKS (24 HOUR B&C)	23,840	2,535
DESIGN SPEED	70MPH	45MPH
LEGAL SPEED	65MPH	35MPH
DESIGN FUNCTIONAL CLASSIFICATION:		
01 - INTERSTATE (URBAN)	03 - OTHER PRINCIPAL ARTERIAL (URBAN)	
NHS PROJECT	YES	

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES
Contact Two Working Days Before You Dig

OHIO811.org
Before You Dig

OHIO 811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)



ENGINEER'S SEAL

STATE OF OHIO
ROBERT A. MCNEILL
E-78025
REGISTERED PROFESSIONAL ENGINEER

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

FRA-71/23 CRACKSEAL PPM FY23 CITY OF COLUMBUS CLINTON TOWNSHIP, SHARON TOWNSHIP FRANKLIN COUNTY

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STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
MT-95.30	7/19/19	TC-41.20	10/18/13	800-2019 SEE PROPOSAL	
MT-95.31	7/19/19	TC-52.10	10/18/13	809 7/15/22	
MT-95.32	4/19/19	TC-52.20	1/15/21	821 4/20/12	
MT-95.45	1/17/20			832 7/15/22	
MT-95.50	7/21/17			921 4/20/12	
MT-98.10	1/17/20				
MT-98.11	1/17/20				
MT-98.20	4/19/19				
MT-98.22	1/17/20				
MT-98.28	1/17/20				
MT-98.29	1/17/20				
MT-99.20	4/19/19				
MT-101.90	7/17/20				
MT-105.10	1/17/20				

FEDERAL PROJECT NUMBER

NON-FEDERAL

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

CRACK SEALING ON I-71 AND US-23 IN FRANKLIN COUNTY.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.0 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.0 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A*
	* MAINTENANCE PROJECT

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

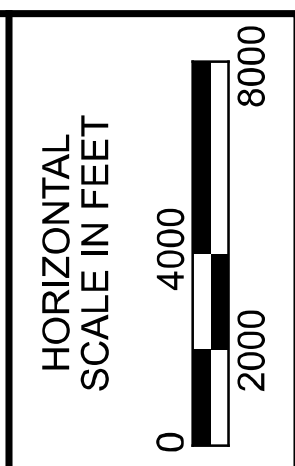
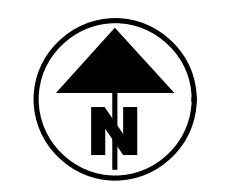
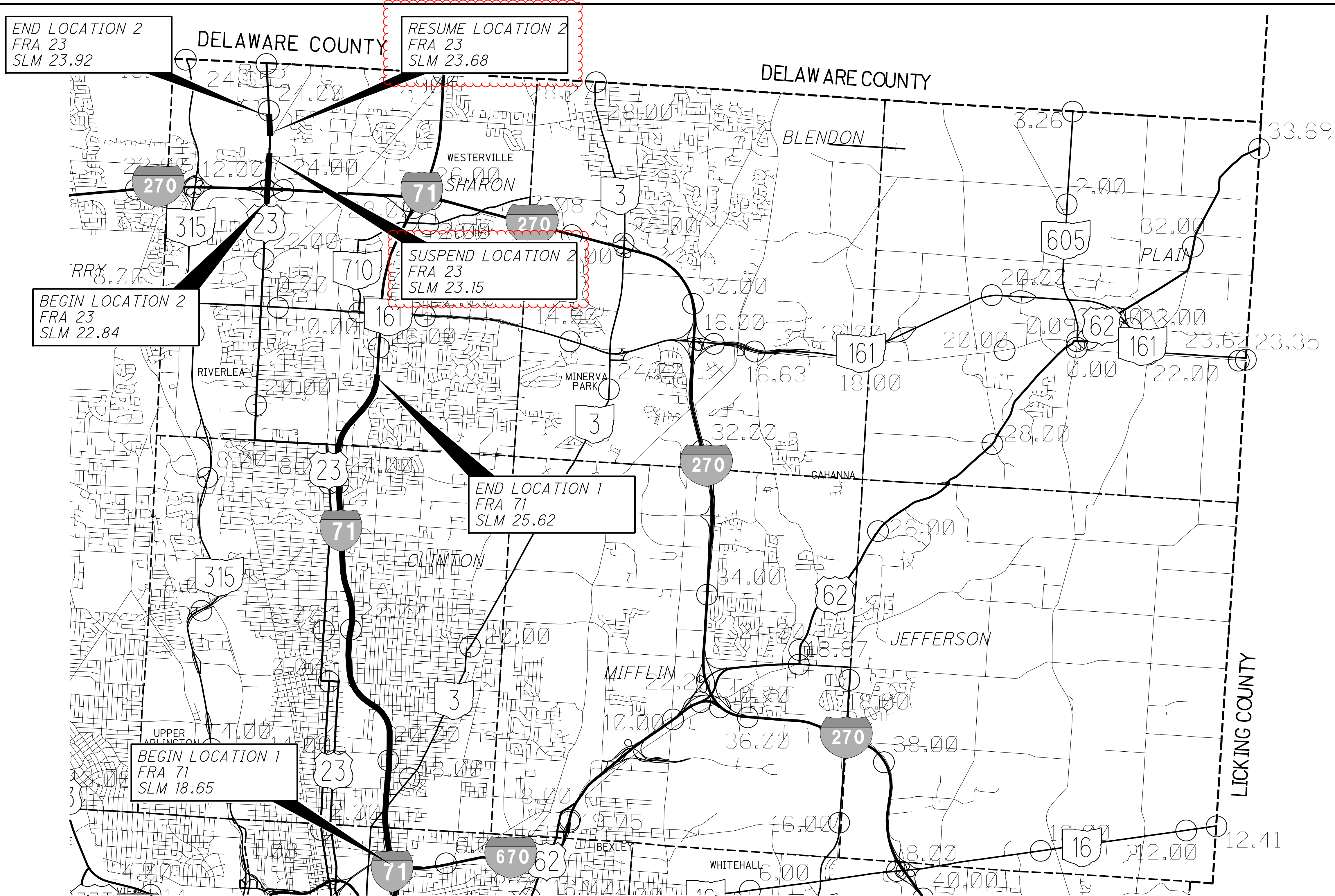
Anthony C. Turowski
Anthony C. Turowski, P.E.
District 06 Deputy Director

Jack Marchbanks
Jack Marchbanks, PhD
Director, Department of Transportation

DESIGN AGENCY



DESIGNER	RAM
REVIEWER	XXX MM-DD-YY
PROJECT ID	105838
SHEET TOTAL	P.01 17



LOCATION MAP

CATEGORY	PLAN SPLIT CODE	DESCRIPTION
INTERSTATE	01/IMS/05	ALL ITEMS ASSOCIATED WITH CRACK SEALING ON I-71 AND US-23.

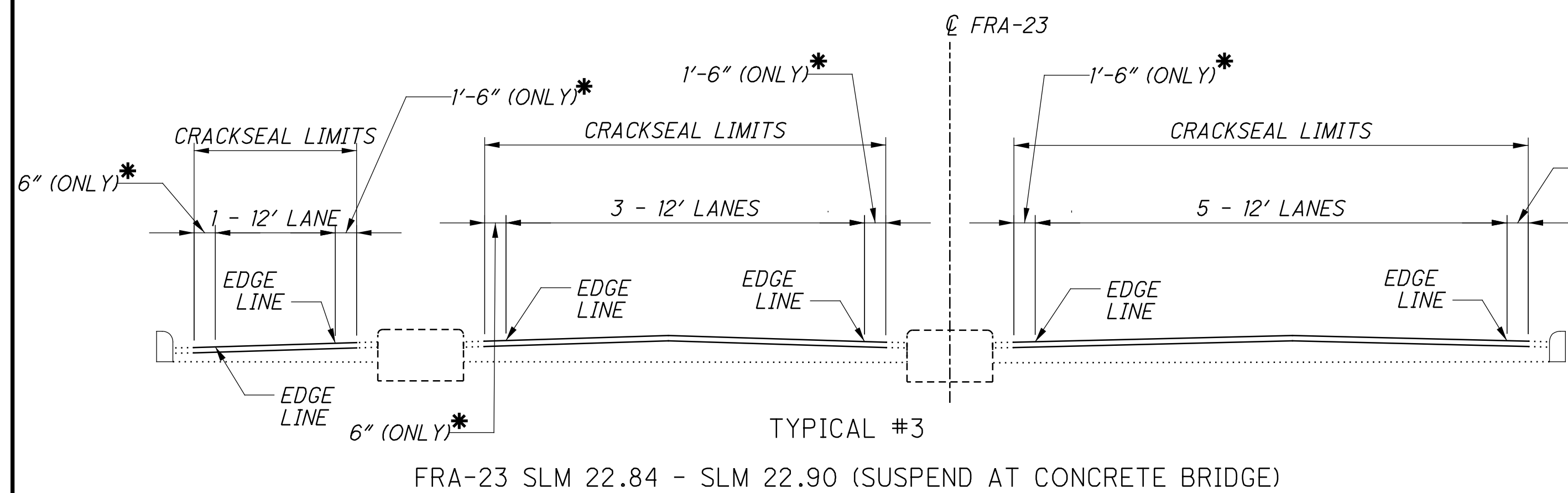
DESIGN AGENCY

DESIGNER
RAM

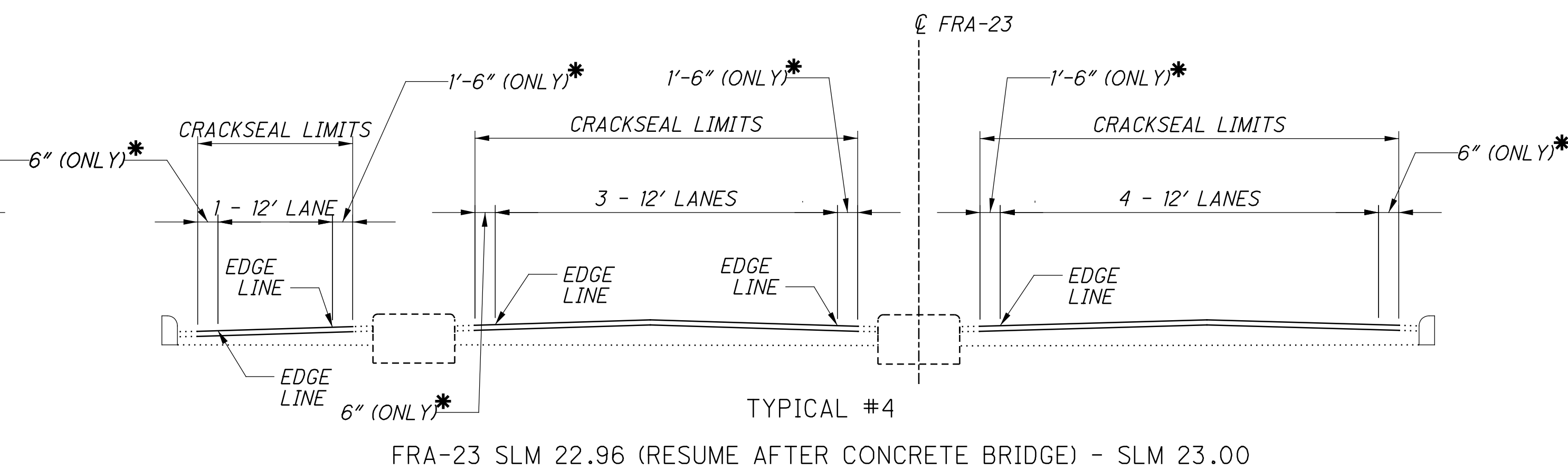
REVIEWER
XXX MM-DD-YY

PROJECT ID
105838

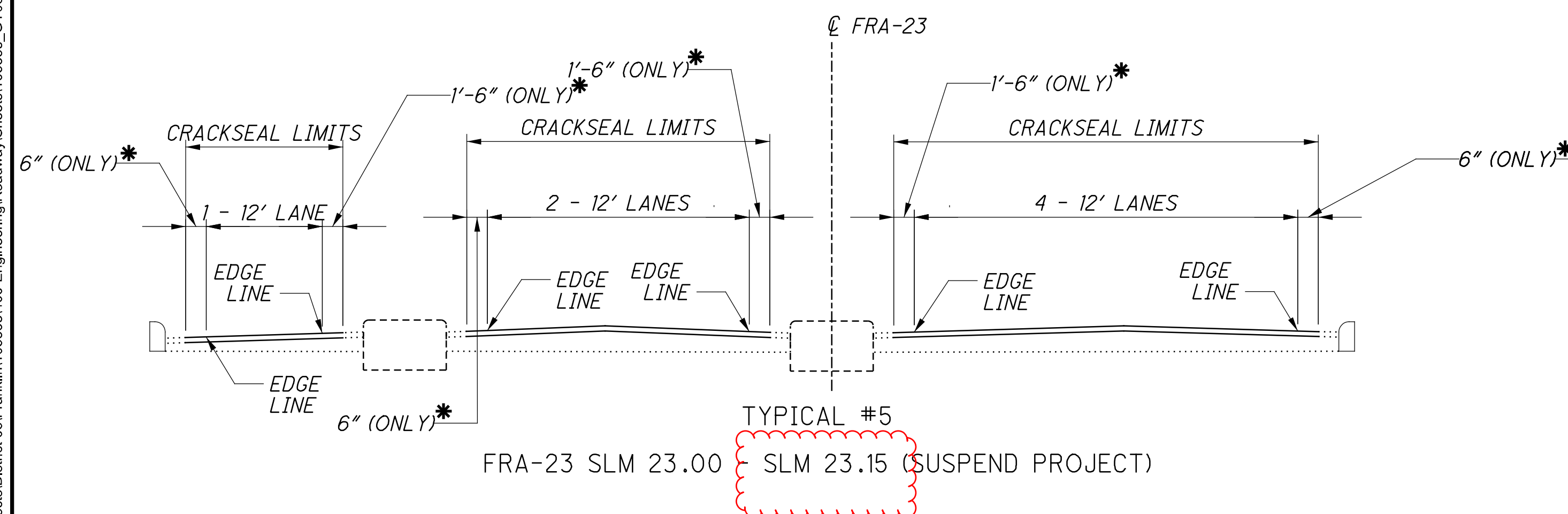
SHEET TOTAL
P.02 17



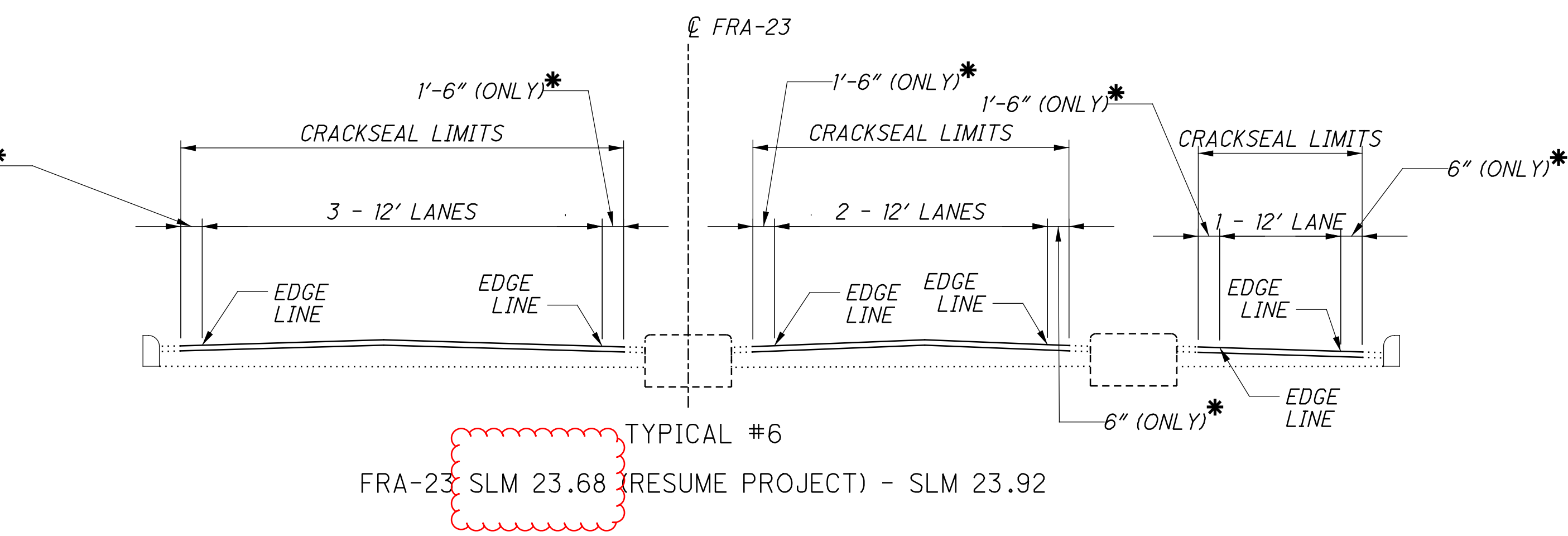
TYPICAL #3
 FRA-23 SLM 22.84 - SLM 22.90 (SUSPEND AT CONCRETE BRIDGE)



TYPICAL #4
 FRA-23 SLM 22.96 (RESUME AFTER CONCRETE BRIDGE) - SLM 23.00



TYPICAL #5
 FRA-23 SLM 23.00 - SLM 23.15 (SUSPEND PROJECT)



TYPICAL #6
 FRA-23 SLM 23.68 (RESUME PROJECT) - SLM 23.92

CRACKSEAL 1'-6" (ONLY) INTO THE INSIDE SHOULDER FOR MAINLINE FRA-23,
 6" (ONLY) INTO THE OUTSIDE SHOULDER FOR MAINLINE FRA-23.
 CRACKSEAL 1'-6" (ONLY) INTO THE INSIDE SHOULDER FOR RAMPS,
 6" (ONLY) INTO THE OUTSIDE SHOULDER FOR RAMPS.



LANE VALUE CONTRACT TABLE:
THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE LANE VALUE CONTRACT TABLE FOR EACH UNIT OF TIME A LANE/SHOULDER/RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE LANE VALUE CONTRACT TABLE.

LANE VALUE CONTRACT TABLE						
FRA-71						
Section (SLM)	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Thur	Fri	Sat-Sun	
Leonard Avenue (18.31) to Cooke Road (23.46)	3	3 to 2	5AM-8PM	9AM-7PM	9AM-7PM	\$520
		3 to 1	5AM-10PM	6AM-10PM	6AM-10PM	\$520
Cooke Road (23.46) to Morse Road (24.51)	3	3 to 2	5AM-8PM	9AM-7PM	9AM-7PM	\$430
		3 to 1	5AM-10PM	9AM-9PM	6AM-9PM	\$430
Morse Road (24.51) to SR 161 (26.46)	3	3 to 2	5AM-8PM	9AM-7PM	9AM-7PM	\$430
		3 to 1	5AM-10PM	9AM-9PM	6AM-9PM	\$430

Short term shoulder closures are NOT permitted 5AM-9AM and 3PM-6PM Monday-Friday.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN:
THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN = 6 SIGN MONTH (ASSUMING 2 PCMS SIGN(S) FOR 3 MONTH(S).)

WORK ZONE PAVEMENT MARKINGS, CLASS III, 642 PAINT:
THESE ITEMS ARE TO BE PERFORMED AT ALL CRACK SEAL LOCATIONS. THE CONTRACTOR SHALL MAINTAIN THESE MARKINGS UNTIL THE PERMANENT MAKINGS CAN BE PLACED IN ACCORDANCE WITH ITEM 642 PAVEMENT MARKINGS.

WORK ZONE LANE LINE, CLASS III, 642 PAINT, 6" = 31.06 MILE
WORK ZONE EDGE LINE, CLASS III, 642 PAINT, 6" = 29.36 MILE

IF EXISTING LANE LINE PAVEMENT MARKINGS ARE COVERED WITH CRACK SEAL MATERIAL, THE LANE SHALL NOT BE OPEN TO TRAFFIC UNTIL THE LINE IS REPLACED.

USE OF WEIGHTED CHANNELIZERS:
THE WEIGHTED CHANNELIZERS MAY BE USED IN ACCORDANCE WITH THIS SECTION. THE WEIGHTED CHANNELIZERS SHALL BE PREDOMINANTLY ORANGE IN COLOR AND SHALL BE MADE OF LIGHTWEIGHT, FLEXIBLE, AND DEFORMABLE MATERIAL. THEY SHALL BE AT LEAST 42 INCHES IN HEIGHT WITH A WEIGHTED BASE. THEY MAY HAVE A HANDLE OR LIFTING DEVICE, WHICH EXTENDS ABOVE THE 42" MINIMUM HEIGHT.

THE MARKINGS ON THE WEIGHTED CHANNELIZERS SHALL BE HORIZONTAL, CIRCUMFERENTIAL, ALTERNATING ORANGE AND WHITE RETRO REFLECTIVE STRIPES 6 INCHES WIDE. EACH WEIGHTED CHANNELIZERS SHALL HAVE A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. ANY NON-RETRO REFLECTIVE SPACES BETWEEN THE HORIZONTAL ORANGE AND WHITE STRIPES SHALL NOT EXCEED 2 INCHES WIDE. THE WEIGHTED CHANNELIZERS SHALL HAVE A 4-INCH MINIMUM WIDTH, REGARDLESS OF ORIENTATION.

USE OF WEIGHTED CHANNELIZERS ON FREEWAYS AND MULTILANE HIGHWAYS SHALL BE LIMITED TO SHORT-TERM OPERATION FOR EITHER DAY OR NIGHT. UPON COMPLETION OF WORK, THE WEIGHTED CHANNELIZERS SHALL BE REMOVED. THE WEIGHTED CHANNELIZERS MAY AGAIN BE PLACED ON THE HIGHWAY WHEN THE WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT. ANY LANE CLOSURE USING CHANNELIZATION DEVICES, EXPECTED TO REMAIN FOR MORE THAN TWELVE HOURS, SHALL REQUIRE THE USE OF DRUMS OR BARRIERS. WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT.

WHEN USED AT NIGHT, WEIGHTED CHANNELIZERS SHALL ONLY BE PLACED IN THE TANGENT AREA. THE TANGENT AREA IS DEFINED AS THE AREA AFTER THE TRANSITION TAPER WHERE THE WORK TAKES PLACE. DRUMS SHALL BE USED IN THE TRANSITION TAPERS FOR NIGHT OPERATIONS. MAXIMUM SPACING OF THE WEIGHTED CHANNELIZERS SHALL BE 40 FEET.

STEPS SHOULD BE TAKEN TO ENSURE THAT THE WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND OR MOVING TRAFFIC.


BALLASTS SHOULD NOT PRESENT A HAZARD IF THE WEIGHTED CHANNELIZERS ARE INADVERTENTLY STRUCK, NOR SHOULD THEY AFFECT THE VISIBILITY OF THE WEIGHTED CHANNELIZERS. ALL BALLASTS USED SHOULD BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

Secondary Route: Interstate Route 270		SLM along 23: (North Side)			
Ramp	Movement	No Closures Allowed		Detour Routes	
		Mon to Fri	Sat to Sun	Primary Route	Secondary Route
270 CD	US-23 NB TO I-270 W (RAMP C)	5AM - 11PM	8AM - 8PM	US-23 N TO I-270 E TO SR-710 TO I-270 W	US-23 N TO SR-750 W TO SR-315 S TO I-270W
270 CD	US-23 S TO I-270 W (RAMP F2)	5AM - 11PM	8AM - 8PM	US-23 S TO SR-161 TO SR-315 N TO I-270 W	US-23 S TO SR-315 S TO SR-161 TO SR-315 N TO I-270 W
B	US-23 NB TO I-270 EB	5AM-7PM	8AM-7PM	US-23 N to 270 W (Ramp C) to SR-315 S (Roadway A) OH-161 (Ramp GA) to OH-315 N to 270 E (Roadway D)	US-23 N to 270 W (Ramp C) to OH-315 N OH-750 to US-23 S to 270 E (Ramp G)
C	US-23 NB TO I-270 WB	5AM-8PM	8AM-7PM	US-23 N to OH-750 to OH-315 to 270 W	US-23 N TO SR-750 W TO SR-315 S TO I-270W
D	I-270 WB TO US-23	5AM-11PM	8AM-8PM	270 W TO SAWMILL RD TO I-270 E TO US-23	270 W TO SR-315S (ROADWAY A) TO SR-161 (RAMP GA) TO SR315 N TO I-270 E TO US-23
F1	US-23 S TO SR-315	5AM - 11PM	8AM - 8PM	US-23 S TO SR-161 W TO SR-315	US-23 S TO I-270 W TO SAWMILL RD TO I-270 E TO SR-315
F2	US-23 S TO I-270 W	5AM - 11PM	8AM-8PM	US-23 S TO SR-161 TO SR-315 N TO I-270 W	US-23 S TO SR-315 S TO SR-161 TO SR-315 N TO I-270 W
F3	US-23 NB & SR-315	5AM - 11PM	8AM - 8PM	US-23 N TO I-270 E TO SR-710 TO I-270 W	US-23 N TO SR-750 W TO SR-315 S TO I-270W
F3	US-23 SB TO SR-315	5AM - 11PM	8AM - 8PM	US-23 S TO SR-161 W TO SR-315	US-23 S TO I-270 W TO SAWMILL RD TO I-270 E TO SR-315
G	US-23 SB TO I-270 EB	5AM-10PM	8AM-8PM	US-23 S TO SR-161 W TO SR-315 N TO 270 E	US-23 S TO 270 W (Ramp F) TO SR-315 S TO SR-161 E TO SR-315 N TO 270 E
H	I-270 EB TO US-23 SB	5AM - 10PM	8AM - 8PM	270 E TO SR-710 TO 270 W TO US-23 (RAMP D)	270 E TO I-71 S TO SR-161 W TO US-23
WBCD	I-270 W TO SR-315	5AM - 11PM	7AM-9PM	I-270 W TO SAWMILL RD TO I-270 E TO SR-315	I-270 W TO TUTTLE CROSSING BLVD. TO I-270 E TO SR-315
WBCD	US-23 N (RAMP C) TO SR-315	5AM - 11PM	7AM-9PM	US-23 N TO I-270 E TO SR-710 TO I-270 W	US-23 N TO SR-750 W TO SR-315 S TO I-270W
WBCD	US-23 S (RAMP F1) TO SR-315	5AM - 11PM	7AM - 9PM	US-23 S TO SR-161 W TO SR-315	US-23 S TO I-270 W TO SAWMILL RD TO I-270 E TO SR-315




SHEET NUMBER						PARTICIPATION		ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
10-11	12-14	15	16	17	18	01/IMS/05							
			413,802	25,309		439,111		423	00204	439,111	SQ YD	PAVEMENT CRACK SEALING, TYPE II	
20,000						20,000		423	98200	20,000	FT	CRACK SEALING, MISC: FILLING WIDE CRACKS AND JOINTS USING HOT APPLIED AGGREGATE REINFORCED MASTIC MATERIAL IN HOT MIX ASPHALT PAVEMENT	
				1.48		1.48		642	00104	1.48	MILE	TRAFFIC CONTROL EDGE LINE, 6", TYPE 1	
				1.35		1.35		642	00204	1.35	MILE	LANE LINE, 6", TYPE 1	
			250			250		614	11110	250	HOURS	MAINTENANCE OF TRAFFIC LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
			6			6		614	18601	6	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	13
			31.06			31.06		614	20560	31.06	MILE	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	
			29.36			29.36		614	22360	29.36	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	
						LS		614	11000	LS		INCIDENTALS MAINTAINING TRAFFIC	
						LS		623	10001	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN	11
						LS		624	10000	LS		MOBILIZATION	

GENERAL SUMMARY

DESIGN AGENCY

 DESIGNER
 RAM
 REVIEWER
 XXX MM-DD-YY
 PROJECT ID
 105838
 SHEET TOTAL
 P.15 | 17

ROUTE	TYPICAL	DISTANCE FT	DISTANCE MILE	AVERAGE WIDTH MILE			423	642		LANE LINE, 6", TYPE 1 MILE	REMARKS	
							CRACK SEALING, TYPE II	EDGE LINE 6", TYPE 1				
							SQ YD	WHITE MILE	YELLOW MILE			
FRA-23		22.84	22.90	3	317	0.06	114			0.30		
FRA-23		22.90	22.96								CONCRETE BRIDGE DECK	
FRA-23		22.96	23.00	4	211	0.04	102	2,394	0.08	0.08	0.12	
FRA-23		23.00	23.15	5	792	0.15	90	7,920	0.30	0.30	0.45	
FRA-23		23.15	23.68								SUSPEND/RESUME PROJECT	
FRA-23		23.68	23.92	6	1,267	0.24	78	10,982	0.24	0.24	0.48	
QUANTITIES CARRIED TO GENERAL SUMMARY									25,309	0.74	0.74	1.35

PLAN SUBSUMMARY

DESIGN AGENCY

 DESIGNER: RAM
 REVIEWER: XXX MM-DD-YY
 PROJECT ID: 105838
 SHEET: P.17 TOTAL: 17