

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

FRA-33-21.71

CITY OF COLUMBUS FRANKLIN COUNTY

FEDERAL PROJECT NUMBER

E201 (228)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

RECONSTRUCTION AND RECONFIGURATION OF 1.07 MILES OF US-33 WB AND 1.26 MILES OF US-33 EB AT THE INTERCHANGE WITH JAMES ROAD AND WINCHESTER PIKE INCLUDING THE CONSTRUCTION OF A SIGNALIZED R-CUT INTERSECTION AT PETZINGER ROAD WITH U-TURNS PROVIDED AT EACH END. WORK INCLUDES RESURFACING OF US-33, JAMES ROAD AND RAMPS, REHABILITATION OF STRUCTURE SFN 2501775 (JAMES ROAD OVER US 33) AND REDECKING OF STRUCTURE SFN 2501805 (WINCHESTER PIKE OVER US 33). WORK ALSO INCLUDES DRAINAGE, LIGHTING, TRAFFIC CONTROL AND SIGNAL IMPROVEMENTS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 27.0 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 1.0 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 28.0 ACRES

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

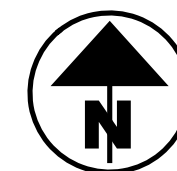
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT AS NOTED ON SHEETS P.88- P.93, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.



LOCATION MAP

LATITUDE: 39°55'35" LONGITUDE: 82°55'25"



PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	
FEDERAL ROUTES	
STATE ROUTES	
COUNTY & TOWNSHIP ROADS	
OTHER ROADS	

DESIGN DESIGNATION

FOR DESIGN DESIGNATIONS, SEE SHEET P.2

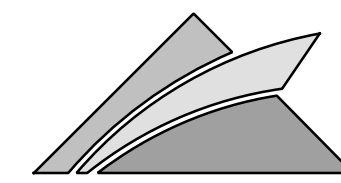
DESIGN EXCEPTIONS

DESIGN FEATURE	APPROVAL DATE	SHEET NUMBERS
VERTICAL CLEARANCE (FRA-33-2276L)	01/17/2023	P.573
HORIZONTAL CURVE RADIUS (US-33 WB)	01/17/2023	P.4 , P.10
HORIZONTAL CURVE RADIUS & SUPERELEVATION (RAMP 1A)	01/17/2023	P.5 , P.10

ADA DESIGN WAIVER

NONE REQUIRED

PLAN PREPARED BY:



GPD GROUP*
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Akron, OH 44311
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STANDARD CONSTRUCTION DRAWINGS										SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS			
BP-2.1	1/21/22	DM-4.3	1/15/16	AS-1-15	1/20/23	MT-95.30	7/18/25	MT-102.20	4/19/19	TC-51.11	7/18/25	800-2023	1/16/26	WATERWAY PERMIT 11/10/25
BP-2.2	1/16/26	DM-4.4	1/15/16	AS-2-15	7/21/23	MT-95.31	7/18/25	MT-103.10	7/18/25	TC-51.12	7/18/25	804	1/16/26	
BP-2.5	1/16/26			EXJ-4-87	1/19/24	MT-95.32	7/18/25	MT-104.10	1/19/24	TC-52.10	10/18/13	809	1/16/26	
BP-3.1	1/19/24	MGS-2.1	1/16/26	GSD-1-19	7/19/24	MT-95.40	7/18/25	MT-105.10	1/17/20	TC-52.20	1/15/21	821	4/20/12	
BP-4.1	7/19/13	MGS-3.1	1/16/26	HW-2.2	7/20/18	MT-95.41	7/18/25	MT-110.10	7/19/13	TC-65.10	1/16/26	832	7/18/25	
BP-5.1	1/16/26	MGS-3.2	7/18/25	SBR-1-20	7/19/24	MT-95.45	7/21/23			TC-65.11	1/17/25	872	1/17/25	
BP-6.1	7/19/13	MGS-4.2	7/18/25	SICD-1-21	1/19/24	MT-97.10	7/18/25	TC-12.31	1/16/26	TC-71.10	1/16/26	878	1/16/26	
BP-7.1	1/16/26	MGS-5.2	7/15/16	SICD-2-14	1/15/21	MT-97.12	1/16/26	TC-15.116	1/16/26	TC-72.20	1/16/26	904	7/15/22	
BP-9.1	1/18/19	MGS-5.3	7/15/16	VPF-1-24	1/17/25	MT-98.29	1/17/20	TC-21.11	1/16/26	TC-73.20	1/16/26	909	1/16/26	
		MGS-6.1	1/19/18			MT-99.20	4/19/19	TC-21.21	7/18/25	TC-74.10	1/16/26	921	7/19/24	
CB-4A, 5A, 8A	7/19/24			HL-20.14	4/17/20	MT-99.30	1/17/20	TC-22.20	7/18/25					
CB-8	7/19/24	RM-1.1	1/16/26	HL-30.31	7/18/25	MT-99.60	7/19/24	TC-41.10	1/16/26					
		RM-3.1	7/20/18			MT-100.00	1/19/24	TC-41.20	10/18/13					
		RM-4.2	1/16/26			MT-101.60	1/17/25	TC-41.30	4/21/23					
DM-1.1	1/17/25	RM-4.3	7/18/25			MT-101.70	7/19/24	TC-41.40	1/16/26					
DM-1.2	1/17/25	RM-4.5	1/16/26			MT-101.75	7/21/23	TC-41.41	7/19/19					
DM-4.1	1/16/26	RM-4.6	7/18/25			MT-101.90	7/17/20	TC-42.10	10/18/13					
DM-4.2	7/20/12					MT-102.10	7/21/23	TC-42.20	10/18/13					

UNDERGROUND UTILITIES
Contact Two Working Days Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

Anthony C. Turowski
Anthony C. Turowski, P.E.
District 06 Deputy Director

Pamela Boratyn
Pamela Boratyn
Director, Department of Transportation

<p>ENGINEER'S SEAL</p> <p>TRAFFIC SIGNALS</p>	<p>ENGINEER'S SEAL</p> <p>BRIDGE</p>	<p>ENGINEER'S SEAL</p> <p>ENTIRE PLAN EXCEPT SHEETS OTHERWISE NOTED</p>
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TITLE SHEET

DESIGN AGENCY	
DESIGNER	ATR
REVIEWER	TMT 12/15/25
PROJECT ID	113744
SHEET	P.1
TOTAL	625

3858-E

FRA-33-21.71

MODEL: Sheet PAPER SIZE: 34x22 (in.) DATE: 4/1/2026 TIME: 10:02:24 AM USER: aluzler C:\Public\OH\DOT\2022\202202113744-00-Engineering\Roadway\Sheets\113744_GTO01.dgn

ITEM 614 - WORK ZONE CROSSOVER LIGHTING SYSTEM

THIS WORK SHALL CONSIST OF FURNISHING, ERECTING, OPERATING, MAINTAINING AND REMOVING A WORK ZONE LIGHTING SYSTEM FOR A SINGLE CROSSOVER, OR OVERLAPPING A PAIR OF CROSSOVERS. THE SYSTEM SHALL BE AS SHOWN ON TRAFFIC SCD MT 100.00. THE CONTRACTOR SHALL ARRANGE FOR AND PAY FOR POWER. ALL MATERIALS AND CONSTRUCTION SHALL COMPLY WITH APPLICABLE PORTIONS OF 625 AND 725 EXCEPT. THE PERFORMANCE TEST OF 625.19F. AND CERTIFIED DRAWING REQUIREMENT OF 625.06, ARE WAIVED AND USED MATERIALS IN GOOD CONDITION ARE ACCEPTABLE.

POLES WHICH ARE NOT PROTECTED BY GUARDRAIL OR PORTABLE BARRIER SHALL BE LOCATED OUTSIDE THE CLEAR ZONE, AND SHOULD BE LOCATED AT LEAST 30 FEET (PREFERABLY 40 FEET) FROM THE EDGE OF PAVEMENT WHEN POSSIBLE. ADDITIONAL POLE LINES, CABLES AND APPURTENANCES NECESSARY TO FURNISH POWER TO THE LIGHTING SYSTEM SHALL BE INCLUDED IN THIS ITEM. SERVICE POLES SHALL BE POSITIONED WITH THE SAME CONSTRAINTS AS THE LIGHTING POLES AS A MINIMUM.

PAYMENT WILL BE MADE AT THE UNIT PRICE PER EACH FOR ITEM 614, WORK ZONE CROSSOVER LIGHTING SYSTEM THROUGHOUT ALL PHASES OF WORK WHEN THE CROSSOVER ROADWAYS ARE USED.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, WORK ZONE CROSSOVER LIGHTING SYSTEM 2 EACH

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 3.25" INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

EARTHWORK FOR MAINTAINING TRAFFIC

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE PLAN FOR INFORMATION ONLY.

EXCAVATION FOR MAINTAINING TRAFFIC 7,477 CY

EMBANKMENT FOR MAINTAINING TRAFFIC 319 CY

WHEN UNDERCUTS ARE NECESSARY FOR MAINLINE PAVEMENT OR EMBANKMENT CONSTRUCTION, EVALUATE THE NEED FOR TEMPORARY ROAD UNDERCUTS IF WITHIN A CLOSE PROXIMITY TO THE MAINLINE UNDERCUTS. A GEOTECHNICAL EVALUATION SHOULD BE CONSIDERED TO DETERMINE IF THE EXISTING SOIL CONDITIONS ARE ADEQUATE TO SUPPORT THE TEMPORARY ROAD. ADDITIONAL SOIL BORINGS ALONG THE TEMPORARY ROAD ARE NOT NORMALLY REQUIRED.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.

THE R11-H5A-48 SIGNS SHALL BE MOUNTED ON 2 NO. 3 POSTS WHEN LOCATED WITHIN CLEAR ZONES.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 2 EACH

ITEM 614 - WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS. FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

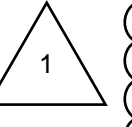
THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.



APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S):

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTION(S) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTION(S) INCLUDE:

ONE (1) NIGHTTIME FULL CLOSURE AND DETOUR OF US-33 WB FOR DECK REMOVAL SHALL BE PERMITTED. ALLOWABLE HOURS FOR THE CLOSURE ARE AS FOLLOWS:

- SUNDAY 10:00 PM TO MONDAY 5:00 AM
- MONDAY 10:00 PM TO TUESDAY 5:00 AM
- TUESDAY 10:00 PM TO WEDNESDAY 5:00 AM
- WEDNESDAY 10:00 PM TO THURSDAY 5:00 AM
- THURSDAY 10:00 PM TO FRIDAY 5:00 AM
- FRIDAY 10:00 PM TO SATURDAY 6:00 AM
- SATURDAY 10:00 PM TO SUNDAY 6:00 AM

ANY CLOSURE OUTSIDE OF THE ALLOWABLE HOURS (OR IN EXCESS OF ONE OCCURRENCE) SHALL BE SUBJECT TO THE DISINCENTIVES AS DETAILED IN THE LANE VALUE CONTRACT TABLE.

SEE DETOUR PLAN SHEET P.89A FOR RAMP CLOSURE AND SPECIFIC TEMPORARY TRAFFIC CONTROL SETUPS REQUIRED TO IMPLEMENT THE CLOSURE.

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AND ODOT AND THE CITY OF COLUMBUS AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED 02/02/2026 FOR PID 113744" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

NOTIFICATIONS DURING CLOSURE REQUIRED

A DESIGNATED ON-SITE POINT OF CONTACT SHOULD COMMUNICATE WITH THE TMC AS THE STATUS OF THE CLOSURE CHANGES.

CONTACT THE TMC:

- IF THE CLOSURE IS POSTPONED OR CANCELLED
- AT THE TIME THE CLOSURE IS IMPLEMENTED
- AT THE TIME THE CLOSURE IS REMOVED AND ALL LANES RESTORED
- IF THE CLOSURE WILL NOT BE OPENING ON TIME

CONTACT CAN BE MADE WITH THE TMC IN THE FOLLOWING WAYS:

- PHONE: 1-614-387-2438
- EMAIL: STATEWIDETMC@DOT.OHIO.GOV
- RADIO: XDOT MAIN

DESIGN AGENCY



DESIGNER
KRM

REVIEWER
AKF 12/15/25

PROJECT ID
107408

SHEET TOTAL
P.47 625

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE PIO (D06.PIO@DOT.OHIO.GOV). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE BUT IS NOT LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME FRAME TABLE			
ITEM	DURATION OF CLOSURE	NOTIFICATION DUE TO DISTRICT 6 COMMUNICATIONS OFFICE	SIGN DISPLAYED TO PUBLIC
ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE	7 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE	2 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE	
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE	
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION	

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

WORK ZONE SIGNING

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE MAINTENANCE OF TRAFFIC GENERAL SUMMARY FOR THE WORK ZONE SIGNING AS SHOWN ON THE MAINTENANCE OF TRAFFIC ELEVATION DETAILS.

- ITEM 630 - SIGN ATTACHMENT ASSEMBLY 2 EACH
- ITEM 630 - SIGN, OVERHEAD EXTRUSHEET 24 SF

ALL MATERIAL, LABOR AND EQUIPMENT REQUIRED TO INSTALL AND SUBSEQUENTLY REMOVE SOLID WOOD POST SUPPORTS (OR APPROVED EQUAL) FOR WORK ZONE SIGNING SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC.

ALL MATERIAL, LABOR AND EQUIPMENT TO REMOVE, ADJUST AND/OR RELOCATE EXISTING OVERHEAD MOUNTED SIGNS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC.

ALL REMAINING WORK ZONE SIGNING AND TEMPORARY SUPPORTS NOT SPECIFICALLY ITEMIZED SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC.

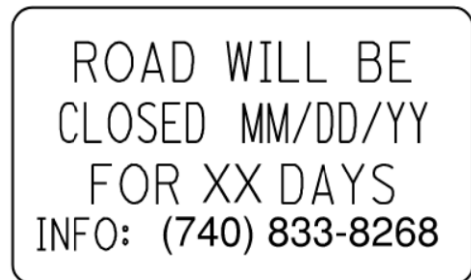
ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS, W20-H13, SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. (AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATION'S OF LESS THAN 1 WEEK.)

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE			
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC	NOTIFICATION DUE TO DISTRICT 6 COMMUNICATIONS OFFICE
RAMP & ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE	4 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN SHALL DISPLAY THE PHONE NUMBER OF THE DISTRICT 6 PUBLIC INFORMATION CONSTRUCTION LINE, (740)833-8268, WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.



W20-H13-60

TIME LIMITATION ON A DETOUR

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED THE CONSECUTIVE CALENDAR DAYS, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEETS P.88 - P.93. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT (SEE THE TABLE BELOW) PER DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

CLOSURE	CALENDAR DAYS TO COMPLETE	DISINCENTIVE \$ PER DAY	WORK WINDOW	
			START	END
NB WINCHESTER PIKE TO WB US-33 (PH 3 & PH 4)	300	\$2,500	PHASE 3	PHASE 4
SB WINCHESTER PIKE (PH 3 & PH 4)	300	\$1,500	PHASE 3	PHASE 4
SB JAMES RD TO US-33 (PH 4 & PH 4A)	90	\$10,000	PHASE 4	PHASE 4A
SB JAMES RD TO REFUGEE RD (PH 4A)	30	\$10,000	PHASE 4A	PHASE 4A
SB US-30 AND SB JAMES ROAD (PRE-PH 5)	7	\$9,000	PRE-PHASE 5	PRE-PHASE 5
EB US-33 TO REFUGEE RD & SB WINCHESTER PIKE (PRE-PH 5 & PH 5)	90	\$10,000	PRE-PHASE 5	PHASE 5

ITEM 614 SPECIAL - WORK ZONE TRAFFIC SIGNAL

UNDER THIS ITEM OF WORK, THE CONTRACTOR SHALL FURNISH, INSTALL, RELOCATE, MODIFY AND SUBSEQUENTLY REMOVE: TEMPORARY SIGNAL SUPPORTS, DOWN GUYS, GROUND RODS, SIGNAL CABLE, POWER CABLE, SERVICE CABLE, CONDUIT RISERS, MESSENGER WIRE, SIGNAL HEADS, COVERING OF VEHICULAR SIGNAL HEADS AND A TEMPORARY CONTROLLER AS NEEDED TO RENDER A FULLY FUNCTIONAL TEMPORARY SIGNALIZED INTERSECTION.

AS DETAILED WITHIN, TEMPORARY TRAFFIC SIGNALS OR TRAFFIC SIGNAL MODIFICATIONS TO ACCOMMODATE INDIVIDUAL MAINTENANCE OF TRAFFIC PHASES SHALL BE INSTALLED AT THE INTERSECTIONS LISTED BELOW.

- US 33 / PETZINGER RD INTERSECTION

ALL TEMPORARY TRAFFIC SIGNAL EQUIPMENT SHALL COMPLY WITH THE SPECIFICATIONS OUTLINED FOR THE PERMANENT SIGNAL INSTALLATION INCLUDING GROUNDING AND BONDING AND "TRAFFIC SIGNAL PLAN AND SPECIFICATION COMPLIANCE". ALL METHODS OF TRAFFIC CONTROL SHALL BE APPROVED BY THE ENGINEER AND SHALL BE IN PLACE AND OPERATING PRIOR TO THE DEACTIVATION AND REMOVAL AND/OR RELOCATION OF ANY EXISTING SIGNAL EQUIPMENT. REFERENCE IS MADE TO THE REQUIREMENTS OF ITEM 614. ALL MODIFICATIONS TO SIGNALIZATION SHALL BE DONE UNDER THE PROTECTION OF A LAW ENFORCEMENT OFFICER. REFERENCE IS MADE TO ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

ANY VEHICULAR TRAFFIC SIGNAL HEAD THAT WILL BE OUT OF OPERATION SHALL BE COVERED IN ACCORDANCE WITH 632.25. ANY EXISTING VEHICULAR OR PEDESTRIAN HEAD THAT IS NOT FUNCTIONAL SHALL BE REMOVED IMMEDIATELY OR COVERED. ANY PEDESTRIAN BUTTONS NOT IN USE SHALL ALSO BE COVERED.

EACH TEMPORARY SIGNAL POLE LOCATION SHALL BE STAKED AND THE LOCATION APPROVED BY THE CITY OF COLUMBUS. THE CONTRACTOR MAY REUSE EXISTING SPAN AND PIGTAILS OR INSTALL NEW AS REQUIRED. THE CONTRACTOR SHALL TRANSFER EXISTING SIGNAL ITEMS AND EXTEND EXISTING CABLE AS NEEDED. WEATHERPROOF CABLE SPLICING IS PERMITTED. DOWN GUYS SHALL BE SPECIFIED FOR ALL TEMPORARY WOOD POLES. ONE DOWN GUY PER POLE SHALL BE USED FOR A LAYOUT THAT CONTAINS A MAXIMUM OF 2 VEHICULAR SIGNAL HEADS PER SPAN. TWO DOWN GUYS PER POLE SHALL BE SPECIFIED FOR 3 OR MORE VEHICULAR SIGNAL HEADS PER SPAN. DOWN GUYS SHALL BE POSITIONED TO COUNTERACT THE MOMENT CREATED BY THE SPAN CONFIGURATION. ANY CHANGE TO THE PLANNED POLE LOCATION OR SPAN CONFIGURATION AS DETAILED IN THE PLAN SHALL BE APPROVED BY THE CITY OF COLUMBUS. THE CONTRACTOR SHALL SUBMIT A DIAGRAM TO THE CITY DOCUMENTING PROPOSED CHANGES.

INSTALL THE SPAN TO PROVIDE FOR A 5 TO 6 PERCENT SAG FOR WOOD POLES. ATTACH THE SPAN NO CLOSER THAN 2 FT. FROM THE TOP OF THE POLE. THE LOWEST VEHICULAR HEAD IN EACH DIRECTION SHALL BE 16.5 FT. ABOVE PAVEMENT SURFACE WITH THE REMAINING VEHICULAR HEADS MEETING THE REQUIREMENTS OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

THE CONTRACTOR SHALL SHIFT EXISTING SIGNAL HEADS TO ALIGN WITH LANES IN THE INDIVIDUAL MAINTENANCE OF TRAFFIC PHASES. DETAILED HEAD PLACEMENT HAS BEEN PROVIDED FOR EACH PHASE OF WORK IN THE MAINTENANCE OF TRAFFIC PLAN. THIS ITEM SHALL CONSIST OF ADJUSTING THE LOCATION OF TEMPORARY TRAFFIC SIGNAL HEADS FOR EACH PHASE OF CONSTRUCTION INCLUDING UNLASHING AND RELASHING ALL WIRING. ALL TEMPORARY AERIAL WIRING SHALL BE A MINIMUM OF 21 FT. ABOVE THE ROADWAY SURFACE.

VEHICULAR DETECTION SHALL BE MAINTAINED AT ALL TIMES AND DURING ALL PHASES OF CONSTRUCTION USING EITHER EXISTING LOOP DETECTORS OR TEMPORARY VIDEO OR RADAR DETECTION.

LOCATE THE NON-FUSED POWER SUPPLY VOLTAGE (120 VOLT) IN A SEPARATE CONDUIT. IN ADDITION, LOCATE THE LOOP DETECTOR, PUSH BUTTON, AND VIDEO DETECTION CABLES IN A SEPARATE CONDUIT FROM ALL OTHER CABLES.

THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY TO PROVIDE POWER TO THE TRAFFIC SIGNAL CONTROLLER FROM THE PROPOSED OR EXISTING POWER SOURCES AS DETERMINED BY CONSTRUCTION SEQUENCING.

THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, EQUIPMENT AND MATERIALS NECESSARY TO FURNISH, INSTALL, MODIFY, REMOVE, STORE, ERECT, RELOCATE, ADJUST AND REPAIR TEMPORARY TRAFFIC SIGNAL ITEMS AS DESCRIBED ABOVE.

ITEM 614 SPECIAL - WORK ZONE TRAFFIC SIGNAL (CONTINUED)

ALL COSTS FOR THE ABOVE WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 614 SPECIAL - WORK ZONE TRAFFIC SIGNAL AND SHALL BE PER EACH INTERSECTION AT WHICH TEMPORARY SIGNALS OR MODIFICATIONS ARE INSTALLED. (8/27/21)

DESIGN AGENCY



DESIGNER
KRM

REVIEWER
AKF 12/15/25

PROJECT ID
107408

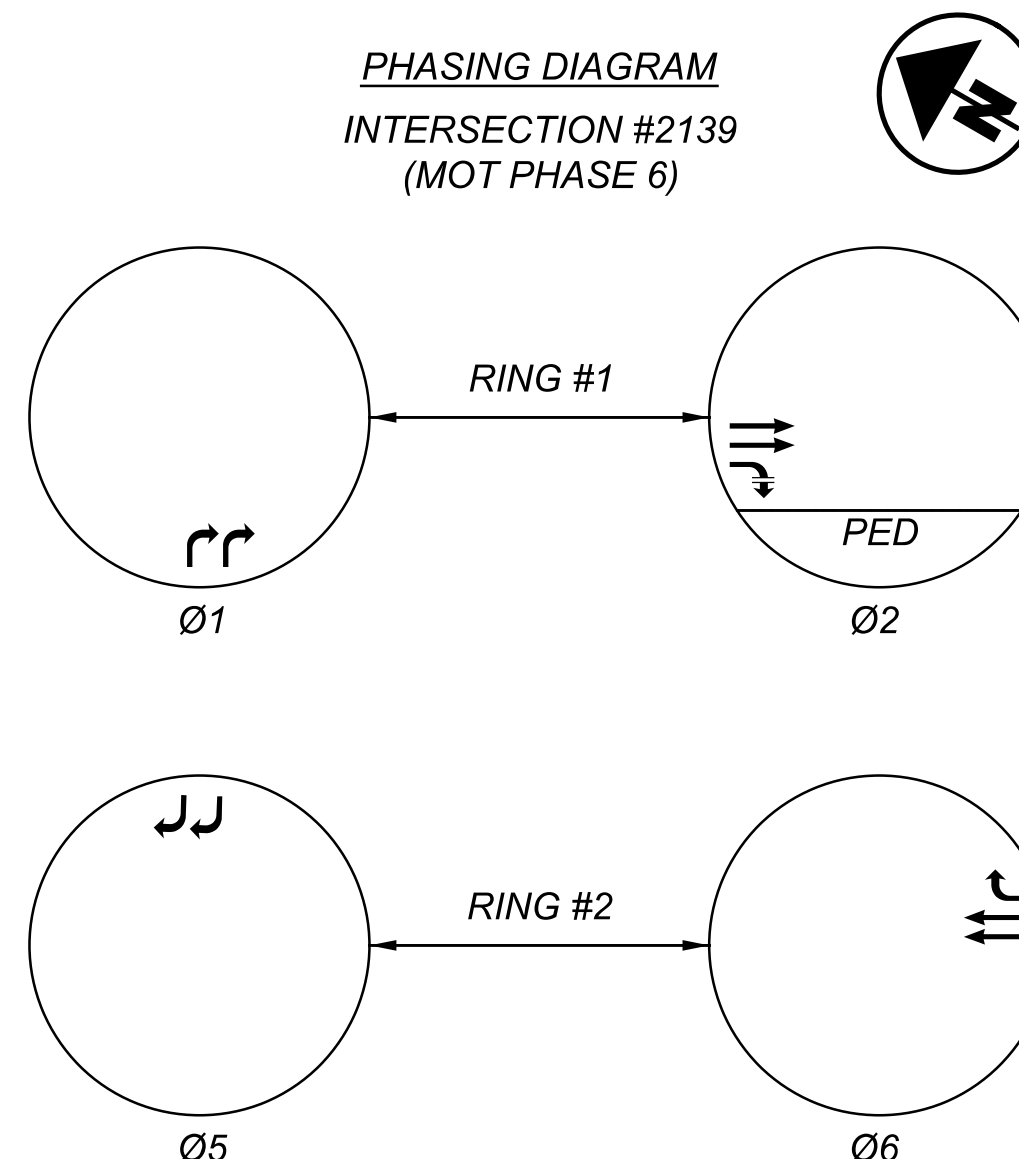
SHEET TOTAL
P.50 625

NOTES:

1. AN EXISTING TRAFFIC SIGNAL, OR ANY PART THEREOF, SHALL NOT BE TAKEN OUT OF SERVICE UNLESS ALTERNATE MEANS OF TRAFFIC CONTROL ARE IN PLACE AND OPERATIONAL. UNLESS DIRECTED BY THE CITY OF COLUMBUS CITY ENGINEER OR APPOINTED DESIGNEE, EXISTING TRAFFIC SIGNALS SHALL NOT BE TAKEN OUT OF SERVICE BETWEEN THE HOURS OF:
 - 7:00 AM TO 9:00 AM MONDAY THROUGH FRIDAY AND
 - 4:00 PM TO 6:00 PM, MONDAY THROUGH FRIDAY (3:30 PM 6:00 PM FOR THE DOWNTOWN BUSINESS DISTRICT)
 - OR ONE HOUR BEFORE SUNSET THROUGH ONE-HALF HOUR AFTER SUN RISE, WHICHEVER IS THE LONGEST DURATION.
2. ANY UNUSED SIGNAL HEAD, PEDESTRIAN SIGNAL HEAD, PEDESTRIAN PUSHBUTTON, OR POLE OR SPAN/ARM MOUNTED SIGN SHALL BE COVERED AND DISCONNECTED. VEHICULAR SIGNAL HEADS AND PEDESTRIAN SIGNAL HEADS SHALL BE COVERED PER THE REQUIREMENTS OF CMSC 632.25.
3. VEHICULAR SIGNAL HEADS SHALL BE ALIGNED PER THE PLAN. NO TWO VEHICULAR SIGNAL HEADS SHALL BE LOCATED WITHIN EIGHT FEET OF ONE ANOTHER, MEASURED PERPENDICULAR TO THE TRAVEL LANE.
4. SIGNS SHALL BE ALIGNED PER THE PLANS. EXISTING SIGNS IN CONFLICT WITH THE PROPOSED TEMPORARY TRAFFIC CONTROL SETUP SHALL BE COVERED OR TEMPORARILY REMOVED
5. WEATHERPROOF SPLICES MAY BE INTRODUCED INTO SIGNAL CABLE IN ORDER TO RELOCATE EXISTING VEHICULAR SIGNAL HEADS. NO SPLICES SHALL REMAIN IN THE CABLE WHEN THE SIGNAL HEADS ARE RETURNED TO THEIR ORIGINAL POSITION.
6. ALL EXISTING VEHICULAR DETECTION SHALL BE MAINTAINED AT ALL TIMES. LOOPS THAT CANNOT BE USED AS A RESULT OF LANE SHIFTS, LANE CLOSURES, ETC. SHALL BE DEACTIVATED DURING CONSTRUCTION. TEMPORARY RADAR OR VIDEO DETECTION SHALL BE USED TO MAINTAIN DETECTION WHEN AN EXISTING LOOP CANNOT BE USED. IF THE EXISTING DETECTION IS RADAR OR VIDEO, THE ZONES ON THE EXISTING RADAR OR VIDEO UNIT SHALL BE RELOCATED TO THE NEW LANE ALIGNMENT. WHEN TEMPORARY RADAR DETECTION IS USED, DILEMMA ZONE DETECTION SHALL BE PROVIDED FOR APPROACHES WITH SPEEDS GREATER THAN 40 MPH.
7. EXISTING PEDESTRIAN PUSHBUTTONS, PUSHBUTTON SIGNS, AND SIGNAL HEADS SHALL BE MAINTAINED FOR ALL CROSSWALKS THAT REMAIN OPEN DURING CONSTRUCTION. TEMPORARY PUSHBUTTONS AND SIGNS OR RELOCATED PUSHBUTTONS AND SIGNS SHALL BE POSITIONED ACCORDING TO THE CITY OF COLUMBUS ADA RULES AND REGULATIONS. RELOCATED PEDESTRIAN SIGNAL HEADS SHALL BE POSITIONED SUCH THAT THE HEAD IS AIMED AT THE CENTER OF THE CROSSWALK AREA (NOT THE CURB RAMP) THAT IS OPPOSITE THE UNIT. A MINIMUM OF ONE CROSSWALK TO CROSS EACH STREET AT A SIGNALIZED INTERSECTION SHALL BE MAINTAINED AT ALL TIMES. FOR SIGNALIZED INTERSECTIONS WITH THREE LEGS, THE CROSSWALK TO CROSS THE DEAD END STREET MAY BE CLOSED AS LONG A PEDESTRIAN PATH IS PROVIDED ALONG THE "TOP SIDE" OF THE INTERSECTION.
8. UNLESS NOTED IN THE PLANS, THE TRAFFIC SIGNAL SHALL UTILIZE THE EXISTING TIMING AND PHASING.
9. IF ANY CHANGES ARE MADE TO THE SIGNAL OPERATION INCLUDING PHASING CHANGES, PHASE OMISSIONS, TIMING CHANGES, ETC., SIGNAL OPERATION CHANGED SIGNS (W23-H2B) SHALL BE INSTALLED ON THE SPAN OR ARM FOR ALL DIRECTIONS. CENTER THE SIGN OVER THE APPROACH. SIGN SHALL BE LEFT IN PLACE NO LONGER THAN THE DURATION SPECIFIED UNDER ITEM 630 SIGNING, MISC.: TRAFFIC SIGNAL SIGNS.

NOTE TO CONTRACTOR:
 IT IS THE DESIGNERS INTENT TO USE THE PROPOSED TRAFFIC SIGNAL INFRASTRUCTURE FOR THE MOT PHASE 6 TEMPORARY SIGNAL WITH THE PROPOSED SIGNAL MODIFICATIONS AS SHOWN ON THIS SHEET.

PHASING DIAGRAM
 INTERSECTION #2139
 (MOT PHASE 6)



PERMITTED VEHICULAR MOVEMENT
 PROTECTED VEHICULAR MOVEMENT
 PEDESTRIAN MOVEMENT

TIMING CHART

MOVEMENT	EB	SB	-	-	WB	NB	-	-
PHASE	Ø1	Ø2	-	-	Ø5	Ø6	-	-
MIN. INITIAL	10	20	-	-	10	20	-	-
WALK	-	-	-	-	9	-	-	-
PED CHANGE	-	-	-	-	17	-	-	-
PASS / EXT	-	-	-	-	-	-	-	-
YELLOW	3.4	6.0	-	-	3.4	6.0	-	-
RED CLR	1.0	1.0	-	-	1.0	1.0	-	-
MAX GRN 1	25	45	-	-	25	45	-	-
MAX GRN 2	25	60	-	-	25	60	-	-
PED RECALL	OFF	ON	OFF	OFF	OFF	ON	OFF	OFF
VEH RECALL	OFF	MIN	OFF	OFF	OFF	MIN	OFF	OFF
MEMORY	OFF	ON	OFF	OFF	OFF	ON	OFF	OFF

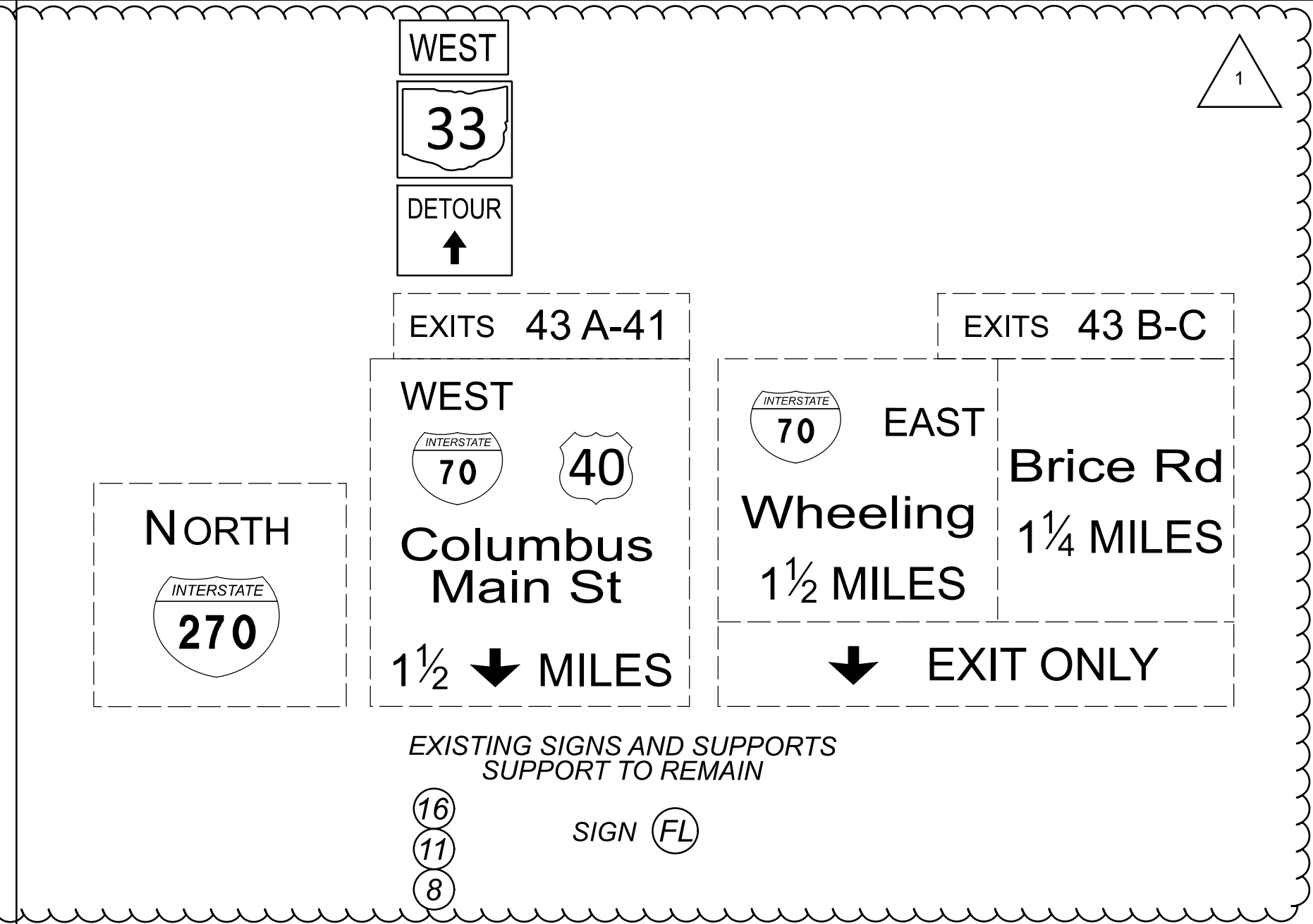
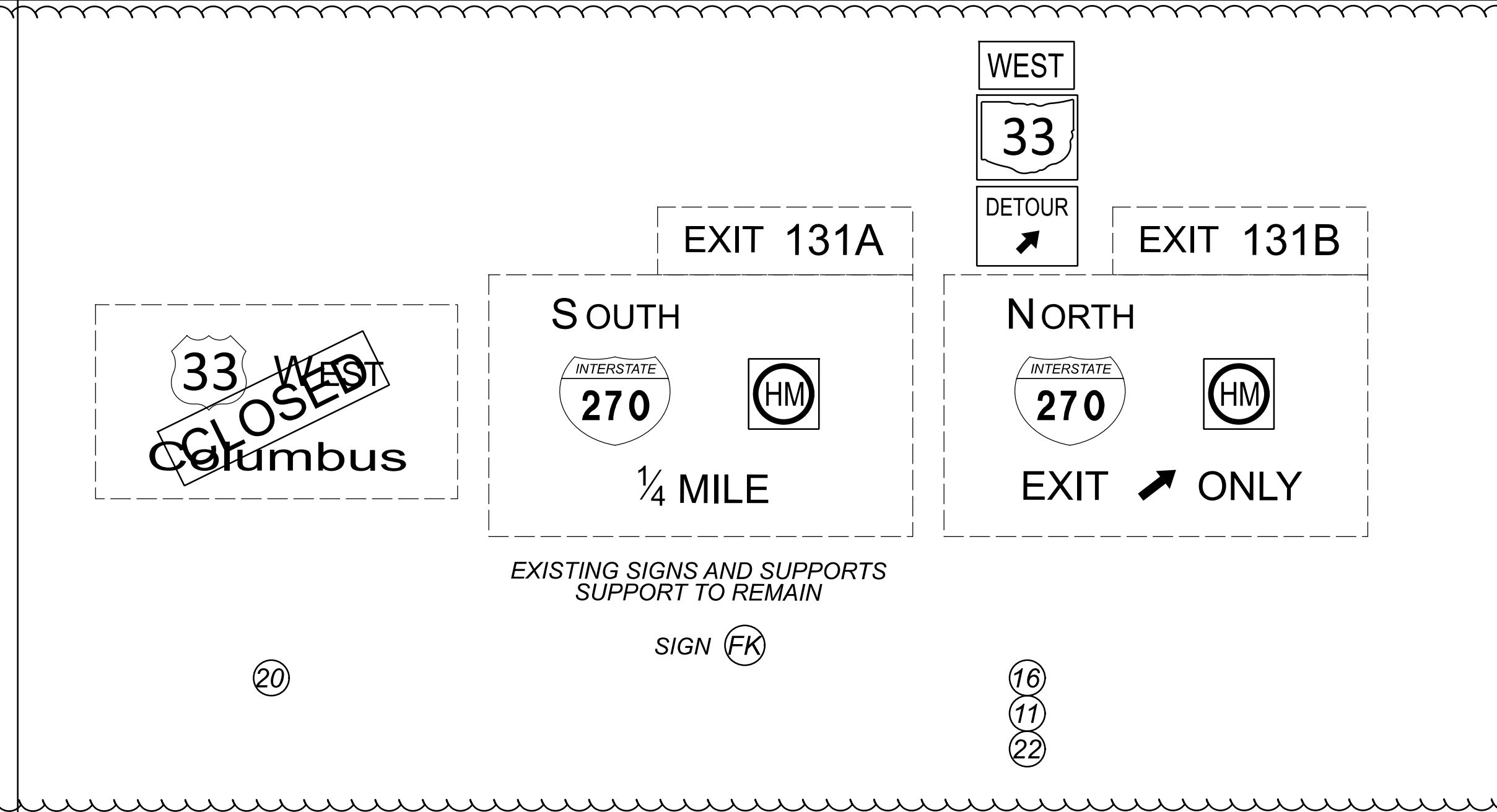
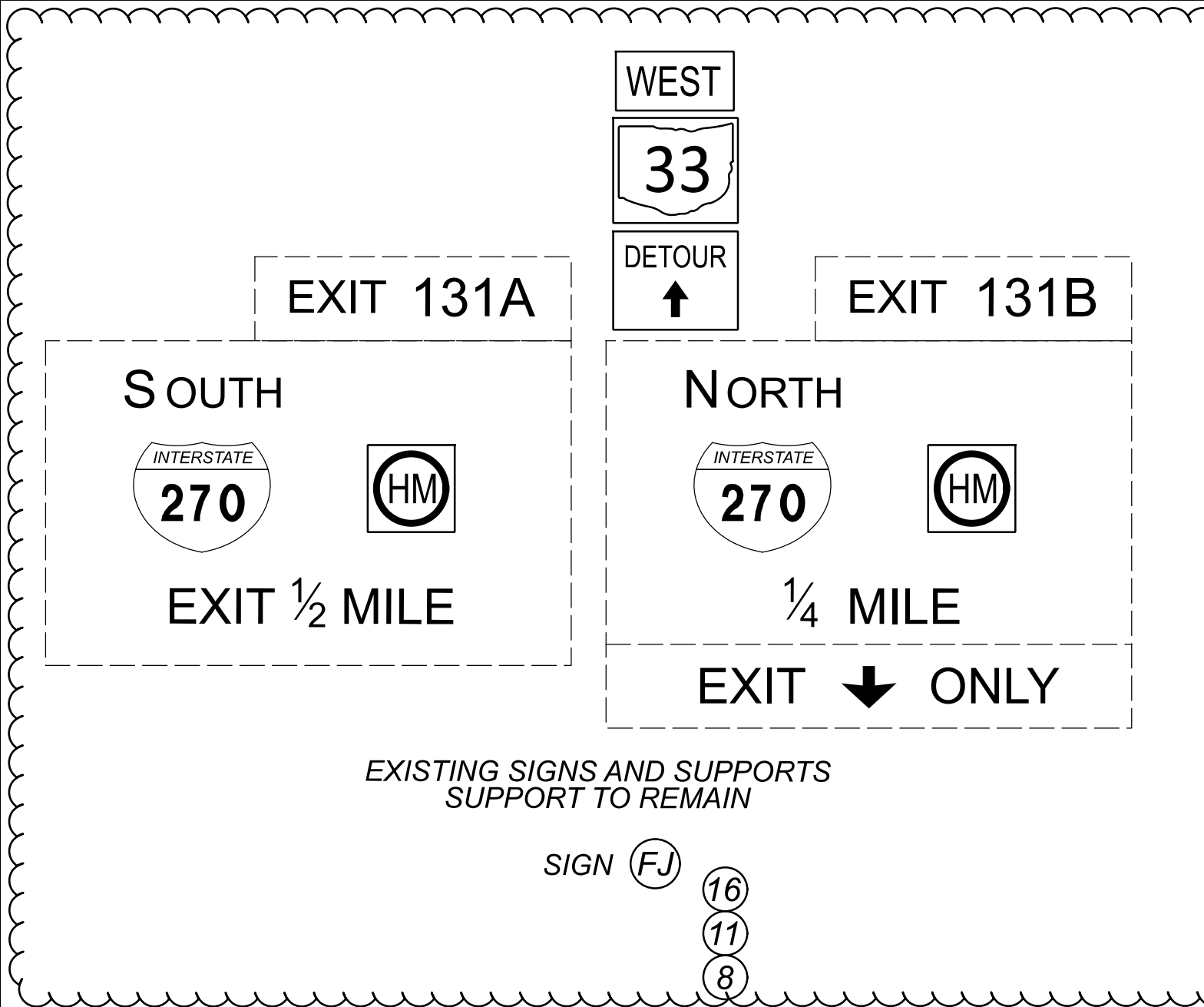
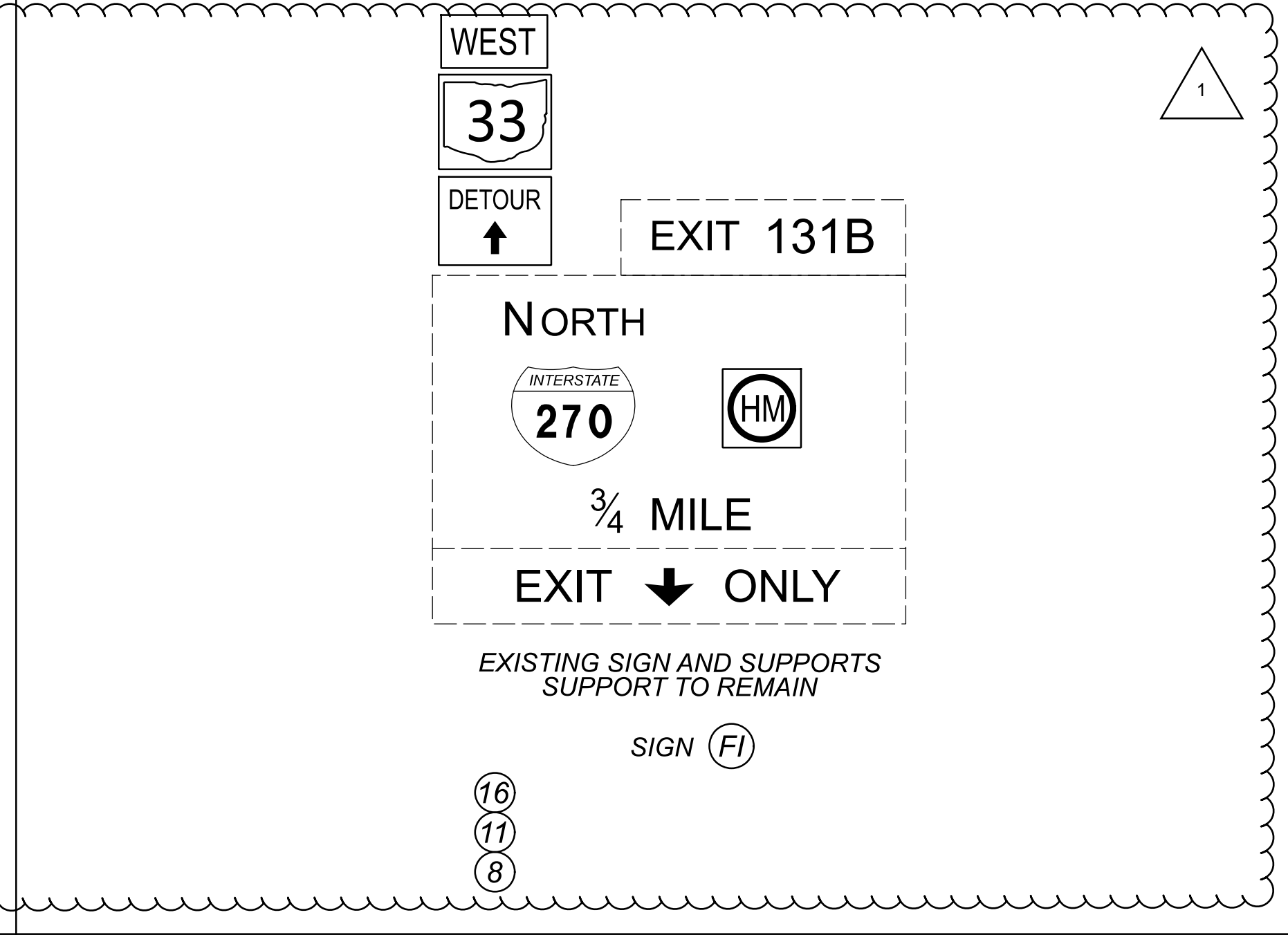
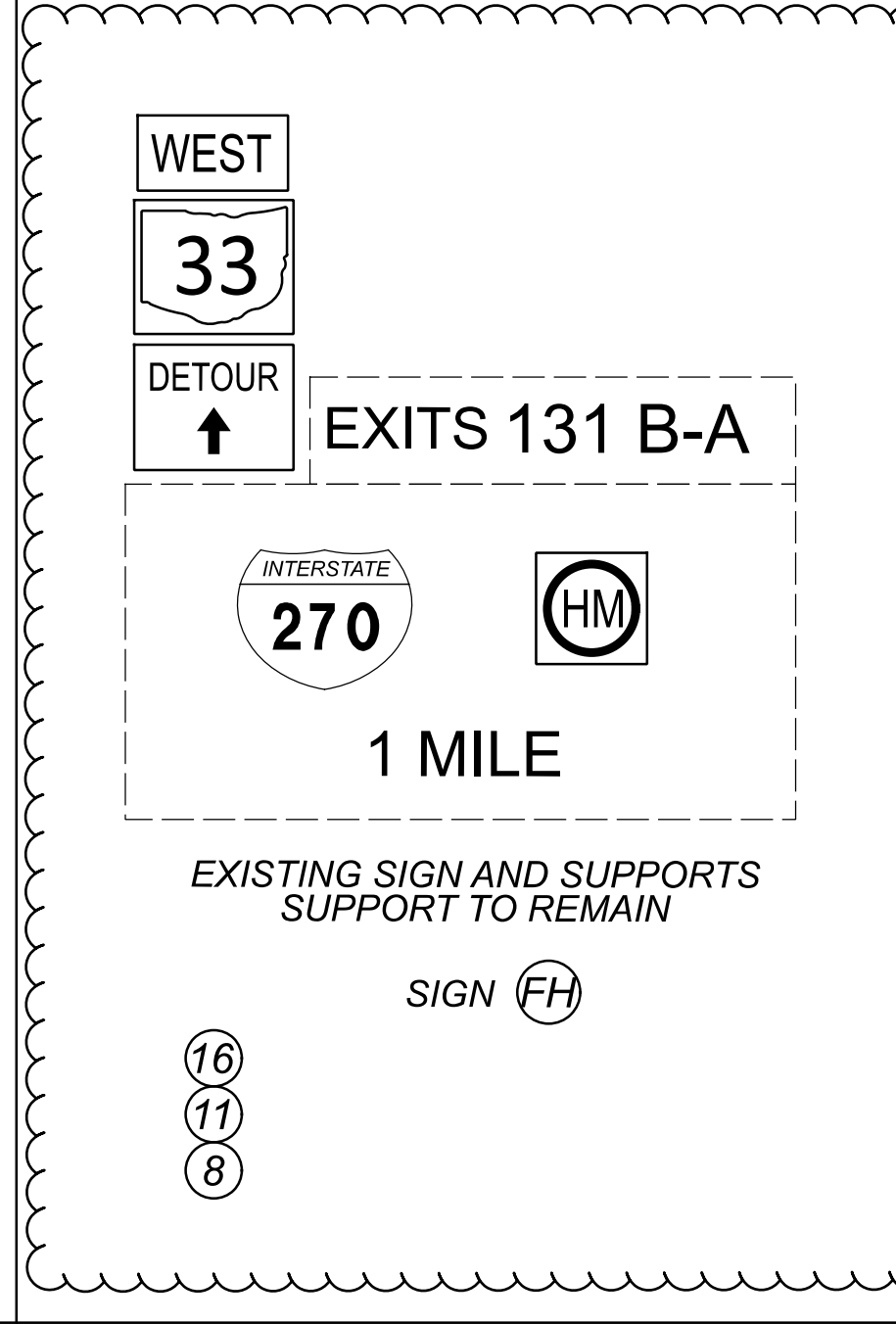
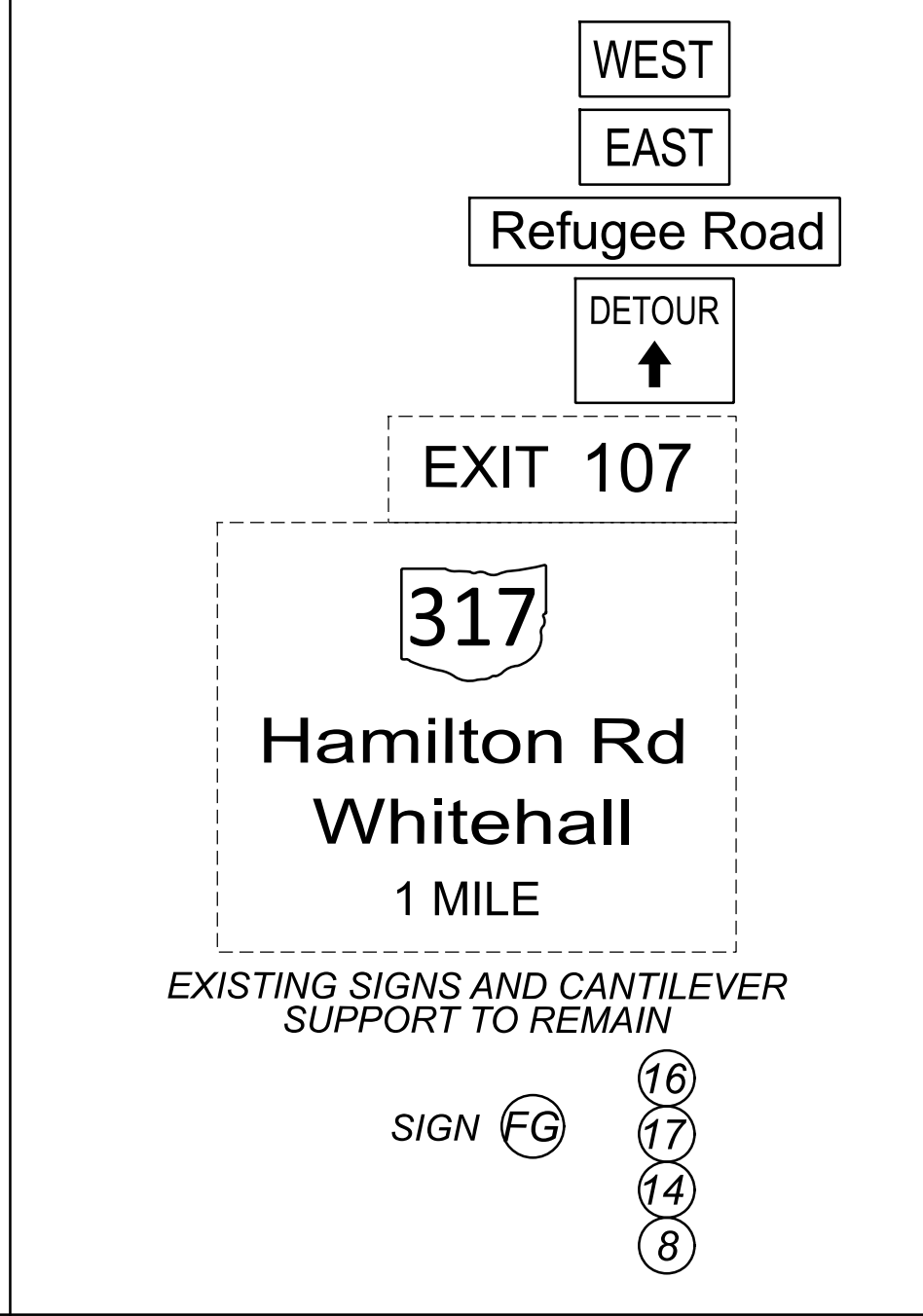
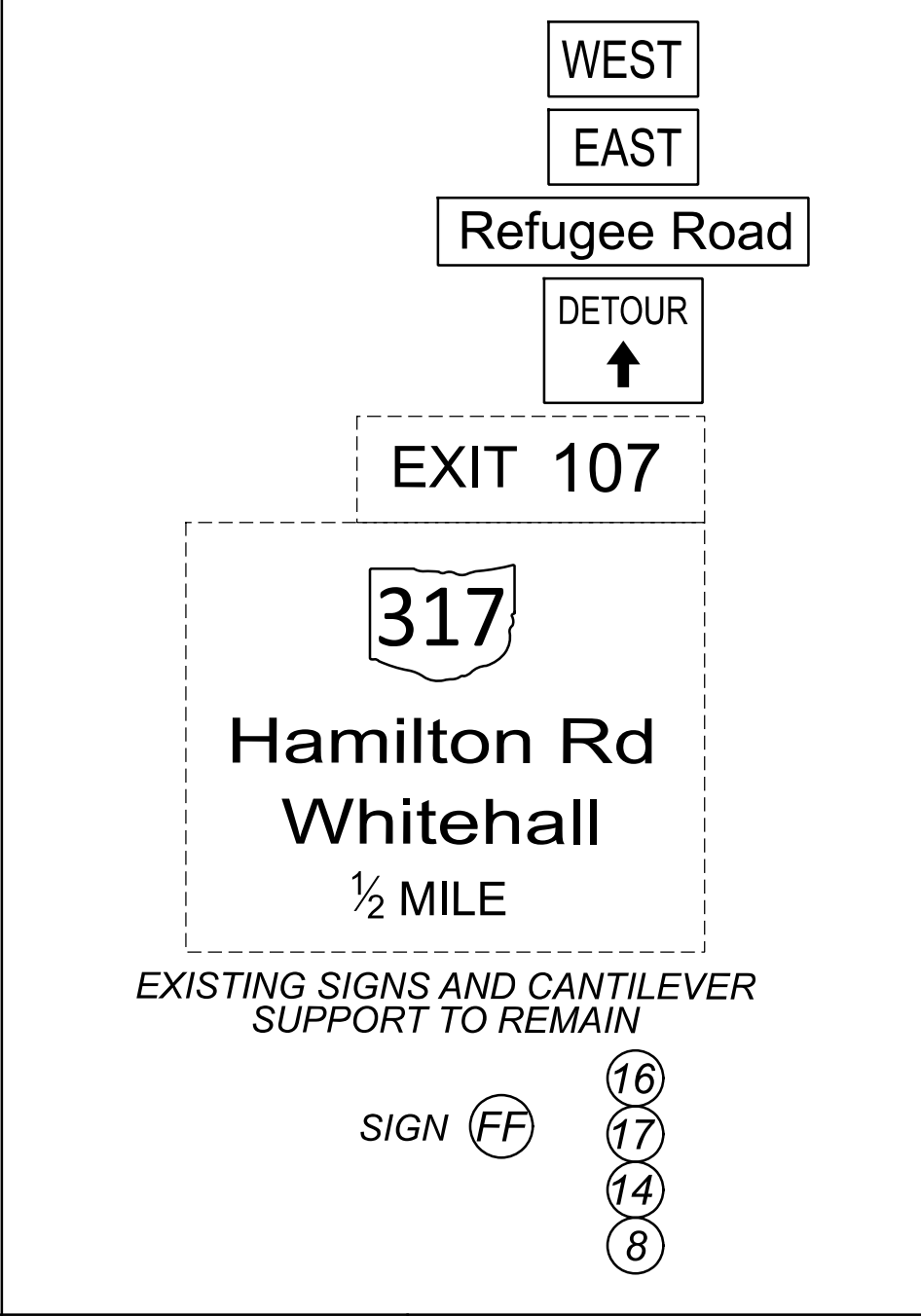
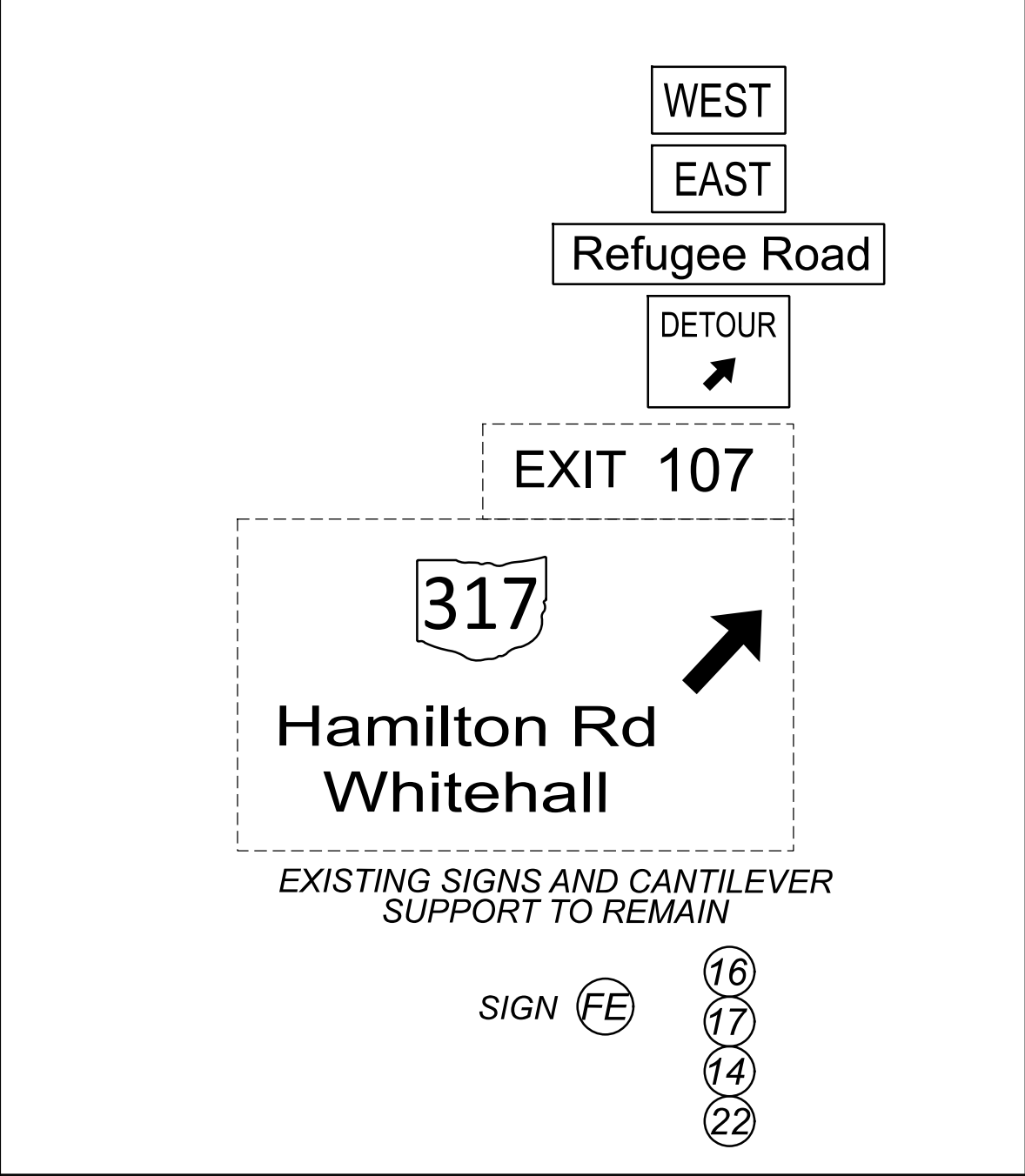
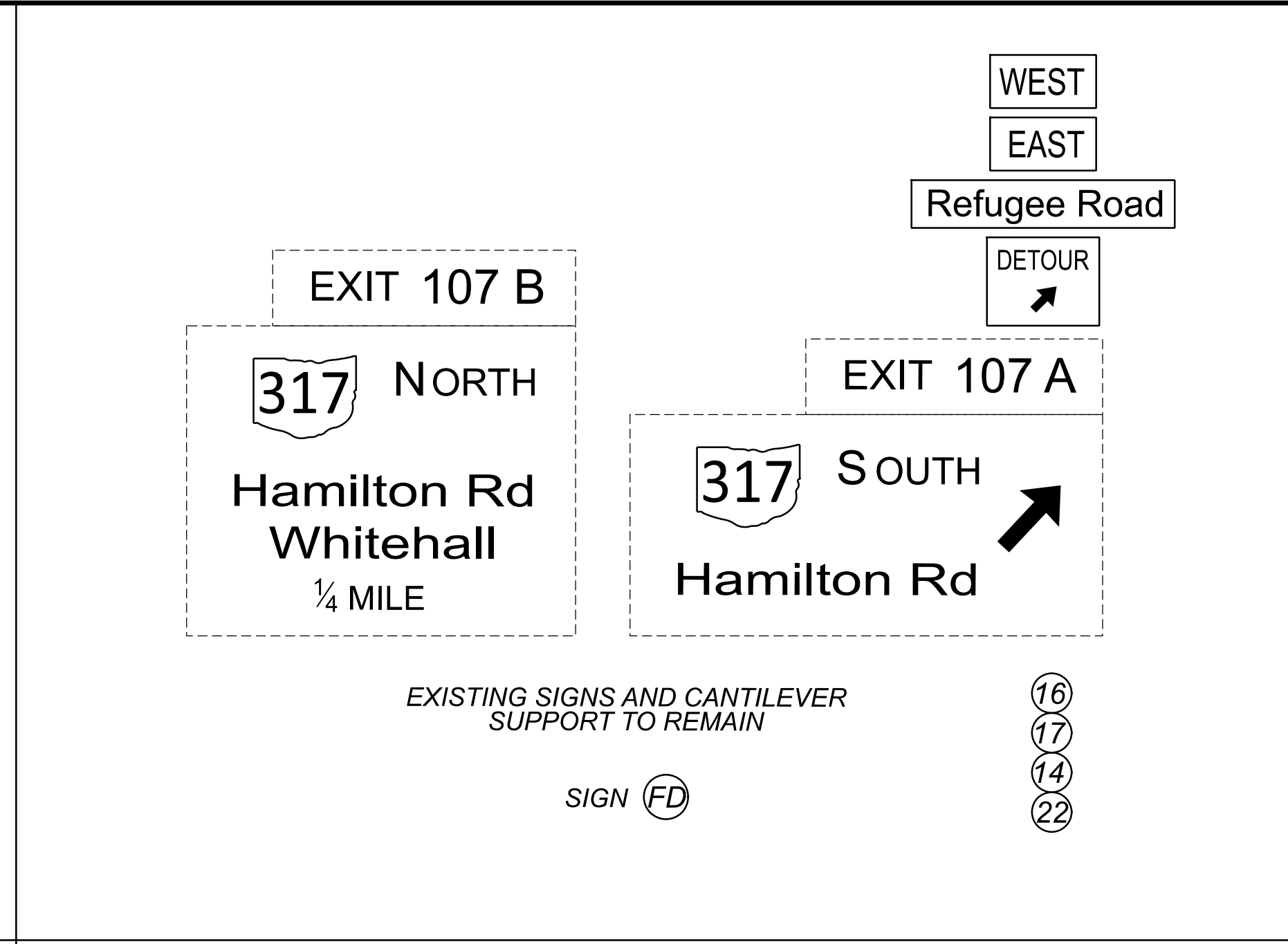
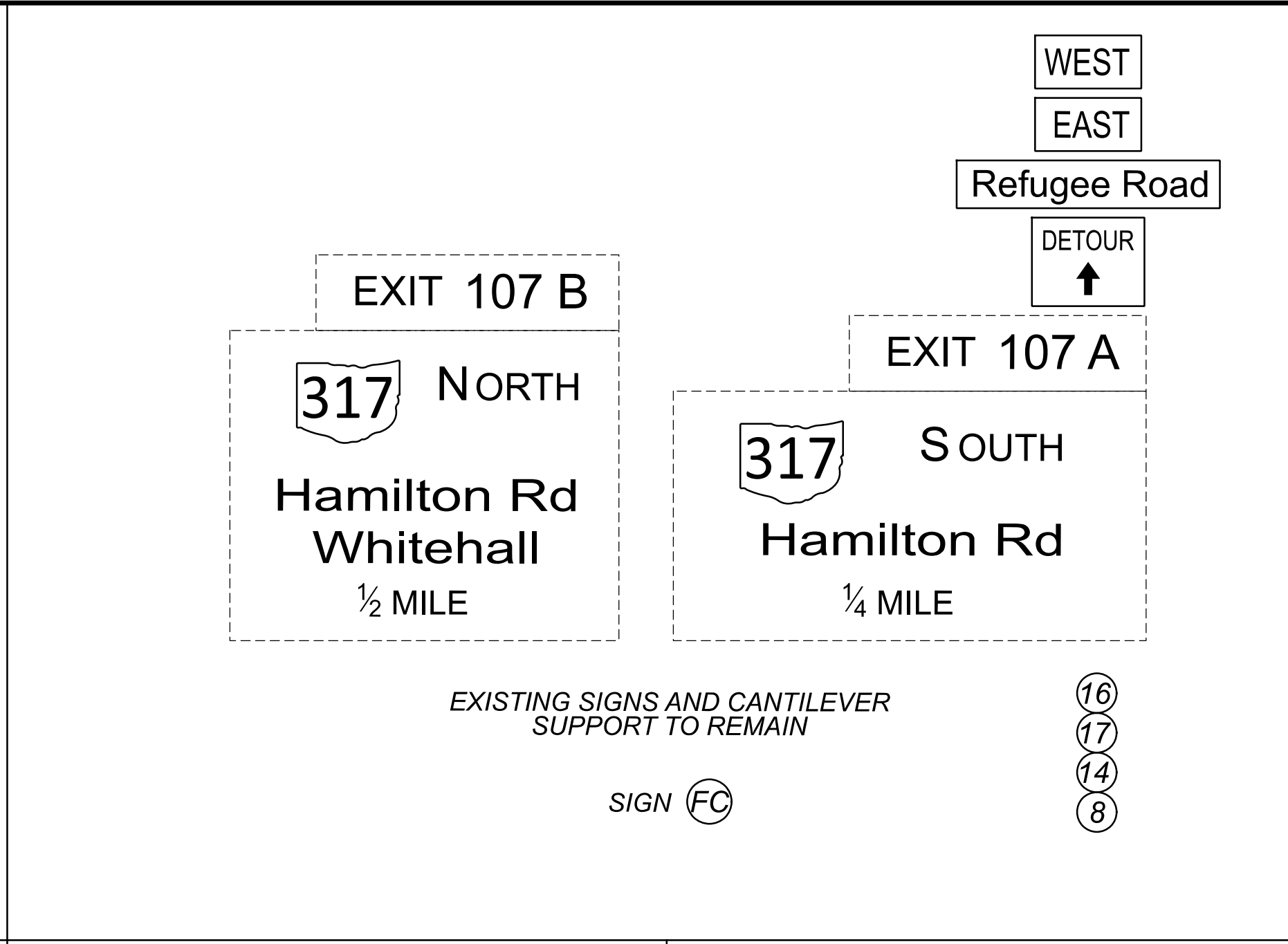
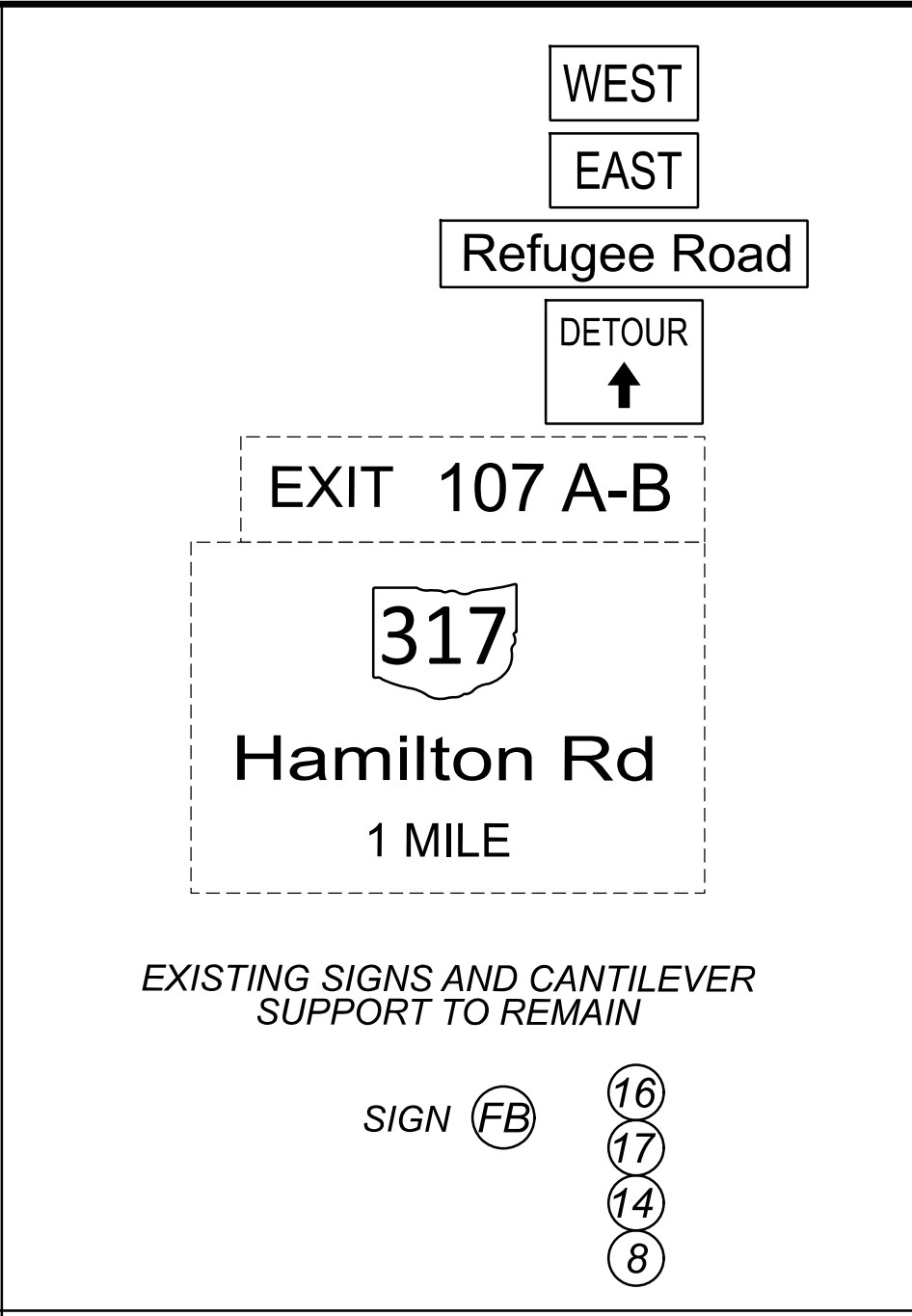
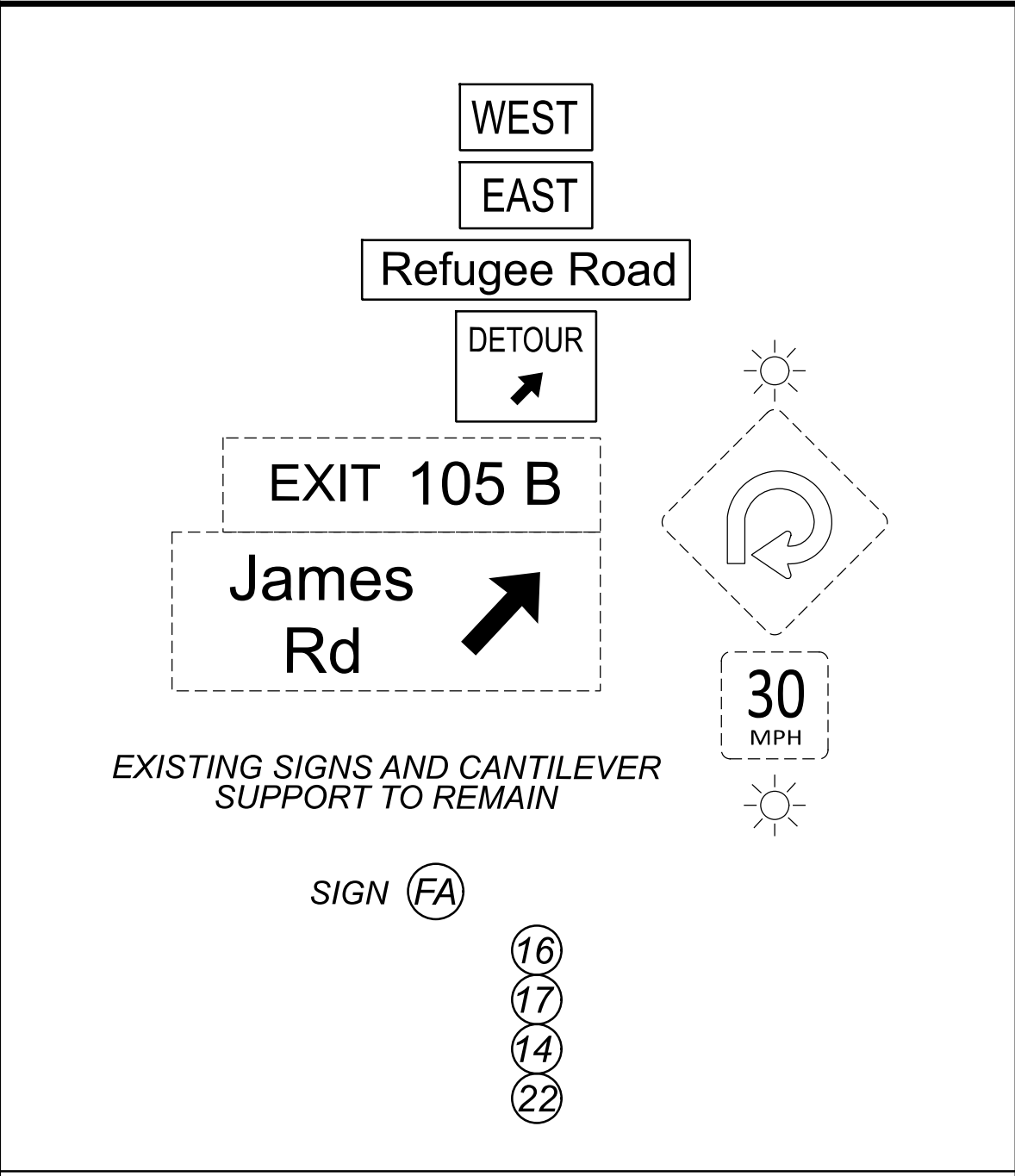
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DESIGN AGENCY	JAH
DESIGNER	JAH
REVIEWER	KMG 12-15-25
PROJECT ID	113744
SHEET TOTAL	P.81 625

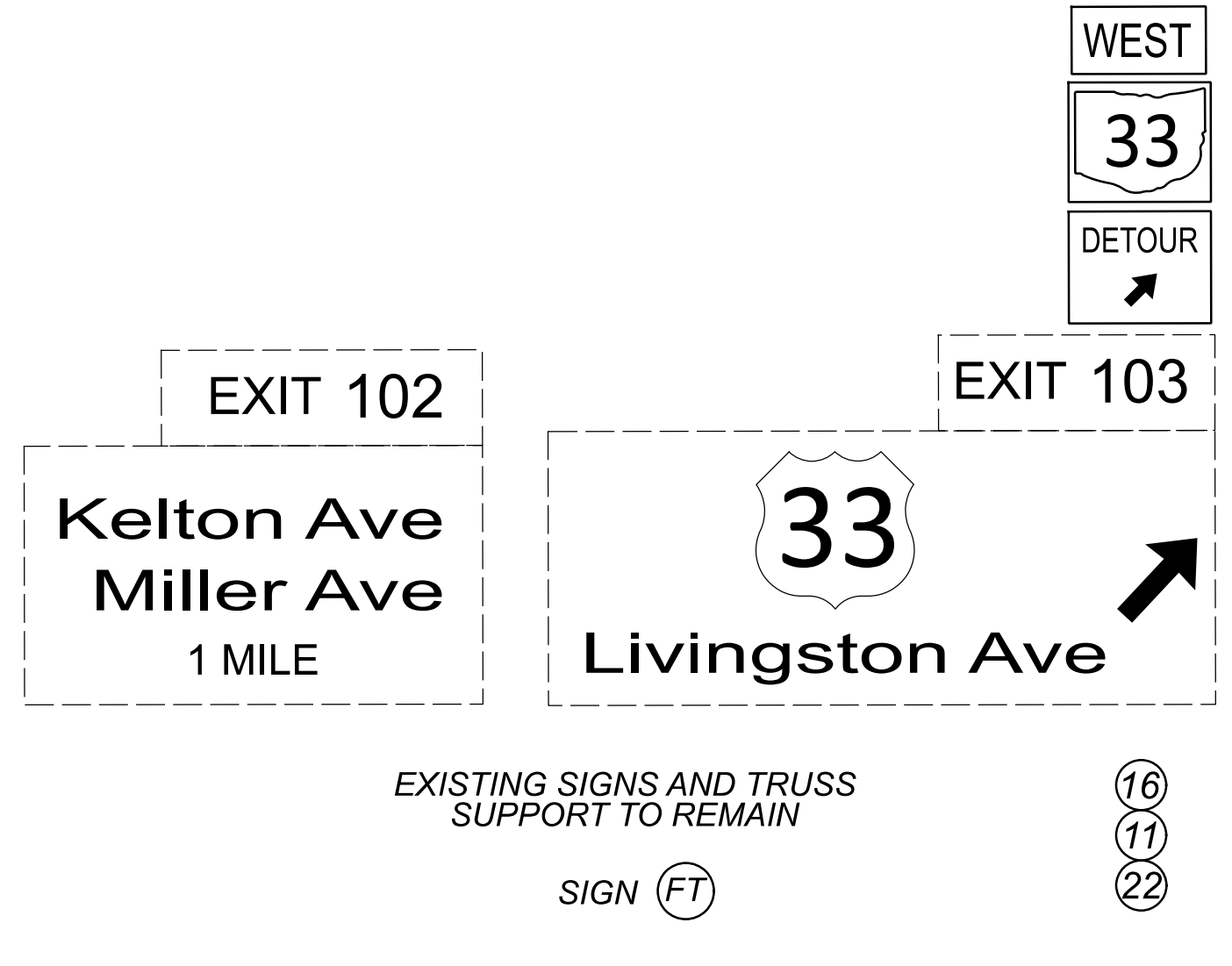
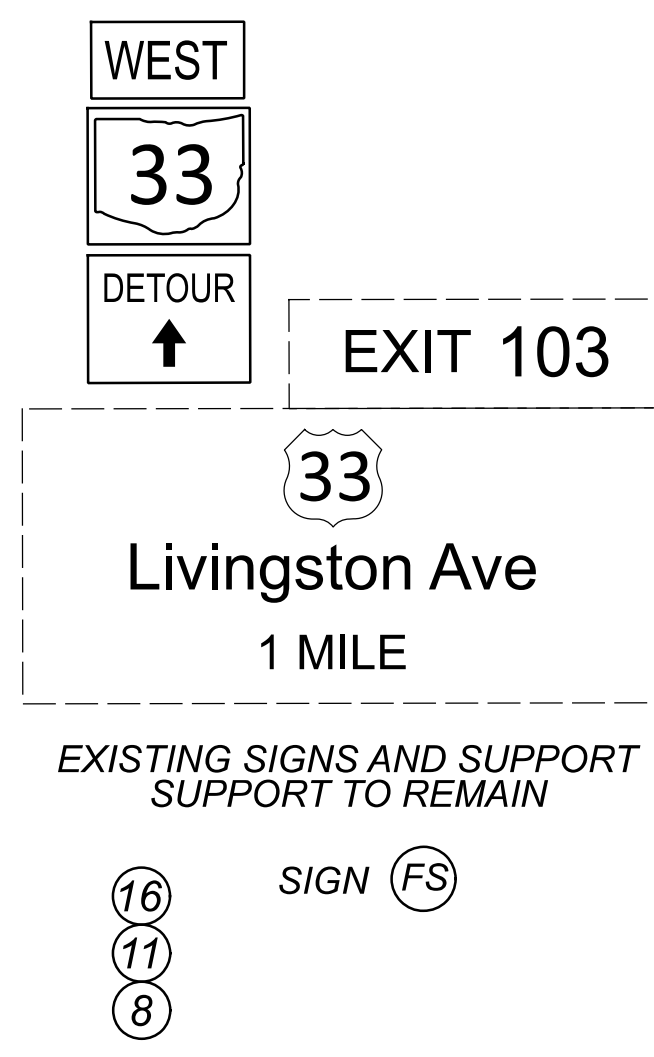
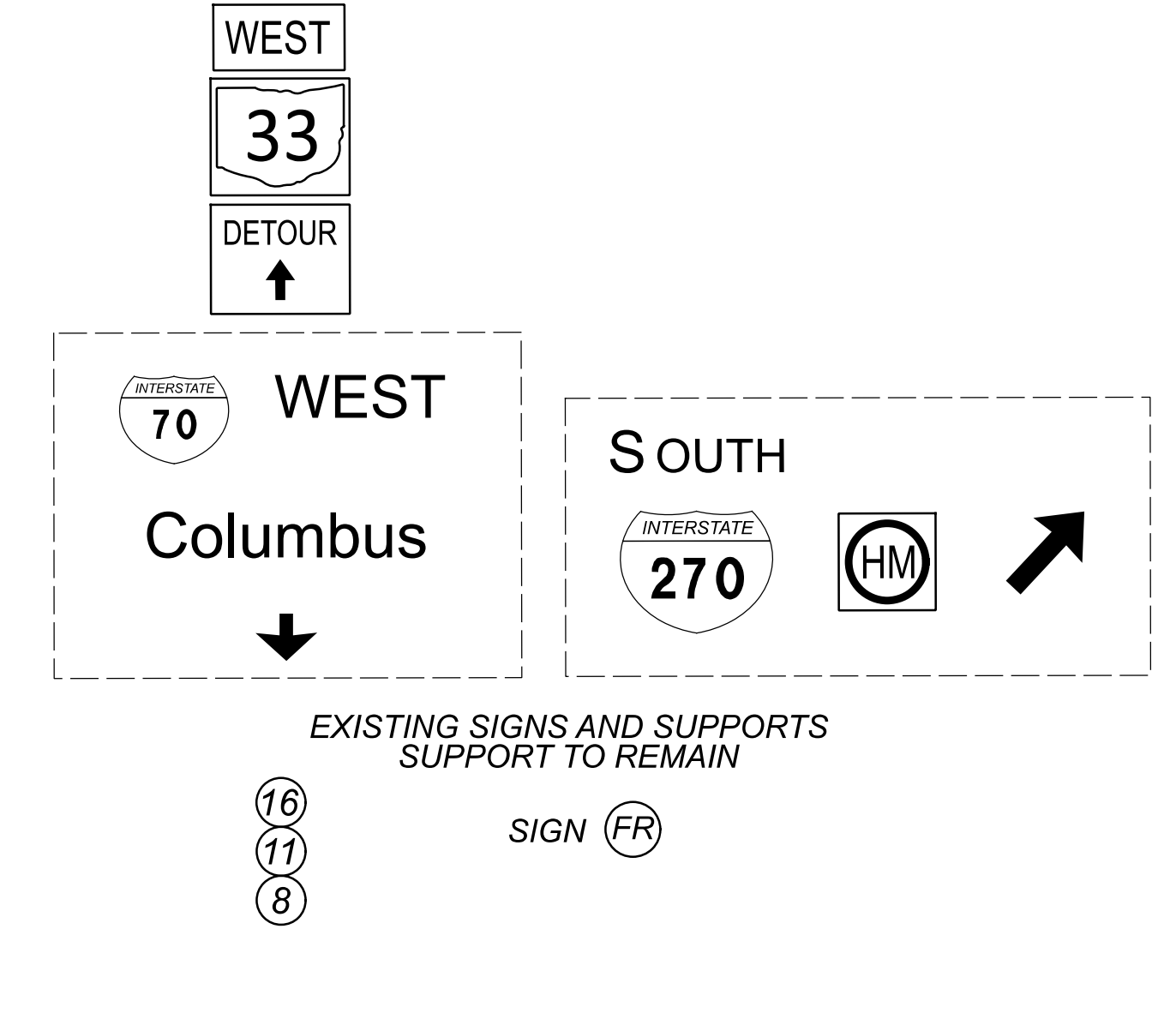
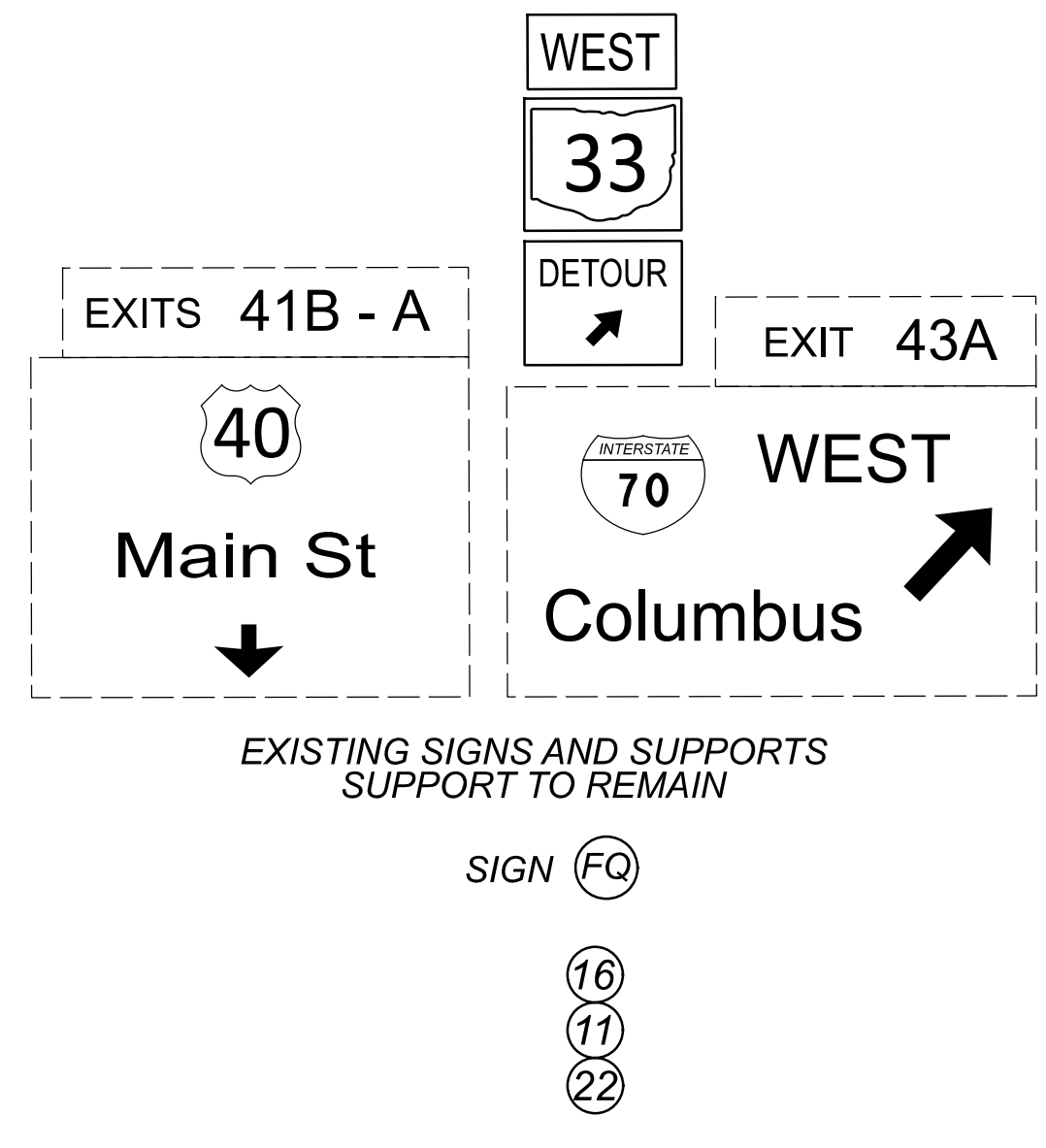
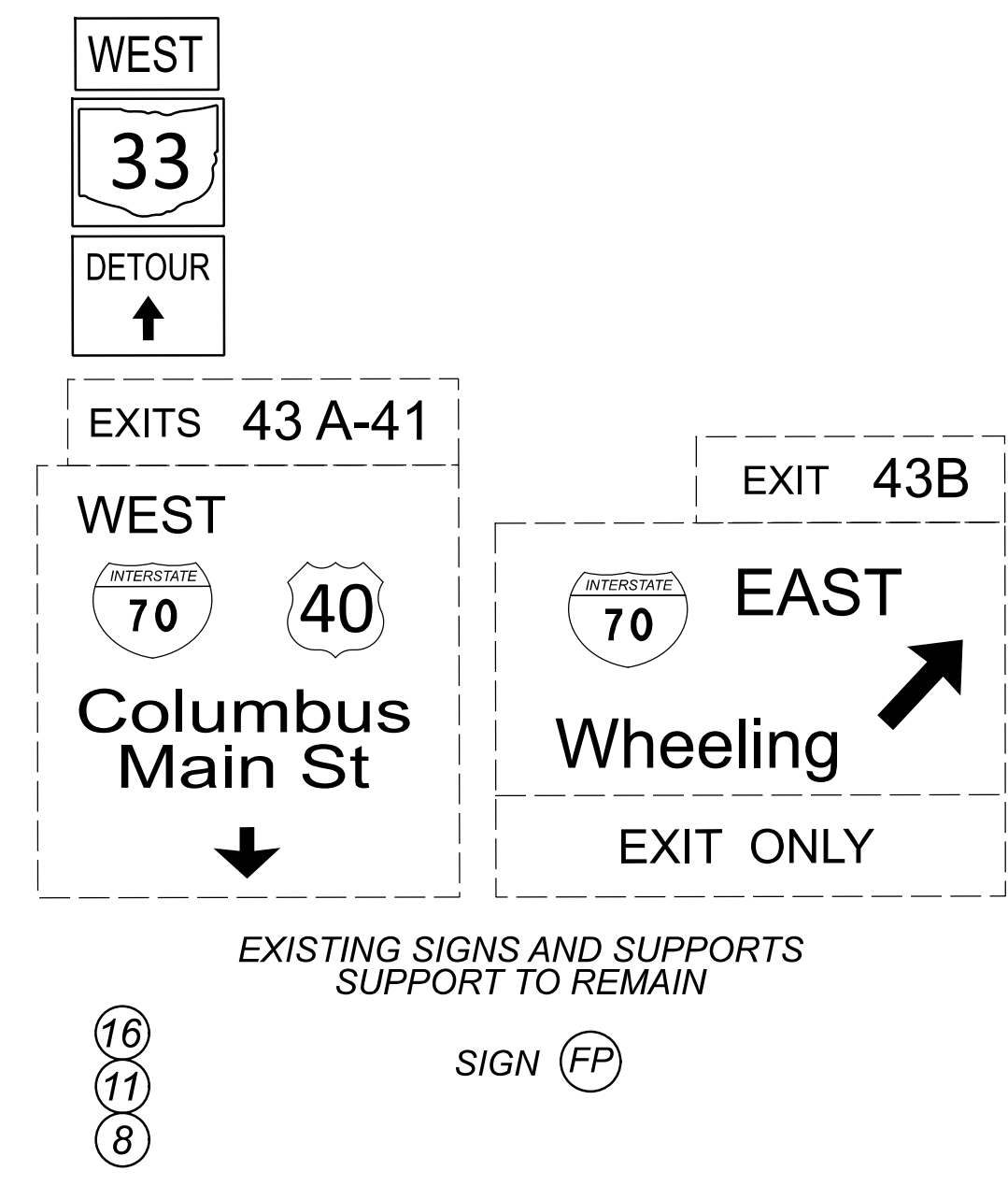
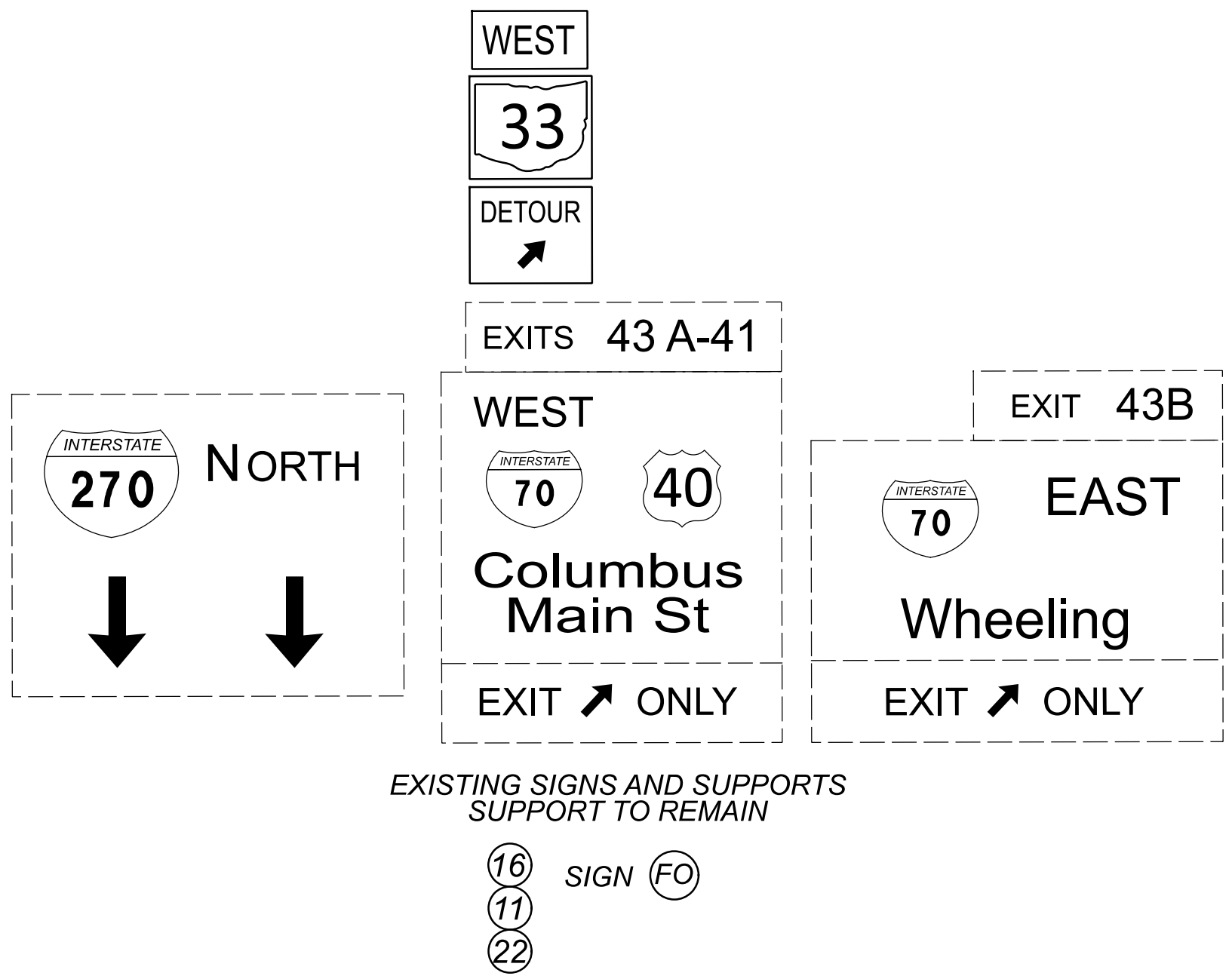
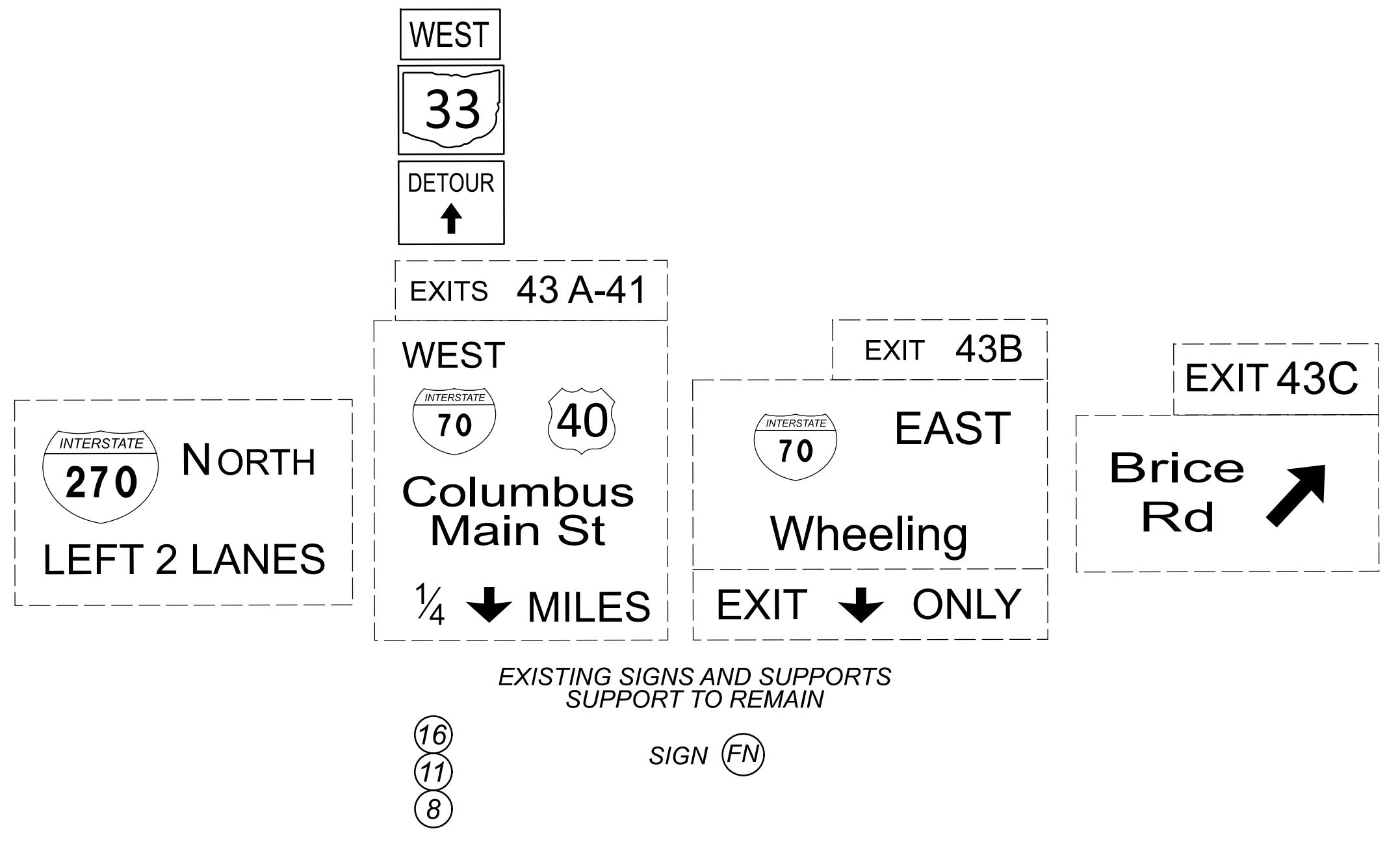
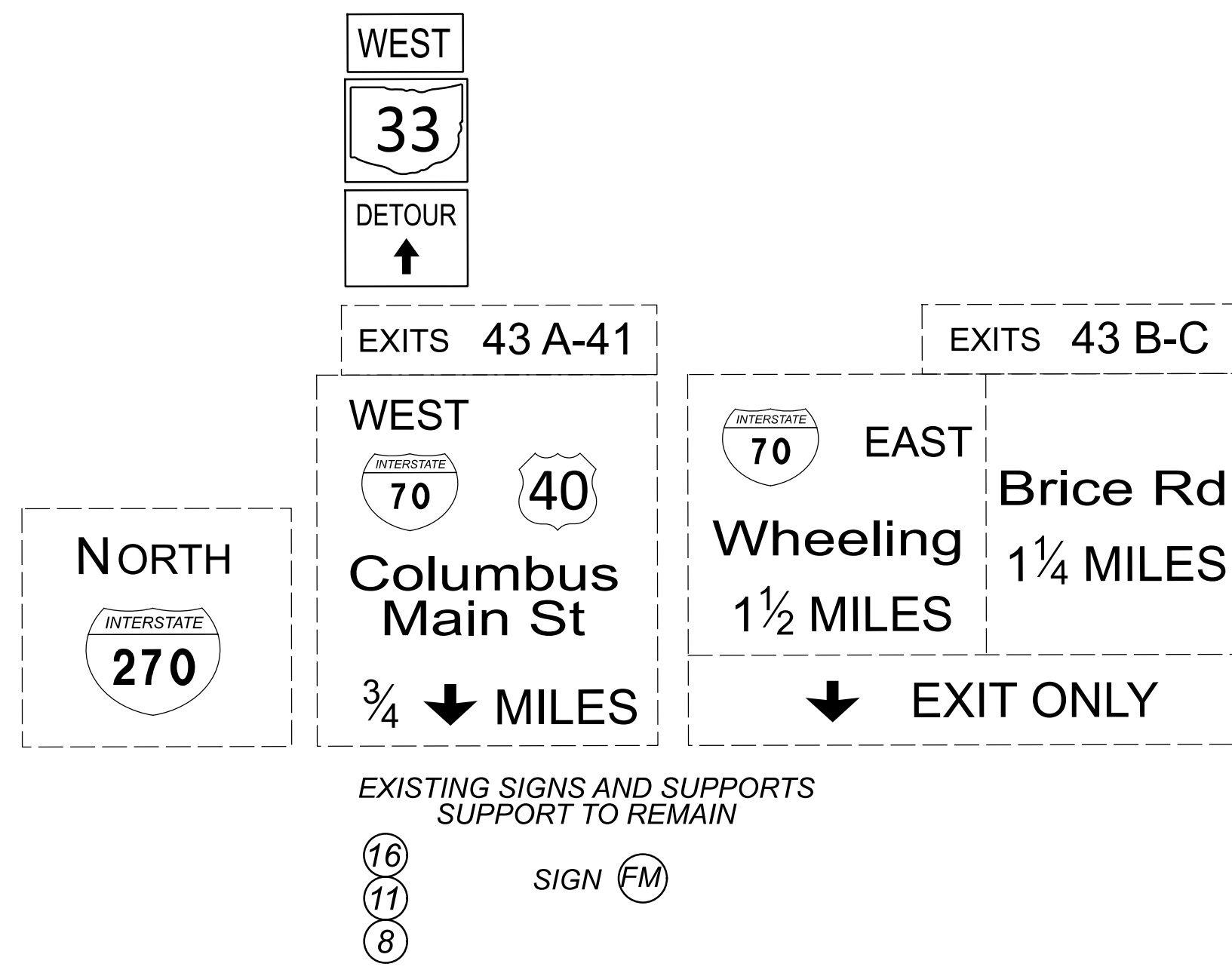
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MAINTENANCE OF TRAFFIC - DETOUR SIGN DETAILS

DESIGN AGENCY	
DESIGNER	DLS
REVIEWER	AKF 12/15/25
PROJECT ID	113744
SHEET TOTAL	P.86 625



MAINTENANCE OF TRAFFIC - DETOUR SIGN DETAILS

DESIGN AGENCY

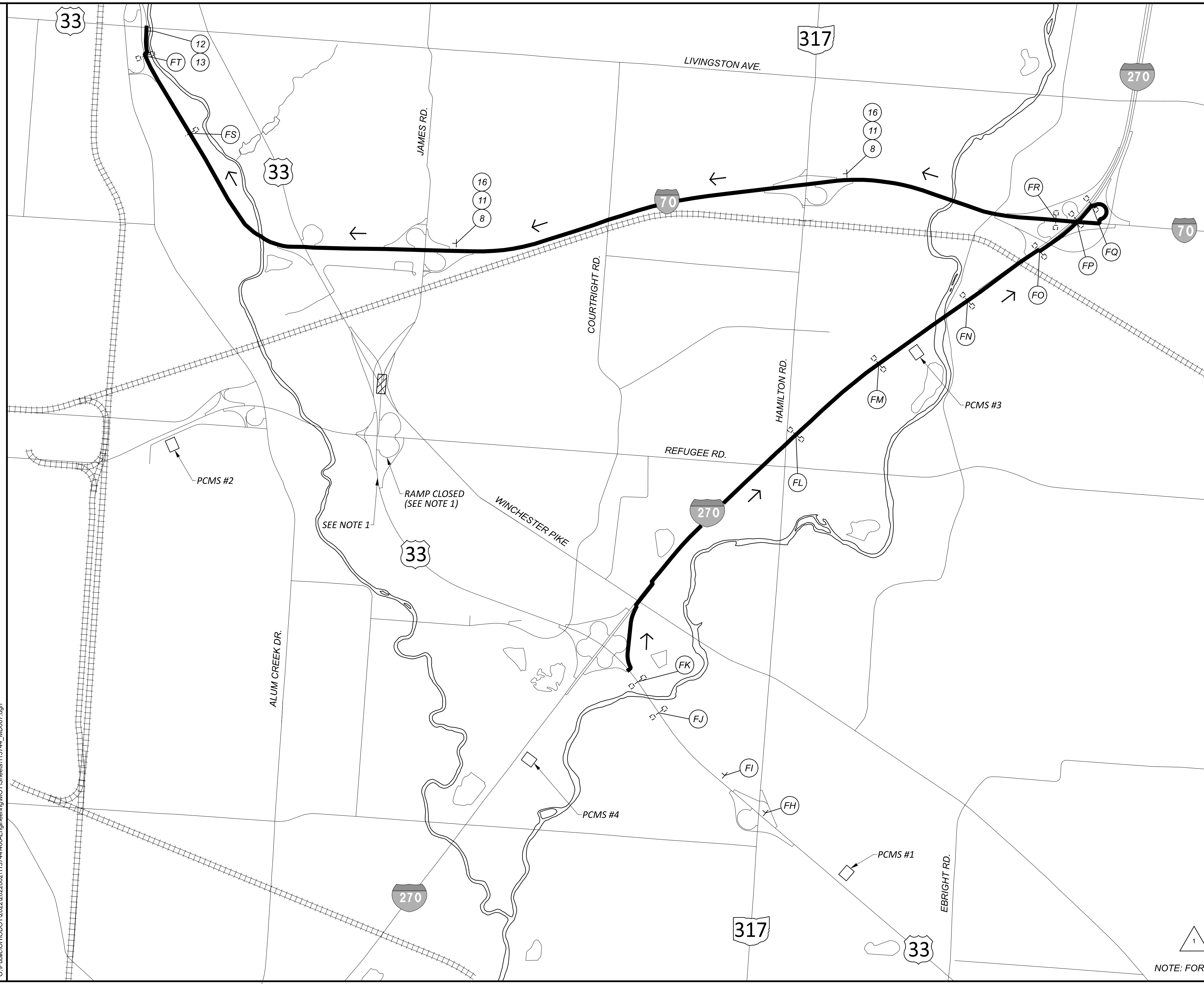


DESIGNER
DLS

REVIEWER
AKF 12/15/25

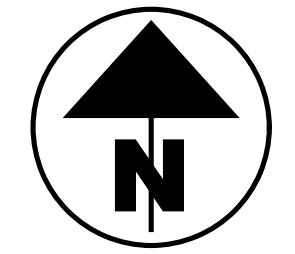
PROJECT ID
113744

SHEET TOTAL
P.86A 625



- PCMS #1:**
 (3 SEC.) US-33 CLOSED AHEAD → (3 SEC.) ALL TRAFFIC USE 270N
 PORTABLE CHANGEABLE MESSAGE SIGN DURING ROAD CLOSURE
- PCMS #2:**
 (3 SEC.) EXIT TO 33 WEST CLOSED → (3 SEC.) USE 33 EAST TO 270 N
 PORTABLE CHANGEABLE MESSAGE SIGN DURING ROAD CLOSURE
- PCMS #3:**
 (3 SEC.) EXIT TO 33 WEST CLOSED → (3 SEC.) USE 70 WEST TO 33
 PORTABLE CHANGEABLE MESSAGE SIGN DURING ROAD CLOSURE
- PCMS #4:**
 (3 SEC.) US-33 WB CLOSED → (3 SEC.) USE 270 NORTH
 PORTABLE CHANGEABLE MESSAGE SIGN DURING ROAD CLOSURE

NOTE:
 1. CONTRACTOR TO CLOSE US-33 PER MT-99.50 AND USE LEO AT THE POINT OF CLOSURE.



LEGEND	
	SIGN
	TYPE 3 BARRICADE
	DETOUR ROUTE
	TRAFFIC FLOW ARROW
	CLOSED AREA

NOTE: FOR DETOUR SIGN KEY SEE SHEETS P.82- P.86A

DESIGN AGENCY

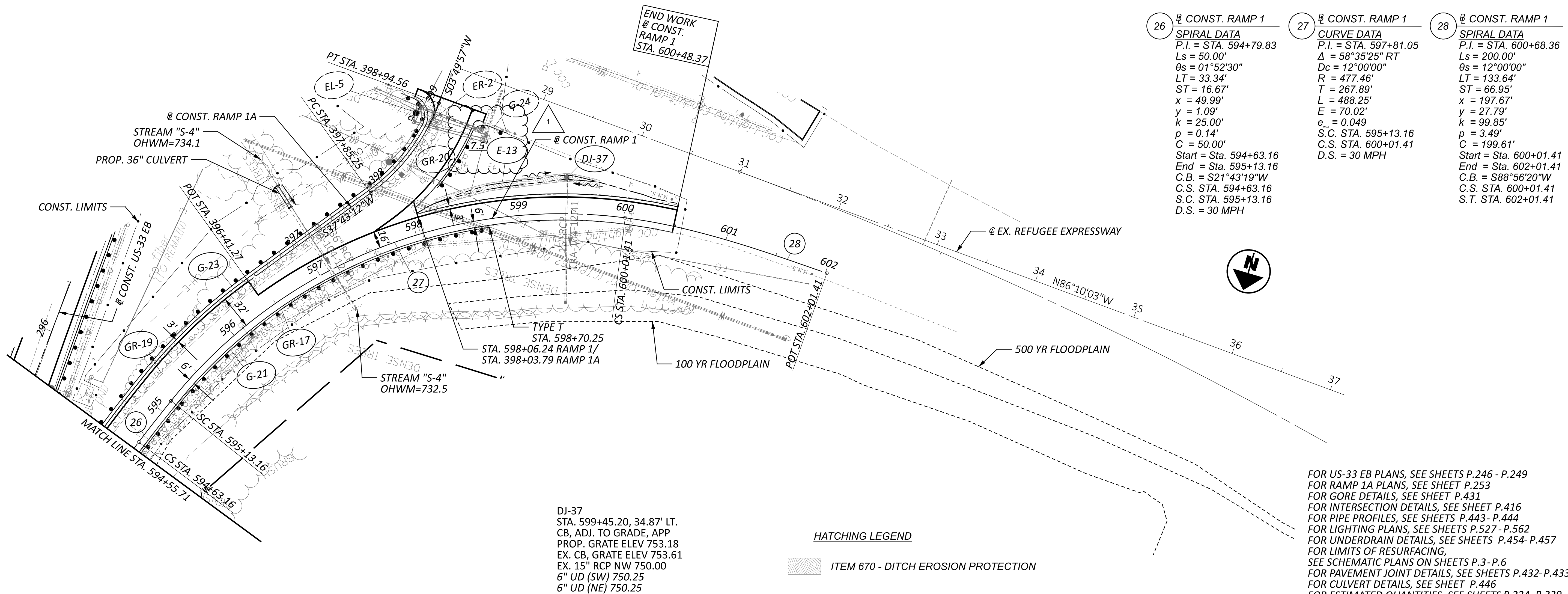
 DESIGNER
 KRM
 REVIEWER
 AKF 12/15/25
 PROJECT ID
 113744
 SHEET TOTAL
 P.89A | 625

SHEET NUMBER										PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
P.221	P.469	P.470	P.475	P.482	P.483		P.516			01/NHS	02/NHS	03/NHS						
	13									13			620	31200	13	EACH	REMOVAL OF DELINEATOR	
		510								274	236		621	00100	510	EACH	RPM	
		351								236	115		621	54000	351	EACH	RAISED PAVEMENT MARKER REMOVED	
					7					5	2		625	32000	7	EACH	GROUND ROD	
6										6			626	00102	6	EACH	BARRIER REFLECTOR, TYPE 1, ONE-WAY	
13										13			626	00102	13	EACH	BARRIER REFLECTOR, TYPE 1, BIDIRECTIONAL	
19										19			626	00110	19	EACH	BARRIER REFLECTOR, TYPE 2, ONE-WAY	
131										131			626	00110	131	EACH	BARRIER REFLECTOR, TYPE 2, BIDIRECTIONAL	
				379.9						99.9	280		630	02100	379.9	FT	GROUND MOUNTED SUPPORT, NO. 2 POST	
				1,821.7						1,278.6	543.1		630	03100	1,821.7	FT	GROUND MOUNTED SUPPORT, NO. 3 POST	
				63						63			630	04100	63	FT	GROUND MOUNTED SUPPORT, NO. 4 POST	
				244.7						107.0	137.7		630	06400	244.7	FT	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, S4X7.7	
				61.8						61.8			630	06500	61.8	FT	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W6X9	
				75.7						75.7			630	07600	75.7	FT	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W10X12	
				91						60	31		630	08600	91	EACH	SIGN POST REFLECTOR	
				22						14	8		630	09000	22	EACH	BREAKAWAY STRUCTURAL BEAM CONNECTION	
				1						1			630	72340	1	EACH	OVERHEAD SIGN SUPPORT, TYPE TC-12.31, DESIGN 12	
				2						1	1		630	72410	2	EACH	OVERHEAD SIGN SUPPORT, TYPE TC-15.116, DESIGN 1	
				1						1			630	72420	1	EACH	OVERHEAD SIGN SUPPORT, TYPE TC-15.116, DESIGN 2	
							10			10			630	79001	10	EACH	SIGN HANGER ASSEMBLY, SPAN WIRE, AS PER PLAN	P.469
				1			10			11			630	79501	11	EACH	SIGN SUPPORT ASSEMBLY, POLE MOUNTED, AS PER PLAN	P.469
				1,355.5			45			910.0	490.5		630	80100	1,400.5	SF	SIGN, FLAT SHEET	
				452						352	100		630	80200	452	SF	SIGN, GROUND MOUNTED EXTRUSHEET	
					1,561					1,126	435		630	80224	1,561	SF	SIGN, OVERHEAD EXTRUSHEET	
				22						14	8		630	84500	22	EACH	GROUND MOUNTED STRUCTURAL BEAM SUPPORT FOUNDATION	
				6						4	2		630	84510	6	EACH	RIGID OVERHEAD SIGN SUPPORT FOUNDATION	
				1						1			630	84511	1	EACH	RIGID OVERHEAD SIGN SUPPORT FOUNDATION, AS PER PLAN	P.469
				85						44	41		630	84900	85	EACH	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	
				5						3	2		630	85400	5	EACH	REMOVAL OF GROUND MOUNTED MAJOR SIGN AND DISPOSAL	
				98						50	48		630	86002	98	EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	
				8						4	4		630	86102	8	EACH	REMOVAL OF GROUND MOUNTED STRUCTURAL BEAM SUPPORT AND DISPOSAL	
				17						12	5		630	87400	17	EACH	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	
				16						10	6		630	87500	16	EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	
				5						5			630	89706	5	EACH	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-12.30	
				2						1	1		630	89802	2	EACH	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-7.65	
							LS			LS			630	95000	LS		SIGNING, MISC.: TRAFFIC SIGNAL SIGNS	P.512
				2							2		630	97700	2	EACH	SIGNING, MISC.: SIGNAL AHEAD SIGN ASSEMBLY SOLAR-POWERED	P.469
				2							2		630	97700	2	EACH	SIGNING, MISC.: REMOVAL OF FLASHER ASSEMBLY	P.469
							29.5			29.5			630	97800	29.5	SF	SIGNING, MISC.: STREET NAME SIGN	P.469
			7.91							6.05	1.86		644	00104	7.91	MILE	EDGE LINE, 6"	
	0.25		2.33							2.23	0.35		644	00204	2.58	MILE	LANE LINE, 6"	
	0.03		0.14							0.17			644	00300	0.17	MILE	CENTER LINE	
	394		6,719							6,522	591		644	00404	7,113	FT	CHANNELIZING LINE, 12"	
	14		221							235			644	00500	235	FT	STOP LINE	
			518							518			644	00620	518	FT	CROSSWALK LINE, 12"	
			807							807			644	00700	807	FT	TRANSVERSE/DIAGONAL LINE	
			404							363	41		644	00720	404	FT	CHEVRON MARKING	
	4		40							41	3		644	01300	44	EACH	LANE ARROW	
			2								2		644	01350	2	EACH	LANE REDUCTION ARROW	
			3							3			644	01400	3	EACH	WORD ON PAVEMENT, 72"	
			5,899							5,310	589		644	01510	5,899	FT	DOTTED LINE, 6"	
	38									38			644	30000	38	FT	REMOVAL OF PAVEMENT MARKING	
			0.64							0.46	0.18		646	10010	0.64	MILE	EDGE LINE, 6"	
			0.12							0.12			646	10100	0.12	MILE	LANE LINE, 4"	
			0.08							0.08			646	10200	0.08	MILE	CENTER LINE	
			689							689			646	10310	689	FT	CHANNELIZING LINE, 12"	
			32							32			646	10400	32	FT	STOP LINE	

GENERAL SUMMARY

DESIGN AGENCY

 GPD GROUP
 Designer: JAN
 Reviewer: JAN
 Project ID: 113744
 Sheet: P.218 of 625

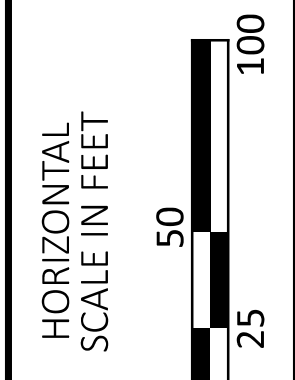
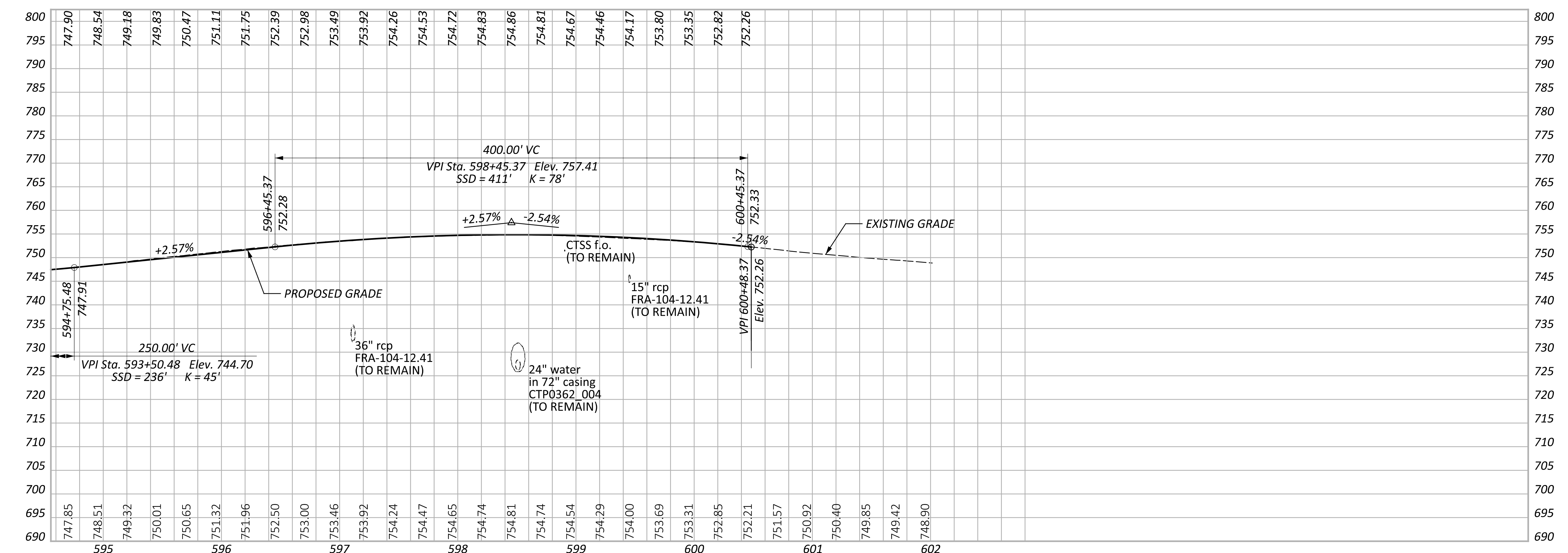


26	CONST. RAMP 1 SPIRAL DATA P.I. = STA. 594+79.83 Ls = 50.00' θs = 01°52'30" LT = 33.34' ST = 16.67' x = 49.99' y = 1.09' k = 25.00' p = 0.14' C = 50.00' Start = Sta. 594+63.16 End = Sta. 595+13.16 C.B. = S21°43'19"W C.S. STA. 594+63.16 S.C. STA. 595+13.16 D.S. = 30 MPH	27	CONST. RAMP 1 CURVE DATA P.I. = STA. 597+81.05 Δ = 58°35'25" RT Dc = 12°00'00" R = 477.46' T = 267.89' L = 488.25' E = 70.02' e _s = 0.049 S.C. STA. 595+13.16 C.S. STA. 600+01.41 D.S. = 30 MPH	28	CONST. RAMP 1 SPIRAL DATA P.I. = STA. 600+68.36 Ls = 200.00' θs = 12°00'00" LT = 133.64' ST = 66.95' x = 197.67' y = 27.79' k = 99.85' p = 3.49' C = 199.61' Start = Sta. 600+01.41 End = Sta. 602+01.41 C.B. = S88°56'20"W C.S. STA. 600+01.41 S.T. STA. 602+01.41
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DJ-37
 STA. 599+45.20, 34.87' LT.
 CB, ADJ. TO GRADE, APP
 PROP. GRATE ELEV 753.18
 EX. CB, GRATE ELEV 753.61
 EX. 15" RCP NW 750.00
 6" UD (SW) 750.25
 6" UD (NE) 750.25

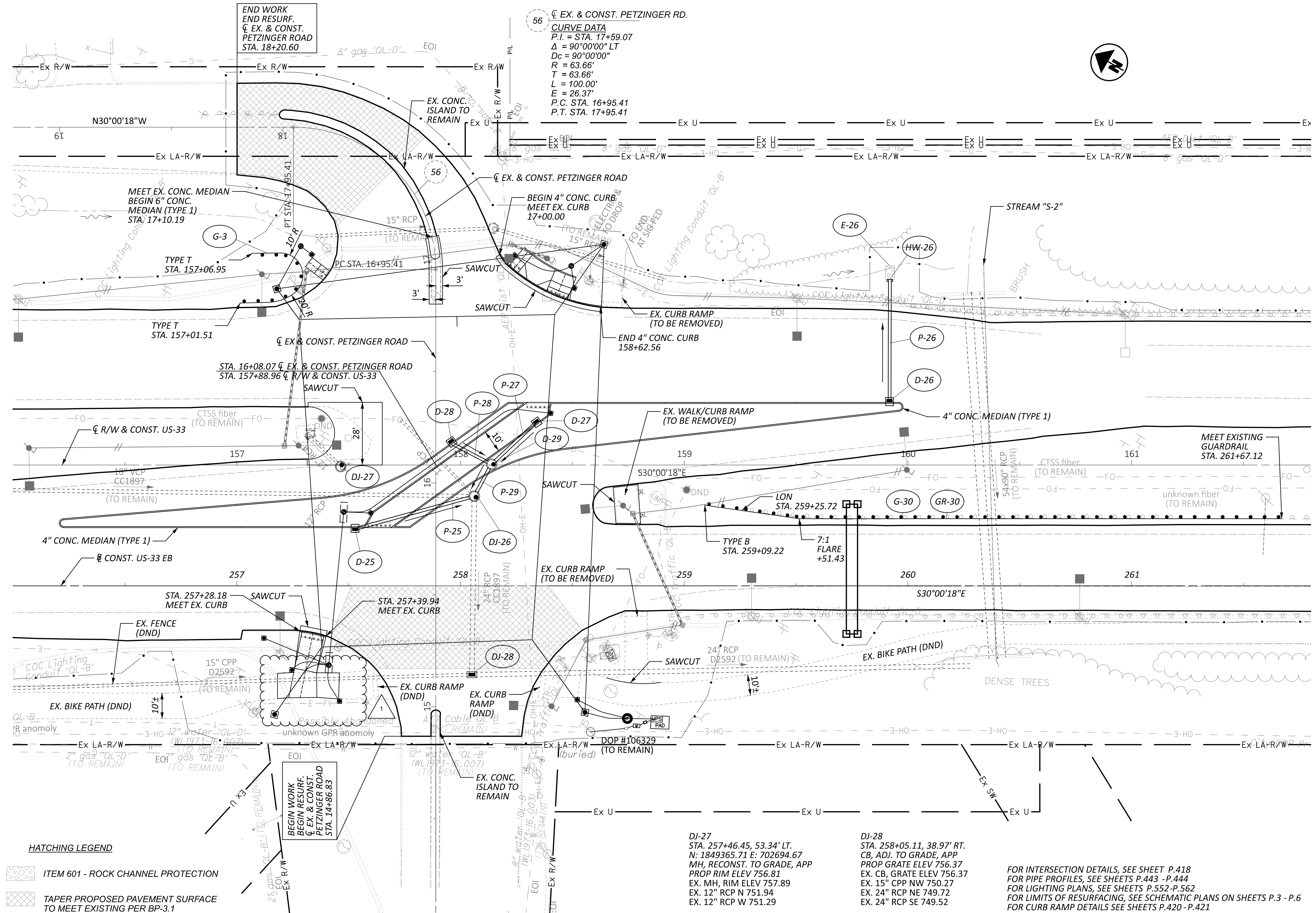
HATCHING LEGEND
 ITEM 670 - DITCH EROSION PROTECTION

FOR US-33 EB PLANS, SEE SHEETS P.246 - P.249
 FOR RAMP 1A PLANS, SEE SHEET P.253
 FOR GORE DETAILS, SEE SHEET P.431
 FOR INTERSECTION DETAILS, SEE SHEET P.416
 FOR PIPE PROFILES, SEE SHEETS P.443 - P.444
 FOR LIGHTING PLANS, SEE SHEETS P.527 - P.562
 FOR UNDERDRAIN DETAILS, SEE SHEETS P.454 - P.457
 FOR LIMITS OF RESURFACING,
 SEE SCHEMATIC PLANS ON SHEETS P.3 - P.6
 FOR PAVEMENT JOINT DETAILS, SEE SHEETS P.432 - P.433
 FOR CULVERT DETAILS, SEE SHEET P.446
 FOR ESTIMATED QUANTITIES, SEE SHEETS P.224 - P.239

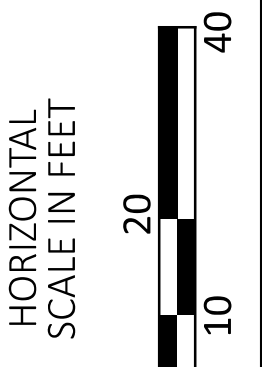
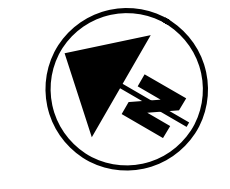


PLAN AND PROFILE - RAMP 1
 STA. 594+55.70 TO END

DESIGN AGENCY	
DESIGNER	NRB
REVIEWER	
CWL	12-15-25
PROJECT ID	113744
SHEET	P.252
TOTAL	625



56 EX. & CONST. PETZINGER RD.
 CURVE DATA
 P.I. = STA. 17+59.07
 $\Delta = 90^{\circ}00'00''$ LT
 $D_c = 90^{\circ}00'00''$
 $R = 63.66'$
 $T = 63.66'$
 $L = 100.00'$
 $E = 26.37'$
 P.C. STA. 16+95.41
 P.T. STA. 17+95.41



PLAN - PETZINGER ROAD
 STA. 14+86.83 TO STA. 18+20.60

HATCHING LEGEND

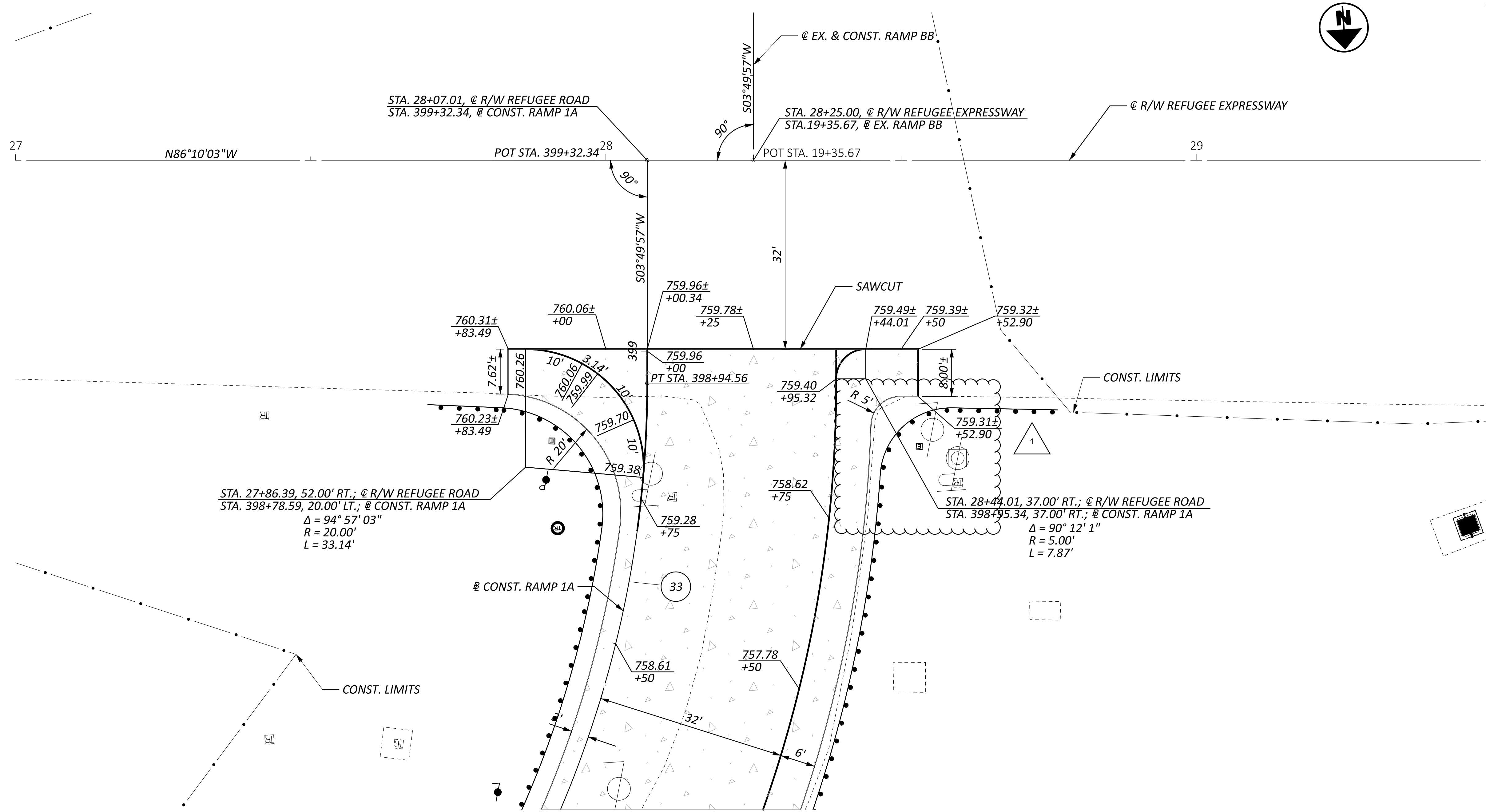
- ITEM 601 - ROCK CHANNEL PROTECTION
- TAPER PROPOSED PAVEMENT SURFACE TO MEET EXISTING PER BP-3.1 (SEE TYPICAL DETAIL ON SHEET P.28)

DJ-27
 STA. 257+46.45, 53.34' LT.
 N: 1849365.71 E: 702694.67
 MH, RECONST. TO GRADE, APP
 PROP RIM ELEV 756.81
 EX. MH, RIM ELEV 757.89
 EX. 12" RCP N 751.94
 EX. 12" RCP W 751.29

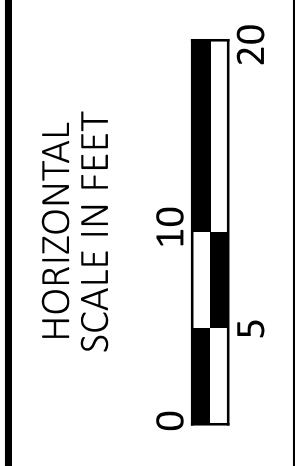
DJ-28
 STA. 258+05.11, 38.97' RT.
 CB, ADJ. TO GRADE, APP
 PROP GRATE ELEV 756.37
 EX. CB, GRATE ELEV 756.37
 EX. 15" CPP NW 750.27
 EX. 24" RCP NE 749.72
 EX. 24" RCP SE 749.52

FOR INTERSECTION DETAILS, SEE SHEET P.418
 FOR PIPE PROFILES, SEE SHEETS P.443 - P.444
 FOR LIGHTING PLANS, SEE SHEETS P.552-P.562
 FOR LIMITS OF RESURFACING, SEE SCHEMATIC PLANS ON SHEETS P.3 - P.6
 FOR CURB RAMP DETAILS SEE SHEETS P.420 - P.421
 FOR ESTIMATED QUANTITIES, SEE SHEETS P.224 - P.239

DESIGN AGENCY	
GPD GROUP	
DESIGNER	
NRB	
REVIEWER	
CWL 12-15-25	
PROJECT ID	
113744	
SHEET	TOTAL
P.263	625



33 @ CONST. RAMP 1A
CURVE DATA
 P.I. = STA. 398+41.56
 $\Delta = 33^\circ 53' 15''$ LT
 $D_c = 31^\circ 00' 00''$
 (NDC = $24^\circ 45' 00''$)
 $R = 184.83'$
 $T = 56.31'$
 $L = 109.31'$
 $E = 8.39'$
 $e_s = 0.036$ (NDC = 0.06)
 P.C. STA. 397+85.25
 P.T. STA. 398+94.56
 D.S. = 30 MPH



INTERSECTION DETAIL
 RAMP 1A AT REFUGEE ROAD

CONCRETE PAVEMENT

FOR SUPERELEVATION TABLES, SEE SHEET P.407

DESIGN AGENCY



DESIGNER
 NRB

REVIEWER
 CWL 12-15-25

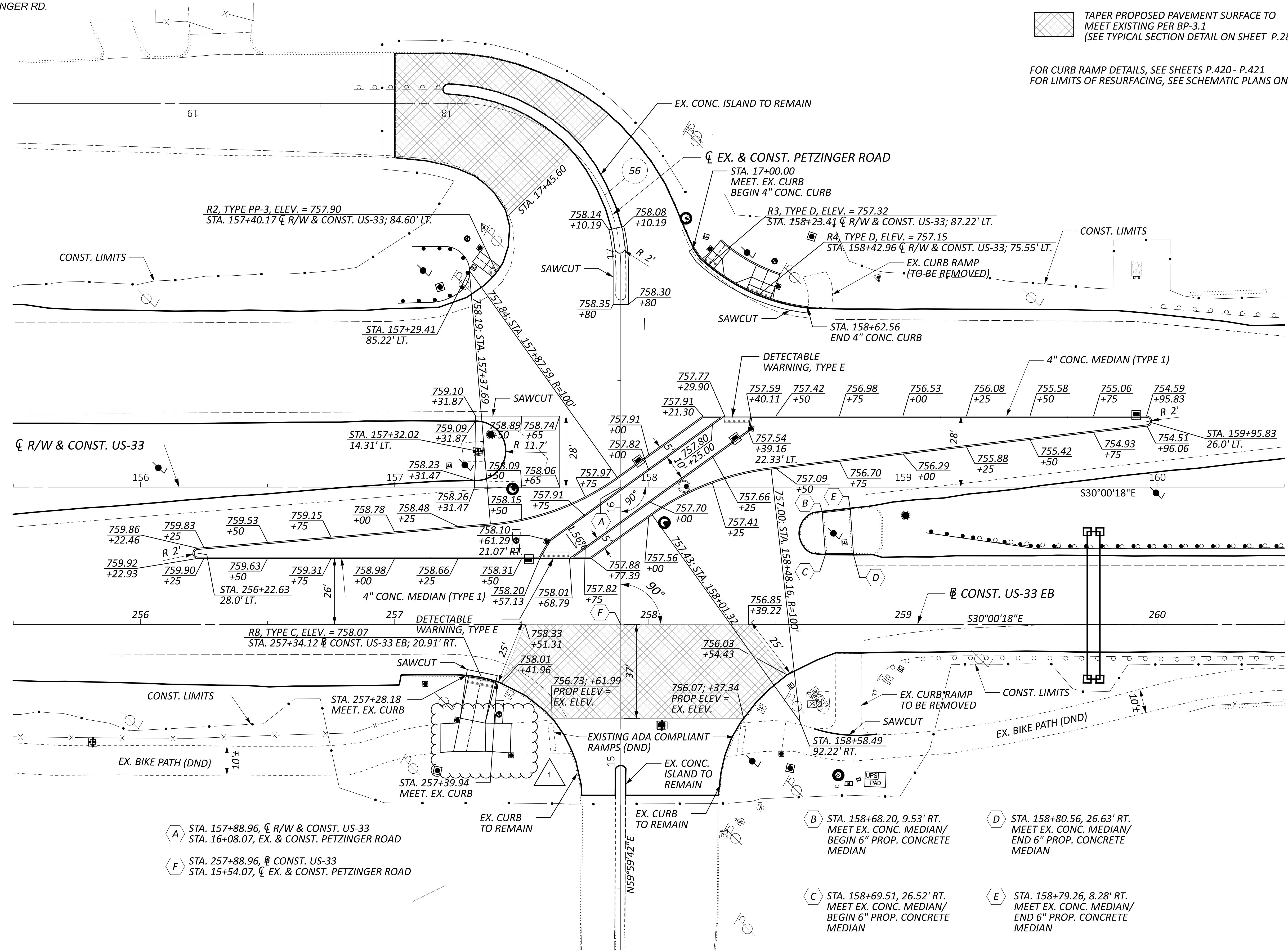
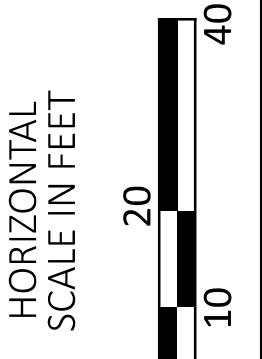
PROJECT ID
 113744

SHEET TOTAL
 P.416 625

56 **EX. & CONST. PETZINGER RD.**
CURVE DATA
 P.I. = STA. 17+59.07
 $\Delta = 90^{\circ}00'00''$ LT
 $D_c = 90^{\circ}00'00''$
 $R = 63.66'$
 $T = 63.66'$
 $L = 100.00'$
 $E = 26.37'$
 P.C. STA. 16+95.41
 P.T. STA. 17+95.41

 TAPER PROPOSED PAVEMENT SURFACE TO MEET EXISTING PER BP-3.1 (SEE TYPICAL SECTION DETAIL ON SHEET P.28)

FOR CURB RAMP DETAILS, SEE SHEETS P.420 - P.421
 FOR LIMITS OF RESURFACING, SEE SCHEMATIC PLANS ON SHEETS P.3 - P.6



- A** STA. 157+88.96, CL R/W & CONST. US-33
 STA. 16+08.07, EX. & CONST. PETZINGER ROAD
- F** STA. 257+88.96, CL CONST. US-33
 STA. 15+54.07, CL EX. & CONST. PETZINGER ROAD

- B** STA. 158+68.20, 9.53' RT.
 MEET EX. CONC. MEDIAN/
 BEGIN 6\"/>
- C** STA. 158+69.51, 26.52' RT.
 MEET EX. CONC. MEDIAN/
 BEGIN 6\"/>
- D** STA. 158+80.56, 26.63' RT.
 MEET EX. CONC. MEDIAN/
 END 6\"/>
- E** STA. 158+79.26, 8.28' RT.
 MEET EX. CONC. MEDIAN/
 END 6\"/>

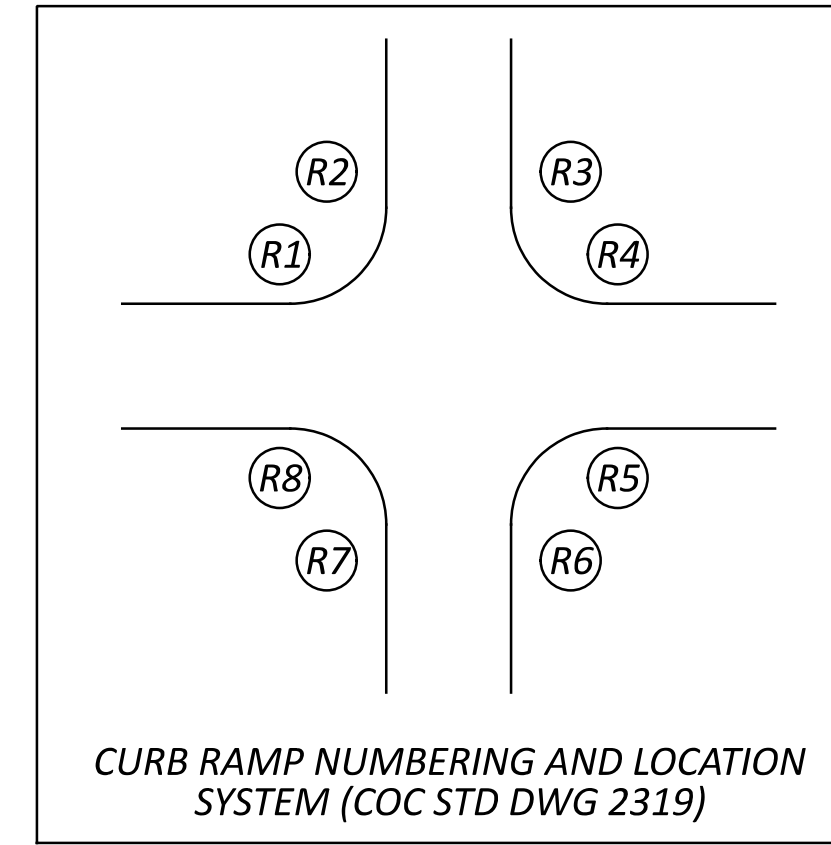
INTERSECTION DETAIL
 PETZINGER ROAD AT US-33

DESIGN AGENCY



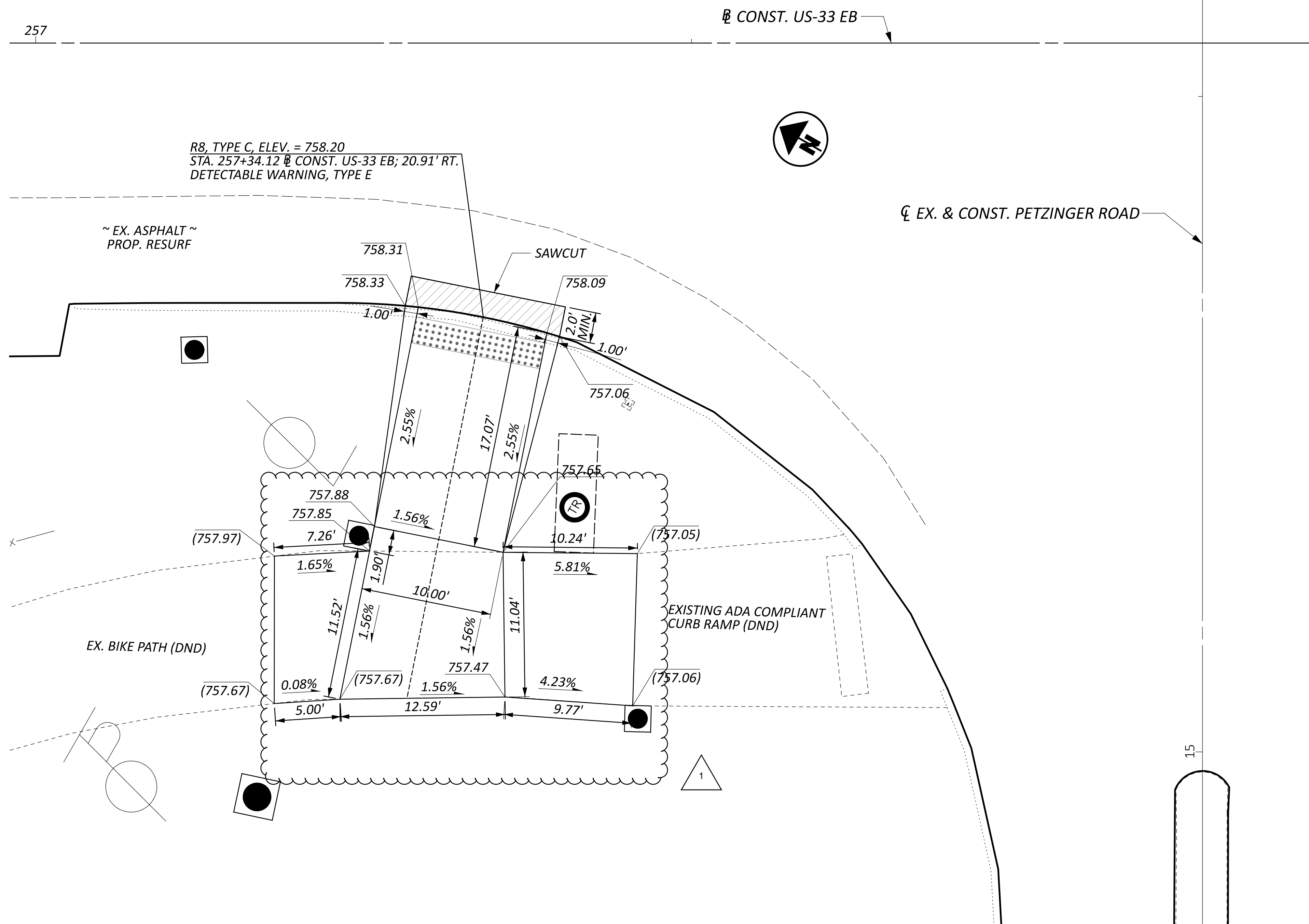
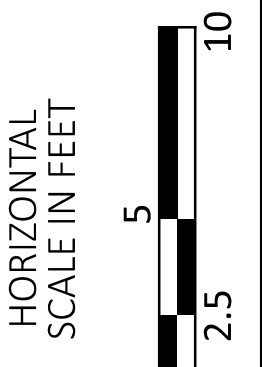
DESIGNER	NRB
REVIEWER	CWL 12-15-25
PROJECT ID	113744
SHEET TOTAL	P.418 625

FOR LIMITS OF RESURFACING, SEE SCHEMATIC PLANS ON SHEETS P.3 - P.6



- FULL DEPTH PAVEMENT REPLACEMENT
- TRUNCATED DOMES
- CATCH BASIN
- MAN HOLE
- SIGNAL POLE
- SIGNAL CONTROLLER
- * - ELEVATION IS INTERPOLATED BETWEEN EXISTING ELEVATION POINTS. CONTRACTOR TO VERIFY AND MATCH EXISTING ELEVATION.

TOP OF CURB ELEVATION
 EDGE OF PAVEMENT ELEVATION
 (789.00) - EXISTING ELEVATION
 789.00 - PROPOSED ELEVATION
 NOTE: ALL RADIUS ELEVATIONS ARE EDGE OF PAVEMENT ELEVATIONS



CURB RAMP DETAILS
PETZINGER ROAD AT US-33

FRA-33-21.71

MODEL: BLP_U033EB - Plan 2 [Sheet] PAPER SIZE: 34x22 (in.) DATE: 3/12/2026 TIME: 11:59:20 AM USER: cluzler
O:\Public\OH\ODOT\2022\113744\400-Engineering\Roadway\Sheets\113744_G1005.dgn

DESIGN AGENCY


DESIGNER
NRB

REVIEWER
CWL 12-15-25

PROJECT ID
113744

SHEET TOTAL
P.421 625

SHEET NO.	REFERENCE NO.	LOCATION	STATION	SIDE	CODE	SIZE (INCHES)	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630
							GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST	GROUND MOUNTED SUPPORT, NO. 4 POST	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, S4X7.7	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W6X9	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W10X12	SIGN POST REFLECTOR	BREAKAWAY STRUCTURAL BEAM	SIGN SUPPORT ASSEMBLY, POLE MOUNTED, AS PER PLAN	SIGN, FLAT SHEET	SIGN, GROUND MOUNTED EXTRUSHEET	GROUND MOUNTED STRUCTURAL BEAM SUPPORT FOUNDATION	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED MAJOR SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED STRUCTURAL BEAM SUPPORT AND DISPOSAL	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-12.30	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-7.65	SIGNING, MISC.: SIGNAL AHEAD SIGN ASSEMBLY SOLAR-POWERED
							FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	SF	SF	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
P.492	R-1	CL R/W US-33		RT	W4-1R																						
P.492	S-1	CL R/W US-33	146+50	LT	M3-2-36 M1-4-36-2 SPECIAL	36 X 18 36 X 36 24 X 24		17.2/17.2								4.5 9.0 4.0											
P.492	S-2	CL R/W US-33	149+50	LT	SPECIAL	180 X 60				18.7/19.1								75.0		2							
P.492	S-3	CL R/W US-33	147+50	LT	SPECIAL	36 X 48																					
P.492	S-4	CL R/W US-33	145+08	LT	R3-1-36 R1-1-36 R5-1-48	36 X 36 36 X 36 48 X 48																					
P.492	S-5	CL R/W US-33	145+82	LT	R3-1-36 R1-1-36 R5-1-48	36 X 36 36 X 36 48 X 48																					
P.492	S-6	CL R/W US-33	145+66	RT	R6-1L-54	54 X 18		12.0/12.0																			
P.492	S-7	CL R/W US-33	146+50	RT	W4-1R-48	48 X 48																					
P.493	R-3	CL R/W US-33		LT	D10-5 D10-5																						
P.493	R-4	CL CONST. PETZINGER RD		LT	SPECIAL																						
P.493	R-6	CL CONST. PETZINGER RD		LT	R3-H8b																						
P.493	R-7	CL CONST. PETZINGER RD		RT	R4-7																						
P.493	R-8	CL CONST. PETZINGER RD		RT	OM1-3																						
P.493	R-9	CL R/W US-33		LT	W14-2																						
P.493	R-11	CL CONST. PETZINGER RD		RT	R2-1																						
P.493	R-13	CL R/W US-33		LT	SPECIAL																						
P.493	R-14	CL R/W US-33		LT	R5-1a																						
P.493	R-15	CL R/W US-33		LT	R3-H8ds																						
P.493	R-16	CL R/W US-33		LT	R5-1a																						
P.493	R-19	CL R/W US-33		LT	R3-H8ds																						
P.493	R-21	CL R/W US-33		LT	R5-1a																						
P.493	R-22	CL R/W US-33		RT	R3-H8ds																						
P.493	R-23	CL R/W US-33		RT	R5-1a																						
P.493	R-24	CL R/W US-33		RT	D6-H5																						
P.493	R-25	CL R/W US-33		RT	R5-1a																						
P.493	R-26	CL R/W US-33		RT	D3-H3																						
P.493	R-27	CL R/W US-33		RT	R3-H8ds																						
P.493	R-28	CL R/W US-33		RT	R5-1a																						
P.493	R-29	CL R/W US-33		RT	R3-H8ds																						
P.493	R-30	CL R/W US-33		RT	R5-1a																						
P.493	R-31	CL R/W US-33		RT	SPECIAL																						
P.493	R-32	CL R/W US-33		RT	W11-1																						
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P.493	R-39	CL CONST. PETZINGER RD		RT	R4-7																						
P.493	R-40	CL CONST. PETZINGER RD		RT	OM1-3																						
P.493	R-43	CL R/W US-33		RT	R3-H8b																						
P.493	R-44	CL R/W US-33		RT	R4-7																						
P.493	R-45	CL R/W US-33		RT	OM1-3																						
P.493	R-46	CL R/W US-33		RT	R5-10b																						
					R2-1																						
					E1-H5bP																						
					SPECIAL																						
TOTALS CARRIED TO SIGNING SUBSUMMARY SHEET 7 OF 7							24.0	153.8								120.3	75.0	2	40		47		2	4	1		

DESIGN AGENCY

 GPD GROUP
Complete Signs, Pole Signs, Signs & Structures, Inc. 2022

DESIGNER
RNP

REVIEWER
AKF 05/22/23


PROJECT ID
113744

SHEET TOTAL
P.476 625

SHEET NO.	REFERENCE NO.	LOCATION	STATION	SIDE	CODE	SIZE (INCHES)	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630
							GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST	GROUND MOUNTED SUPPORT, NO. 4 POST	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, SAK7.7	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W6X9	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W10X12	SIGN POST REFLECTOR	BREAKAWAY STRUCTURAL BEAM	SIGN SUPPORT ASSEMBLY, POLE MOUNTED	SIGN, FLAT SHEET	SIGN, GROUND MOUNTED EXTRUSHEET	GROUND MOUNTED STRUCTURAL BEAM SUPPORT FOUNDATION	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED MAJOR SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED STRUCTURAL BEAM SUPPORT AND DISPOSAL	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-12.30	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-7.65
FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	EACH	EACH	SF	SF	SF	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
P.493	S-8	CL R/W US-33	152+65	LT	D10-5-18	18 X 60	12.5																			
P.493	S-9	CL CONST. PETZINGER RD	19+20	LT	D10-5-18	18 X 60																				
					M1-4-24-2	24 X 24		13.9																		
					M5-1-21	21 X 15																				
					M3-4-24	24 X 12		15.0																		
					M1-1-24-2	24 X 24																				
					M5-1R-21	21 X 15																				
P.493	S-10	CL CONST. PETZINGER RD	18+17	RT	R3-H8bf-30	30 X 30	13.0																			
P.493	S-11	CL CONST. PETZINGER RD	17+03	CEN	R4-7-24	24 X 30		13.0																		
					OM1-3-18	18 X 18																				
P.493	S-12	CL CONST. PETZINGER RD	17+03	RT	W14-2-36	36 X 36		14.8																		
P.493	S-13	CL R/W US-33	161+00	LT	R3-H8ds-66	66 X 30		13.0/13.0																		
P.493	S-14	CL R/W US-33	161+00	LT	R3-H8ds-66	66 X 30		13.0/13.0																		
P.493	S-15	CL R/W US-33	157+32	LT	R3-4-36	36 X 36		13.5																		
P.493	S-16	CL R/W US-33	156+70	LT	R2-1-48	48 X 60																				
P.493	S-17	CL CONST. PETZINGER RD	16+34	LT	SPECIAL	144 X 48																				
P.493	S-19	CL R/W US-33	157+50	RT	R6-1R-54	54 X 18		15.1/15.1																		
					R5-1-36	36 X 36																				
P.493	S-20	CL R/W US-33	158+50	LT	R9-3-24	24 X 24	13.6																			
					R9-3bPL-18	18 X 12																				
P.493	S-21	CL R/W US-33	159+00	LT	R5-10b-30	30 X 18	12.0																			
P.493	S-22	CL R/W US-33	158+80	RT	R3-4-36	36 X 36		13.5																		
P.493	S-23	CL R/W US-33	159+10	RT	R5-10b-30	30 X 18	12.0																			
P.493	S-24	CL R/W US-33	161+80	LT	D3-H3-108	108 X 24		12.5/12.5																		
P.493	S-25	CL R/W US-33	151+71	RT	R5-1-48	48 X 48		14.5/14.5																		
					R5-1a-42	42 X 30																				
P.493	S-26	CL R/W US-33	151+50	RT	D6-H5-96	96 X 24		12.5/12.5																		
P.493	S-27	CL R/W US-33	153+50	RT	R3-7L-30	30 X 30	13.0																			
P.493	S-28	CL R/W US-33	151+71	RT	R5-1-48	48 X 48		14.5/14.5																		
					R5-1a-42	42 X 30																				
P.493	S-29	CL R/W US-33	153+70	RT	D3-H3-108	108 X 24		12.5/12.5																		
P.493	S-30	CL R/W US-33	154+80	RT	R3-H8ds-66	66 X 30		13.0/13.0																		
P.493	S-31	CL R/W US-33	154+80	RT	R3-H8ds-66	66 X 30		13.0/13.0																		
P.493	S-32	CL R/W US-33	155+33	RT	R3-7L-30	30 X 30	13.0																			
P.493	S-33	CL R/W US-33	157+30	RT	R9-3-24	24 X 24	13.6																			
					R9-3bPL-18	18 X 12																				
P.493	S-34	CL R/W US-33	157+00	RT	W11-1-36	36 X 36		14.8																		
P.493	S-35	CL R/W US-33	158+05	LT	R6-1R-54	54 X 18		15.1/15.1																		
					R5-1-36	36 X 36																				
P.493	S-36	CL CONST. PETZINGER RD	14+50	RT	R3-H8bf-30	30 X 30	13.0																			
P.493	S-37	CL CONST. PETZINGER RD	14+91	CEN	R4-7-24	24 X 30		13.0																		
					OM1-3-18	18 X 18																				
P.493	S-38	CL R/W US-33	159+00	RT	R5-10b-30	30 X 18	12.0																			
P.493	S-40	CL CONST. PETZINGER RD	15+87	LT	SPECIAL	144 X 48																				
P.493	S-41	CL R/W US-33	159+00	LT	R4-7-36	36 X 48		14.5/14.5																		
					OM1-3-18	18 X 18																				
					OM1-3-18	18 X 18																				
P.493	S-42	CL R/W US-33	259+75	RT	R2-1-48	48 X 60																				
P.493	S-110	CL R/W US-33	160+90	LT	R5-1-48	48 X 48		14.5/14.5																		
					R5-1a-42	42 X 30																				
P.493	S-111	CL R/W US-33	160+90	LT	R5-1-48	48 X 48		14.5/14.5																		
					R5-1a-42	42 X 30																				
P.493	S-112	CL R/W US-33	154+90	RT	R5-1-48	48 X 48		14.5/14.5																		
					R5-1a-42	42 X 30																				
P.493	S-113	CL R/W US-33	154+90	RT	R5-1-48	48 X 48		14.5/14.5																		
					R5-1a-42	42 X 30																				

TOTALS CARRIED TO SIGNING SUBSUMMARY SHEET 7 OF 7


127.7 553.9 35.1 61.8 28 6 1 431.8 136.0 6

DESIGN AGENCY

 GPD GROUP
 DESIGNER: RNP
 REVIEWER: AKF 05/22/23
 PROJECT ID: 113744
 SHEET TOTAL: P.477 625

SHEET NO.	REFERENCE NO.	LOCATION	STATION	SIDE	CODE	SIZE (INCHES)	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	
							GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST	GROUND MOUNTED SUPPORT, NO. 4 POST	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, S&X7.7	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W&X9	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W10X12	SIGN POST REFLECTOR	BREAKAWAY STRUCTURAL BEAM	SIGN SUPPORT ASSEMBLY, POLE MOUNTED, AS PER PLAN	SIGN, FLAT SHEET	SIGN, GROUND MOUNTED EXTRUSHEET	GROUND MOUNTED STRUCTURAL BEAM SUPPORT FOUNDATION	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED MAJOR SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED STRUCTURAL BEAM SUPPORT AND DISPOSAL	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-12.30	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-7.65	SIGNING, MISC.: SIGNAL AHEAD SIGN ASSEMBLY SOLAR-POWERED
							FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	SF	SF	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
P.496	R-69	BL CONST. US-33 WB		LT	SPECIAL E1-H5P																						
P.496	R-70	BL CONST. US-33 WB		LT	E5-H1c																						
P.496	R-71	BL CONST. RAMP 1		RT	E1-H5P																						
P.496	R-72	BL CONST. RAMP 1		RT	W3-3																						
P.496	R-73	BL CONST. RAMP 1		LT	R5-1a																						
P.496	R-74	BL CONST. RAMP 1		RT	R5-1a																						
P.496	R-75	BL CONST. RAMP 1		RT	D10-H5a																						
P.496	R-76	EX US-33 EB		RT	D10-H5																						
P.496	R-77	BL CONST. RAMP 1		RT	SPECIAL																						
P.496	R-78	BL CONST. RAMP 1		RT	R5-1a																						
P.496	R-79	BL CONST. RAMP 1		LT	R5-1a																						
P.496	R-79A	EX US-33 EB		RT	E5-H1a																						
P.496	S-70	BL CONST. WINCHESTER RAMP A	788+50	LT	W6-1-36	36 X 36		14.8																			
P.496	S-71	CL CONST. US-33 WB	195+50	LT	E5-H1c-48	48 X 84																					
P.496	S-72	BL CONST. RAMP 2	446+00	LT	W4-1R-48	48 X 48		16.2/16.2																			
P.496	S-73	BL CONST. RAMP 1	588+50	LT	W4-3R-48	48 X 48		16.2/16.2																			
P.496	S-74	BL CONST. RAMP 1	591+50	RT	W3-3-48	48 X 48		16.2/16.2																			
P.496	S-75	BL CONST. RAMP 1	592+50	RT	D10-H5a-30	30 X 30	13.0																				
P.496	S-76	BL CONST. RAMP 1	593+22	LT	R5-1-48	48 X 48		14.5/14.5																			
P.496	S-77	BL CONST. RAMP 1	593+22	RT	R5-1a-42	42 X 30																					
P.496	S-78	CL R/W US-33	196+08	LT	R5-1-48	48 X 48		14.5/14.5																			
P.496	S-79	BL CONST. RAMP 1	596+00	RT	R5-1a-42	42 X 30																					
P.496	S-80	BL CONST. RAMP 1	596+50	RT	D10-5-18	18 X 60	12.5																				
P.496	S-81	BL CONST. RAMP 1	596+50	LT	M3-3-36	36 X 18		17.0/17.0																			
					M1-5-36-3	36 X 36																					
					M6-2R-30	30 X 21																					
					R5-1-48	48 X 48																					
					R5-1a-42	42 X 30																					
					R5-1-48	48 X 48																					
					R5-1a-42	42 X 30																					

TOTALS CARRIED TO SIGNING SUBSUMMARY SHEET 7 OF 7

25.5 204.0 63.0 35.2 16 2 195.4 28.0 2 9 2 10 4 6 2 1

DESIGN AGENCY

 GPD GROUP
 Designer: RNP
 Reviewer: AKF 05/23/22
 Project ID: 113744
 Sheet: P.480 Total: 625

SHEET NO.	REFERENCE NO.	LOCATION	STATION	SIDE	CODE	SIZE (INCHES)	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630
							GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST	GROUND MOUNTED SUPPORT, NO. 4 POST	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, S4X7.7	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W6X9	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W10X12	SIGN POST REFLECTOR	BREAKAWAY STRUCTURAL BEAM	SIGN SUPPORT ASSEMBLY, POLE MOUNTED, AS PER PLAN	SIGN, FLAT SHEET	SIGN, GROUND MOUNTED EXTRUSHEET	GROUND MOUNTED STRUCTURAL BEAM SUPPORT FOUNDATION	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED MAJOR SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED STRUCTURAL BEAM SUPPORT AND DISPOSAL	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-12.30	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-7.65
FT	FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	SF	SF	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
P.497	R-80	BL CONST. US-33 EB		RT	SPECIAL E1-H5P																					
P.497	R-81	BL CONST. RAMP 1A		LT	D5-10b																					
P.497	R-82	BL CONST. RAMP 1A		LT	D6-H5-96																					
P.497	R-83	BL CONST. RAMP 1A		RT	R5-10b																					
P.497	R-84	BL CONST. RAMP 1A		RT	SPECIAL R6-1L																					
P.497	R-86	BL CONST. RAMP 1A		LT	R6-1L																					
					R6-1R																					
					R5-1																					
P.497	R-88	BL EX. RAMP B		RT	D10-H5a																					
P.497	R-89	BL EX. RAMP B		LT	W1-8R																					
P.497	R-90	BL EX. RAMP B		LT	W1-8R																					
P.497	R-91	BL EX. RAMP B		LT	W4-1R																					
P.497	R-92	BL EX. RAMP B		LT	R1-2																					
P.497	R-93	BL EX. RAMP BB		LT	W1-8R																					
P.497	R-94	BL EX. RAMP BB		LT	W1-8R																					
P.497	R-95	BL EX. RAMP BB		RT	R5-10b																					
P.497	R-96	BL EX. RAMP BB		LT	W4-1R																					
P.497	S-82	BL CONST. RAMP 1A	397+70	LT	R5-10b-30	30 X 18	12.0																			
P.497	S-83	BL CONST. RAMP 1A	398+77	LT	D6-H5-96	96 X 24		12.5																		
P.497	S-84	BL CONST. RAMP 1A	398+53	LT	SPECIAL	90 X 48																				
					R5-10b-30	30 X 18																				
P.497	S-85	BL CONST. RAMP 1A	398+85	RT	R6-1L-54	54 X 18																				
					R6-1R-54	54 X 18																				
					R5-1-36	36 X 36																				
P.497	S-86	CL R/W REFUGEE RD	30+00	RT	W4-3R-48	48 X 48																				
P.497	S-88	BL EX. RAMP B	136+25	RT	D10-H5a-30	30 X 30	13.0																			
P.497	S-89	BL EX. RAMP B	137+90	RT	W1-8R-36	30 X 36	10.5																			
P.497	S-90	BL EX. RAMP B	138+20	RT	W1-8R-36	30 X 36	10.5																			
P.497	S-91	BL EX. RAMP B	138+80	RT	W4-1R-48	48 X 48																				
P.497	S-92	BL EX. RAMP B	139+00	RT	R2-1-48	48 X 48 X 48																				
P.497	S-93	BL EX. RAMP BB	21+32	LT	W1-8R-36	30 X 36	10.5																			
P.497	S-94	BL EX. RAMP BB	22+12	LT	W1-8R-36	30 X 36	10.5																			
P.497	S-95	BL EX. RAMP B	142+50	RT	R5-10b-30	30 X 18	12.0																			
P.497	S-96	CL R/W US-33	206+00	RT	W4-1R-48	48 X 48																				
P.497	S-97	BL CONST. RAMP 1A	398+85	LT	R6-1L-54	54 X 18																				
					R6-1R-54	54 X 18																				
					R5-1-36	36 X 36																				

TOTALS CARRIED TO SIGNING SUBSUMMARY SHEET 7 OF 7

79.0

194.9

30.6

10

2

163.8

30.0

2

13

1

15

2

4

1

DESIGN AGENCY




DESIGNER RNP

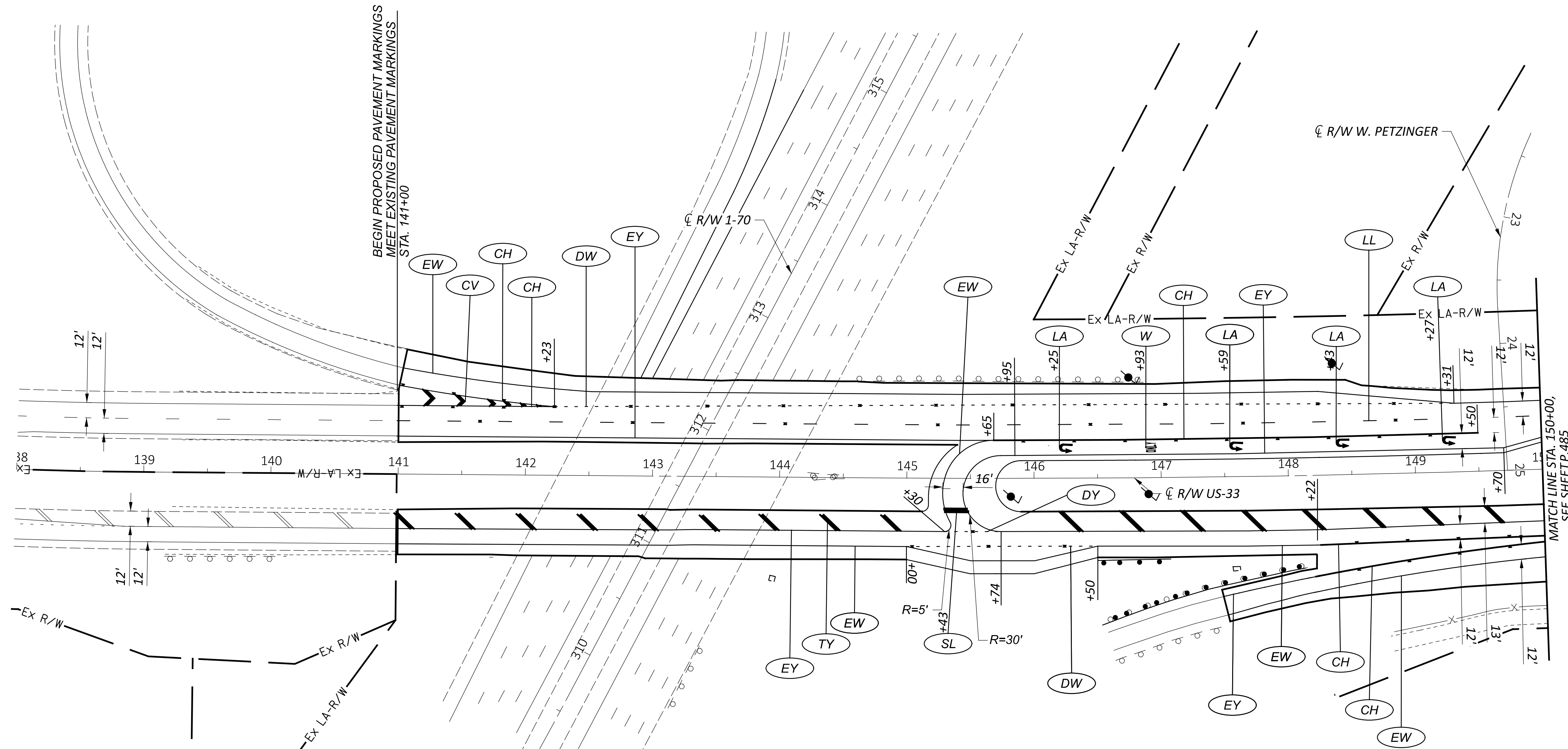
REVIEWER AKF 05/22/23

PROJECT ID 113744

SHEET TOTAL P.481 625

SHEET NO.	REFERENCE NO.	LOCATION	STATION	SIDE	CODE	SIZE (INCHES)	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630	630			
							GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST	GROUND MOUNTED SUPPORT, NO. 4 POST	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, S4X7.7	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W6X9	GROUND MOUNTED STRUCTURAL BEAM SUPPORT, W10X12	SIGN POST REFLECTOR	BREAKAWAY STRUCTURAL BEAM	SIGN SUPPORT ASSEMBLY, POLE MOUNTED, AS PER PLAN	SIGN, FLAT SHEET	SIGN, GROUND MOUNTED EXTRUSHEET	GROUND MOUNTED STRUCTURAL BEAM SUPPORT FOUNDATION	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED MAJOR SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED STRUCTURAL BEAM SUPPORT AND DISPOSAL	REMOVAL OF OVERHEAD MOUNTED SIGN AND DISPOSAL	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-12.30	REMOVAL OF OVERHEAD SIGN SUPPORT AND DISPOSAL, TYPE TC-7.65	SIGNING, MISC.: SIGNAL AHEAD SIGN ASSEMBLY SOLAR-POWERED	SIGNING, MISC.: REMOVAL OF FLASHER ASSEMBLY	
							FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	SF	SF	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH			
P.498	R-97	BL CONST. JAMES RD SB		RT	W9-2L																								
P.498	R-98	BL CONST. JAMES RD SB		RT	D1-H16																								
P.498	R-99	BL CONST. JAMES RD SB		RT	W4-2R																								
P.498	R-100	BL CONST. JAMES RD SB		RT	R5-1																								
P.498	R-101	BL CONST. JAMES RD SB		LT	R5-1L																								
					R5-1R																								
P.498	R-102	BL CONST. JAMES RD SB		RT	R5-1L																								
					R5-1R																								
P.498	R-102A	BL CONST. JAMES RD SB		LT	R5-1																								
P.498	R-102B	BL CONST. JAMES RD SB		RT	R5-1a																								
P.498	S-98	BL CONST JAMES RD SB	116+00	RT	W9-2L-36	36 X 36		14.7																					
P.498	S-99	BL CONST JAMES RD SB	120+00	RT	D1-H16-72	72 X 12	12.6/12.6																						
					D1-H16-72	72 X 12																							
P.498	S-100	BL CONST JAMES RD SB	121+50	RT	W4-2R-36	36 X 36		14.7																					
P.498	S-101	BL CONST JAMES RD SB	122+56	RT	R5-1-36	36 X 36		13.5/13.5																					
					R5-1a-42	42 X 30																							
P.498	S-102	BL CONST JAMES RD SB	123+11	LT	R6-1L-36	36 X 12		14.1																					
					R6-1R-36	36 X 12																							
					R1-1-30	30 X 30																							
P.498	S-103	BL CONST JAMES RD SB	123+68	RT	R6-1L-36	36 X 12		14.1																					
					R6-1R-36	36 X 12																							
					R1-1-30	30 X 30																							
P.498	S-104	BL CONST JAMES RD SB	120+00	LT	R3-H8cg-48	48 X 30		13.0/13.0																					
P.498	S-104A	BL CONST JAMES RD SB	122+50	LT	R5-1-36	36 X 36		13.5																					
P.498	S-104B	BL CONST JAMES RD SB	122+00	LT	R5-1a-42	42 X 30		13.0																					
P.498	S-104C	BL CONST JAMES RD SB	120+00	RT	R5-1a-42	42 X 30		13.0																					
P.498	S-105	BL CONST JAMES RD SB	123+90	RT	R5-10b-30	30 X 18	13.0																						
P.499	R-103	BL CONST JAMES RD SB		RT	R5-10b																								
P.499	R-104	BL CONST JAMES RD SB		RT	SPECIAL																								
P.498	S-106	BL CONST JAMES RD SB	131+20	RT	SPECIAL	96 X 36																							
TOTALS THIS SHEET							38.2	150.1		30.6			9	2		112.8	24.0	2	9	1	6	2	3						
TOTALS CARRIED FROM SIGNING SUBSUMMARY SHEET 1 OF 7							24.0	153.8					8	2		120.3	75.0	2	40		47		4						
TOTALS CARRIED FROM SIGNING SUBSUMMARY SHEET 2 OF 7							127.7	553.9		35.1	61.8		28	6	1	431.8	136.0	6											
TOTALS CARRIED FROM SIGNING SUBSUMMARY SHEET 3 OF 7							25.5	149.2		39.0			8	2		111.4	28.0	2	9	1	13	2	5				1	2	
TOTALS CARRIED FROM SIGNING SUBSUMMARY SHEET 4 OF 7							60.0	415.8		74.2			12	6		220.0	131.0	6	5		7		2	3		1			2
TOTALS CARRIED FROM SIGNING SUBSUMMARY SHEET 5 OF 7							25.5	204.0		35.2			16	2		195.4	28.0	2	9	2	10		6	2		1		1	
TOTALS CARRIED FROM SIGNING SUBSUMMARY SHEET 6 OF 7							79.0	184.8		30.6			10	2		163.8	30.0	2	13	1	15		2	4		1		1	
TOTALS CARRIED TO GENERAL SUMMARY							379.9	1821.7		63.0	244.7	61.8	91	22	1	1355.5	452.0	22	85	5	98	8	17	16	5	2	2	2	

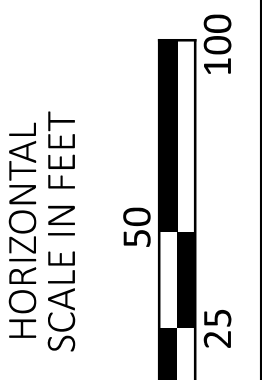
DESIGN AGENCY

 GPD GROUP
 DESIGNER: RNP
 REVIEWER: AKF 05/22/23
 PROJECT ID: 113744
 SHEET TOTAL: P.482 | 625



BEGIN PROPOSED PAVEMENT MARKINGS
 MEET EXISTING PAVEMENT MARKINGS
 STA. 141+00

MATCH LINE STA. 150+00;
 SEE SHEET P.485

TRAFFIC CONTROL LEGEND			
	PROPOSED SIGN		EXISTING SIGN TO BE REMOVED
	EXISTING SIGN TO REMAIN		PROPOSED SIGN
	EXISTING SIGN TO BE REMOVED AND REERECTED		OVERHEAD SIGN SUPPORT
	EXISTING SIGN TO BE REMOVED		EDGE LINE, WHITE, 6"
	SIGN SUPPORT		EDGE LINE, YELLOW, 6"
	EXISTING CANTILEVER SIGN SUPPORT		LANE LINE, 6"
	PROPOSED CANTILEVER SIGN SUPPORT		CENTER LINE, DOUBLE SOLID, 6"
	EXISTING TRUSS SIGN SUPPORT		CHANNELIZING LINE, 12"
	PROPOSED TRUSS SIGN SUPPORT		CROSSWALK LINE, 12"
	RPM (RAISED PAVEMENT MARKING)		LANE REDUCTION ARROW
			WRONG WAY ARROW
			TRANSVERSE/DIAGONAL LINE, WHITE
			TRANSVERSE/DIAGONAL LINE, YELLOW
			STOP LINE, 24"
			LANE ARROW
			DOTTED LINE, WHITE, 6"
			DOTTED LINE, YELLOW, 6"
			WORD ON PAVEMENT
			CHEVRON MARKING



PAVEMENT MARKING PLAN
 BEGIN TO STA. 150+00

DESIGN AGENCY

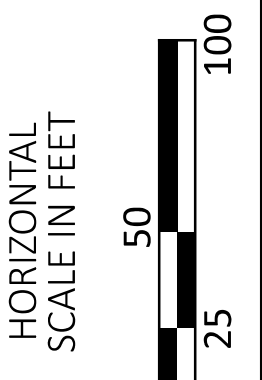
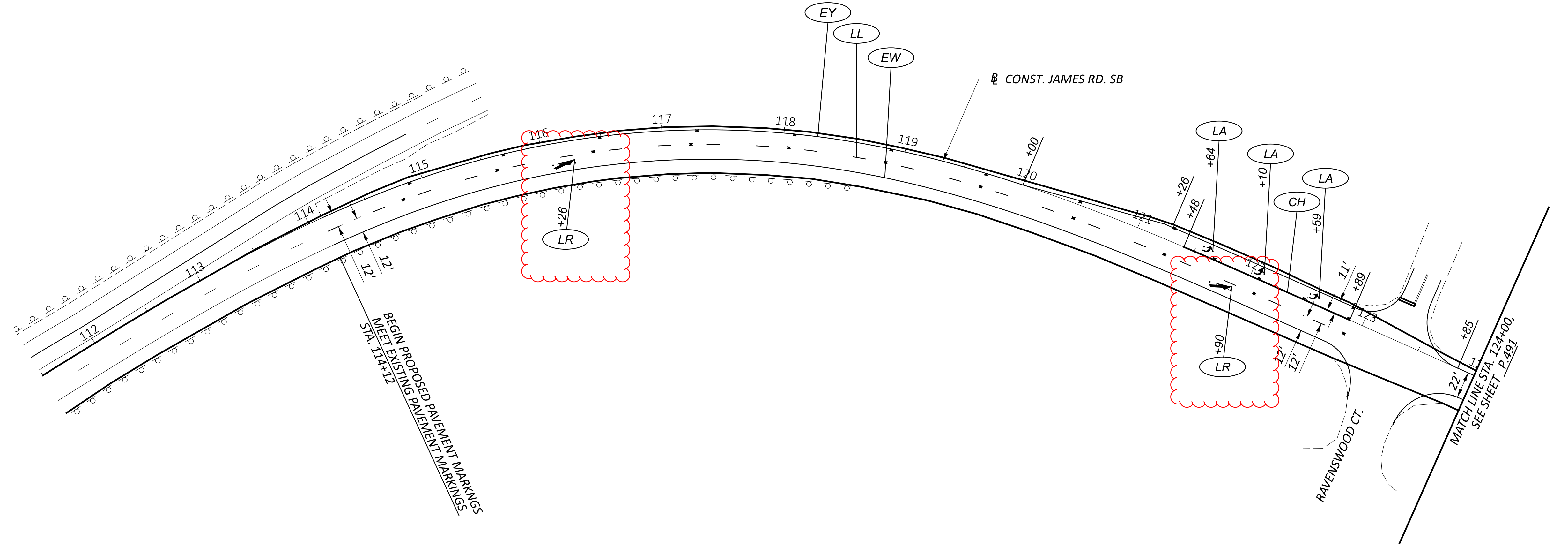


DESIGNER
 DLS

REVIEWER
 AKF 11/02/22

PROJECT ID
 113744

SHEET TOTAL
 P.484 625



PAVEMENT MARKING PLAN - JAMES RD SB
BEGIN TO STA. 124+00

DESIGN AGENCY



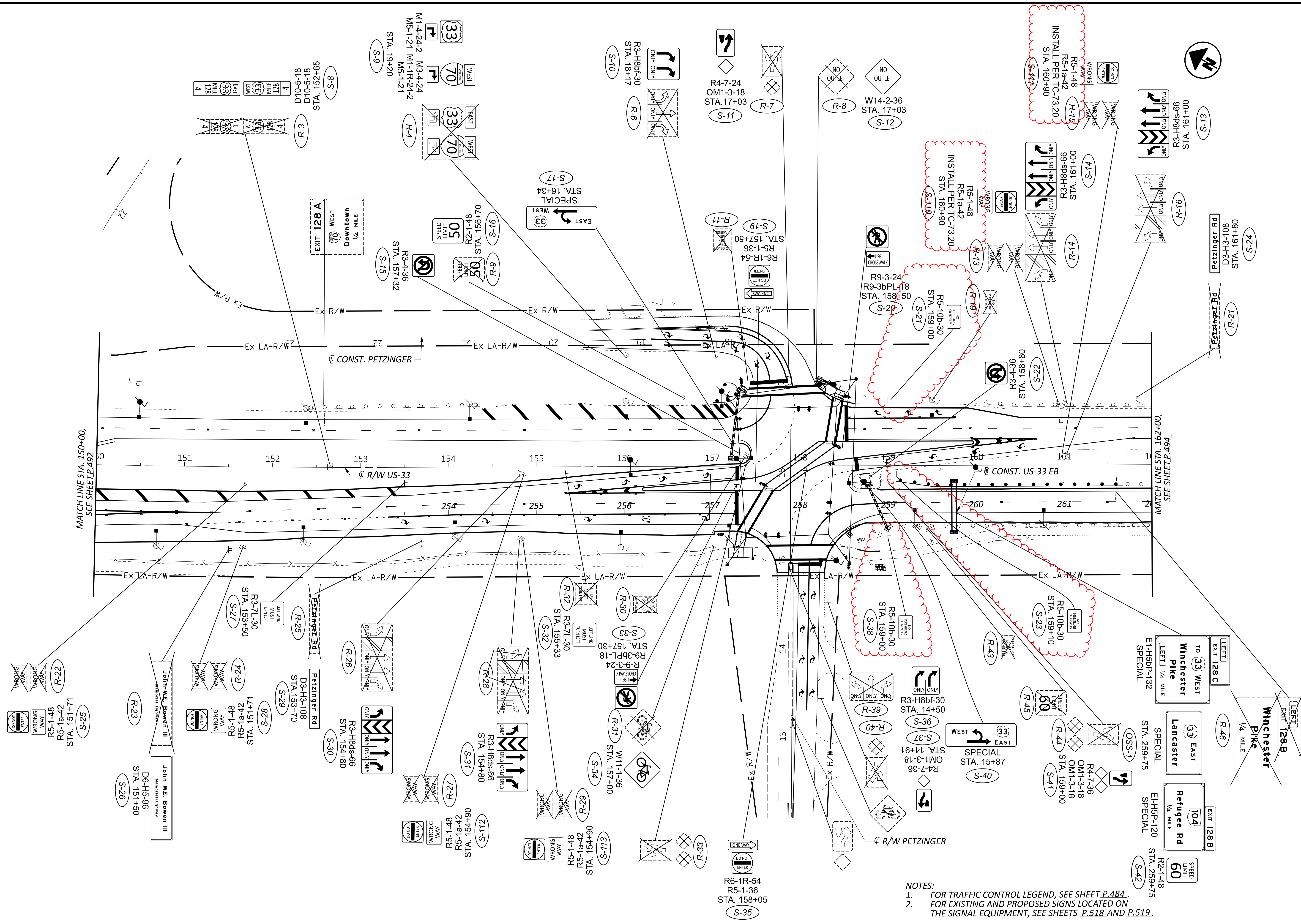
DESIGNER
DLS

REVIEWER
AKF 11/02/22

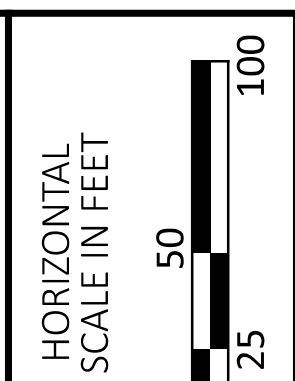
PROJECT ID
113744

SHEET TOTAL
P.490 625

FOR TRAFFIC CONTROL LEGEND, SEE SHEET P.484 .

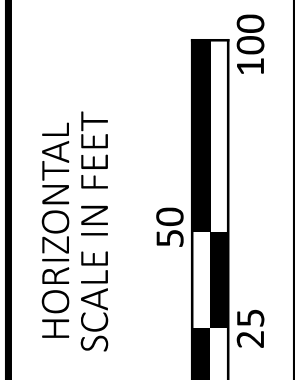
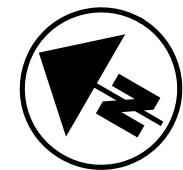
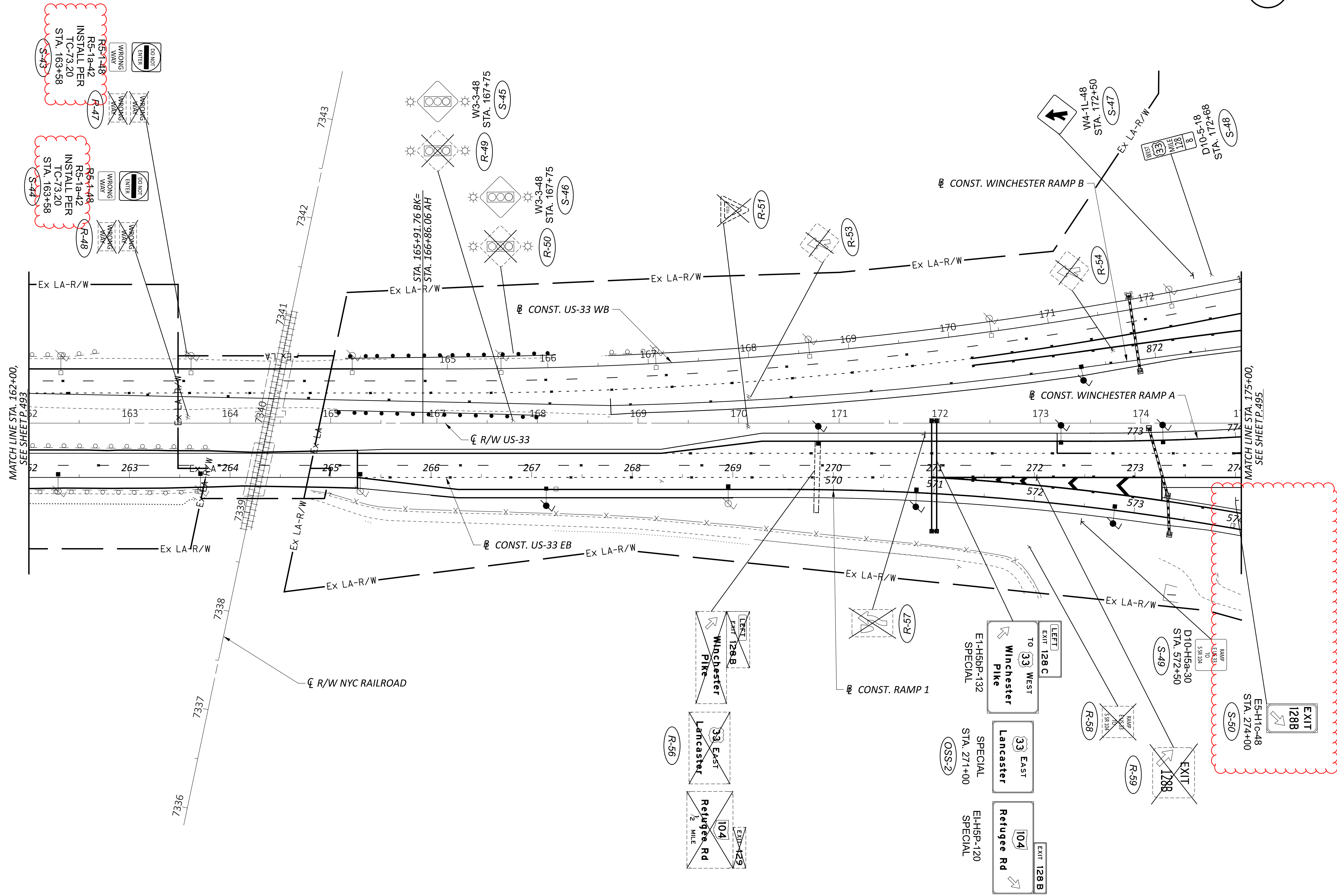


NOTES:
 1. FOR TRAFFIC CONTROL LEGEND, SEE SHEET P.484.
 2. FOR EXISTING AND PROPOSED SIGNS LOCATED ON THE SIGNAL EQUIPMENT, SEE SHEETS P.518 AND P.519.



SIGNING PLAN
 STA. 150+00 TO STA. 162+00

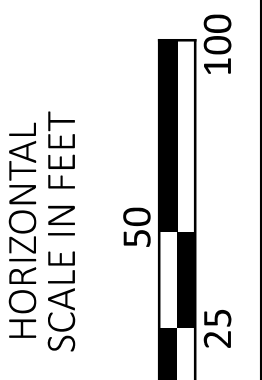
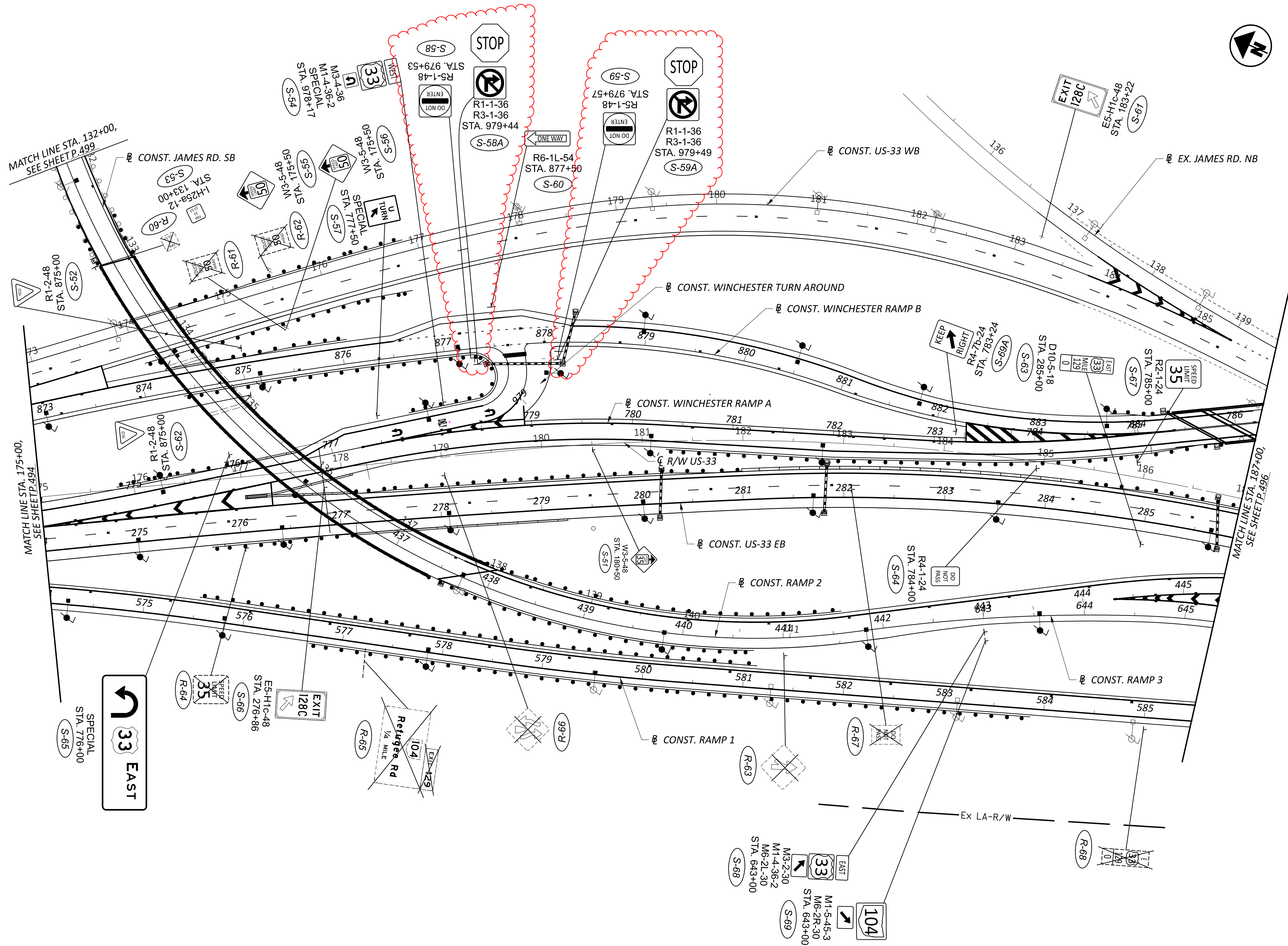
DESIGN AGENCY	
GPD GROUP	
DESIGNER	DLS
REVIEWER	AKF 11/02/22
PROJECT ID	113744
SHEET TOTAL	P.493 625



SIGNING PLAN
STA. 162+00 TO STA. 175+00



DESIGN AGENCY	GPD GROUP
DESIGNER	DLS
REVIEWER	AKF
DATE	11/02/22
PROJECT ID	113744
SHEET TOTAL	P.494 625



SIGNING PLAN
STA. 175+00 TO STA. 187+00

DESIGN AGENCY



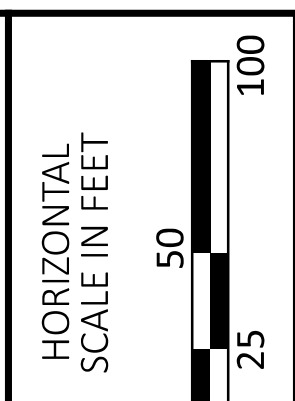
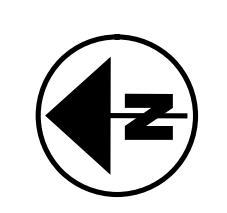
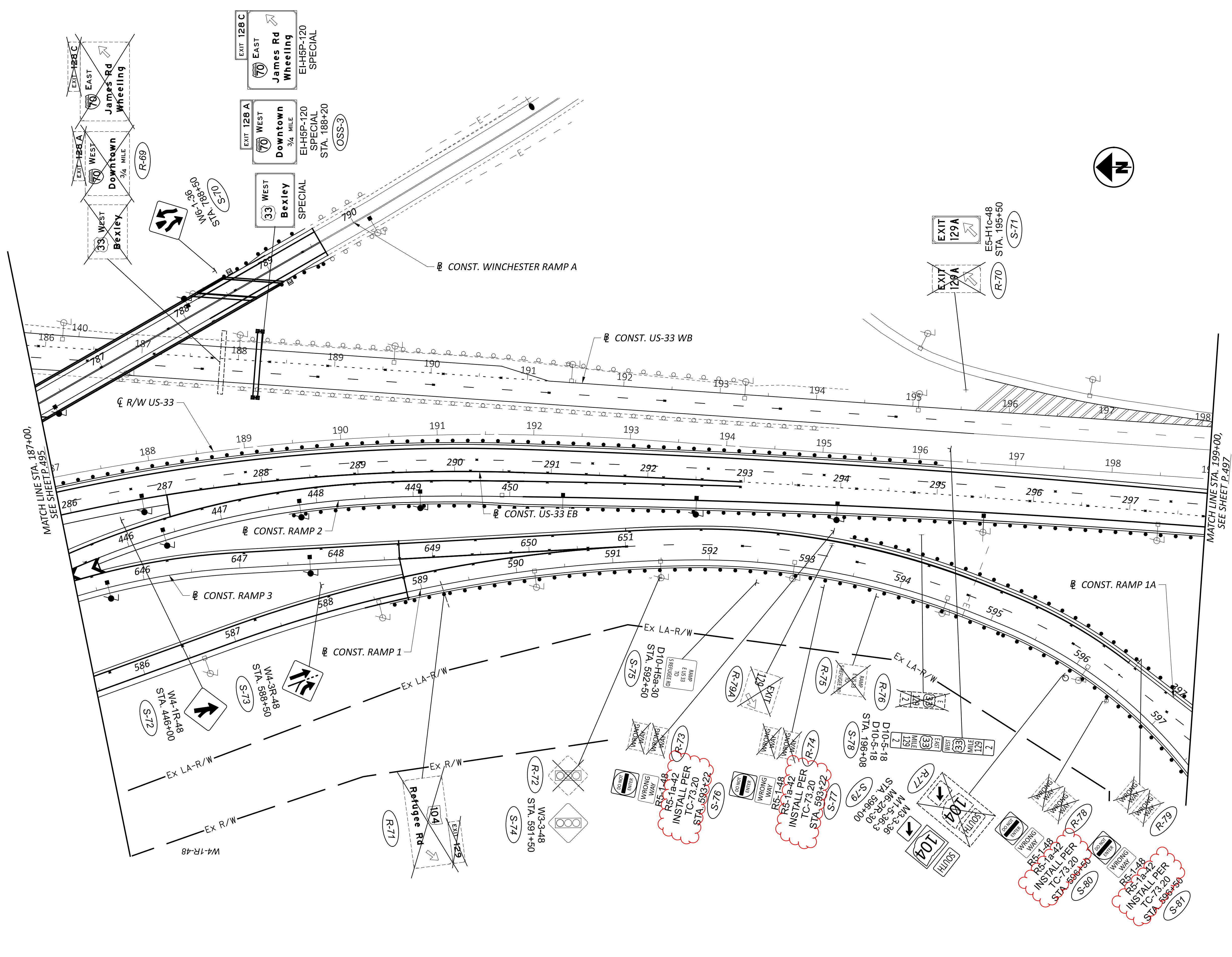
DESIGNER
DLS

REVIEWER
AKF 11/02/22

PROJECT ID
113744

SHEET TOTAL
P.495 625

FOR TRAFFIC CONTROL LEGEND, SEE SHEET P.484 .

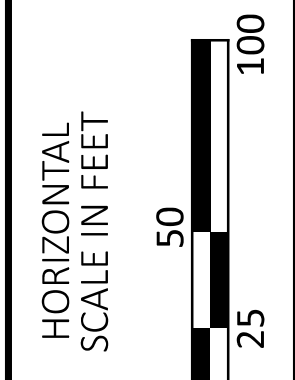
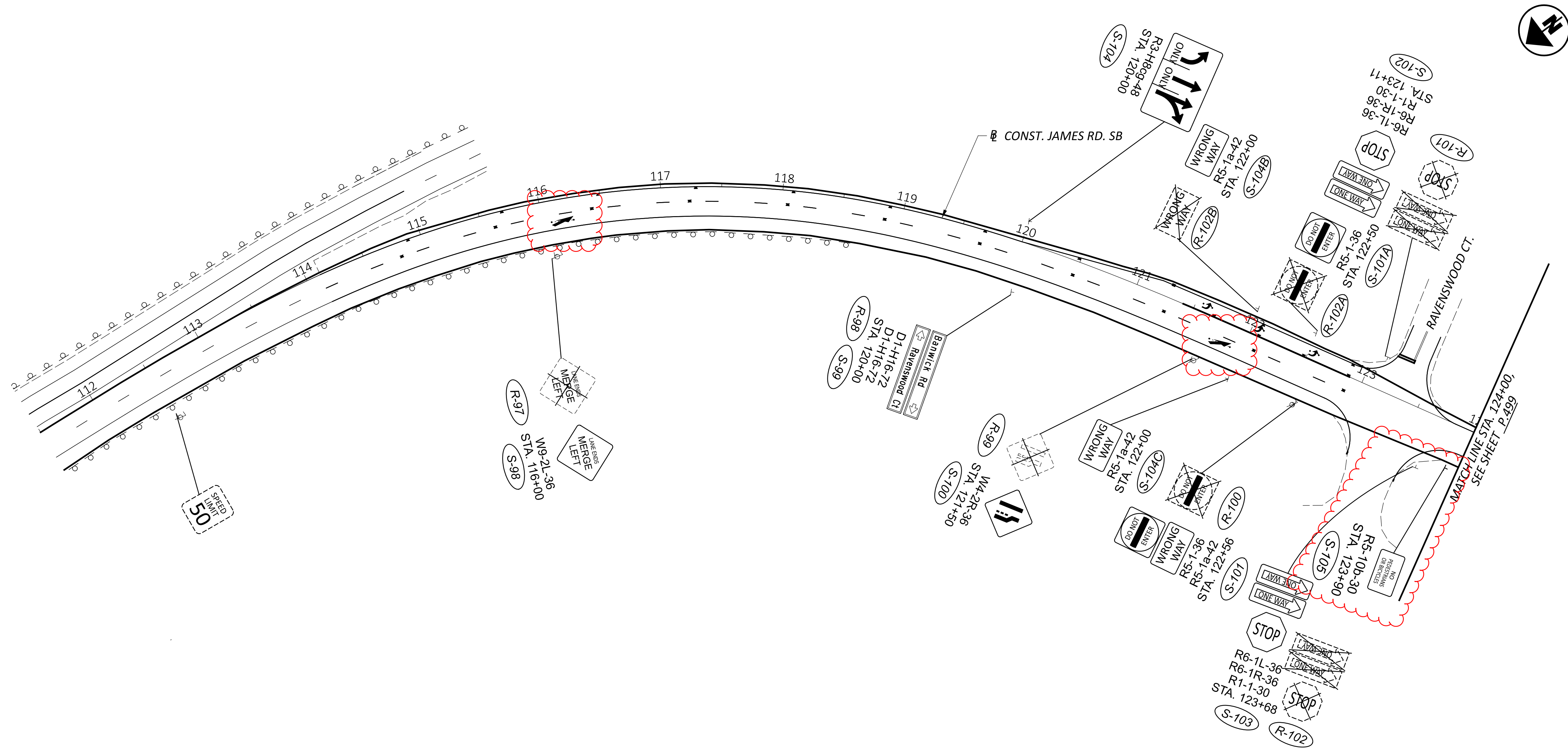


SIGNING PLAN
STA. 187+00 TO STA. 199+00



DESIGNER	DLS
REVIEWER	AKF 11/02/22
PROJECT ID	113744
SHEET TOTAL	P.496 625

FOR TRAFFIC CONTROL LEGEND, SEE SHEET P.484 .



**SIGNING PLAN - JAMES RD SB
BEGIN TO STA. 124+00**

DESIGN AGENCY



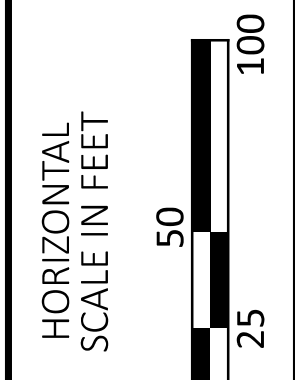
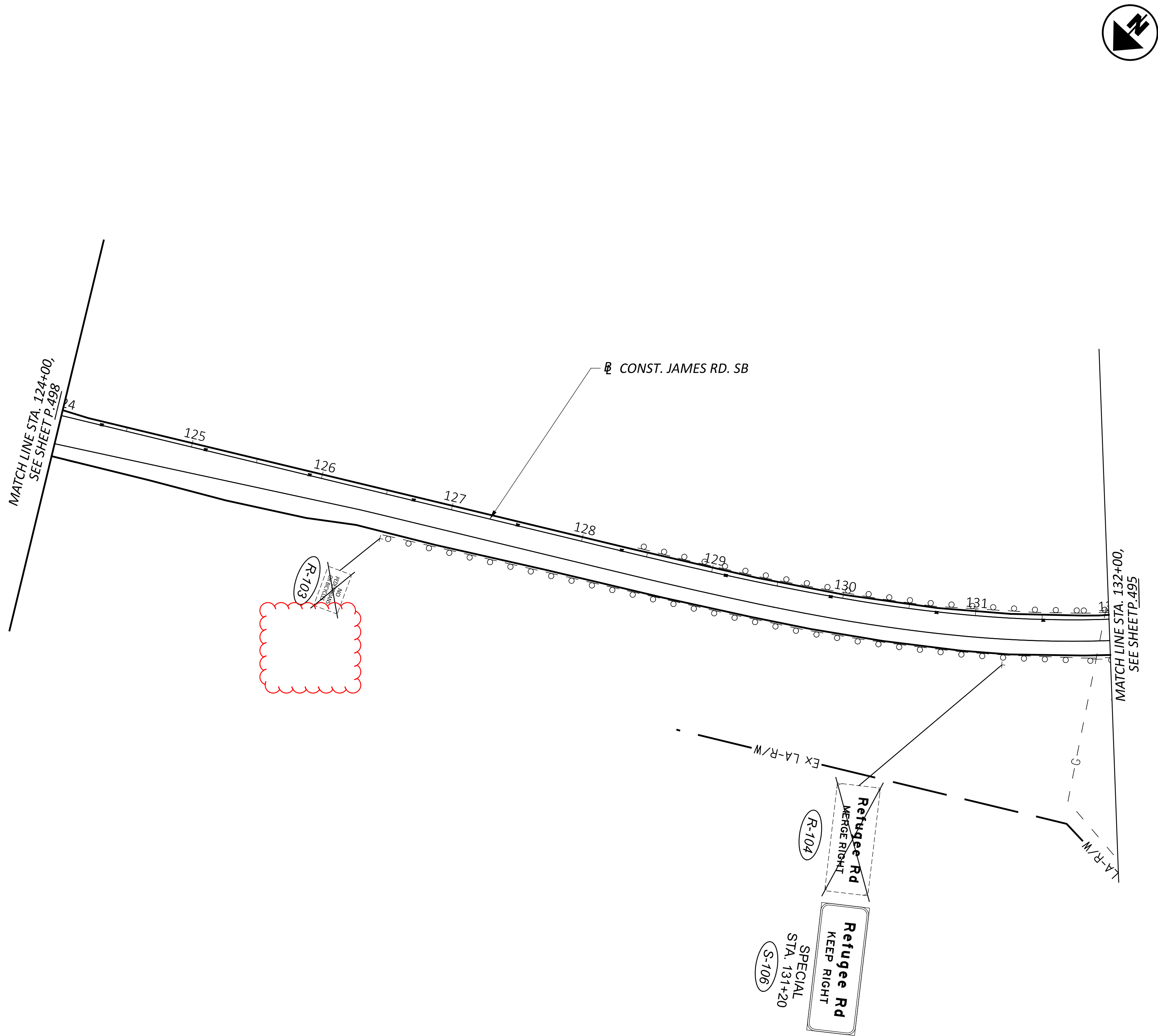
DESIGNER
DLS

REVIEWER
AKF 11/02/22

PROJECT ID
113744

SHEET TOTAL
P.498 625

FOR TRAFFIC CONTROL LEGEND, SEE SHEET P.484 .



SIGNING PLAN - JAMES RD. SB
STA. 124+00 TO STA. 132+00

DESIGN AGENCY



DESIGNER
DLS

REVIEWER
AKF 11/02/22

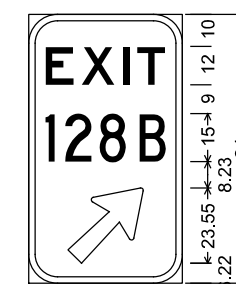
PROJECT ID
113744

SHEET	TOTAL
P.499	625

FOR TRAFFIC CONTROL LEGEND, SEE SHEET P.484 .

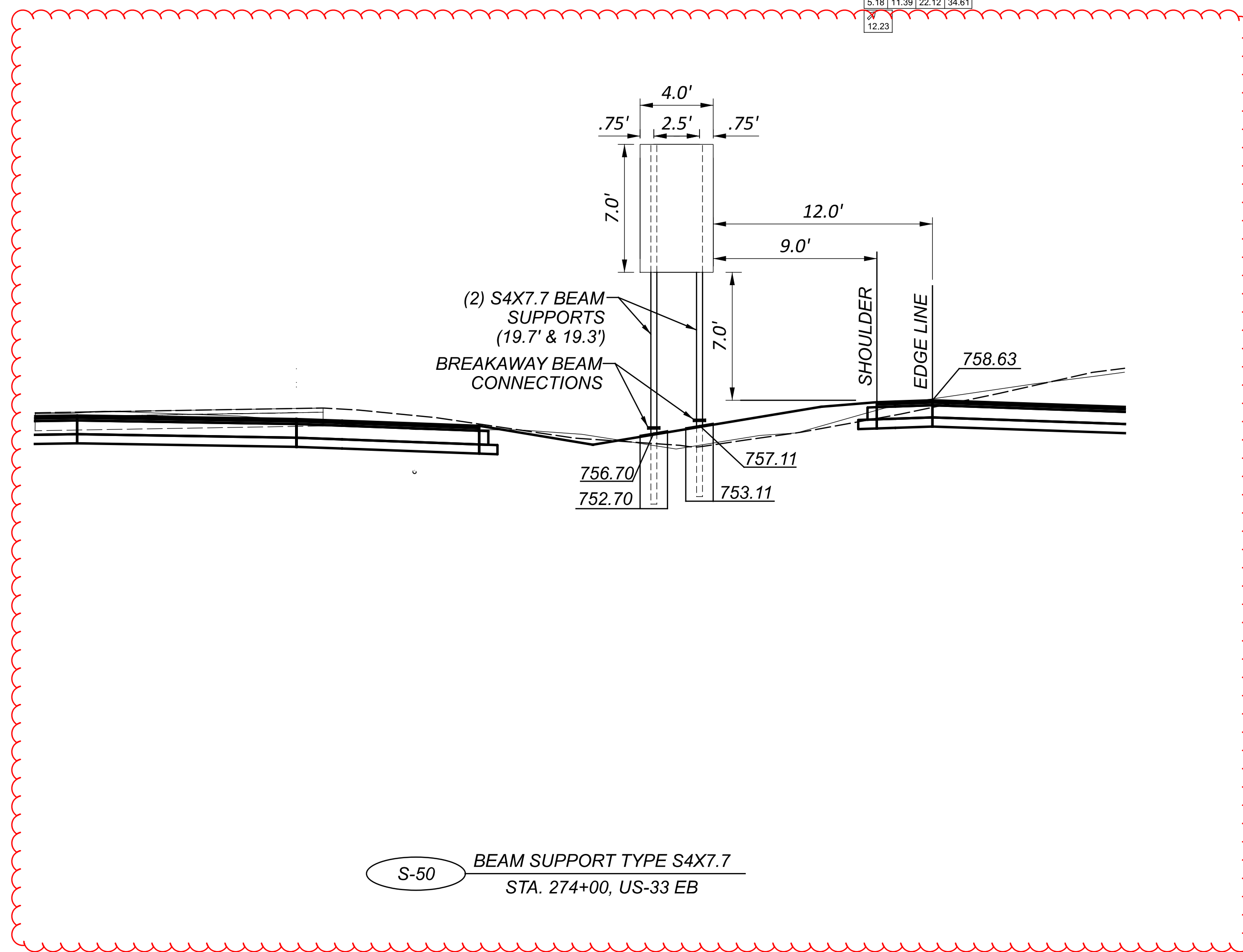
PANEL A	
SIGN DESIGNATION:	E5-H1c-48
PANEL SIZE:	4' X 7'
BACKGROUND:	GREEN
FILL COLOR:	WHITE
PROPOSED BEAM INFORMATION	
TOTAL SIGN AREA:	28.0 SQ. FT.
DESIGN TYPE:	S4X7.7
SIZE:	4" X 2-5/8"
FOUNDATION INFORMATION	
DIAMETER:	1.5'
DEPTH:	4.0'

- NOTES:**
1. CONTRACTOR SHALL FIELD VERIFY EXISTING ELEVATIONS PRIOR TO THE ORDERING OF ANY MATERIALS.
 2. ALL SIGNS ARE VIEWED IN THE DIRECTION OF TRAVEL.
 3. CONTRACTOR SHALL LOCATE ALL UNDERGROUND UTILITIES IN FIELD PRIOR TO ANY EXCAVATION.



9.00" Radius, 2.00" Border, White on Green:
 "EXIT", E;
 "128B", C;
 Arrow A-2 - 30.00" 45";
 Table of letter and object lefts

E	X	1	T
5.29	16.58	23.36	33.90
1	2	8	C
5.18	11.39	22.12	34.61
3"			
12.23			

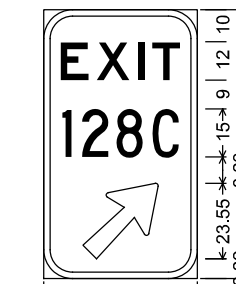


S-50 BEAM SUPPORT TYPE S4X7.7
 STA. 274+00, US-33 EB

SEE SHEET P.494 FOR PLAN VIEW.

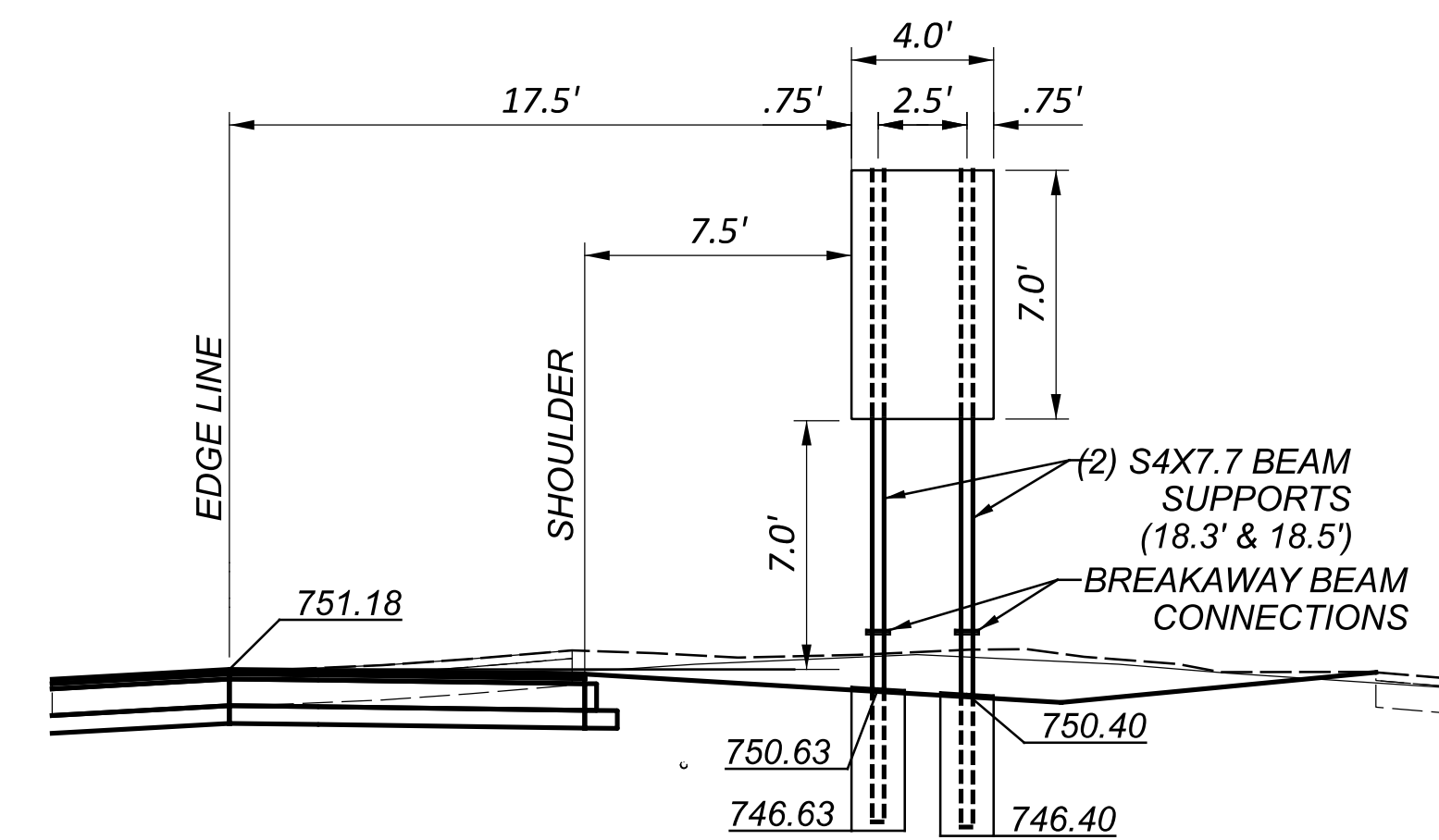
PANEL A	
SIGN DESIGNATION:	E5-H1c-48
PANEL SIZE:	4' X 7'
BACKGROUND:	GREEN
FILL COLOR:	WHITE
PROPOSED BEAM INFORMATION	
TOTAL SIGN AREA:	28.0 SQ. FT.
DESIGN TYPE:	S4X7.7
SIZE:	4" X 2-5/8"
FOUNDATION INFORMATION	
DIAMETER:	1.5'
DEPTH:	4.0'

- NOTES:**
1. CONTRACTOR SHALL FIELD VERIFY EXISTING ELEVATIONS PRIOR TO THE ORDERING OF ANY MATERIALS.
 2. ALL SIGNS ARE VIEWED IN THE DIRECTION OF TRAVEL.
 3. CONTRACTOR SHALL LOCATE ALL UNDERGROUND UTILITIES IN FIELD PRIOR TO ANY EXCAVATION.



9.00" Radius, 2.00" Border, White on Green:
 "EXIT", E;
 "128C", C;
 Arrow A-2 - 30.00" 45";
 Table of letter and object lefts

E	X	1	T
5.29	16.58	23.36	33.90
1	2	8	C
5.79	11.99	22.73	34.01
3"			
12.23			



S-61 BEAM SUPPORT TYPE S4X7.7
 STA. 183+22, US-33 WB

SEE SHEET P.495 FOR PLAN VIEW.

DESIGN AGENCY



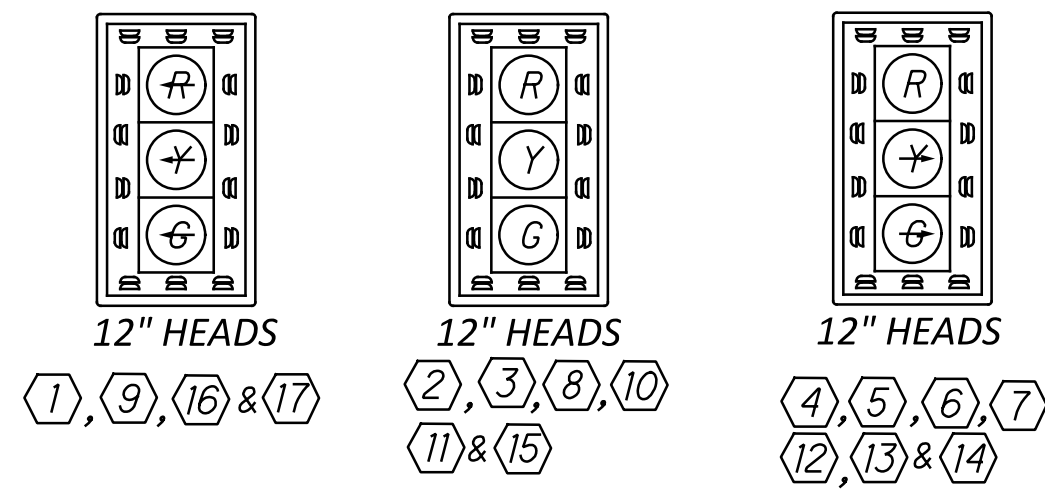
DESIGNER
 RNP

REVIEWER
 AFK 12-15-25

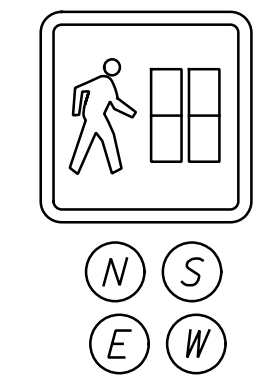
PROJECT ID
 113744

SHEET TOTAL
 P.507 625

VEHICULAR TRAFFIC SIGNAL HEAD CONFIGURATION

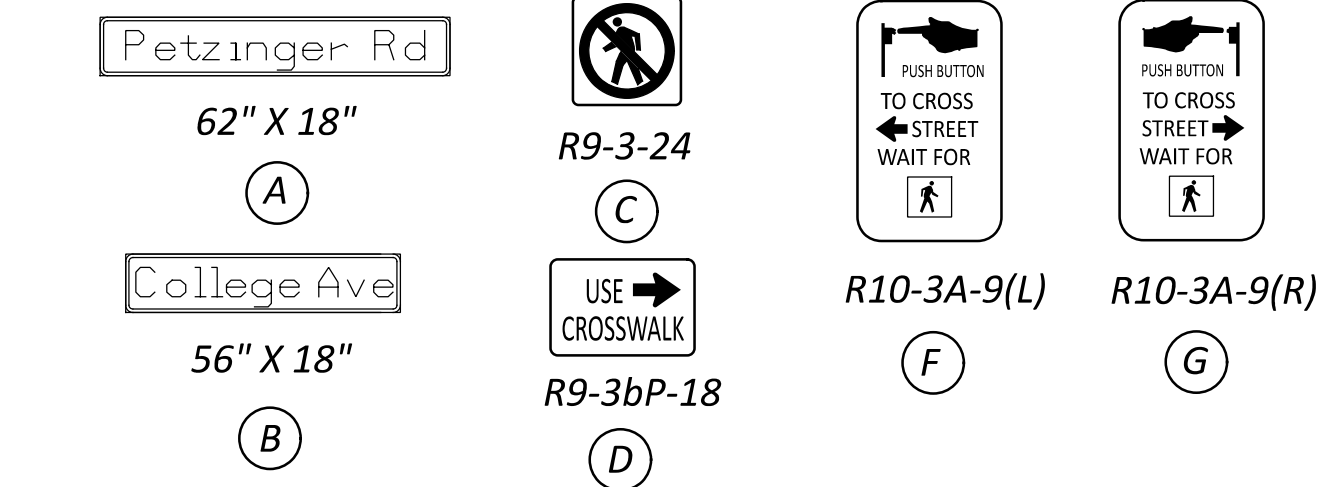


PROPOSED PEDESTRIAN SIGNAL HEAD CONFIGURATION

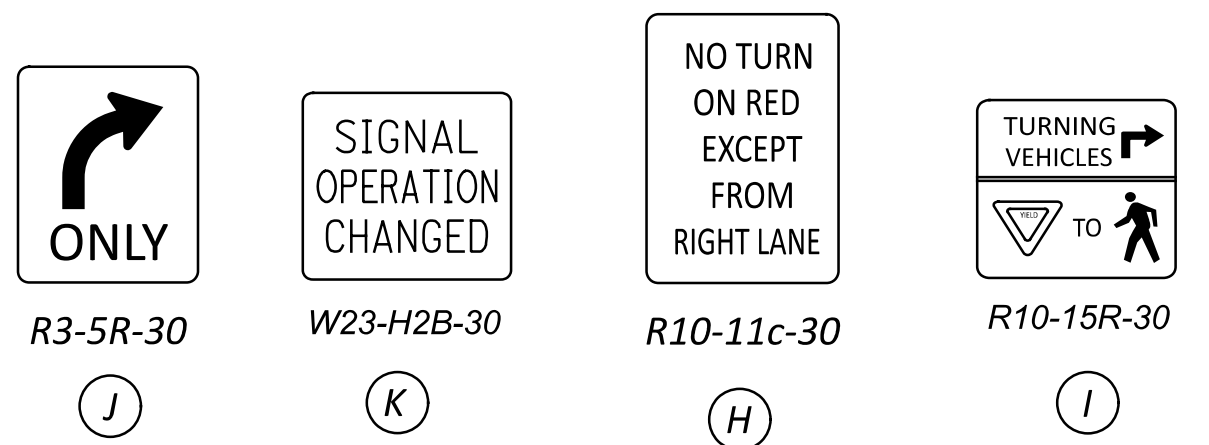


POLE N/E-1 COMBINATION STRAIN POLE
W/30' BRACKET ARM
W/TRAFFIC FLOW MONITOR
STA. 157+17.9, 78.8' LT.

POLE MOUNTED SIGNS



SPAN WIRE MOUNTED SIGNS



PULL BOX, 27"
STA. 157+47.8, 21.0' RT.
(1) - 2" CONDUIT W/ (1) - 2C
(1) - 2" CONDUIT W/ (3) - 7C & (3) - GND
IN TRENCH = 13'

BORE PIT
POLE N/W-4 PEDESTRIAN PEDESTAL, 12.7'
W/ PEDESTRIAN PUSHBUTTON
W/ PEDESTRIAN SIGNAL HEAD
W/SUPPLEMENTAL SIGNAL HEAD
STA. 157+59.9', 21.4' RT.
DILEMMA ZONE RADAR UNIT #1

(1) - 2" CONDUIT W/ (2) - 2C
(1) - 2" CONDUIT W/ (3) - 7C & (1) - GND
DIRECTIONALLY DRILLED = 69'

POLE N/W-5 PEDESTRIAN PEDESTAL, 21.0'
W/ SUPPLEMENTAL SIGNAL HEAD
(1) - 2" CONDUIT W/ (1) - 7C & (1) - GND
IN TRENCH = 32'

POLE N/W-2 PEDESTRIAN PEDESTAL, 10.7'
W/ PEDESTRIAN PUSHBUTTON
W/ PEDESTRIAN SIGNAL HEAD
STA. 157+25.6, 86.7' RT.

(1) - 2" CONDUIT W/ (2) - 2C
(1) - 2" CONDUIT W/ (2) - 7C & (1) - GND
IN TRENCH = 16'

(1) - 2" CONDUIT W/ (2) - 2C
(1) - 2" CONDUIT W/ (3) - 7C & (5) - GND
IN TRENCH = 22'
IN TRENCH IN PAVED AREA = 14'

POLE N/W-1 STRAIN POLE W/DEEP FOUNDATION (20')
W/ (2) - STOP BAR LINE RADAR UNITS #2 (SB) AND #3 (WB)
STA. 157+16.9, 111.5' RT.

BORE PIT
PULL BOX, 27"
STA. 157+41.1, 89.9' RT.

(1) - 2" CONDUIT W/ (1) - 2C
(1) - 2" CONDUIT W/ (2) - 7C & (1) - GND
IN TRENCH IN PAVED AREA = 18'

POLE N/W-3 PEDESTRIAN PEDESTAL, 10.7'
STA. 157+46.5, 105.2' RT

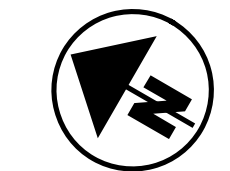
(1) - 2" CONDUIT W/ (1) - 2C
(1) - 2" CONDUIT W/ (1) - 10C, (1) - 9C & (2) - GND
ENCASED IN TRENCH = 24'

PULL BOX, 27"
STA. 157+28.5, 97.9' LT.

(1) - 2" CONDUIT W/ (1) - 2C
(1) - 2" CONDUIT W/ (1) - 10C, (1) - 9C & (1) - GND
IN TRENCH = 7'

POLE N/E-2 PEDESTRIAN PEDESTAL, 21.0'
W/ PEDESTRIAN PUSHBUTTON
W/ PEDESTRIAN SIGNAL HEAD
W/ SUPPLEMENTAL SIGNAL HEAD
STA. 157+33.6, 94.0' LT.

NOTE: R/W IS BEYOND EXTENT OF THE SHEET ON THE EAST SIDE



POLE S/E-3 PEDESTRIAN PEDESTAL, 21.0'
W/ PEDESTRIAN PUSHBUTTON
W/ PEDESTRIAN SIGNAL HEAD
W/ SUPPLEMENTAL SIGNAL HEAD
STA. 158+23.8, 94' LT.

(1) - 2" CONDUIT W/ (2) - 2C
(1) - 2" CONDUIT W/ (2) - 10C, (2) - 9C & (1) - GND
IN TRENCH = 28'

POLE S/E-1 STRAIN POLE
W/ (2) - STOP BAR LINE RADAR UNITS #5 (NB) AND #6 (EB)
STA. 158+64.1, 98.6' LT.

(1) - 2" CONDUIT W/ (2) - 2C
(1) - 2" CONDUIT W/ (2) - 10C, (2) - 9C, (1) - 7C & (3) - GND
IN TRENCH = 18'

PULL BOX, 27"
STA. 158+40.6, 94.7' LT.

(1) - 2" CONDUIT W/ (1) - 2C
(1) - 2" CONDUIT W/ (2) - 9C, (1) - 7C & (1) - GND
IN TRENCH = 11'

POLE S/E-2 PEDESTRIAN PEDESTAL, 21'
W/ PEDESTRIAN PUSHBUTTON
W/ PEDESTRIAN SIGNAL HEAD
W/ SUPPLEMENTAL SIGNAL HEAD
STA. 158+50.9, 78.9' LT.

DILEMMA ZONE RADAR UNIT #4

POLE S/E-4 PEDESTRIAN PEDESTAL, 12.7'
W/ PEDESTRIAN PUSHBUTTON
W/ PEDESTRIAN SIGNAL HEAD
W/ SUPPLEMENTAL SIGNAL HEAD
STA. 158+40.3, 23.2' LT.

(1) - 2" CONDUIT W/ (1) - 2C
(1) - 2" CONDUIT W/ (2) - 7C & (1) - GND
IN TRENCH = 108'

POLE S/W-2 PEDESTRIAN PEDESTAL, 10.7'
W/ PEDESTRIAN PUSH BUTTON
W/ PEDESTRIAN SIGNAL HEAD
STA. 158+52.2, 104.9' RT

(1) - 2" CONDUIT W/ (2) - 2C
(1) - 2" CONDUIT W/ (1) - 7C & (1) - GND
IN TRENCH = 25'

POLE S/W-1 STRAIN POLE
W/ SUPPLEMENTAL SIGNAL HEAD
STA. 158+55.64, 110.6' RT

PULL BOX, 48"
STA. 158+74.6, 113.3' RT.

(1) - 3" CONDUIT W/ INTERCONNECT & (1) CAT5E (SEE INTERCONNECT PLANS)
(1) - 3" CONDUIT W/ (4) - 10C
(1) - 3" CONDUIT W/ (1) - 9C, (7) - 7C & (2) - GND
(1) - 3" CONDUIT W/ (5) - 2C & (6) - RADAR
IN TRENCH = 13'

GROUND MOUNTED CONTROLLER CABINET WITH UPS AND WORK PAD
STA. 158+88.6, 114.4' RT.

(1) - 3" CONDUIT W/ (4) - 10C
(1) - 3" CONDUIT W/ (1) - 9C, (7) - 7C & (2) - GND
(1) - 3" CONDUIT W/ (5) - 2C, (6) - RADAR & (1) - CAT5E
IN TRENCH = 19'

(1) - 2" CONDUIT W/ (1) - 2/C NO. 6 AWG POWER & (1) - GND
ENCASED IN TRENCH = 4'
PULL BOX, 12" X 18"
STA. 158+81.72, 115.3' RT.

(1) - 2" CONDUIT W/ (1) - 2/C NO. 6 AWG POWER & (1) - GND
ENCASED IN TRENCH = 2'

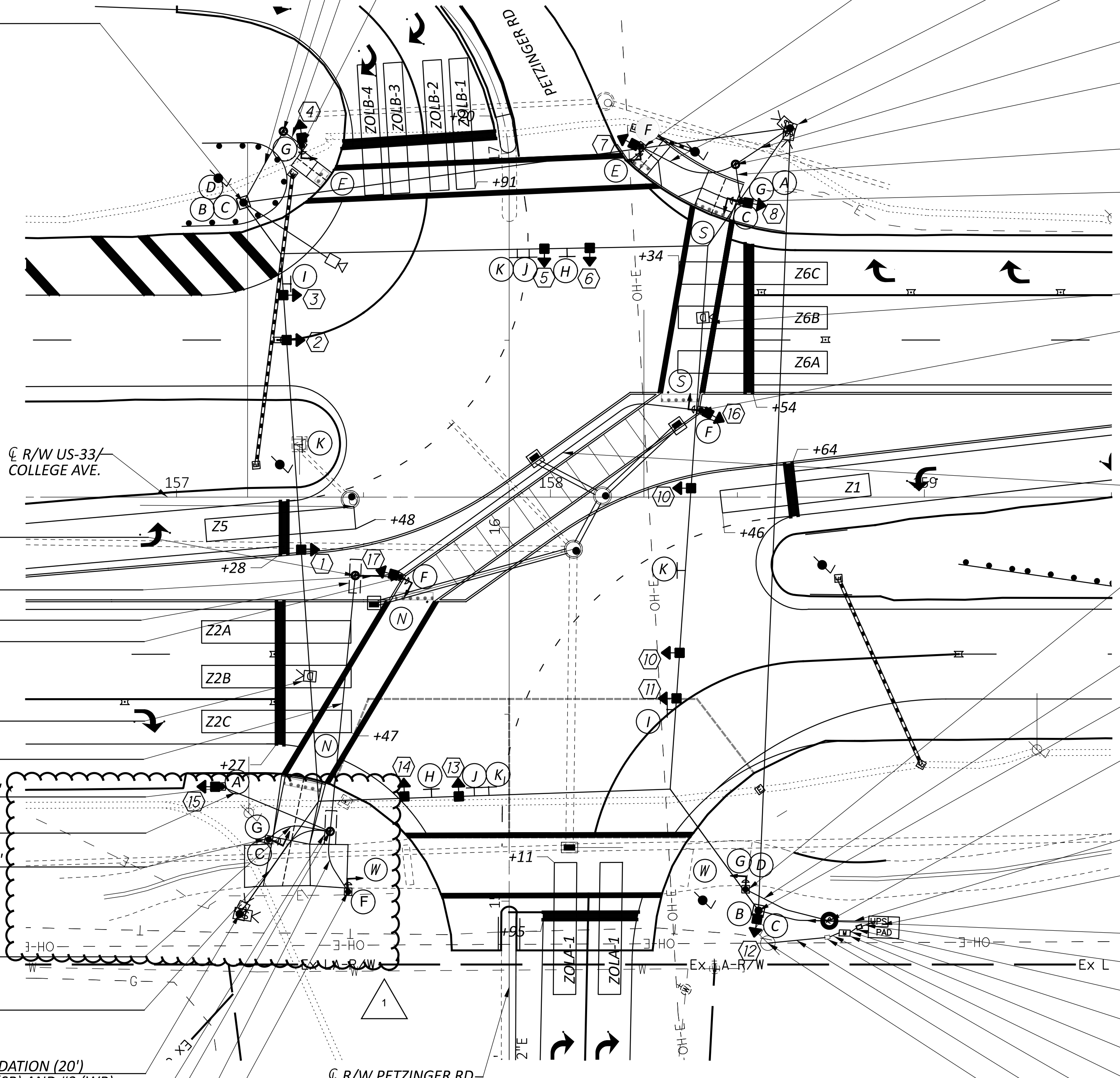
POWER METER CABINET*
STA. 158+81.03, 118.0' RT
(1) - 2" CONDUIT W/ (1) - 3/C NO. 6 AWG POWER
ENCASED IN TRENCH = 2'

PULL BOX, 12" X 18"
STA. 158+74.0, 117.7' RT.

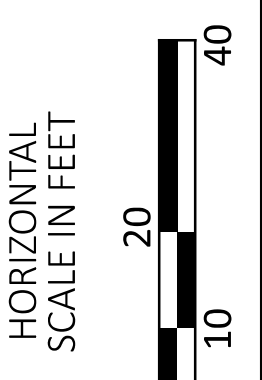
(1) - 2" CONDUIT W/ (1) - 2/C NO. 6 AWG POWER
ENCASED IN TRENCH = 16'

PROPOSED POWER SOURCE
W/ 2" CONDUIT RISER, 725.053
STA. 158+58.14, 119.3' RT

*POWER METER CABINET HOOD SHALL OPEN TOWARDS US33/COLLEGE AVE.



AT THE TIME OF DESIGN, COORDINATION WITH AMERICAN ELECTRIC POWER (AEP) WAS ONGOING. THE RELOCATION AND/OR RAISING OF OVERHEAD POWER LINES IS REQUIRED IN ORDER TO MEET THE MINIMUM CLEARANCE REQUIREMENTS AS SPECIFIED ON OSHA SECTION 1928.1408. IT SHALL BE THE CONTRACTOR AND ODOT'S RESPONSIBILITY TO ENSURE THAT ALL PROPOSED TRAFFIC SIGNAL FACILITIES INCLUDING THE POLES, SIGNAL TETHER, SIGNAL MESSENGER WIRE, AND BRACKET ARMS MEET THE MINIMUM CLEARANCE FROM THE NEUTRAL, SECONDARIES, PRIMARIES, ETC., AS SPECIFIED IN OSHA 1926.1408. ADDITIONALLY, IT SHALL BE THE CONTRACTOR AND ODOT'S RESPONSIBILITY TO ENSURE THAT NO OVERHEAD COMMUNICATION CABLES OBSTRUCT VISIBILITY OF ANY TRAFFIC SIGNAL HEAD OR REST ON OR MAKE ANY CONTACT WITH THE TRAFFIC SIGNAL FACILITIES. THE CONTRACTOR SHALL DEMONSTRATE TO THE ENGINEER THAT THESE CLEARANCE REQUIREMENTS HAVE BEEN SATISFIED PRIOR TO ACCEPTANCE OF THE SIGNAL BY THE CITY. IF THE CLEARANCE REQUIREMENTS CANNOT BE MET, A DESIGN REVISION MAY BE REQUIRED TO REDESIGN THE SIGNAL TO MEET THE REQUIRED CLEARANCES.



TRAFFIC SIGNAL INSTALLATION PLAN
COLLEGE AVE. / U.S. 33 AT PETZINGER RD

DESIGN AGENCY



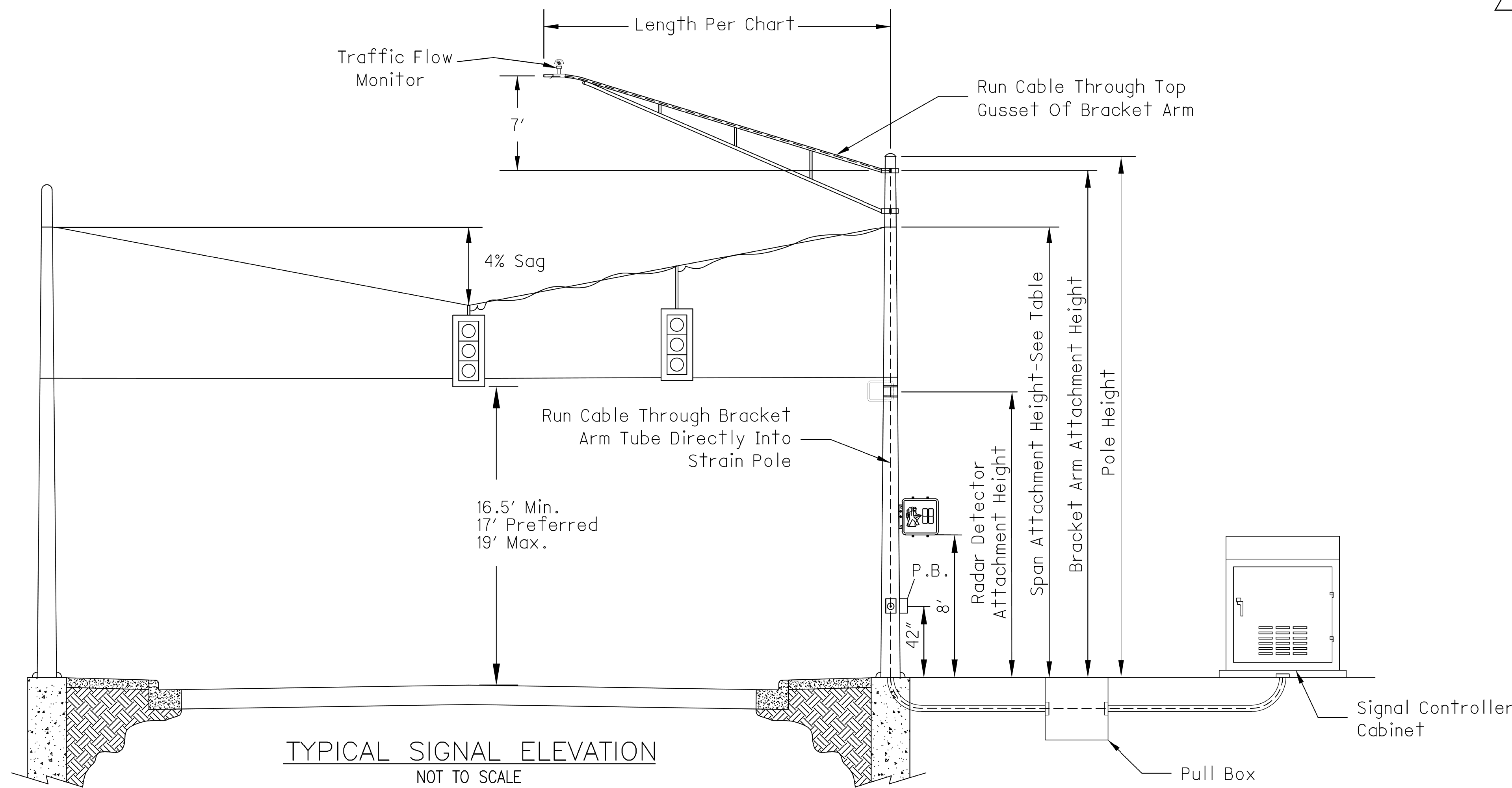
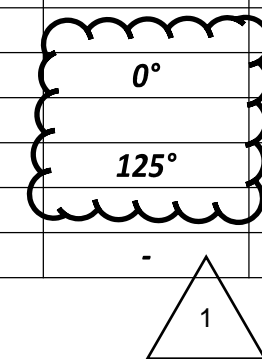
DESIGNER
AOK

REVIEWER
KMG 12/15/25

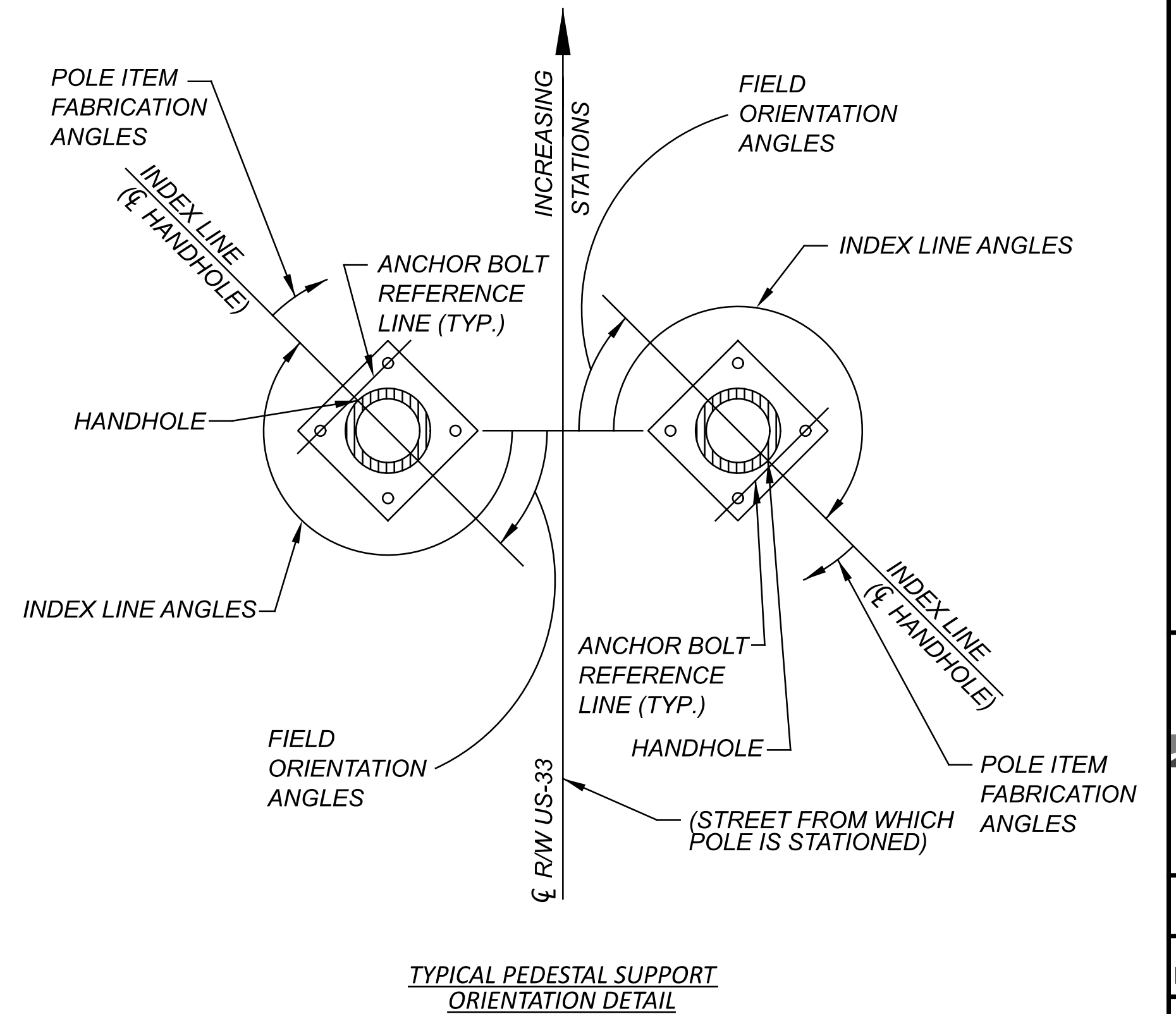
PROJECT ID
113744

SHEET TOTAL
P.519 625

INTERSECTION	POLE SIZES & ATTACHMENT HEIGHTS						BRACKET ARM		POLE FABRICATION DATA CLOCKWISE FROM HANDHOLE AT 0 DEGREES							FIELD ORIENTATION		
	SUPPORT DESIGNATION	POLE COLOR FED. STD. 595B	POLE DESIGN NO.	POLE HT. (FT.)	SPAN ATTACHMENT HT. (FT.)	RADAR DETECTOR (FT.)	ATTACHMENT HEIGHT (FT.)	BRACKET ARM LENGTH (FT.)	ANCHOR BOLT REFERENCE LINE	2" BHC ANGLE-HT. DEG.-FT.	3" BHC ANGLE-HT. DEG.-FT.	PED. SIGNAL	PED. PUSH BUTTON	STREET NAME SIGNS	BRACKET ARM	AUX. SIGNAL HEAD	INDEX LINE ANGLE (HANDHOLE)	ANCHOR BOLT REF. LINE
	S/E-1	↑	13	33'	28.5'	18'/18.5'	-	-	90°	180° - 30'	-	-	-	-	-	198°	108°	759.86
	S/E-2		PEDESTAL	21'	-	-	-	-	90°	-	-	263°	257°	158	-	78°	202°	SEE SHEET P.420
	S/E-3		PEDESTAL	21'	-	-	-	-	90°	-	-	138°	135°	-	-	257°	140°	SEE SHEET P.420
	S/E-4		PEDESTAL	12.7'	-	-	-	-	90°	-	-	176°	179°	-	-	18°	280°	SEE SHEET P.418
	N/E-1		13	32'	26'	-	31'	30'	90°	180° - 27'	-	-	-	107°	141°	-	163°	758.50
	N/E-2		PEDESTAL	21'	-	-	-	-	90°	-	-	236°	230°	-	-	46°	127°	SEE SHEET P.420
PETZINGER RD. AT US-33	N/W-1	SEMI-GLOSS BLACK #27038	13	34'	29'	18'/18.5'	-	-	90°	180° - 30.5'	-	-	-	-	-	192°	102°	757.69
	N/W-2		PEDESTAL	10.7	-	-	-	-	90°	-	-	293°	290°	-	-	191°	101°	SEE SHEET P.421
	N/W-3		PEDESTAL	10.7'	-	-	-	-	90°	-	-	185°	182°	-	-	181°	91°	SEE SHEET P.421
	N/W-4		PEDESTAL	12.7'	-	-	-	-	90°	-	-	176°	179°	-	-	344°	301°	SEE SHEET P.418
	N/W-5		PEDESTAL	21'	-	-	-	-	90°	-	-	-	-	-	79°	182°	92°	758.68
	S/W-1		13	38'	33.5'	-	-	-	90°	-	150° - 34.5' / 210° - 34.5'	-	-	-	45°	192°	102°	756.62
	S/W-2	↓	PEDESTAL	10.7'	-	-	-	-	90°	-	-	183°	181°	-	-	180°	90°	756.34



TYPICAL SIGNAL ELEVATION
NOT TO SCALE

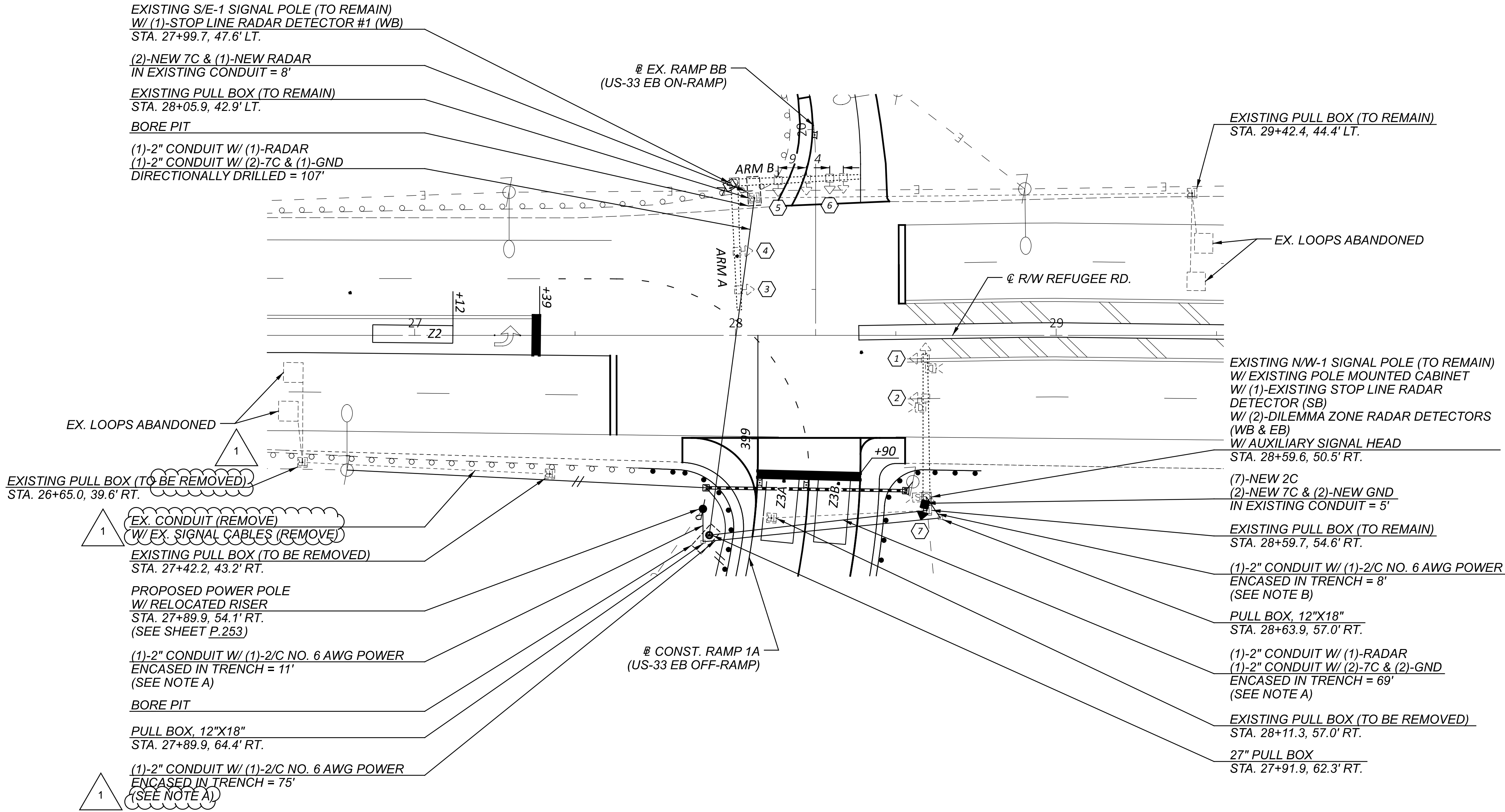
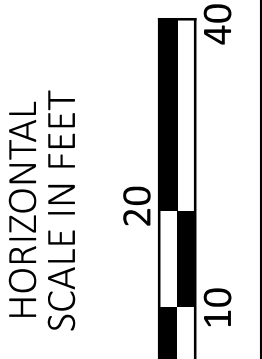
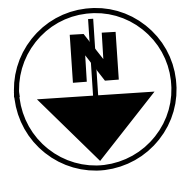


TYPICAL PEDESTAL SUPPORT
ORIENTATION DETAIL

INTERSECTION LAYOUT NOTES:

- CENTER ALL LOOPS OR DETECTION ZONES IN THE CENTER OF THEIR LANE, UNLESS SPECIFIED OTHERWISE. INSTALL LOOPS AFTER THE ASPHALT SURFACE COURSE IS LAID.
- N/A
- N/A
- N/A
- UNDERGROUND CONDUIT AND TRENCH THAT ARE UNDER PROPOSED SIDEWALK OR ROADWAY AREAS SHALL BE INSTALLED PRIOR TO THE PLACEMENT OF SIDEWALKS OR ANY ASPHALT OR CONCRETE ROADWAY COURSE.
- N/A
- N/A
- N/A
- TAGGING OF CABLE IN THE PULL BOX IMMEDIATELY ADJACENT TO THE CONTROLLER CABINET IS NOT REQUIRED EXCEPT FOR TAGGING OF CERTAIN CABLE AS DIRECTED BY THE PROJECT ENGINEER, OR AS PER PLAN.
- ALL CABLES, UNLESS SPECIFIED IN THE PLANS, ARE TO BE ROUTED INSIDE THE ANCHOR BASE SIGNAL SUPPORT POLE OR PEDESTAL. CABLES NOT SERVING A GIVEN POLE OR PEDESTAL SHALL NOT BE ROUTED THROUGH THE POLE.
- USE A SEPARATE CONDUIT FOR EACH GROUPING OF CABLES UNLESS OTHERWISE INDICATED: ONE CONDUIT FOR 120VAC SIGNAL CABLE (3/C, 7/C, 9/C); ONE CONDUIT FOR POWER; ONE CONDUIT FOR 2 CONDUCTOR CABLE (LOOP & PUSHBUTTON); AND ONE CONDUIT FOR INTERCONNECT/COMMUNICATIONS CABLE (FIBER OPTIC, CAT 5E, ETC). ANY OTHER LOW VOLTAGE CABLE NOT SPECIFIED ABOVE CAN BE PLACED IN THE 2 CONDUCTOR CABLE CONDUIT. POWER CABLE MUST BE PLACED IN ITS OWN CONDUIT.
- UNLESS OTHERWISE SPECIFIED THE FOLLOWING SHALL APPLY. A PREFORMED PVC CONDUIT ELBOW SHALL BE USED TO CHANGE THE PVC CONDUIT DIRECTION BEYOND WHAT ITS NATURAL BENDING FLEX WOULD YIELD. RIGID METAL CONDUIT CAN BE BENT TO FORM AN ELBOW OR ANY OTHER BENDING ANGLE REQUIRED ONLY IF A PROPER CONDUIT BENDING MACHINE IS USED. THE ELBOW RADIUS FOR ANY NON-INTERCONNECT CONDUIT SHALL BE 24" OR LARGER WHEN USED IN A HORIZONTAL OR VERTICAL MANNER. ANY TYPE OF ELBOW USED FOR INTERCONNECT CONDUIT SHALL HAVE A RADIUS OF 36" OR LARGER WHEN USED IN A HORIZONTAL DIRECTION OR IN A VERTICAL DIRECTION WHEN THE TRENCH IS 36" OR DEEPER. IF THE TRENCH IS LESS THAN 36" THEN THE VERTICAL ELBOW RADIUS SHALL BE 24".
- ALL CLAMPS AND BANDING MATERIAL SHALL BE PAINTED TO MATCH THE SIGNAL SUPPORTS.
7/1/24

NOTE: R/W IS BEYOND EXTENT OF THE PLAN SHEET.



LEGEND

	PROP.	REL.	EXIST.
TRAFFIC SIGNAL 3 UNIT HEAD, 12"			
TRAFFIC SIGNAL 5 UNIT HEAD, 12"			
STOP LINE RADAR DETECTION UNIT			
DILEMMA DETECTION UNIT			
SIGNAL POLE			
PULL BOX, 27"			
PULL BOX, 12"x18"			
CABINET			

VEHICULAR TRAFFIC SIGNAL HEAD CONFIGURATION

Configuration	Symbol	Status
EXISTING 12" HEADS		1
EXISTING 12" HEADS		2, 3, 4
EXISTING 12" HEADS (TO BE RELOCATED)		5, 6
PROPOSED 12" HEADS		7

TRAFFIC SIGNAL REMOVAL CHART

QUANTITY	REMOVED ITEM DESCRIPTION	DELIVERED	DISPOSED	REUSE
2	VEHICULAR SIGNAL HEAD			X
3	PULL BOX LIDS AND FRAMES	X		
3	PULL BOX CASTINGS		X	
LUMP	SIGNAL CABLE AND CONDUIT		X	

DELIVERED TO 1820 E 17TH AVE.

- NOTES:**
- THE CONTRACTOR SHALL INTALL THE SIGNAL AND POWER CONDUIT IN THE SAME TRENCH WITH A 3" SEPARATION BETWEEN THE CONDUITS.
 - THE CONTRACTOR SHALL CONNECT THE PROPOSED CONDUIT TO AN EXISTING CONDUIT ELL AT THE BASE OF THE SIGNAL POLE SUPPORT.

TRAFFIC SIGNAL MODIFICATION PLAN
REFUGEE RD. / US-33 EB

DETAIL SHEET NOTES:

1. N/A
2. N/A
3. N/A
4. N/A
5. N/A
6. N/A
 - A. N/A
 - B. N/A
 - C. N/A
 - D. N/A
7. N/A
 - A. N/A
 - B. N/A

RADAR DETECTION ASSIGNMENTS

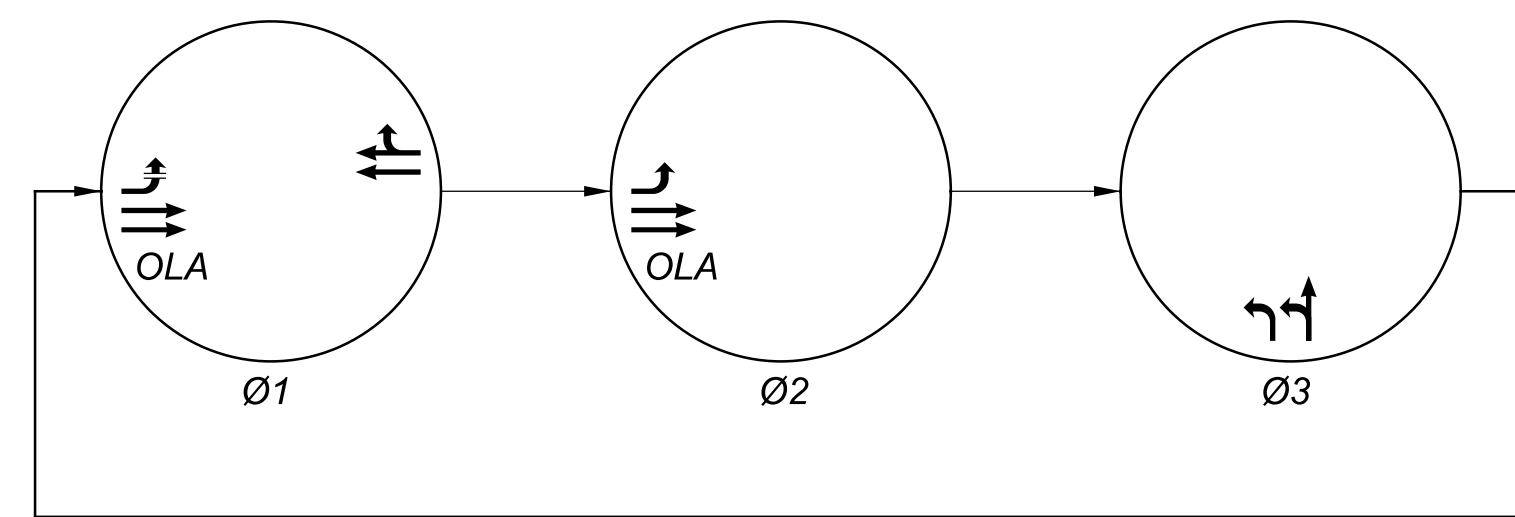
DETECTION ZONE (#)	RADAR DETECTOR (#)	PHASE	DETECTION ZONE SIZE (W' x L')	PRESENCE	PURPOSE	DELAY DATA	
						DELAY (SEC.)	INHIBIT DELAY DURING GREEN Ø
Z2	1	Ø2	5.5'x25'	X	CALL/EXTEND	3	Ø2
Z3A	EX.	Ø3	10'x20'	X	CALL/EXTEND	EX.	EX.
Z3B	EX.	Ø3	10'x20'	X	CALL/EXTEND	EX.	EX.

FIELD WIRING HOOK-UP CHART

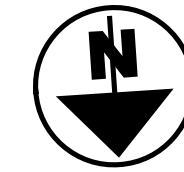
SIGNAL HEAD #	INDICATION	FIELD TERMINAL	FLASH
1 (WB LT)	R	OLA	R
	Y	OLA	
	G	OLA	
	<--Y-->	Ø2 Y	
2 (WB)	R	OLA	R
	Y	OLA	
	G	OLA	
3, 4 (EB)	R	Ø1 R	R
	Y	Ø1 Y	
5, 6, 7 (SB)	R	Ø3 R	R
	Y	Ø3 Y	
	G	Ø3 G	

OLA = Ø1 & Ø2 (EX. LOAD SWITCH)

PHASING DIAGRAM



INTERSECTION #2184

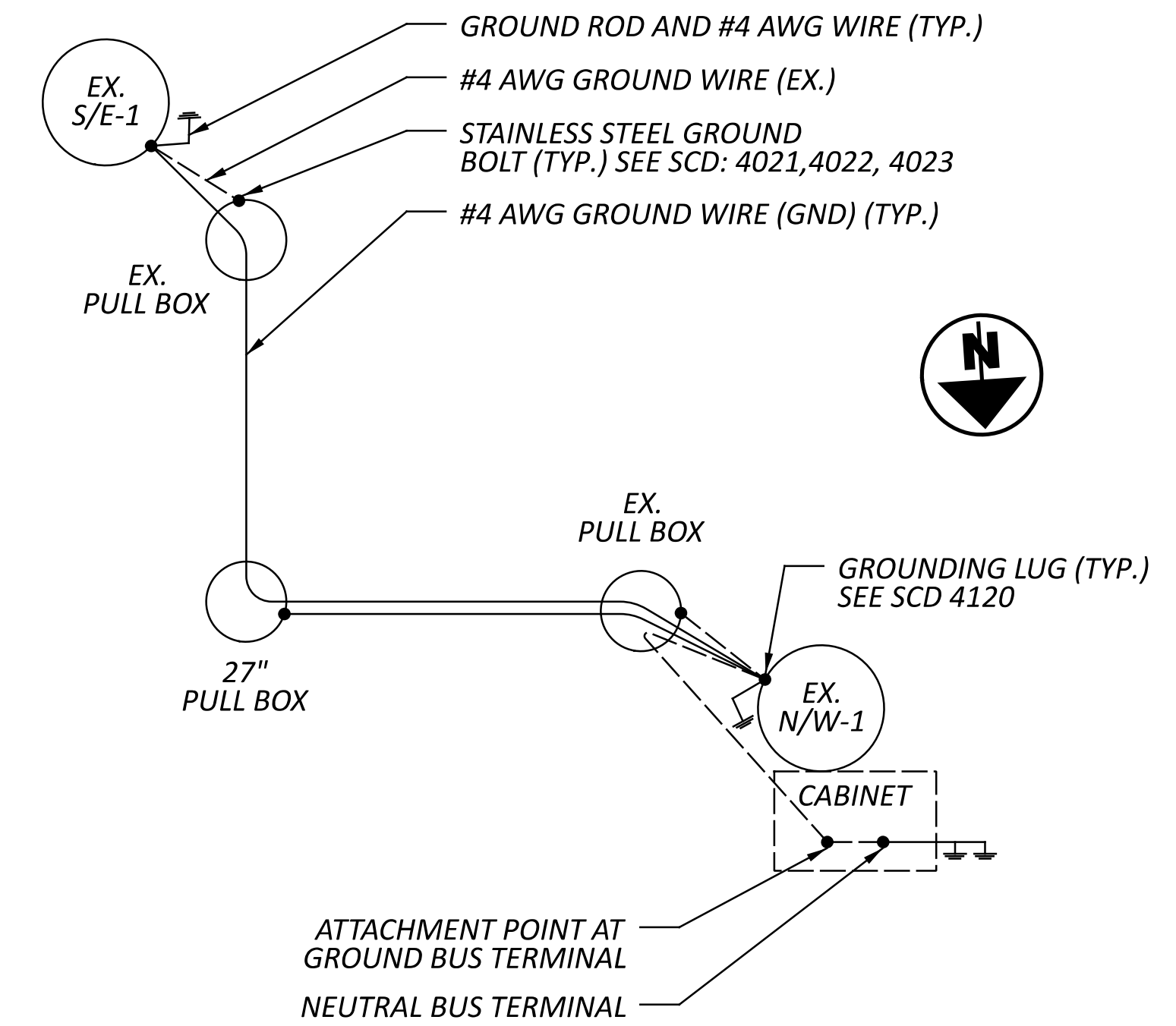


- PERMITTED VEHICULAR MOVEMENT
- PROTECTED VEHICULAR MOVEMENT

TIMING CHART

NOTE: ALL SIGNAL TIMINGS AND PHASING SHALL REMAIN AS CURRENTLY PROGRAMMED IN THE EXISTING CONTROLLER.

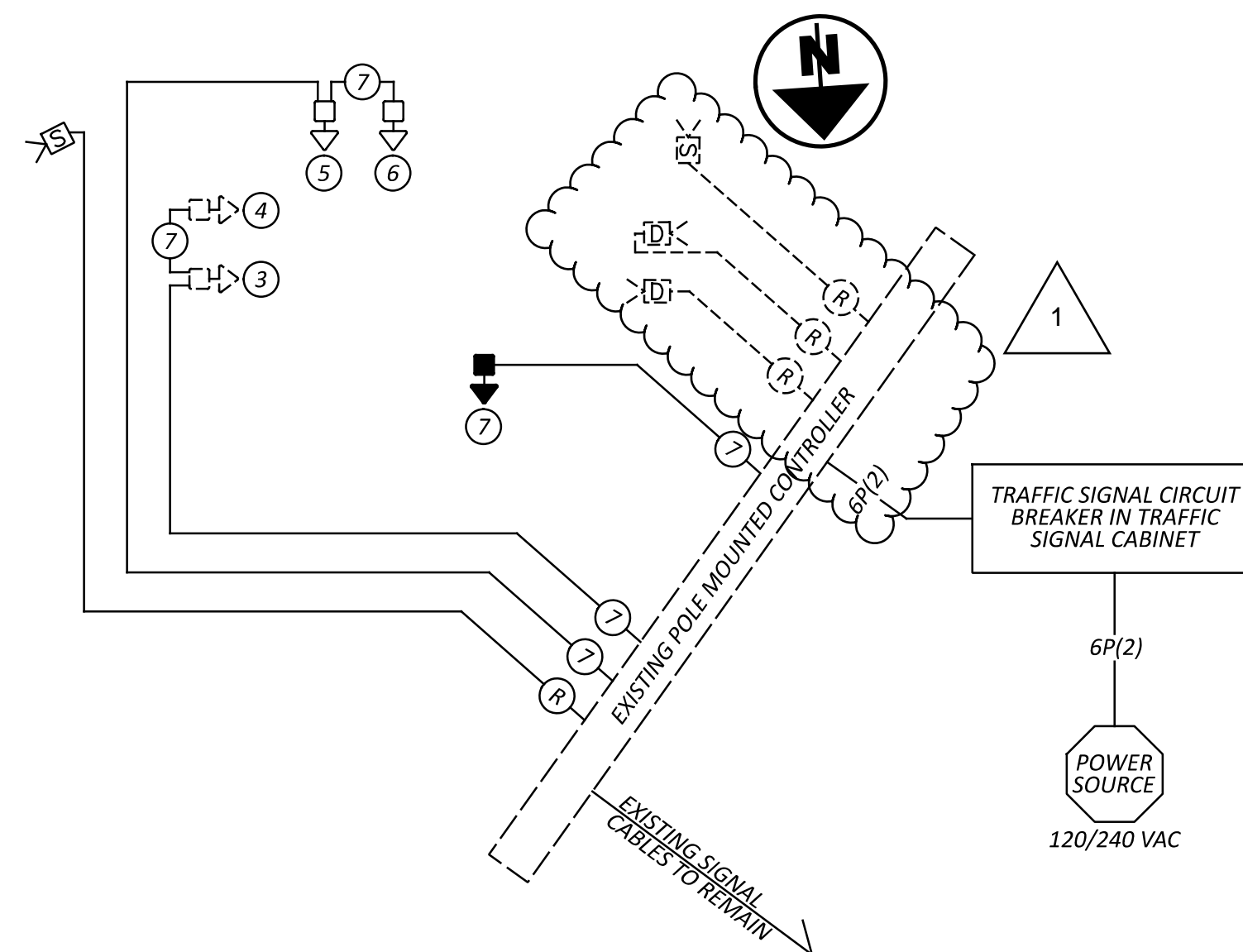
GROUNDING & BONDING DIAGRAM



GROUNDING & BONDING LEGEND

- PULL BOX
- ATTACHMENT POINT
- POLE
- CABINET
- EXISTING TRAFFIC SIGNAL CONTROLLER CABINET
- CABINET GROUND TERMINAL
- GROUND ROD & #4 AWG WIRE
- #4 AWG GROUND WIRE

WIRING DIAGRAM



WIRING DIAGRAM LEGEND

- 2/C #6 AWG., CU.
- RADAR DETECTOR CABLE
- 7/C #14 AWG., CU.
- PROPOSED STOP LINE RADAR DETECTION UNIT
- PROPOSED VEHICULAR SIGNAL HEAD
- EXISTING VEHICULAR SIGNAL HEAD
- RELOCATED VEHICULAR SIGNAL HEAD
- EX. RADAR DETECTOR CABLE (TO REMAIN)
- EX. STOP LINE RADAR UNIT (TO REMAIN)
- EX. STOP DILEMMA DETECTION UNIT (TO REMAIN)

SHEET NO.	LOCATION		SIDE	POLE/PULL BOX NO.	625		625		625		625		625		625		625		625	
	FROM	TO			EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
P.557	184+00.0	883+69.2	LT/LT	C-13 / C-14																
P.557	883+69.2		LT	C-14																
P.557	883+69.2	884+26.8	LT	C-14 / PULL BOX																
P.557	884+26.8		LT	PULL BOX	1															
P.557	884+26.8	788+08.6	LT/LT	PULL BOX / C-15																
P.557	788+08.6		LT	C-15																
P.557	788+08.6	788+63.1	LT	C-15 / PULL BOX																
P.557	788+63.1		LT	PULL BOX	1															
P.557	788+63.1	792+19.9	LT	PULL BOX / #223443																
P.557	792+19.9		LT	#223443																
P.557	184+00.0	283+52.7	RT/RT	D-13 / D-14																
P.557	283+52.7		RT	D-14																
P.557	283+52.7	285+66.6	RT	D-14 / D-15																
P.557	285+66.6		RT	D-15																
P.557	285+66.6	285+75.6	RT	D-13 / PULL BOX																
P.557	285+75.6		RT	PULL BOX	1															
P.557	285+75.6	286+74.8	RT/RT	PULL BOX / D-16																
P.557	286+74.8		RT	D-16																
P.557	285+75.6	285+67.0	RT/LT	PULL BOX / PULL BOX																
P.557	285+67.0		LT	PULL BOX	1															
P.557	285+67.0	785+79.7	LT/RT	PULL BOX / PULL BOX																
P.557	785+79.7		RT	PULL BOX	1															
P.557	785+79.7	786+37.0	RT	PULL BOX / D-17																
P.557	786+37.0		RT	D-17																
P.557	786+37.0	789+12.5	RT	D-17 / PULL BOX																
P.557	789+12.5		RT	PULL BOX	1															
P.557	789+12.5	790+16.7	RT	PULL BOX / #223440																
P.557	790+16.7		RT	#223440																
P.557	184+00.0	443+54.0	RT/RT	F-4 / F-5																
P.557	443+54.0		RT	F-5																
P.557	443+54.0	645+62.3	RT/RT	F-5 / F-6																
P.557	645+62.3		RT	F-6																
P.557	645+62.3	647+72.9	RT	F-6 / F-7																
P.557	647+72.9		RT	F-7																
P.557	446+38.0		RT	E-6																
P.557	446+38.0	447+82.3	RT	E-6 / E-5																
P.557	447+82.3		RT	E-5																
P.557	447+82.3	449+07.4	RT	E-5 / E-4																
P.557	449+07.4		RT	E-4																
P.557	449+07.4	192+00.0	RT/RT	E-4 / E-3																
P.558	192+00.0	291+13.1	RT/RT	E-4 / E-3																
P.558	291+13.1		RT	E-3																
P.558	291+13.1	292+51.5	RT/RT	E-3 / E-2																
P.558	292+51.5		RT	E-2																
P.558	292+51.5	293+97.8	RT	E-2 / E-1																
P.558	293+97.8		RT	E-1																
P.558	293+97.8	295+50.3	RT	E-1 / #223547																
P.558	295+50.3		RT	#223547																
P.558	26+78.9		RT	#223467																
P.558	26+78.9	27+90.9	RT	#223467 / PULL BOX																
P.558	27+90.9		RT	PULL BOX	1															
P.558	27+90.9	28+53.1	RT/RT	PULL BOX / PULL BOX																
P.558	28+53.1		RT	PULL BOX	1															
P.558	28+53.1	28+55.3	RT/RT	PULL BOX / #223468																
P.558	28+55.3		RT	#223468																
P.558	27+90.9	398+18.8	RT/LT	PULL BOX / C-11																
P.558	398+18.8		LT	C-11																
TOTALS CARRIED TO GENERAL SUMMARY					8		16	2	14	5	19		3681	2614	166	668				

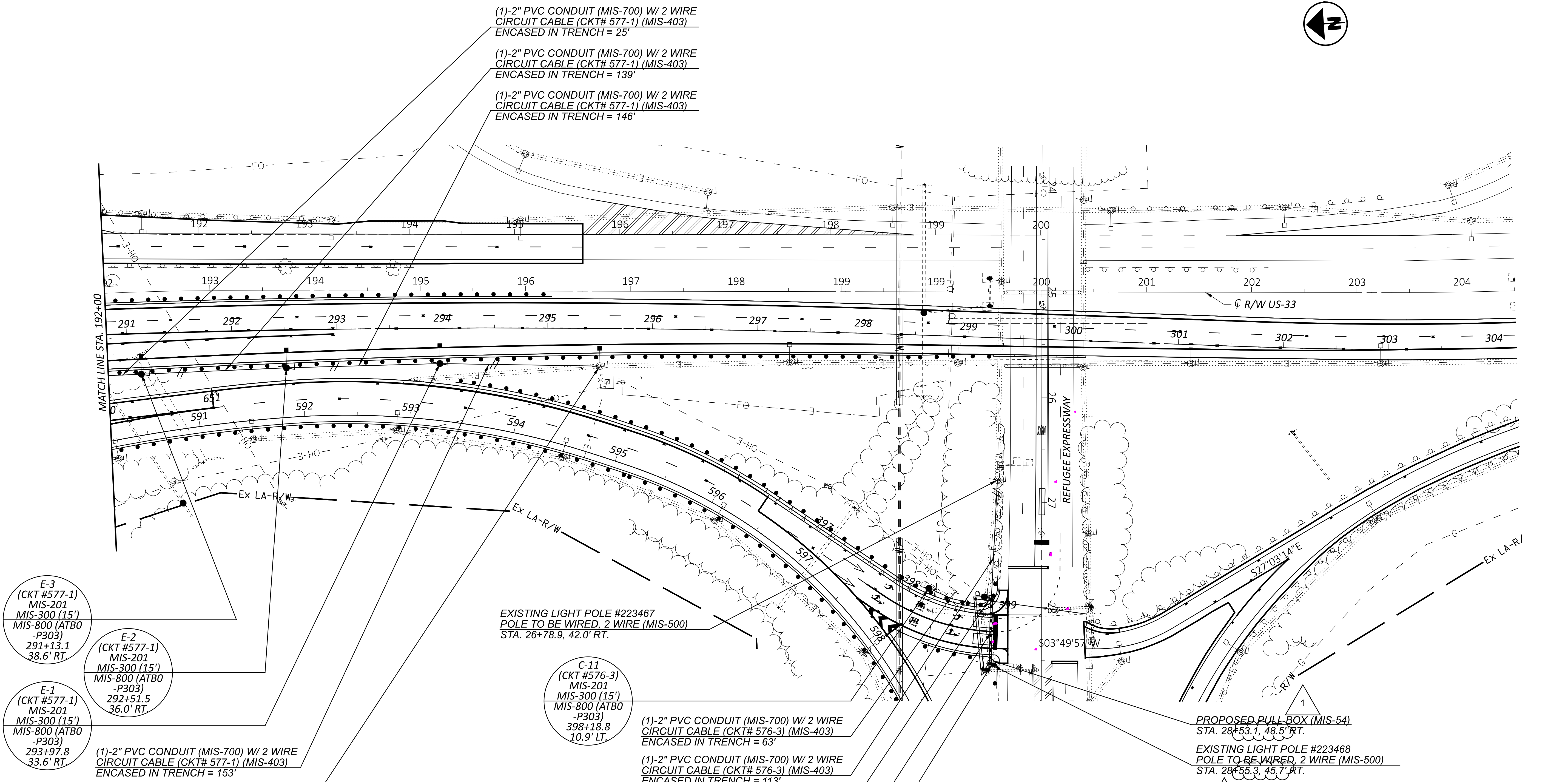
LIGHTING SUBSUMMARY

DESIGN AGENCY

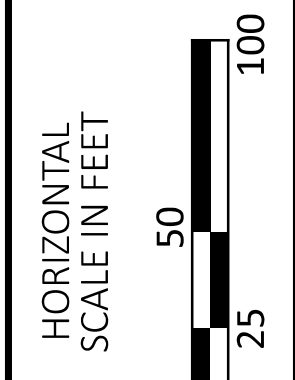
 GPD GROUP
 DESIGNER
 JAH
 REVIEWER
 KMG 12/15/25
 PROJECT ID
 113744
 SHEET TOTAL
 P.538 625

NOTES:

1. CIRCUITS 405, 476 AND 577 ARE OWNED AND MAINTAINED BY THE CITY OF COLUMBUS.



STREET LIGHTING LEGEND	
	PROPOSED POLE (MIS-302) W/ PROPOSED LUMINAIRE (AS NOTED) (MIS-800)
	EXISTING LIGHT POLE TO REMAIN W/ NEW LUMINAIRE (AS NOTED) (MIS-800)
	EXISTING LIGHT POLE (TO REMAIN)
	PROPOSED PULL BOX (MIS-54)
	PROPOSED CONDUIT (MIS-700) W/ 2-WIRE CIRCUIT (MIS-404)
	PROPOSED 3" STEEL CONDUIT (MIS-702) W/ 2-WIRE CIRCUIT (MIS-404)
	EXISTING PULL BOX (TO REMAIN)
	EXISTING CONTROL CENTER (TO REMAIN)
	EXISTING 2 WIRE CKT (AS NOTED)



LIGHTING PLAN
STA. 192+00 TO END

DESIGN AGENCY



DESIGNER
JAH

REVIEWER
KMG 12/15/25

PROJECT ID
113744

SHEET TOTAL
P.558 625