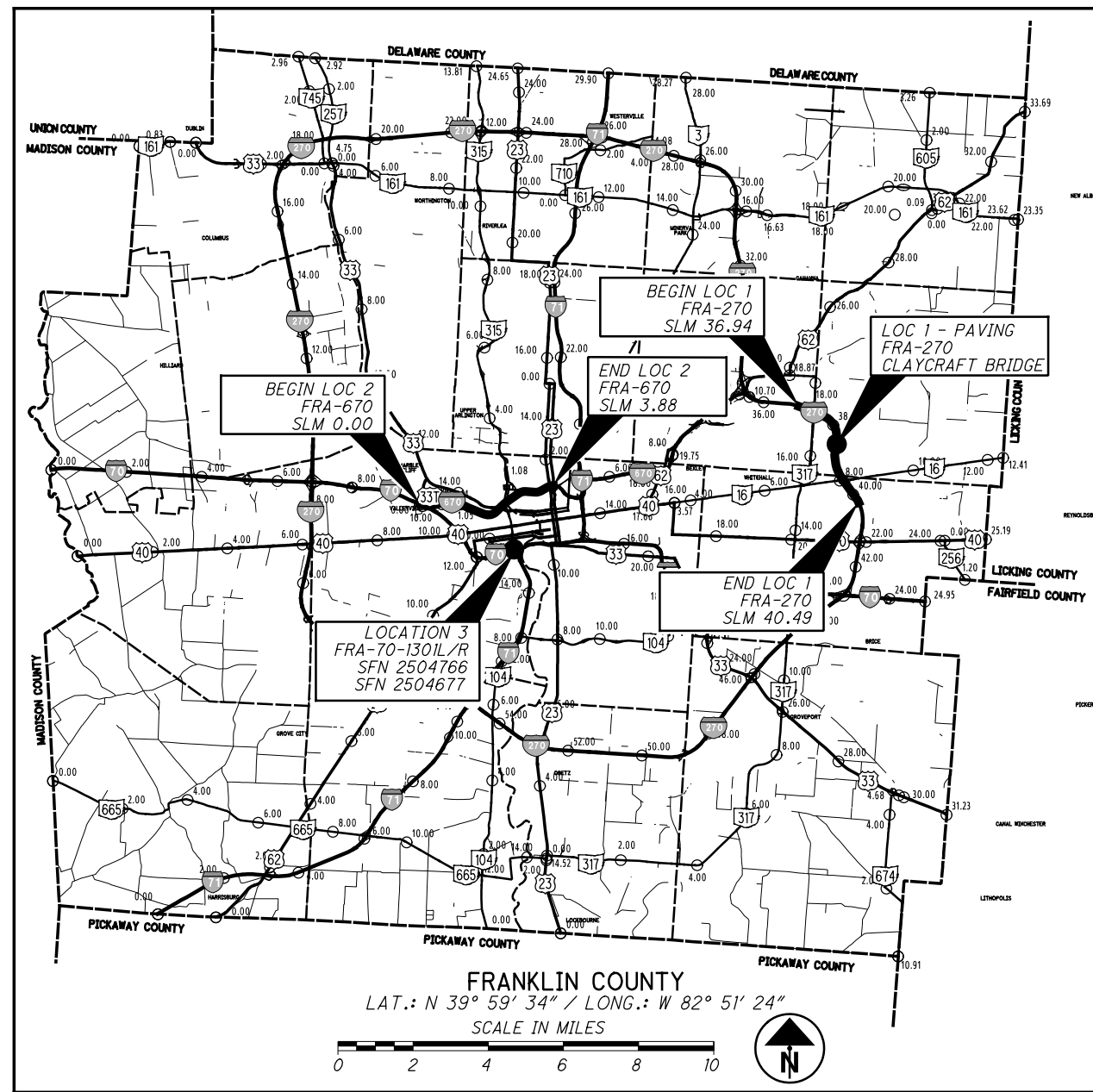


I:\ProjectData\91903_SP_FY20\Design\Roadway\Sheet\91903_GTO01.dgn 91903_GTO01 4/17/2020 7:18:42 AM GDENNIS



STATE OF OHIO
 DEPARTMENT OF TRANSPORTATION
FRA-270/670 SP
FY20 PRIORITY
 CITY OF COLUMBUS/CITY OF GAHANNA

LOC	COUNTY	ROUTE	BEGIN SLM	END SLM	COUNTY	TOWNSHIP
1	FRA	270	36.94	40.49	FRANKLIN	MIFFLIN, JEFFERSON, TRURO
2	FRA	670	0.00	3.88	FRANKLIN	FRANKLIN, MARION
3	FRA	070	1301L/R SFN 2504766, 2504677		FRANKLIN	FRANKLIN

INDEX OF SHEETS

TITLE	1
LOCATION MAP - FRANKLIN COUNTY	2
TYPICAL SECTIONS	3
PAVEMENT REPAIR DETAILS	4
GENERAL NOTES	5 - 6
MAINTENANCE OF TRAFFIC	7 - 10, 10A
GENERAL SUMMARY	11
PAVEMENT SUBSUMMARY	12
PAVEMENT QUANTITIES - LOCATION 1	13
PAVEMENT REPAIR QUANTITIES	14 - 17
TRAFFIC CONTROL SUBSUMMARY	18
STRUCTURE SHEETS	18A - 18D

PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF PARTIAL DEPTH PAVEMENT REPAIRS AT VARIOUS LOCATIONS ALONG IR-270 AND IR-670 IN FRANKLIN COUNTY, RESURFACING OF A PORTION OF IR-270 IN THE NORTHBOUND DIRECTION UNDER THE CLAYCRAFT BRIDGE AND SOME MINOR BRIDGE WORK ON SFN 2504766 AND SFN 2504677 (EASTBOUND AND WESTBOUND BRIDGES OVER S.R.315).

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE OF TRAFFIC AND SAFETY WILL BE AS SET FORTH ON PLANS AND ESTIMATES.

EARTH DISTURBED AREA:

PROJECT EARTH DISTURBED AREA	N/A *
EST. CONTRACTOR EARTH DISTURBED AREA	N/A *
NOTICE OF INTENT EARTH DISTURBED AREA	N/A *

* MAINTENANCE PROJECT

UNDERGROUND UTILITIES
 Contact Two Working Days Before You Dig

OHIO811. 8-1-1. or 1-800-362-2764
 (Non-members must be called directly)

PLAN PREPARED BY:

ENGINEERS SEAL:	STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	
	BP-3.1	10/18/19	MT-95.30	07/19/19	TC-41.20	10/18/13	800	01/17/20
 SIGNED: <i>Kevin Fiant</i> DATE: 1-10-2020			MT-95.50	07/21/17	TC-42.20	10/18/13	821	04/20/12
			MT-98.10	01/20/17	TC-52.10	10/18/13	832	10/19/18
			MT-98.11	04/19/19	TC-52.20	07/20/18	872	07/19/19
			MT-98.20	04/19/19	TC-65.10	01/17/14	921	04/20/12
			MT-98.22	01/20/17	TC-65.11	07/21/17		
			MT-98.28	01/20/17	TC-72.20	07/20/18		
			MT-98.29	07/19/19				
			MT-99.20	04/19/19				
			MT-101.60	01/20/17				
			MT-101.90	07/21/17				
			MT-105.10	07/19/13				

PLANS CERTIFIED BY:
 NAME: *Mauro Dennis* DATE: 1-10-20
 DISTRICT 6
 OHIO DEPT. OF TRANSPORTATION

APPROVED: *Michael R. Bluff*
 DATE: 1/13/2020
 DISTRICT DEPUTY DIRECTOR

APPROVED: _____
 DATE: _____
 DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. NONE
 PID NO. 91903
 CONSTRUCTION PROJECT NO.
 RAILROAD INVOLVEMENT NONE
 FRA-270/670 SP
 FY20 PRIORITY
 1/18

I:\Project+Data\91903_D06_SP_FY20_Design\Roadway\Sheets\91903_GN001.dgn 91903_GN002 4/17/2020 7:19:29 AM GDENNIS

UNDERGROUND UTILITIES (CONT'D) :

IN ADDITION TO THE INFORMATION OUTLINED IN THE UTILITY NOTE OF THIS CONTRACT, THE CONTRACTOR SHALL TAKE THE FOLLOWING ACTION TO PROTECT ODOT'S FACILITIES DURING CONSTRUCTION.

HIGHWAY LIGHTING AND TRAFFIC SIGNALS:

EVEN THOUGH ODOT IS LISTED AS A MEMBER OF THE OHIO UTILITIES PROTECTION SERVICE (OUPS), THE ODOT CONTRACTOR IS REQUIRED TO CONTACT ODOT, DISTRICT 6 TRAFFIC MAINTENANCE DEPARTMENT DIRECTLY ONCE LINES ARE MARKED BY OUPS SO THAT THE ODOT UTILITIES LOCATED WITHIN THIS PROJECT CAN BE DISCUSSED OR CONFIRMED AS NECESSARY PRIOR TO EXCAVATION. THE CONTRACTOR SHALL NOTIFY DISTRICT 6 TRAFFIC MAINTENANCE AT 740-833-8198 AND THE CONSTRUCTION PROJECT ENGINEER, FOURTEEN (14) CALENDAR DAYS IN ADVANCE OF ANY WORK, FOR THE NEED TO VERIFY/DISCUSS ODOT OWNED UTILITIES.

WATERSHED PROTECTION:

THE PROJECT IS LOCATED WITHIN SEVERAL SOURCE WATER PROTECTION AREAS. IT IS ESSENTIAL THAT ALL ACTIVITIES ASSOCIATED WITH THIS WORK BE PERFORMED IN A MANNER CONSISTENT WITH BEST WATERSHED MANAGEMENT PRACTICES INCLUDING, BUT NOT LIMITED TO: AREAS OF DISTURBED GROUND SHALL HAVE APPROPRIATE EROSION AND SEDIMENT CONTROLS. IF HAZARDOUS/TOXIC MATERIALS INCLUDING BUT NOT LIMITED TO FUELS, OILS, BITUMEN'S PAINTS, SEALANTS, OR OTHER CHEMICALS, ARE STORED ON SITE, THEY SHALL BE STORED IN A DOUBLE-CONTAINMENT MANNER. ALL EQUIPMENT REPAIRS, MAINTENANCE, AND MECHANICAL WORK THAT COULD RESULT IN THE RELEASE OF HAZARDOUS/TOXIC MATERIALS SHALL BE PERFORMED IN AN APPROPRIATELY CONTAINED AREA, PREFERABLY OFF SITE OR AN APPROPRIATE OFF-SITE FACILITY. IN THE EVENT THAT ANY HAZARDOUS/TOXIC MATERIALS INCLUDING, BUT NOT LIMITED TO FUELS, OILS, BITUMEN'S PAINTS, SEALANTS, OR OTHER CHEMICALS ARE SPILLED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY EMERGENCY SERVICES BY CALLING 911 AND THE OHIO EPA @ (800) 282-9378. THE CONTRACTOR SHOULD BE PREPARED TO PROVIDE DETAILED INFORMATION RELATIVE TO THE TYPE AND QUANTITY OF MATERIAL THAT HAS BEEN SPILLED AS WELL AS THE EXACT LOCATION AND THE EXACT TIME AT WHICH THE SPILL OCCURRED. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR INFORMING ALL SUBCONTRACTORS AND OTHER AGENTS OF THESE RESPONSIBILITIES, PRECAUTIONS, AND PROHIBITIONS.

ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446), AS PER PLAN:

JOINT CORING IN ACCORDANCE WITH 446.04 IS NOT REQUIRED FOR COLD LONGITUDINAL JOINTS PLACED OVER VOID REDUCING ASPHALT MEMBRANE (VRAM). CONSTRUCT COLD LONGITUDINAL JOINTS OVER VRAM USING THE SAME TECHNIQUES, EQUIPMENT, AND ROLLER PATTERNS USED ON THE REST OF THE MAT. OBTAIN 10 MAT CORES FOR EACH LOT OF MATERIAL IN ACCORDANCE WITH 446.04. PAY FACTORS FOR EACH LOT OF MATERIAL WILL BE DETERMINED ACCORDING TO TABLE 446.04-2

ITEM 530, SPECIAL - STRUCTURES MISC.: FALSEWORK:

PROVIDE FALSEWORK FOR ALL BAYS OVER THE TRAVEL LANES AND SHOULDERS AS SHOWN IN THE PLANS. FALSEWORK IS INTENDED TO BE USED TO CONTAIN FALLING DELAMINATED CONCRETE. IT SHALL BE REMOVED ONCE THE PROJECT IS COMPLETE AND BE CONSTRUCTED IN ACCORDANCE WITH CMS 508.

THE DEPARTMENT WILL MEASURE THE QUANTITY OF FALSEWORK ON A LUMP SUM BASIS. THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES OF FALSEWORK AT THE CONTRACT PRICE FOR ITEM 530 - SPECIAL, STRUCTURES MISC.: FALSEWORK.

CALCULATED
GVD
CHECKED
XXX

GENERAL NOTES

FRA - 270 / 670 SP
FY20 PRIORITY

ITEM 614 - DETOUR SIGNING

SIZE AND PLACEMENT OF DETOUR SIGNS (M4-9) SHOULD FOLLOW THE REQUIREMENTS OF THE OMUTCD SECTION 6F.03, SECTION 2A.11 AND TABLE 6F.01.

DETOUR SIGNING SHALL PROVIDE DRIVERS ADEQUATE TIME TO CLEARLY READ THE SIGNS AND MAKE THE PROPER DECISIONS AT EACH REQUIRED TURNING MOVEMENT. THE DESIGNATED DETOUR ROUTE SHALL BE SIGNED IN ACCORDANCE WITH THE REQUIREMENTS BELOW:

- APPROXIMATELY 1500 FEET PRIOR TO TIP OF THE PAINTED GORE AT AN INTERCHANGE WHEN EXITING A HIGH SPEED (45 MPH OR HIGHER) FACILITY.
- AT OR NEAR THE EXISTING SIGN IN THE GORE OF AN INTERCHANGE RAMP.
- AT OR NEAR THE FIRST EXISTING LANE ASSIGNMENT SIGN ON AN INTERCHANGE EXIT RAMP.
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT THE END OF AN EXIT RAMP.
- APPROXIMATELY 500 FEET PRIOR TO A REQUIRED TURN AT AN INTERSECTION NOT CONTROLLED BY A STOP SIGN (FOR 45 MPH OR HIGHER ONLY).
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT AN INTERSECTION.
- EVERY TWO MILES ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS OUTSIDE A CITY.
- EVERY TWO BLOCKS ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS WITHIN A CITY.
- AT ANY OTHER INTERSECTION OR DECISION POINT WHERE THE DETOUR ROUTE IS CONTRARY TO THE NORMAL, EXPECTED TURNING MANEUVER OR OTHERWISE UNCLEAR.

DETOUR SIGNS SHALL BE PLACED, WHEN POSSIBLE, NEXT TO BUT NOT BLOCKING EXISTING ROUTE MARKERS OR LANE ASSIGNMENT SIGNS. DETOUR SIGNS SHALL NOT OBSCURE OR BE OBSCURED BY OTHER EXISTING OR TEMPORARY SIGNS.

DETOUR SIGNS SHALL BE ERECTED AND/OR UNCOVERED PRIOR TO THE ROAD OR RAMP BEING CLOSED TO TRAFFIC BUT NO EARLIER THAN FOUR HOURS PRIOR TO THE CLOSURE. DETOUR SIGNS SHALL BE COVERED AND/OR REMOVED NO LATER THAN FOUR HOURS FOLLOWING THE ROAD OR RAMP RE-OPENING TO TRAFFIC.

PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, PROPER SIGN PLACEMENT AND SIZING, TIMELY ERECTING AND/OR UNCOVERING OF SIGNS, MAINTAINING SIGNS, AND TIMELY COVERING AND/OR REMOVING SIGNS AND SUPPORTS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - DETOUR SIGNING = LUMP SUM

MAINTENANCE OF TRAFFIC FOR MARKING PAVEMENT REPAIRS:

PROVIDE LANE CLOSURES AS PER THE MAINTENANCE OF TRAFFIC NOTES IN THESE PLANS A MINIMUM OF 24 HOURS PRIOR TO PERFORMING PAVEMENT REPAIRS TO ALLOW THE ENGINEER TO IDENTIFY AND MARK THE AREAS OF THE PAVEMENT IN NEED OF REPAIRS.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, LEO HOURS, AND INCIDENTALS NEEDED TO PERFORM THE ABOVE LISTED WORK IS CONSIDERED INCIDENTAL TO MAINTAINING TRAFFIC ON THE PROJECT AND WILL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

COORDINATION WITH ADJACENT PROJECTS:

THE CONTRACTOR SHALL COORDINATE WORK WITH ODOT AND THE CONTRACTORS ON THE ADJACENT PROJECTS. FRA-270-38.53, PID 110368, FRA-70-14.54, PID 105322, FRA-71-9.62/9.71 PART 1 & 2, PID 104799 AND FRA-71-17.46, PID 105453. COORDINATION SHALL BE MADE TO PREVENT CONFLICTING ADVANCE WARNING SIGNS, CONFLICTING DETOUR ROUTES, OVERLAPPING/CONFLICTING LANE CLOSURES, AND TO ENSURE THAT A MINIMUM DISTANCE OF 2 MILES BETWEEN ADJACENT LANE CLOSURES IS MAINTAINED. THIS IS NOT AN EXHAUSTIVE LIST OF COORDINATION ITEMS THAT MAY NEED TO BE RESOLVED BETWEEN PROJECTS. THE DEPARTMENT RESERVES THE RIGHT TO DECIDE WHICH PROJECT'S ACTIVITIES TAKE PRECEDENCE. PROJECTS THAT HAVE ACTIVITIES DELAYED DUE TO CONFLICTS WILL CONSIDER THIS AN EXCUSABLE, NON-COMPENSABLE DELAY PER 108.06.B. ON PROJECTS THAT HAVE ACTIVITIES DELAYED DUE TO CONFLICTS WHERE THE CONTRACTOR FAILED TO MEET THE NOTIFICATION REQUIREMENTS, THE DELAYS SHALL NOT BE CONSIDERED EXCUSABLE OR COMPENSABLE. ATTENDANCE AT DEPARTMENT ORDERED TRAFFIC COORDINATION MEETINGS BETWEEN ADJACENT PROJECTS SHALL BE CONSIDERED MANDATORY FOR EACH PROJECT'S SUPERINTENDENT AND WORKSITE TRAFFIC SUPERVISOR (WTS), AND INCIDENTAL TO THE LUMP SUM MAINTENANCE OF TRAFFIC PAYMENT ITEM.

INTERSTATE ENTRANCE RAMP CLOSURES (FRANKLIN COUNTY ONLY):

IF THE INTERSTATE IS REDUCED TO ONE THROUGH LANE, ALL NON-INTERSTATE ENTRANCE RAMP ENTERING DIRECTLY INTO THE WORK ZONE TRANSITION AREA, ACTIVITY AREA OR TERMINATION AREA AS DEFINED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) OR ENTERING WITHIN 1000 FEET OF THE FIRST ARROW BOARD SHALL BE CLOSED IN ACCORDANCE WITH THE SHORT DURATION RAMP CLOSURES NOTE.

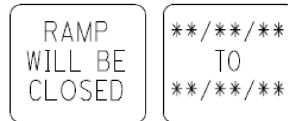
SR-315 AND RAMPS UNDER STRUCTURE LANE CLOSURE REQUIREMENTS:

THE FOLLOWING REQUIREMENTS ARE IN PLACE TO PROVIDE CONTRACTOR ACCESS TO THE UNDERSIDE OF THE STRUCTURE IN ORDER TO PLACE FALSE-DECKING TO CATCH ANY DEBRIS AND WASTEWATER GENERATED DURING HYDRODEMOLITION OPERATIONS. ALL FALSE-DECKING SHALL BE REMOVED AT THE COMPLETION OF THE PROJECT UTILIZING THE SAME CLOSURE REQUIREMENTS AS GIVEN BELOW.

NOTIFICATION OF RAMP CLOSURES:

NOTIFY THE ENGINEER IN WRITING AT LEAST 7 DAYS IN ADVANCE OF THE RAMP CLOSURE. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN 2 CLASS A PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) IN ACCORDANCE WITH SUPPLEMENT 1061. PCMS'S SHALL BE POSITIONED TO DISPLAY THE DATE AND DURATION OF THE RAMP CLOSURE FOR THE PERIOD OF TIME AND AT THE RAMPS SPECIFIED IN THE PLANS FOR FULL CLOSURE, OR AS DIRECTED BY THE ENGINEER.

PROVIDE THE FOLLOWING MESSAGES, OR OTHER MESSAGES AS DIRECTED BY THE ENGINEER (* DENOTED LIMITS OF EACH SCREEN MESSAGE):



BEFORE CLOSURE

THE CONTRACTOR OR ENGINEER SHALL PROVIDE THE ACTUAL DATES AND TIMES.

LANE VALUE CONTRACT TABLE:

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE LANE VALUE CONTRACT TABLE FOR EACH UNIT OF TIME A LANE/SHOULDER/RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE LANE VALUE CONTRACT TABLE.

LANE VALUE CONTRACT TABLE						
Section (SLM)	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Fri	Sat	Sun	
FRA-270						
Goshen Lane (35.90) to SR 317 (37.36)	4	4 to 3	5AM-9AM & 3PM-7PM	No Restriction	No Restriction	\$200
		4 to 2	5AM-8PM	7AM-9AM & 2PM-7PM	7AM-9AM & 2PM-7PM	\$200
		4 to 1	5AM-11PM	6AM-10PM	6AM-10PM	\$200
SR 317 (37.36) to US 40 (40.90)	4	4 to 3	5AM-9AM & 3PM-7PM	No Restriction	No Restriction	\$200
		4 to 2	5AM-8PM	7AM-9AM & 2PM-7PM	7AM-9AM & 2PM-7PM	\$200
		4 to 1	5AM-11PM	6AM-10PM	6AM-10PM	\$200
US 40 (40.90) to Livingston Avenue (42.13) Main line	2	2 to 1	5AM-8PM	6AM-7PM	6AM-7PM	\$200
US 40 (40.90) to Livingston Avenue (42.13) Collector Distributor	4	4 to 3	5AM-9AM & 3PM-7PM	No Restriction	No Restriction	\$200
		4 to 2	5AM-8PM	7AM-9AM & 2PM-7PM	7AM-9AM & 2PM-7PM	\$200
		4 to 1	5AM-11PM	6AM-10PM	6AM-10PM	\$200
Short term shoulder closures are not permitted 5AM-9AM and 3PM-7PM Monday-Friday.						

LANE VALUE CONTRACT TABLE						
Section	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Fri	Sat	Sun	
FRA-670						
I-70 (0.00) to Grandview Ave (0.90)	3	3 to 2	5AM-9AM & 2PM-7PM	4PM-6PM	4PM-6PM	\$100
		3 to 1	5AM-9PM	6AM-8PM	6AM-11PM	\$100
Grandview Ave (0.90) to SR 315 SB exit (1.65)	4	4 to 3	5AM-9AM & 3PM-6PM	No Restriction	No Restriction	\$100
		4 to 2	5AM-9AM & 2PM-7PM	4PM-6PM	4PM-6PM	\$100
		4 to 1	5AM-9PM	6AM-8PM	6AM-8PM	\$100
SR 315 SB exit (1.65) to Neil Avenue (3.14)	3	3 to 2	5AM-9PM	6AM-7PM	6AM-7PM	\$200
		3 to 1	5AM-11PM	6AM-10PM	6AM-11PM	\$200
Neil Avenue (3.14) to 3rd Street (3.72)	4	4 to 3	5AM-9AM & 3PM-6PM	No Restriction	No Restriction	\$200
		4 to 2	5AM-9PM	6AM-7PM	6AM-7PM	\$200
		4 to 1	5AM-11PM	6AM-11PM	6AM-11PM	\$200
3rd Street (3.72) to Cleveland Avenue (4.30) Westbound	2	2 to 1	5AM-11PM	6AM-11PM	6AM-11PM	\$200
3rd Street (3.72) to Cleveland Avenue exit (4.04) Eastbound	2	2 to 1	5AM-11PM	6AM-11PM	6AM-11PM	\$200
Short term shoulder closures are NOT permitted 5AM-9AM and 3PM-6PM Monday-Friday.						

Ramp Closure Restrictions Interstate 270 in Franklin County					
Secondary Route: State Route 317 SLM along 270: 37.36 (East Side)					
Ramp Designation	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
A	OH-317 SB to I-270 WB	5AM-8PM	8AM-8PM	OH-317 S to 270 S (Ramp H) to OH-16 to 270 N	OH-317 S to OH-16 to 270 N
E	I-270 EB to OH-317 SB	5AM-11PM	8AM-8PM	270 S to OH-16 to 270 N to OH-317 (Ramp L)	270 S to OH-16 to OH-317
H	OH-317 SB to I-270 EB	5AM-7PM	8AM-7PM	OH-317 S to OH-16 to 270 S	None
L	I-270 WB to OH-317 NB	5AM-7PM	8AM-7PM	270 N to US-62 E to OH-317	270 N to W. Johnstown Rd.* to S. Stygler Rd.* to US-62 W to 270 S to oh-317 (Ramp E)
N	OH-317 NB to I-270 WB	5AM-7PM	8AM-7PM	OH-317 to US-62 W to 270 N	OH-317 to 270 S (Ramp T) to OH-16 to 270 N
T	OH-317 NB to I-270 EB	5AM-9AM & 3PM-7PM	No Restriction	317 to 270 N (Ramp N) to US-62 E to OH-317 to 270 S (Ramp H)	OH-317 to US-62 W to 270 S

I:\ProjectData\91903_D06.SP.FY20\Design\Roadway\Sheets\91903_MN001.dgn 91903_MN006 4/17/2020 7:24:50 AM GDENNIS

NOTIFICATIONS PRIOR TO CLOSURES REQUIRED

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO

- OFFICE OF ROADWAY ENGINEERING (EMILY.WILLIS@DOT.OHIO.GOV; WILLIAM.FEEHAN@DOT.OHIO.GOV)
- STATEWIDE TMC (STATEWIDETMC@DOT.OHIO.GOV)
- DWZTM (GARY.FETHEROLF@DOT.OHIO.GOV)
- SPECIAL HAULING PERMITS (HAULING.PERMITS@DOT.OHIO.GOV)

AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE.

REFERENCE "EXCEPTION REQUEST APPROVAL DATED 4/16/2020 FOR PID 91903" IN THE NOTIFICATION AND OTHER CORRESPONDENCE. ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE APPLICABLE ODOT CENTRAL OFFICE COMMITTEE (MOTEC OR PIAC).

EXTRA ADVANCE WARNING SIGNS:

AN EXTRA ADVANCE WARNING SIGN GROUP CONSISTS OF TWO W20-1 (ROAD WORK AHEAD) SIGNS, TWO W20-5 (RIGHT /LEFT LANE CLOSED AHEAD) SIGNS WITH W16-3A DISTANCE PLATES, AND TWO W3-H7 (WATCH FOR STOPPED TRAFFIC) SIGNS AND REQUIRED WARNING LIGHTS.

THE CONTRACTOR SHALL HAVE AN ADDITIONAL EXTRA ADVANCE WARNING SIGN GROUP (6 SIGNS AND 2 DISTANCE PLATES) AVAILABLE FOR USE WHEN DIRECTED BY THE ENGINEER. THE DISTANCE PLATES FOR THIS GROUP SHALL BE ABLE TO BE MODIFIED IN THE FIELD TO SHOW APPROPRIATE WHOLE MILES TO THE LANE TAPER.)

PAYMENT FOR PROVIDING, ERECTING, MAINTAINING AND REMOVING EXTRA ADVANCE WARNING SIGN GROUPS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S) :

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE BEEN APPROVED BY THE MOT EXCEPTION COMMITTEE (MOTEC) OR THE PROJECT IMPACT ADVISORY COUNCIL (PIAC) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)). THE EXCEPTION IS ONLY APPROVED FOR TWO WEEKEND CLOSURES FROM FRIDAY 9PM TO MONDAY 5AM, ONE WEEKEND FOR WORK ON I-70 WESTBOUND & ONE WEEKEND FOR I-70 EASTBOUND.

APPROVED MOT EXCEPTION(S) INCLUDE:

- CLOSURE AND DETOUR OF I-70 WB AT I-71/SR-315 INTERCHANGE.
- CLOSURE AND DETOUR OF I-70 EB FROM I-670 TO I-71.
- CLOSURE AND DETOUR OF I-70 EB TO SR-315N SYSTEM RAMP.

THESE EXCEPTIONS WERE USED TO CREATE THE MAINTENANCE OF TRAFFIC PLAN ON SHEETS 10D TO 10T.

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 3 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER (d06.mot@dot.ohio.gov) AS WELL AS THE CONTRACTOR AND ANY OF HIS SUB-CONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL. FOR COLUMBUS SECTIONS OF ROADWAY, ALSO INCLUDE THE TEMPORARY CONTROL COORDINATOR (614-645-6269 OR 614-645-5845) FROM THE CITY OF COLUMBUS TRANSPORTATION DIVISION. AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

ANY ADDITIONAL WORK NEEDED TO COMPLETE THE PROJECT BEFORE OR AFTER THE APPROVED TWO WEEKEND MOT EXCEPTION CLOSURE SHALL BE COMPLETED USING THE NORMAL MAINTENANCE OF TRAFFIC PROCEDURES UTILIZING LANE VALUE CONTRACT TABLE & RAMP CLOSURE RESTRICTION CHART.

THE CONTRACTOR WILL ALSO WORK CLOSELY WITH ODOT D06 PUBLIC INFORMATION OFFICE IN CARRYING OUT COMMUNICATIONS PLAN TO ENSURE THE LOCAL JURISDICTIONS, PERTINENT BUSINESSES AND DRIVING PUBLIC ARE WELL INFORMED AND AWARE OF THE ASSOCIATED TRAFFIC IMPACTS.

SEQUENCE OF MAINTENANCE OF TRAFFIC FOR POLICY EXCEPTIONS.

CLOSURES MUST FOLLOW LANE VALUE CONTRACT TABLE & RAMP CLOSURE RESTRICTIONS CHART TIMES. DURING THIS PHASE OF MOT, DRUMS SHALL BE UTILIZED IN LEU OF WEIGHT CHANNELIZERS.

I-70 WB CLOSURE:

1. CLOSURE OF RAMP N/A, 2ND ST. DB TO I70/71WB. SHEET 10D FOR DETOUR ROUTE.
2. CLOSURE OF LANES ON I-70 WB AS SHOWN ON SHEET 10F TO 10J WITH DETOUR PLAN AS SHOWN ON SHEET 10D.

I-70 EB CLOSURE:

1. CLOSURE OF I-70 EB AT I-670/I-70 SPLIT, SEE SHEET 10O TO 10Q.
2. CLOSURE OF I-70EB & I-70EB TO SR-315N RAMP UTILIZING LANE CLOSURES AND DETOURS AS SHOWN ON SHEET 10K & SHEETS 10R TO 10T.

NOTE: ALL CLOSURES WILL REQUIRE THE SHOULDER TO ALSO BE CLOSED. CLOSURES SHALL BE IN ACCORDINACE WITH THE LANE VALUE CONTRACT TABLE & RAMP CLOSURE RESTRICTION CHART.

NOTIFICATIONS DURING CLOSURE REQUIRED:

A DESIGNATED ON-SITE POINT OF CONTACT SHOULD COMMUNICATE WITH THE TMC & D06 PIO AS THE STATUS OF THE CLOSURE CHANGES. CONTACT THE TMC:

- IF THE CLOSURE IS POSTPONED OR CANCELLED
 - AT THE TIME THE CLOSURE IS IMPLEMENTED
 - AT THE TIME THE CLOSURE IS REMOVED AND ALL LANES RESTORED
 - IF THE CLOSURE WILL NOT BE OPENING ON TIME
- CONTACT CAN BE MADE WITH THE TMC IN THE FOLLOWING WAYS:
- PHONE: 1-614-387-2438 OR 1-800-884-4030
 - EMAIL: STATEWIDETMC@DOT.OHIO.GOV
 - RADIO: XDOT MAIN
 - PIO EMAIL: D06.PIO@DOT.OHIO.GOV
 - PIO NUMBER: 614-205-3131

DRUM REQUIREMENTS:

IN ADDITION TO THE REQUIREMENTS OF THE PLANS, SPECIFICATION AND PROPOSAL, DRUMS FURNISHED BY THE CONTRACTOR SHALL BE NEW AND UNUSED AT THE TIME OF ARRIVAL ON THE PROJECT. ANY DRUMS BROUGHT ON THE PROJECT, WHICH HAVE PREVIOUSLY BEEN USED ELSEWHERE, WILL NOT BE ACCEPTED.

PAYMENT FOR DRUMS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED.

PRE-MAINTENANCE OF TRAFFIC MEETING:

A PRE-MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD (MINIMUM 10 WORK DAYS) PRIOR TO WORK BEGINNING OR ANY CHANGE OF PHASING. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER (d06.mot@dot.ohio.gov) AS WELL AS THE CONTRACTOR AND ANY OF HIS SUB-CONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL. FOR COLUMBUS SECTIONS OF ROADWAY, ALSO INCLUDE THE TEMPORARY CONTROL COORDINATOR (614-645-6269 OR 614-645-5845) FROM THE CITY OF COLUMBUS TRANSPORTATION DIVISION

MAINTENANCE OF TRAFFIC NOTES

FRA-270/670 SP
FY20 PRIORITY

10B
18

CALCULATED
GVD
CHECKED
XXX

I:\ProjectData\91903_006.SP.FY20.Design\Roadway\Sheets\91903.MN001.dgn 91903.MN007 4/17/2020 8:36:08 AM GDENNIS

WINDOW CONTRACT: SHORT TERM HOURLY CLOSURE WITH COMPLETION DISINCENTIVE:

HOURLY CLOSURES:

THE CONTRACTOR HAS THE NUMBER OF HOURS DESIGNATED IN THE WINDOW CONTRACT: SHORT TERM HOURLY CLOSURE WITH COMPLETION DISINCENTIVE TABLE (TABLE) IN WHICH TO COMPLETE ALL ITEMS OF CRITICAL WORK AS DEFINED TABLE. THE TABLE IS LOCATED IN THE PLAN MAINTENANCE OF TRAFFIC NOTES. THE CONTRACTOR MUST COMPLETE THE CRITICAL WORK WITHIN THE TIMEFRAME DESIGNATED.

COMPLETION OF THE CRITICAL WORK IS DEFINED AS HAVING THE DESIGNATED WORK SECTION OPEN TO UNRESTRICTED TRAFFIC; ALL TRAFFIC LANES BEING AVAILABLE FOR USE AT THEIR REQUIRED WIDTH WITH SAFETY FEATURES INSTALLED.

THE CONTRACTOR AND PROJECT ENGINEER MUST MUTUALLY REVIEW THE FORECASTED WEATHER REGARDING THE ACCEPTABILITY TO PERFORM THE CRITICAL WORK FOR THE NECESSARY WORK DURATION PRIOR TO BEGINNING CRITICAL WORK. A COPY OF THE WEATHER FORECAST SHALL BE KEPT IN THE PROJECT RECORD. THE CRITICAL WORK MAY NEED TO BE RESCHEDULED.

TIME EXTENSIONS FOR WEATHER IMPACTED WEATHER-SENSITIVE CRITICAL WORK WILL ONLY BE FOR THE DURATION OF THE ACTUAL UNANTICIPATED WEATHER EVENT AND FOR ANY SUBSEQUENT NECESSARY WEATHER-CAUSED REMEDIATION WORK. TIME EXTENSIONS WILL BE CALCULATED IN HOURS AND ON AN HOUR-FOR-HOUR BASIS. DISINCENTIVES WILL BE WAIVED FOR THE DURATION OF ANY UNANTICIPATED WEATHER IMPACTS AND FOR THE DURATION NECESSARY TO PERFORM REMEDIATION WORK CAUSED BY UNANTICIPATED WEATHER.

IN THE EVENT OF WEATHER IMPACTS, THE CONTRACTOR SHALL REMAIN REASONABLY ONSITE DURING A WEATHER EVENT AND SHALL RESUME WORK IMMEDIATELY FOLLOWING THE CESSATION OF AN IMPACTING WEATHER EVENT. WEATHER DELAY DURATION IS NON-COMPENSABLE (I.E. "DOWNTIME"). THE CONTRACTOR WILL BE COMPENSATED FOR NECESSARY WORK CAUSED BY UNANTICIPATED WEATHER.

WEEKEND CLOSURE DISINCENTIVE:

UPON AWARD OF THE CONTRACT, IMMEDIATELY BEGIN COORDINATION WITH THE PROJECT ENGINEER TO IDENTIFY WEEKENDS TO PERFORM THE CLOSURES DURING THE REDUCED TRAFFIC VOLUMES MADE AVAILABLE DURING THE DEPARTMENT OF HEALTH "STAY-AT-HOME ORDER". IF THE ORDER IS RESCINDED, CONTINUE COORDINATION WITH THE PROJECT ENGINEER TO ENSURE CRITICAL WORK COMPLETION PRIOR TO THE CLOSURE COMPLETION DATE.

THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE ACCORDING TO THE TABLE IDENTIFIED AS THE CLOSURE COMPLETION DISINCENTIVE FOR EACH WEEKEND THE CONTRACTOR DOES NOT COMPLETE THE CRITICAL WORK.

EXTENSIONS WILL BE FOR WEEKENDS GRANTED FOR EXCUSABLE COMPENSABLE OR EXCUSABLE NON-COMPENSABLE REASONS (AS DEFINED BY THE CM&S) EXCEPT AS FOLLOWS: NO EXTENSIONS OF TIME WILL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, LABOR STRIKES, PANDEMICS OR EPIDEMICS. EXTENSIONS WILL NOT BE GRANTED IF IMMEDIATE COORDINATION DOES NOT OCCUR, OR INITIALLY IDENTIFIED WEEKEND CLOSURES ARE 2 WEEKENDS (OR LESS) PRIOR TO THE WEEKEND CLOSURE COMPLETION DATE. THE WEEKEND CLOSURE COMPLETION DATE WILL NOT BE REVISED REGARDLESS OF OTHER PROJECT DELAYS AS THIS WORK IS EXCLUSIVE OF OTHER CONTRACT WORK.

SPECIAL - WINDOW CONTRACT: SHORT TERM HOURLY CLOSURE WITH COMPLETION DISINCENTIVE TABLE

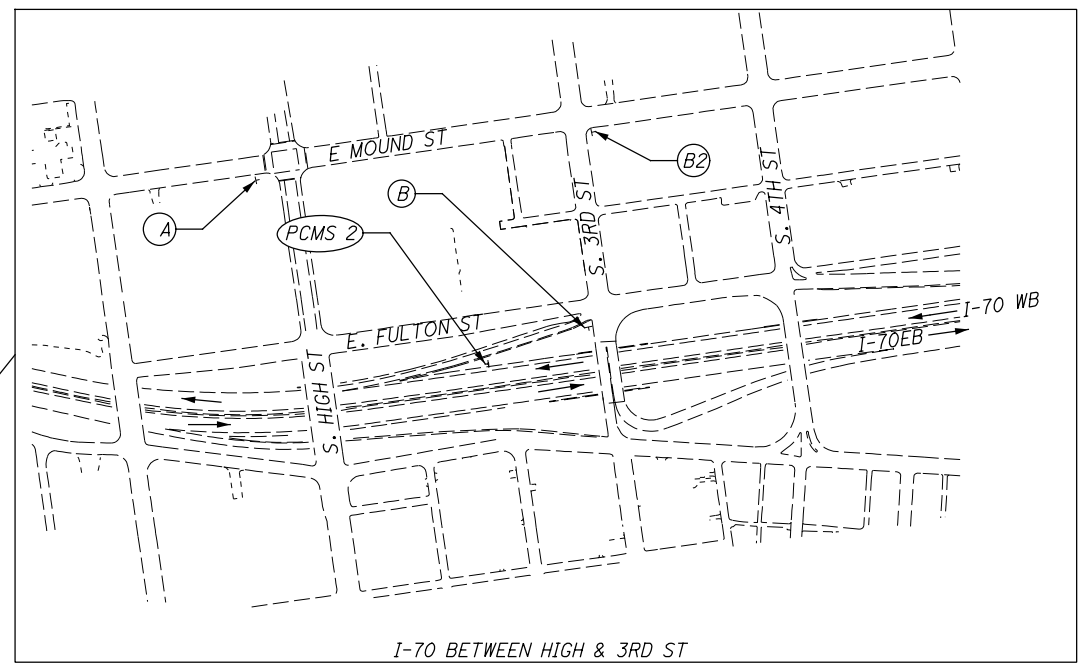
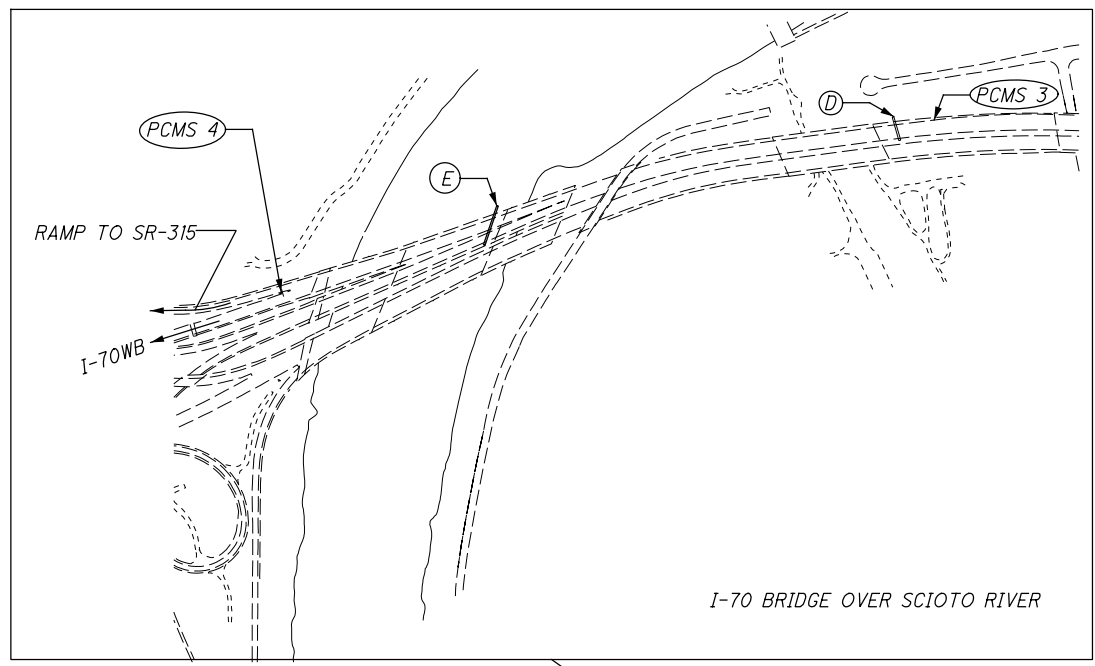
DESCRIPTION OF CRITICAL WORK	HOURS TO COMPLETE	DISINCENTIVE \$ PER HOUR	WEEKEND CLOSURE DATE (BOTH DIRECTIONS)	CLOSURE COMPLETION DISINCENTIVE PER WEEKEND
<p>FRA-70-1301L: COMPLETE CLOSURE OF I70WB BETWEEN THE HOURS 7PM FRIDAY THROUGH 5AM MONDAY TO COMPLETE THE REMOVAL OF ALL EXISTING ASPHALT COURSES, PERFORM ALL CONCRETE BRIDGE DECK PATCHING AND ALL FULL DEPTH BRIDGE REPAIRS, PLACE ALL PROPOSED ASPHALT OVERLAYS AND HAVE ALL I-70WB LANES OPEN IN ORIGINAL ALIGNMENT. THIS WORK SHALL BE COMPLETED PRIOR TO THE WEEKEND WORK ASSOCIATED WITH 1301R.</p> <p>FRA-70-1301R: COMPLETE CLOSURE OF I-70EB BETWEEN THE HOURS OF 7PM FRIDAY AND 5AM MONDAY TO COMPLETE THE REMOVAL OF ALL EXISTING ASPHALT COURSES AS DETAILED IN THE PLANS PERFORM ALL CONCRETE BRIDGE DECK PATCHING AND ALL FULL DEPTH BRIDGE REPAIRS, PLACE ALL PROPOSED ASPHALT OVERLAYS AND HAVE ALL I-70EB LANES AND I-70EB TO SR-315 RAMP OPEN IN THE ORIGINAL ALIGNMENT. THIS WORK IS TO BE COMPLETED IN ONE WEEKEND.</p>	58 hours per direction	\$10,000 per hour	6/16/20 20	\$50,000

CALCULATED
GVD
CHECKED
XXX

MAINTENANCE OF TRAFFIC NOTES

**FRA-270/670 SP
FY20 PRIORITY**

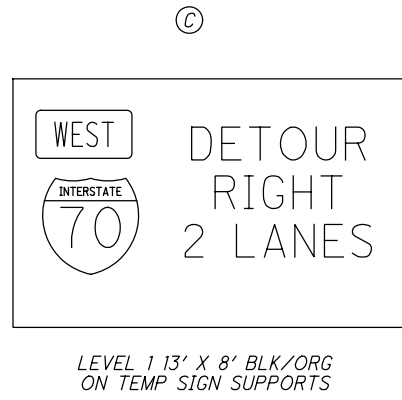
\\dfs-fra02.ms.gads.net\cadd\gpa\21002-SP-Cadd\Design\poschms\sheet\18c.dwg 1:22:14 AM 10/23/2018



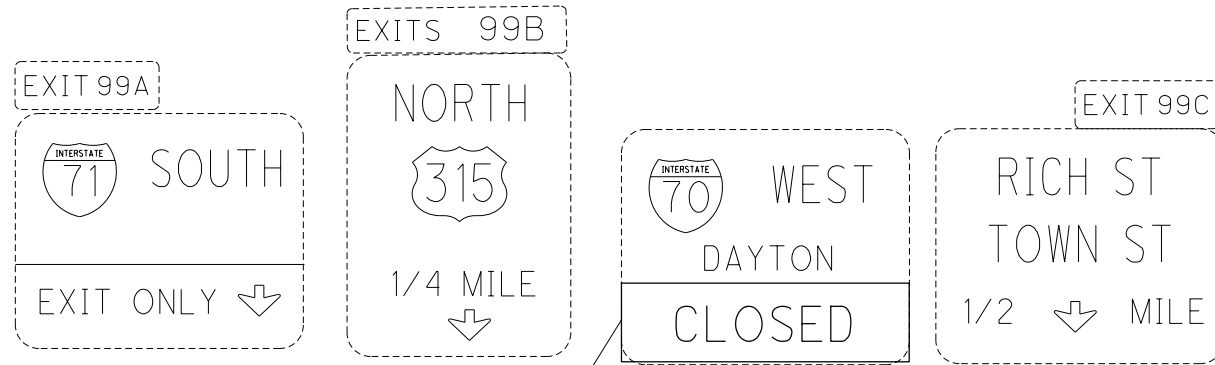
SEE SHEETS 18C - 18D FOR I-70 WEST CLOSURE

<p>FRA-270/670 SP</p> <p>FY20 PRIORITY</p>	<p>MAINTENANCE OF TRAFFIC - PHASE 1</p> <p>IR-70 WESTBOUND DETOUR</p>	<p>VP XXX</p> <p>CHECKED XXX</p>	
			<p>HORIZONTAL SCALE IN FEET</p> <p>2000 8000</p>

\\dpcfs002\cadd\p\proj\670\670-SP-FY20-Design\Roadway\Sheets\91903-91903-MD002-417-20-99-7-22-15-AM-CDEMNIS

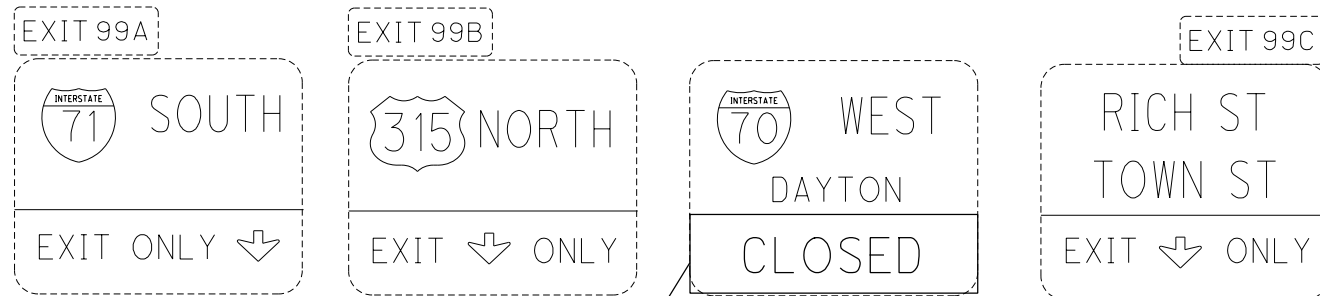


D OVERHEAD SIGN

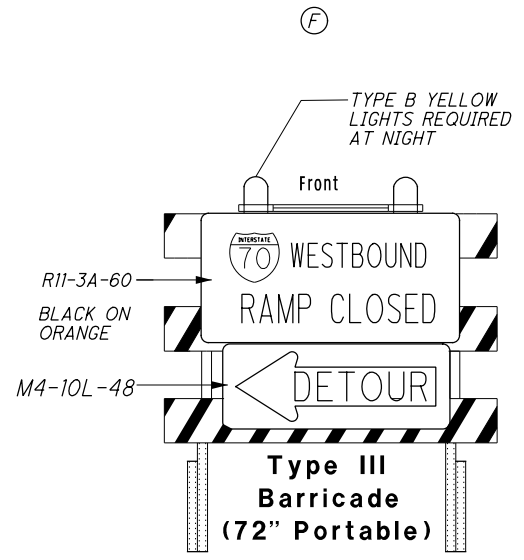
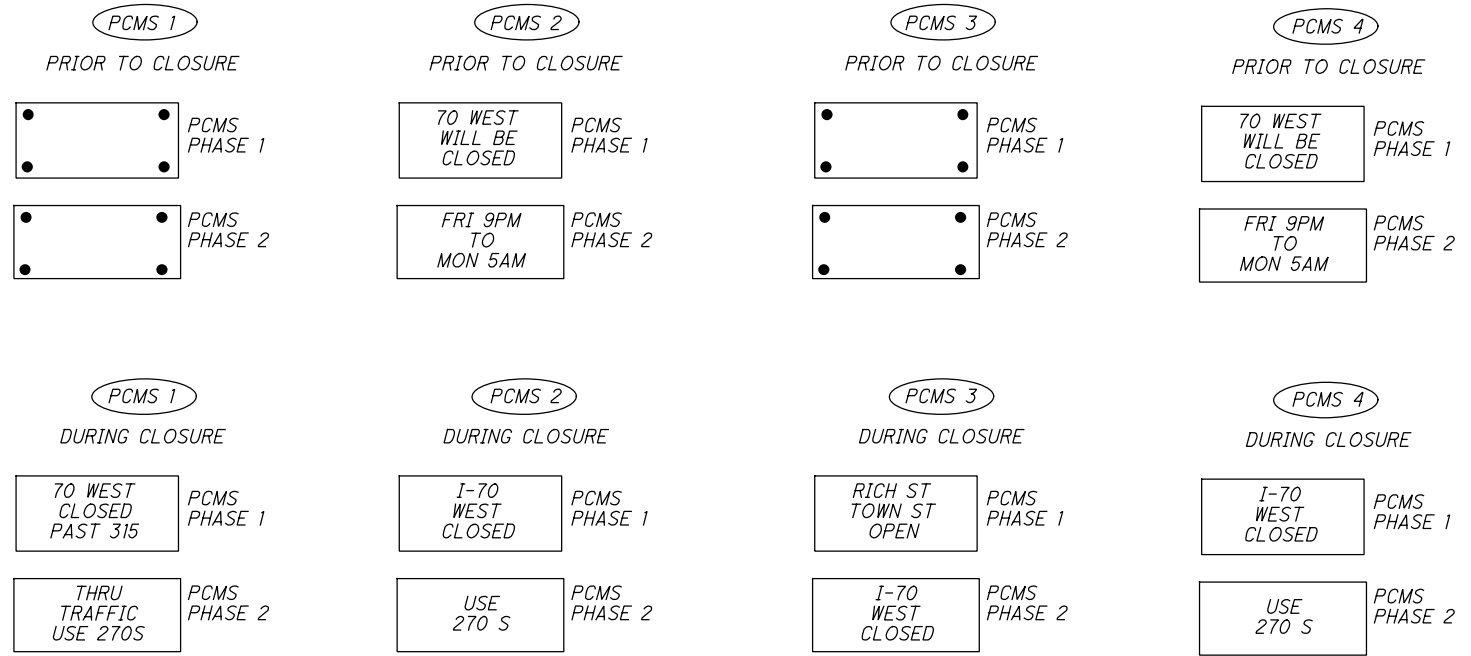


OVERLAY
"CLOSED" 16", SERIES E
BLACK ON ORAGNE 126" X 36"

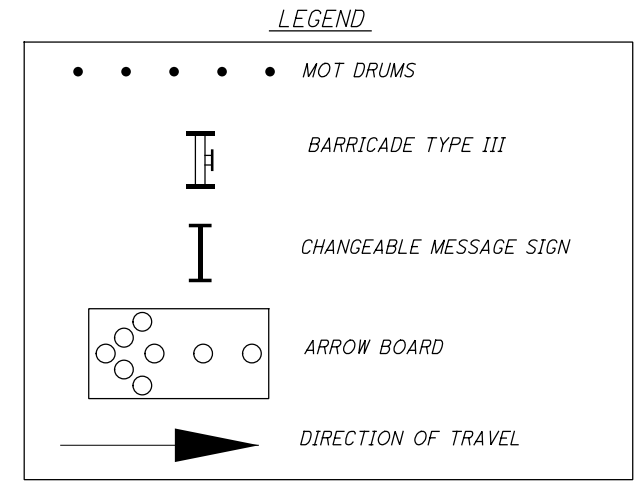
E OVERHEAD SIGN



OVERLAY
"CLOSED" 16", SERIES E
BLACK ON ORAGNE 126" X 36"



See Section 6F.63 and Figure 6F-7 of the OMTCD.



MAINTENANCE OF TRAFFIC - PHASE 1 DETOUR SIGNS

FRA-270/670 SP
FY20 PRIORITY

10E
18

CALCULATED
VP
CHECKED
XXX

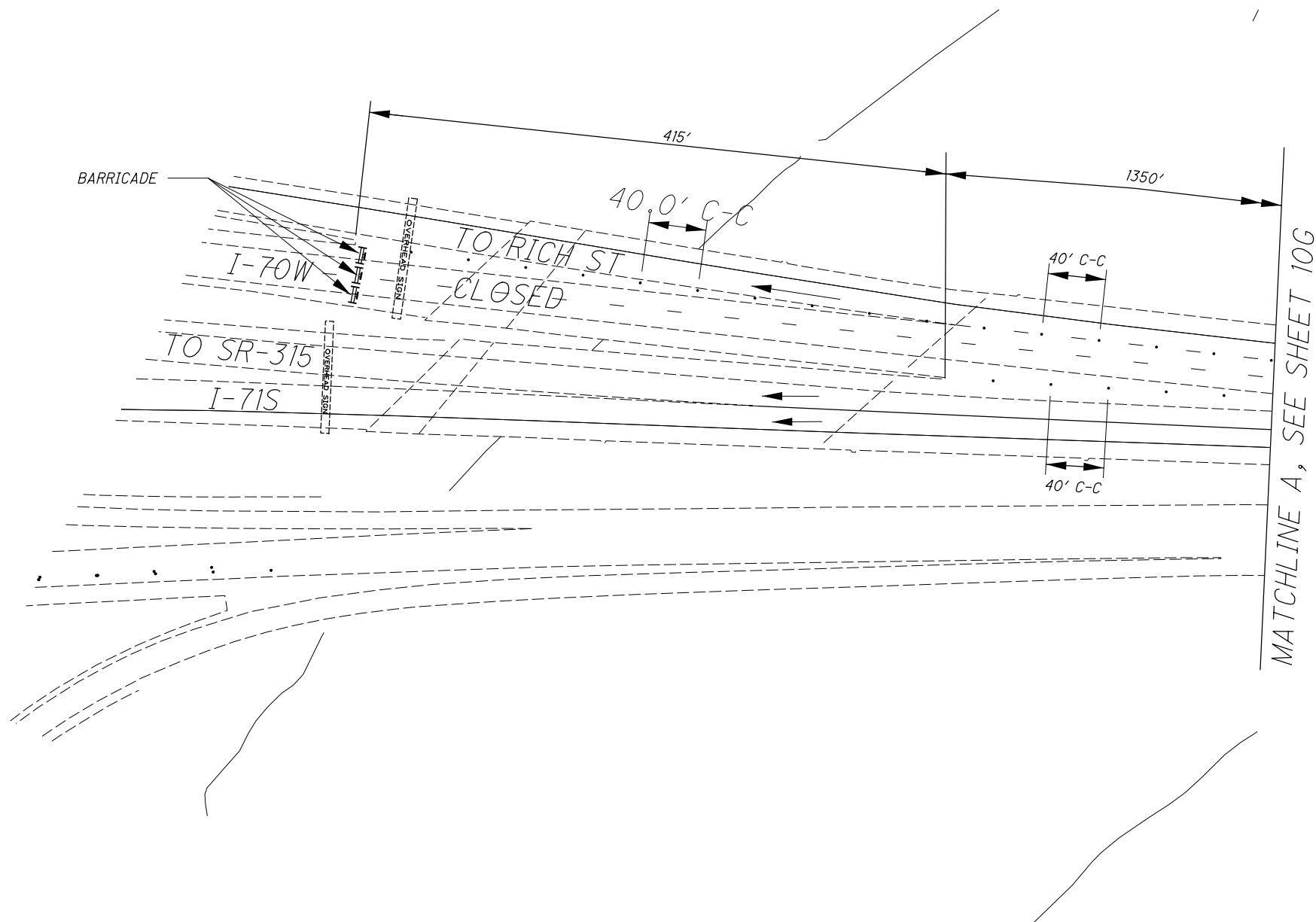
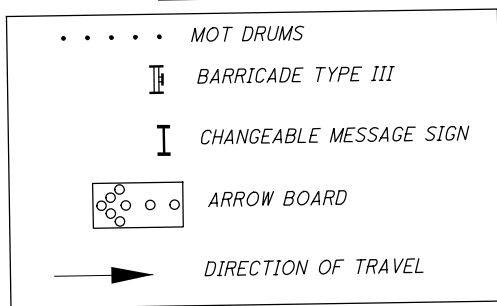
\\d06.fs002\cadd\ProjectData\91903_D06.SP.FY20\Desian\Roadway\Sheets\91903_MD001.dgn 91903_MD003 4/17/2020 7:22:15 AM GDENNIS

NOTE:

1. FOR ADDITIONAL DETOUR PLAN INFORMATION SEE SHEETS 10D - 10E

2. DRUM SPACING SHALL CONFORM TO OMTCD GUIDELINES UNLESS OTHER WISE SHOWN IN THE PLANS.

LEGEND



CALCULATED VP
CHECKED XXX

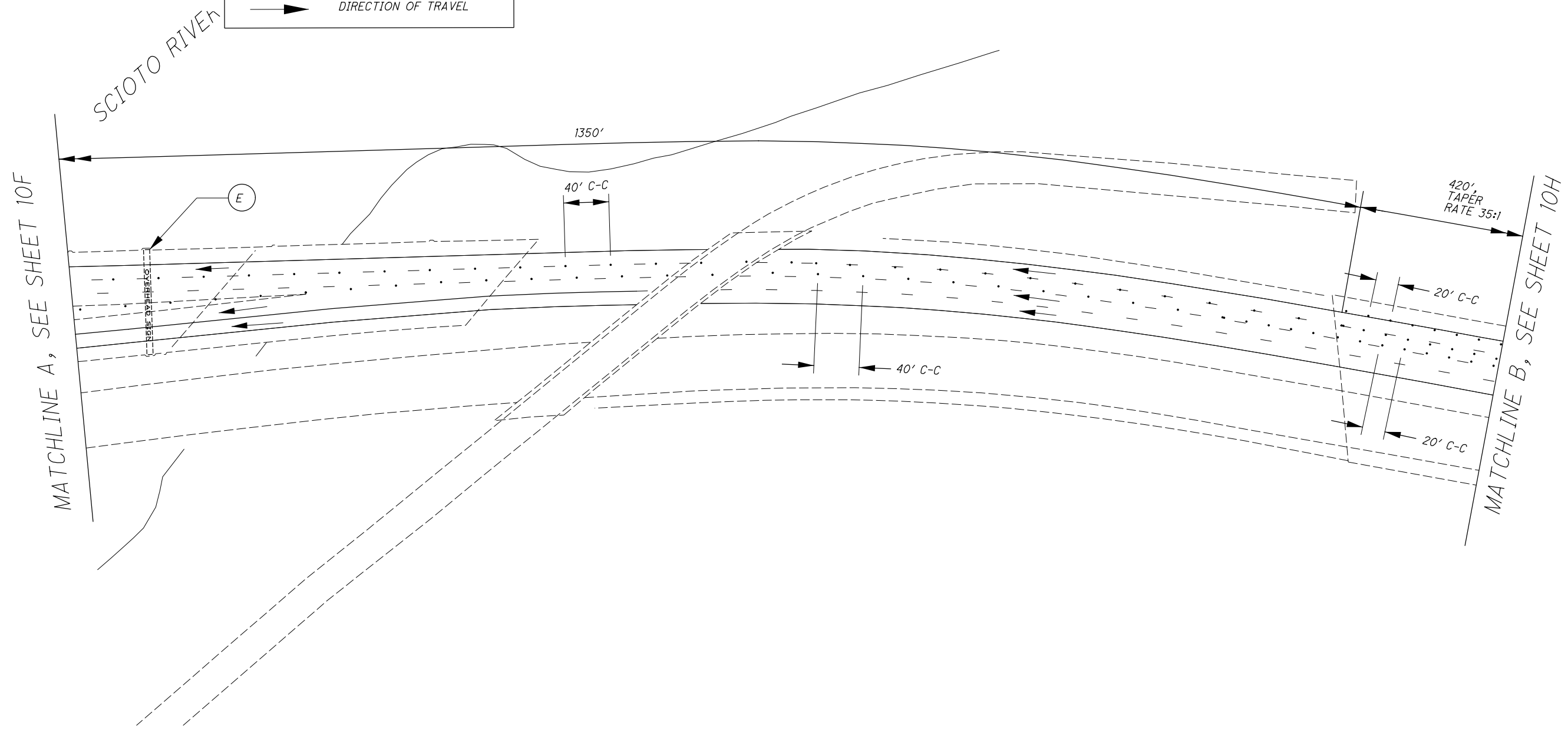
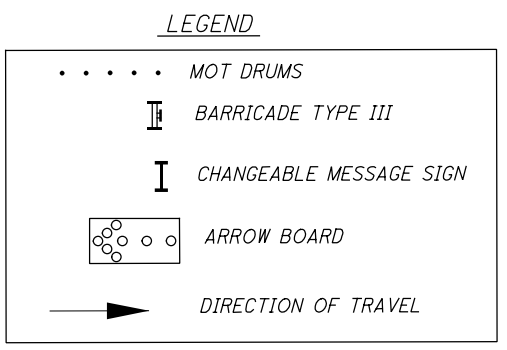
HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 1
IR-70 WESTBOUND DETOUR PLAN

FRA-270/670 SP
FY20 PRIORITY

\\d06.fs002\cadd\ProjectData\91903.D06.SP.FY20\Desjia\Roadway\Sheets\91903.MD001.dgn 9/9/03 10:22:16 AM G DENNIS

NOTE:
1. FOR ADDITIONAL DETOUR PLAN INFORMATION SEE SHEETS 10D - 10E
2. DRUM SPACING SHALL CONFORM TO OMTCD GUIDELINES UNLESS OTHERWISE SHOWN IN THE PLANS.



CALCULATED VP
CHECKED XXX

0 50 100
HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 1
IR-70 WESTBOUND DETOUR PLAN

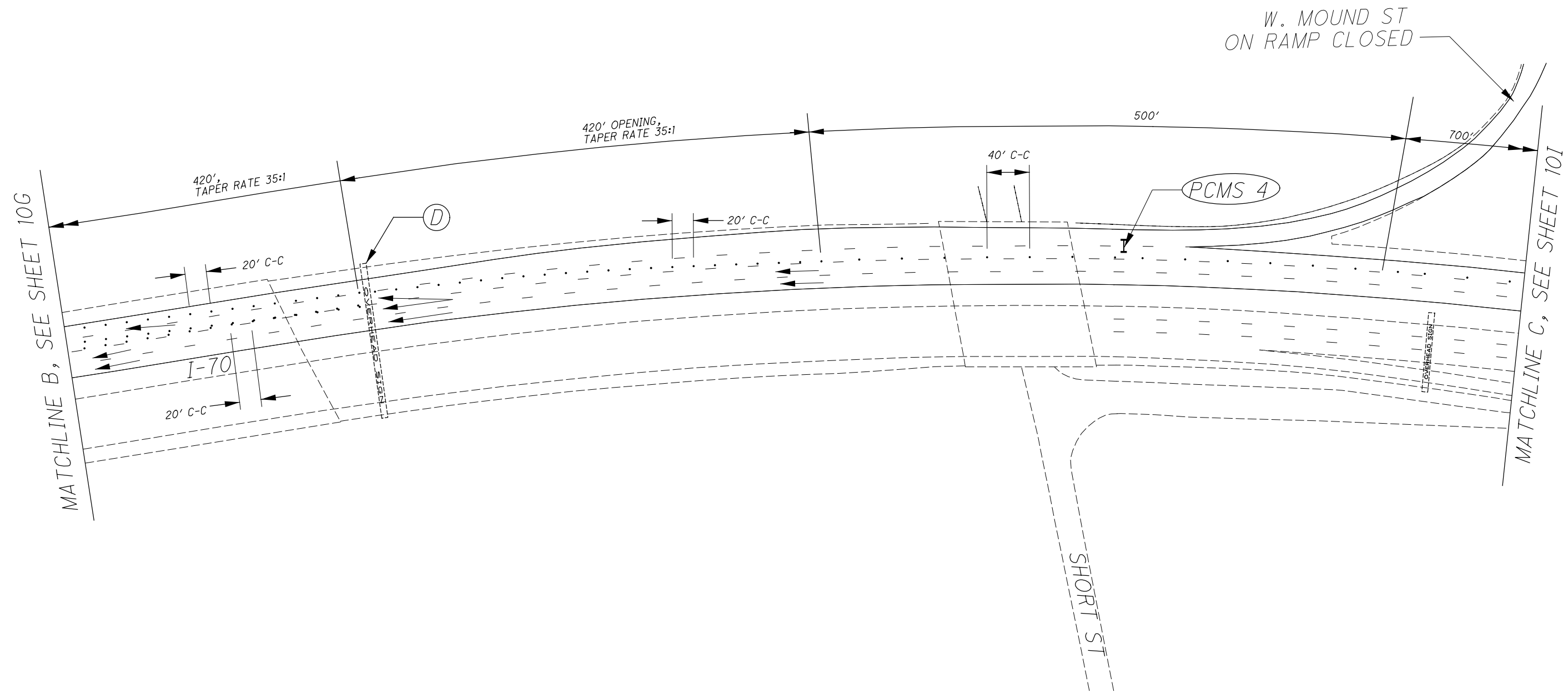
FRA-270/670 SP
FY20 PRIORITY

\\d06-fs002\cadda\Pr-o\ec+Data\91903_D06.SP.FY20\Design\Roadway\Sheets\91903_MD001.dgn_91903_MD005_4/17/2020_7:22:16 AM GDENNIS

NOTE:
1. FOR ADDITIONAL DETOUR PLAN INFORMATION SEE SHEETS 10D - 10E
2. DRUM SPACING SHALL CONFORM TO O MUTCD GUIDELINES UNLESS OTHERWISE SHOWN IN THE PLANS.

LEGEND

- MOT DRUMS
- [Symbol] BARRICADE TYPE III
- [Symbol] CHANGEABLE MESSAGE SIGN
- [Symbol] ARROW BOARD
- [Symbol] DIRECTION OF TRAVEL



CALCULATED VP
CHECKED XXX

0 50 100
HORIZONTAL SCALE IN FEET

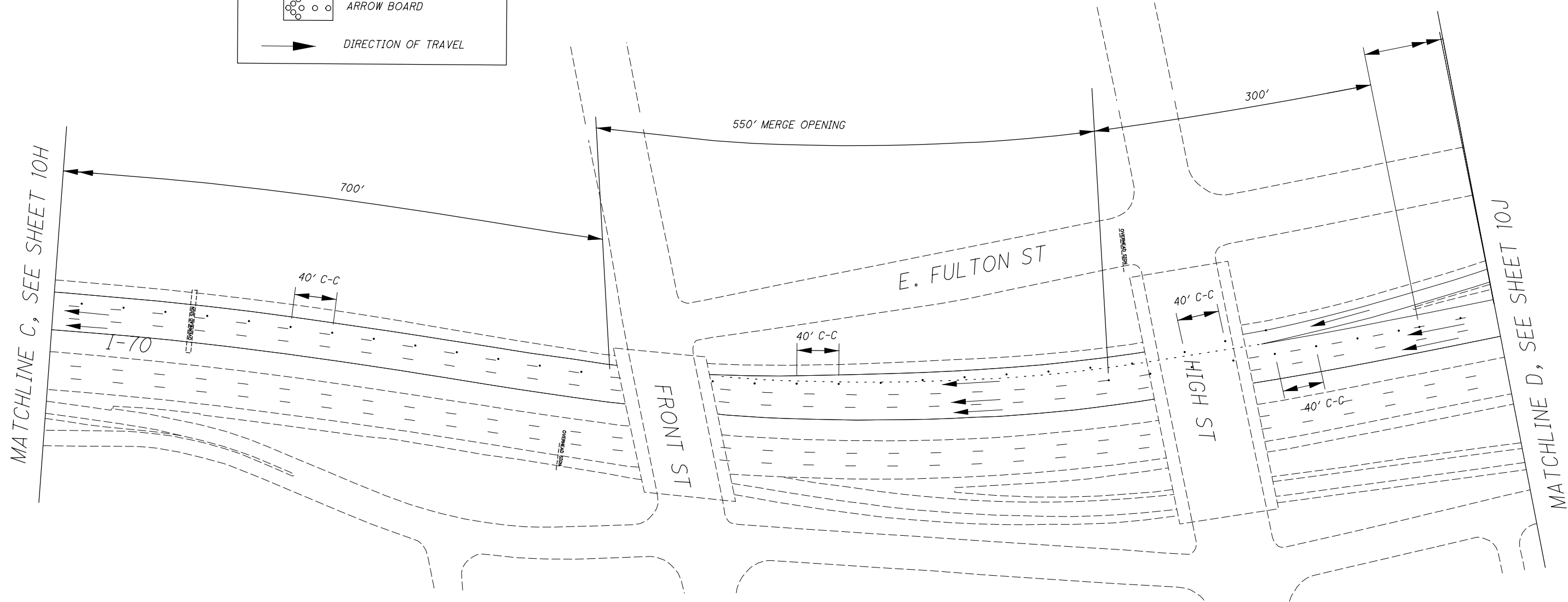
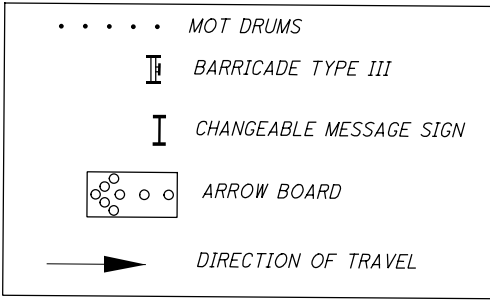
[North Arrow]

MAINTENANCE OF TRAFFIC - PHASE 1
IR-70 WESTBOUND DETOUR PLAN

FRA-270/670 SP
FY20 PRIORITY

NOTE:
 1. FOR ADDITIONAL DETOUR PLAN INFORMATION SEE SHEETS 10D - 10E
 2. DRUM SPACING SHALL CONFORM TO OMTCD GUIDELINES UNLESS OTHERWISE SHOWN IN THE PLANS.

LEGEND



CALCULATED VP
 CHECKED XXX
 HORIZONTAL SCALE IN FEET
 0 25 50 100

MAINTENANCE OF TRAFFIC - PHASE 1
IR-70 WESTBOUND DETOUR PLAN

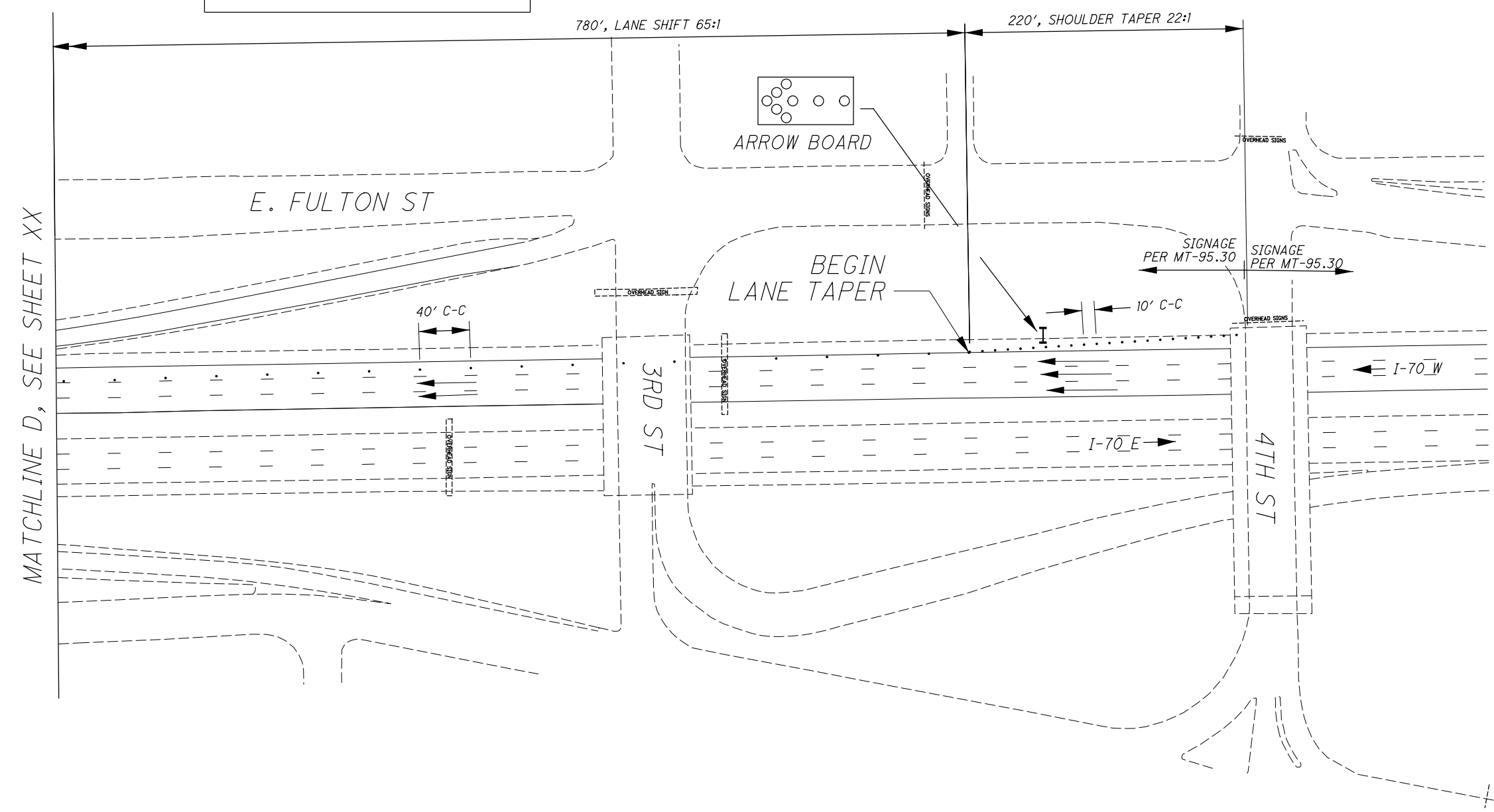
FRA-270/670 SP
FY20 PRIORITY

\\d06fs002\cadd\ProjectData\91903.D06.SP.FY20.Design.Roadway\Sheets\91903.MD001.dgn 91903.MD007 4/17/2020 7:22:17 AM GDENNIS

NOTE:
1. FOR ADDITIONAL DETOUR PLAN INFORMATION SEE SHEETS 10D - 10E
2. DRUM SPACING SHALL CONFORM TO OMTCD GUIDELINES UNLESS OTHERWISE SHOWN IN THE PLANS.

LEGEND

- MOT DRUMS
- ▬ BARRICADE TYPE III
- ▬ CHANGEABLE MESSAGE SIGN
- ○ ○ ○ ○ ARROW BOARD
- ➔ DIRECTION OF TRAVEL



CALCULATED VP
CHECKED XXX

0 25 50 100
HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 1
IR-70 WESTBOUND DETOUR PLAN

FRA-270/670 SP
FY20 PRIORITY

I:\ProjectData\91903_D06_SP_FY20_Design\Roadway\Sheets\91903_MDI01.dgn 91903_MDI01 4/17/2020 7:22:18 AM GDENNIS



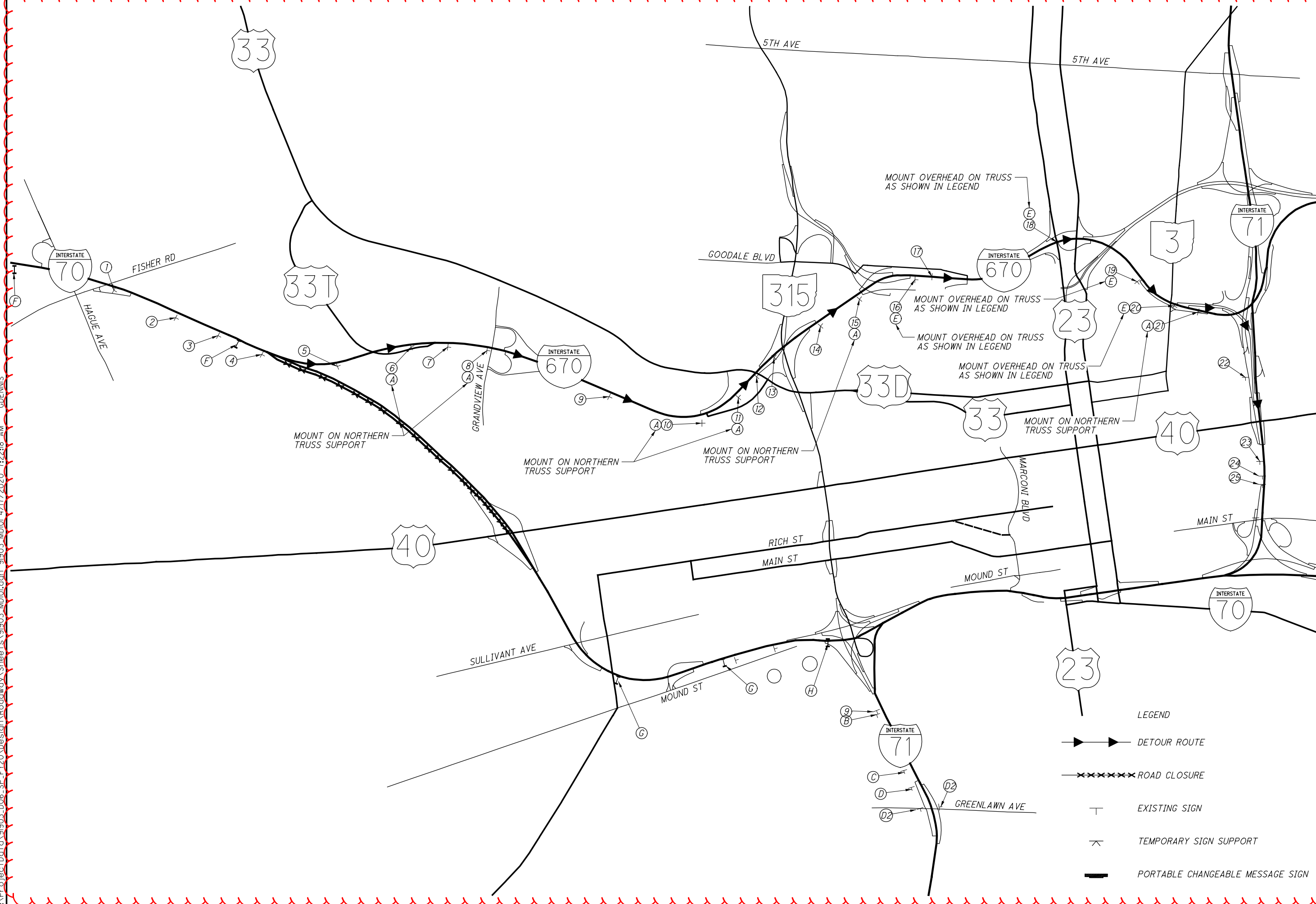
500
HORIZONTAL
SCALE IN FEET

CHECKED

**MAINTENANCE OF TRAFFIC - PHASE 2
IR-70 EASTBOUND DETOUR**

**FRA-270/670 SP
FY20 PRIORITY**

10K
18



- LEGEND**
- DETOUR ROUTE
 - ROAD CLOSURE
 - EXISTING SIGN
 - TEMPORARY SIGN SUPPORT
 - PORTABLE CHANGEABLE MESSAGE SIGN

MOUNT ON NORTHERN TRUSS SUPPORT

MOUNT ON NORTHERN TRUSS SUPPORT

MOUNT ON NORTHERN TRUSS SUPPORT

MOUNT OVERHEAD ON TRUSS AS SHOWN IN LEGEND

MOUNT OVERHEAD ON TRUSS AS SHOWN IN LEGEND

MOUNT OVERHEAD ON TRUSS AS SHOWN IN LEGEND

MOUNT OVERHEAD ON TRUSS AS SHOWN IN LEGEND

MOUNT ON NORTHERN TRUSS SUPPORT

LEGEND

DETOUR ROUTE

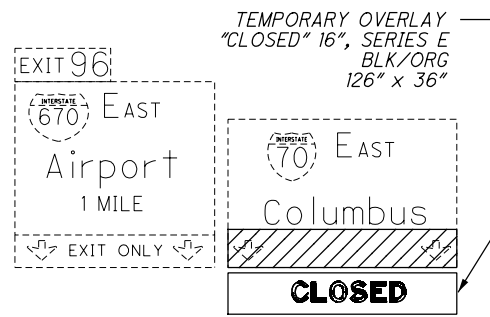
ROAD CLOSURE

EXISTING SIGN

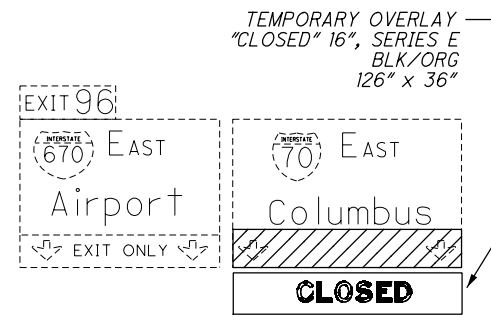
TEMPORARY SIGN SUPPORT

PORTABLE CHANGEABLE MESSAGE SIGN

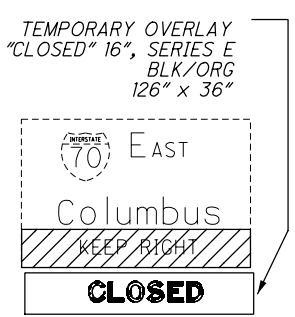
I:\ProjectData\91903_006.SP\FY20.Design\Roadway\Sheets\91903.MDI01.dgn 91903.MDI02 4/17/2020 7:22:18 AM GDENNIS



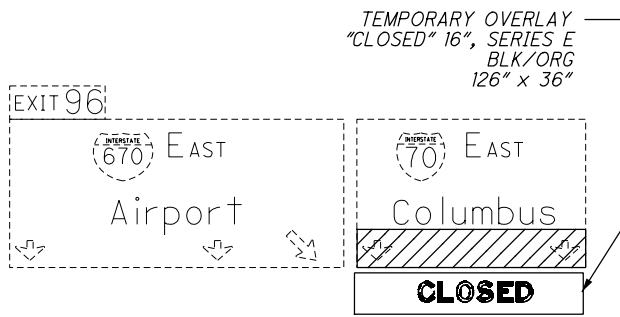
① TRUSS MOUNTED SIGN



② TRUSS MOUNTED SIGN



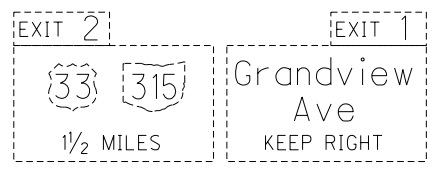
③ CANTILEVER MOUNTED SIGN



④ TRUSS MOUNTED SIGN



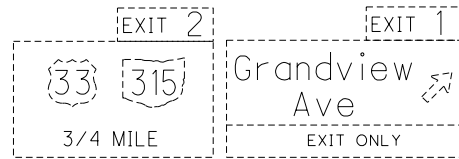
CANTILEVER MOUNTED SIGN



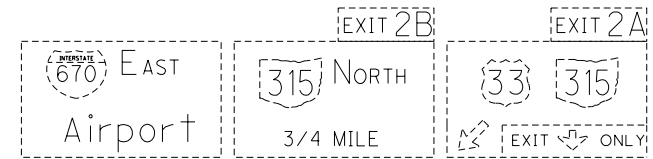
TRUSS MOUNTED SIGN



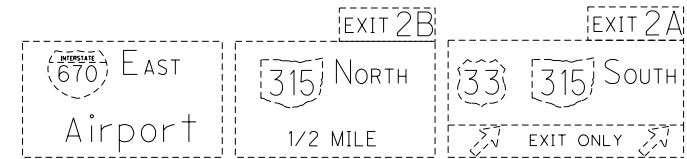
CANTILEVER MOUNTED SIGN



TRUSS MOUNTED SIGN



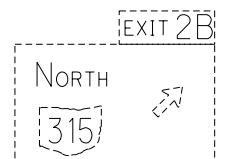
TRUSS MOUNTED SIGN



TRUSS MOUNTED SIGN



TRUSS MOUNTED SIGN



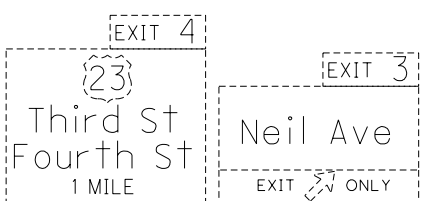
CANTILEVER MOUNTED SIGN



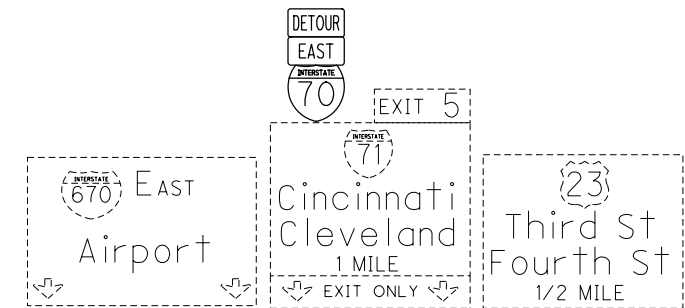
CANTILEVER MOUNTED SIGN



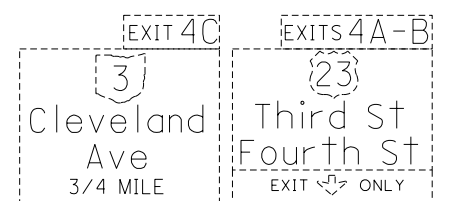
CANTILEVER MOUNTED SIGN



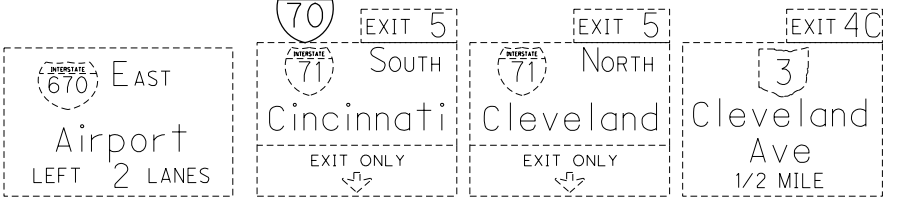
TRUSS MOUNTED SIGN



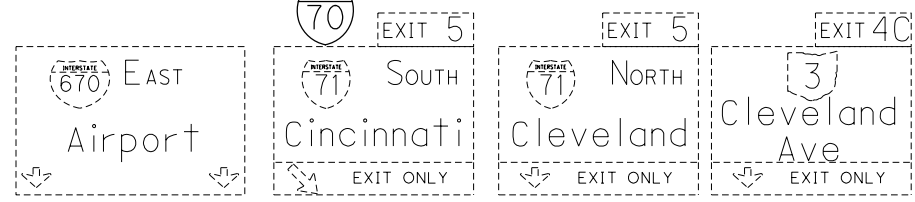
TRUSS MOUNTED SIGN



STRUCTURE MOUNTED SIGN



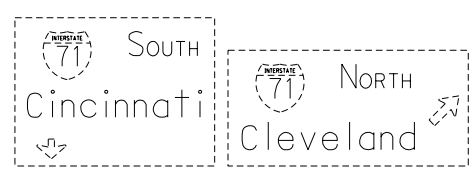
STRUCTURE MOUNTED SIGN



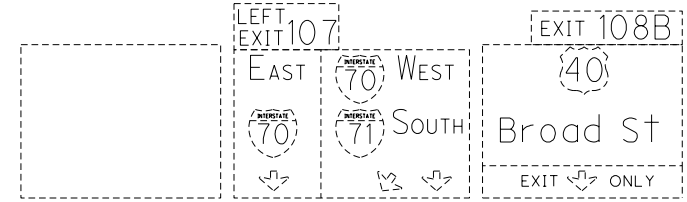
TRUSS MOUNTED SIGN



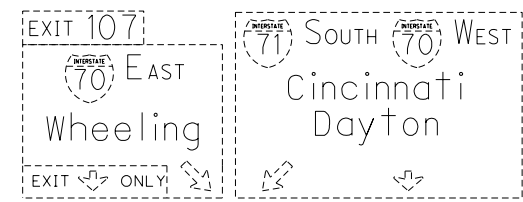
TRUSS MOUNTED SIGN



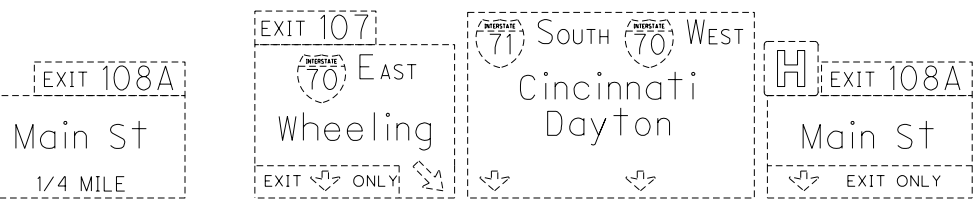
TRUSS MOUNTED SIGN



TRUSS MOUNTED SIGN



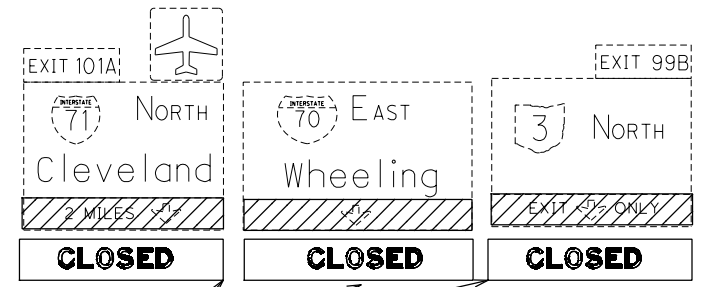
TRUSS MOUNTED SIGN



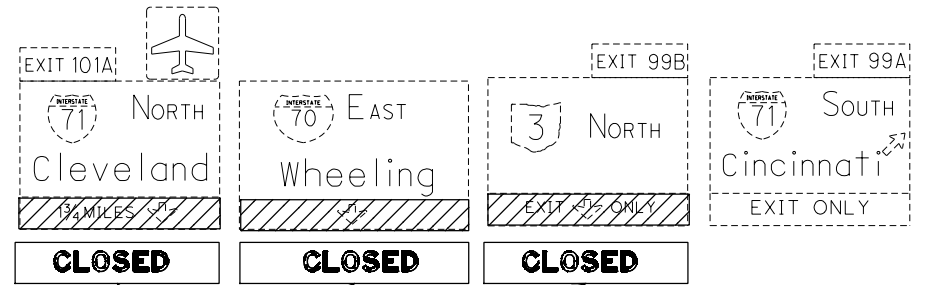
TRUSS MOUNTED SIGN



TRUSS MOUNTED SIGN



TRUSS MOUNTED SIGN



TRUSS MOUNTED SIGN

NOTE: OVERHEAD DETOUR SIGNS SHALL BE TAKEN DOWN OR COVERED WITH A BLANK OVERLAY WHEN NOT IN USE.

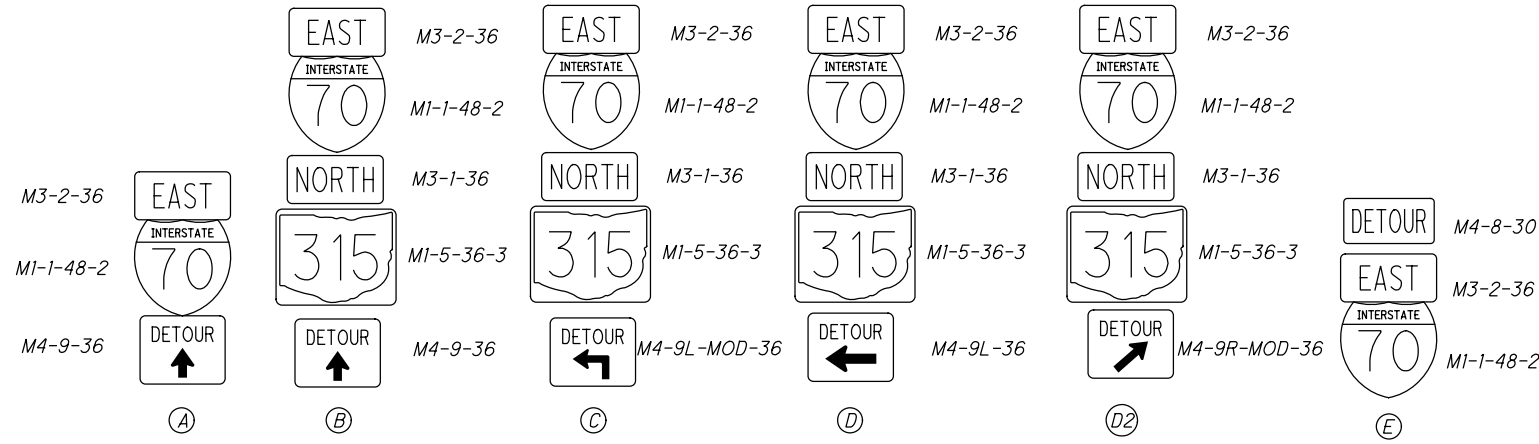
TEMPORARY OVERLAY "CLOSED" 16", SERIES E BLK/ORG 126" x 36"

TEMPORARY OVERLAY "CLOSED" 16", SERIES E BLK/ORG 126" x 36"

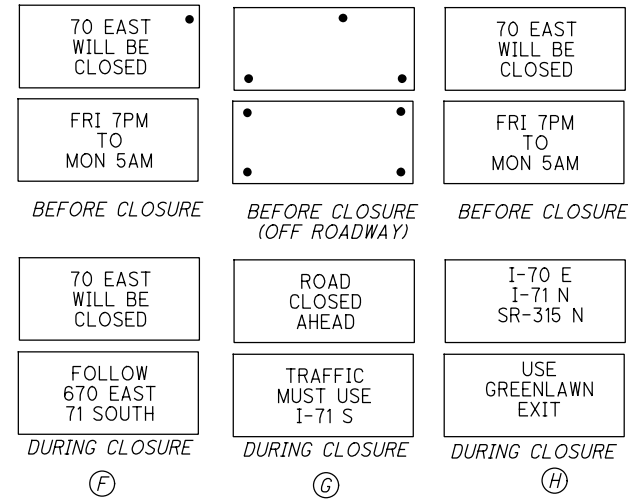
CALCULATED VP CHECKED

MAINTENANCE OF TRAFFIC - PHASE 2 IR-70 EASTBOUND DETOUR SIGNS

FRA-270/670 SP FY20 PRIORITY



PORTABLE CHANGEABLE MESSAGE SIGN



CALCULATED
VP
CHECKED

MAINTENANCE OF TRAFFIC - PHASE 2
IR-70 EASTBOUND DETOUR SIGNS

FRA-270/670 SP
FY20 PRIORITY

I:\Projects\Detour\903.Dwg.SP.FY20.Detour.Roadway.Sheets\903-MD101.dwg 9/17/2020 7:22:39 AM GDENNIS

LEGEND

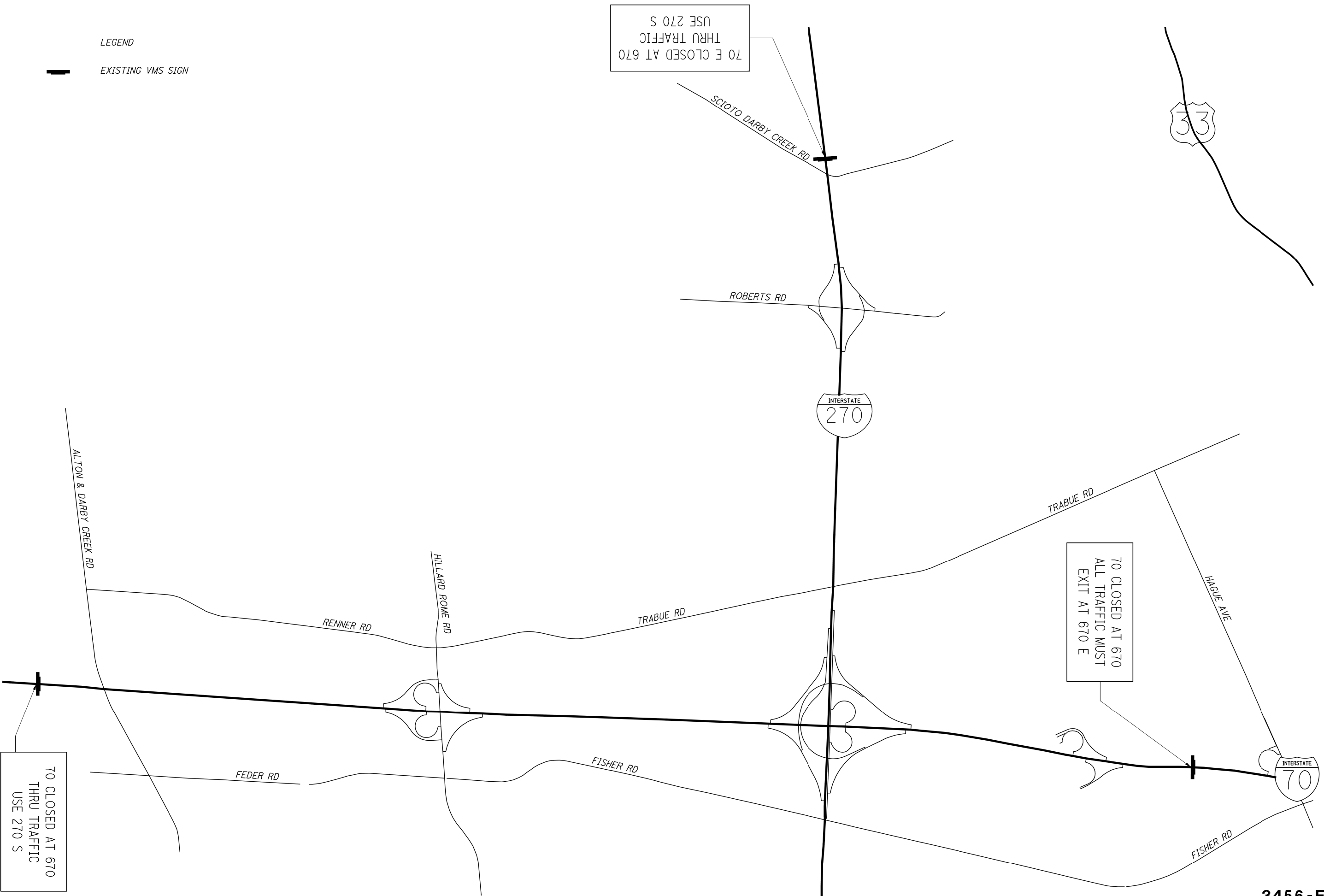
EXISTING VMS SIGN



70 CLOSED AT 670
THRU TRAFFIC
USE 270 S

70 E CLOSED AT 670
THRU TRAFFIC
USE 270 S

70 CLOSED AT 670
ALL TRAFFIC MUST
EXIT AT 670 E



SCALE BAR
0 500 1000
HORIZONTAL SCALE IN FEET

VP
CHECKED XXX

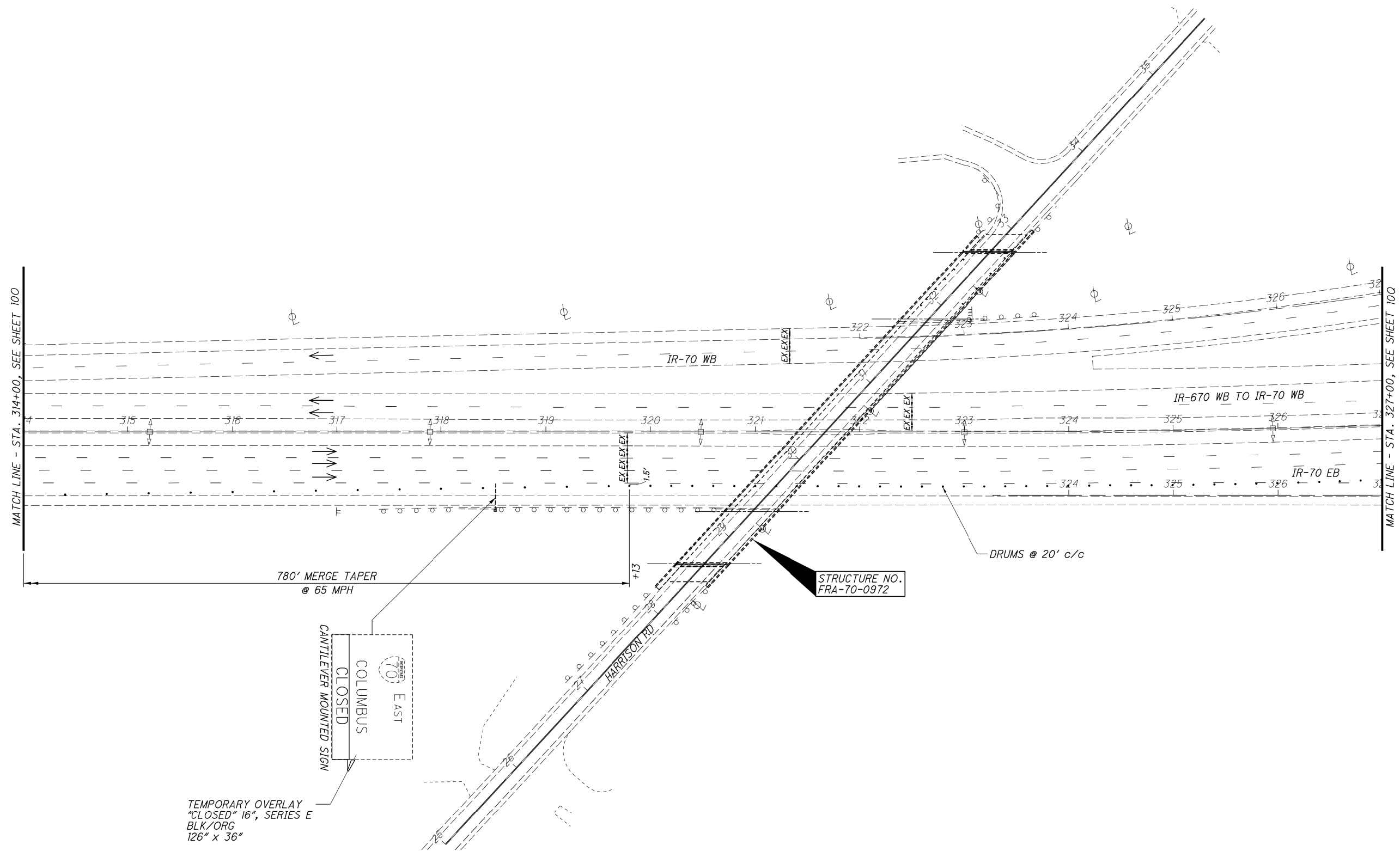
MAINTENANCE OF TRAFFIC - PHASE 2
IR-70 EASTBOUND DETOUR PLAN

FRA-270/670 SP
FY20 PRIORITY

10N
18

3456 - E

AT 10/18/16 10:00 AM, THE FOLLOWING INFORMATION WAS OBTAINED FROM THE RECORD DRAWINGS AND FIELD SURVEY:



TEMPORARY OVERLAY
 "CLOSED" 16", SERIES E
 BLK/ORG
 126" x 36"



780' MERGE TAPER
 @ 65 MPH

STRUCTURE NO.
 FRA-70-0972

DRUMS @ 20' c/c

DRUM SPACING CHART	
TANGENT	80' c/c
TAPER	40' c/c
RADII	10' c/c

LEGEND
 • DRUM
 → OPEN TRAVEL LANE

FRA-270/670-SP

FY20 PRIORITY

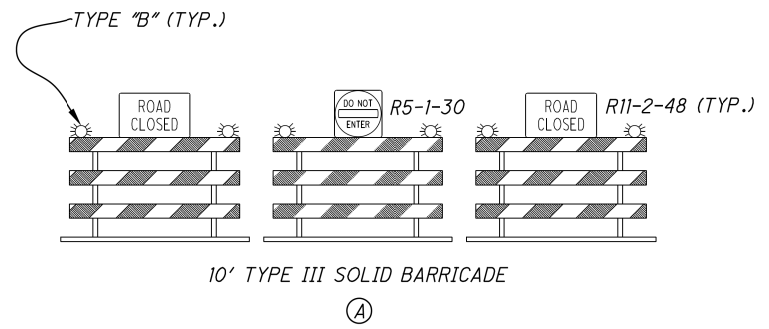
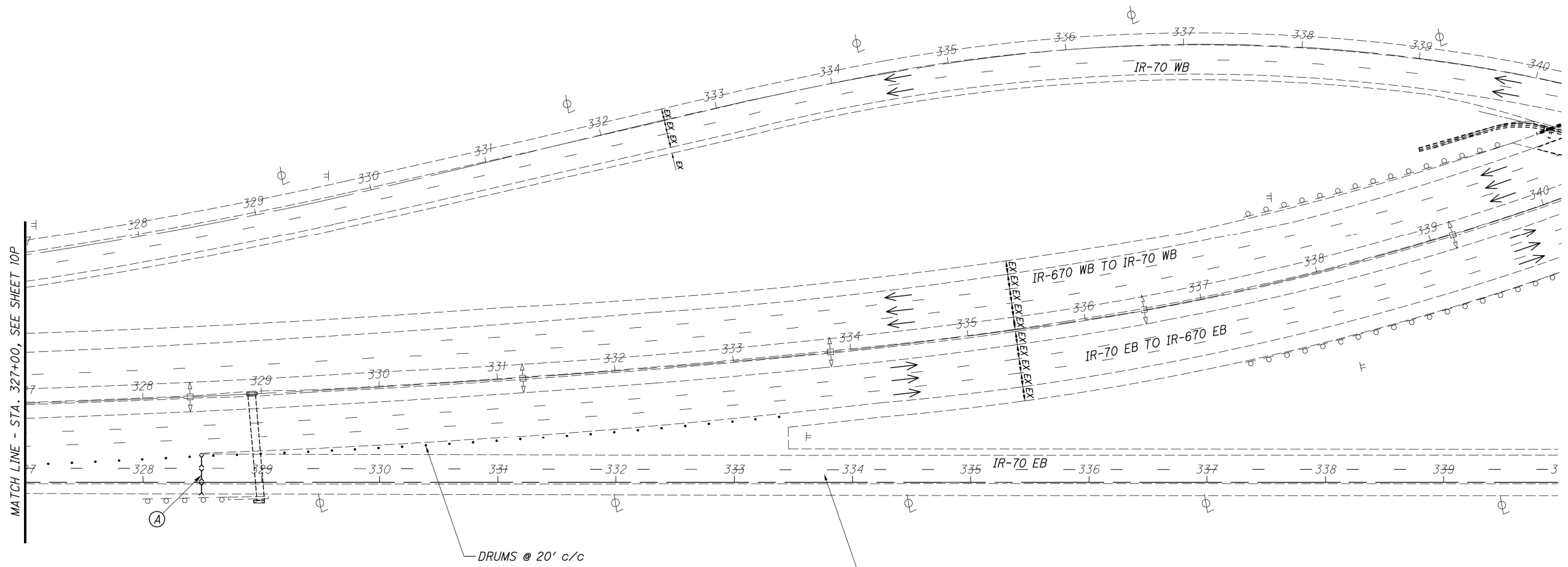
MAINTENANCE OF TRAFFIC - PHASE 2

IR-70 EASTBOUND DETOUR PLAN

CALCULATED BY: VP
 CHECKED: XXX

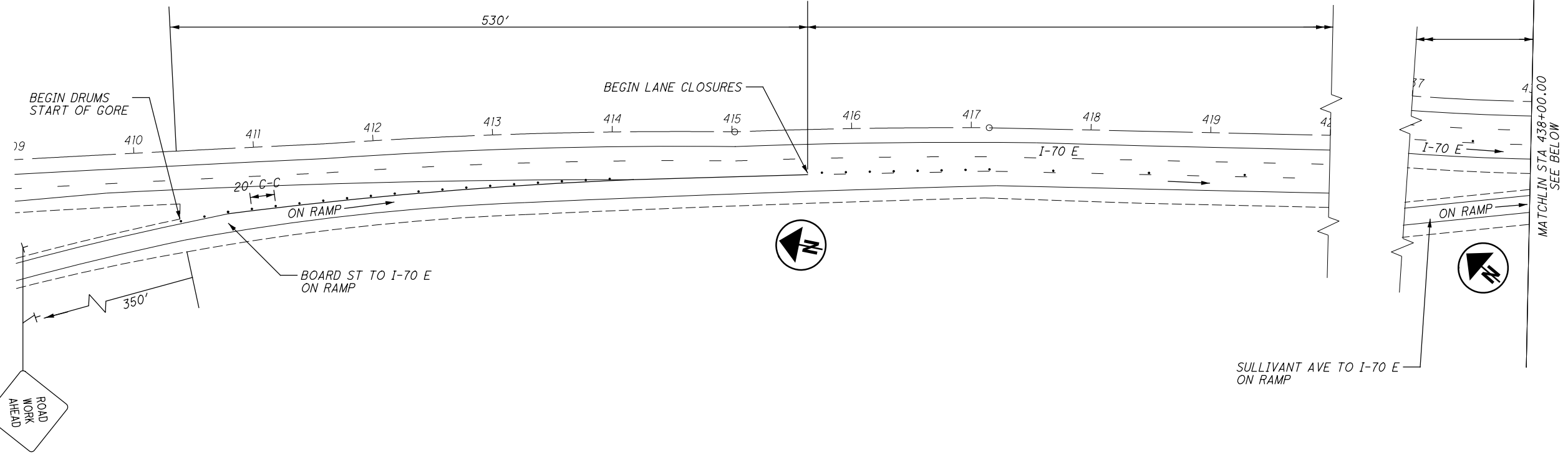
10P
 18

0 500 1000 2000
 HORIZONTAL SCALE IN FEET

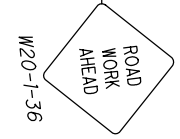


DRUM SPACING CHART	
TANGENT	80' c/c
TAPER	40' c/c
RADII	10' c/c

- LEGEND**
- DRUM
 - X TYPE III BARRICADE
 - OPEN TRAVEL LANE



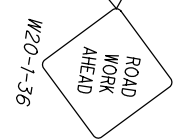
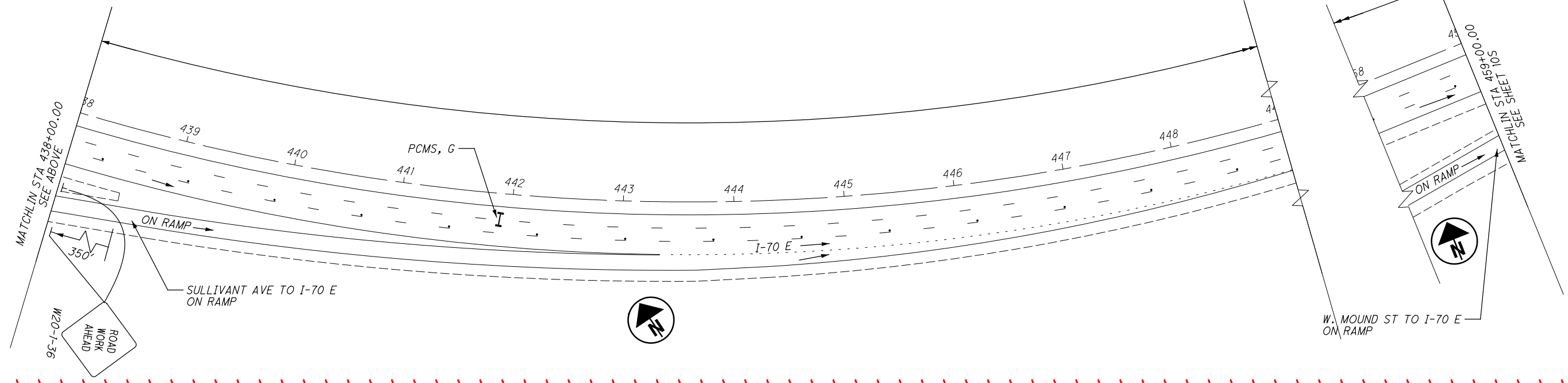
SULLIVANT AVE TO I-70 E
ON RAMP



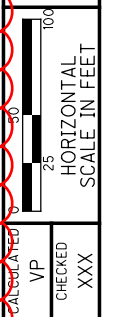
- NOTE:
1. STATIONING SHOWN ON SHEET SHALL ONLY BE USED FOR DISTANCE CALCULATION. STATIONING SHALL NOT BE USED FOR CONSTRUCTION.
 2. FOR ADDITIONAL DETOUR PLAN INFORMATION SEE SHEETS 10K-10M
 3. DRUM SPACING SHALL CONFORM TO OMTCD GUIDELINES UNLESS OTHERWISE SHOWN IN THE PLANS.

LEGEND

.....	DRUMS
— —	CHANGEABLE MESSAGE SIGN
— — —	BARRICADE TYPE III
→	DIRECTION OF TRAVEL
—	TEMPORARY SIGNAGE

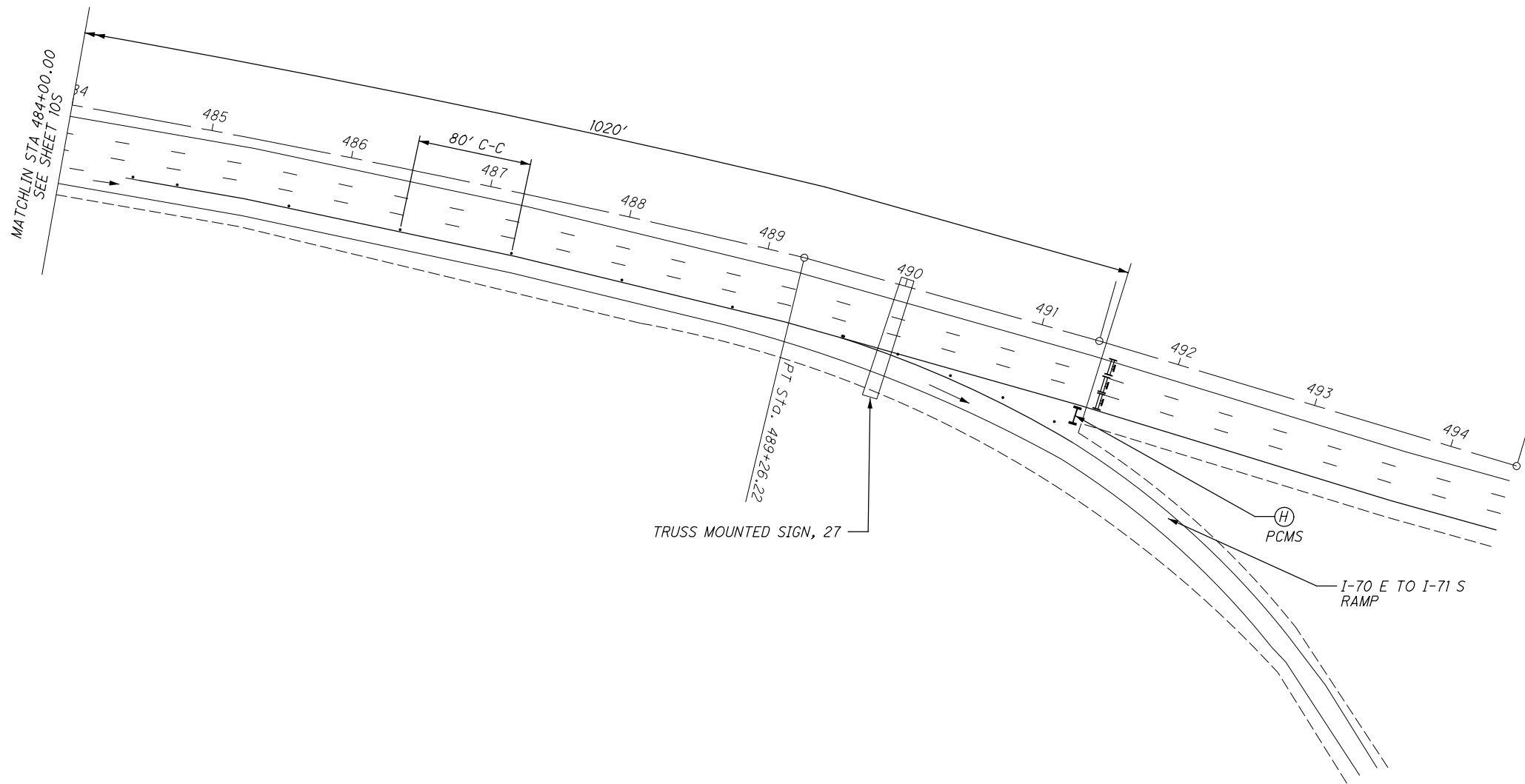


W. MOUND ST TO I-70 E
ON RAMP



MAINTENANCE OF TRAFFIC - PHASE 2
IR-70 EASTBOUND DETOUR PLAN

FRA-270/670 SP
FY20 PRIORITY



NOTE:
 1. STATIONING SHOWN ON SHEET SHALL ONLY BE USED FOR DISTANCE CALCULATION. STATIONING SHALL NOT BE USED FOR CONSTRUCTION.

2. FOR ADDITIONAL DETOUR PLAN INFORMATION SEE SHEETS 10K-10M

3. DRUM SPACING SHALL CONFORM TO OMTCD GUIDELINES UNLESS OTHERWISE SHOWN IN THE PLANS.

LEGEND

- DRUMS
- CHANGEABLE MESSAGE SIGN
- BARRICADE TYPE III
- DIRECTION OF TRAVEL
- TEMPORARY SIGNAGE

CALCULATED BY: [Signature]

CHECKED: XXX

0 25 50 100
 HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 2
IR-70 EASTBOUND DETOUR PLAN

FRA-270X670 SP
FY20 PRIORITY

I:\Project+Data\91903_006_SP_FY20_Design\Roadway\Sheets\91903_GG001.dgn 91903_GG001 4/17/2020 7:26:54 AM GDENNIS

SHEET NUMBER						PARTICIPATION			ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	CALCULATED GVD	CHECKED XXX
5-6	7-10	12	18	18A		01/IMS/PV	02/IMS/PV									
												EROSION CONTROL				
							1000		832	30000	1000	EACH	EROSION CONTROL			
													PAVEMENT			
							863		251	01021	863	SY	PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN, TYPE 1	5		
							1,173		251	01021	1,173	SY	PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN, TYPE 2	5		
							497		251	01031	497	CY	PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN, TYPE 3	5		
							21,824		254	01000	21,824	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1.5"			
							1,855		407	20000	1,855	GAL	NON-TRACKING TACK COAT			
							909		442	10001	909	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (446), AS PER PLAN	6		
							15,840		872	10000	15,840	FT	VOID REDUCING ASPHALT MEMBRANE (VRAM)	6		
													TRAFFIC CONTROL			
							257		621	00100	257	EACH	RPM			
							678		621	54000	678	EACH	RAISED PAVEMENT MARKER REMOVED			
	4								632	26501	4	EACH	DETECTOR LOOP, AS PER PLAN	5		
	4								632	27200	4	EACH	LOOP DETECTOR TIE IN			
							4.92		642	00104	4.92	MILE	EDGE LINE, 6", TYPE 1			
							3.72		642	00204	3.72	MILE	LANE LINE, 6", TYPE 1			
							3560		642	00404	3,560	FT	CHANNELIZING LINE, 12", TYPE 1			
													MAINTENANCE OF TRAFFIC			
							1440		614	11110	1440	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE			
							LS		614	12421	LS		DETOUR SIGNING			
							15		614	18601	15	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	8		
							3.72		614	20110	3.72	MILE	WORK ZONE LANE LINE, CLASS I, 6", 642 PAINT			
							4.92		614	22110	4.92	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT			
							3560		614	23210	3,560	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 642 PAINT			
													STRUCTURES OVER 20 FOOT SPAN			
													FRA-70-13.01L (SFN 2504677)			
							3,529		202	23500	3,529	SY	WEARING COURSE REMOVED			
							706		407	20000	706	GAL	NON-TRACKING TACK COAT			
							393		442	20200	393	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (448)			
							177		519	12304	177	SY	PATCHING CONCRETE BRIDGE DECK - TYPE C, AS PER PLAN	18A		
							5		519	12510	5	SF	SPECIAL - PATCHING CONCRETE BRIDGE DECK, FULL DEPTH, AS PER PLAN	18A		
							LUMP		530	00200	LS		SPECIAL - STRUCTURES, MISC.: FALSEWORK	6		
							2		614	12337	2	EACH	WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL), AS PER PLAN	18A		
							1,509		622	41001	1,509	FT	PORTABLE BARRIER, 32", AS PER PLAN			
							0.29		646	10010	0.29	MI	EDGE LINE, 6"			
							0.15		646	10110	0.15	MI	LANE LINE, 6"			
							148		856	10000	148	CY	BRIDGE DECK WATERPROOFING ASPHALT CONCRETE			
													FRA-70-13.01R (SFN 2504766)			
							1,954		202	23500	1,954	SY	WEARING COURSE REMOVED			
							391		407	20000	391	GAL	NON-TRACKING TACK COAT			
							218		442	20200	218	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (448)			
							98		519	12304	98	SY	PATCHING CONCRETE BRIDGE DECK - TYPE C, AS PER PLAN	18A		
							5		519	12510	5	SF	SPECIAL - PATCHING CONCRETE BRIDGE DECK, FULL DEPTH, AS PER PLAN	18A		
							LUMP		530	00200	LS		SPECIAL - STRUCTURES, MISC.: FALSEWORK	6		
							0.23		646	10010	0.23	MI	EDGE LINE, 6"			
							0.12		646	10110	0.12	MI	LANE LINE, 6"			
							82		856	10000	82	CY	BRIDGE DECK WATERPROOFING ASPHALT CONCRETE			
													INCIDENTALS			
							LS		614	11000	LS		MAINTAINING TRAFFIC			
							LS		623	10001	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN	5		
							LS		624	10000	LS		MOBILIZATION			

GENERAL SUMMARY

FRA-270/670 SP
FY20 PRIORITY

ESTIMATED QUANTITIES

ITEM	ITEM EXT.	TOTAL	UNIT	DESCRIPTION	CALC.	DATE	CHK'D	DATE
					ELP	10/7/19	ATM	10/7/19
					FRA-70-1301L	FRA-70-1301R		SHEET REF.
202	23500	5483	SY	WEARING COURSE REMOVED	3,529	1,954		
407	20000	1097	GAL	NON-TRACKING TACK COAT	706	391		
442	20200	611	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (448)	393	218		
519	230	276	SY	PATCHING CONCRETE BRIDGE DECK, TYPE C, AS PER PLAN	177	98		2, 4
519	12510	10	SY	SPECIAL - PATCHING CONCRETE BRIDGE DECK, FULL DEPTH, AS PER PLAN	5	5		2, 4
530	00200	LUMP	LS	SPECIAL - STRUCTURES, MISC.: FALSEWORK	LS	LS		
611	12337	2	EACH	WORK ZONE IMPACT ATTENUATOR UNIDIRECTIONAL, AS PER PLAN	2			4
622	41001	1509	FT	PORTABLE BARRIER, 32", AS PER PLAN	1,509			4
646	10010	0.52	MI	EDGE LINE, 6"	0.29	0.23		
646	10110	0.27	MI	LANE LINE, 6"	0.15	0.12		
856	10000	230	CU YD	BRIDGE DECK WATERPROOFING ASPHALT CONCRETE	148	82		

EXISTING STRUCTURE

TYPE: 6 SPAN CONTINUOUS STEEL GIRDER WITH NON-COMPOSITE REINFORCED CONCRETE DECK AND SUBSTRUCTURE ON FRICTION PILES
 SPANS: 72.00'±, 118.00'±, 84.00'±, 84.00'±, 95.00'±, 82.00'± c/c BEARINGS
 ROADWAY: 58'-0", MIN., VARIES F/F SAFETY CURB
 LOADING: HS-20-33 & INTER. ALTERNATE
 SKEW: 5°45" RT. FWD TO REFERENCE CHORD
 APPROACH SLABS: AS-1-67 (25' LONG)
 ALIGNMENT: VARIES
 STRUCTURAL FILE NUMBER: 2504766
 DATE BUILT: 1975
 DISPOSITION: TO BE RESURFACED

DESIGN AGENCY
ms consultants, inc.
 2221 Schrock Road
 Columbus, Ohio 43229

DATE
 SEP 2019
 REVIEWED
 GLG
 STRUCTURE FILE NUMBER
 2504766

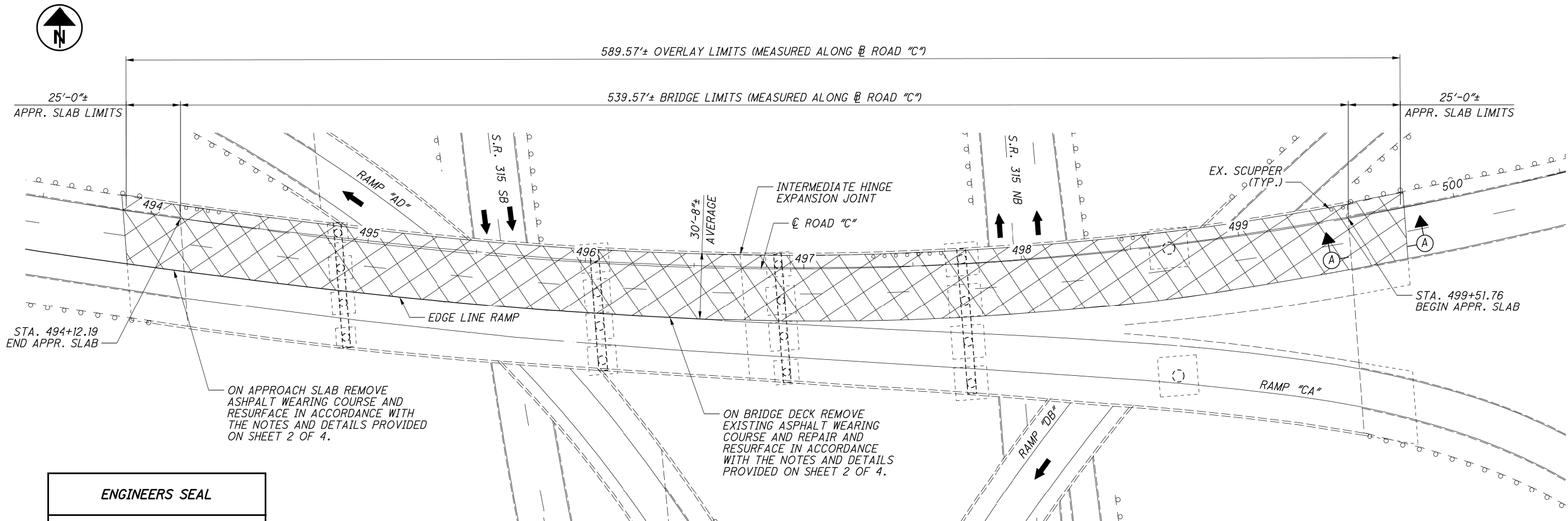
DRAWN
 CDH
 CHECKED
 ELP

FRANKLIN COUNTY
 STA. 494+12.19
 STA. 499+51.76

SITE PLAN
 BRIDGE NO. FRA-70-1301R
 EASTBOUND I-70 OVER S.R.315

FRA-270/670 SP
 FY20 PRIORITY
 PID No. 91903

1 / 4
 18A
 18



PLAN VIEW
 FRA-70-1301R

LEGEND:

☒ LIMITS OF ASPHALT REMOVAL AND REPLACEMENT

ENGINEERS SEAL

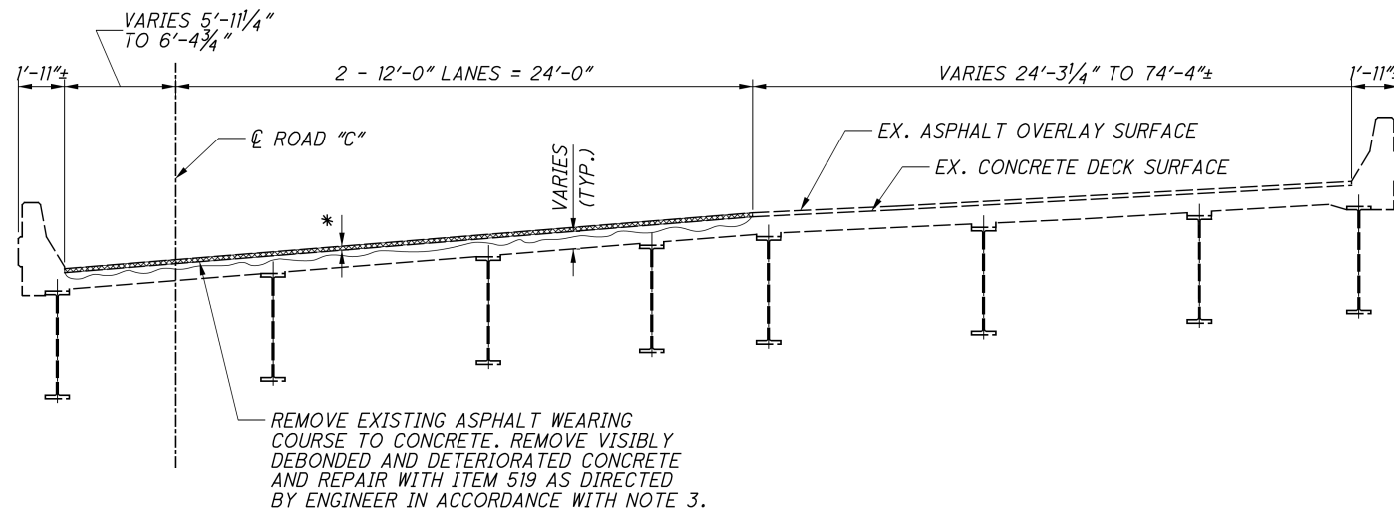


SIGNED: *Gary L. Gardner, Jr.*
 DATE:

PROPOSED STRUCTURE

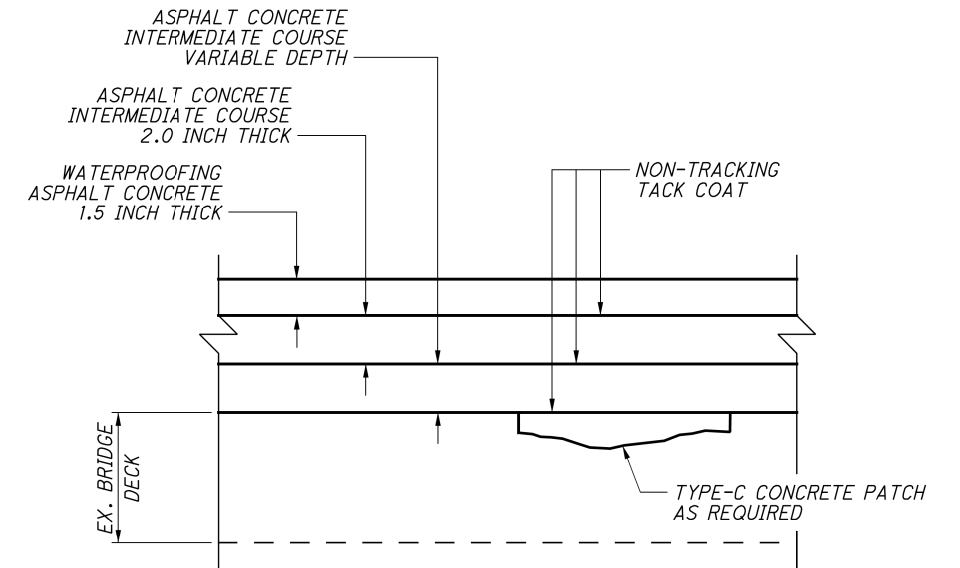
PROPOSED WORK: REMOVAL OF EXISTING ASPHALT OVERLAY AND INSTALLATION OF NEW ASPHALT OVERLAY
 COORDINATES: LATITUDE 39°57'01"
 LONGITUDE 83°01'03"

I:\ProjectData\91903_SP_FY20\Design\Roadway\Sheets\91903_SPOOL.dgn 91903_SPOOL 4/3/2020 7:51:41 AM GDENNIS

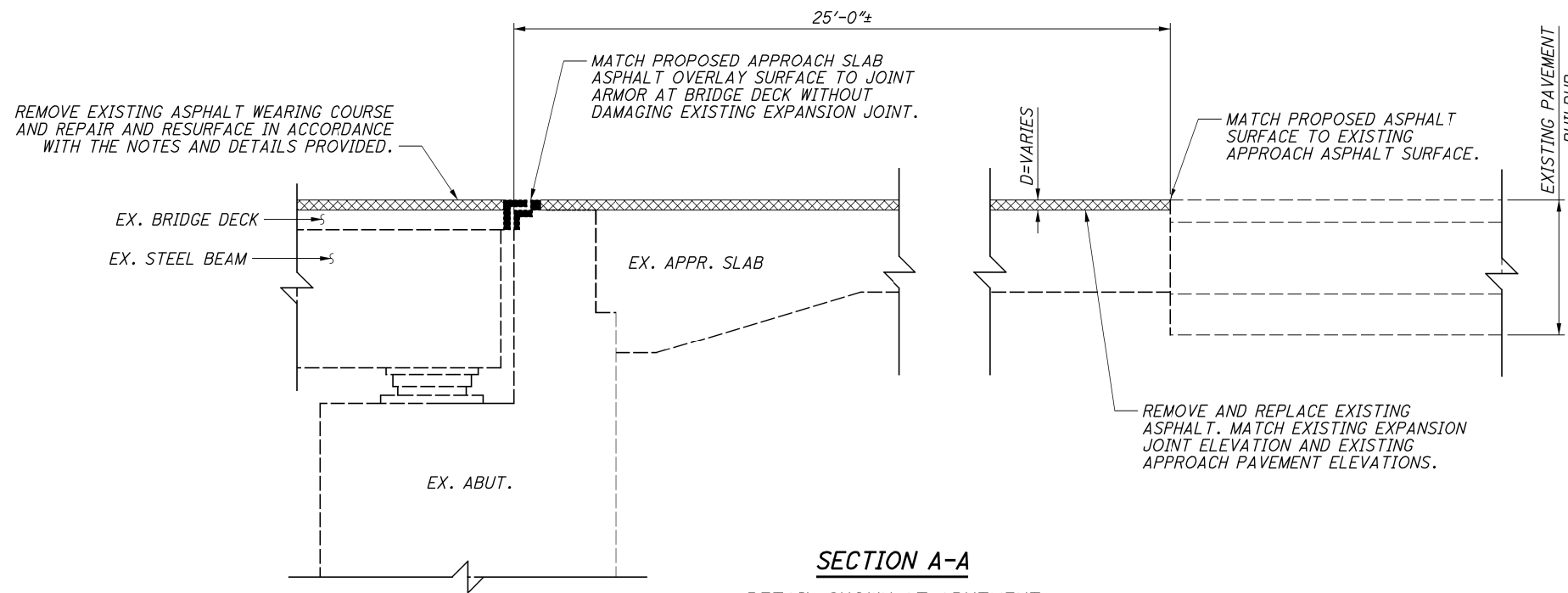


TYPICAL TRANSVERSE SECTION

FRA-70-1301R



TYPICAL PAVEMENT DETAIL



SECTION A-A

DETAIL SHOWN AT ABUTMENT EXPANSION JOINT. DETAIL AT INTERMEDIATE HINGE EXPANSION JOINT SIMILAR.

NOTES:

- PERFORM RESURFACING WORK MOT IN ACCORDANCE WITH THE MAINTENANCE OF TRAFFIC NOTES ON SHEETS 7 THROUGH 10, 10A. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING TRAFFIC UNDER AND ADJACENT TO THE WORK ON THE BRIDGE WHILE REMOVING DECK CONCRETE. USE RAMP CLOSURES OR PROTECTIVE FALSE DECKING TO PREVENT THE POSSIBILITY OF DEBRIS FALLING ON TRAFFIC.
- PAVEMENT WEDGING AND MOT SIGNAGE SHALL BE INCIDENTAL TO ITEM 614 - MAINTAINING TRAFFIC.
- ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE C, SHALL BE IN ACCORDANCE WITH CMS 256.08, TYPE C MATERIAL, EXCEPT AS NOTED. AFTER REMOVING EXISTING ASPHALT OVERLAY FROM THE BRIDGE DECK, REMOVE ALL VISIBLE LOOSE AND SPALLED CONCRETE FROM THE BRIDGE DECK USING 35 POUND MAXIMUM HAMMER SIZE. DO NOT SOUND THE DECK FOR DELAMINATION. USE TYPE C CONCRETE REPAIR MATERIAL ONLY WHERE EXISTING DECK REINFORCEMENT IS VISIBLE. SURFACE SPALLS THAT DO NOT HAVE VISIBLE REINFORCING MAY BE FILLED WITH VARIABLE DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE. WHERE REINFORCING IS ENCOUNTERED, PROVIDE 1/2" CLEARANCE AROUND THE REINFORCING BUT DO NOT REMOVE CONCRETE TO MORE THAN 1" CLEAR. (IT IS NOT THE DEPARTMENT'S INTENT TO REMOVE ALL UNSOUND CONCRETE OR PERFORM FULL-DEPTH REPAIRS ON THIS BRIDGE) TYPE C REPAIR MATERIAL SHALL BE EITHER OF THE FOLLOWING MATERIALS: CHEMSPEED 65 BY CHEMMASTERS/MADISON; OR RAPID ROAD REPAIR BY QUIKRETE/GA.
- THE THICKNESS OF THE VARIABLE DEPTH ASPHALT LAYER IS ASSUMED TO BE 2" FOR ESTIMATING PURPOSES.
- ITEM 519 - SPECIAL - PATCHING CONCRETE BRIDGE DECK, FULL DEPTH, AS PER PLAN. ALTHOUGH IT IS THE DEPARTMENT'S INTENT TO TRY TO AVOID FULL-DEPTH DECK REPAIRS, A CONTINGENCY QUANTITY HAS BEEN PROVIDED FOR EACH BRIDGE IN THE EVENT THAT THE NEED FOR FULL-DEPTH REPAIRS IS IDENTIFIED DURING BRIDGE DECK PREPARATION FOR THE NEW ASPHALT OVERLAY. FULL DEPTH REPAIRS SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER. THIS WORK SHALL CONSIST OF FURNISHING THE NECESSARY LABOR, MATERIALS, AND EQUIPMENT TO PERFORM FULL-DEPTH BRIDGE DECK REPAIRS INCLUDING REMOVING LOOSE AND UNSOUND CONCRETE, FORMING AND PLACING CONCRETE FOR FULL-DEPTH REPAIRS. PERFORM FULL DEPTH REPAIRS IN ACCORDANCE WITH ODOT SUPPLEMENTAL SPECIFICATION 848, DATED JANUARY 20, 2017, SECTION 848.23 EXCEPT THAT THE ENGINEER SHALL BE RESPONSIBLE FOR IDENTIFYING ALL FULL-DEPTH REPAIR AREAS. CONCRETE FOR FULL-DEPTH REPAIRS MAY BE CLASS QC2 CONCRETE OR MAY BE THE SAME MATERIAL AS THE PARTIAL-DEPTH TYPE C BRIDGE DECK PATCHES.

6. BE COMPLETED BY SEPTEMBER 1, 2020. INSTALLATION OF PRECAST BARRIER IS TO

EXISTING STRUCTURE VERIFICATION:

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02, AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

LEGEND:

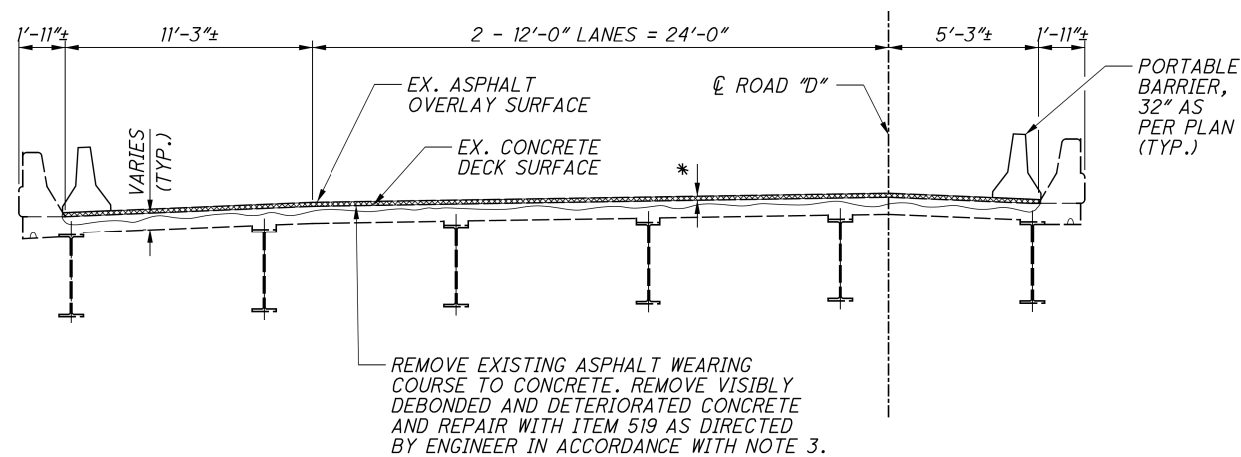
LIMITS OF ASPHALT REMOVAL AND REPLACEMENT

* EXISTING ASPHALT OVERLAY THICKNESS VARIES 3 1/2" TO 5 1/2" RESURFACE IN ACCORDANCE WITH TYPICAL PAVEMENT DETAIL.

I:\ProjectData\91903_006_SP_FY20\Design\Roadway\Sheet\91903_SPO01.dgn 91903_SPO02 4/3/2020 7:52:26 AM GDENNIS

DESIGNED ATM	DRAWN CDH	REVIEWED GLG	DATE SEP 2019	DESIGN AGENCY ms consultants, inc. 2221 Schrock Road Columbus, Ohio 43229
CHECKED ELP	REVISED	STRUCTURE FILE NUMBER 2504766		
GENERAL NOTES AND DETAILS				
FRA-70-1301R EASTBOUND I-70 OVER S.R.315				
FRA-270/670 SP FY20 PRIORITY PID No. 91903				
2 / 4				
18B 18				

I:\ProjectData\91903_06.SP_FY20\Design\Roadway\Sheets\91903.SP001.dgn 91903.SP004 4/3/2020 7:54:06 AM GDENNIS



TYPICAL TRANSVERSE SECTION

FRA-70-1301L

EXISTING STRUCTURE VERIFICATION:

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02, AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

LEGEND:

LIMITS OF ASPHALT REMOVAL AND REPLACEMENT

* EXISTING ASPHALT OVERLAY THICKNESS VARIES 3" TO 5 1/2" RESURFACE IN ACCORDANCE WITH TYPICAL PAVEMENT DETAIL.

NOTES:

- PERFORM RESURFACING WORK NOT IN ACCORDANCE WITH THE MAINTENANCE OF TRAFFIC NOTES ON SHEETS 7 THROUGH 10, 10A THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING TRAFFIC UNDER AND ADJACENT TO THE WORK ON THE BRIDGE WHILE REMOVING DECK CONCRETE. USE RAMP CLOSURES OR PROTECTIVE FALSE DECKING TO PREVENT THE POSSIBILITY OF DEBRIS FALLING ON TRAFFIC.
- PAVEMENT WEDGING AND MOT SIGNAGE SHALL BE INCIDENTAL TO ITEM 614 - MAINTAINING TRAFFIC.
- ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE C, SHALL BE IN ACCORDANCE WITH CMS 256.08, TYPE C MATERIAL, EXCEPT AS NOTED. AFTER REMOVING EXISTING ASPHALT OVERLAY FROM THE BRIDGE DECK, REMOVE ALL VISIBLE LOOSE AND SPALLED CONCRETE FROM THE BRIDGE DECK USING 35 POUND MAXIMUM HAMMER SIZE. DO NOT SOUND THE DECK FOR DELAMINATION. USE TYPE C CONCRETE REPAIR MATERIAL ONLY WHERE EXISTING DECK REINFORCEMENT IS VISIBLE. SURFACE SPALLS THAT DO NOT HAVE VISIBLE REINFORCING MAY BE FILLED WITH VARIABLE DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE. WHERE REINFORCING IS ENCOUNTERED, PROVIDE 1/2" CLEARANCE AROUND THE REINFORCING BUT DO NOT REMOVE CONCRETE TO MORE THAN 1" CLEAR. (IT IS NOT THE DEPARTMENT'S INTENT TO REMOVE ALL UNSOUND CONCRETE OR PERFORM FULL-DEPTH REPAIRS ON THIS BRIDGE) TYPE C REPAIR MATERIAL SHALL BE EITHER OF THE FOLLOWING MATERIALS: CHEMSPEED 65 BY CHEMMASTERS/MADISON; OR RAPID ROAD REPAIR BY QUIKRETE/GA.
- THE THICKNESS OF THE VARIABLE DEPTH ASPHALT LAYER IS ASSUMED TO BE 2" FOR ESTIMATING PURPOSES.
- ITEM 519 - SPECIAL - PATCHING CONCRETE BRIDGE DECK, FULL DEPTH, AS PER PLAN ALTHOUGH IT IS THE DEPARTMENT'S INTENT TO TRY TO AVOID FULL-DEPTH DECK REPAIRS, A CONTINGENCY QUANTITY HAS BEEN PROVIDED FOR EACH BRIDGE IN THE EVENT THAT THE NEED FOR FULL-DEPTH REPAIRS IS IDENTIFIED DURING BRIDGE DECK PREPARATION FOR THE NEW ASPHALT OVERLAY. FULL DEPTH REPAIRS SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER. THIS WORK SHALL CONSIST OF FURNISHING THE NECESSARY LABOR, MATERIALS, AND EQUIPMENT TO PERFORM FULL-DEPTH BRIDGE DECK REPAIRS INCLUDING REMOVING LOOSE AND UNSOUND CONCRETE, FORMING AND PLACING CONCRETE FOR FULL-DEPTH REPAIRS. PERFORM FULL DEPTH REPAIRS IN ACCORDANCE WITH ODOT SUPPLEMENTAL SPECIFICATION 848, DATED JANUARY 20, 2017, SECTION 848.23 EXCEPT THAT THE ENGINEER SHALL BE RESPONSIBLE FOR IDENTIFYING ALL FULL-DEPTH REPAIR AREAS. CONCRETE FOR FULL-DEPTH REPAIRS MAY BE CLASS QC2 CONCRETE OR MAY BE THE SAME MATERIAL AS THE PARTIAL-DEPTH TYPE C BRIDGE DECK PATCHES.
- PORTABLE BARRIER, 32" AS PER PLAN. PORTABLE BARRIER IS TO BE LEFT IN PLACE. DO NOT REMOVE.
- ITEM 614-WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL), AS PER PLAN THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING TYPE IMPACT ATTENUATOR FOR A 24" WIDE HAZARD. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS AND IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS. PLACE THE WORK ZONE IMPACT ATTENUATOR AS CLOSE AS IS PRACTICAL TO THE EXISTING BARRIER AND GUARDRAIL TO MAXIMIZE AVAILABLE SHOULDER WIDTH. THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT. DO NOT USE GATING IMPACT ATTENUATORS. PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE WORK ZONE IMPACT ATTENUATOR UNTIL SUBSTANTIAL COMPLETION OF THE PROJECT. AFTER SUBSTANTIAL COMPLETION THE WORK ZONE IMPACT ATTENUATOR IS TO BE LEFT IN PLACE AND MAINTENANCE RESPONSIBILITY WILL BE ASSUMED BY ODOT.
- INSTALLATION OF PRECAST BARRIER IS TO BE COMPLETED BY SEPTEMBER 1, 2020.
- FOR SECTION A-A AND TYPICAL PAVEMENT DETAIL. REFER TO SHEET 274.

DESIGN AGENCY		ms consultants, inc.		2221 Schrock Road Columbus, Ohio 43229	
DATE	MM/DD/YY	REVIEWED	XXX	STRUCTURE FILE NUMBER	2504677
DRAWN	CDH	CHECKED	ELP	DESIGNED	ATM
GENERAL NOTES AND DETAILS					
FRA-70-1301R					
WESTBOUND I-70 OVER S.R.315					
FRA-270/670 SP					
FY20 PRIORITY					
PID No. 91903					
4 / 4					
18D					
18					