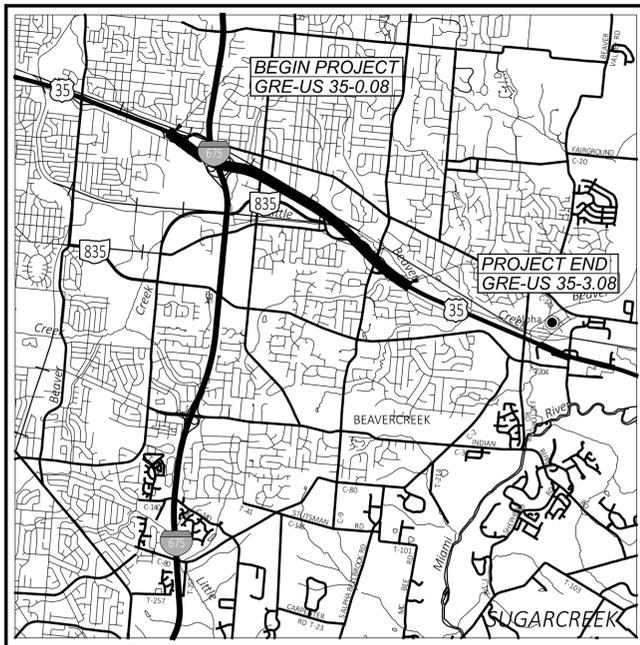


STATE OF OHIO DEPARTMENT OF TRANSPORTATION **GRE US 35 0.08 PART 1**

CITY OF BEAVERCREEK
BEAVERCREEK TOWNSHIP
GREENE COUNTY

FOR PART 2, SEE GRE US 35/835 2.65/2.47



LOCATION MAP

LATITUDE: 39°43'04" LONGITUDE: -84°03'17"



PORTION TO BE IMPROVED	—————	=====
INTERSTATE HIGHWAY	—————	=====
FEDERAL ROUTES	—————	=====
STATE ROUTES	—————	=====
COUNTY & TOWNSHIP ROADS	—————	=====
OTHER ROADS	—————	=====

DESIGN DESIGNATION - SEE SHEET 2

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FEDERAL PROJECT NUMBER

E170(714)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

REHABILITATION OF PAVEMENT ON A PORTION OF US 35 IN GREENE COUNTY. PROJECT INCLUDES RESURFACING OF GRE-35-0.08 TO 3.08, REPLACEMENT OF CONCRETE PAVEMENT ON WESTBOUND LANES FROM GRE-35-0.13 TO 1.06, BRIDGE REPAIRS TO BE COMPLETED AT VARIOUS LOCATIONS ON GRE-35, AND A MEDIAN INLET TO BE ADDED AT GRE-35-1.91.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 4.24 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.1 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 4.34 ACRES*
*ROUTINE MAINTENANCE PROJECT

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT AS NOTED ON SHEET 13, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Douglas A. Gruver, P.E.
District 08 Deputy Director

Pamela Boratyn
Director, Department of Transportation

DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE

UNDERGROUND UTILITIES
Contact Two Working Days
Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 8 ENGINEERING
505 S. STATE ROUTE 741 &
LEBANON, OH 45036



STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-3.1	1/19/24	MT-99.20	4/19/19	TC-71.10	4/21/23	800-2023	1/17/25	ASBESTOS	
BP-9.1	1/18/19	MT-99.30	1/17/20	TC-72.20	7/21/23	807	1/17/25	INSPECTION	
		MT-99.60	7/19/24	TC-73.20	1/17/25	809	1/17/25	REPORT	
RM-4.2	4/17/20	MT-101.60	1/17/25	TC-74.10	7/21/23	832	7/19/24	10/22/04	
		MT-101.70	7/19/24	TC-82.10	7/19/19	840	1/17/25		
MT-95.30	7/19/19	MT-101.75	7/21/23			842	7/15/11		
MT-95.40	7/21/23	MT-102.20	4/19/19	AS-1-15	1/20/23	843	1/19/24		
MT-95.45	7/21/23	MT-102.30	10/16/15	AS-2-15	7/21/23	844	1/17/25		
MT-95.50	7/21/17	MT-103.10	1/21/22	CPA-1-08	1/19/24	846	4/17/15		
MT-95.70	7/21/23	MT-104.10	1/19/24	CPP-1-08	7/21/17	847	7/19/24		
MT-98.10	1/17/20	MT-105.10	1/17/20	CS-1-24	7/19/24	850	7/21/23		
MT-98.11	1/17/20			DS-1-92	7/15/22	878	1/21/22		
MT-98.20	4/19/19	TC-16.22	7/21/23	TST-2-21	1/17/25	896	7/21/17		
MT-98.21	7/21/23	TC-21.21	7/15/22						
MT-98.22	1/17/20	TC-65.10	1/17/14	I-3C	1/17/25				
MT-98.29	1/17/20	TC-65.11	1/17/25	DM-1.1	1/17/25				

ENGINEER'S SEAL ROADWAY	ENGINEER'S SEAL BRIDGE	ENGINEER'S SEAL MAINTENANCE OF TRAFFIC

TITLE SHEET

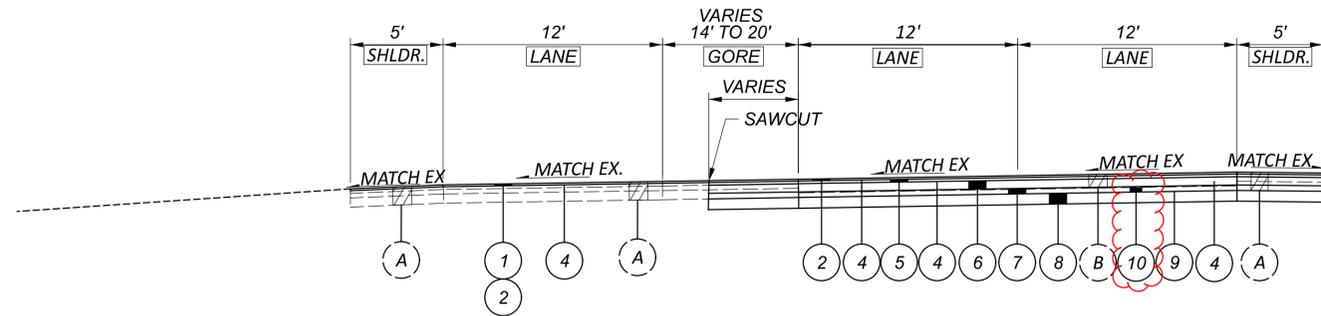
DESIGN AGENCY



DESIGNER
GAT
REVIEWER
XXX MM-DD-YY
PROJECT ID
96680
SHEET TOTAL
P.1 | 171

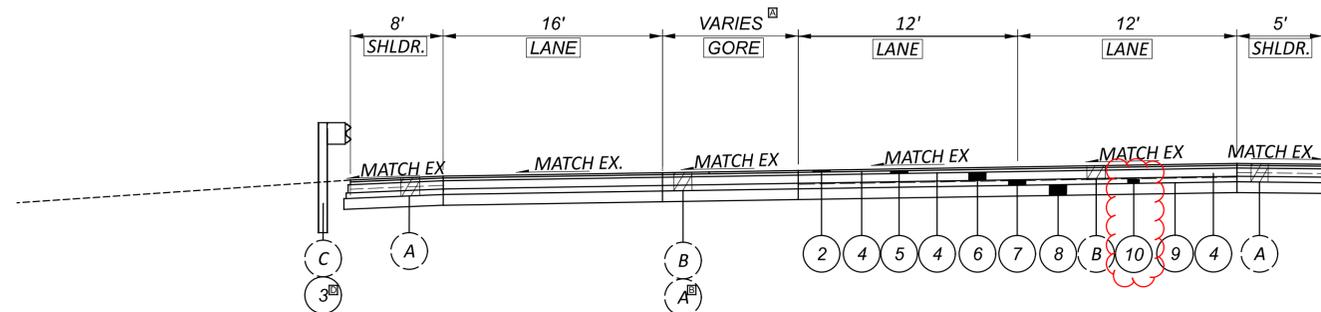
GRE US 35 0.08

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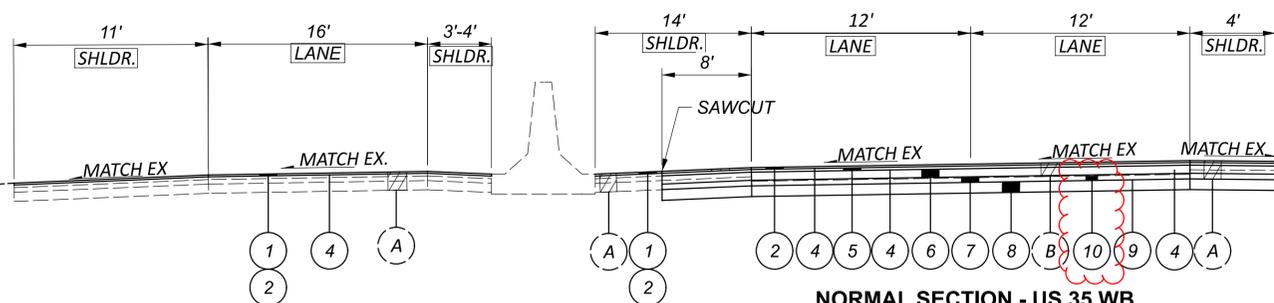
NORMAL SECTION - US 35 WB

STA. 6+92.04 TO STA. 12+72.23
 STA. 16+58.60 TO STA. 19+68.50



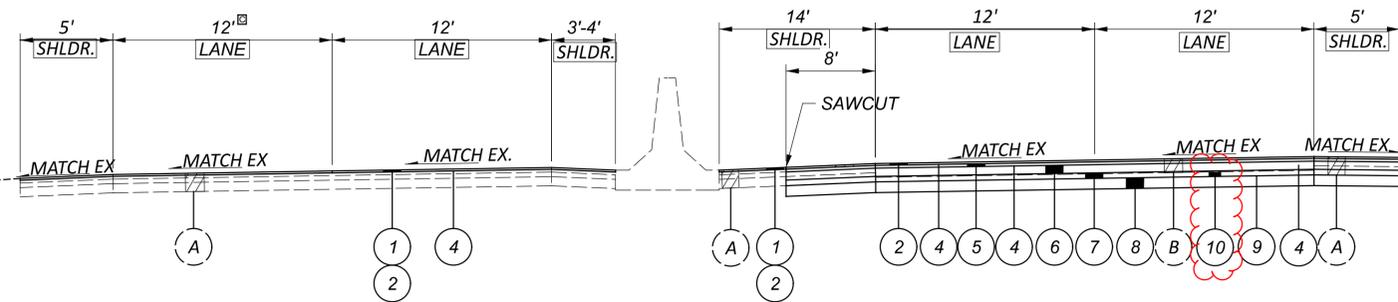
NORMAL SECTION - US 35 WB

STA. 12+72.23 TO STA. 16+58.60
 STA. 51+78.49 TO STA. 55+86.23



NORMAL SECTION - US 35 WB

STA. 19+68.50 TO STA. 21+05.99
 STA. 33+64.51 TO STA. 44+04.48
 STA. 50+00.04 TO STA. 51+78.49



NORMAL SECTION - US 35 WB

STA. 21+05.99 TO STA. 27+64.75
 STA. 32+65.53 TO STA. 33+64.51
 STA. 44+04.48 TO STA. 50+00.04

CONST. US 35

CONST. US 35

CONST. US 35

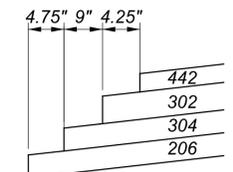
CONST. US 35

LEGEND

- 1 ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (1.5" THICK)
- 2 ITEM 442 - 1-1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447)
- 3 GUARDRAIL, TYPE MGS
- 4 ITEM 407 - NON-TRACKING TACK COAT
- 5 ITEM 442 - 2-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446)
- 6 ITEM 302 - 9" ASPHALT CONCRETE BASE, PG64-22, (449) (IN TWO LIFTS)
- 7 ITEM 304 - 6" AGGREGATE BASE
- 8 ITEM 206 - CEMENT STABILIZED SUBGRADE, 14 INCHES
- 9 ITEM 204 - PROOF ROLLING
- 10 ITEM 204 - EXCAVATION OF SUBGRADE
- A EXISTING ASPHALT PAVEMENT (DEPTH 13")
- B EXISTING CONCRETE PAVEMENT (DEPTH 9")
- C EXISTING GUARDRAIL

☐ - GORE VARIES 6' TO 9' FROM STA. 15+32.06 TO STA. 19+68.50
 GORE VARIES 19' TO 0' FROM STA. 51+80.97 TO STA. 54+70.74
 ☐ - STA. 16+57.23 TO STA. 19+68.50
 ☐ - LANE WIDTH VARIES FROM 16' TO 0' FROM STA. 45+55.86 TO STA. 50+00.04
 ☐ - FROM STA. 51+79.86 TO STA. 55+86.23

NOTE:
 WESTBOUND PAVEMENT REPLACEMENT SAWCUT LINE NEAR THE MEDIAN BARRIER WILL NOT IMPACT THE EXISTING MEDIAN DRAINAGE OR CATCH BASINS.



STEP DETAIL
 NOT TO SCALE

ITEM 614, MAINTAINING TRAFFIC

MAINTAIN ALL EXISTING LANES OF TRAFFIC IN EACH DIRECTION AT ALL TIMES, EXCEPT LANE CLOSURES ARE PERMITTED ACCORDING TO THE LANE VALUE CONTRACT TABLE, BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, AND TEMPORARY SURFACES USING ITEMS 614 AND 615.

ALL EXISTING LANES, INCLUDING RAMPS, SHALL BE OPEN AND AVAILABLE TO TRAFFIC IN THE ORIGINAL OR PROPOSED FINAL ALIGNMENT BETWEEN NOVEMBER 1 AND APRIL 1. ANY PORTION OF PAVEMENT WORK INCLUDING FULL DEPTH REPLACEMENT AND RESURFACING STARTED IN 2025 SHALL BE COMPLETED THROUGH THE FINAL SURFACE COURSE WITH PERMANENT RPMs INSTALLED FOR THE ENTIRE WIDTH OF PAVEMENT IN THAT DIRECTION. SHOULD THE CONTRACTOR FAIL TO MEET THESE REQUIREMENTS, A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$5,300 PER CALENDAR DAY.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS; NOT INCLUDING THE WESTBOUND US 35 LONG-TERM LANE CLOSURE:

NEW YEAR'S (OBSERVED)	GENERAL/REGULAR ELECTION DAY (NOV)
	THANKSGIVING
MEMORIAL DAY	CHRISTMAS (OBSERVED)
FOURTH OF JULY (OBSERVED)	ANKENEY SOCCER TOURNAMENTS
LABOR DAY	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY TIME ALL LANES
OR SPECIAL EVENT MUST BE OPEN TO TRAFFIC

SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
MONDAY	(TOTAL SOLAR ECLIPSE)
	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY	(GEN./REG. ELECTION)
	5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY	(THANKSGIVING ONLY)
	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

LANE VALUE CONTRACT TABLE			
DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT PER LANE
US 35: ALL LANES OPEN TO TRAFFIC	SEE PLCS	1 MINUTE	\$330
WB US 35: ONE LANE OPEN TO TRAFFIC	90 DAYS	1 DAY	\$25,200
EB US 35: ONE LANE OPEN TO TRAFFIC ON A WEEKEND	MONDAY 6 AM TO FRIDAY 8 PM	1 MINUTE	\$115
LOCAL RAMPS	5 AM TO 10 PM	1 MINUTE	\$25
I-675/US 35 RAMPS	5 AM TO 10 PM	1 MINUTE	\$405

NOTES:
1. IN PHASE 1 AND IN PHASE 2A, US 35 MAY BE DIVERTED INTO THE CD ROAD DURING PLCS PERMITTED LANE CLOSURE TIMES. 2 PCMS SHALL BE USED TO WARN TRAFFIC DURING THIS TRAFFIC CONTROL OPERATION.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP &	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
ROAD	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
CLOSURES	<= 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.



W20-H13-60

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

FULL DEPTH PAVEMENT REPLACEMENT: WORK ZONE PAVEMENT MARKINGS LOCATED ON AND WEST OF THE DAYTON-XENIA ROAD OVERPASS SHALL USE ITEM 740.06 TYPE I REMOVABLE TAPE AND CONFLICTING MARKINGS SHALL BE COVERED USING REMOVABLE, NON-REFLECTIVE, PRE-FORMED BLACK OUT TAPE PER 614.11.G1B. WORK ZONE PAVEMENT MARKINGS LOCATED EAST OF DAYTON-XENIA ROAD OVERPASS SHALL USE MAY USE ITEM 807 PAINT.

SEE THE MAINTENANCE OF TRAFFIC SUBSUMMARIES FOR QUANTITIES

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC, CLASS A 25 CU. YD.
ITEM 616, WATER 20 M. GAL.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH RAMP CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE FOLLOWING LOCATIONS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

RAMP D AT GORE

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS FOLLOWS:

W20-1-48	ROAD WORK AHEAD	US 35 EB
W20-1-48	ROAD WORK AHEAD	RAMP E
W20-1-48	ROAD WORK AHEAD	RAMP H
W20-1-48	ROAD WORK AHEAD	RAMP G
W20-1-48	ROAD WORK AHEAD	RAMP FROM N FAIRFIELD RD TO US 35 EB
W20-1-48	ROAD WORK AHEAD	US 35 WB
W20-1-48	ROAD WORK AHEAD	RAMP FROM N FAIRFIELD RD TO US 35 WB
W20-1-48	ROAD WORK AHEAD	RAMP D
W20-1-48	ROAD WORK AHEAD	RAMP B
G20-2-48	END ROAD WORK	RAMP A
G20-2-48	END ROAD WORK	RAMP FROM US 35 EB TO RESEARCH BLVD
G20-2-48	END ROAD WORK	RAMP FROM US 35 EB TO N FAIRFIELD RD
G20-2-48	END ROAD WORK	US 35 EB
G20-2-48	END ROAD WORK	RAMP FROM US 35 WB TO N FAIRFIELD RD
G20-2-48	END ROAD WORK	RAMP F
G20-2-48	END ROAD WORK	RAMP G
G20-2-48	END ROAD WORK	US 35 WB

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TRENCH FOR WIDENING

THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

WORK ZONE INCREASED PENALTIES SIGN (R11-H5A) (CONT'D)

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 4 EACH WORK ZONE INCREASED PENALTIES SIGNS WILL BE PLACED AT THE FOLLOWING LOCATIONS:

EAST OF NORTH FAIRFIELD ROAD PER MT-120.30 EB WEST OF NORTH FAIRFIELD ROAD APPROXIMATELY STA. 404+00 EB EAST OF I-675 INTERCHANGE APPROXIMATELY STA. 80+00 EB ON RAMP D APPROXIMATELY STA. 346+00

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS UNIDIRECTIONAL

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTIONS PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTIONS INCLUDE:

1. CLOSE 1 LANE OF WESTBOUND US 35 FOR 90 CONSECUTIVE DAYS TO PERFORM PART-WIDTH PAVEMENT REPLACEMENT IN 2 PHASES (45 DAYS PER PHASE).
2. CLOSE RAMPS AT THE I-675/US 35 INTERCHANGE OVERNIGHT FROM 10 PM TO 5 AM TO PERFORM DECK PATCHING, RESURFACING WITHIN THE RAMP MERGE AREA, AND MOT TRAFFIC SWITCHES. EXCEPT FOR THE NORTHBOUND I-675 TO WESTBOUND US 35 AND THE WESTBOUND US 35 TO SOUTHBOUND I-675 RAMPS, ONLY 1 RAMP IS PERMITTED TO BE CLOSED AT A TIME.
3. CLOSE 1 LANE OF EASTBOUND US 35 FOR A WEEKEND, FROM FRIDAY NIGHT AT 9 PM TO MONDAY MORNING AT 6 AM, TO COMPLETE THE DRAINAGE CROSSING.

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AND CITY OF BEAVERCREEK AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTIONS REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTIONS REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED []/[]/[]" FOR PID 96680" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTIONS LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

EXTRA ADVANCE WARNING SIGNS

AN EXTRA ADVANCE WARNING SIGN GROUP CONSISTS OF TWO W20-1 (ROAD WORK AHEAD) SIGNS, TWO W20-5 (RIGHT/LEFT LANE CLOSED AHEAD) SIGNS WITH W16-3A DISTANCE PLATES, AND TWO W3-H7 (WATCH FOR STOPPED TRAFFIC) SIGNS AND REQUIRED WARNING LIGHTS.

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND REMOVE EXTRA ADVANCE WARNING SIGN GROUPS AS SHOWN ON TRAFFIC SCD MT-95.50 AT THE FOLLOWING DISTANCES IN ADVANCE OF THE LANE TAPERS WITH THE APPROPRIATE W16-3A DISTANCE PLATES:

- 1) EAST END OF PROJECT, ALL PHASES; PROVIDE SIGN GROUPS AT 2 MILES AND 4 MILES FOR THE WESTBOUND LANE CLOSURES.
- 2) LANE REDUCTION, STATION 75+40, EASTBOUND WEEKEND LANE CLOSURE; PROVIDE SIGN GROUPS AT 2 MILES AND 4 MILES FOR THE EASTBOUND LANE CLOSURE.

THE CONTRACTOR SHALL HAVE AN ADDITIONAL EXTRA ADVANCE WARNING SIGN GROUP (6 SIGNS AND 2 DISTANCE PLATES) AVAILABLE FOR USE WHEN DIRECTED BY THE ENGINEER. THE DISTANCE PLATES FOR THIS GROUP SHALL BE ABLE TO BE MODIFIED IN THE FIELD TO SHOW APPROPRIATE WHOLE MILES TO THE LANE TAPER.

PAYMENT FOR PROVIDING, ERECTING, MAINTAINING AND REMOVING EXTRA ADVANCE WARNING SIGN GROUPS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET P.13 OF THE PLAN AND AT APPROXIMATELY STA. 80+00. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 2 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT. THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 6 SIGN MONTH ASSUMING 2 PCMS SIGNS FOR 10 MONTHS



ITEM 614, WORK ZONE RAISED PAVEMENT MARKERS ON PERMANENT CONCRETE SURFACES

RAISED PAVEMENT MARKERS IN WORK ZONES, INSTALLED ON PERMANENT CONCRETE SURFACES, SHALL BE ITEM 614 WORK ZONE RAISED PAVEMENT MARKERS. WZRPMS ARE INTENDED FOR USE ONLY DURING THE NON-SNOW-PLOWING SEASON. WZRPMS SHALL NOT BE PROVIDED DURING THE SNOW-PLOWING SEASON.

THE SNOW-PLOWING SEASON SHALL RUN FROM NOVEMBER 15 THROUGH MARCH 1.

WHERE A TEMPORARY ALIGNMENT WILL REMAIN IN USE THROUGH THE WINTER, THE WZRPMS SHALL BE REMOVED PRIOR TO THE BEGINNING OF THE SNOW-PLOWING SEASON AND REPLACED APPROXIMATELY APRIL 1, OR AS OTHERWISE DETERMINED BY THE ENGINEER.

THIS ITEM SHALL INCLUDE PURCHASE, INSTALLATION AND REMOVAL OF ITEM 614 WORK ZONE RAISED PAVEMENT MARKERS.

AN ESTIMATED QUANTITY OF 250 EACH OF ITEM 614 WORK ZONE RAISED PAVEMENT MARKER HAS BEEN PROVIDED IN THE MAINTENANCE OF TRAFFIC SUBSUMMARY.

ITEM 614, WORK ZONE IMPACT ATTENUATOR, MISC.: 48" WIDE ATTENUATOR (UNIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS. THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM 614, MAINTAINING TRAFFIC, MISC.: 24" WHITE CHEVRON

PROVIDE A WHITE CHEVRON WITH THE SAME REQUIREMENTS AS 614 WORK ZONE STOP LINE, CLASS I. PAYMENT WILL BE MADE PER FOOT OF CHEVRON LINE APPLIED AND REMOVED PER ITEM 614.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB OR PERMANENT BARRIER (INCLUDING BRIDGE PARAPETS) CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND PERMANENT CONCRETE BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE UNDER EITHER OF THE FOLLOWING CONDITIONS: ALONG TAPERS AND TRANSITION AREAS; OR ALONG CURVES (OUTSIDE ONLY) WITH DEGREE OF CURVATURE GREATER THAN OR EQUAL TO 3 DEGREES.

THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.

DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT-101.70.

TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT-101.70.

ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE MAINTENANCE OF TRAFFIC SUBSUMMARIES.

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:

- ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
- AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
- AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

- THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
- THE ACTIVE WORK AREA LATERALLY CLOSEST TO THE OPEN TRAVELED LANE; OR
- OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 500 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.



NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

PATCHING RUMBLE STRIPS

THE CONTRACTOR SHALL MILL THE EXISTING RUMBLE STRIPS A WIDTH OF 3 FEET AT 1 1/2 INCH DEPTH AND PAVE WITH 1 1/2 INCH ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1. PAYMENT FOR ALL MATERIALS, LABOR AND EQUIPMENT SHALL BE INCLUDED FOR PAYMENT PER FOOT UNDER ITEM 614 MAINTAINING TRAFFIC, MISC.: RUMBLE STRIP MILLED/FILLED.

TRANSPORTATION MANAGEMENT PLAN DESIGNATED TRAINED PERSON

ENSURE ALL INDIVIDUALS CONTRACTED BY, SECURED BY, DIRECTED BY OR EMPLOYED BY THE CONTRACTOR WHOM ARE INVOLVED IN THE DEVELOPMENT, DESIGN, IMPLEMENTATION, OPERATION, INSPECTION AND ENFORCEMENT OF WORK ZONE RELATED TRANSPORTATION MANAGEMENT AND TRAFFIC CONTROL HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS EACH INDIVIDUAL IS REQUIRED TO MAKE. REPEAT TRAINING IN INTERVALS OF NO MORE THAN 5 YEARS TO REFLECT CHANGING PRACTICES. DESIGNATE A TRAINED PERSON AT THE PROJECT LEVEL THAT HAS THE PRIMARY RESPONSIBILITY AND SUFFICIENT AUTHORITY FOR IMPLEMENTING AND MAINTAINING THE TRANSPORTATION MANAGEMENT PLAN (TMP) AND OTHER SAFETY AND MOBILITY ASPECTS OF THE PROJECT. FOR INFORMATION AND REQUIREMENTS REGARDING TMPs AND RELATED COMPONENTS SEE ODOT TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)). MAINTAIN A 24-HOUR CONTACT FOR THE DESIGNATED TRAINED PERSON AND PROVIDE THIS CONTACT INFORMATION TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. THE DESIGNATED TRAINED PERSON SHALL BE PRESENT ON SITE FOR, AND INVOLVED WITH, EACH TEMPORARY TRAFFIC CONTROL SET UP/TAKE DOWN AND EACH PHASE CHANGE.

THE DUTIES OF THE DESIGNATED TRAINED PERSON ARE AS FOLLOWS:

- BE AVAILABLE ON A 24-HOUR PER DAY BASIS IN ACCORDANCE WITH CMS 614.03.
- BE AWARE OF ALL EXISTING AND PROPOSED TTC OPERATIONS OF THE CONTRACTOR, SUBCONTRACTORS AND SUPPLIERS, AND ENSURE COORDINATION OCCURS BETWEEN THEM TO ELIMINATE CONFLICTING TEMPORARY AND/OR PERMANENT TRAFFIC CONTROL.
- BE PRESENT, ON SITE FOR, AND INVOLVED WITH, EACH TEMPORARY TRAFFIC CONTROL (TTC) SET UP/TAKE DOWN AND EACH PHASE CHANGE IN ACCORDANCE WITH CMS 614.03.
- ENSURE THAT THE TTC ZONE AND ALL RELATED DEVICES ARE INSTALLED, MAINTAINED AND REMOVED IN COMPLIANCE WITH THE CONTRACT DOCUMENTS.
- FACILITATE CORRECTIVE ACTION(S) NECESSARY TO BRING DEFICIENT TTC ZONES AND ALL RELATED DEVICES INTO COMPLIANCE WITH CONTRACT DOCUMENTS IN THE TIMEFRAME DETERMINED BY THE ENGINEER.

THE DEPARTMENT WILL DEDUCT:

- THE PRORATED DAILY AMOUNT OF ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY IN WHICH THE DESIGNATED TRAINED PERSON FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. THE PRORATED DAILY AMOUNT WILL BE EQUAL TO THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC DIVIDED BY THE DIFFERENCE BETWEEN THE ORIGINAL COMPLETION DATE AND THE FIRST DAY OF WORK, IN CALENDAR DAYS.
- 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY THAT A TTC ISSUE IS IDENTIFIED IN THE FIELD AND IS NOT CORRECTED IN THE GIVEN TIMEFRAME PER THE ENGINEER. DEDUCTION B SHALL NOT APPLY TO SITUATIONS COVERED BY DEDUCTION C.
- 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY THAT A LANE OR RAMP IS BLOCKED (FULLY OR PARTIALLY) WITHOUT TTC, AS DETERMINED BY THE ENGINEER. THIS DEDUCTION SHALL BE IN ADDITION TO ANY OTHER DISINCENTIVES ESTABLISHED FOR UNAUTHORIZED LANE USE.

FOR DAYS IN WHICH MORE THAN ONE DEDUCTION LISTED ABOVE OCCUR, THE HIGHEST DEDUCTION AMOUNT WILL APPLY.

PAYMENT FOR THE ABOVE REQUIREMENTS, RESPONSIBILITIES AND DUTIES SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

SEQUENCE OF CONSTRUCTION

PHASE 1

- CLOSE THE LEFT LANE OF US 35 WESTBOUND. MAINTAIN ONE 11' LANE WITH 2' SHOULDERS, UNLESS SHOWN OTHERWISE IN THE PLANS.
- CONSTRUCT THE FULL DEPTH PAVEMENT FOR LEFT LANE AND SHOULDER TO THE TOP OF THE INTERMEDIATE COURSE. PERFORM BRIDGE REPAIRS IN THE CLOSED LANES.
- CONSTRUCT MEDIAN DRAINAGE IMPROVEMENTS AT 421+50.

- CLOSE THE LEFT OR RIGHT LANE OF US-35 EASTBOUND PER 95.30 ON A WEEKEND; SHIFT THE MAINTAINED LANE AS NEEDED PER MT-102.20. CONSTRUCT THE PIPE CROSSING AT 421+50 USING RECESSED STEEL PLATES TO MAINTAIN 1 LANE AT ALL TIMES DURING TRENCH EXCAVATION AND RESTORATION.

PHASE 2A

- CLOSE THE RIGHT LANE OF US-35 WESTBOUND. MAINTAIN ONE 11' LANE WITH 2' SHOULDERS, UNLESS SHOWN OTHERWISE IN THE PLANS.
- AT THE I-675/C-D ROAD RAMP, PLACE PORTABLE BARRIER AND WIDE BODY IMPACT ATTENUATORS PER MT-98.20 AND MT-98.21. PLACE WORK ZONE CHEVRONS IN GORE AREA PER TC-71.10.
- CONSTRUCT THE FULL DEPTH PAVEMENT FOR RIGHT LANE AND SHOULDER TO THE TOP OF THE INTERMEDIATE COURSE. PERFORM BRIDGE REPAIRS IN THE CLOSED LANES.
- FOR FUTURE PHASE 2B CROSSOVER: REMOVE A PORTION OF THE EXISTING CONCRETE BARRIER BETWEEN US-35 WESTBOUND AND THE US-35 WESTBOUND EXIT RAMP TO I-675. CONSTRUCT TEMPORARY PAVEMENT FOR TEMPORARY RAMP CONNECTION.
- AT THE I-675 ENTRANCE RAMPS TO US-35 WESTBOUND, MAINTAIN BOTH THE ENTRANCE FROM THE C-D RAMP AND FROM THE I-675 ENTRANCE RAMP.

PHASE 2B

- THE RIGHT LANE OF US -35 WESTBOUND REMAINS CLOSED. THE EXIT RAMP TO I-675 IS RELOCATED TO THE TEMPORARY RAMP CONSTRUCTED IN PHASE 2A. THE ENTRANCE RAMP FROM I-675 IS SHIFTED INTO THE GORE AREA OVER DAYTON-XENIA ROAD.
- CONSTRUCT FULL-DEPTH PAVEMENT AT THE RAMP AREAS TO THE TOP OF THE INTERMEDIATE COURSE. PERFORM BRIDGE REPAIRS IN THE CLOSED LANES.
- CONSTRUCT THE REMAINING PORTION OF THE FULL DEPTH US 35 WESTBOUND PAVEMENT TO THE TOP OF THE INTERMEDIATE COURSE. CLOSE AND DETOUR RAMP D.

PHASE 2C

- THE RIGHT LANE OF US 35 REMAINS CLOSED. THE EXIT RAMP TO I-675 IS PLACED BACK INTO PHASE 2A ALIGNMENT.
- RECONSTRUCT THE PORTIONS OF THE REMOVED EXISTING CONCRETE BARRIER BETWEEN US-35 WESTBOUND AND THE C-D ROAD.

PHASE 3

- PLACE CLASS I PAVEMENT MARKINGS AND OPEN WESTBOUND US-35 IN EXISTING CONFIGURATION.
- PERFORM ANY REMAINING PAVEMENT MILLING AND PLACE INTERMEDIATE PER MT-95.30
- PLACE THE SURFACE COURSE FOR THE ENTIRE PROJECT USING MT-95.30.
- PLACE FINAL PAVEMENT MARKINGS USING MT-99.20.

EASTBOUND US 35 LANE CLOSURE

THE EASTBOUND SHOULDER CLOSURE AND PORTABLE BARRIER IS FOR THE MEDIAN DRAINAGE CONSTRUCTION. A PORTION OF MEDIAN BARRIER WILL BE REMOVED TO INSTALL A MEDIAN CATCH BASIN. THE SHOULDER CLOSURE AND BARRIER WILL BE USED TO PROTECT THE GAP IN BARRIER AND UNTIL THE WALL IS COMPLETE. DURING PHASE 1 THE EASTBOUND LEFT SHOULDER MAY BE CLOSED WITH PORTABLE BARRIER WITH SIGNS PER MT-95.45.

IF ACCESS IS NEEDED FROM THE EASTBOUND LANE, SUCH AS DURING THE WEEKEND LANE CLOSURE BUT NOT LIMITED TO SUCH, THE CONTRACTOR MAY MOVE THE PORTABLE BARRIER FROM THE SHOULDER INTO THE EASTBOUND CLOSED LEFT LANE. THE PORTABLE BARRIER MUST BE RETURNED TO THE SHOULDER BEFORE OPENING THE LANE TO TRAFFIC. THIS MOVEMENT OF THE PORTABLE BARRIER IS CONSIDERED INCIDENTAL TO THE LUMP SUM CONTRACT ITEM 614 MAINTAINING TRAFFIC.

WORK ZONE QUEUE DETECTION WARNING SYSTEM

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN AN APPROVED WORK ZONE QUEUE DETECTION WARNING SYSTEM (WZQDWS) AS PER SUPPLEMENTAL SPECIFICATION 896.

THE PROBABLE INITIAL LOCATIONS OF THE WZQDWS DEVICES ARE SHOWN ON SHEET(S) _____ OF THE PLAN. IT IS EXPECTED THAT THESE LOCATIONS WILL VARY BASED ON PLANNED OR UNPLANNED PHASE AND TRAFFIC PATTERN CHANGES. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE DEVICES BY THE CONTRACTOR SHALL BE DIRECTED BY THE ENGINEER.

THE FOLLOWING TRAFFIC SENSOR THRESHOLDS AND PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) MESSAGES SHALL BE USED:

GREATER THAN OR EQUAL TO 50 MPH - USE FOUR CORNER FLASHING CAUTION MODE BETWEEN 50 MPH AND 25 MPH - TRAFFIC AHEAD XX MPH / SLOW DOWN BELOW OR EQUAL TO 25 MPH - TRAFFIC AHEAD XX MPH / PREPARE TO STOP

FOUR CORNER FLASHING CAUTION MODE SHALL CONSIST OF THE USE OF ONE ASTERISK IN EACH CORNER OF THE PCMS DISPLAY (4 TOTAL ASTERISKS).

XX SHALL BE ROUNDED UP TO THE NEAREST MULTIPLE OF 5 MPH MINUS 1. OCCUPANCY MAY BE DIRECTED TO BE USED BASED ON CERTAIN TRAFFIC CONDITIONS AND SCENARIOS. ODOT WILL DIRECT THE CONTRACTOR OF THE THRESHOLDS TO BE USED FOR THOSE AREAS WHERE OCCUPANCY IS DIRECTED TO BE USED.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 896, PORTABLE NON-INTRUSIVE TRAFFIC SENSOR, CLASS I 12 SIGN MONTHS ASSUMING 4 SENSOR(S) FOR 3 MONTH(S)

ITEM 896, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 12 SIGN MONTHS ASSUMING 4 PCMS SIGN(S) FOR 3 MONTH(S)

REF NO.	SHEET NO.	STATION TO STATION		614	614	614	614	614	614	630	614	614	614	622	622	614	614	614	614	615		
				BARRIER REFLECTOR, TYPE 1, ONE WAY	WORK ZONE EDGE LINE, CLASS I, 6" (WHITE)	WORK ZONE EDGE LINE, CLASS I, 6" (YELLOW)	MAINTAINING TRAFFIC, MISC.: 24" WHITE CHEVRON	WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I (YELLOW)	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 740.06, TYPE I		SIGN, TEMPORARY OVERLAY	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	WORK ZONE DOTTED LINE, CLASS I, 6", 642 PAINT	WORK ZONE LANE LINE, CLASS I, 6"		PORTABLE BARRIER, UNANCHORED	PORTABLE BARRIER, ANCHORED	WORK ZONE IMPACT ATTENUATOR, OVER 24" AND LESS THAN 36" WIDE HAZARDS, (UNIDIRECTIONAL)	INCREASED BARRIER DELINEATION	WORK ZONE IMPACT ATTENUATOR, MISC.: 48" WIDE ATTENUATOR (UNIDIRECTIONAL)	MAINTAINING TRAFFIC, MISC.: RUMBLE STRIP MILLED/FILLED	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A
				EACH	MILE	MILE	FT	MILE	FT	MILE	FT	FT	MILE		FT	FT	EACH	FT	EACH	FT		SY
		PHASE 1	TO	WESTBOUND	117	2.75	3.01		587			1376	1895	0.05	4786	968	1	264		14564		
				EASTBOUND		2.75	3.01													14556		
		PHASE 2A		WESTBOUND	149	3.42	3.63	129	0.13	1070		1736	648		6594	788			1		53	
				EASTBOUND		3.42	3.63															
		PHASE 2B		WESTBOUND	131			53					415		6218	238	1		1			
				EASTBOUND		0.36	0.08															
		PHASE 2C		WESTBOUND				134				1958	290				2		1			
				EASTBOUND																		
	P.36										45											
TOTALS CARRIED TO GENERAL SUMMARY					397	26.06	13.36	317	0.13	1657	45	5070	3249	0.05	17598	1994	4	264	3	29120	53	

MAINTENANCE OF TRAFFIC SUBSUMMARY

DESIGN AGENCY



DESIGNER

SKB

REVIEWER

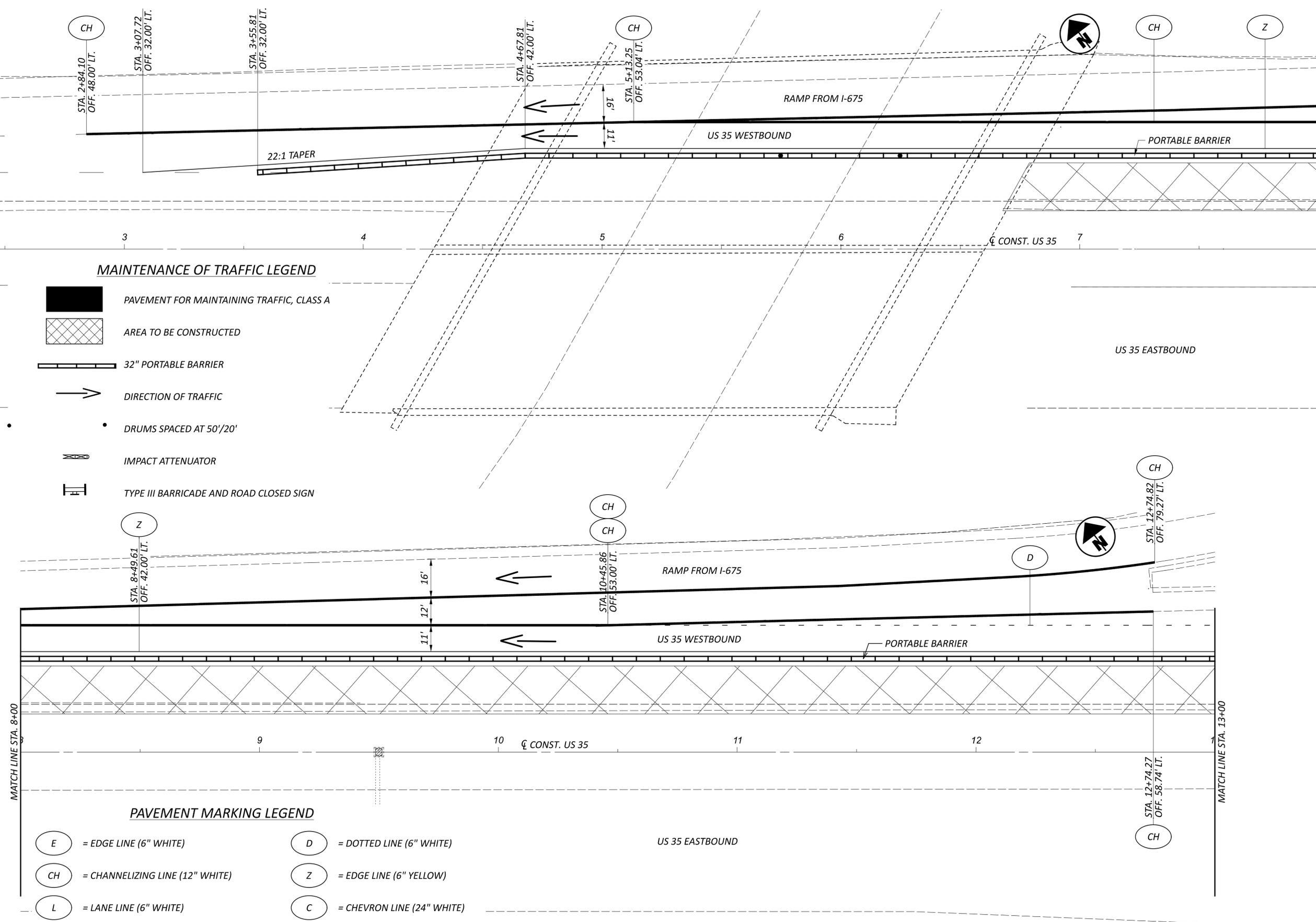
LAS 02/03/25

PROJECT ID

96680

SHEET TOTAL

P.15 P.171

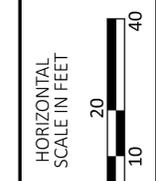


MAINTENANCE OF TRAFFIC LEGEND

-  PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A
-  AREA TO BE CONSTRUCTED
-  32" PORTABLE BARRIER
-  DIRECTION OF TRAFFIC
-  DRUMS SPACED AT 50'/20'
-  IMPACT ATTENUATOR
-  TYPE III BARRICADE AND ROAD CLOSED SIGN

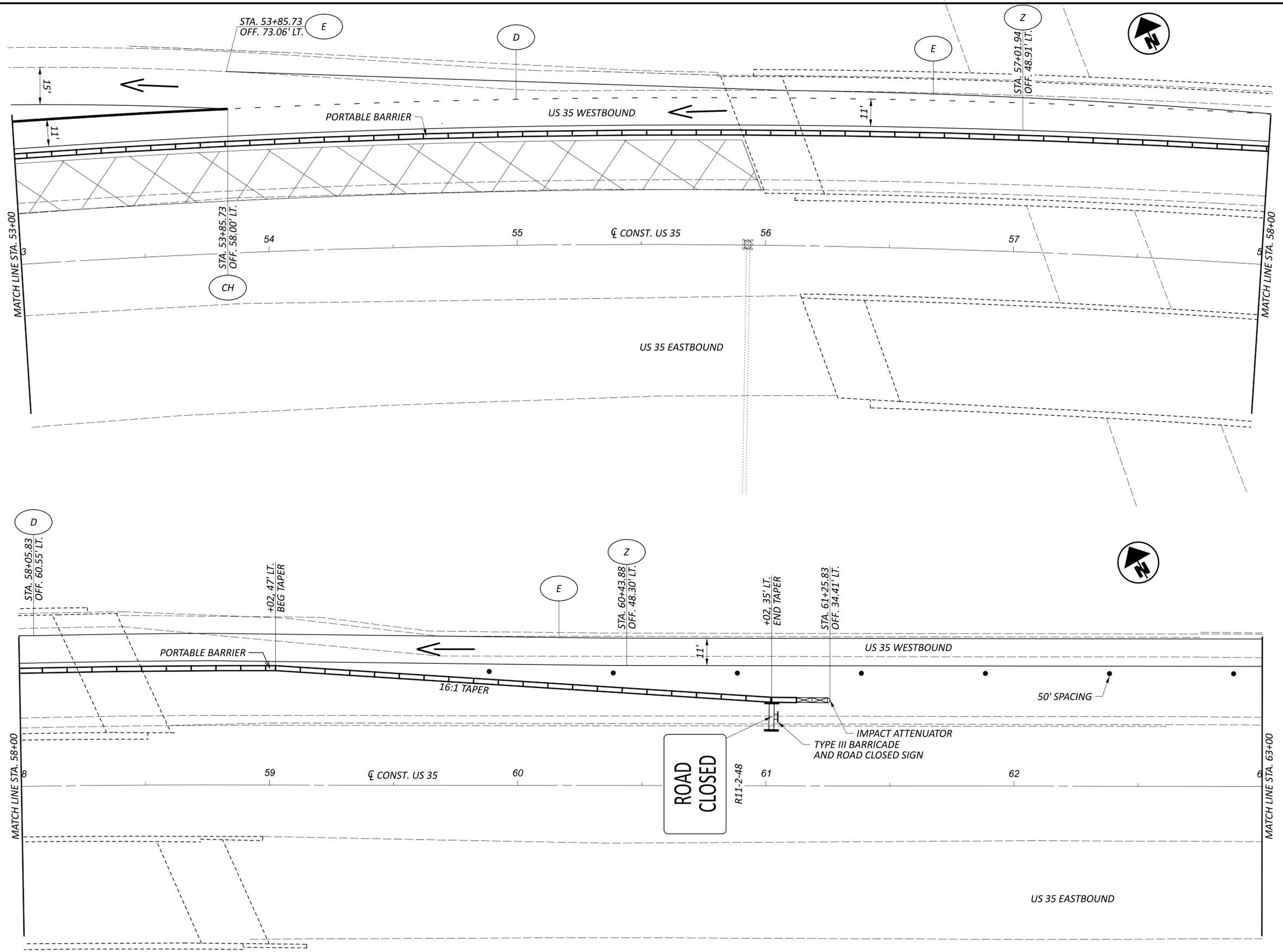
PAVEMENT MARKING LEGEND

- | | |
|---|--|
|  = EDGE LINE (6" WHITE) |  = DOTTED LINE (6" WHITE) |
|  = CHANNELIZING LINE (12" WHITE) |  = EDGE LINE (6" YELLOW) |
|  = LANE LINE (6" WHITE) |  = CHEVRON LINE (24" WHITE) |

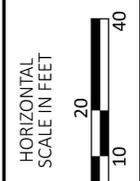


**MAINTENANCE OF TRAFFIC PLAN
PHASE 1**

DESIGN AGENCY	
	
DESIGNER	SKB
REVIEWER	LAS
PROJECT ID	02/03/25
	96680
SHEET	TOTAL
P.16	P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
 PHASE 1

DESIGN AGENCY



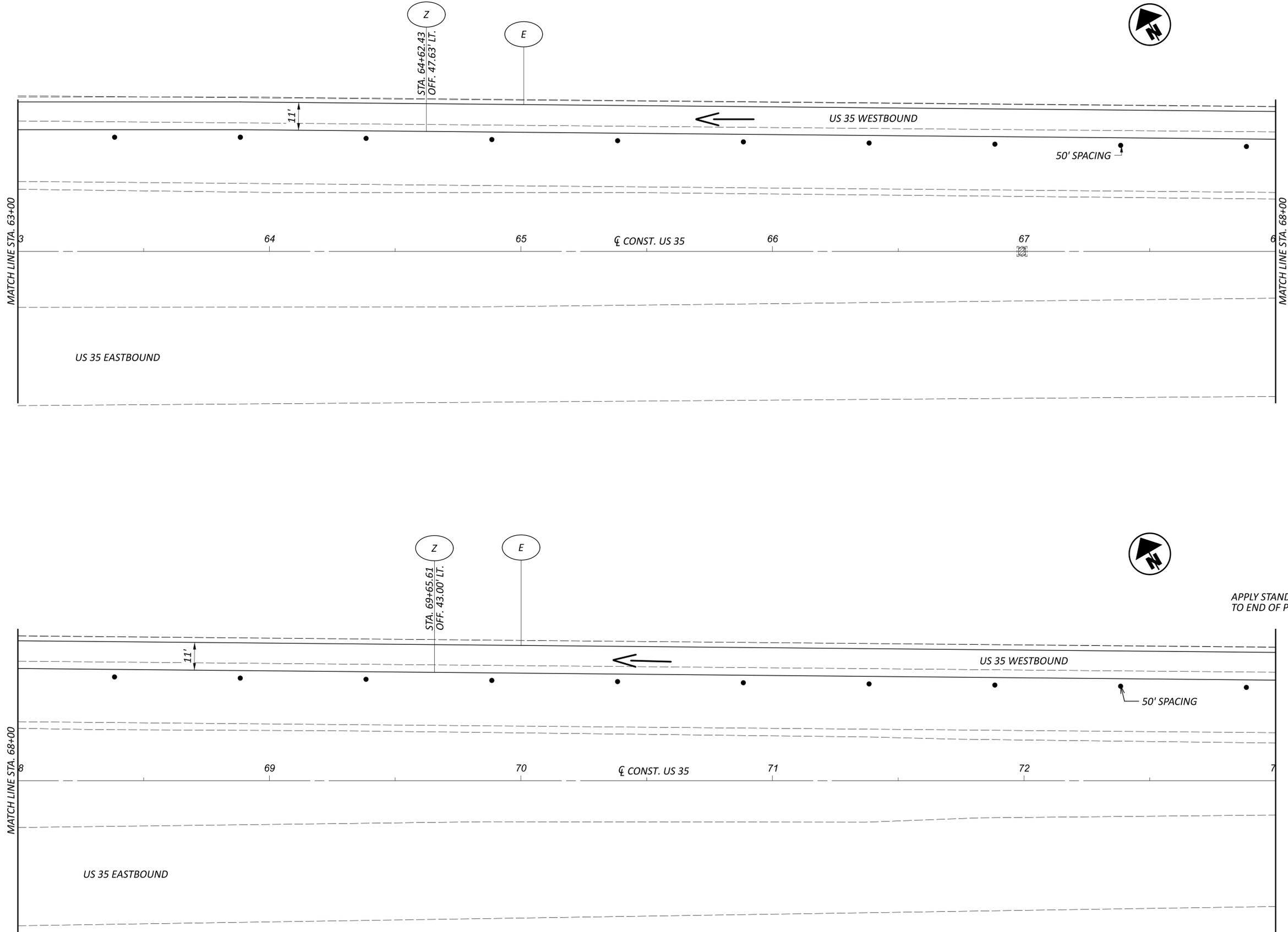
DESIGNER
 SKB

REVIEWER
 LAS

DATE
 02/03/25

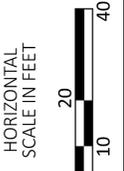
PROJECT ID
 96680

SHEET TOTAL
 P.21 P.171



APPLY STANDARD DRAWINGS TO END OF PROJECT

FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
PHASE 1

DESIGN AGENCY



DESIGNER

SKB

REVIEWER

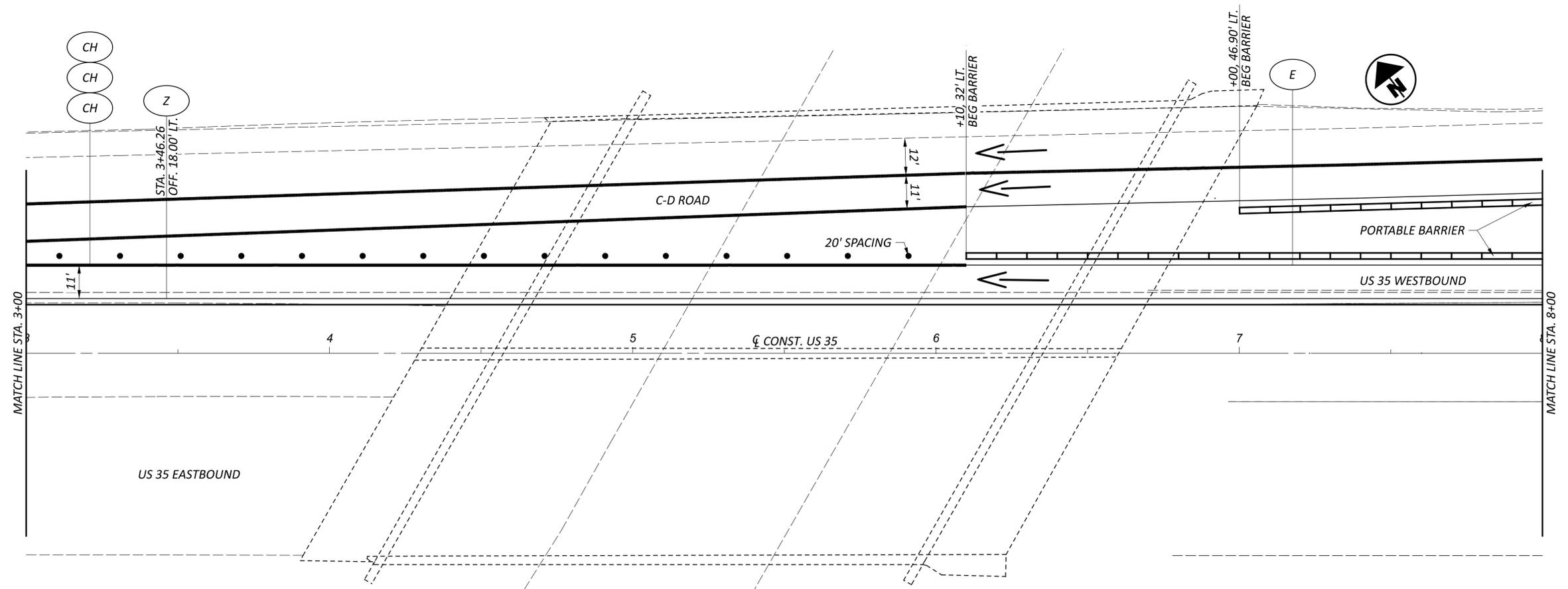
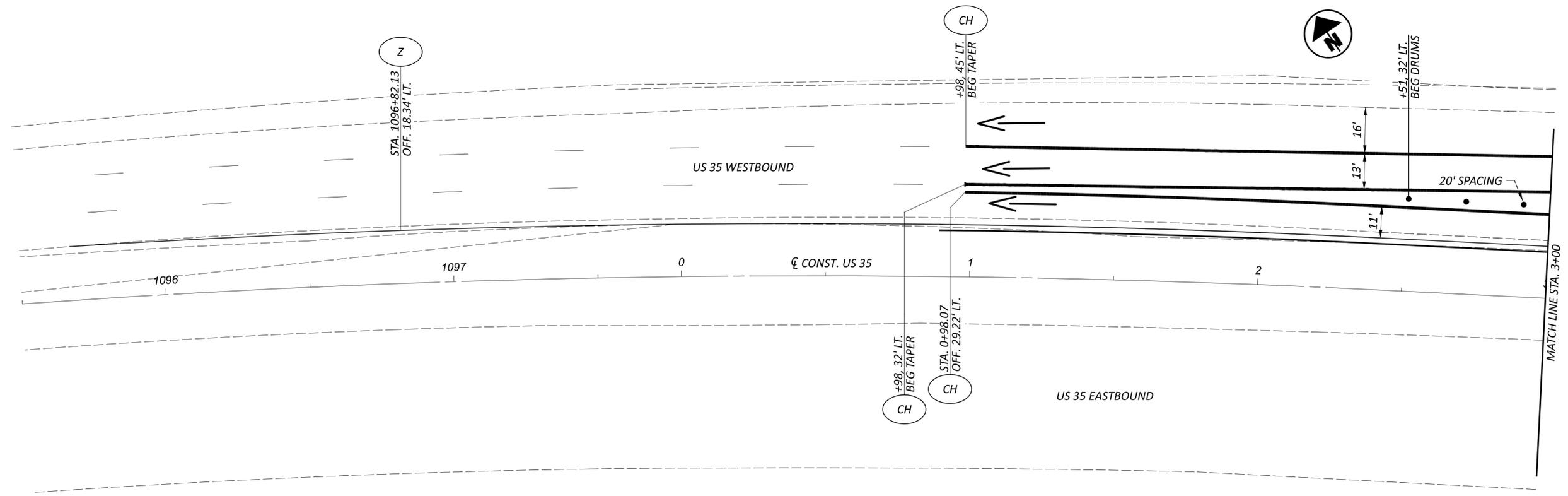
LAS 02/03/25

PROJECT ID

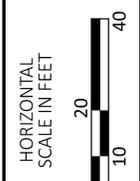
96680

SHEET TOTAL

P.22 P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
PHASE 2A

DESIGN AGENCY



DESIGNER

SKB

REVIEWER

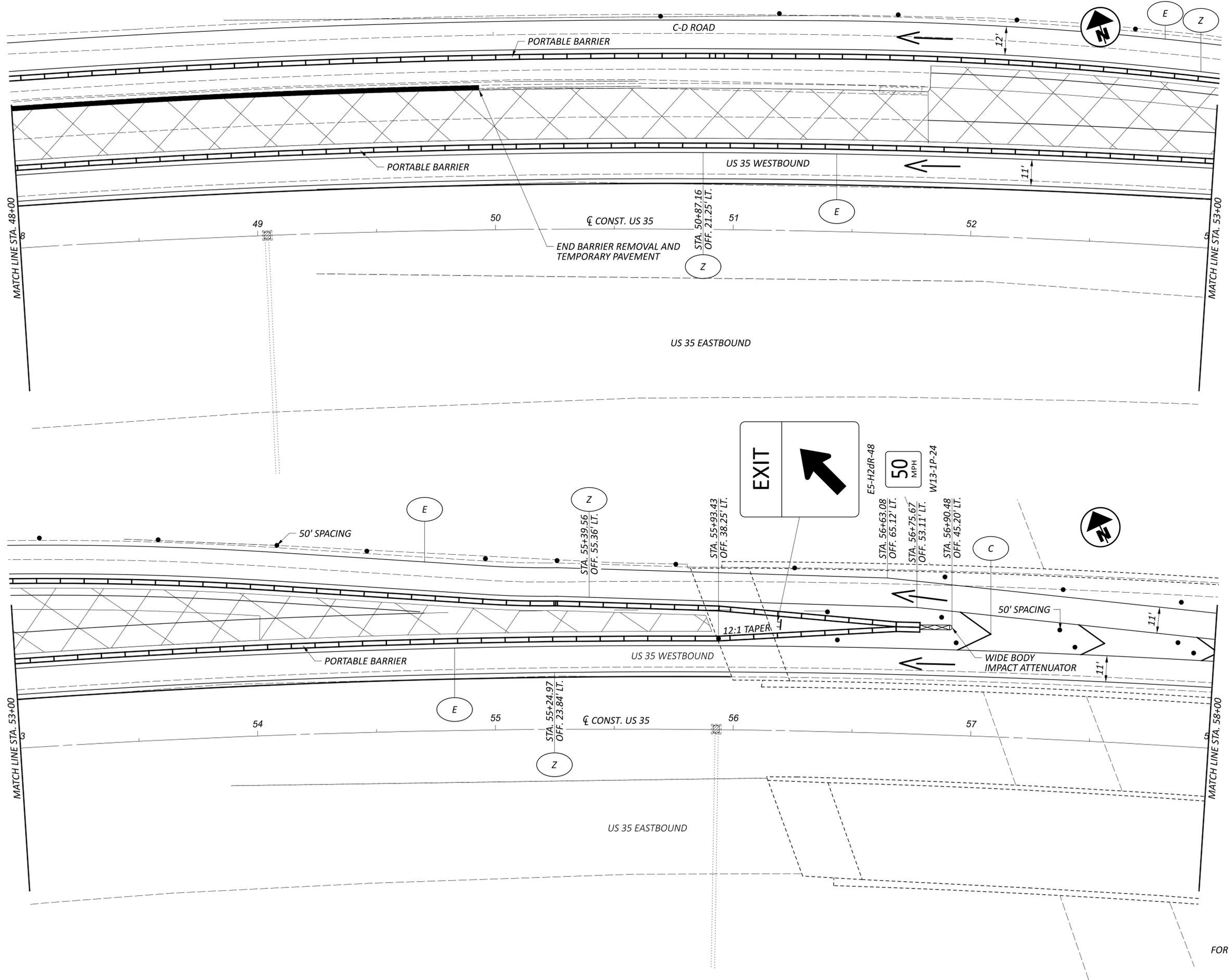
LAS 02/03/25

PROJECT ID

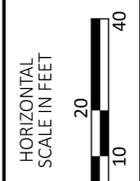
96680

SHEET TOTAL

P.23 P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
 PHASE 2A

DESIGN AGENCY



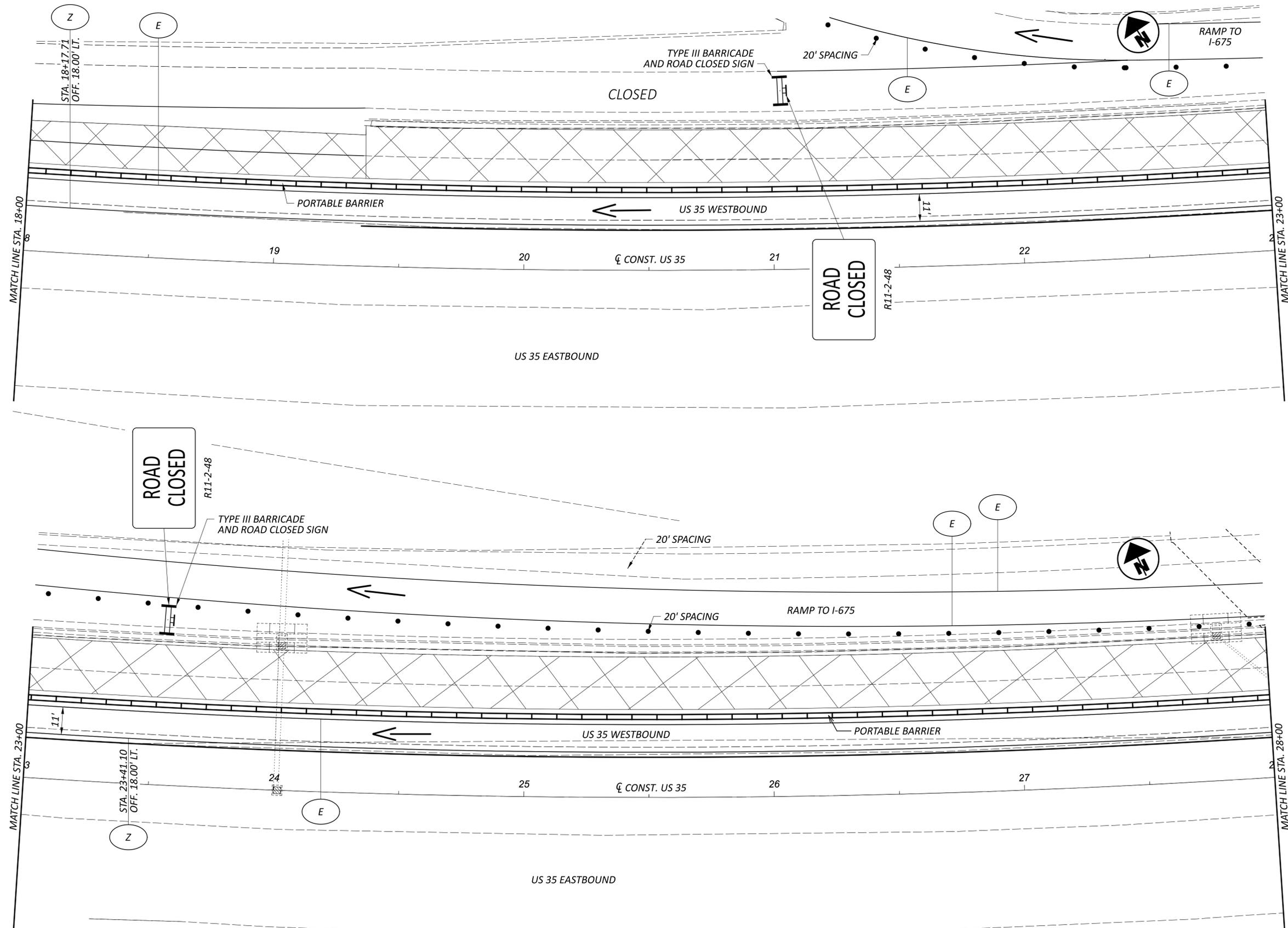
DESIGNER
 SKB

REVIEWER
 LAS

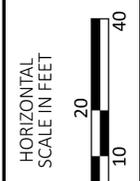
DATE
 02/03/25

PROJECT ID
 96680

SHEET TOTAL
 P.28 P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
PHASE 2B

DESIGN AGENCY



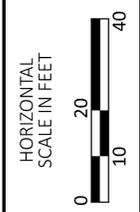
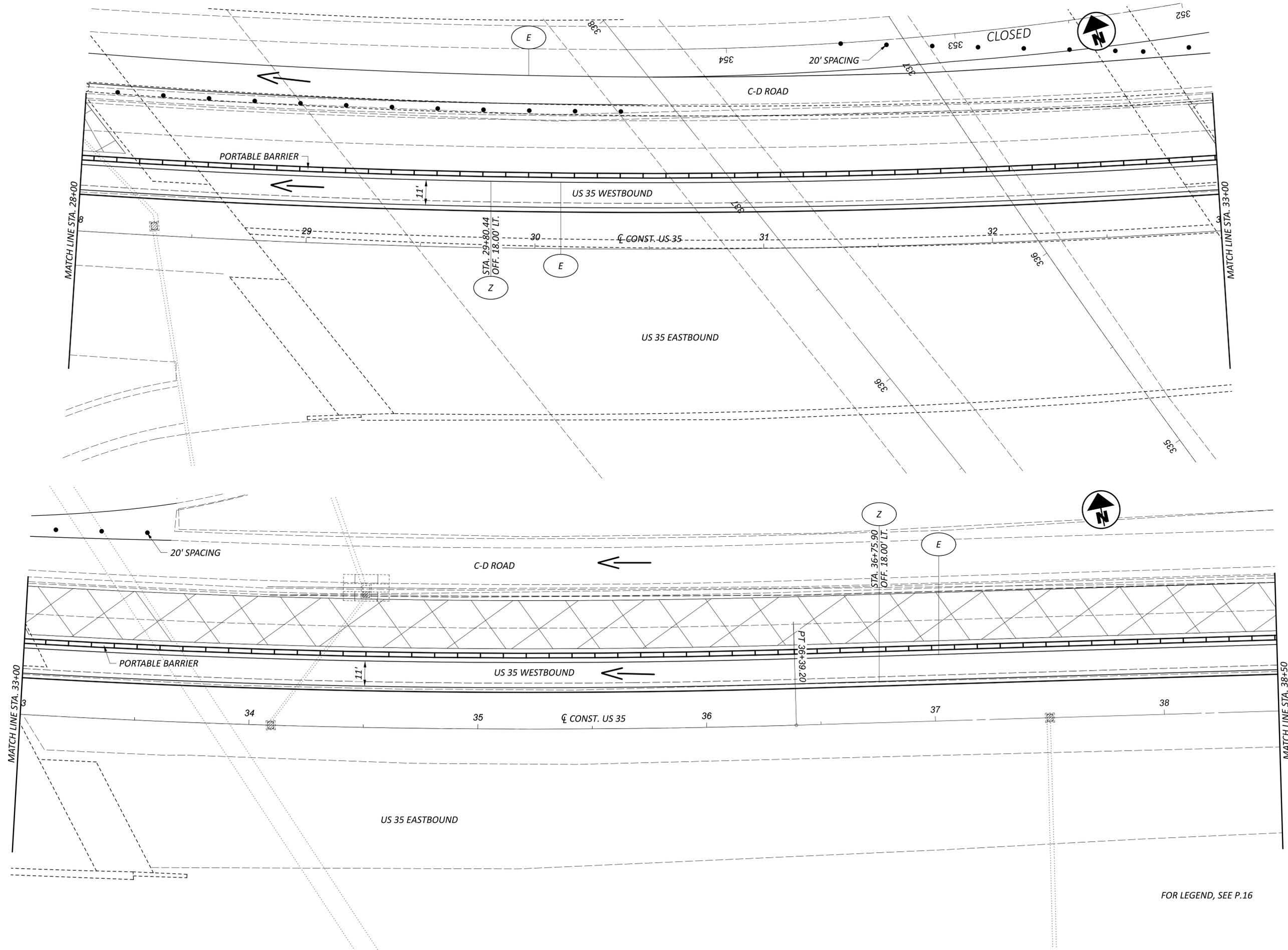
DESIGNER
SKB

REVIEWER
LAS

02/03/25

PROJECT ID
96680

SHEET	TOTAL
P.33	P.171



MAINTENANCE OF TRAFFIC PLAN
PHASE 2B

DESIGN AGENCY



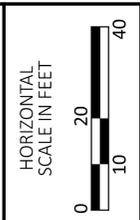
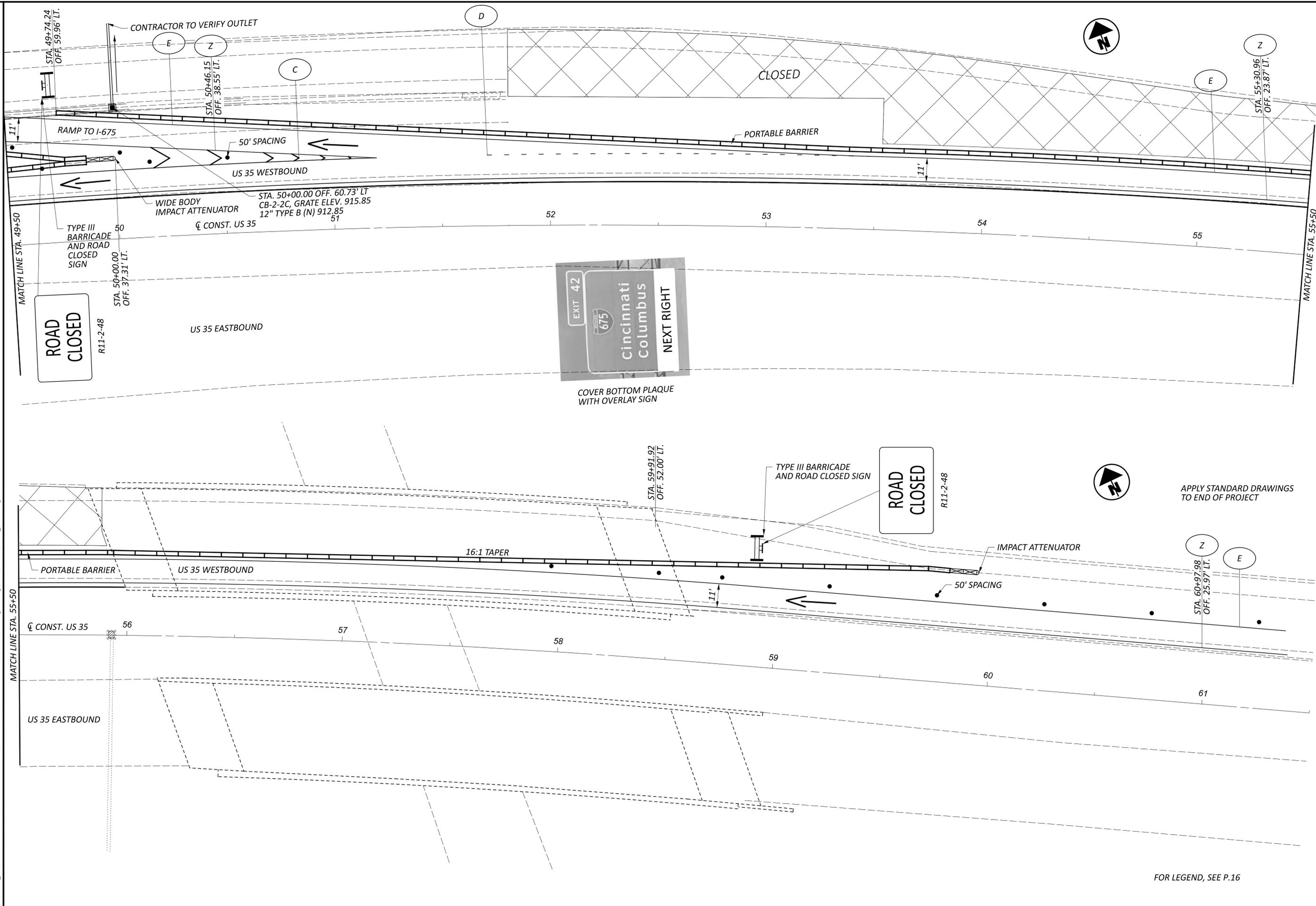
DESIGNER
SKB

REVIEWER
LAS 02/03/25

PROJECT ID
96680

SHEET TOTAL
P.34 P.171

FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
PHASE 2B

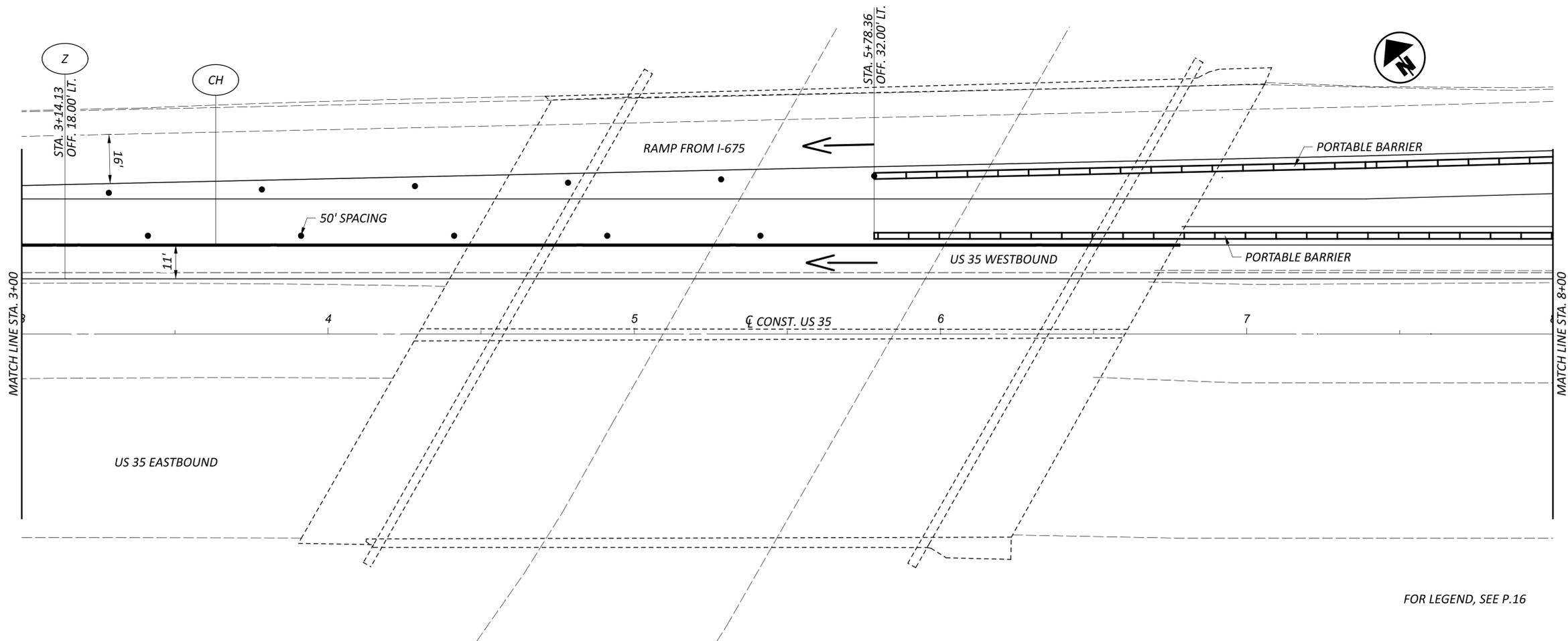
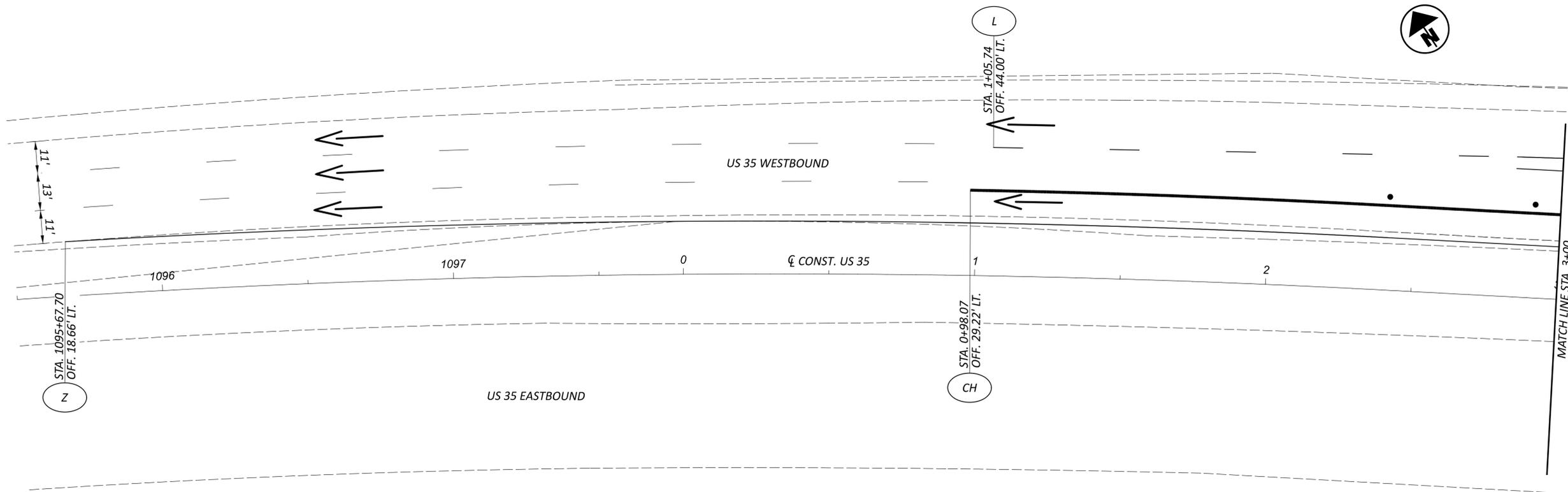
APPLY STANDARD DRAWINGS TO END OF PROJECT

FOR LEGEND, SEE P.16

DESIGN AGENCY



DESIGNER	SKB
REVIEWER	LAS
PROJECT ID	96680
SHEET	P.36
TOTAL	P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
PHASE 2C

DESIGN AGENCY



DESIGNER

SKB

REVIEWER

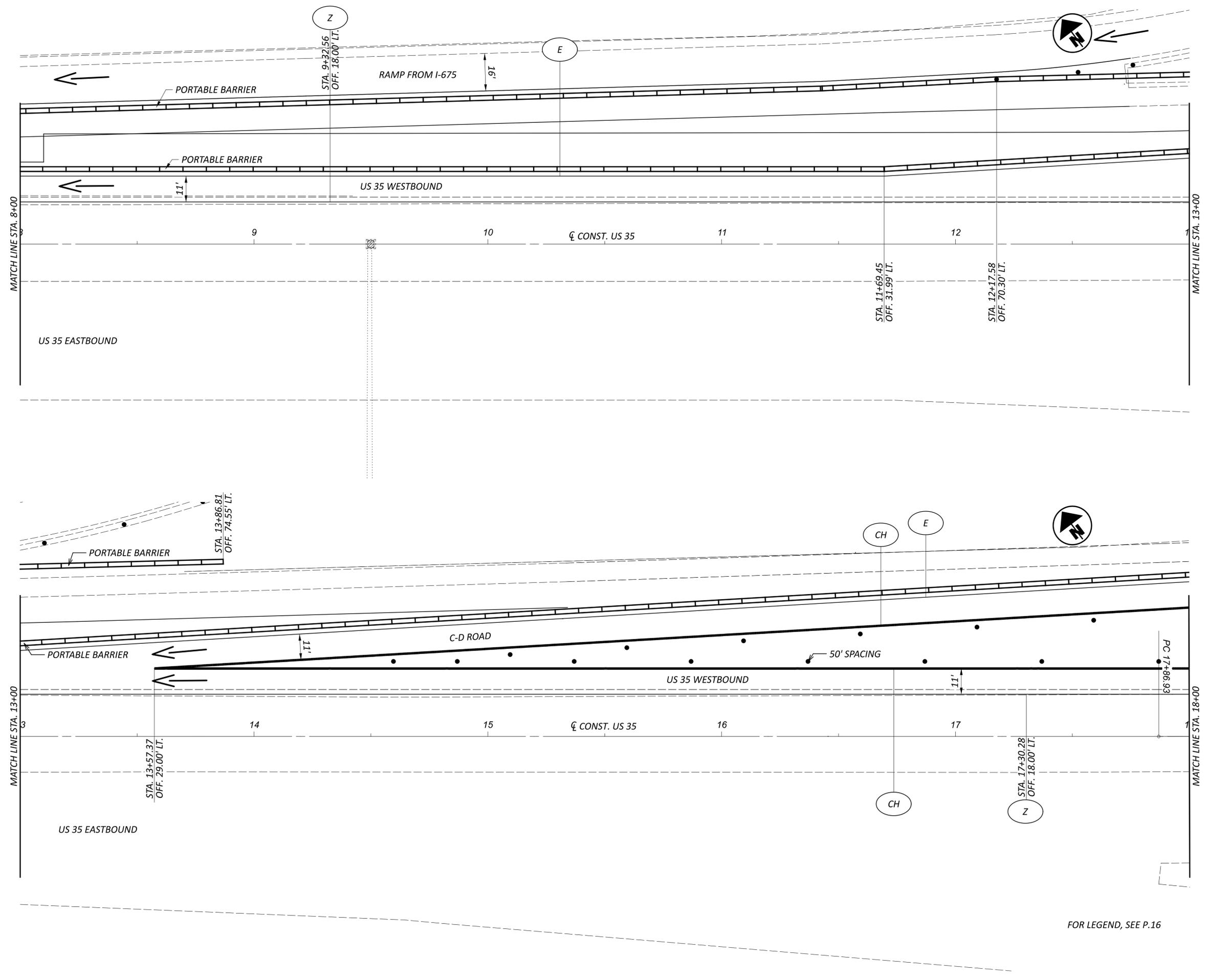
LAS 02/03/25

PROJECT ID

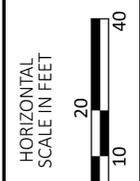
96680

SHEET TOTAL

P.37 P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
 PHASE 2C

DESIGN AGENCY

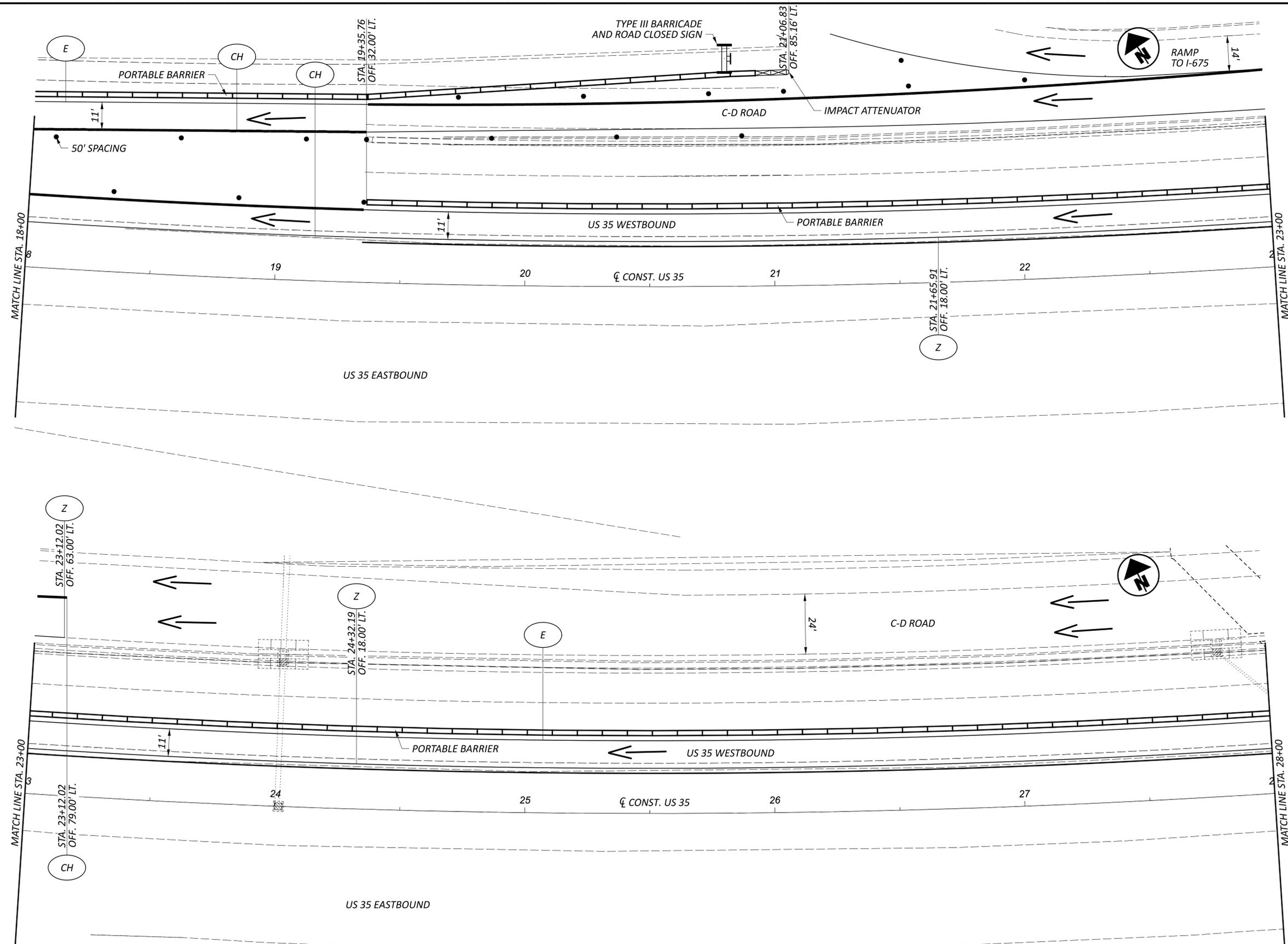


DESIGNER
 SKB

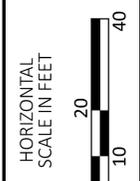
REVIEWER
 LAS 02/03/25

PROJECT ID
 96680

SHEET TOTAL
 P.38 P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
 PHASE 2C

DESIGN AGENCY

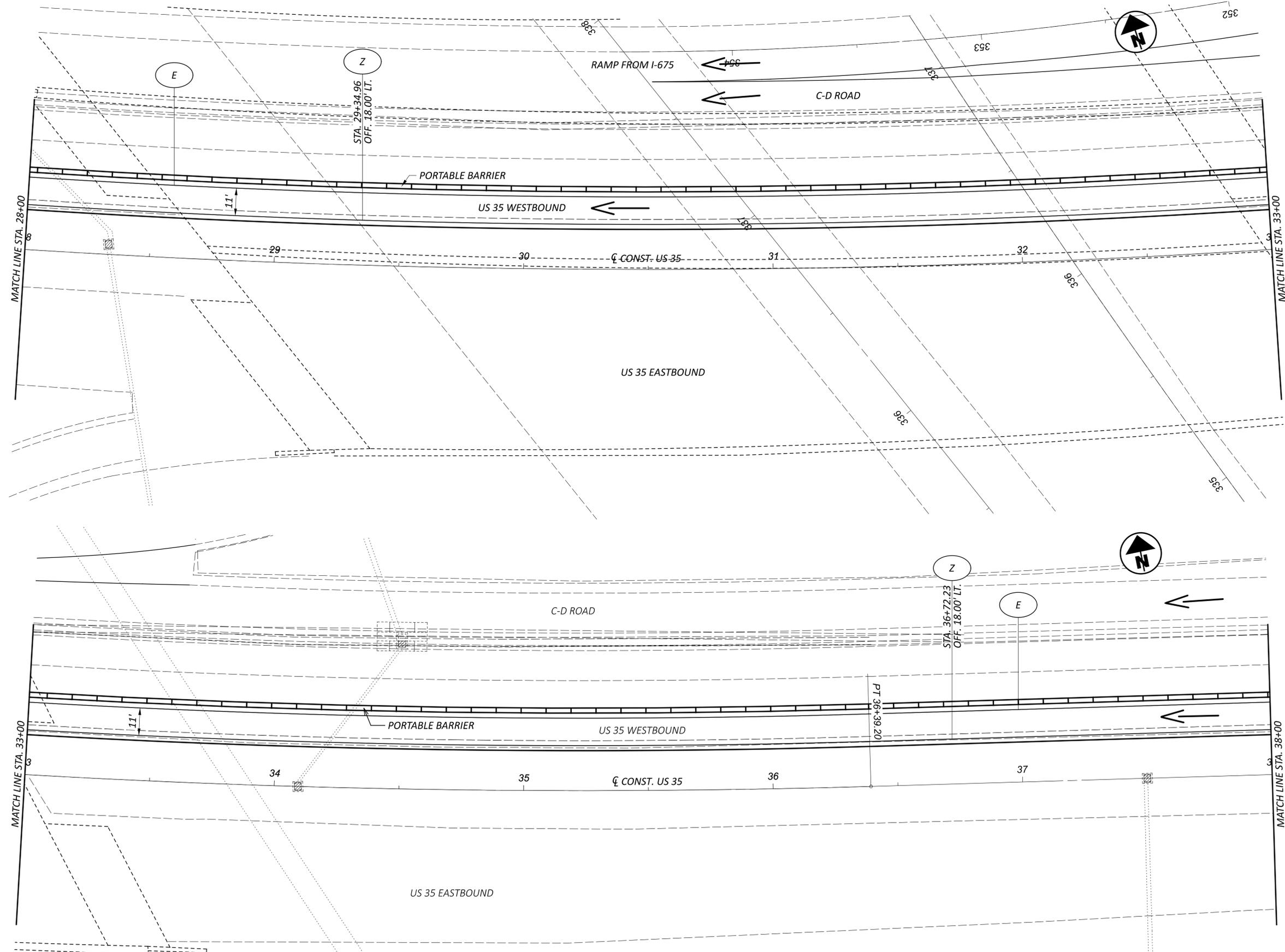


DESIGNER
 SKB

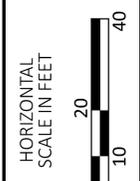
REVIEWER
 LAS 02/03/25

PROJECT ID
 96680

SHEET TOTAL
 P.39 P.171

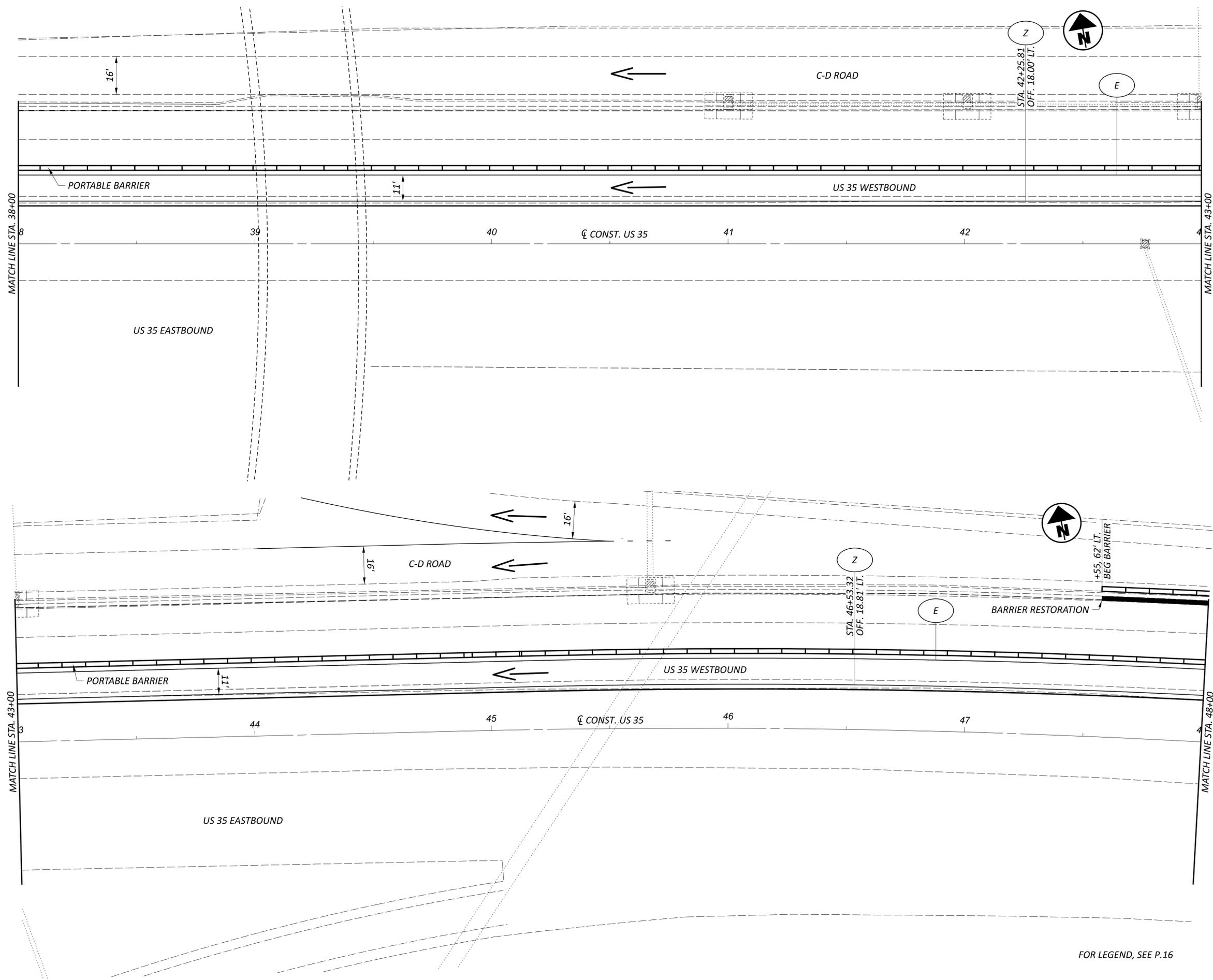


FOR LEGEND, SEE P.16

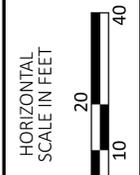


MAINTENANCE OF TRAFFIC PLAN
PHASE 2C

DESIGN AGENCY	
DESIGNER	SKB
REVIEWER	LAS
PROJECT ID	96680
SHEET	TOTAL
P.40	P.171



FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
PHASE 2C

DESIGN AGENCY



DESIGNER

SKB

REVIEWER

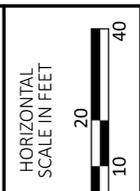
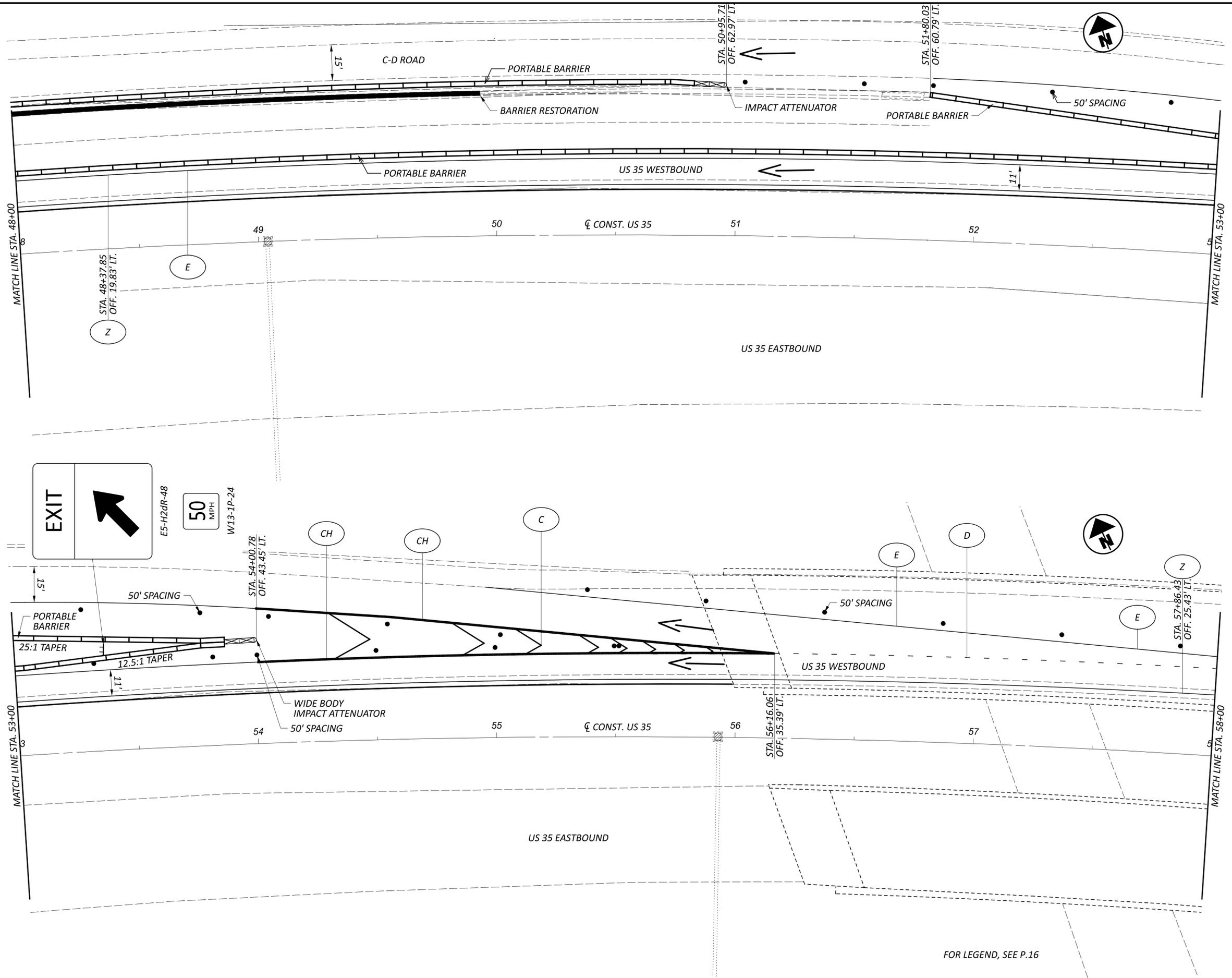
LAS 02/03/25

PROJECT ID

96680

SHEET TOTAL

P.41 P.171



MAINTENANCE OF TRAFFIC PLAN
PHASE 2C

DESIGN AGENCY



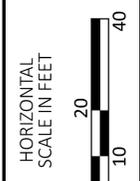
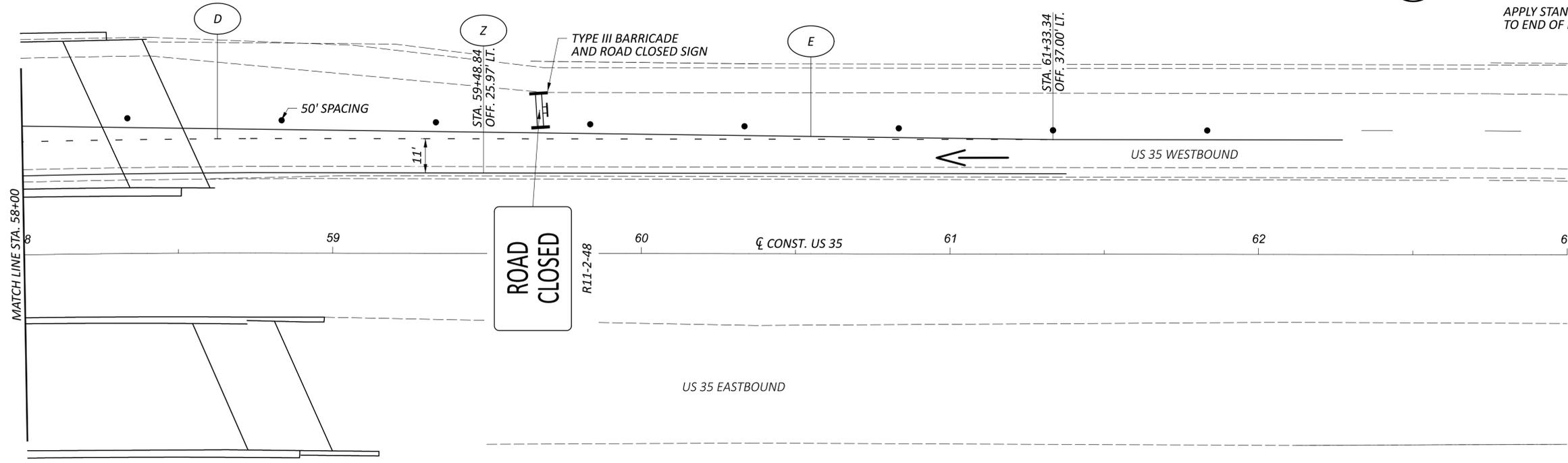
DESIGNER
SKB

REVIEWER
LAS 02/03/25

PROJECT ID
96680

SHEET TOTAL
P.42 P.171

FOR LEGEND, SEE P.16



MAINTENANCE OF TRAFFIC PLAN
PHASE 2C

DESIGN AGENCY



DESIGNER
SKB

REVIEWER
LAS 02/03/25

PROJECT ID
96680

SHEET TOTAL
P.43 P.171

FOR LEGEND, SEE P.16

SHEET NUM.										PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
P.6	P.7	P.8	P.9	P.10	P.15	P.48	P.107			01/NHS/05	02/NHS/13	03/MS/13						
						20.78				20.78			850	10010	20.78	MILE	TRAFFIC CONTROL (CONT.) GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	
						6,036				6,036			850	10130	6,036	FT	GROOVING FOR 12" RECESSED PAVEMENT MARKING, (ASPHALT)	
							2.2					2.2	442	22400	2.2	CY	STRUCTURE OVER 20 FOOT SPAN (SFN 2902990) ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (449), (VARIABLE THICKNESS)	
							14					14	516	46701	14	EACH	RESET BEARING, AS PER PLAN	
							0.4					0.4	519	12300	0.4	SY	PATCHING CONCRETE BRIDGE DECK - TYPE B	
							270					270	842	10000	270	LB	CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE	
							129					129	844	10000	129	SF	CONCRETE PATCHING WITH GALVANIC ANODE PROTECTION	
							7					7	516	46701	7	EACH	STRUCTURE OVER 20 FOOT SPAN (SFN 2903008) RESET BEARING, AS PER PLAN	
							5.4					5.4	519	12300	5.4	SY	PATCHING CONCRETE BRIDGE DECK - TYPE B	
							113					113	844	10000	113	SF	CONCRETE PATCHING WITH GALVANIC ANODE PROTECTION	
							9					9	516	46701	9	EACH	STRUCTURE OVER 20 FOOT SPAN (SFN 2903016) RESET BEARING, AS PER PLAN	
							6					6	519	12300	6	SY	PATCHING CONCRETE BRIDGE DECK - TYPE B	
							39					39	844	10000	39	SF	CONCRETE PATCHING WITH GALVANIC ANODE PROTECTION	
							2.7				2.7	519	12300	2.7	SY	STRUCTURE OVER 20 FOOT SPAN (SFN 2903067) PATCHING CONCRETE BRIDGE DECK - TYPE B		
							4				4	519	12300	4	SY	STRUCTURE OVER 20 FOOT SPAN (SFN 2903075) PATCHING CONCRETE BRIDGE DECK - TYPE B		
							477				477	512	73500	477	SY	STRUCTURE OVER 20 FOOT SPAN (SFN 2900092) TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN		
							141				141	512	74500	141	FT	REMOVAL OF EXISTING PAVEMENT MARKING		
							15				15	519	12300	15	SY	PATCHING CONCRETE BRIDGE DECK - TYPE B		
							10				10	519	11100	10	SF	PATCHING CONCRETE STRUCTURE		
							16.6				16.6	519	12300	16.6	SY	STRUCTURE OVER 20 FOOT SPAN (SFN 2900122) PATCHING CONCRETE BRIDGE DECK - TYPE B		
			500							500			614	11110	500	HOUR	MAINTENANCE OF TRAFFIC LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
					264					264			614	11630	264	FT	INCREASED BARRIER DELINEATION	
					7					7			614	12390	7	EACH	WORK ZONE IMPACT ATTENUATOR, OVER 24" AND LESS THAN 36" WIDE HAZARDS, (UNIDIRECTIONAL)	
LS					3					3			614	12400	3	EACH	WORK ZONE IMPACT ATTENUATOR, MISC.:48" WIDE ATTENUATOR (UNIDIRECTIONAL)	P.9
										LS			614	12420	LS		DETOUR SIGNING	
		4								4			614	12484	4	EACH	WORK ZONE INCREASED PENALTIES SIGN	
25			250							250			614	12800	250	EACH	WORK ZONE RAISED PAVEMENT MARKER	
						397				25			614	13000	25	CY	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
						29,120				397			614	13310	397	EACH	BARRIER REFLECTOR, TYPE 1, ONE-WAY	
										29,120			614	18030	29,120	FT	MAINTAINING TRAFFIC, MISC.: RUMBLE STRIP MILLED/FILLED	P.6
				10						10			614	18601	10	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	P.8
					0.05					0.05			614	20010	0.05	MILE	WORK ZONE LANE LINE, CLASS I, 6"	
					26.06					26.06			614	22010	26.06	MILE	WORK ZONE EDGE LINE, CLASS I, 6"	
					0.13					0.13			614	22210	0.13	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I	
					5,070					5,070			614	23010	5,070	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	
					1,657					1,657			614	23410	1,657	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 740.06, TYPE I	
					3,249					3,249			614	24202	3,249	FT	WORK ZONE DOTTED LINE, CLASS I, 6", 642 PAINT	
					317					317			614	98100	317	FT	WORK ZONE PAVEMENT MARKING, MISC.:24" WHITE CHEVRON	P.9
					53					53			615	20000	53	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	
20	20									40			616	10000	40	MGAL	WATER	
					17,598					17,598			622	41100	17,598	FT	PORTABLE BARRIER, UNANCHORED	
					1,994					1,994			622	41110	1,994	FT	PORTABLE BARRIER, ANCHORED	
				12						12			808	18700	12	SNMT	DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY	
										12			896	00010	12	SNMT	PORTABLE NON-INTRUSIVE TRAFFIC SENSOR, CLASS I	
				12						12			896	00020	12	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN	
										LS	LS	LS	614	11000	LS		INCIDENTALS MAINTAINING TRAFFIC	
										LS	LS	LS	623	10001	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN	P.5
										LS	LS	LS	624	10000	LS		MOBILIZATION	

GENERAL SUMMARY

DESIGN AGENCY

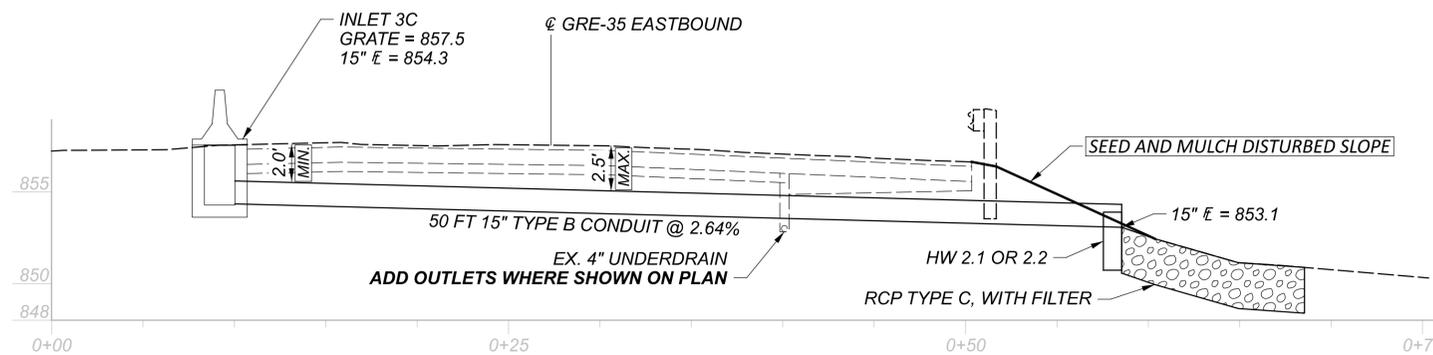
 DESIGNER
 GAT
 REVIEWER
 XXX MM-DD-YY
 PROJECT ID
 96680
 SHEET TOTAL
 P.45 171

STATION RANGE			REF.	SIDE	DISTANCE (D) FT	AVERAGE WIDTH (W) FT	SURFACE AREA (A) A=DxW/9 SY	CADD GENERATED AREA SY	442	442	407	302	304	206	206	204	206	204	202	202	202	606	606					
									ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447) CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446) CY	NON-TRACKING TACK COAT GAL	ASPHALT CONCRETE BASE, PG64-22, (449) CY	AGGREGATE BASE CY	CEMENT STABILIZED SUBGRADE, 14 INCHES DEEP SY	CURING COAT SY	PROOF ROLLING HOUR	CEMENT TON	EXCAVATION OF SUBGRADE CY	PAVEMENT REMOVED SY	GUARDRAIL REMOVED FT	BRIDGE TERMINAL ASSEMBLY REMOVED EACH	GUARDRAIL, TYPE MGS FT	BRIDGE TERMINAL ASSEMBLY, TYPE 2 EACH					
SLM 0.08	TO	SLM 1.02																										
US 35 WESTBOUND																												
6+58.96	TO	17+86.93		LT	1127.97	53.00	6642.49	5439.68	226.65	415.53	299.18																	
						53.35		5498.82			604.87	1374.70																
						54.10		5623.92				937.32																
						54.49		5690.12					5690.12	5690.12	2.85	171.77	1620.10	5690.12										
17+86.93	TO	28+39.63		LT	1052.70	35.00	4093.83	3761.27	156.72	287.32	206.87																	
						35.35		3802.30			418.25	950.58																
						36.10		3889.08				648.18																
						36.49		3934.84					3934.84	3934.84	1.97	118.78	1120.34	3934.84										
33+21.89	TO	51+79.93		LT	1858.04	32.91	6794.23	7039.79	293.32	537.76	387.19																	
						33.26		7113.37			782.47	1778.34																
						34.01		7269.14				1211.52																
						34.41		7351.38					7351.38	7351.38	3.68	221.92	2093.10	7351.38										
51+79.93	TO	56+06.29		LT	426.36	57.00	2700.28	2818.36	117.43	215.29	155.01																	
						57.35		2850.91			313.60	712.73																
						58.10		2919.83				486.64																
						58.49		2956.22					2956.22	2956.22	1.48	89.24	841.70	2956.22										
51+79.86	TO	56+06.29	R-1		426.43														437.50	1								
51+79.86	TO	56+06.29	G-1		426.43																437.50	1						
SUBTOTALS									794.13	1455.90	3167.44	4816.35	3283.66	19932.56	19932.56	9.97	601.71	5675.24	19932.56	437.50	1.00	437.50	1.00					
TOTALS CARRIED TO GENERAL SUMMARY									795	1456	3168	4817	3284	19933	19933	10	602	5676	19933	437.50	1	437.50	1					

ROADWAY & PAVEMENT SUBSUMMARY - FULL DEPTH

DESIGN AGENCY

 DESIGNER
 GAT
 REVIEWER
 XXX MM-DD-YY
 PROJECT ID
 96680
 SHEET TOTAL
 P.46 171



ESTIMATED QUANTITIES (CARRIED TO GENERAL SUMMARY)			
ITEM	QUANTITY	UNIT	DESCRIPTION
202	20	FT	BARRIER REMOVED
201	1.4	CY	ROCK CHANNEL PROTECTION, TYPE C WITH FILTER
301	13.0	CY	ASPHALT CONCRETE BASE, PG 64-22
304	6.5	CY	AGGREGATE BASE
601	3.6	SY	TIED CONCRETE BLOCK MAT WITH TYPE 1 UNDERLAYMENT
602	0.27	CY	CONCRETE MASONRY
611	50	FT	15" CONDUIT, TYPE B
611	1	EA	INLET, NO. 3C, AS PER PLAN
611	2	EA	PRECAST REINFORCED CONCRETE OUTLET
611	40	FT	4" CONDUIT, TYPE F
659	56	SY	SEEDING AND MULCHING

ACCESS BEHIND GUARDRAIL

REMOVE AND RE-ERECT GUARDRAIL AS NECESSARY FOR ACCESS TO A WORK LOCATION. REMOVE GUARDRAIL ONLY WHEN IT CAN BE REPLACED ON THE SAME DAY. OBTAIN APPROVAL FROM THE ENGINEER FOR EACH LOCATION, PRIOR TO PERFORMING THE WORK. THIS WORK INCLUDES REMOVAL OF EXISTING GUARDRAIL AND POSTS AND RE-ERECTION OF THE SAME MATERIALS. EXISTING RAIL ELEMENTS AND BARRIER REFLECTORS MAY BE REUSED. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

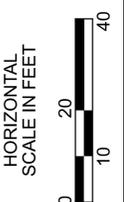
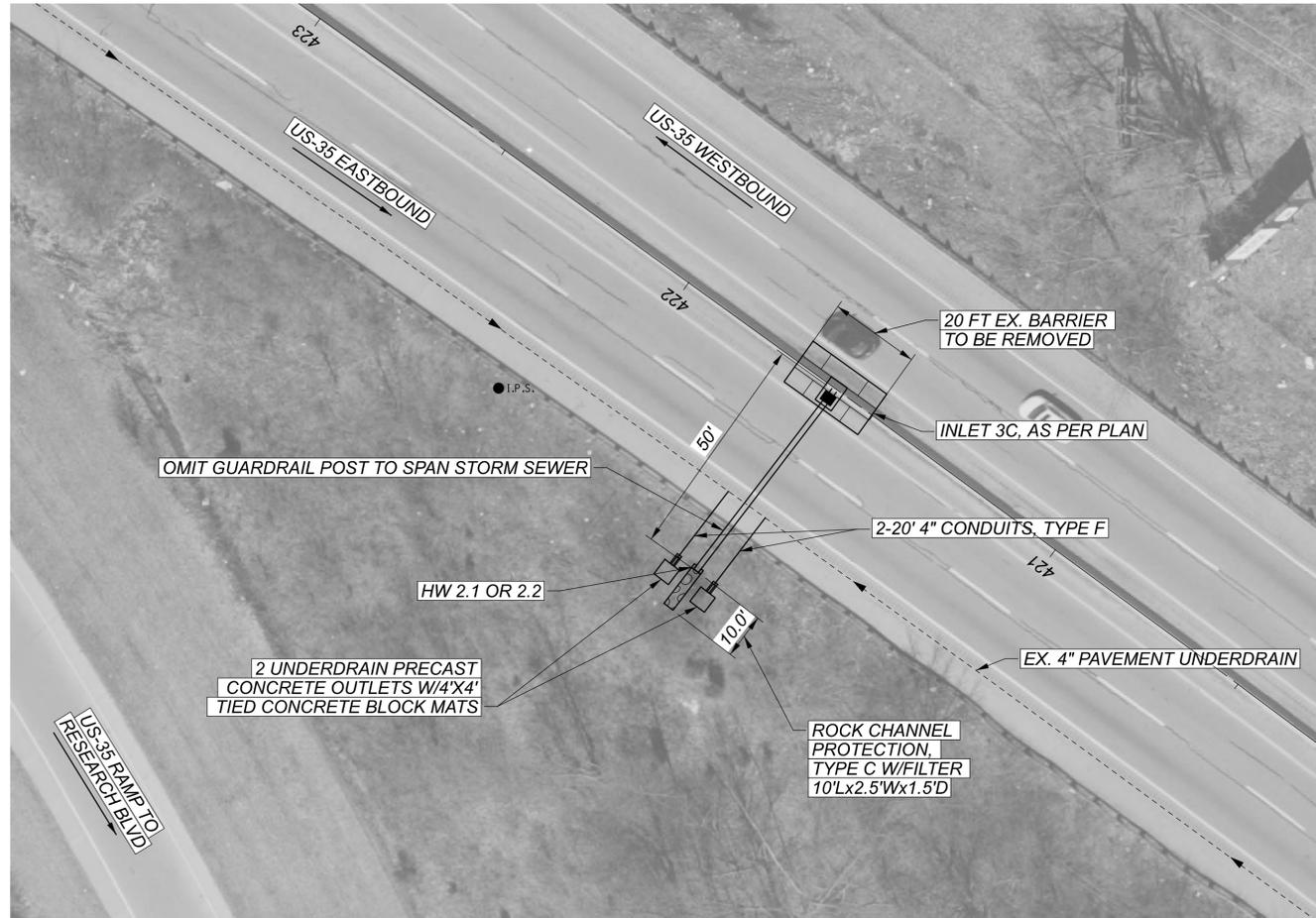
ITEM 611 - INLET, NO 3C, AS PER PLAN

ALL INLET GEOMETRY SHALL BE CONSTRUCTED PER THE CURRENT I-3C SCD AS LISTED ON THE TITLE SHEET. THE BARRIER GEOMETRY SHALL FOLLOW GEOMETRY DEPICTED IN ARCHIVED SCD I-3C&D DATED 4/1/80, AS SHOWN IN THE MISCELLANEOUS DETAILS.

TEMPORARY PAVEMENT SECTION*



* THIS PAVEMENT SECTION IS TO BE TEMPORARY WITH THE CONDUIT INSTALLATION OCCURRING PRIOR TO THE RESURFACING ACTIVITIES. THIS PAVEMENT SECTION IS TO REMAIN IN PLACE UNTIL FINAL RESURFACING OCCURS PER THE SEQUENCE OF CONSTRUCTION IN THE MAINTENANCE OF TRAFFIC GENERAL NOTES.



STORM SEWER PLAN & PROFILE
 STA 421+50

DESIGN AGENCY



DESIGNER

MLB

REVIEWER

TRB 05-23-2024

PROJECT ID

96680

SHEET

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TOTAL

171