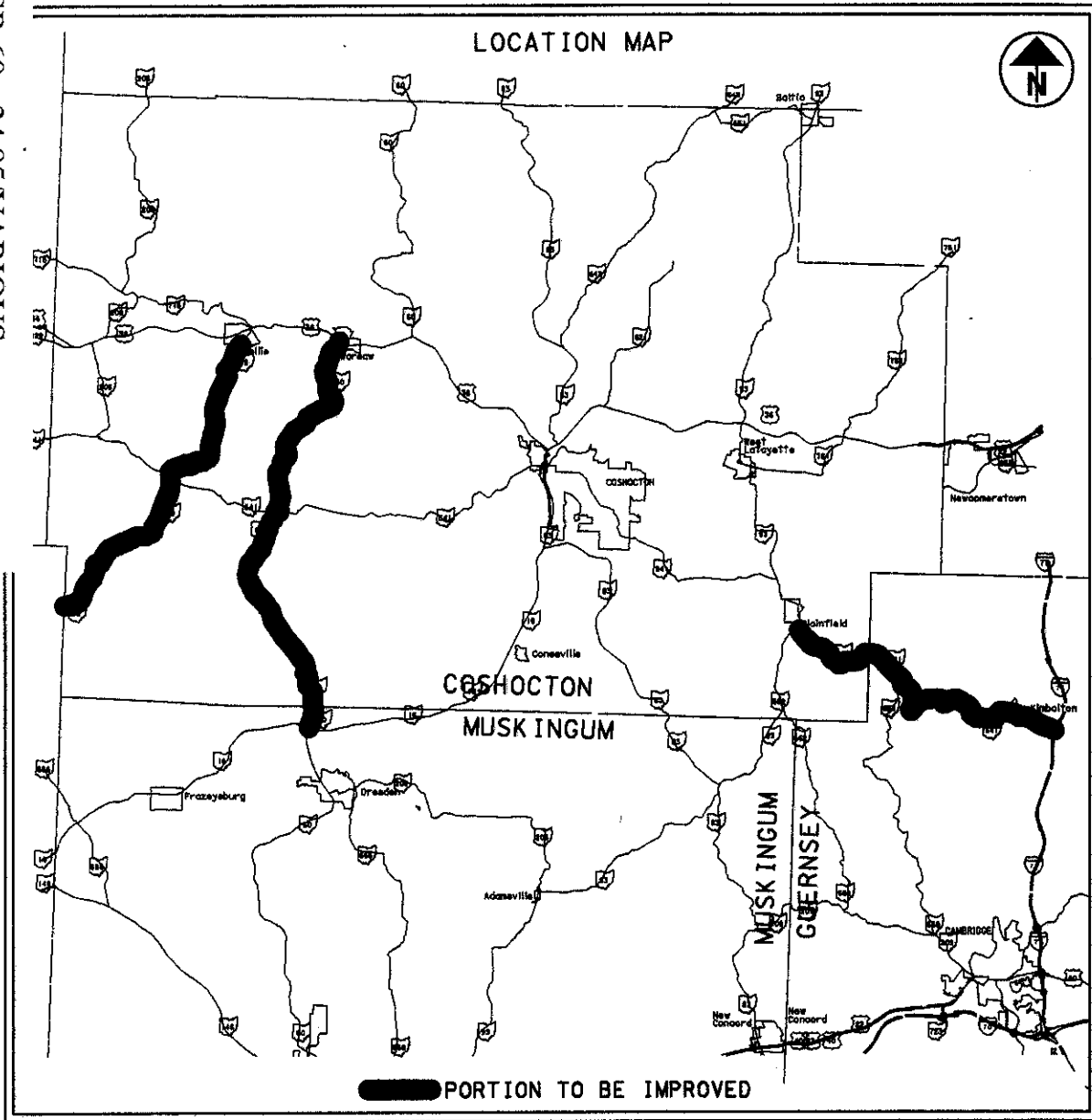


OHIO DEPARTMENT OF TRANSPORTATION

PLAN NO. _____

IUS - SR 60 - 34.85/VARIOUS
 90270
 IST 05
 PID# 19016
 04-28-99



CRACK SEALING PROJECT

SECTION NO.	COUNTY	ROUTE	SECTIONS	PROJECT TERMINI		NET LENGTH MILES	CITY	VILLAGE
				BEGIN	END			
1	MUS	SR 60	(34.85)	34.85	35.60	0.75		
2	COS	SR 60	(0.00-13.82)	0.00	14.61	14.61		
3	COS	SR 79	(0.00-11.74)	0.00	12.40	12.40		
4	COS	SR 54I	(31.87)	31.87	34.90	3.03		
5	GUE	SR 54I	(0.00-8.48)	0.00	8.73	8.73		

INDEX OF SHEETS:

TITLE SHEET	1
CRACK SEALING DATA	2
TRAFFIC CONTROL	3
EDGE/CENTER LINE SUB-SUMMARY	4
GENERAL SUMMARY/NOTES	5

1997 SPECIFICATIONS

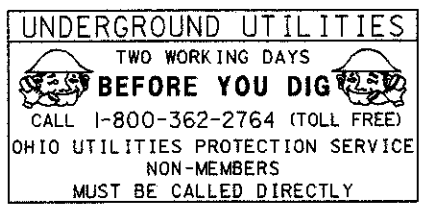
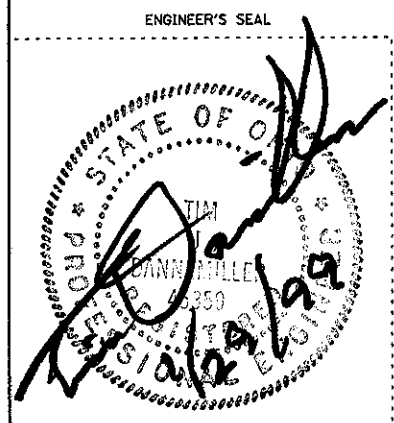
The standard specifications of the State of Ohio, Department of Transportation, including changes and supplemental specifications listed in the proposal shall govern this improvement.

I hereby approve these plans and declare that the making of this improvement will not require the closing to traffic of the highway and that provisions for the maintenance and safety will be as set forth on plans and estimates.

Approved: *Michael D. Coyette*
 Date: 10/29/99 District Deputy Director of Transportation

Approved: *Jordan Proctor*
 Date: 2-4-99 Director, Department of Transportation

STANDARD DRAWINGS		SUPPLEMENTAL SPECIFICATIONS	
MT-95.30	10-10-88	825	01-6-99
MT-97.11	10-4-89		
TC-35.10	8-29-84		

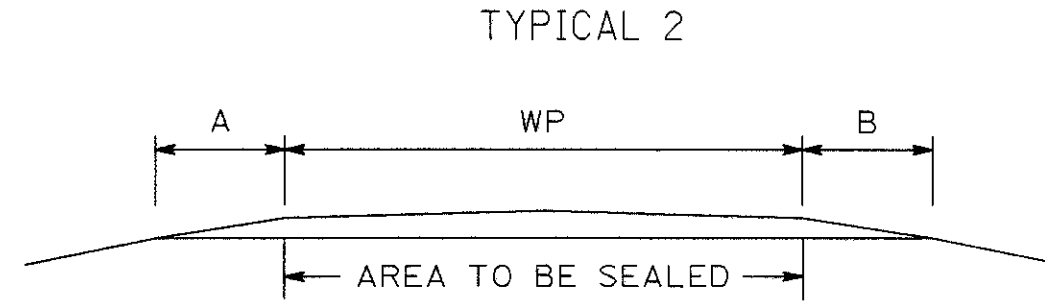
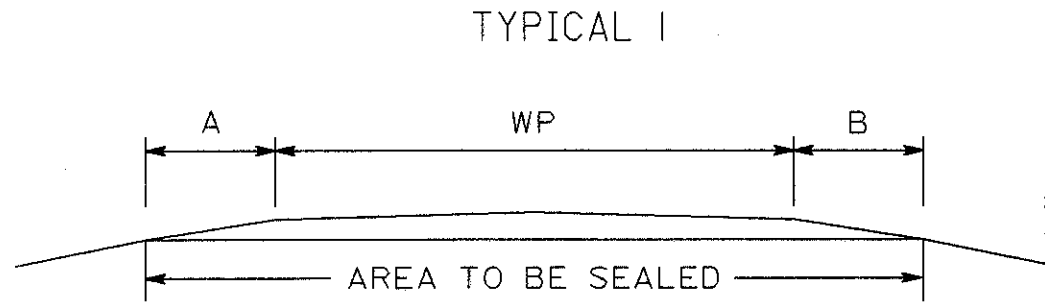


PLAN PREPARED BY:
D5 District
 Production

FEDERAL PROJECT NO. _____
 PID NO. **19016**
 CONSTRUCTION PROJECT NO. _____
 RAILROAD INVOLVEMENT _____
MUS-60-34.85
 1/5

66-28-1 SLW 1000090W

CRACK SEALING



LOCATION	CO.	ROUTE	LOG POINT TO LOG POINT	LENGTH		WP FEET (AVG.)	TYPICAL	EXISTING WIDTH (FEET)				EXISTING TYPE PAVEMENT	825 CRACK SEALING, TYPE II POUND	DESCRIPTION
				MILE	FEET			A	B	C	D			
1	MUS	SR 60	34.85-35.60	0.75	3960	20	2					448	1980	SR 16 TO MUS. CO. LINE
2	COS	SR 60	0.00-14.61	14.61	77141	20	2					448	38570	COS. CO. LINE TO WARSAW CORP.
3	COS	SR 79	0.00-12.40	12.40	65472	18	2					448	49104	COS. CO. LINE TO NELLIE CORP.
4	COS	SR 541	31.87-34.90	3.03	15999	20	2					448	8000	SR 93 TO COS. CO. LINE
5	GUE	SR 541	0.00-8.73	8.73	46094	20	2					448	23047	GUE. CO. LINE TO IR 77

M060001.MCD 1-29-99

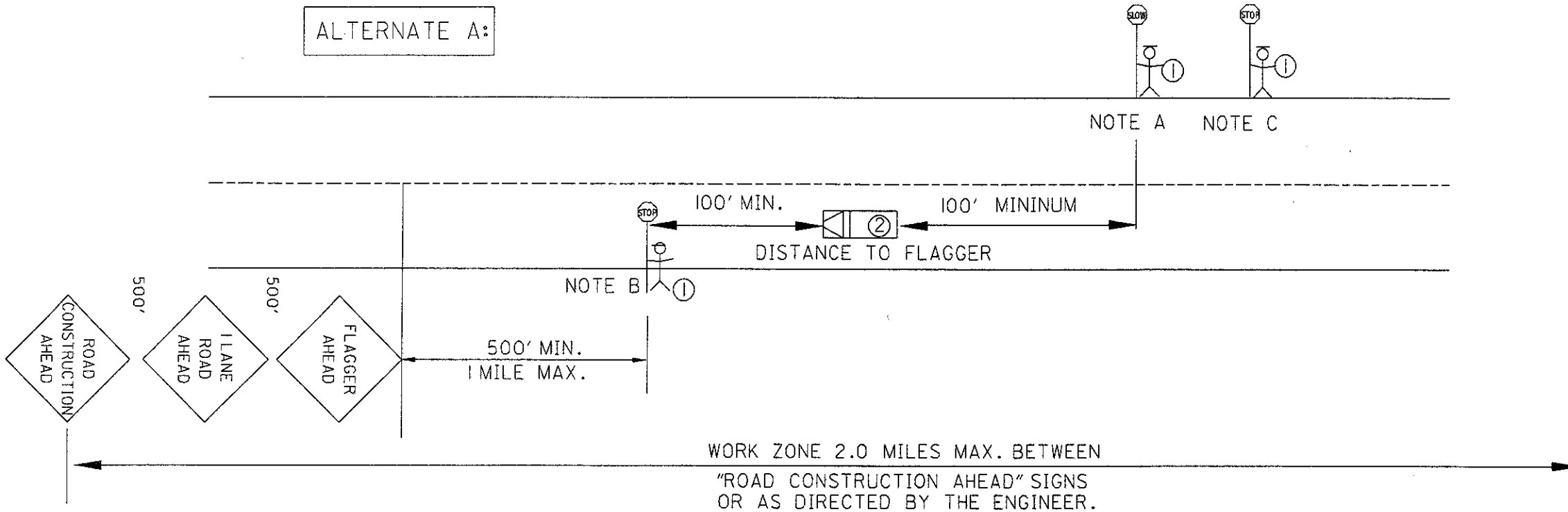
CALCULATED LME CHECKED TJD
CRACK SEALING DATA
MUS-60-34.85
5/2

TRAFFIC CONTROL

PLAN NO. _____

THE TWO DRAWINGS BELOW ILLUSTRATE
ACCEPTABLE ALTERNATIVES TO STD.
DRAWING MT-97.11 FOR MAINTENANCE
OF TRAFFIC ON TWO LANE HIGHWAYS,
WORKING LANE FACING TRAFFIC.

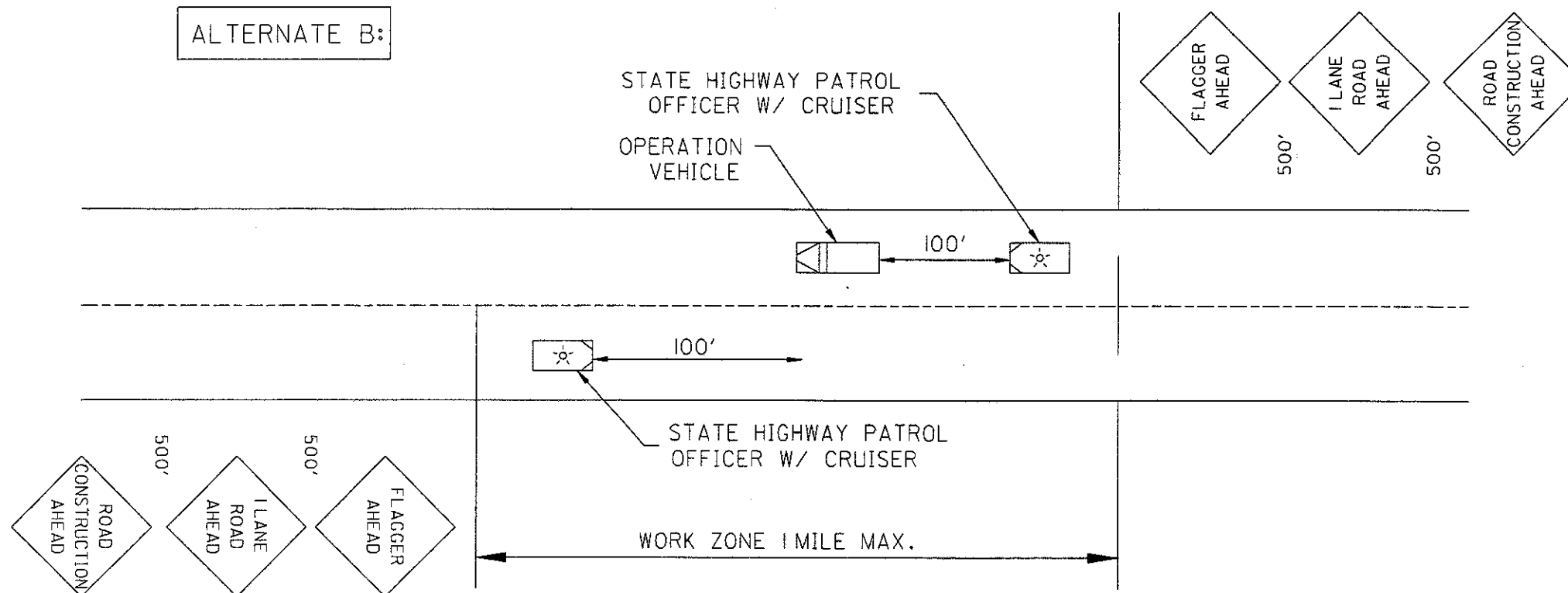
ALTERNATE A:



NOTE:
FLAGGERS SHALL NOT PERFORM WORK
OTHER THAN FLAGGING. THERE SHALL
BE A MINIMUM OF THREE FLAGGERS AT
ALL TIMES.

- 1 = FLAGGER
- 2 = OPERATION VEHICLE
- A = REAR FLAGGER GUIDES TRAFFIC
BACK TO PROPER LANE.
- B = FRONT FLAGGER GUIDES TRAFFIC
THROUGH ZONE BACK TO REAR
FLAGGER.
- C = ADVANCE MAN WILL PROVIDE
ADVANCE WARNING TO SLOW
ONCOMING TRAFFIC WHEN
SITUATIONS WARRANTS (SUCH
AS ONCOMING CURVE, MAJOR
INTERSECTING ROADS, AND
OTHER VISUAL OBSTRUCTIONS).
FLAGGERS MUST USE STOP/SLOW
AND BE EQUIPPED WITH 2-WAY
RADIOS.

ALTERNATE B:



NOTE:
PATROL OFFICERS SHALL PERFORM
ALL FLAGGING DUTIES.

EDGE LINE SUB-SUMMARY

LOCATION	CO.	ROUTE	S.L.M.		WHITE EDGE LINE QU.			YELLOW EDGE LINE QU.			PARTICIPATION TYPE				TOTAL EDGE LINE MILE	REMARKS
			FROM	TO	MILE	HIGHWAY	RAMP	MILE	HIGHWAY	RAMP	IRG	FG	RSG	NON FED STATE		
1	MUS	SR 60	34.85	35.60	1.50									1.50	SR 16 TO MUS. CO. LINE	
2	COS	SR 60	0.00	14.61	29.22									29.22	COS. CO. LINE TO WARSAW CORP.	
3	COS	SR 79	0.00	12.40	24.80									24.80	COS. CO. LINE TO NELLIE CORP.	
4	COS	SR 541	31.87	34.90	6.06									6.06	SR 93 TO COS, CO. LINE	
5	GUE	SR 541	0.00	8.73	17.46									17.46	GUE. CO. LINE TO IR 77	

CENTER LINE SUB-SUMMARY

QUANTITIES INCLUDE CL AROUND OUTSIDE OF PAINTED GORE

LOCATION	CO.	ROUTE	S.L.M.		CENTER LINES QUANTITIES		PARTICIPATION TYPE				TOTAL CENTER LINE MILE	REMARKS
			FROM	TO	TOTAL MILE	EQUIVALENT SOLID LINE	IRG	FG	RSG	NON FED STATE		
1	MUS	SR 60	34.811	35.566	0.755	1.510					0.755	SR 16 TO MUS. CO. LINE
2	COS	SR 60	0.000	14.451	14.451	26.366					14.451	COS. CO. LINE TO WARSAW CORP.
3	COS	SR 79	0.000	12.393	12.393	20.930					12.393	COS. CO. LINE TO NELLIE CORP.
4	COS	SR 541	31.870	34.881	3.011	5.756					3.011	SR 93 TO COS, CO. LINE
5	GUE	SR 541	0.000	8.358	8.358	13.580					8.358	GUE. CO. LINE TO IR 77

TOTALS CARRIED TO GENERAL SUMMARY

P0600001.TEL 1-29-99

CALCULATED LIME CHECKED TJD

EDGE/CENTER LINE SUB-SUMMARY

MUS-60-34.85

5/4

GENERAL SUMMARY

L O C A T I O N # 1	L O C A T I O N # 2	L O C A T I O N # 3	L O C A T I O N # 4	L O C A T I O N # 5	I T E M	I T E M E X T. N O.	G R A N D T O T A L	U N I T	D E S C R I P T I O N
					614	11000		LUMP	MAINTAINING TRAFFIC
					624	10000		LUMP	MOBILIZATION
1980	38570	49104	8000	23047	825	00104	120701	POUNDS	CRACK SEALING, TYPE II
1.50	29.22	24.80	6.06	17.46	642	00100	79.04	MILE	EDGE LINE, TYPE I
0.755	14.451	12.393	3.011	8.358	642	00300	38.213	MILE	CENTER LINE, TYPE I

NOTES:

1. MOST OF THE CRACKS WHICH SHALL BE SEALED TRAVEL EITHER PARALLEL (LONGITUDINAL) OR PERPENDICULAR (TRANSVERSE) TO THE ROADWAY. LOCATIONS OF EXTENSIVE RANDOM CRACKING I.E., ALLIGATOR CRACKING, SHALL NOT BE COVERED WITH CRACK SEAL. ONLY THE PERIMETER OF THESE FRACTURED AREAS SHALL BE SEALED. IF THERE IS TO BE ANY ADDITIONAL SEALING AT THESE LOCATIONS, IT SHALL BE AS DIRECTED BY THE ENGINEER.

THE INTENT IS TO SEAL LINEARLY ALONG CRACKS IN THE PAVEMENT WITHOUT CREATING BLOCKED AREAS COVERED WITH SEALER.
2. DO NOT SEAL ON WORDS OR SYMBOLS.
DO NOT SEAL ON RAISED PAVEMENT MARKERS.
DO NOT SEAL ANY CRACK WIDTH THAT IS UNDER 1/4 INCHES OR EXCEEDS 3/4 INCHES.
FHWA-TS-84-221
NO ALLIGATOR CRACKING SHALL BE SEALED.
3. REFERENCE OHIO SUPPLEMENTAL SPECIFICATION 825.08 1-6-99
WIDTH OF BAND OF SEALANT ON THE PAVEMENT SURFACE IN EXCESS OF 4 INCHES WILL NOT BE ACCEPTABLE.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL CITIES AND VILLAGES AFFECTED BY ANY WORK AT LEAST 30 DAYS PRIOR TO PERFORMING WORK WITHIN CORPORATION LIMITS.

