

SEQUENCE OF CONSTRUCTION

U.S. 42 PRE-PHASE
REMOVE TRENCH DRAIN IN THE MEDIAN OF U.S. 42.

RAMP C PRE-PHASE
CONSTRUCT TEMPORARY PAVEMENT ON SOUTH SIDE OF RAMP C.

U.S. 42 STAGE 1
SHIFT TRAFFIC TO WEST SIDE OF U.S. 42. CONSTRUCT TURN LANE FOR RAMP C.

RAMP C PHASE 1
SHIFT TRAFFIC TO SOUTH SIDE OF RAMP C. REMOVE ISLAND AT RAMP ENTRANCE. CONSTRUCT NORTH SIDE OF RAMP C. CONSTRUCT PROPOSED GUARD RAIL ON NORTH SIDE OF RAMP C.

RAMP D PHASE 1
SHIFT TRAFFIC TO NORTH SIDE OF RAMP D. CONSTRUCT SOUTH SIDE OF RAMP D.

RAMP C PHASE 2
SHIFT TRAFFIC TO NORTH SIDE OF RAMP C. CONSTRUCT SOUTH SIDE OF RAMP C. CONSTRUCT PROPOSED GUARD RAIL ON NORTH SIDE OF RAMP C.

RAMP D PHASE 2
SHIFT TRAFFIC TO NORTH SIDE OF RAMP D. CONSTRUCT SOUTH SIDE OF RAMP D. INSTALL PROPOSED GUARD RAIL ON SOUTH SIDE OF RAMP D.

U.S. 42 STAGE 2
SHIFT TRAFFIC TO EAST SIDE OF U.S. 42. CONSTRUCT HAUCK ROAD DECELERATION LANE. CONSTRUCT RAMP C DECELERATION LANE.

RAMP A PHASE 1
SHIFT TRAFFIC TO SOUTH SIDE OF RAMP A. CONSTRUCT NORTH SIDE OF PROPOSED RAMP A. INSTALL PROPOSED GUARD RAIL ON NORTH SIDE OF RAMP A.

RAMP B PHASE 1
SHIFT TRAFFIC TO NORTH SIDE OF RAMP B. CONSTRUCT SOUTH SIDE OF RAMP B.

RAMP A PHASE 2
SHIFT TRAFFIC TO NORTH SIDE OF RAMP A. CONSTRUCT SOUTH SIDE OF RAMP A.

RAMP B PHASE 2
SHIFT TRAFFIC TO SOUTH SIDE OF RAMP B. CONSTRUCT NORTH SIDE OF RAMP B.

THE CONTRACTOR MAY CHOSE TO BUILD US 42 STAGE 2 AND ASSOCIATED RAMPS PRIOR TO US 42 STAGE 1 AND RAMPS.

INTERIM COMPLETION REQUIREMENTS

THE PROJECT HAS AN INTERIM COMPLETION DATE OF NOVEMBER 15, 2021. ON OR BEFORE THE INTERIM COMPLETION DATE;

FROM 843+25 TO 853+64: ALL WORK SHALL BE COMPLETED, EXCEPT INSTALLING PROPOSED TRAFFIC SIGNALS ON PROPOSED STRAIN POLES. THE ROADWAY SHALL BE PLACED IN THE FINAL CONDITION, ALL PERMANENT PAVEMENT MARKINGS AND RPMS IN PLACE AND OPEN TO TRAFFIC. TO CLARIFY, ALL STRAIN POLE FOUNDATIONS SHALL BE COMPLETED BY THE INTERIM COMPLETION DATE.

FROM 853+64 TO 856+12: ALL WORK SHALL BE COMPLETED, EXCEPT INSTALLING PROPOSED TRAFFIC SIGNALS ON PROPOSED STRAIN POLES AND RESURFACING THE ROADWAY. THE ROADWAY SHALL BE PLACED IN THE FINAL ALIGNMENT, WORK ZONE PAVEMENT MARKINGS AND RPMS IN PLACE AND OPEN TO TRAFFIC. TO CLARIFY, ALL STRAIN POLE FOUNDATIONS SHALL BE COMPLETED BY THE INTERIM COMPLETION DATE. PAVEMENT DROP OFFS ARE NOT PERMITTED BEYOND THE INTERIM COMPLETION DATE, THE WORK SHALL BE COMPLETED FLUSH WITH EXISTING OR PROPOSED SURFACE COURSES; ANY TEMPORARY OR WASTED MATERIAL USED TO ELIMINATE A DROP-OFF SHALL BE CONSIDERED INCIDENTAL TO THE LUMP SUM ITEM 614 MAINTAINING TRAFFIC.

FROM 856+12 TO 863+48: THE ROADWAY SHALL BE PLACED IN MAINTENANCE OF TRAFFIC STAGE 2 PHASE 2 CONDITION USING THE EXISTING PAVEMENT AND ALL INTERCONNECT RELOCATION WORK PER SHEET 120 SHALL BE COMPLETED. THE CONTRACT WILL BE SUBJECT DAILY DISINCENTIVES IN THE AMOUNT OF \$3,500 PER DAY FOR FAILURE TO COMPLETE ALL THE REQUIRED WORK, AND ASSOCIATED INCIDENTALS RELATED TO THE WORK. DAILY DISINCENTIVES ARE APPLICABLE TO THE WORK REQUIRED TO THE INTERIM COMPLETION DATE ONLY. THE CONTRACT IS STILL SUBJECT TO LIQUIDATED DAMAGES AS OUTLINED IN CMS 108.07 FOR THE REMAINDER OF THE CONTRACT.

DESIGN AGENCY



DESIGNER

BLD

REVIEWER

KF 06/08/20

PROJECT ID

106411

SHEET TOTAL

13 137

SHEET NUM.							PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
89	103					01/SAF/OT	02/SAF/OT							
TRAFFIC CONTROL CONT.														
129						129		646	10620	129	FT	CHEVRON MARKING		
48						48		646	20300	48	EACH	LANE ARROW		
2						2		646	20350	2	EACH	LANE REDUCTION ARROW		
8						8		646	20320	8	EACH	WRONG WAY ARROW		
2						2		646	20400	2	EACH	WORD ON PAVEMENT, 72"		
782						782		646	20504	782	FT	DOTTED LINE, 6"		
4						4		646	50000	4	EACH	REMOVAL OF PAVEMENT MARKING		
1,977						1,977		646	50100	1,977	FT	REMOVAL OF PAVEMENT MARKING		
TRAFFIC SIGNALS														
	30					30		611	00400	30	FT	4" CONDUIT, TYPE E - UNDERDRAIN FOR PULL BOXES		
	83					83		625	25410	83	FT	CONDUIT, 2", 725.052		
	36					36		625	25606	36	FT	CONDUIT, 4", 725.052		
	119					119		625	29002	119	FT	TRENCH, 24" DEEP		
	11					11		625	31510	11	EACH	PULL BOX REMOVED		
	12					12		625	32000	12	EACH	GROUND ROD		
	7					7		630	79000	7	EACH	SIGN HANGER ASSEMBLY, SPAN WIRE		
	16					16		630	79500	16	EACH	SIGN SUPPORT ASSEMBLY, POLE MOUNTED		
	97					97		630	80100	97	SF	SIGN, FLAT SHEET		
	17					17		632	05006	17	EACH	VEHICULAR SIGNAL HEAD, (LED), 3-SECTION, 12" LENS, 1-WAY, POLYCARBONATEVEHICULAR SIGNAL HEAD, (LED), 3-SECTION, 12" LENS, 1-WAY, POLYCARBONATE, BLACK	101	
	4					4		632	05086	4	EACH	VEHICULAR SIGNAL HEAD, (LED), 5-SECTION, 12" LENS, 1-WAY, POLYCARBONATEVEHICULAR SIGNAL HEAD, (LED), 5-SECTION, 12" LENS, 1-WAY, POLYCARBONATE, BLACK	101	
	1					1		632	05086	1	EACH	VEHICULAR SIGNAL HEAD, (LED), 5-SECTION, 12" LENS, 1-WAY, POLYCARBONATEVEHICULAR SIGNAL HEAD, (LED), 5-SECTION, 12" LENS, 1-WAY, POLYCARBONATE, YELLOW	101	
	6					6		632	20731	6	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN	101	
	22					22		632	25000	22	EACH	COVERING OF VEHICULAR SIGNAL HEAD		
	6					6		632	25010	6	EACH	COVERING OF PEDESTRIAN SIGNAL HEAD		
	4					4		632	26001	4	EACH	PEDESTRIAN PUSHBUTTON, AS PER PLAN	101	
	5					5		632	26501	5	EACH	DETECTOR LOOP, AS PER PLAN	102	
	677					677		632	30400	677	FT	MESSENGER WIRE, 7 STRAND, 1/2" DIAMETER WITH ACCESSORIES		
	677					677		632	30600	677	FT	TETHER WIRE, WITH ACCESSORIES		
	650					650		632	40300	650	FT	SIGNAL CABLE, 3 CONDUCTOR, NO. 14 AWG		
	930					930		632	40500	930	FT	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG		
	3,156					3,156		632	40700	3,156	FT	SIGNAL CABLE, 7 CONDUCTOR, NO. 14 AWG		
	8					8		632	64010	8	EACH	SIGNAL SUPPORT FOUNDATION		
	2					2		632	64020	2	EACH	PEDESTAL FOUNDATION		
	150					150		632	68200	150	FT	POWER CABLE, 2 CONDUCTOR, NO. 6 AWG		
	155					155		632	69800	155	FT	SERVICE CABLE, 3 CONDUCTOR, NO. 6 AWG		
	2					2		632	70000	2	EACH	POWER SERVICE	101	
	3					3		632	86130	3	EACH	STRAIN POLE, TYPE TC-81.11, DESIGN 10		
	1					1		632	86131	1	EACH	STRAIN POLE, TYPE TC-81.11, DESIGN 10, AS PER PLAN	101	
	2					2		632	86140	2	EACH	STRAIN POLE, TYPE TC-81.11, DESIGN 12		
	1					1		632	86141	1	EACH	STRAIN POLE, TYPE TC-81.11, DESIGN 12, AS PER PLAN	101	
	1					1		632	86150	1	EACH	STRAIN POLE, TYPE TC-81.11, DESIGN 13		
	1					1		632	89600	1	EACH	PEDESTAL, 8"		
	1					1		632	89800	1	EACH	PEDESTAL, 8" TRANSFORMER BASE		
	1					1		632	90020	1	EACH	REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM - SIGNAL HEAD	101	
	3					3		632	90020	3	EACH	REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM - PEDESTRIAN AND AND PUSHBUTTON	101	
	100					100		632	90030	100	FT	REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM - 5C SIGNAL CABLE		
	2					2		632	90101	2	EACH	REMOVAL OF TRAFFIC SIGNAL INSTALLATION, AS PER PLAN	101	
	162					162		632	90500	162	FT	SIGNALIZATION, MISC.: UNLASH AND RELASH MESSENGER WIRE	101	
	2					2		633	65520	2	EACH	CABINET, TYPE 332		
	2					2		633	67100	2	EACH	CABINET FOUNDATION		
	2					2		633	67200	2	EACH	CONTROLLER WORK PAD		
	2					2		633	75001	2	EACH	UNINTERRUPTIBLE POWER SUPPLY (UPS), 1000 WATT, AS PER PLAN	102	
	8					8		809	69110	8	EACH	STOP LINE AND ADVANCE RADAR DETECTION	102	
	2					2		809	69123	2	EACH	ATC V6.24 CONTROLLER, AS PER PLAN	102	

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER
MJL

REVIEWER
JAS 06/08/20

PROJECT ID
106411

SHEET TOTAL
33 | 137

SEEDING AND MULCHING

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

659, SOIL ANALYSIS TEST	2 EACH
659, TOPSOIL	988 CU. YD.
659, SEEDING AND MULCHING	8895 SQ. YD.
659, REPAIR SEEDING AND MULCHING	445 SQ. YD.
659, INTER-SEEDING	445 SQ. YD.
659, COMMERCIAL FERTILIZER	1.24 TON
659, LIME	1.8 ACRES
659, WATER	50 M. GAL.
670, DITCH EROSION PROTECTION MAT, TYPE A	155 SQ. YD.

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

PAVEMENT RESTORATION FOR PIPE INSTALLATIONS AND/OR REMOVALS

THE FOLLOWING QUANTITY HAS BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION AND/OR REMOVAL OF PIPES.

ITEM 301 - ASPHALT CONCRETE BASE, PG64-22 **55 CU. YDS.**

THE ABOVE QUANTITY IS BASED ON A 301 THICKNESS OF 8 INCHES AND A PAVEMENT RESTORATION WIDTH THAT INCLUDES THE TRENCH WIDTH PLUS TWO FEET ON EACH SIDE OF THE TRENCH.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

MANHOLES, CATCH BASINS AND INLETS REMOVED OR ABANDONED

ALL CASTINGS SHALL BE CAREFULLY REMOVED AND STORED WITHIN THE RIGHT OF WAY FOR SALVAGE BY CITY OF SHARONVILLE FORCES.

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 202 ITEM.

ITEM SPECIAL - MAILBOX REMOVED AND RESET

THIS WORK SHALL CONSIST OF REMOVING AND DISPOSING OF THE EXISTING MAILBOX SUPPORT AND RELOCATING THE EXISTING MAILBOX TO A NEW SUPPORT. FURNISH AND ERECT THE NEW MAILBOX SUPPORT WITH ANY ASSOCIATED MOUNTING HARDWARE IN ACCORDANCE WITH PLAN DETAILS, AND ATTACH THE MAILBOX AT LOCATIONS SPECIFIED IN THE PLAN, OR OTHERWISE ESTABLISHED BY THE ENGINEER.

WOOD POSTS SHALL BE NOMINAL 4 INCHES BY 4 INCHES SQUARE OR 4.5 INCHES DIAMETER ROUND, AND CONFORM TO 710.14.

STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2 INCHES I.D., AND CONFORM TO AASHTO M 181.

ALL HARDWARE INCLUDING BUT NOT LIMITED TO PLATES, SCREWS, BOLTS, AND ETC. SHALL BE COMMERCIAL-GRADE GALVANIZED STEEL.

POSTS SHALL BE SET PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO BOXES MAY BE MOUNTED ON A SINGLE POST.

THE MAILBOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS, AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION.

DUE CARE SHALL BE EXERCISED DURING REMOVAL OF THE EXISTING MAILBOX, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POST MASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO A NEW LOCATION.

PAYMENT UNDER THIS ITEM SHALL BE LIMITED TO FINAL PERMANENT INSTALLATIONS. TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 107.10. HOWEVER, THE SAME MATERIAL AND SIZE LIMITATIONS AS FOR PERMANENT INSTALLATIONS SHALL APPLY.

THE RELOCATED MAILBOX, COMPLETE IN PLACE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH, FOR ITEM SPECIAL MAILBOX REMOVED AND RESET.

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES (CONT.)

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEM.

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE CITY, REPRESENTATIVES OF THE CITY AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE CITY.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE CITY.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

ITEM SPECIAL - FILL AND PLUG EXISTING CONDUIT

THIS ITEM SHALL CONSIST OF THE CONSTRUCTION OF BULKHEADS IN AN EXISTING CONDUIT <= 24" φ AND FILLING THE AREA THUS SEALED OFF WITH ITEM 613, SAND OR OTHER MATERIAL APPROVED BY THE ENGINEER.

BULKHEADS SHALL BE LOCATED AT THE LIMITS OF THE AREA TO BE FILLED AS INDICATED ON THE PLANS. THE BULKHEADS

ITEM SPECIAL - FILL AND PLUG EXISTING CONDUIT (CONT.)

SHALL CONSIST OF BRICK OR CONCRETE MASONRY WITH A MINIMUM THICKNESS OF 12 INCHES.

THE FILL MATERIAL SHALL BE PUMPED INTO PLACE, OR PLACED BY OTHER MEANS APPROVED BY THE ENGINEER, SO THAT, AFTER SETTLEMENT, AT LEAST 90 PERCENT OF THE CROSSSECTIONAL AREA OF THE CONDUIT, FOR ITS ENTIRE LENGTH, SHALL BE FILLED. THE LENGTH OF FILLED AND PLUGGED CONDUIT TO BE PAID FOR SHALL BE THE ACTUAL NUMBER OF FEET (MEASURED ALONG THE CENTERLINE OF EACH CONDUIT FROM OUTER FACE TO OUTER FACE OF BULKHEADS) FILLED AND PLUGGED AS DESCRIBED ABOVE.

IN LIEU OF FILLING AND PLUGGING THE EXISTING CONDUIT, THE PIPE MAY BE CRUSHED AND BACKFILLED IN ACCORDANCE WITH THE PROVISIONS OF 203, OR IT MAY BE REMOVED. THE LENGTH, MEASURED AS PROVIDED ABOVE, SHALL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR, ITEM SPECIAL, FILL AND PLUG EXISTING CONDUIT.

EXISTING SUBSURFACE DRAINAGE

PROVIDE UNOBSTRUCTED OUTLETS FOR ALL EXISTING UNDERDRAINS OR AGGREGATE DRAINS ENCOUNTERED DURING CONSTRUCTION.

UNDERDRAINS THAT CAN BE CONNECTED TO THE NEW OR EXISTING UNDERDRAINS AT THE END OF THE PROJECT LIMITS AS WELL AS ALL NECESSARY BENDS OR BRANCHES REQUIRED FOR CONNECTION ARE INCLUDED IN THE BASIS OF PAYMENT FOR UNCLASSIFIED PIPE UNDERDRAINS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

ITEM 605, 6" UNCLASSIFIED PIPE UNDERDRAINS	100 FT.
ITEM 611, 6" CONDUIT, TYPE F	50 FT.

ITEM 202 - REMOVAL MISC.: REMOVE PRIVATE SIGN FOR SALVAGE

IN ADDITION TO THE REQUIREMENTS C&MS SECTION 202, THIS ITEM OF WORK SHALL CONSIST OF REMOVING AND SALVAGING AN EXISTING PRIVATE SIGN AND RETURNING THE SIGN TO IT'S OWNER.

DUE CARE SHALL BE EXERCISED DURING REMOVAL OF THE EXISTING SIGN AND SUPPORT (WHEN APPLICABLE). THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY SIGN AND SUPPORT DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE ENGINEER.

PAYMENT FOR THIS ITEM SHALL BE AT THE CONTRACT UNIT BID PRICE PER EACH PRIVATE SIGN REMOVED FOR SALVAGE, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIAL TO COMPLETE THE ABOVE WORK.

ENVIRONMENTAL COMMITMENT - REGULATED MATERIALS AT 3580 HAUCK ROAD

THE SOILS FROM RM-015, GILKEY WINDOW COMPANY, 3580 HAUCK ROAD, MAY BE REUSED ON THE PROJECT. HOWEVER, IF THESE SOILS CANNOT BE REUSED, THEY ARE TO BE DISPOSED OF AS A SOLID WASTE. THE FOLLOWING QUANTITY PROVIDED FOR REMOVAL OF SOLID WASTE FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL INCLUDE ALL LABOR, MATERIALS, AND INCIDENTALS NECESSARY FOR THE REMOVAL OF SOLID WASTE

ITEM 690 SPECIAL - WORK INVOLVING SOLID WASTE 370 TONS

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CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR THE ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED IN TO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THE PROJECT.

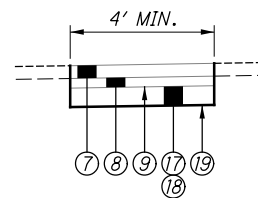
ITEM 253 - PAVEMENT REPAIR, AS PER PLAN

THE FOLLOWING QUANTITY IS PROVIDED FOR FULL DEPTH PAVEMENT REPAIR FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL INCLUDE SAWCUTTING AND REMOVAL OF EXISTING PAVEMENT, AS WELL AS PROPOSED AS SHOWN IN THE FULL DEPTH PAVEMENT REPAIR BUILD-UP DETAIL.

ITEM 253, PAVEMENT REPAIR, AS PER PLAN 545 CY

PAYMENT FOR THIS WORK SHALL BE MADE AT THE PER CUBIC YARD UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK.

FULL DEPTH PAVEMENT REPAIR DETAIL



- ⑦ ITEM 301 - 8" ASPHALT CONCRETE BASE, PG64-22
- ⑧ ITEM 304 - 6" AGGREGATE BASE
- ⑨ ITEM 204 - SUBGRADE COMPACTION
- ⑰ ITEM 204 - EXCAVATION OF SUBGRADE (12" DEPTH)
- ⑱ ITEM 204 - GRANULAR MATERIAL, TYPE B (12" DEPTH)
- ⑲ ITEM 204 - GEOGRID

MANHOLES AND VALVES ADJUSTED TO GRADE (PRIVATELY OWNED)

ALL MANHOLES AND VALVES ENCOUNTERED IN AREAS THAT REQUIRE GRADE ADJUSTMENT WILL BE PERFORMED PRIOR TO THE APPLICATION OF THE SURFACE COURSE BY THE UTILITY OWNER. CONTACT THE UTILITY OWNER 2 WEEKS PRIOR TO WHEN THE ADJUSTMENTS ARE TO BE COMPLETED.

ITEM 611 - MANHOLE ADJUSTED TO GRADE

THIS WORK SHALL CONSIST OF ADJUSTING SANITARY SEWER MANHOLES TO GRADE PRIOR TO THE APPLICATION OF THE SURFACE COURSE AS DIRECTED BY THE ENGINEER. THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 611, MANHOLE ADJUSTED TO GRADE 12 EACH

ITEM 611 - CONDUIT BORED OR JACKED, AS PER PLAN (12" TYPE B)

THIS ITEM OF WORK SHALL CONSIST OF LAYING A 12" DUCTILE IRON PIPE THROUGH A 24" STEEL CASING. WORK SHALL CONFORM TO CMS 625.14 EXCEPT THAT THE CONDUIT SHALL BE FURNISHED THROUGH ANOTHER ITEM.

ALL LABOR, MATERIALS, AND INCIDENTALS NECESSARY FOR THE WORK DESCRIBED ABOVE SHALL BE INCLUDED FOR PAYMENT WITH ITEM 611, CONDUIT BORED OR JACKED, AS PER PLAN (12" TYPE B).

ITEM SPECIAL - GAS VALVE BOX ADJUSTED TO GRADE

THIS WORK SHALL CONSIST OF ADJUSTING GAS VALVE BOXES TO GRADE PRIOR TO THE APPLICATION OF THE SURFACE COURSE AS DIRECTED BY THE ENGINEER. THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM SPECIAL, GAS VALVE BOX ADJUSTED TO GRADE 13 EACH

ITEM SPECIAL MISC.: CONSULTANT FOR CONCRETE QUALITY CONTROL INCLUDING TESTING AND INSPECTION

ALL CONCRETE SHALL BE TESTED. ALL TESTING, INSPECTION AND QUALITY CONTROL FOR CONCRETE, NOT INCLUDED UNDER QC/QA PAY ITEMS, SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL PROVIDE A CONCRETE TESTING CONSULTANT WITH PREVIOUS EXPERIENCE AND FAMILIARITY IN ODOT PROCEDURES, CONCRETE TESTING REQUIREMENTS AND CONCRETE TESTING DOCUMENTATION. AT LEAST 30 DAYS PRIOR TO CONCRETE PLACEMENT, SUBMIT TO THE ENGINEER FOR APPROVAL, THE PROPOSED CONCRETE TESTING CONSULTANT ALONG WITH THE RESUMES OF THE PROPOSED TESTING PERSONNEL.

TESTING CONCRETE FOR STRUCTURES AND PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE PERFORMED AS OUTLINED IN CMS SPECIFICATIONS 455 RESPECTIVELY.

THROUGH THE CONTRACTOR, THE CONSULTANT SHALL BE RESPONSIBLE FOR ENSURING THAT ALL CONCRETE PLACED IS IN ACCORDANCE WITH THE SPECIFICATIONS. SUCH WORK SHALL BE IN ACCORDANCE WITH THE APPLICABLE CONSTRUCTION AND MATERIAL SPECIFICATIONS AND THE ODOT CONSTRUCTION INSPECTION MANUAL OF PROCEDURES FOR CONCRETE. THE CONCRETE CONSULTANT SHALL PROVIDE THE NECESSARY TRAINED TECHNICIAN(S), ALL EQUIPMENT, AND SHALL FURNISH THE PROJECT ENGINEER WITH TWO (2) COPIES OF ALL TEST RESULTS WITHIN 24 HOURS AFTER COMPLETION OF CONCRETE PLACEMENT.

THE TECHNICIAN SHALL BE ACI LEVEL 1 CERTIFIED AND WILL BE REQUIRED TO DEMONSTRATE HIS/HER COMPETENCE AND EXPERIENCE LEVELS TO THE ENGINEER PRIOR TO BEGINNING WORK. THE ENGINEER WILL ORDER THE CONTRACTOR TO REPLACE ANY TECHNICIAN THAT IS NOT VERSED IN THE REQUIRED TESTING PROCEDURE.

THE TECHNICIAN SHALL VERBALLY NOTIFY THE ODOT PROJECT ENGINEER OF ANY FAILING TEST AND SHALL SUBMIT FOLLOW-UP WRITTEN NOTIFICATION TO THE PROJECT ENGINEER OF REMEDIAL ACTION(S) TAKEN. TESTS SHALL BE TAKEN AS SPECIFIED WITHIN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, CONCRETE MANUAL OR APPROPRIATE SUPPLEMENTAL SPECIFICATION AS LISTED IN THE PROPOSAL GOVERNING THE PROJECT. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO MAKE IMMEDIATE CORRECTIONS OR ADJUSTMENTS TO THE CONCRETE MIX VIA DIRECT COMMUNICATION WITH THE CONCRETE SUPPLIER S PLANT PERSONNEL TO MAINTAIN UNINTERRUPTED COMPLIANCE WITH THE SPECIFICATIONS UPON NOTIFICATION OF CONCRETE MIX NON-COMPLIANCE BY THE CONSULTANT TECHNICIAN. THE PROJECT ENGINEER MAY REQUIRE MORE FREQUENT TESTING AS CONDITIONS WARRANT.

UPON COMPLETION OF DAILY CONCRETE PLACEMENT(S), THE CONCRETE CONSULTANT SHALL PROVIDE THE PROJECT ENGINEER WITH DAILY TEST REPORTS, TE-45'S, INSPECTORS DAILY REPORT AND SUPPORTING DOCUMENTATION FOR EACH ITEM OF CONCRETE WORK PERFORMED SEPARATED BY MIX DESIGN. SUBSEQUENTLY, UPON COMPLETION OF AN ENTIRE CONCRETE SPECIFICATION ITEM, THE CONCRETE CONSULTANT SHALL ALSO PROVIDE THE PROJECT ENGINEER WITH TWO (2) COPIES OF AN

ITEM SPECIAL MISC.: CONSULTANT FOR CONCRETE QUALITY CONTROL INCLUDING TESTING AND INSPECTION (CONT.)

ADDITIONAL INSPECTION REPORT BY A REGISTERED PROFESSIONAL ENGINEER, STATE OF OHIO, WHICH CONTAINS THE TESTING-RESULTS SUMMARY FOR EACH ITEM BY CONTRACT REFERENCE NUMBER AND THE CONSULTANT S CONCLUSIONS RELATIVE TO SPECIFICATION COMPLIANCE FOR ALL CONCRETE-TESTING WORK.

THE ODOT PROJECT ENGINEER RESERVES THE RIGHT TO MAKE UNANNOUNCED QUALITY-CONTROL TESTS TO VERIFY PROCEDURES USED AND RESULTS BEING OBTAINED BY THE CONTRACTOR.

THE CONCRETE TECHNICIAN SHALL WORK UNDER THE DIRECTION OF A REGISTERED PROFESSIONAL ENGINEER, STATE OF OHIO, WHO WILL MONITOR THE CONCRETE TEST RESULTS. THE FINAL INSPECTION REPORTS FOR EACH COMPLETED ITEM SHALL BE SIGNED BY A REGISTERED PROFESSIONAL ENGINEER, STATE OF OHIO, CERTIFYING THAT ALL CONCRETE TESTS PROVIDED BY THE CONTRACTOR MET APPLICABLE CONTRACT REQUIREMENTS. A FINAL REPORT ISSUED BY THE CONSULTING FIRM SHALL CONTAIN A CERTIFIED STATEMENT OF COMPLIANCE WITH ODOT SPECIFICATIONS AND ANY OTHER CONCLUSIONS REGARDING THE CONCRETE MATERIALS INCORPORATED INTO THE PROJECT. SUCH STATEMENT SHALL BE SIGNED BY A REGISTERED PROFESSIONAL ENGINEER, STATE OF OHIO. AND, THE CONCRETE CONSULTANT SHALL BE REQUIRED TO ATTEND MONTHLY PROGRESS MEETINGS AS REQUIRED BY THE PROJECT ENGINEER.

ADDITIONALLY, THE CONTRACTOR SHALL BE REQUIRED TO KEEP A POSTED LIST OF BEAM AND CYLINDER IDENTIFICATION NUMBERS FOR THE PURPOSE OF IDENTIFYING THE CORRESPONDING PLACEMENT LOCATION AND CONCRETE SPECIFICATION ITEM.

PAYMENT SHALL BE BID AS LUMP SUM FOR ITEM SPECIAL MISC.: CONSULTANT FOR CONCRETE QUALITY CONTROL INCLUDING TESTING AND INSPECTION. THE ITEM WILL BE PAID FOR AS FOLLOWS:

- UPON APPROVAL OF CONSULTANT 20%
- PROGRESSIVE EQUIVALENT PAYMENTS 50%
- UPON SUBMISSION OF FINAL REPORT 30%.

THE TECHNICIAN SHALL HAVE THE FULL EFFECT AND AUTHORITY OF AN ODOT PROJECT INSPECTOR IN DETERMINING ACCEPTABILITY OF MATERIAL AND CONCRETE PLACEMENT PRACTICES.

TREE CUTTING RESTRICTION

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. THE CONTRACTOR SHALL NOT REMOVE TREES UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THE CONTRACTOR SHALL DEMARCAT CLEARING LIMITS IN THE FIELD TO AVOID ANY UNAUTHORIZED TREE CLEARING. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

ITEM 517 - RAILING, MISC.: 1 1/2" NOMINAL DIAMETER PIPE RAILING

THIS ITEM SHALL CONFORM TO ITEM 517 EXCEPT THAT ALL STEEL RAILING AND POSTS SHALL BE PAINTED WITH BLACK (FEDERAL COLOR NO. 17038) POLYVINYL COATING AFTER FABRICATION AND GALVANIZATION.

RAILING AND POSTS SHALL BE FABRICATED FROM NOMINAL SIZE 1 1/2" DIAMETER 0.145" WALL THICKNESS STEEL PIPE MEETING THE REQUIREMENTS OF THE SPECIFICATION FOR WELDED AND SEAMLESS STEEL PIPE ASTM A 53 STANDARD WEIGHT, SCHEDULE NUMBER 40.

GALVANIZE STEEL RAILING, POSTS, AND FLOOR PLATES AFTER FABRICATION, AS SPECIFIED IN ASTM A 123. RE-GALVANIZE AREAS ON WHICH SPELTER COATING HAS BEEN DAMAGED, AS SPECIFIED IN AASHTO M 36, SECTION 24. METALIZING PROCESS OR REPAIR UNDER THE DIRECTION OF THE ENGINEER WITH STICK-FORM GALVANIZING REPAIR COMPOUND MEETING FEDERAL SPECIFICATION 0-G-93.

ALL LABOR, MATERIALS, AND INCIDENTALS NECESSARY FOR FABRICATION, DELIVERY, AND INSTALLATION OF THE RAILING SHALL BE INCLUDED FOR PAYMENT WITH ITEM 517, RAILING, MISC.: 1 1/2" NOMINAL DIAMETER PIPE RAILING.

DIFFERENCES IN CADD STANDARDS

PART 1 AND PART 2 PLAN SETS WERE CREATED USING DIFFERENT CADD STANDARDS. PART 1 PLANS WERE COMPLETED USING OPEN ROADS CADD STANDARDS AND PART 2 PLANS WERE COMPLETED USING GEOPAK CADD STANDARDS PER DIRECTION FROM ODOT.

ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF REMOVING AN APPROXIMATE 10' (L) X 1' (H) TOP SECTION OF EXISTING RETAINING WALL. THE REMAINING PORTION OF THE EXISTING RETAINING WALL SHALL BE TAPERED TO MATCH THE PROPOSED GRADE.

ALL LABOR, MATERIALS, AND INCIDENTALS NECESSARY FOR REMOVAL, STRAIGHTENING, AND GRADING OF THE EXISTING WALL SHALL BE INCLUDED FOR PAYMENT WITH ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN.

ITEM 870, PREFABRICATED MODULAR BLOCK WALL, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF SS 870, PROVIDE A GRAVITY BLOCK MODULAR RETAINING WALL USING WET-CAST BLOCK UNITS. PROVIDE AND INSTALL PREFABRICATED MODULAR BLOCK UNITS WITH A BROWN RANDOM STONE FINISH. THE MINIMUM RELIEF FOR THE WALL AESTHETIC PATTERN IS 1 IN. PRIOR TO SUBMISSION OF SHOP DRAWINGS AND ORDERING ANY MATERIALS, SUBMIT TO THE ENGINEER A 4 FT. BY 4 FT. MOCK-UP OF THE PROPOSED RETAINING WALL. THE ENGINEER WILL PROVIDE WRITTEN ACCEPTANCE OR DENIAL OF THE PROPOSED UNITS AS OUTLINED IN CMS 105.02.

ADA WAIVER

AN APPROVED ADA DESIGN WAIVER IS REQUIRED ON THIS PROJECT. THE FOLLOWING FEATURES LISTED BELOW CANNOT FEASIBLY BE CONSTRUCTED TO MEET ADA GUIDELINES.

ADA FEATURE	ADA DESIGN WAIVER APPROVAL DATE	SHEET NUMBERS
RMPO006744	01/28/2021	48
RMPO006743	01/28/2021	46

ITEM 614, MAINTAINING TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED 135 CALENDAR DAYS (45 CONSECUTIVE CALENDARS DAYS FOR THE CULVERT REPLACEMENT AND 90 CONSECUTIVE CALENDAR DAYS FOR THE SANITARY SEWER REPLACEMENT, RAILROAD CROSSING, AND HAUCK/READING INTERSECTION IMPROVEMENTS) PRIOR TO PHASE 1, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEET 12. STORM SEWER WORK (D106, D107, D110, D113, D115-D117, D119, D121, D124, D205, AND D208) MAY ALSO BE COMPLETED DURING EITHER DETOUR. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$7,700 PER DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT PER CLOSURE PERIOD.

TRAFFIC SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, AND ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC. DRIVEWAY ACCESS SHALL BE MAINTAINED AT ALL TIMES BY USE OF EXISTING PAVEMENT, THE PROPOSED PAVEMENT, AND TEMPORARY SURFACES COMPOSED OF INTERIM MATERIALS. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS WITH THE SOUTHWEST TRANSIT AUTHORITY METRO (ROUTE 67) TO ENSURE THAT THE EXISTING BUS STOP LOCATED NEAR THE HOLIDAY INN IS PROPERLY RELOCATED DURING CONSTRUCTION.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

DRIVEWAYS SHALL BE CONSTRUCTED PART-WIDTH SO AS TO MAINTAIN BUSINESS ACCESS AT ALL TIMES. IF A SINGLE BUSINESS HAS MULTIPLE DRIVES CONNECTED TO HAUCK ROAD, THEN ONE DRIVE AT A TIME MAY BE CLOSED DURING CONSTRUCTION; WITH THE EXCEPTION OF THE DRIVE AT APPROXIMATE STATION 47+10 WHICH MUST REMAIN OPEN AT ALL TIMES.

THE CONTRACTOR MUST NOTE THAT ANY INTERIM MATERIAL USED FOR PROVIDING DRIVEWAY INGRESS AND EGRESS WILL NOT BE A SEPERATE PAY ITEM AND THE COST OF SAID INTERIM MATERIAL SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC. INTERIM MATERIAL SHALL CONFORM TO ITEM 410, TRAFFIC COMPACTED SURFACE , TYPE A OR B.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

ITEM 615,	ROADS FOR MAINTAINING TRAFFIC	LUMP SUM
ITEM 615,	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	340 SQ. YD.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES IN LOCATIONS AS SPECIFIED IN THE PLANS AND STANDARD CONSTRUCTION DRAWINGS.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE

ITEM 614, MAINTAINING TRAFFIC (CONT.)

CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP &	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
ROAD	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
CLOSURES	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

TEMPORARY DRAINAGE ITEMS

TEMPORARY DRAINAGE ITEMS ARE LABELED ON THE MAINTENANCE OF TRAFFIC PLANS. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS FOR INSTALLATION AND REMOVAL OF THE TEMPORARY DRAINAGE ITEMS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV), THE DISTRICT PUBLIC INFORMATION OFFICE (PIO) (DOT.D08.PIO@DOT.OHIO.GOV), THE DISTRICT PERMIT SECTION (D08.PERMITS@DOT.OHIO.GOV), AND THE CONTACT FOR METRO/SORTA (TED MEYER, 513-632-7427, TCMeyer@metro-go.com). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 12 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

EARTHWORK FOR MAINTAINING TRAFFIC

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE PLAN FOR INFORMATION ONLY:

EXCAVATION FOR MAINTAINING TRAFFIC	1,270 CU. YD.
EMBANKMENT FOR MAINTAINING TRAFFIC	980 CU. YD.

WHEN UNDERCUTS ARE NECESSARY FOR MAINLINE PAVEMENT OR EMBANKMENT CONSTRUCTION, EVALUATE THE NEED FOR TEMPORARY ROAD UNDERCUTS IF WITHIN A CLOSE PROXIMITY TO THE MAINLINE UNDERCUTS. A GEOTECHNICAL EVALUATION SHOULD BE CONSIDERED TO DETERMINE IF THE EXISTING SOIL CONDITIONS ARE ADEQUATE TO SUPPORT THE TEMPORARY ROAD. ADDITIONAL SOIL BORINGS ALONG THE TEMPORARY ROAD ARE NOT NORMALLY REQUIRED.

TEMPORARY PAVEMENT WEDGE

TEMPORARY PAVEMENT WEDGES SHALL BE PROVIDED AT ALL TIMES WHERE TRAFFIC IS REQUIRED TO TRAVEL FROM OR ONTO A PAVEMENT SURFACE OF A DIFFERENT ELEVATION, AROUND MANHOLES, AT CATCH BASINS, ETC. THE MINIMUM SLOPE OF THE TEMPORARY PAVEMENT WEDGE SHALL BE 3:1 ALONG LONGITUDINAL JOINTS AND 120:1 AT TRANSVERSE JOINTS. THESE WEDGES SHALL BE REMOVED PRIOR TO PLACING THE SPECIFIED PAVEMENT COURSE. PAYMENT FOR ALL WORK, MATERIALS, ETC. ASSOCIATED WITH THIS ITEM SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616,	WATER	27 M. GAL.
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ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET 12 OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614, WORK ZONE CENTERLINE, CLASS I, 642 PAINT	1.27 MILE
ITEM 614, WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	3.03 MILE
ITEM 614, WORK ZONE STOP LINE, CLASS I, 642 PAINT	81 FT
ITEM 614, WORK ZONE, DOTTED LINE, CLASS I, 8", 642 PAINT	226 FT
ITEM 614, WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT	150 FT
ITEM 614, WORK ZONE ISLAND MARKING, CLASS I	56 SF
ITEM 614, WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT	114 FT
ITEM 614, WORK ZONE ARROW, CLASS I, 642 PAINT	14 EACH
ITEM 614, WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	0.03 MILE
ITEM 614, WORK ZONE CENTER LINE, CLASS III, 642 PAINT	1.44 MILE
ITEM 614, WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	0.07 MILE
ITEM 614, WORK ZONE CHANNELIZING LINE, CLASS III, 8", 642 PAINT	730 FT
ITEM 614, WORK ZONE STOP LINE, CLASS III, 642 PAINT	148 FT
ITEM 614, WORK ZONE CROSSWALK LINE, CLASS III, 642 PAINT	682 FT
ITEM 614, WORK ZONE ISLAND MARKING, CLASS III, 642 PAINT	86 SF
ITEM 614, WORK ZONE RAILROAD SYMBOL MARKING, CLASS I, 642 PAINT	3 EACH
ITEM 614, WORK ZONE RAILROAD SYMBOL MARKING, CLASS III, 642 PAINT	3 EACH
ITEM 614, WORK ZONE ARROW, CLASS III, 642 PAINT	33 EACH
ITEM 614, WORK ZONE WORD ON PAVEMENT, 72", CLASS III, 642 PAINT	4 EACH
ITEM 614, WORK ZONE, DOTTED LINE, CLASS III, 8", 642 PAINT	274 FT

ITEM 614, WORK ZONE PAVEMENT MARKINGS, AS PER PLAN (SPRAY THERMOPLASTIC)

THE CONTRACTOR SHALL PLACE THE WORK ZONE PAVEMENT MARKINGS, SPRAY THERMOPLASTIC, AS PER PLAN PER ODOT SPECIFICATION 614.11 AND ODOT SPECIFICATION 648 WITH THE EXCEPTION ODOT SPECIFICATION 648.05 SHALL BE MODIFIED TO ALLOW PLACEMENT OF THE MATERIAL AT A TEMPERATURE OF NOT LESS THAN 35 DEGREES FAHRENHEIT.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS AND AT TIMES AS DIRECTED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS PER THE REQUIREMENTS OF C&MS 614.11.

ITEM 614, WORK ZONE LANE LINE, CLASS I, 6", AS PER PLAN (SPRAY THERMOPLASTIC)	0.03 MILE
ITEM 614, WORK ZONE CENTER LINE, CLASS I, AS PER PLAN (SPRAY THERMOPLASTIC)	1.31 MILE
ITEM 614, WORK ZONE EDGE LINE, CLASS I, 6", AS PER PLAN (SPRAY THERMOPLASTIC)	0.07 MILE
ITEM 614, WORK ZONE CHANNELIZING LINE, CLASS I, 8", AS PER PLAN (SPRAY THERMOPLASTIC)	728 FT
ITEM 614, WORK ZONE DOTTED LINE, CLASS I, AS PER PLAN (SPRAY THERMOPLASTIC)	277 FT

ITEM 622, PORTABLE BARRIER, UNANCHORED, AS PER PLAN

THIS WORK SHALL CONSIST OF FURNISHING, MAINTAINING, AND SUBSEQUENTLY REMOVING A 32-INCH PORTABLE CONCRETE BARRIER. THE CONCRETE BARRIER IS TO BE PLACED A MINIMUM OF 20'-0" FROM THE CENTERLINE OF TRACK, PERPENDICULAR TO HAUCK ROAD, TO PREVENT PUBLIC ACCESS OVER THE AT-GRADE CROSSING WHILE THE CROSSING SURFACE IS BEING UPGRADED BY NORFOLK SOUTHERN FORCES. FOR DETAILS, SEE SCD RM-4.2.

A QUANTITY OF 80 FEET HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THE AMOUNT OF PORTABLE CONCRETE BARRIER NEEDED TO PROTECT BOTH ENTRANCES OF THE RAILROAD CROSSING.

PAYMENT SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT NECESSARY TO PERFORM THE WORK AND SHALL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR ITEM 622, PORTABLE BARRIER, UNANCHORED, AS PER PLAN.

INTERIM COMPLETION DATE 1

THE PROJECT HAS AN INTERIM COMPLETION DATE (1) OF 10/31/2021. ON OR BEFORE THE INTERIM COMPLETION DATE, THE CULVERT REPLACEMENT AND ASSOCIATED WORK AT APPROXIMATE STATION 33+50 REQUIRING THE 45 DAY CLOSURE SHALL BE COMPLETED.

INTERIM COMPLETION DATE 2

THE PROJECT HAS AN INTERIM COMPLETION DATE (2) OF 4/30/2022. ON OR BEFORE THIS INTERIM COMPLETION DATE, ALL MSD WORK WITHIN RAILROAD R/W SHALL BE COMPLETED. WORK SHALL NOT BEGIN UNTIL 4/1/2022

INTERIM COMPLETION DATE 3

THE PROJECT HAS AN INTERIM COMPLETION DATE (3) OF 7/1/2022. ON OR BEFORE THIS COMPLETION DATE, ALL WORK ASSOCIATED WITH THE 60 DAY PORTION OF THE 90 DAY CLOSURE SHALL BE COMPLETED. WORK SHALL NOT BEGIN UNTIL 5/1/2022.

INTERIM COMPLETION REQUIREMENTS

THE CONTRACT WILL BE SUBJECT TO DAILY DISINCENTIVES FOR FAILURE TO COMPLETE ALL THE REQUIRED WORK, AND ASSOCIATED INCIDENTALS RELATED TO THE WORK, AS OUTLINED IN THE TABLE INCLUDED IN THIS NOTE. APPLICATION OF THE DISINCENTIVES WILL BE BASED ON THE OVERALL CONTRACT AMOUNT. DAILY DISINCENTIVES ARE APPLICABLE TO THE WORK REQUIRED TO THE INTERIM COMPLETION DATE ONLY. THE CONTRACT IS STILL SUBJECT TO LIQUIDATED DAMAGES AS OUTLINED IN CMS 108.07 FOR THE REMAINDER OF THE CONTRACT.

SCHEDULE OF DAILY DISINCENTIVES FOR FAILURE TO MEET THE INTERIM COMPLETION REQUIREMENTS		
ORIGINAL CONTRACT AMOUNT (TOTAL AMOUNT AT THE TIME OF BIDDING)		DAILY DISINCENTIVE FOR EACH FULL OR PARTIAL CALENDAR DAY OF TIME OVERRUN BEYOND THE PLAN INTERIM COMPLETION DATE
FROM MORE THAN	TO AND INCLUDING	
\$0.00	\$500,000	\$800
\$500,000	\$1,000,000	\$1,200
\$1,000,000	\$5,000,000	\$2,500
\$5,000,000	\$10,000,000	\$3,500
\$10,000,000	\$50,000,000	\$5,000
OVER \$50,000,000		\$7,500

SEQUENCE OF CONSTRUCTION

PRIOR TO PHASE 1

INSTALL PCMS TO INFORM THE PUBLIC PRIOR TO IMPLEMENTING ANY DETOURS. DETOUR TRAFFIC AS PER THE FIRST DETOUR MAP TO REMOVE EX. CULVERT AT APPROXIMATE STA. 33+50 AND INSTALL NEW CULVERT WITH HEADWALLS, MAKE CHANNEL IMPROVEMENTS, AND INSTALL FULL DEPTH PERMANENT PAVEMENT OVER PROPOSED CULVERT. THIS WORK SHALL BE COMPLETED PER INTERIM COMPLETION DATE (1) REQUIREMENTS.

DETOUR TRAFFIC AS PER THE SECOND DETOUR MAP. THIS DETOUR SHALL BE IN PLACE FOR 90 CONSECUTIVE DAYS. DURING THE FIRST 30 DAYS OF THE 90 DAY DETOUR, COMPLETE THE MSD SANITARY LINE REPLACEMENT WORK WITHIN THE RAILROAD RIGHT OF WAY. THIS WORK SHALL BE COMPLETED PER INTERIM COMPLETION DATE (2) REQUIREMENTS.

DURING THE NEXT 60 DAYS OF THE 90 DAY DETOUR, PERFORM THE RR CROSSING WORK (COMPLETED BY RR CONTRACTOR), CULVERT EXTENSION AND CHANNEL IMPROVEMENTS, AND ROADWAY IMPROVEMENTS BETWEEN READING ROAD TO STATION 13+85. THIS WORK SHALL BE COMPLETED PER INTERIM COMPLETION DATE (3) REQUIREMENTS.

THE RAILROAD CONTRACTOR WILL BE WORKING CONCURRENTLY WITH THIS PROJECT'S WORK ACTIVITIES WITHIN THE RAILROAD RIGHT OF WAY DURING THE 60 DAY CLOSURE. THE RAILROAD WORK INCLUDES: INSTALLATION OF NEW GATES, INSTALLATION OF A CANTILEVER OVERHEAD SIGNAL, WIDENING THE VEHICULAR CROSSING BETWEEN THE TRACKS, AND INSTALLING ASPHALT VEHICULAR CROSSING TO 2' THE OUTSIDE OF THE TRACKS.

PHASE 1

CHANGE PCMS MESSAGES TO INFORM THE PUBLIC OF THE CHANGE IN TRAFFIC OPERATION. CONSTRUCT FULL DEPTH PAVEMENT, CURB, AND SIDEWALK ALONG THE NORTH SIDE OF HAUCK ROAD FROM APPROXIMATE STA. 13+85 TO STA. 47+67 BY DETOURING EASTBOUND TRAFFIC AS PER SHEET 12 AND SHIFTING WESTBOUND TRAFFIC TO THE SOUTH SIDE OF HAUCK ROAD. CONSTRUCT PERMANENT DRIVEWAY PAVEMENT, PERMANENT DRAINAGE FEATURES, PERMANENT SIGNAL IMPROVEMENTS, PERMANENT GUARDRAIL, AND THE PERMANENT RETAINING WALL FROM STA. 15+85 TO STA. 17+00 AS PER THE PHASING PLANS.

CONSTRUCT PAVEMENT FOR MAINTAINING TRAFFIC WITH TEMPORARY CURB FROM APPROXIMATE STA. 51+51 TO STA. 54+27. TEMPORARILY CLOSE THE RIGHT TURN LANE WITHIN THE ABOVE STATION LIMITS, SAW CUT ONE FOOT WITHIN THE EX. EDGE OF PAVEMENT OR UNTIL SOUND PAVEMENT IS FOUND, AND INSTALL TEMPORARY DRAINAGE STRUCTURES TO INTERCEPT EX. 12 INCH PIPES AS PER THE PHASING PLANS.

PHASE 2

REMOVE ALL DETOUR SIGNAGE AND SHIFT BOTH LANES OF TRAFFIC TO THE NORTH SIDE OF HAUCK ROAD AND CONSTRUCT FULL DEPTH PAVEMENT, CURB, AND SIDESLOPE GRADING ALONG THE SOUTH SIDE OF HAUCK ROAD. CONSTRUCT DRAINAGE IMPROVEMENTS ALONG THE SOUTH SIDE OF HAUCK ROAD AND (WHERE APPLICABLE) CONNECT TO PROPOSED STORM SEWER CONSTRUCTED DURING PHASE 1 AS PER THE PHASING PLANS. CONSTRUCT PAVEMENT FOR MAINTAINING TRAFFIC WITH TEMPORARY CURB FROM APPROXIMATE STA. 48+90 TO STA. 50+33 ALONG THE PROPOSED EDGE OF PAVEMENT AS PER THE PHASING PLANS.

SEQUENCE OF CONSTRUCTION (CONT.)

BETWEEN PHASES 2 AND 2A

THE END OF PHASE 2 SHALL MARK THE INTERIM COMPLETION DATE FOR THE END OF THE SEASON. PRIOR TO 11/15/2021, SPRAY THERMOPLASTIC WORK ZONE PAVEMENT MARKINGS SHALL BE APPLIED TO THE PAVEMENT AS PER THE FINAL PERMANENT LANE CONFIGURATION. ALL LANES SHALL BE REOPENED TO TRAFFIC UNTIL THE START OF PHASE 2A DURING THE FOLLOWING CONSTRUCTION SEASON.

PHASE 2A

WESTBOUND TRAFFIC SHALL REMAIN AS PER THE PHASE 2 CONFIGURATION. EASTBOUND TRAFFIC SHALL BE SHIFTED TO THE PROPOSED SOUTH EDGE OF PAVEMENT. CONSTRUCT FULL DEPTH PAVEMENT AND LATERAL DRAINAGE CONNECTIONS AS PER THE PHASING PLANS.

PHASE 2B

EASTBOUND TRAFFIC SHALL REMAIN AS PER THE PHASE 3 CONFIGURATION. WESTBOUND TRAFFIC SHALL BE SHIFTED TO THE SOUTH SO THAT THERE IS NO LONGER A SPLIT BETWEEN EASTBOUND AND WESTBOUND TRAFFIC LANES. REMOVE ALL TEMPORARY NORTHERN SURFACES AND DRAINAGE STRUCTURES. CONSTRUCT NORTHERN FULL DEPTH PAVEMENT, CURBS, SIDEWALK, CURB CATCH BASINS, AND SIDESLOPE GRADING AS PER THE PHASING PLANS.

PHASE 3

TRAFFIC SHALL USE THE FINAL PERMANENT LANE CONFIGURATION. DURING NON-PEAK OR OVERNIGHT HOURS, THE CONTRACTOR SHALL CLOSE THE EASTBOUND LANE FROM APPROXIMATE STA. 48+90 TO STA. 50+33 SO AS TO REMOVE THE SOUTHERN TEMPORARY PAVEMENT AND CURB ALONG THE PROPOSED EDGE OF PAVEMENT. THE SAME LANE CLOSURE SHALL BE USED TO CONSTRUCT THE PROPOSED CURB AND SIDESLOPE GRADING BETWEEN THE ABOVE STATIONS. TWO-WAY TRAFFIC SHALL BE MAINTAINED AS PER SCD MT-95.61, BY USING THE PERMANENT WESTBOUND RIGHT TURN LANE FOR WESTBOUND THRU TRAFFIC AND THE PERMANENT WESTBOUND THRU LANE FOR EASTBOUND THRU TRAFFIC.

PHASE 4

TRAFFIC SHALL USE THE FINAL PERMANENT LANE CONFIGURATION. DURING NON-PEAK OR OVERNIGHT HOURS, THE CONTRACTOR SHALL MAINTAIN A SINGLE TWO-WAY FLAGGER CONTROLLED LANE AS PER SCD MT-97.12. COMPLETELY RESURFACE HAUCK ROAD AND APPLY FINAL PAVEMENT MARKINGS.

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MAINTENANCE OF TRAFFIC GENERAL NOTES

**HAM-HAUCK ROAD -
WIDENING PART 2**

11A
151

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SHEET NUM.					PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
	110	111	112	120	CALC	03/MPO/OT							05/SAF/OT
WATER WORK (CONT.)													
						11		SPECIAL	63820454	11	FT	16" STEEL PIPE ENCASUREMENT, OPEN CUT, CINCINNATI SPEC 1108	101-109
						45		SPECIAL	63820466	45	FT	24" STEEL PIPE ENCASUREMENT, OPEN CUT, CINCINNATI SPEC 1108	101-109
						14		SPECIAL	63820498	14	EACH	VALVE BOX, COMPLETE, CINCINNATI SPEC 1116	101-109
						4		SPECIAL	63820500	4	EACH	VALVE BOX ADJUSTED TO GRADE, CINCINNATI SPEC 604	101-109
						5		SPECIAL	63820742	5	EACH	1" AIR RELEASE VALVE WITH VALVE BOX, COMPLETE, , CINCINNATI SPEC 1116	101-109
						8		SPECIAL	63820750	8	EACH	6" FIRE HYDRANT, CINCINNATI SPEC 1112	101-109
						8		SPECIAL	63820760	8	EACH	FIRE HYDRANT REMOVED AND DISPOSED OF, CINCINNATI SPEC 1114	101-109
						2		SPECIAL	63820762	2	EACH	FIRE HYDRANT SERVICE LINE EXTENDED AND ADJUSTED TO GRADE, 8" LONG, CINCINNATI SPEC 1115	101-109
						3		SPECIAL	63820762	3	EACH	FIRE HYDRANT SERVICE LINE EXTENDED AND ADJUSTED TO GRADE, 12" LONG, CINCINNATI SPEC 1115	101-109
						3		SPECIAL	63820762	3	EACH	FIRE HYDRANT SERVICE LINE EXTENDED AND ADJUSTED TO GRADE, 18" LONG, CINCINNATI SPEC 1115	101-109
						181		SPECIAL	63820766	181	FT	3/4" COPPER WATER SERVICE LINE, WITH AQUA SHIELD, CINCINNATI SPEC 1126	101-109
						131		SPECIAL	63820770	131	FT	1" COPPER WATER SERVICE LINE, WITH AQUA SHIELD, CINCINNATI SPEC 1126	101-109
						73		SPECIAL	63820774	73	FT	1 1/2" COPPER WATER SERVICE LINE, WITH AQUA SHIELD, CINCINNATI SPEC 1126	101-109
						1		SPECIAL	63820786	1	FT	RETAP AND RECONNECT WATER SERVICE CONNECTION, CINCINNATI SPEC 1128	101-109
						1		SPECIAL	63830002	1	MBF	SHEETING AND BRACING ORDERED LEFT IN PLACE, CINCINNATI SPEC 637	101-109
						2		638	98000	2	EACH	WATER WORK, MISC.:REMOVING EXISTING MANHOLE CURB AND COVER, CINCINNATI SPEC 1122	101-109
						10		638	98000	10	EACH	WATER WORK, MISC.:REMOVING EXISTING VALVE BOX, CINCINNATI SPEC 1122	101-109
						3		638	98000	3	EACH	WATER WORK, MISC.:REMOVING EXISTING VALVE BOXES COMPLETE, CINCINNATI SPEC 1125	101-109
						12		638	98000	12	EACH	WATER WORK, MISC.:FURNISHING AND INSTALLING CURB AND ROADWAY BOX (RENEW), CINCINNATI SPEC 1131	101-109
						4		638	98000	4	EACH	WATER WORK, MISC.:FURNISHING AND INSTALLING CURB AND ROADWAY BOX (RECONNECT), CINCINNATI SPEC 1131	101-109
						5		638	98000	5	EACH	WATER WORK, MISC.:RELOCATING EXISTING 5/8" FROST-PROOF METER SETTING, CINCINNATI SPEC 1134	101-109
						1		638	98000	1	EACH	WATER WORK, MISC.:RELOCATING EXISTING 3/4" FROST-PROOF METER SETTING, CINCINNATI SPEC 1134	101-109
						2		638	98000	2	EACH	WATER WORK, MISC.:RELOCATING EXISTING 1" FROST-PROOF METER SETTING, CINCINNATI SPEC 1134	101-109
						2		638	98000	2	EACH	WATER WORK, MISC.:RELOCATING EXISTING 2" FROST-PROOF METER SETTING, CINCINNATI SPEC 1134	101-109
						1		638	98000	1	EACH	WATER WORK, MISC.:FURNISHING & INSTALLING 8" METER SETTING APPURT. & CONSTRUCT PIT, CINC. SPEC 1137	101-109
TRAFFIC CONTROL													
					7	5	2	626	00110	7	EACH	BARRIER REFLECTOR, TYPE 2, BIDIRECTIONAL	
	227	80				227	80	630	03100	307	FT	GROUND MOUNTED SUPPORT, NO. 3 POST	
	26					26		630	08520	26	FT	STREET NAME SIGN SUPPORT, NO. 3 POST	
				1		1		630	79100	1	EACH	SIGN HANGER ASSEMBLY, MAST ARM	
	2					2		630	79500	2	EACH	SIGN SUPPORT ASSEMBLY, POLE MOUNTED	
	105.4	40.4		7.5		112.9	40.4	630	80100	153.3	SF	SIGN, FLAT SHEET	
	5	7				5	7	630	84900	12	EACH	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	
	8					8		630	85100	8	EACH	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION	
	8	6				8	6	630	86002	14	EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	
	3					3		630	87500	3	EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	
				0.07		0.07		644	00100	0.07	MILE	EDGE LINE, 4"	
				0.03		0.03		644	00200	0.03	MILE	LANE LINE, 4"	
				1.44		1.32	0.12	644	00300	1.44	MILE	CENTER LINE	
				739		324	415	644	00400	739	FT	CHANNELIZING LINE, 8"	
				148		71	77	644	00500	148	FT	STOP LINE	
				682		437	245	644	00600	682	FT	CROSSWALK LINE	
				86			86	644	00700	86	FT	TRANSVERSE/DIAGONAL LINE	
				3		3		644	01000	3	EACH	RAILROAD SYMBOL MARKING	
				32		25	7	644	01300	32	EACH	LANE ARROW	
				3		1	2	644	01400	3	EACH	WORD ON PAVEMENT, 72"	
				274		121	153	644	01500	274	FT	DOTTED LINE, 4"	
TRAFFIC SIGNALS													
				21		21		625	25408	21	FT	CONDUIT, 2", 725.051	
				12		12		625	25604	12	FT	CONDUIT, 4", 725.051	
				80		80		625	25606	80	FT	CONDUIT, 4", 725.052	
				33		33		625	29002	33	FT	TRENCH, 24" DEEP	
				80		80		625	29400	80	FT	TRENCH IN PAVED AREA	
				2		2		625	30700	2	EACH	PULL BOX, 725.08, 18"	
				1		1		625	32000	1	EACH	GROUND ROD	
				2		2		631	97700	2	EACH	SIGN LIGHTING MISC.: REMOVAL AND REPLACEMENT OF LED BLANKOUT SIGN	118
				4		4		632	20721	4	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, AS PER PLAN	118
				2		2		632	26001	2	EACH	PEDESTRIAN PUSHBUTTON, AS PER PLAN	118

GENERAL SUMMARY

HAM-HAUCK ROAD - WIDENING PART 2

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REF. NO.	SHEET NO.	STATION TO STATION		SIDE	PARTICIPATION	202	202	202	202	202	202	202	202	202	202	202	202	202	202		
						PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	HEADWALL REMOVED	WALK REMOVED	CURB REMOVED	CURB AND GUTTER REMOVED	GUTTER REMOVED	PIPE REMOVED, 24" AND UNDER	PIPE REMOVED OVER 24"	GUARDRAIL REMOVED	MANHOLE REMOVED	CATCH BASIN REMOVED	CATCH BASIN ABANDONED	SPECIAL - FILL AND PLUG EXISTING CONDUIT	FENCE REMOVED		
						SY	EACH	SF	FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	FT	FT		
R1	41	11+23	12+02	LT	03/MPO/OT					86											
R2	41	11+35	11+58	RT	03/MPO/OT					26											
R3	41	12+55	12+84	LT & RT	03/MPO/OT								51								
R4	41	12+77	13+65	LT	03/MPO/OT							85				1					
R5	41	13+88	14+45	LT	03/MPO/OT							58									
R6	41	14+95	15+30	LT	03/MPO/OT						1	38									
R7	42	17+26	18+27	LT	03/MPO/OT						1	75				1		52			
R8	42	18+59	19+30	LT	03/MPO/OT							70									
R9	42	19+07	19+39	RT	03/MPO/OT					40											
R10	42	19+43	20+16	LT	03/MPO/OT																
R11	42	19+76	20+08	RT	03/MPO/OT					41											
R12	43	20+55	22+79	LT	03/MPO/OT							222				3					
R13	43	22+23	22+47	LT	03/MPO/OT																
R14	43	22+75	22+94	LT	03/MPO/OT																
R15	43	22+88	22+88	LT	03/MPO/OT	1				38											
R16	43	23+68	23+74	RT	03/MPO/OT					12											
R17	43	23+50	25+16	LT	03/MPO/OT							168									
R18	43	25+39	26+10	LT	03/MPO/OT							71									
R19	44	26+81	29+73	LT	03/MPO/OT							290				1					
R20	45	31+91	31+91	LT	03/MPO/OT													25			
R21	45	32+53	33+00	LT	03/MPO/OT							47									
R22	45	32+97	34+73	LT	03/MPO/OT								179								
R23	41	11+01	13+30	LT	03/MPO/OT													229			
R24	45	33+61	34+16	RT	03/MPO/OT													65			
R25	45	34+21	39+97	LT	03/MPO/OT							356	230			5					
R26	46	37+19	37+25	LT	03/MPO/OT																
R27	46	39+21	39+48	LT	03/MPO/OT					6	39										
R28	47	41+48	45+13	LT	03/MPO/OT					2		360			1	1					
R29	47	42+71	42+83	RT	03/MPO/OT					25											
R30	47	43+22	43+39	RT	03/MPO/OT					25											
R31	48	48+21	48+87	RT	05/SAF/OT							66									
R32	48	48+38	48+39	RT	05/SAF/OT					7											
R33	48	48+77	48+77	RT	05/SAF/OT					11											
R34	48-49	48+88	51+58	LT	05/SAF/OT							277			1	2	1				
R35	48	49+34	49+77	LT	05/SAF/OT							47									
R36	48-49	50+22	50+52	LT	05/SAF/OT					3	36										
R37	49	50+64	51+02	LT	05/SAF/OT																
R38	49	50+83	50+94	RT	05/SAF/OT					20											
R39	49	50+97	51+25	LT	05/SAF/OT										41						
R40	49	51+02	51+26	LT	05/SAF/OT																
R41	49	51+33	51+41	RT	05/SAF/OT					37											
R42	49	50+84	51+52	RT	05/SAF/OT					27											
R43	49	51+58	51+71	LT	05/SAF/OT							63				1					
R44	49	51+51	54+27	LT	05/SAF/OT							18				1					
R45	49	52+58	53+31	RT	05/SAF/OT																
R46	49	52+93	53+10	LT	05/SAF/OT													1			
R47	49	53+99	54+26	RT	05/SAF/OT					39	25							38			
R48	49	53+97	53+97	RT	05/SAF/OT											1		70			
R49	48	48+22	48+27	LT	05/SAF/OT					16											
PARTICIPATION SPLIT 03/MPO/OT SUBTOTALS						1	5	108	94	286	0	1910	230	230	1	12	0	346	25		
PARTICIPATION SPLIT 05/SAF/OT SUBTOTALS						0	0	0	167	528	41	424	0	41	1	6	1	108	0		
TOTALS CARRIED TO GENERAL SUMMARY						1	5	108	261	814	41	2334	230	271	2	18	1	454	25		

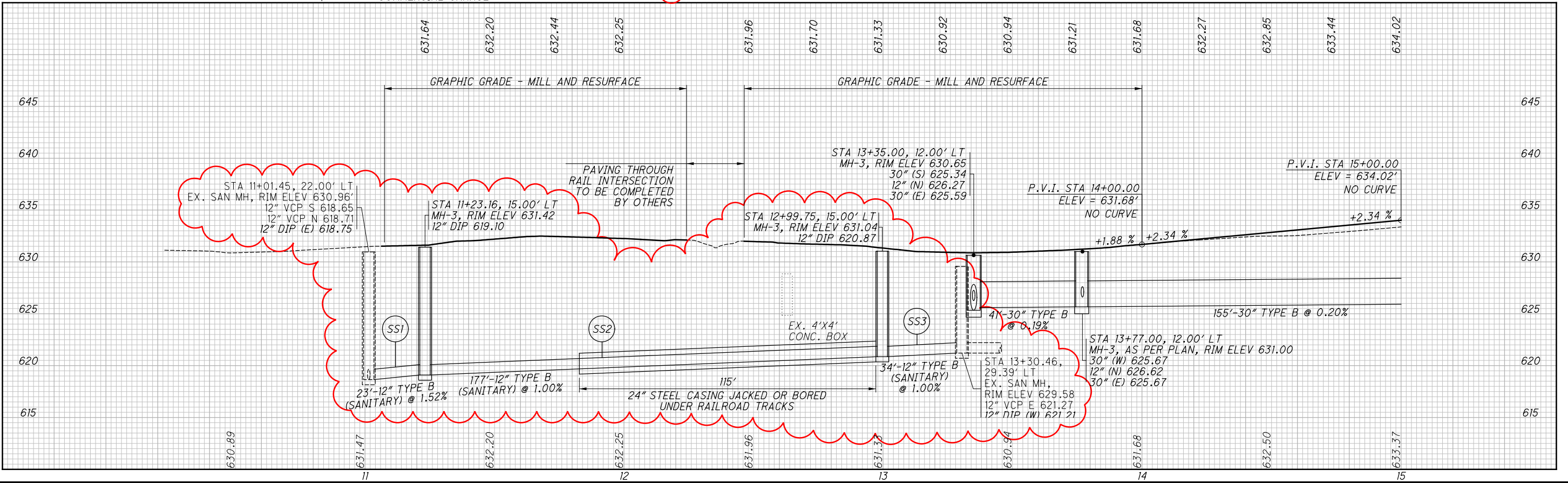
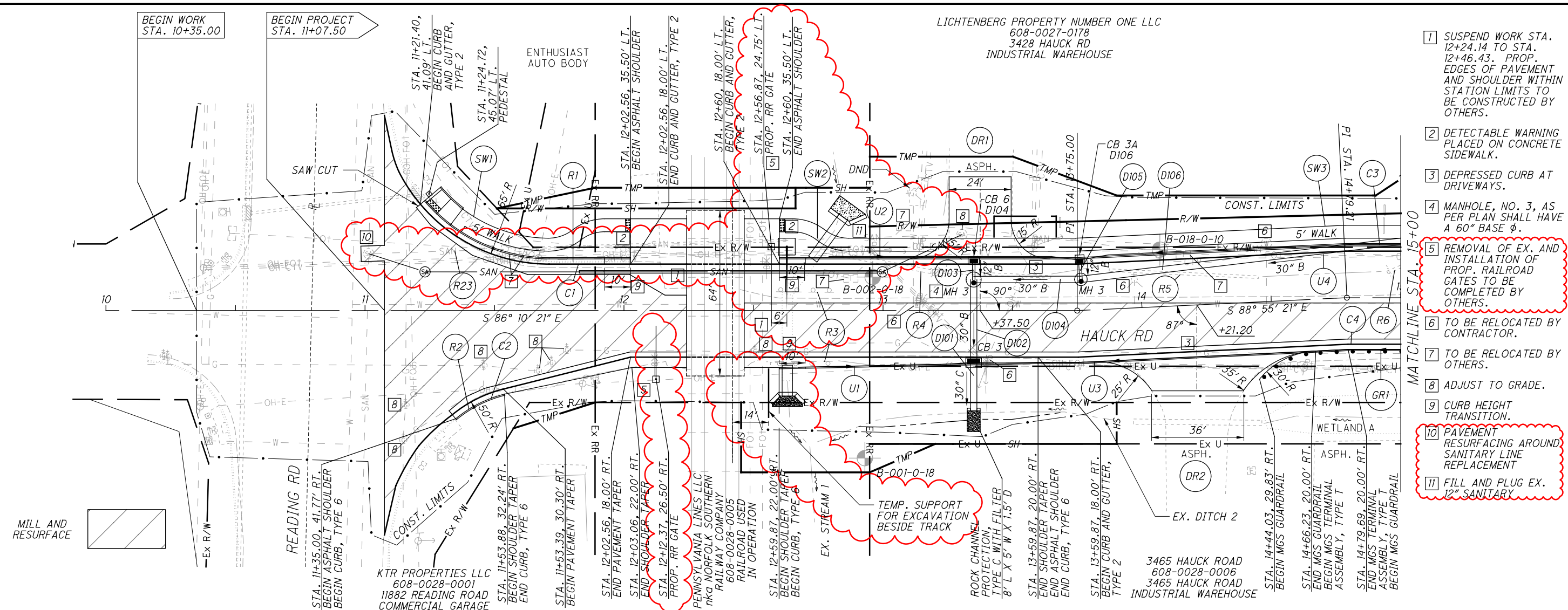
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ROADWAY SUBSUMMARY
HAM-HAUCK ROAD - WIDENING PART 2
 34
 151

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REF. NO.	SHEET NO.	STATION TO STATION		SIDE	PARTICIPATION	602	611		611		611		611	611	611	611	611	611	611				
						CONCRETE MASONRY	12" CONDUIT, TYPE		18" CONDUIT, TYPE		CATCH BASIN, NO.		CATCH BASIN, NO. 2-2B	CATCH BASIN, NO. 2-3	MANHOLE, NO. 3	MANHOLE, NO. 3, AS PER PLAN	MANHOLE ADJUSTED TO GRADE						
							CY	B FT	C FT	B FT	C FT	3 EACH											
D301	48	48+78	48+78	LT	05/SAF/OT		39				1												
D302	48	48+78	48+78	LT	05/SAF/OT																		
D303	48	48+78	49+26	LT	05/SAF/OT				49						1								
D304	48	49+26	49+26	LT	05/SAF/OT		9				1												
D305	48	48+90	49+00	RT	05/SAF/OT			12			1												
D306	48	48+20	48+90	RT	05/SAF/OT	0.2	70					1											
D307	48	49+26	49+88	LT	05/SAF/OT				61						1								
D308	48-49	49+88	51+00	LT	05/SAF/OT				114						1								
D309	49	51+00	51+77	LT	05/SAF/OT				81						1								
D310	49	51+75	51+77	LT	05/SAF/OT					16				1									
D311	49	51+77	51+77	LT	05/SAF/OT				8														
D312	49	50+76	51+73	RT	05/SAF/OT	0.2	89				1												
DX1	49	51+58	51+75	LT	05/SAF/OT					22									1				
DX2	48	49+26	49+26	LT	05/SAF/OT			19															
DX3	48	49+88	49+88	LT	05/SAF/OT		17												1				
TOTALS CARRIED TO GENERAL SUMMARY						0.4	224	31	313	38	0	5	1	1	4	1	2						

CALCULATED	DJR
	CHECKED
SKR	
DRAINAGE SUBSUMMARY	
HAM-HAUCK ROAD - WIDENING PART 2	
37 151	

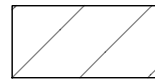
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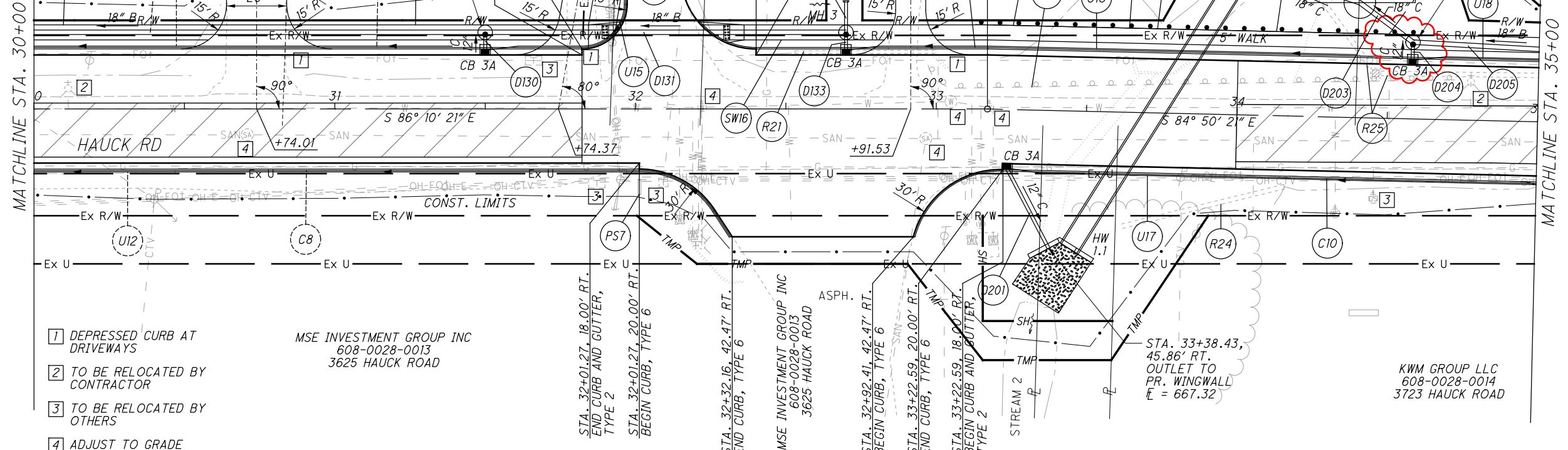
LICHTENBERG PROPERTY NUMBER ONE LLC
608-0027-0178
3428 HAUCK RD
INDUSTRIAL WAREHOUSE

- 1 SUSPEND WORK STA. 12+24.14 TO STA. 12+46.43. PROP. EDGES OF PAVEMENT AND SHOULDER WITHIN STATION LIMITS TO BE CONSTRUCTED BY OTHERS.
- 2 DETECTABLE WARNING PLACED ON CONCRETE SIDEWALK.
- 3 DEPRESSED CURB AT DRIVEWAYS.
- 4 MANHOLE, NO. 3, AS PER PLAN SHALL HAVE A 60" BASE ϕ .
- 5 REMOVAL OF EX. AND INSTALLATION OF PROP. RAILROAD GATES TO BE COMPLETED BY OTHERS.
- 6 TO BE RELOCATED BY CONTRACTOR.
- 7 TO BE RELOCATED BY OTHERS.
- 8 ADJUST TO GRADE.
- 9 CURB HEIGHT TRANSITION.
- 10 PAVEMENT RESURFACING AROUND SANITARY LINE REPLACEMENT
- 11 FILL AND PLUG EX. 12" SANITARY

MILL AND RESURFACE



MATCHLINE STA. 30+00



- 1 DEPRESSED CURB AT DRIVEWAYS
- 2 TO BE RELOCATED BY CONTRACTOR
- 3 TO BE RELOCATED BY OTHERS
- 4 ADJUST TO GRADE

MSE INVESTMENT GROUP INC
608-0028-0013
3625 HAUCK ROAD

KWM GROUP LLC
608-0028-0014
3723 HAUCK ROAD

JAMES KOSSEN AND RHONDA KOSSEN
608-0027-0025
3708 HAUCK ROAD

STA. 34+08.68,
47.74' LT.
OUTLET TO
PR. WINGWALL
E = 667.00

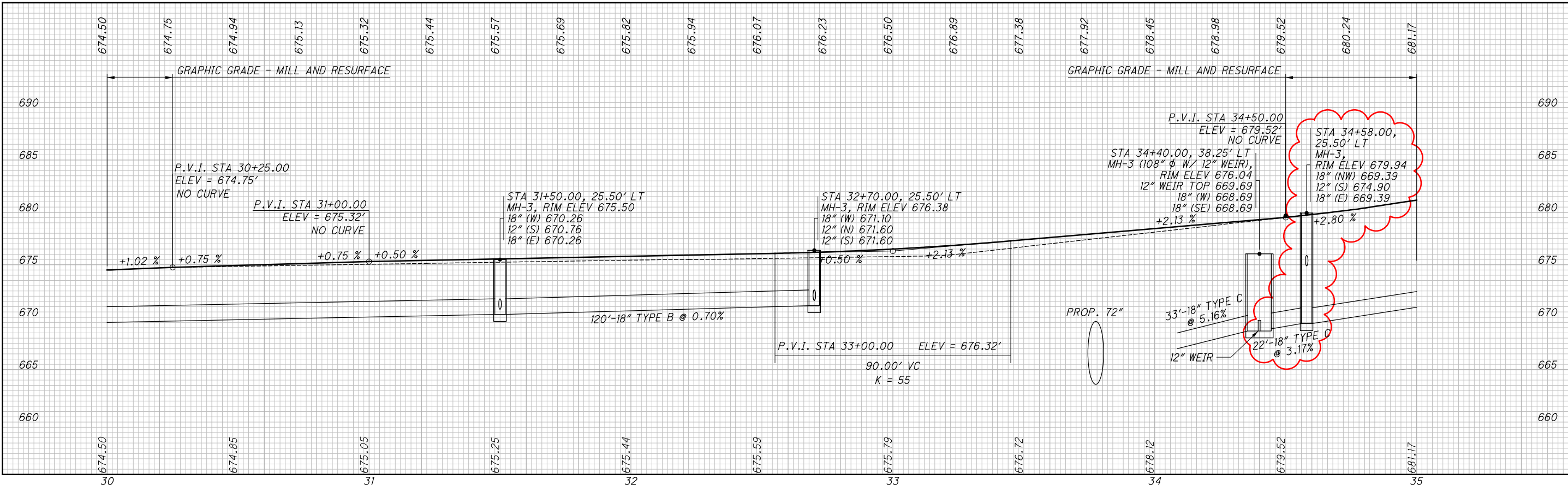
RESERVED AREA FOR
MANUFACTURED SYSTEM,
TYPE 4 (25' L X 37' W)
STA. 34+50.00,
28.00' LT.
LON POINT

CALCULATED
DJR
CHECKED
SKR

0 20 40
HORIZONTAL
SCALE IN FEET

PLAN AND PROFILE
HAUCK ROAD STA. 30+00 TO STA. 35+00

HAM-HAUCK ROAD -
WIDENING PART 2



GRAPHIC GRADE - MILL AND RESURFACE

GRAPHIC GRADE - MILL AND RESURFACE

P.V.I. STA 30+25.00
ELEV = 674.75'
NO CURVE

P.V.I. STA 31+00.00
ELEV = 675.32'
NO CURVE

STA 31+50.00, 25.50' LT
MH-3, RIM ELEV 675.50
18" (W) 670.26
12" (S) 670.76
18" (E) 670.26

STA 32+70.00, 25.50' LT
MH-3, RIM ELEV 676.38
18" (W) 671.10
12" (N) 671.60
12" (S) 671.60

P.V.I. STA 34+50.00
ELEV = 679.52'
NO CURVE
STA 34+40.00, 38.25' LT
MH-3 (108" φ W/ 12" WEIR),
RIM ELEV 676.04
12" WEIR TOP 669.69
18" (W) 668.69
18" (SE) 668.69

STA 34+58.00,
25.50' LT
MH-3,
RIM ELEV 679.94
18" (NW) 669.39
12" (S) 674.90
18" (E) 669.39

P.V.I. STA 33+00.00 ELEV = 676.32'
90.00' VC
K = 55

PROP. 72"

33'-18" TYPE C @ 5.16%

22'-18" TYPE C @ 3.17%

12" WEIR

30

31

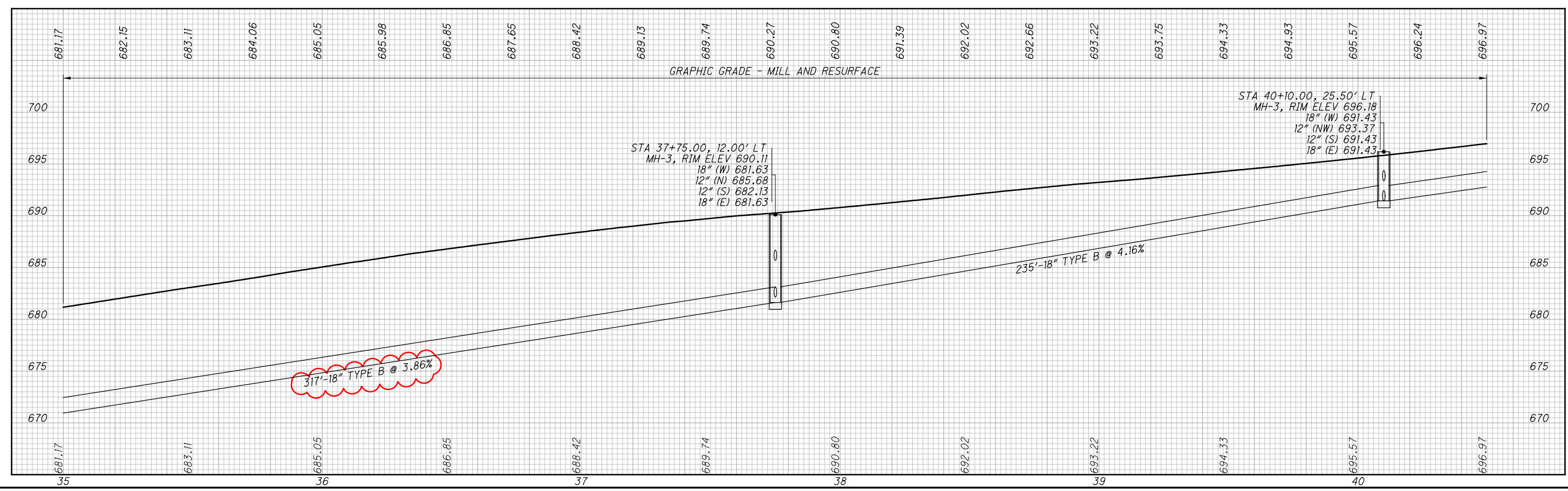
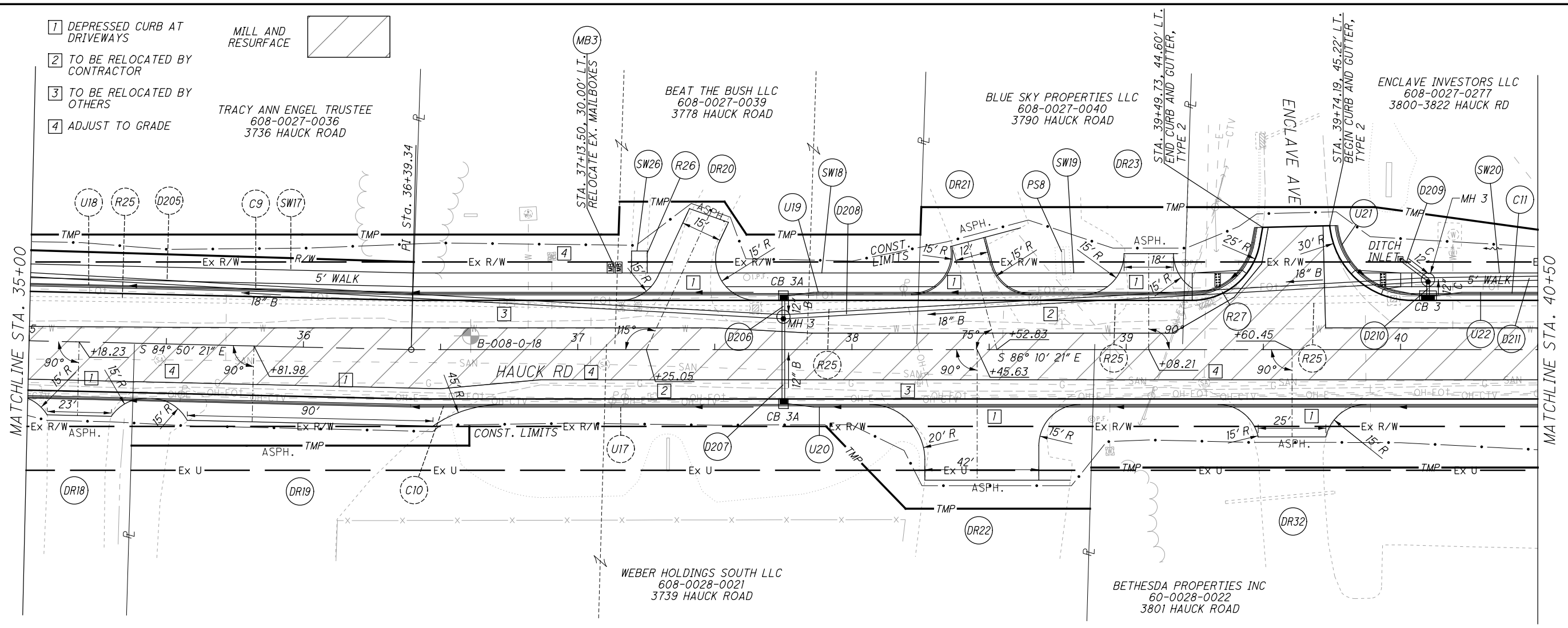
32

33

34

35

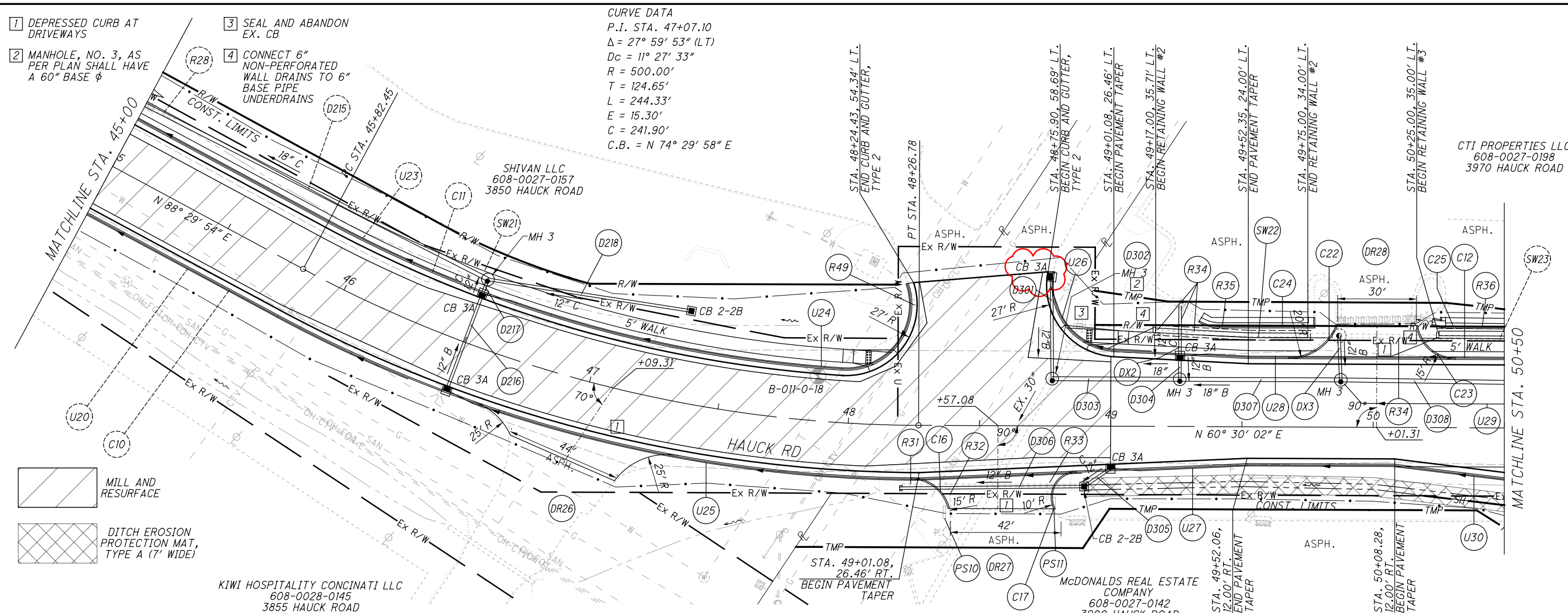
- 1 DEPRESSED CURB AT DRIVEWAYS
 - 2 TO BE RELOCATED BY CONTRACTOR
 - 3 TO BE RELOCATED BY OTHERS
 - 4 ADJUST TO GRADE
- MILL AND RESURFACE



PLAN AND PROFILE
HAUCK ROAD STA. 35+00 TO STA. 40+50

HAM-HAUCK ROAD -
WIDENING PART 2

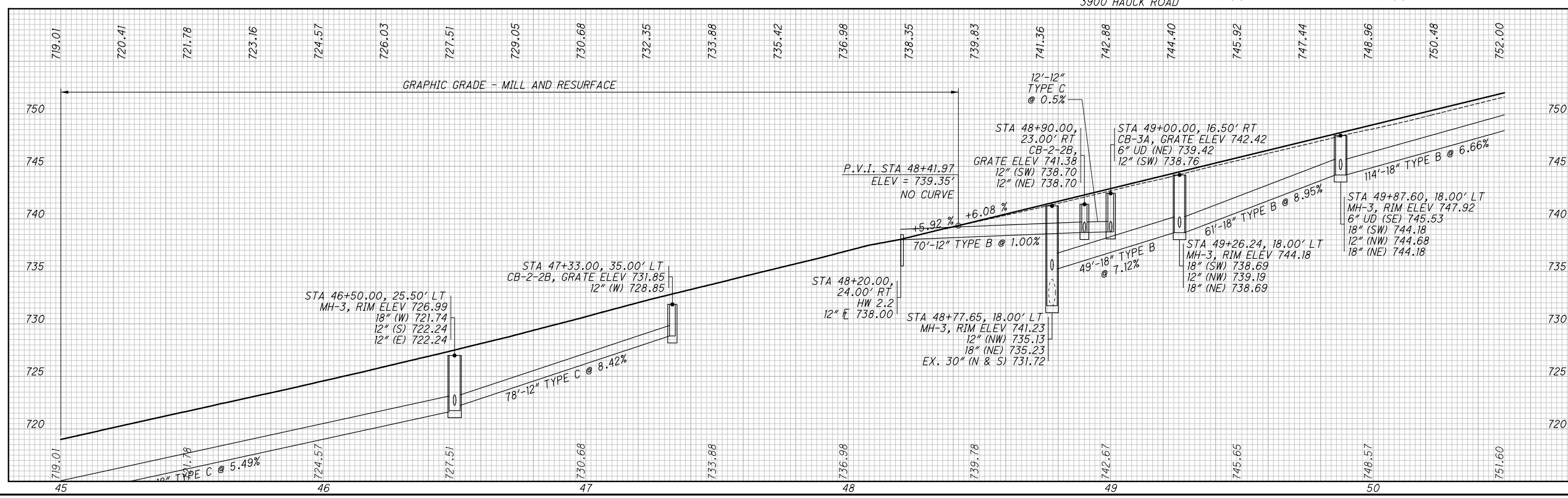
H:\2017\0342\DWG\05181\Design\Roadway\Sheets\05181_GP006.dgn Sheet 4/7/2021 8:20:55 AM rotella



CURVE DATA
 P.I. STA. 47+07.10
 $\Delta = 27^\circ 59' 53''$ (LT)
 $D_c = 11^\circ 27' 33''$
 $R = 500.00'$
 $T = 124.65'$
 $L = 244.33'$
 $E = 15.30'$
 $C = 241.90'$
 $C.B. = N 74^\circ 29' 58'' E$

- 1 DEPRESSED CURB AT DRIVEWAYS
- 2 MANHOLE, NO. 3, AS PER PLAN SHALL HAVE A 60" BASE ϕ
- 3 SEAL AND ABANDON EX. CB
- 4 CONNECT 6" NON-PERFORATED WALL DRAINS TO 6" BASE PIPE UNDERDRAINS

MILL AND RESURFACE
 DITCH EROSION PROTECTION MAT, TYPE A (7' WIDE)



CALCULATED
 DJR
 CHECKED
 SKR

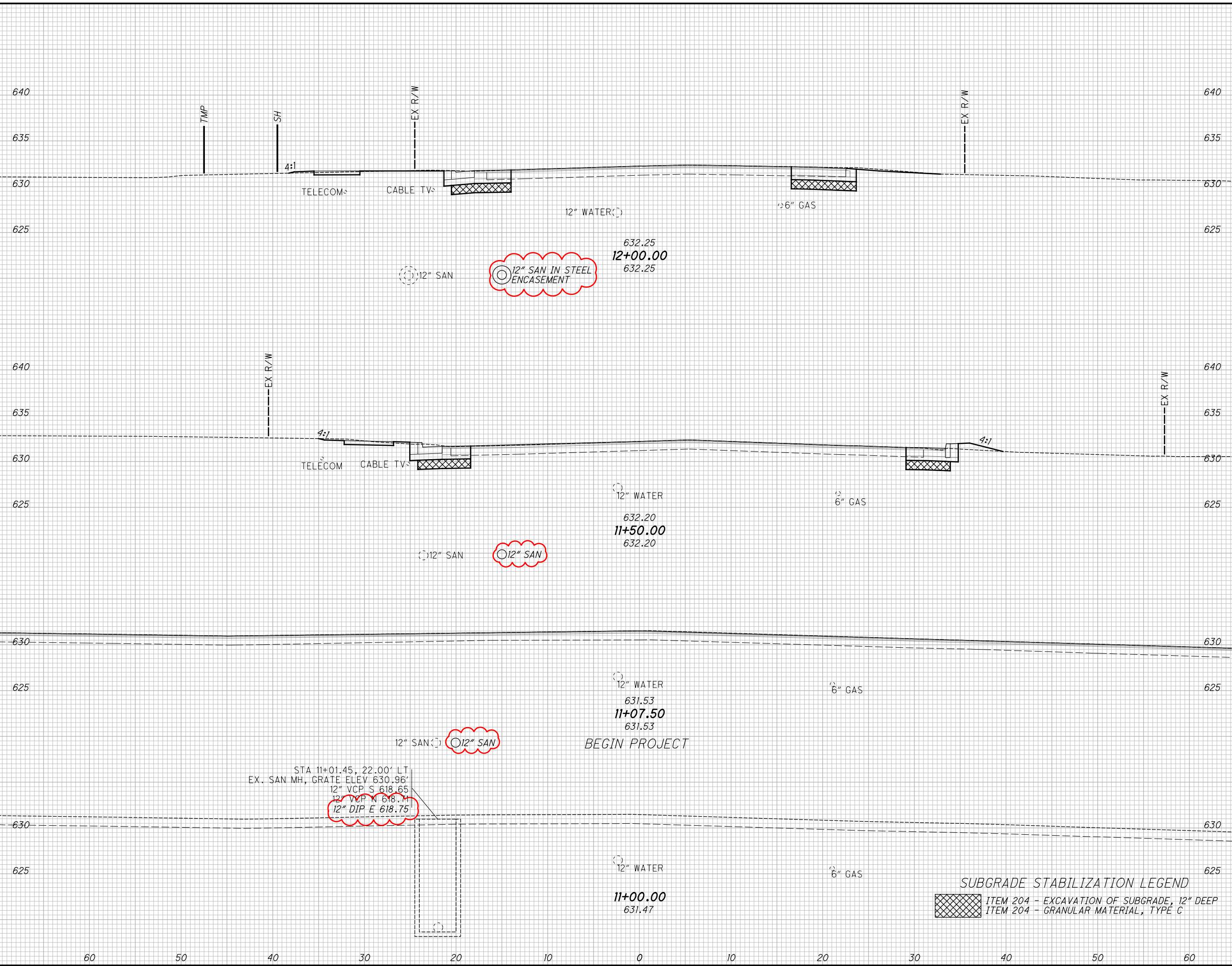
PLAN AND PROFILE
 HAUCK ROAD STA. 45+00 TO STA. 50+00

HAM-HAUCK ROAD -
 WIDENING PART 2

H:\2017\0342\DWG\05181\Design\Roadway\Sheets\05181_GPO08.dgn Sheet 3/1/2021 9:43:00 AM rotella

H:\2017\170342\DWG\05181\Design\Roadway\Sheets\05181_XS001.dgn_Sheet 5/4/2021 9:11:30 AM rotella

SEEDING	
END WIDTH	SO. YDS.
28	
128	
16	
38	
0	
0	
0	
166	



END AREA		VOLUME	
CUT	FILL	CUT	FILL
15	0	30	2
17	2	13	2
0	0	0	0
0	0	0	0
43	4		

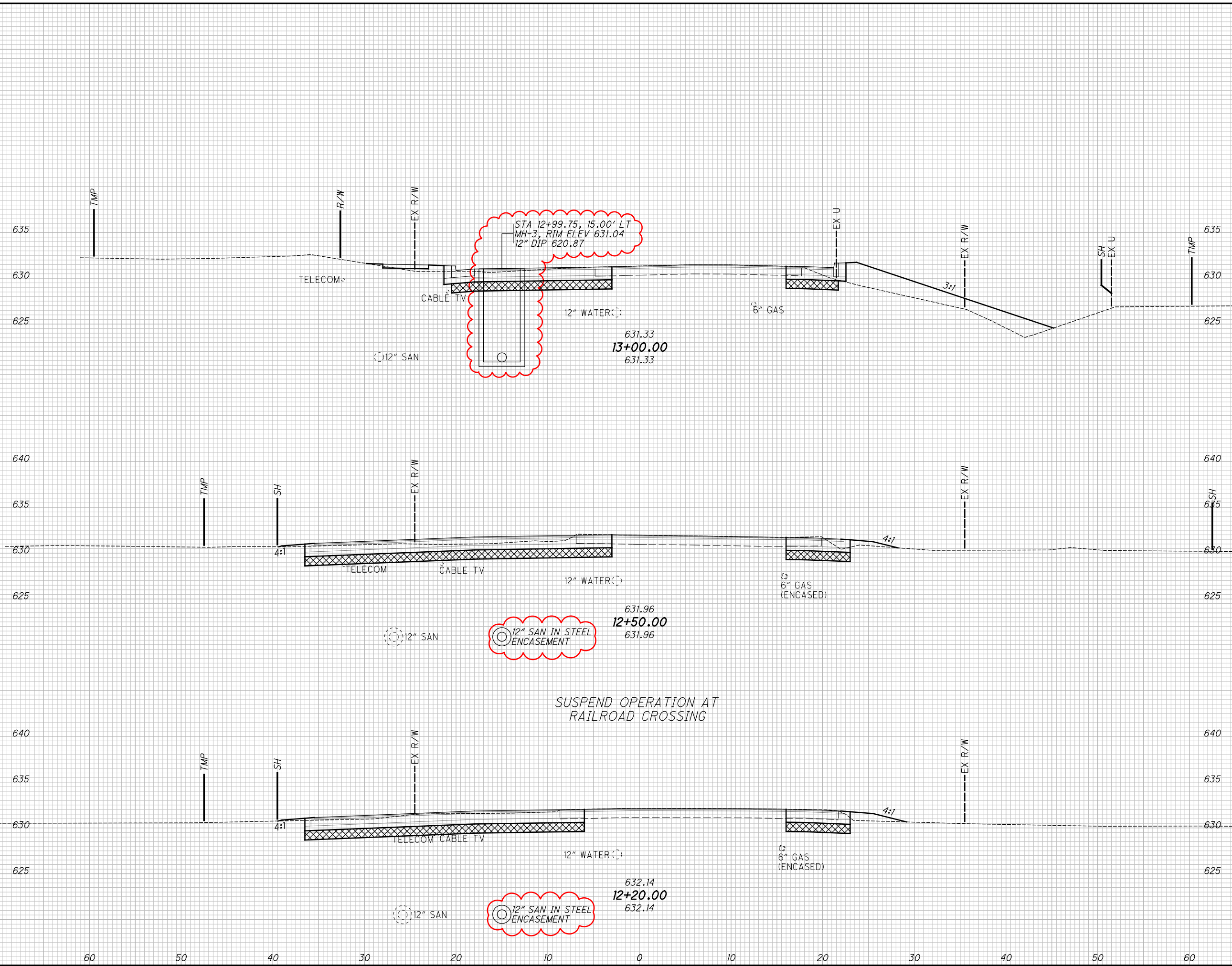
CROSS SECTIONS HAUCK ROAD
 STA. 11+00.00 TO STA. 12+00.00
 WIDENING PART 2

CALCULATED DJR
 CHECKED SKR

50
151

H:\2017\170342\DWG\05181\Roadway\Sheets\05181_XS001.dgn Sheet 5/4/2021 9:12:30 AM rotella

SEEDING	END AREA		VOLUME		CALCULATED DJR	CHECKED SKR
	END WIDTH	SO. YDS.	CUT	FILL		
33	60	630	25	43		
133	50	625		54		
14	60	630	33	2		
50	50	625		0		
15	60	630	37	4		
48	50	625		19		
231	60	630		73		



END AREA	VOLUME	CALCULATED DJR	CHECKED SKR
25	43		
	54		
33	2		
	0		
37	4		
	19		
	73		

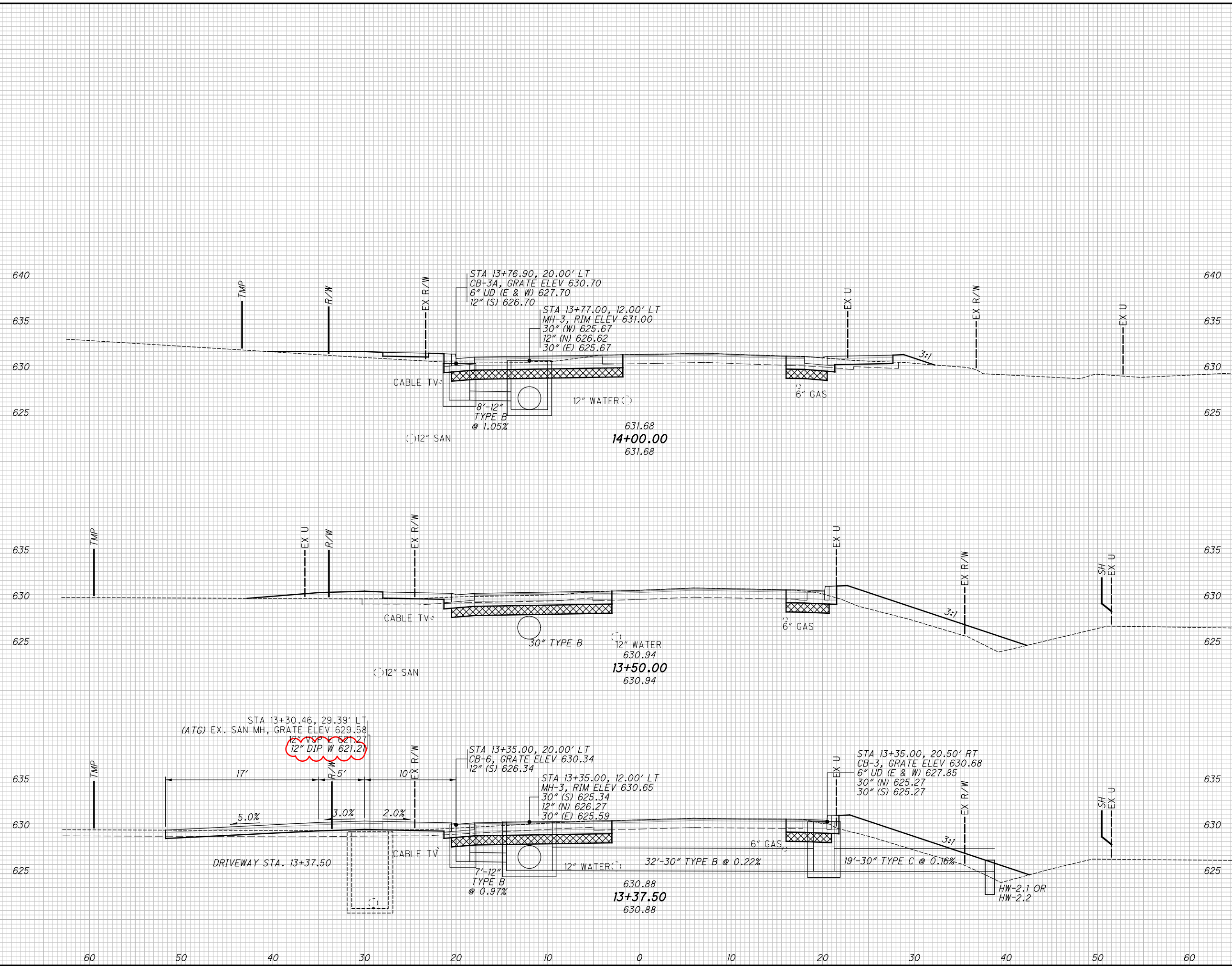
CROSS SECTIONS HAUCK ROAD
STA. 12+20.00 TO STA. 13+00.00

HAM-HAUCK ROAD -
WIDENING PART 2

51
151

H:\2017\170342\DWG\05181\Design\Roadway\Sheets\05181_XS001.dgn Sheet 5/4/2021 9:13:39 AM rotella

SEEDING	
END WIDTH	SO. YDS.
60	24
50	183
40	42
30	49
20	27
10	125
0	357

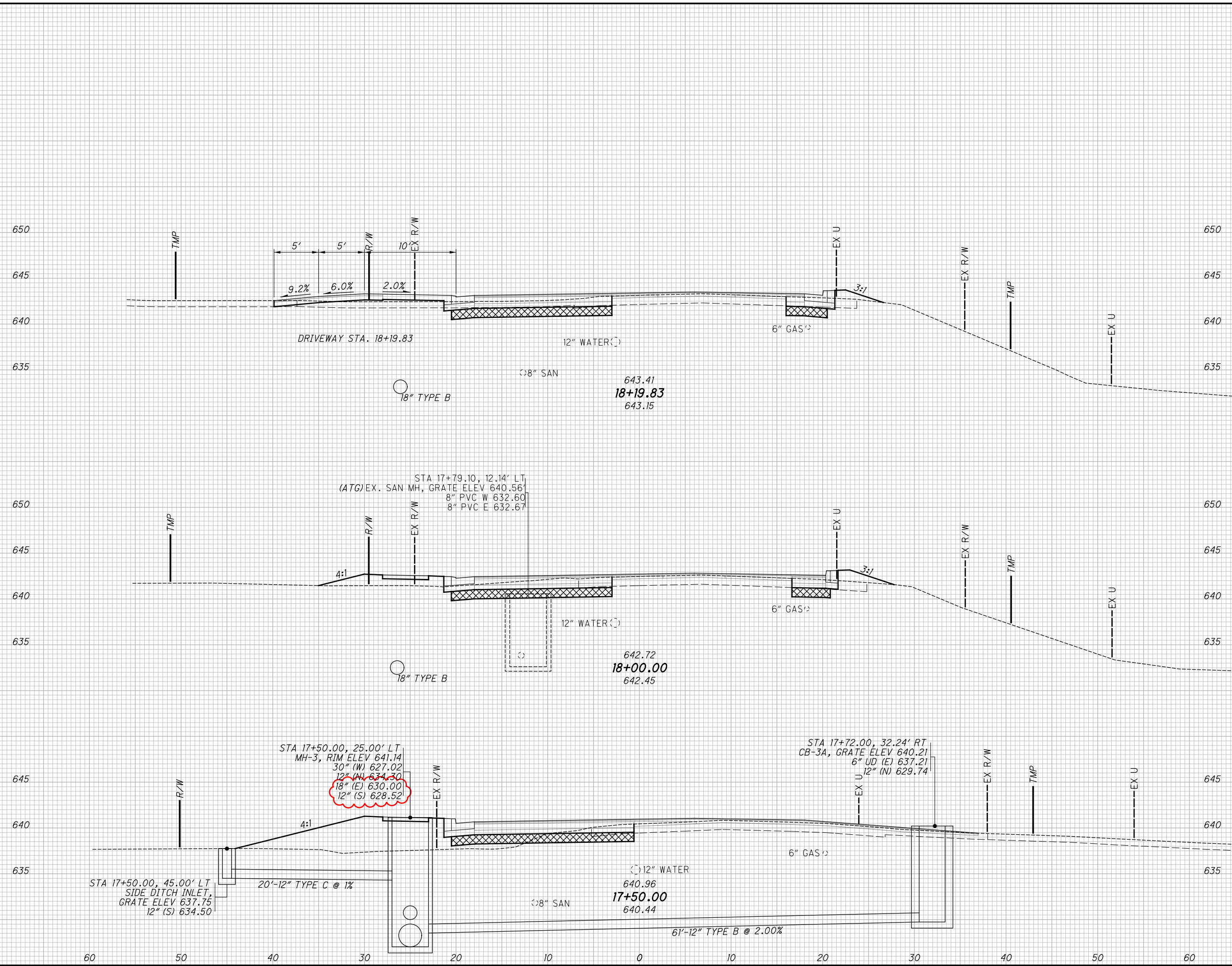


END AREA		VOLUME	
CUT	FILL	CUT	FILL
20	10	31	44
13	37	6	16
14	30	27	51
		64	111

CROSS SECTIONS HAUCK ROAD
 STA. 13+37.50 TO STA. 14+00.00
 HAM-HAUCK ROAD -
 WIDENING PART 2
 CALCULATED DJR
 CHECKED SKR
 52
 151

H:\2017\170342\DWG\05181\Design\Roadway\Sheets\05181_XS001.dgn Sheet 3/3/2021 4:25:31PM rotella

SEEDING	
END WIDTH	SO. YDS.
195	60
33	50
23	40
21	30
10	20



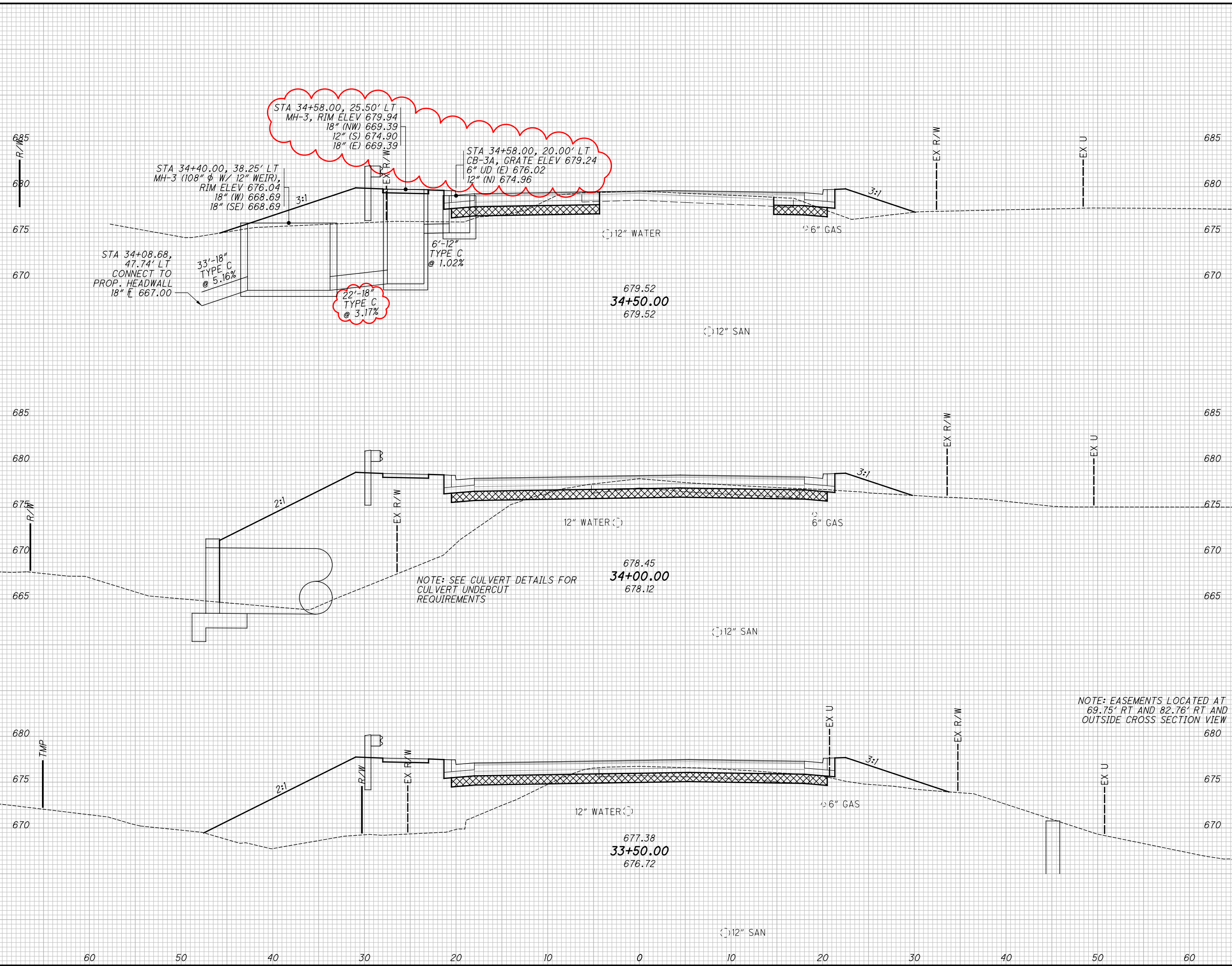
END AREA		VOLUME	
CUT	FILL	CUT	FILL
4	5	6	8
12	16	12	81
1	71	5	33
		23	122

CROSS SECTIONS HAUCK ROAD
STA. 17+50.00 TO STA. 18+19.83
HAM-HAUCK ROAD -
WIDENING PART 2

56
151

H:\2017\170342\DWG\105181\Roadway\Sheets\105181_XS001.dgn Sheet 4/7/2021 8:16:08 AM rotella

SEEDING	END	
	WIDTH	SO. YDS.
	60	35
	50	223
	45	45
	245	42
	189	657



END AREA	VOLUME	
	CUT	FILL
6	85	
7	358	
2	302	
3	485	
1	222	
6	276	
16	1119	

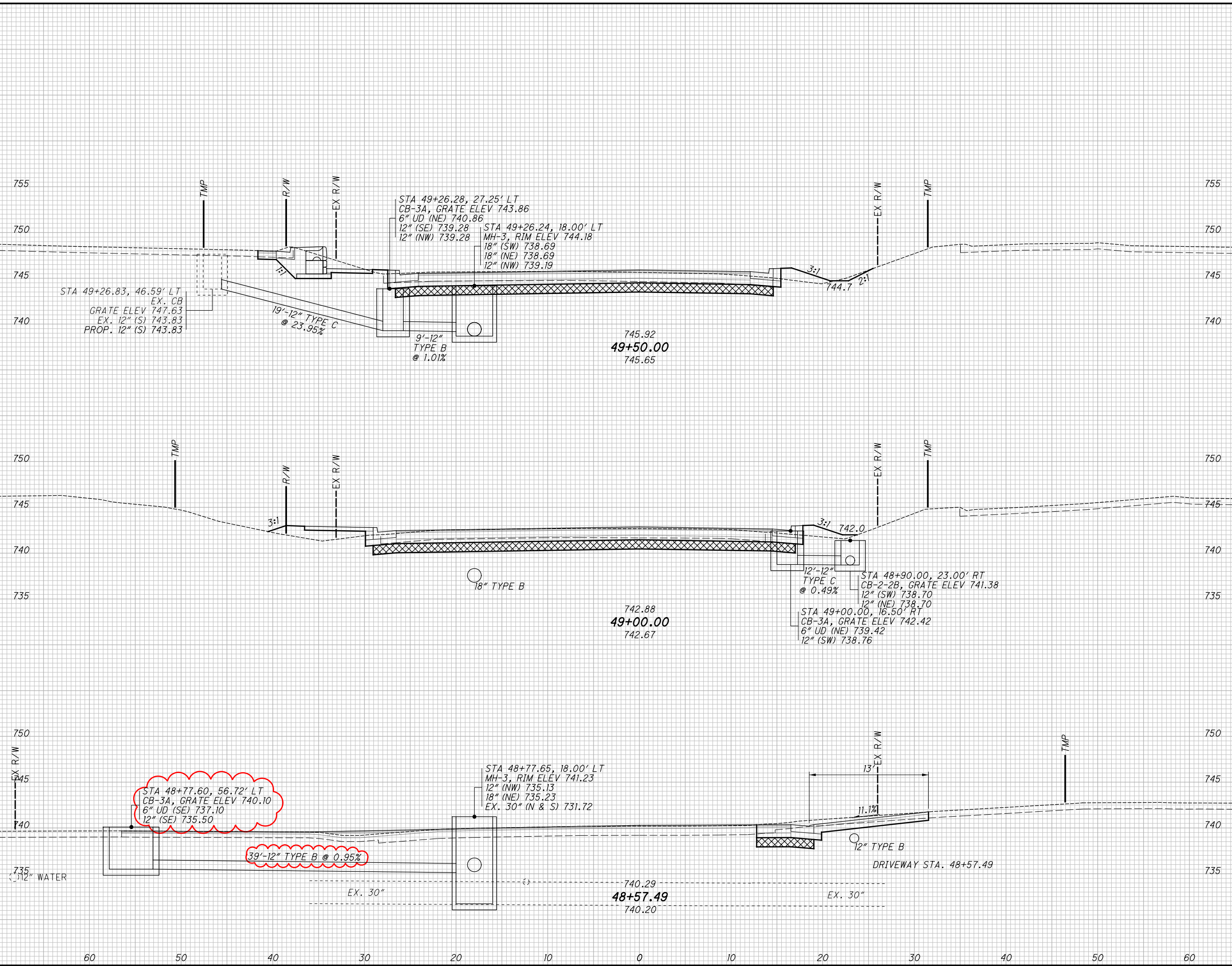
CROSS SECTIONS HAUCK ROAD
STA. 33+50.00 TO STA. 34+50.00
HAM-HAUCK ROAD - WIDENING PART 2

CALCULATED DJR
 CHECKED SKR

70
 151

H:\2017\170342\DWG\05181\Design\Roadway\Sheets\05181_XS001.dgn_Sheet_3/1/2021 10:53:03 AM rotella

SEEDING	
END WIDTH	SO. YDS.
138	

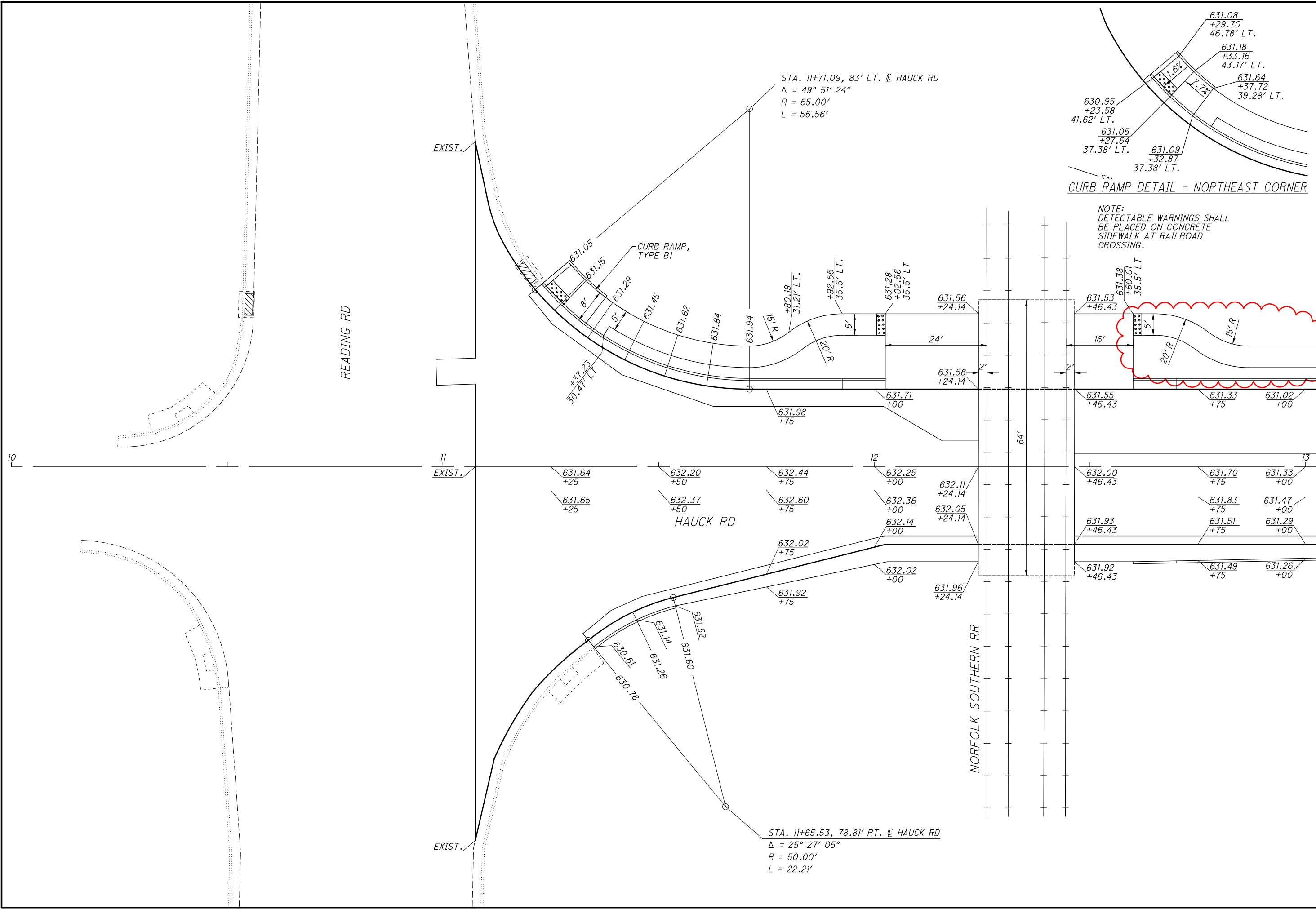


END STA	AREA		VOLUME	
	CUT	FILL	CUT	FILL
35		16		
49		28		
18		14		
24		11		
13		0		
3		0		
76		39		

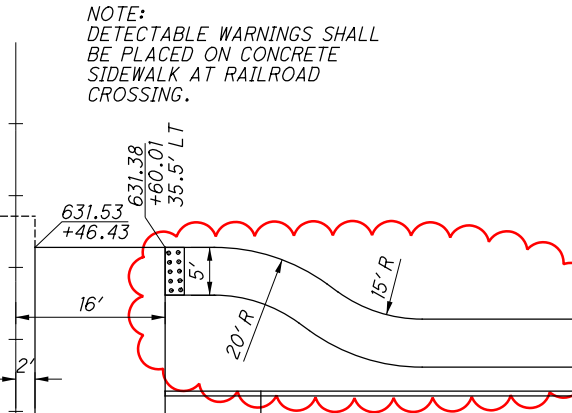
CROSS SECTIONS HAUCK ROAD
STA. 48+57.49 TO STA. 49+50.00
HAM-HAUCK ROAD -
WIDENING PART 2

82
151

H:\2017\170342\DWG\05181\Design\Roadway\Sheets\05181_G1001.dgn Sheet 5/14/2021 9:16:25 AM rotella



CURB RAMP DETAIL - NORTHEAST CORNER



CALCULATED
DJR
CHECKED
SKR

0 5 10 20
HORIZONTAL SCALE IN FEET

↑
N

INTERSECTION DETAILS
HAUCK ROAD STA. 10+00 TO STA. 13+00

HAM-HAUCK ROAD - WIDENING PART 2



0 5 10 20
HORIZONTAL SCALE IN FEET

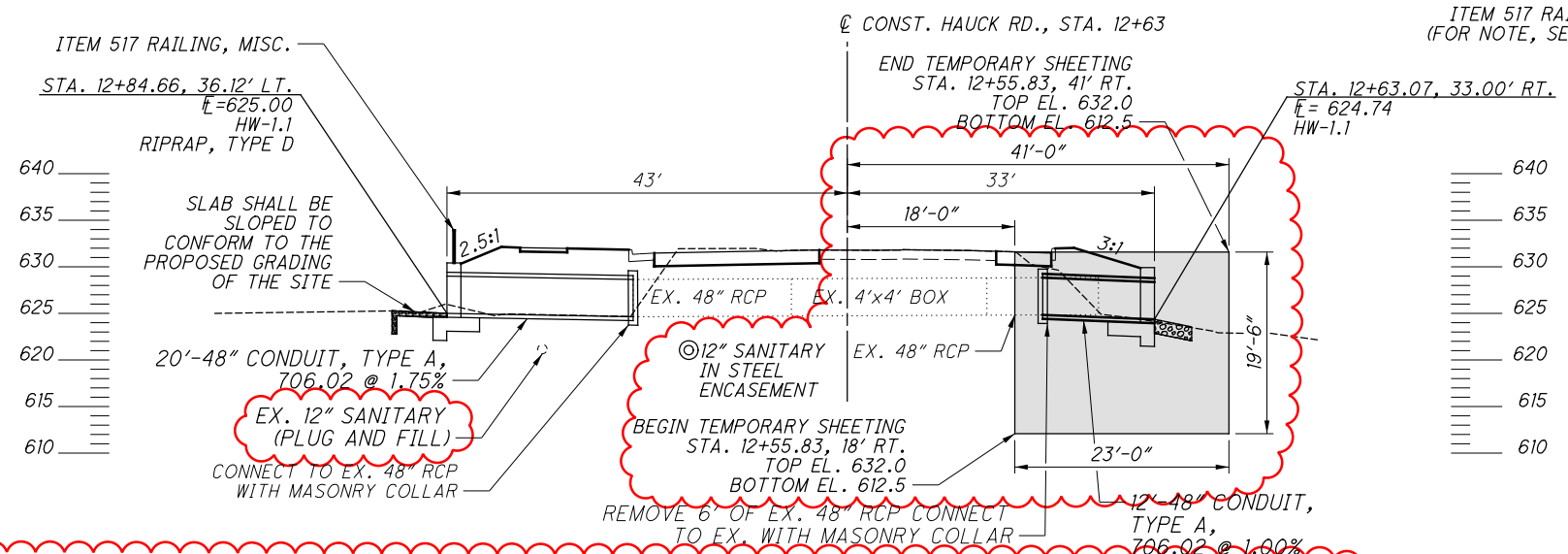
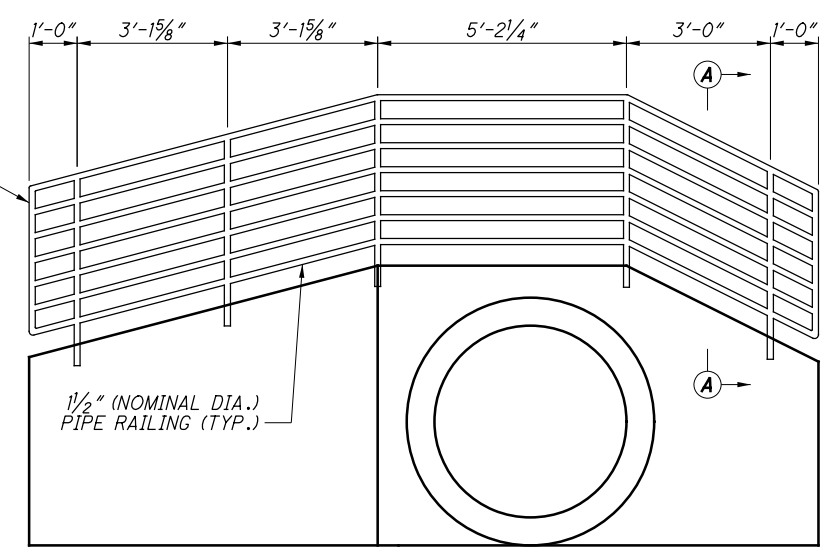
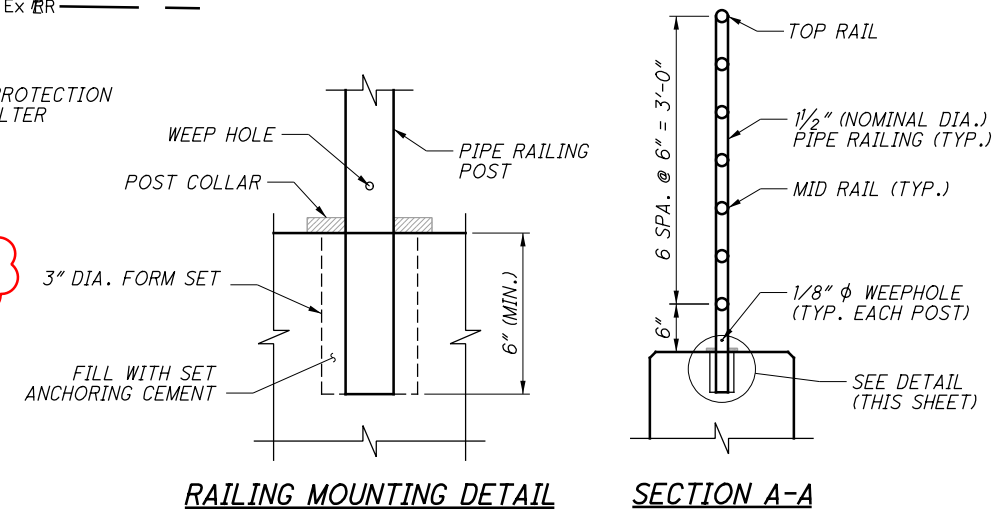
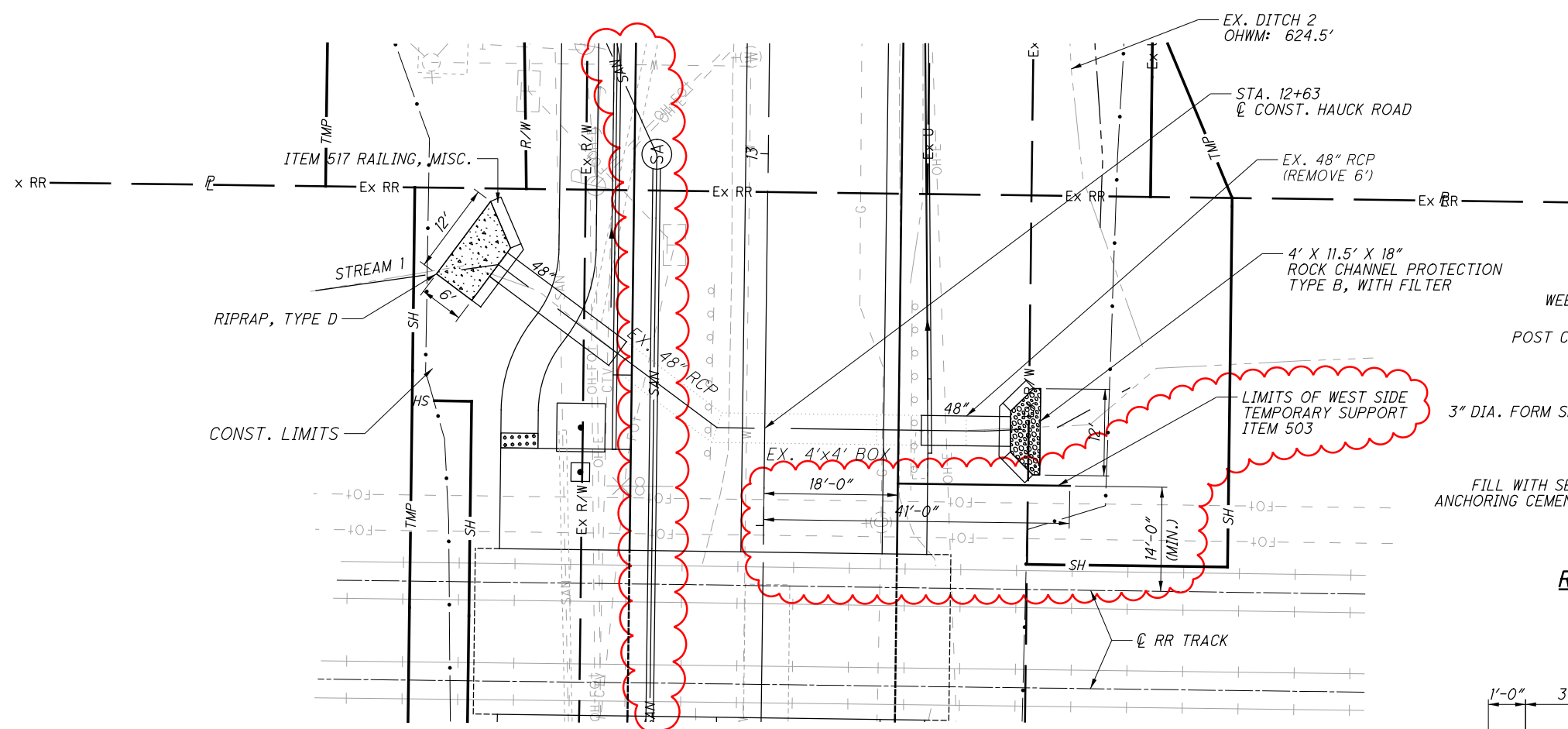
CALCULATED
JRE
CHECKED
SKR

CULVERT DETAIL
HAUCK RD STA. 12+63.00

HAM-HAUCK ROAD -
WIDENING PART 2

99
151

EXISTING STRUCTURE
TYPE: 48"X48" CONC. BOX AND 48" RCP
SKEW: 40° L.F.
ALIGNMENT: ANGLED
STREAM 1 OHWM: 626



ITEM 503 - COFFERDAMS AND EXCAVATION BRACING, AS PER PLAN:
THE TEMPORARY SHORING DESIGN SHOWN ON THE PLANS FOR EXCAVATION SUPPORT ADJACENT TO THE RAILROAD REQUIRES FINAL CONCURRENCE AND ACCEPTANCE BY THE RAILROAD. THE CONTRACTOR IS TO SUBMIT THE DESIGN ILLUSTRATED IN THE PLANS OR AN ACCEPTABLE ALTERNATE TO THE RAILROAD FOR ACCEPTANCE. INCLUDE ITEMS IN THE PROJECT SCHEDULE FOR SUBMISSION AND ACCEPTANCE OF THE SHORING TO THE RAILROAD. THE CONTRACTOR MAY CONSTRUCT THE DESIGN SHOWN ON THE PLANS OR PREPARE AN ALTERNATE DESIGN. IF CONSTRUCTING AN ALTERNATE DESIGN, FOR TEMPORARY SUPPORT OF EXCAVATION, PREPARE AND PROVIDE PLANS IN ACCORDANCE WITH C&MS 501.05 IN ADDITION TO THE RAILROAD SUBMISSION. THE DEPARTMENT WILL PAY FOR THE TEMPORARY SUPPORT OF EXCAVATION AT THE CONTRACT LUMP SUM PRICE FOR COFFERDAMS AND EXCAVATION BRACING, AS PER PLAN. NO ADDITIONAL PAYMENT WILL BE MADE FOR PROVIDING AN ALTERNATE DESIGN. DELAYS RELATED TO THE RAILROAD SHORING DESIGN, APPROVAL, AND ACCEPTANCE BY THE RAILROAD ARE NON-EXCUSABLE AND THE DEPARTMENT WILL NOT CONSIDER ADDITIONAL COMPENSATION OR CONTRACT EXTENSION.

THE WORK SHALL BE IN ACCORDANCE WITH ITEM 503, EXCEPT THAT STEEL SHEET PILING SATISFYING THE MINIMUM SECTION AND MATERIAL PROPERTIES LISTED BELOW SHALL BE USED:

MINIMUM SECTION MODULUS: 18.4 IN³/FT.
MINIMUM MOMENT OF INERTIA: 85.0 IN⁴/FT.
STRUCTURAL STEEL: ASTM A572
MINIMUM YIELD STRENGTH: 50,000 PSI

ITEM	ITEM EXT.	QUANTITY	UNIT	DESCRIPTION
202	35200	6	FT	PIPE REMOVED, OVER 24"
503	11101	LS		COFFERDAMS AND EXCAVATION BRACING, AS PER PLAN
517	76800	17	FT	RAILING, MISC.: 1 1/2" NOMINAL DIAMETER PIPE RAILING
601	11000	7	SY	RIPRAP, TYPE D
601	32100	3	CY	ROCK CHANNEL PROTECTION, TYPE B WITH FILTER
602	20000	18	CY	CONCRETE MASONRY
611	20700	30	FT	48" CONDUIT, TYPE A, 706.02

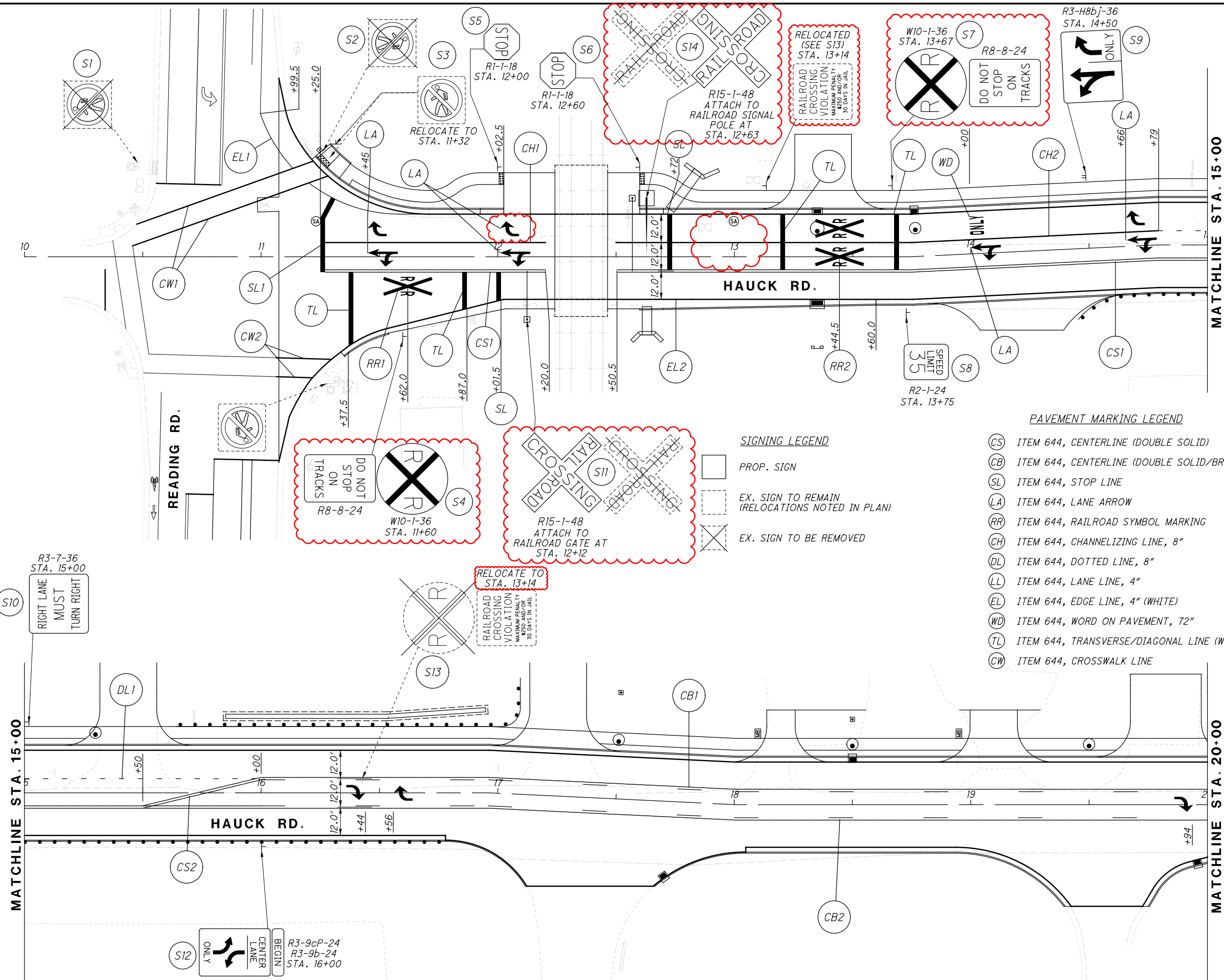
ITEMS CARRIED TO GENERAL SUMMARY

H:\2017\70342\DWG\05181\Design\Roadway\Sheets\05181_DC002.dgn Sheet 5/4/2021 12:22:28 PM rotella

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SHEET NO.	REFERENCE NO.	LOCATION	STATION	SIDE	CODE	SIZE (INCHES)	PARTICIPATION	630	630	630	630	630	630	630	630
								GROUND MOUNTED SUPPORT, NO. 3 POST FT	STREET NAME SIGN SUPPORT, NO. 3 POST FT	SIGN SUPPORT ASSEMBLY, POLE MOUNTED EACH	SIGN, FLAT SHEET SF	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL EACH	REMOVAL OF GROUND MOUNTED SIGN AND REELECTION EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL EACH
113	S1	HAUCK RD.	10+45	LT			03/MPO/OT								1
113	S2	HAUCK RD.	11+32	LT			03/MPO/OT					1		1	
113	S3	HAUCK RD.	11+32	LT			03/MPO/OT	12				1		1	
113	S4	HAUCK RD.	11+60	RT	W10-1 R8-8	36 DIA. 24 X 30	03/MPO/OT	16			9 5				
113	S5	HAUCK RD.	12+00	LT	R1-1	18 X 18	03/MPO/OT	12			2.3				
113	S6	HAUCK RD.	12+57	LT	R1-1	18 X 18	03/MPO/OT	12			2.3				
113	S7	HAUCK RD.	13+67	LT	W10-1 R8-8	36 DIA. 24 X 30	03/MPO/OT	16			9 5				
113	S8	HAUCK RD.	13+75	RT	R2-1	24 X 30	03/MPO/OT	13			5				
113	S9	HAUCK RD.	14+50	LT	R3-H8bj	36 X 30	03/MPO/OT	13			7.5				
113	S10	HAUCK RD.	15+00	LT	R3-7	36 X 36	03/MPO/OT	15			9				
113	S11	HAUCK RD.	12+12	RT	R15-1	48 X 9	03/MPO/OT			1	6				1
113	S12	HAUCK RD.	16+00	RT	R3-9cP R3-9b	24 X 6 24 X 30	03/MPO/OT	13.5			1 5				
113	S13	HAUCK RD.	16+43	LT			03/MPO/OT	13.5				1	1	1	
113	S14	HAUCK RD.	12+63	LT	R15-1	48 X 9	03/MPO/OT			1	6				1
114	S15	HAUCK RD.	22+41	LT			03/MPO/OT			13			3	1	
114	S16	HAUCK RD.	23+78	RT			03/MPO/OT			13			2	1	
114	S17	HAUCK RD.	25+00	RT	R2-1	24 X 30	03/MPO/OT	13			5				
115	S18	HAUCK RD.	31+35	LT	R2-1	24 X 30	03/MPO/OT	13			5				
115	S19	HAUCK RD.	31+95	LT	R1-1	30 X 30	03/MPO/OT	13			6.3				
	S20	NOT	USED												
116	S21	HAUCK RD.	44+27	RT			03/MPO/OT					1		1	
116	S22	HAUCK RD.	45+31	RT			03/MPO/OT					1		1	
	S23	NOT	USED												
116	S24	HAUCK RD.	46+00	LT	R2-1	24 X 30	03/MPO/OT	13			5				
116	S25	HAUCK RD.	47+00	LT			03/MPO/OT	12				1	1	1	
116	S26	HAUCK RD.	48+00	LT	R3-9cP R3-9b	24 X 6 24 X 30	03/MPO/OT	13.5			1 5				
116	S27	HAUCK RD.	48+00	RT	R3-9dP R3-9b	24 X 6 24 X 30	03/MPO/OT	13.5			1 5				
TOTALS CARRIED TO GENERAL SUMMARY								227	26	2	105.4	5	8	8	3

SIGNING SUBSUMMARY	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 50%; text-align: center;">CALCULATED DJR</td> <td style="width: 50%; text-align: center;">CHECKED JAK</td> </tr> <tr> <td style="text-align: center;">110</td> <td style="text-align: center;">151</td> </tr> </table>	CALCULATED DJR	CHECKED JAK	110	151
CALCULATED DJR	CHECKED JAK				
110	151				



SIGNING LEGEND

- PROP. SIGN
- EX. SIGN TO REMAIN (RELOCATIONS NOTED IN PLAN)
- EX. SIGN TO BE REMOVED

PAVEMENT MARKING LEGEND

- (CS) ITEM 644, CENTERLINE (DOUBLE SOLID)
- (CB) ITEM 644, CENTERLINE (DOUBLE SOLID/BROKEN)
- (SL) ITEM 644, STOP LINE
- (LA) ITEM 644, LANE ARROW
- (RR) ITEM 644, RAILROAD SYMBOL MARKING
- (CH) ITEM 644, CHANNELIZING LINE, 8"
- (DL) ITEM 644, DOTTED LINE, 8"
- (LL) ITEM 644, LANE LINE, 4"
- (EL) ITEM 644, EDGE LINE, 4" (WHITE)
- (WD) ITEM 644, WORD ON PAVEMENT, 72"
- (TL) ITEM 644, TRANSVERSE/DIAGONAL LINE (WHITE)
- (CW) ITEM 644, CROSSWALK LINE

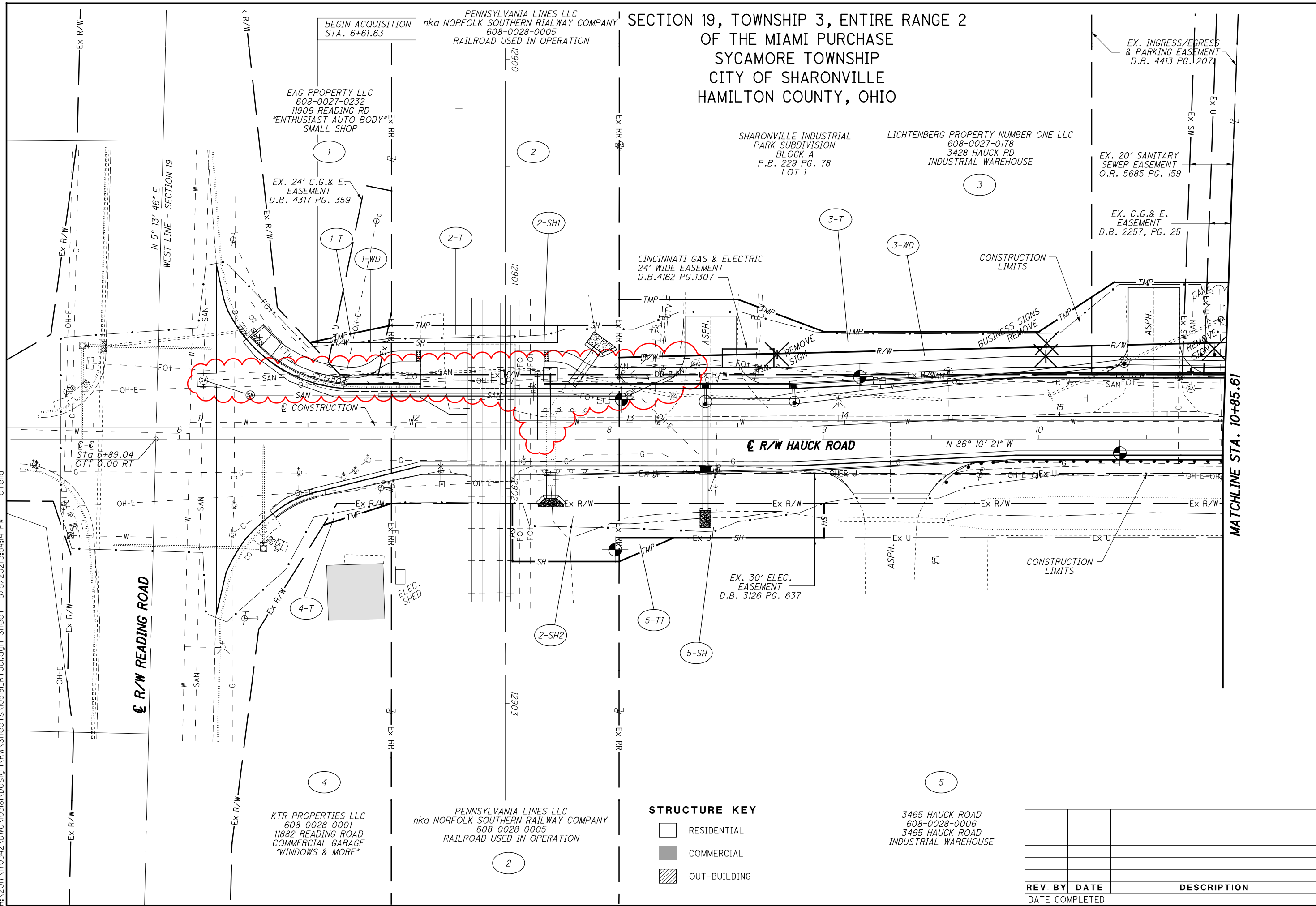
CALCULATED
DJR
CHECKED
JAK

0 20 40
HORIZONTAL
SCALE IN FEET

SIGNING AND PAVEMENT MARKING PLAN

HAM-HAUCK ROAD - WIDENING PART 2

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SECTION 19, TOWNSHIP 3, ENTIRE RANGE 2
 OF THE MIAMI PURCHASE
 SYCAMORE TOWNSHIP
 CITY OF SHARONVILLE
 HAMILTON COUNTY, OHIO

PENNSYLVANIA LINES LLC
 nka NORFOLK SOUTHERN RAILWAY COMPANY
 608-0028-0005
 RAILROAD USED IN OPERATION

EAG PROPERTY LLC
 608-0027-0232
 11906 READING RD
 "ENTHUSIAST AUTO BODY"
 SMALL SHOP

SHARONVILLE INDUSTRIAL
 PARK SUBDIVISION
 BLOCK A
 P.B. 229 PG. 78
 LOT 1

LICHTENBERG PROPERTY NUMBER ONE LLC
 608-0027-0178
 3428 HAUCK RD
 INDUSTRIAL WAREHOUSE

EX. 20' SANITARY
 SEWER EASEMENT
 O.R. 5685 PG. 159

EX. C.G.& E.
 EASEMENT
 D.B. 2257, PG. 25

CINCINNATI GAS & ELECTRIC
 24" WIDE EASEMENT
 D.B. 4162 PG. 1307

CONSTRUCTION
 LIMITS

R/W HAUCK ROAD

MATCHLINE STA. 10+85.61

KTR PROPERTIES LLC
 608-0028-0001
 11882 READING ROAD
 COMMERCIAL GARAGE
 "WINDOWS & MORE"

PENNSYLVANIA LINES LLC
 nka NORFOLK SOUTHERN RAILWAY COMPANY
 608-0028-0005
 RAILROAD USED IN OPERATION

3465 HAUCK ROAD
 608-0028-0006
 3465 HAUCK ROAD
 INDUSTRIAL WAREHOUSE

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

REV. BY	DATE	DESCRIPTION

HORIZONTAL SCALE IN FEET

PID NO. **105181**

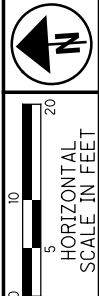
R/W DESIGNER: PSL
 R/W REVIEWER: DAH

RIGHT OF WAY TOPO SHEET
HAM-HAUCK ROAD - WIDENING
STA. 10+50 TO STA. 10+85.61

9 / 28

132 / 151

PENNSYLVANIA LINES RAILROAD
SECTION 19, TOWNSHIP 3, ENTIRE RANGE 2
OF THE MIAMI PURCHASE
SYCAMORE TOWNSHIP
CITY OF SHARONVILLE
HAMILTON COUNTY, OHIO



PID NO. **105181**
R/W DESIGNER PLS R/W REVIEWER DAH

RAILROAD PLAT

**HAM-HAUCK ROAD -
WIDENING**

27/28
150
151

3
LICHENBERG PROPERTY NUMBER ONE LLC
608-0027-0178
3428 HAUCK ROAD
INDUSTRIAL WAREHOUSE

5
3465 HAUCK ROAD
608-0028-0006
3465 HAUCK ROAD
INDUSTRIAL WAREHOUSE

RIGHT OF WAY AND TRACK MAP
THE CLEVELAND, CINCINNATI, CHICAGO & ST. LOUIS RY. CO.
CINCINNATI DIVISION MAIL LINE
STA. 12818+93 TO STA. 12924+53
JUNE 30, 1919

2
PENNSYLVANIA LINES LLC
nka NORFOLK SOUTHERN RAILWAY COMPANY
608-0028-0005
RAILROAD USED IN OPERATION

2
PENNSYLVANIA LINES LLC
nka NORFOLK SOUTHERN RAILWAY COMPANY
608-0028-0005
RAILROAD USED IN OPERATION

RR MM 245

2-SH
"AREA OF JOINT USE"

7+51.65 @ HAUCK RD =
12901+76.89 @ RR

4
KTR PROPERTIES LLC
608-0028-0001
11882 READING ROAD
COMMERCIAL GARAGE
"WINDOWS & MORE"

1
EAG PROPERTY LLC
608-0027-0232
11906 READING ROAD
"ENTHUSIAST AUTO BODY"
SMALL SHOP

REV. BY	DATE	DESCRIPTION

H:\2017\170342\DWG\105181\Design\RW_Sheets\105181_RR001.dgn Sheet 5/5/2021 3:51:54 PM rotella