

CLEARING AND GRUBBING, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF CUTTING AND REMOVAL OF ALL TREES AND BRUSH AS DETAILED ON SHEETS 5A THRU 5 P. THE OUTSIDE CLEARING LIMITS ARE DEFINED BY EITHER THE CONSTRUCTION LIMITS OR THE RIGHT-OF-WAY LIMITS. THE TREE AND BRUSH REMOVAL IS REQUIRED IN ALL OF THE MEDIAN AS IT IS WITHIN THE RIGHT-OF-WAY. (RIGHT OF WAY IS DEFINED AS EXISTING AND/OR PROPOSED R/W - INCLUDING EASEMENTS OF TEMPORARY, SLOPE & CHANNEL, ETC.)

IT SHOULD BE NOTED THE EXISTING L/A FENCE IS TO REMAIN IN PLACE AND UNDISTURBED BY THE TREE & BRUSH REMOVAL OPERATIONS. THE TREES AND BRUSH SHALL BE REMOVED AROUND AND WITHIN THE FENCE TO THE BEST OF THE CONTRACTORS ABILITY WITHOUT DISTURBING THE FENCE.

TREE STUMPS MAY REMAIN IN PLACE AND SHALL NOT BE MORE THAN 1 FT ABOVE THE EXISTING GROUND ELEVATION. ALL DEBRIS SHALL BE REMOVED AND PROPERLY DISPOSED OF. IF THE CONTRACTOR REMOVES THE STUMPS, THIS REMOVAL WILL BE CONSIDERED INCIDENTAL TO THE LUMP SUM BID PRICE FOR ITEM 201 CLEARING AND GRUBBING, AS PER PLAN.

THE PLAN AND R/W SHEETS OF HAM-75-14.61 PID 76256; CONSTRUCTION PROJECT NUMBER 210043 WERE USED AS THE PLAN SHEETS FOR THIS PROJECT. SOME OF THE TREES WITHIN THE CLEARING LIMITS ARE MARKED WITH AN "X" AND SOME ARE NOT. IT SHOULD BE CLARIFIED - THE HEAVILY DENSE VEGETATED AREAS ARE DEPICTED BY A WOODED OUTLINE AND ARE TO BE REMOVED IF WITHIN THE CLEARING LIMITS. THERE ARE A FEW TREES MARKED AS "SAVE" ON CHESTER AND SHARON ROAD. THE INTENT OF THIS PROJECT, AS STATED ABOVE, IS FOR THE REMOVAL OF TREES AND BRUSH WITHIN THE PROJECT LIMITS AS DEFINED ON PLAN SHEETS 5A-5P. ALL WORK SHALL BE AS DIRECTED BY THE ENGINEER.

AFTER THE CONTRACTOR HAS PERFORMED THE TREE REMOVAL LAYOUT, THE ENGINEER AND THE CONTRACTOR SHALL MEET TO FIELD REVIEW THE LIMITS OF CLEARING AND BE IN AGREEMENT, PRIOR TO PHYSICAL TREE REMOVAL OPERATIONS COMMENCING.

IF DURING TREE REMOVAL OPERATIONS THERE IS A QUESTION "IF" A TREE IS TO BE REMOVED OR NOT; THE CONTRACTOR SHALL CONTACT THE ODOT REPRESENTATIVE PRIOR TO REMOVAL FOR CLARIFICATION.

ALL LABOR, EQUIPMENT, MATERIAL AND INCIDENTAL ITEMS REQUIRED TO COMPLETE THIS ITEM OF WORK SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 201 CLEARING AND GRUBBING, AS PER PLAN.

SEE RECORDED SHEETS FOR P.S. STAMP AND SIGNATURE. STAMP NOT CARRIED FORWARD AS SHEET NUMBERING WAS MODIFIED FOR THIS TREE CLEARING PLAN SET.

ENDANGERED BAT HABITAT REMOVAL

THIS PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS: A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

ITEM 623 CONSTRUCTION LAYOUT STAKES, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ITEM 623 OF THE 2019 CMS SPECIFICATIONS, THE CONTRACTOR SHALL BE REQUIRED TO STAKE THE RIGHT-OF-WAY LIMITS OF THE PROJECT TO CLEARLY DEFINING THE LIMITS OF VEGETATION REMOVAL REQUIRED. IN ADDITION TO STAKING THE IRON PIN SETS, TURNING POINTS, AND R/W CORNERS, INTERMEDIATE POINTS ALONG THE R/W MAY BE REQUIRED TO CLEARLY IDENTIFY THE R/W LIMITS. THE STAKING OPERATION WILL BE TO THE SATISFACTION AND DIRECTION OF THE PROJECT ENGINEER.

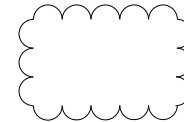
PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 623 CONSTRUCTION LAYOUT STAKES, AS PER PLAN.

ITEM 614, MAINTAINING TRAFFIC

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

UNAUTHORIZED LANE USE TABLE										
LOCATION	DIRECTION	EX. NO. OF THRU LANES	1 LANE CLOSED		2 LANES CLOSED		15 MIN. SHORT DURATION COMPLETE CLOSURE	COMPLETE CLOSURE	TIME UNIT	DISINCENTIVE PER TIME UNIT PER LANE
			WEEKDAY	WEEKEND	WEEKDAY	WEEKEND				
I-75	NB/SB	3	8 PM - 6 AM	8 PM - 8 AM	11 PM - 5 AM	11 PM - 6 AM	12 AM - 4 AM	NONE	1 MIN.	\$495
RAMPS	ALL	1	NONE	NONE	NONE	NONE	NONE	NONE	1 MIN.	\$495
		2	8 PM - 6 AM	8 PM - 8 AM	NONE	NONE	NONE	NONE	1 MIN.	\$495
SHARON RD	BOTH	2	9 AM - 3 PM 7 PM - 6 AM	7 PM - 3 PM	NONE	NONE	11 PM - 5 AM	NONE	1 MIN.	\$115
CHESTER RD	BOTH	1	9 AM - 4 PM 7 PM - 6 AM	ALL TIMES	NONE	NONE	NONE	NONE	1 MIN.	\$35
KEMPER RD	BOTH	2	7 PM - 6 AM	7 PM - 6 AM	NONE	NONE	NONE	NONE	1 MIN.	\$70

PAYMENTS FOR ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC



WOODED AREA HEAVILY VEGETATION

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CALCULATED GAT CHECKED XXX
GENERAL NOTES & GENERAL SUMMARY

SHEET NUM.										PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
													201	11001	LS		CLEARING AND GRUBBING, AS PER PLAN	
													614	11000	LS		MAINTAINING TRAFFIC	
													614	11110	30	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
													623	10001	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN	
													624	10000	LS		MOBILIZATION	

HAM - 75 - 14.61
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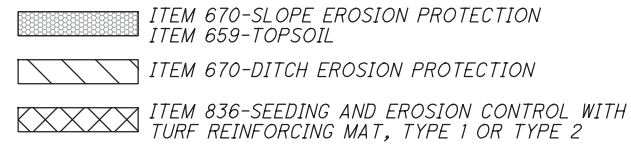
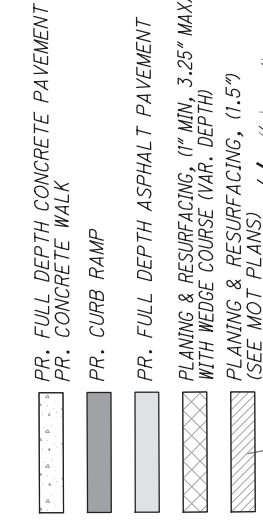
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--- HAM-75-12.60 (PID 82288)
--- ORIGINAL HAM-75
** DENOTES ITEM CONSTRUCTED DURING HAM-75-12.60 (PID 82288)

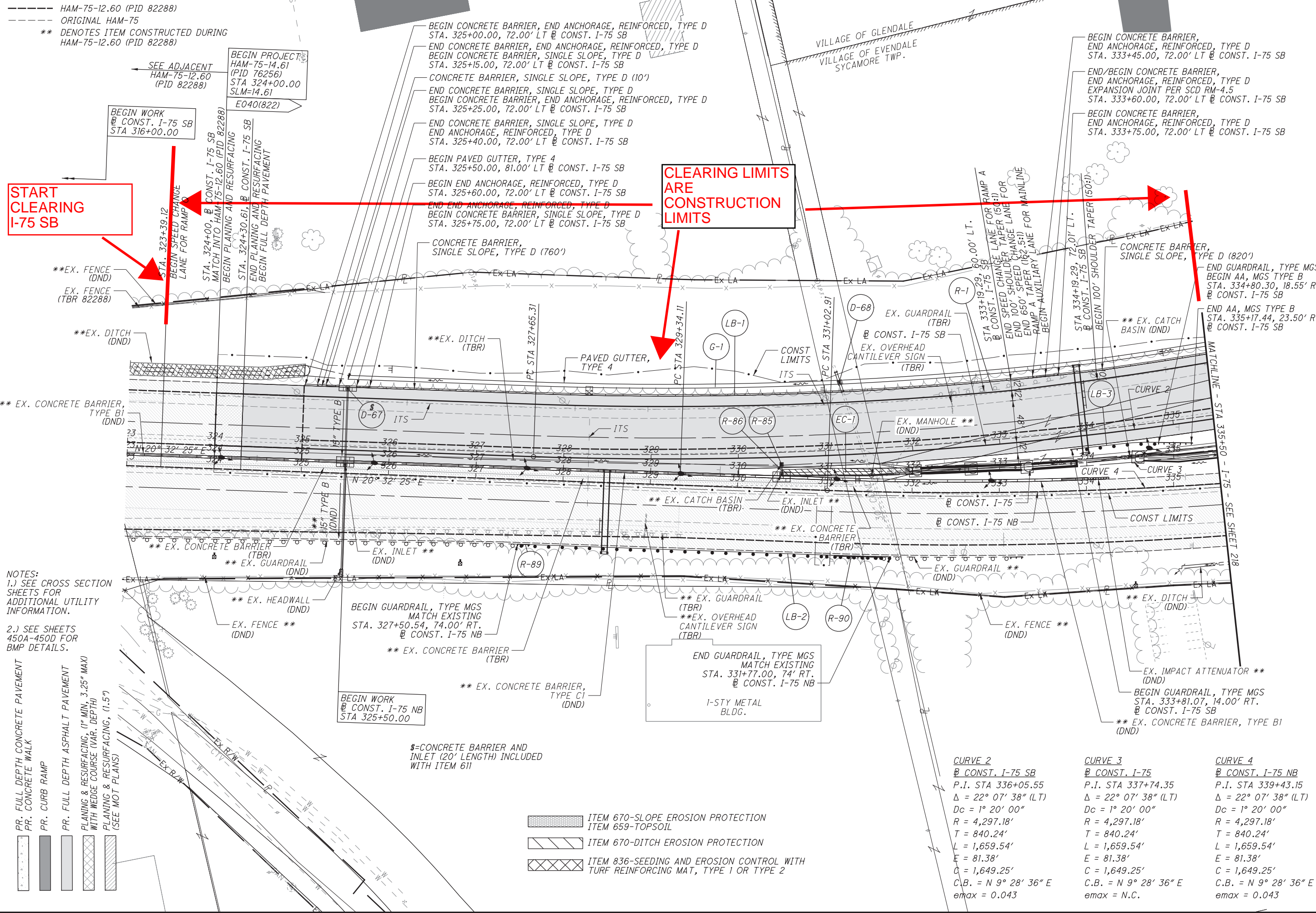
START CLEARING I-75 SB

CLEARING LIMITS ARE CONSTRUCTION LIMITS

NOTES:
1.) SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.
2.) SEE SHEETS 450A-450D FOR BMP DETAILS.



CURVE 2	CURVE 3	CURVE 4
CONST. I-75 SB	CONST. I-75	CONST. I-75 NB
P.I. STA 336+05.55	P.I. STA 337+74.35	P.I. STA 339+43.15
$\Delta = 22^\circ 07' 38" (LT)$	$\Delta = 22^\circ 07' 38" (LT)$	$\Delta = 22^\circ 07' 38" (LT)$
$Dc = 1^\circ 20' 00"$	$Dc = 1^\circ 20' 00"$	$Dc = 1^\circ 20' 00"$
$R = 4,297.18'$	$R = 4,297.18'$	$R = 4,297.18'$
$T = 840.24'$	$T = 840.24'$	$T = 840.24'$
$L = 1,659.54'$	$L = 1,659.54'$	$L = 1,659.54'$
$E = 81.38'$	$E = 81.38'$	$E = 81.38'$
$C = 1,649.25'$	$C = 1,649.25'$	$C = 1,649.25'$
C.B. = N 9° 28' 36" E	C.B. = N 9° 28' 36" E	C.B. = N 9° 28' 36" E
$e_{max} = 0.043$	$e_{max} = N.C.$	$e_{max} = 0.043$



--- HAM-75-12.60 (PID 82288)
 --- ORIGINAL HAM-75
 ** DENOTES ITEM CONSTRUCTED DURING
 HAM-75-12.60 (PID 82288)

ITEM 670-SLOPE EROSION PROTECTION
 ITEM 659-TOPSOIL
 ITEM 670-DITCH EROSION PROTECTION
 ITEM 836-SEEDING AND EROSION CONTROL WITH
 TURF REINFORCING MAT, TYPE 1 OR TYPE 2

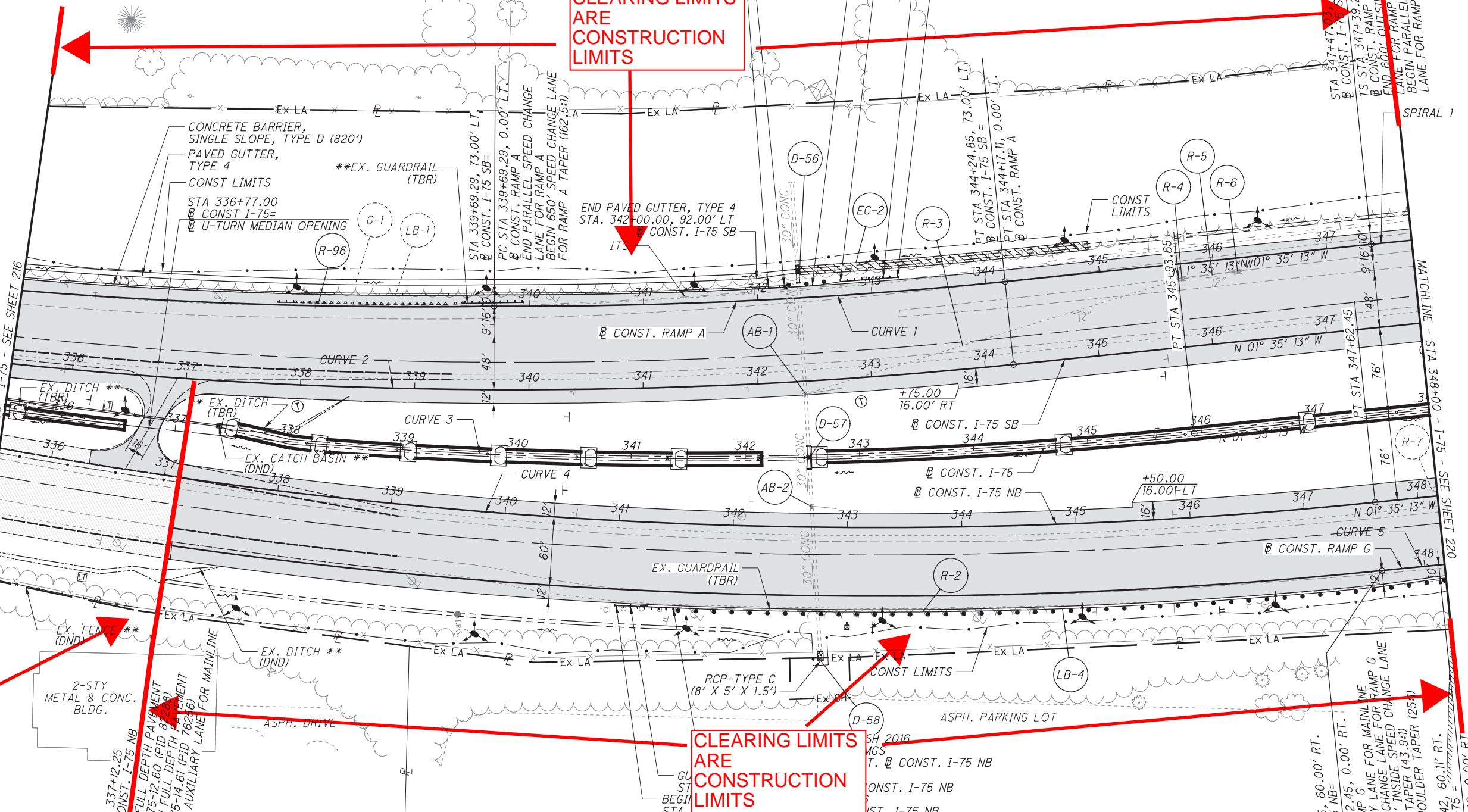
**CLEARING LIMITS
 ARE
 CONSTRUCTION
 LIMITS**

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NOTES:
 1.) SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.
 2.) SEE SHEETS 450A-450D FOR BMP DETAILS

**START
 CLEARING
 I-75 NB**

PR. FULL DEPTH CONCRETE PAVEMENT
 PR. CONCRETE WALK
 PR. CURB RAMP
 PR. FULL DEPTH ASPHALT PAVEMENT
 PLANING & RESURFACING, (1" MIN. WITH WEDGE COURSE (VAR. DEPTH)
 PLANING & RESURFACING, (1.5" (SEE MOT PLANS)



STA 337+12.25
 @ CONST. I-75 NB
 END FULL DEPTH PAVEMENT
 HAM-75-12.60 (PID 82288)
 BEGIN FULL DEPTH PAVEMENT
 HAM-75-14.61 (PID 76236)
 BEGIN AUXILIARY LANE FOR MAINLINE

CURVE 1 @ CONST. RAMP A P.I. STA 341+93.41 $\Delta = 6^\circ 04' 27''$ (LT) $Dc = 1^\circ 21' 23''$ $R = 4,224.18'$ $T = 224.12'$ $L = 447.82'$ $E = 5.94'$ $C = 447.61'$ $C.B. = N 1^\circ 27' 00'' E$ $emax = 0.043$	CURVE 2 @ CONST. I-75 SB P.I. STA 336+05.55 $\Delta = 22^\circ 07' 38''$ (LT) $Dc = 1^\circ 20' 00''$ $R = 4,297.18'$ $T = 840.24'$ $L = 1,659.54'$ $E = 81.38'$ $C = 1,649.25'$ $C.B. = N 9^\circ 28' 36'' E$ $emax = 0.043$	CURVE 3 @ CONST. I-75 P.I. STA 337+74.35 $\Delta = 22^\circ 07' 38''$ (LT) $Dc = 1^\circ 20' 00''$ $R = 4,297.18'$ $T = 840.24'$ $L = 1,659.54'$ $E = 81.38'$ $C = 1,649.25'$ $C.B. = N 9^\circ 28' 36'' E$ $emax = N.C.$	CURVE 4 @ CONST. I-75 NB P.I. STA 339+43.15 $\Delta = 22^\circ 07' 38''$ (LT) $Dc = 1^\circ 20' 00''$ $R = 4,297.18'$ $T = 840.24'$ $L = 1,659.54'$ $E = 81.38'$ $C = 1,649.25'$ $C.B. = N 9^\circ 28' 36'' E$ $emax = 0.043$	CURVE 5 @ CONST. RAMP G P.I. STA 352+70.82 $\Delta = 5^\circ 04' 49''$ (RT) $Dc = 0^\circ 30' 00''$ $R = 11,459.16'$ $T = 508.37'$ $L = 1,016.07'$ $E = 11.27'$ $C = 1,015.73'$ $C.B. = N 0^\circ 57' 11'' E$ $emax = N.C.$	SPIRAL 1 @ CONST. RAMP A P.I. STA 348+72.63 $Ls = 200.00'$ $LT = 133.34'$ $ST = 66.67'$ $x = 199.99'$ $y = 1.75'$ $k = 100.00'$ $p = 0.44'$ $emax = 0.028$
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1-STY BRICK & METAL BLDG.

HORIZONTAL SCALE IN FEET

CALCULATED WLC CHECKED JDH

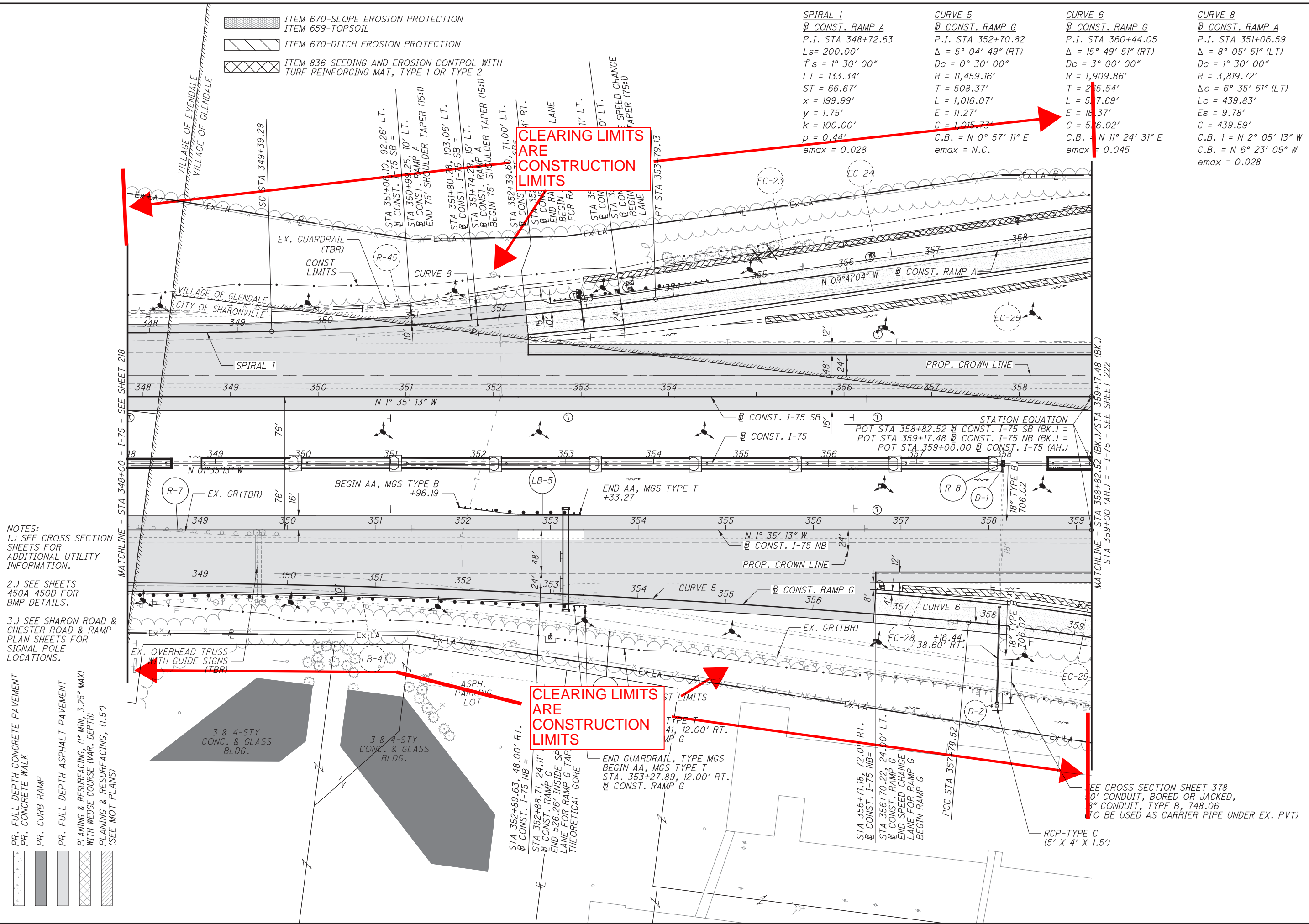
PLAN - I-75 SB / NB
 STA 335+50 TO STA 348+00

HAM-75-14.61

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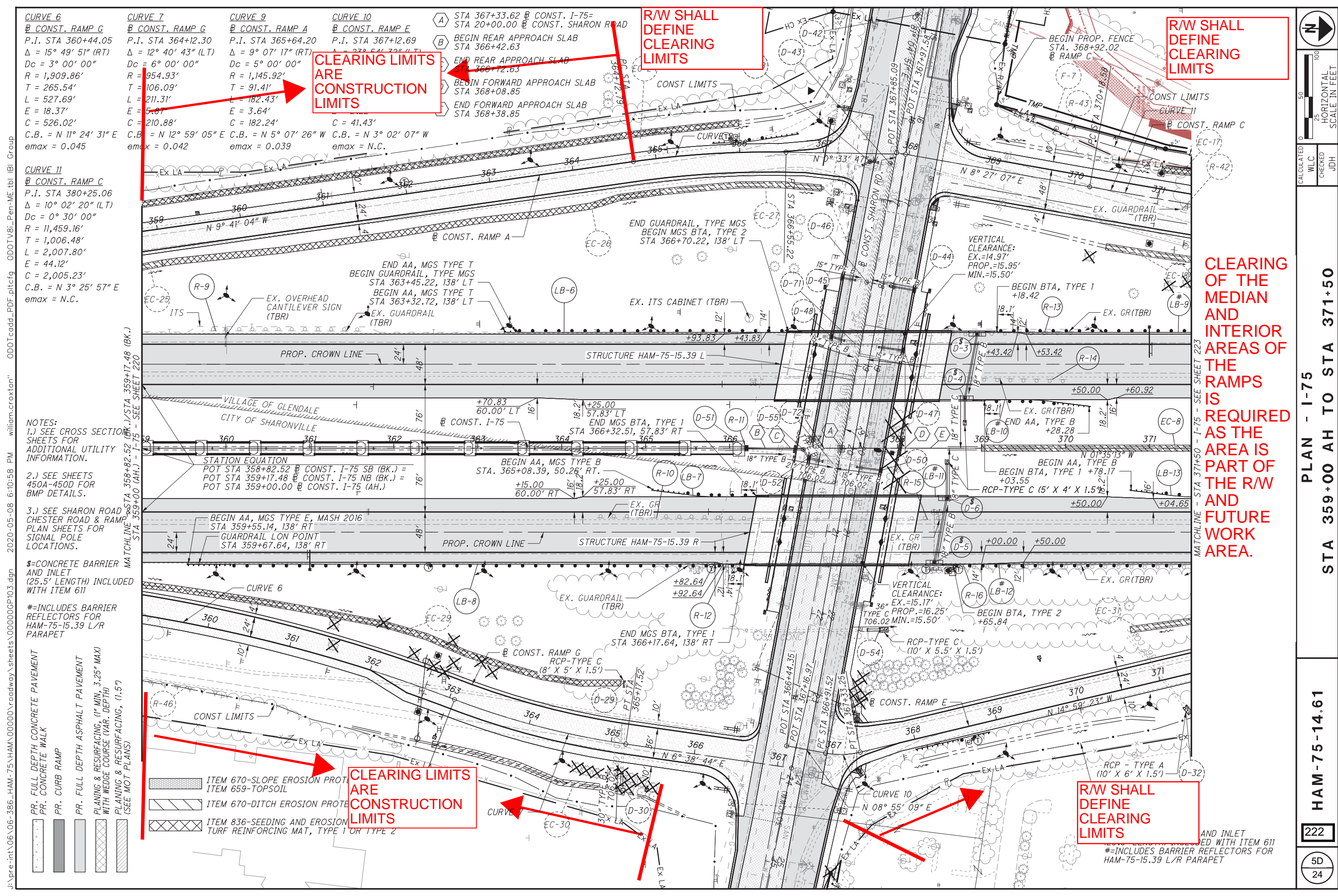


- ITEM 670-SLOPE EROSION PROTECTION
- ITEM 659-TOPSOIL
- ITEM 670-DITCH EROSION PROTECTION
- ITEM 836-SEEDING AND EROSION CONTROL WITH TURF REINFORCING MAT, TYPE 1 OR TYPE 2

SPiral 1	CURVE 5	CURVE 6	CURVE 8
CONST. RAMP A	CONST. RAMP G	CONST. RAMP G	CONST. RAMP A
P.I. STA 348+72.63	P.I. STA 352+70.82	P.I. STA 360+44.05	P.I. STA 351+06.59
Ls= 200.00'	$\Delta = 5^\circ 04' 49''$ (RT)	$\Delta = 15^\circ 49' 51''$ (RT)	$\Delta = 8^\circ 05' 51''$ (LT)
$f_s = 1^\circ 30' 00''$	$D_c = 0^\circ 30' 00''$	$D_c = 3^\circ 00' 00''$	$D_c = 1^\circ 30' 00''$
LT = 133.34'	R = 11,459.16'	R = 1,909.86'	R = 3,819.72'
ST = 66.67'	T = 508.37'	T = 25.54'	T = 6.35' (LT)
x = 199.99'	L = 1,016.07'	L = 537.69'	Lc = 439.83'
y = 1.75'	E = 11.27'	E = 18.37'	Es = 9.78'
k = 100.00'	C = 1,015.73'	C = 536.02'	C = 439.59'
p = 0.44'	C.B. = N 0° 57' 11" E	C.B. = N 11° 24' 31" E	C.B. 1 = N 2° 05' 13" W
emax = 0.028	emax = N.C.	emax = 0.045	emax = 0.028

- NOTES:
- SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.
 - SEE SHEETS 450A-450D FOR BMP DETAILS.
 - SEE SHARON ROAD & CHESTER ROAD & RAMP PLAN SHEETS FOR SIGNAL POLE LOCATIONS.

- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANING & RESURFACING, (1.5") (SEE MOT PLANS)



CURVE 6	CURVE 7	CURVE 9	CURVE 10
CONST. RAMP G	CONST. RAMP G	CONST. RAMP A	CONST. RAMP E
P.I. STA 360+44.05	P.I. STA 364+12.30	P.I. STA 365+64.20	P.I. STA 367+12.69
$\Delta = 15^\circ 49' 51''$ (RT)	$\Delta = 12^\circ 40' 43''$ (LT)	$\Delta = 9^\circ 07' 17''$ (RT)	$\Delta = 27^\circ 54' 30''$ (LT)
$D_c = 3^\circ 00' 00''$	$D_c = 6^\circ 00' 00''$	$D_c = 5^\circ 00' 00''$	$D_c = 27^\circ 54' 30''$
$R = 1,909.86'$	$R = 954.93'$	$R = 1,145.92'$	$R = 1,145.92'$
$T = 265.54'$	$T = 106.09'$	$T = 91.41'$	$T = 91.41'$
$L = 527.69'$	$L = 211.31'$	$L = 182.43'$	$L = 182.43'$
$E = 18.37'$	$E = 5.87'$	$E = 3.64'$	$E = 3.64'$
$C = 526.02'$	$C = 210.88'$	$C = 182.24'$	$C = 41.43'$
C.B. = $N 11^\circ 24' 31'' E$	C.B. = $N 12^\circ 59' 05'' E$	C.B. = $N 5^\circ 07' 26'' W$	C.B. = $N 3^\circ 02' 07'' W$
$e_{max} = 0.045$	$e_{max} = 0.042$	$e_{max} = 0.039$	$e_{max} = N.C.$

CURVE 11
CONST. RAMP C
P.I. STA 380+25.06
$\Delta = 10^\circ 02' 20''$ (LT)
$D_c = 0^\circ 30' 00''$
$R = 11,459.16'$
$T = 1,006.48'$
$L = 2,007.80'$
$E = 44.12'$
$C = 2,005.23'$
C.B. = $N 3^\circ 25' 57'' E$
$e_{max} = N.C.$

- NOTES:
- SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.
 - SEE SHEETS 450A-450D FOR BMP DETAILS.
 - SEE SHARON ROAD CHESTER ROAD & RAMP PLAN SHEETS FOR SIGNAL POLE LOCATIONS.

#=CONCRETE BARRIER AND INLET (25.5' LENGTH) INCLUDED WITH ITEM 611

#=INCLUDES BARRIER REFLECTORS FOR HAM-75-15.39 L/R PARAPET

- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANNING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANNING & RESURFACING, (1.5") (SEE MOT PLANS)
- ITEM 670-SLOPE EROSION PROTECTION WITH ITEM 659-TOPSOIL
- ITEM 670-DITCH EROSION PROTECTION WITH ITEM 659-TOPSOIL
- ITEM 836-SEEDING AND EROSION CONTROL WITH TURF REINFORCING MAT, TYPE 1 OR TYPE 2

CLEARING LIMITS ARE CONSTRUCTION LIMITS

CLEARING LIMITS ARE CONSTRUCTION LIMITS

R/W SHALL DEFINE CLEARING LIMITS

R/W SHALL DEFINE CLEARING LIMITS

CLEARING OF THE MEDIAN AND INTERIOR AREAS OF THE RAMPS IS REQUIRED AS THE AREA IS PART OF THE R/W AND FUTURE WORK AREA.

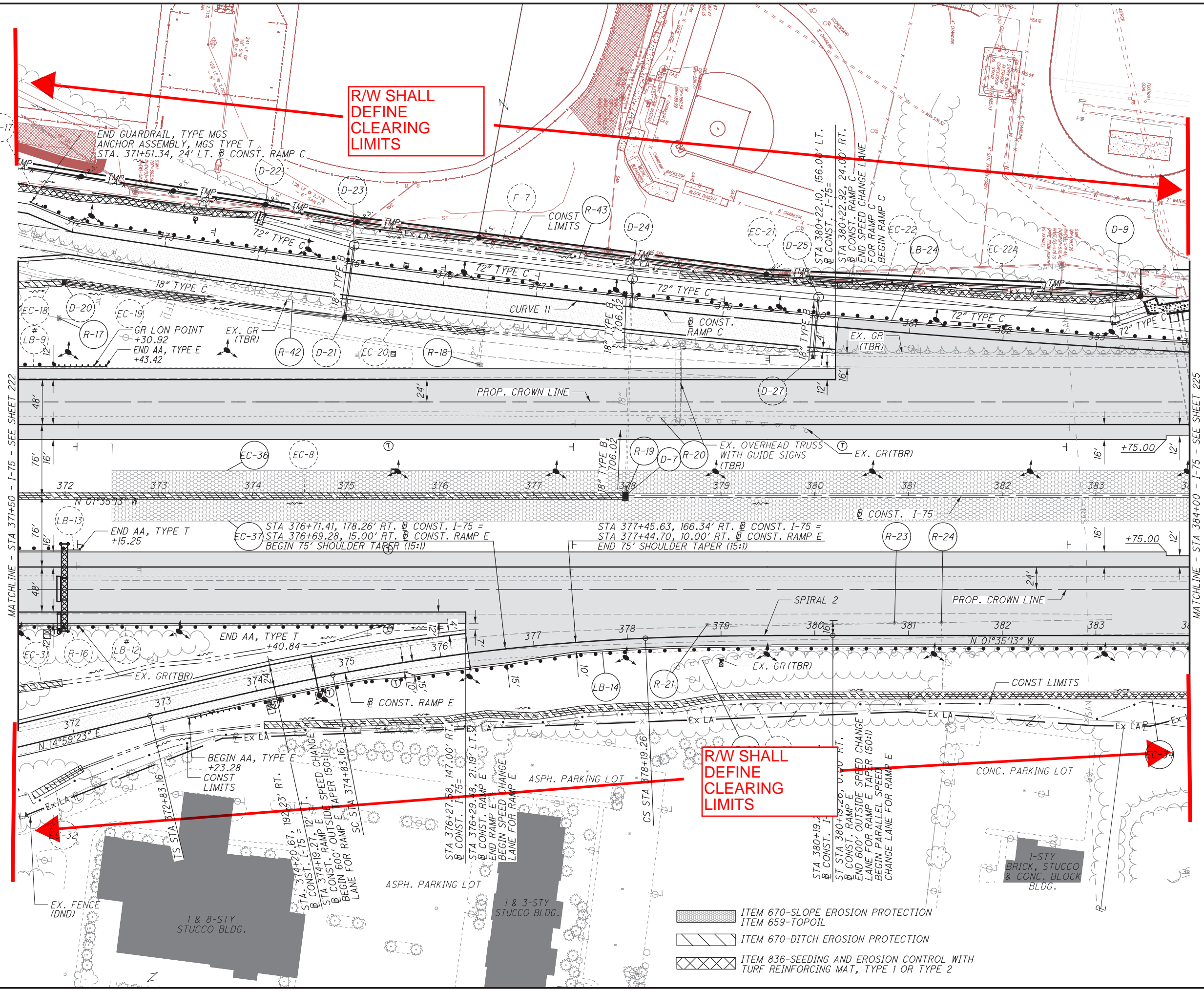
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CURVE 11
CONST. RAMP C
P.I. STA 380+25.06
 $\Delta = 10^\circ 02' 20''$ (LT)
 $D_c = 0^\circ 30' 00''$
 $R = 11,459.16'$
 $T = 1,006.48'$
 $L = 2,007.80'$
 $E = 44.12'$
 $C = 2,005.23'$
C.B. = $N 3^\circ 25' 57'' E$
emax = N.C.

SPIRAL 2
CONST. RAMP E
P.I. STA 376+52.52
 $\Delta = 13^\circ 24' 10''$ (RT)
 $D_c = 2^\circ 30' 00''$
 $R = 2,291.83'$
 $L_s = 200.00'$
 $\theta_s = 2^\circ 30' 00''$
 $LT = 133.35'$
 $ST = 66.68'$
 $x = 199.96'$
 $y = 2.91'$
 $k = 99.99'$
 $p = 0.73'$
 $\Delta_c = 8^\circ 24' 10''$ (RT)
 $L_c = 336.11'$
 $T_s = 369.36'$
 $E_s = 16.50'$
 $C = 335.81'$
 $C1 = C2 = 199.98'$
C.B. 1 = $N 14^\circ 09' 23'' W$
C.B. 2 = $N 8^\circ 17' 18'' W$
C.B. 3 = $S 2^\circ 25' 13'' E$
emax = 0.040

NOTES:
1.) SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.
2.) SEE SHARON ROAD & CHESTER ROAD & RAMP PLAN SHEETS FOR SIGNAL POLE LOCATIONS.
#-INCLUDES BARRIER REFLECTORS FOR HAM-75-15.39 L/R PARAPET

- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CONCRETE RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANING & RESURFACING, (1.5") (SEE MOT PLANS)

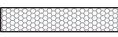
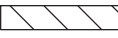



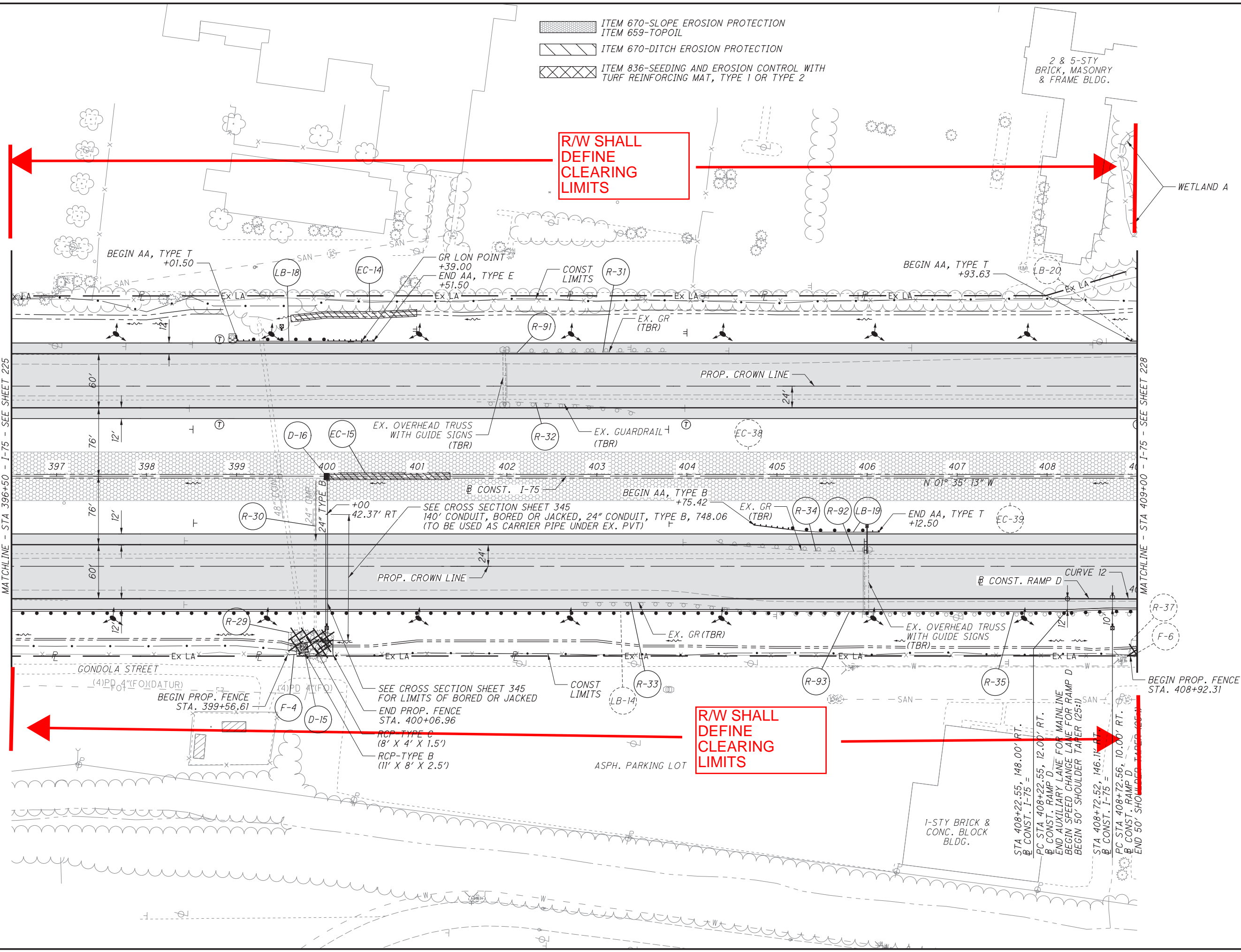
MATCHLINE - STA 371+50 - I-75 - SEE SHEET 222

MATCHLINE - STA 384+00 - I-75 - SEE SHEET 225







- ITEM 670-SLOPE EROSION PROTECTION
- ITEM 670-DITCH EROSION PROTECTION
- ITEM 836-SEEDING AND EROSION CONTROL WITH TURF REINFORCING MAT, TYPE 1 OR TYPE 2

CURVE 12
 @ CONST. RAMP D
 P.I. STA 411+52.48
 $\Delta = 3^\circ 17' 54''$ (RT)
 $D_c = 0^\circ 30' 00''$
 $R = 11,459.16'$
 $T = 329.93'$
 $L = 659.67'$
 $E = 4.75'$
 $C = 659.58'$
 $C.B. = N 0^\circ 03' 44'' E$
 $emax = N.C.$

-  ITEM 670-SLOPE EROSION PROTECTION
ITEM 659-TOPOIL
-  ITEM 670-DITCH EROSION PROTECTION
-  ITEM 836-SEEDING AND EROSION CONTROL WITH
TURF REINFORCING MAT, TYPE 1 OR TYPE 2



NOTES:
 1.) SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.

-  PR. FULL DEPTH CONCRETE PAVEMENT
-  PR. CONCRETE WALK
-  PR. CURB RAMP
-  PR. FULL DEPTH ASPHALT PAVEMENT
-  PLANING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
-  PLANING & RESURFACING, (1.5") (SEE MOT PLANS)

PLAN - I-75
 STA 396+50 TO STA 409+00

HAM-75-14.61

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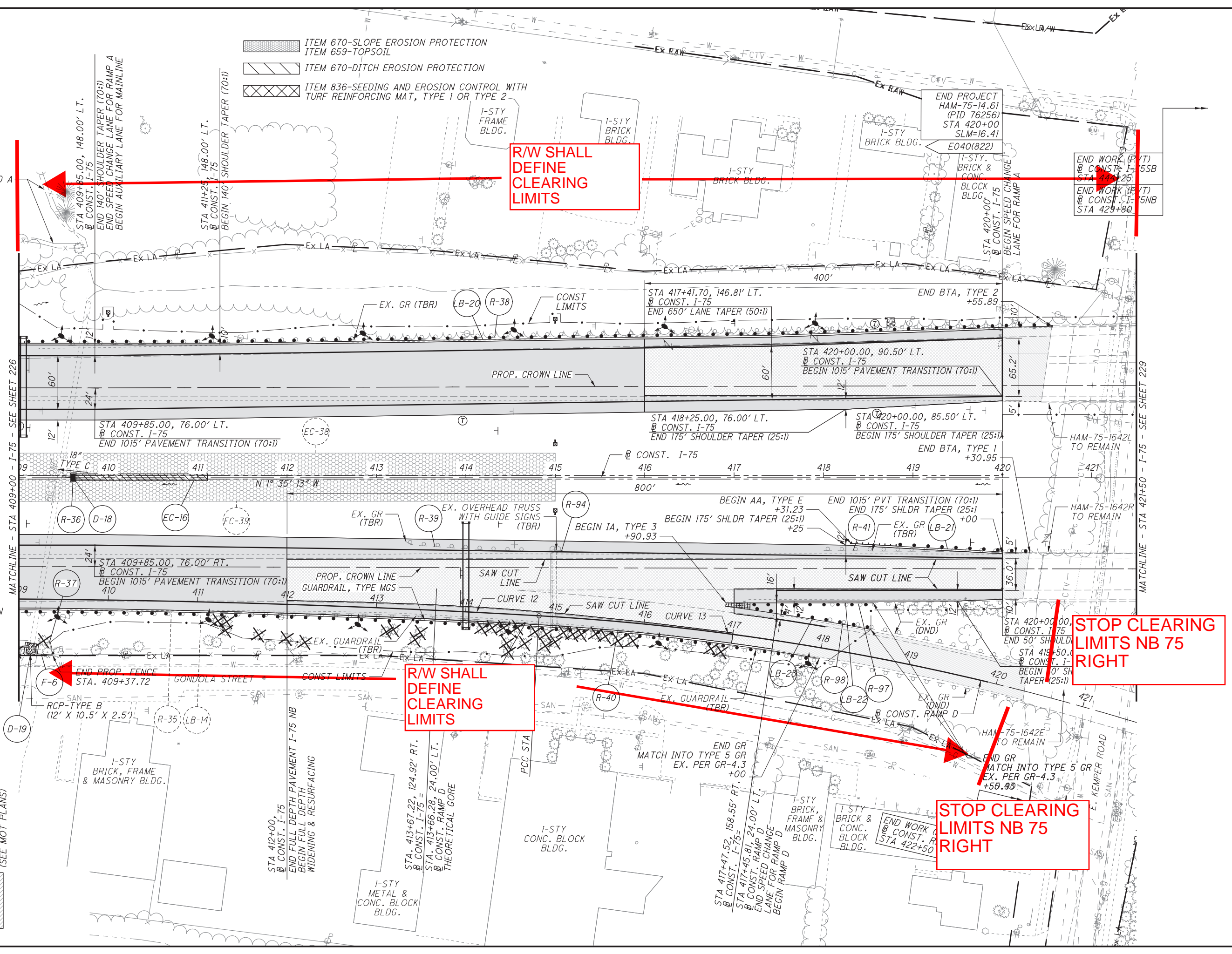
CURVE 12
CONST. RAMP D
P.I. STA 411+52.48
 $\Delta = 3^\circ 17' 54''$ (RT)
 $Dc = 0^\circ 30' 00''$
 $R = 11,459.16'$
 $T = 329.93'$
 $L = 659.67'$
 $E = 4.75'$
 $C = 659.58'$
C.B. = $N 0^\circ 03' 44'' E$
 $e_{max} = N.C.$

CURVE 13
CONST. RAMP D
P.I. STA 420+80.84
 $\Delta = 23^\circ 42' 02''$ (RT)
 $Dc = 2^\circ 00' 30''$
 $R = 2,852.90'$
 $T = 598.62'$
 $L = 1,180.11'$
 $E = 62.13'$
 $C = 1,171.72'$
C.B. = $N 13^\circ 33' 44'' E$
 $e_{max} = 0.033$

NOTES:
1.) SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.

- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANING & RESURFACING, (1.5") (SEE MOT PLANS)

- ITEM 670-SLOPE EROSION PROTECTION
- ITEM 659-TOPSOIL
- ITEM 670-DITCH EROSION PROTECTION
- ITEM 836-SEEDING AND EROSION CONTROL WITH TURF REINFORCING MAT, TYPE 1 OR TYPE 2



R/W SHALL DEFINE CLEARING LIMITS

R/W SHALL DEFINE CLEARING LIMITS

STOP CLEARING LIMITS NB 75 RIGHT

STOP CLEARING LIMITS NB 75 RIGHT

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CURVE 13
CONST. RAMP D
P.I. STA 420+80.84
 $\Delta = 23^\circ 42' 02''$ (RT)
 $Dc = 2^\circ 00' 30''$
 $R = 2,852.90'$
 $T = 598.62'$
 $L = 1,180.11'$
 $E = 62.13'$
 $C = 1,171.72'$
C.B. = N 13° 33' 44"
 $e_{max} = 0.033$

CURVE 14
CONST. I-75
P.I. STA 454+45.38
 $\Delta = 18^\circ 53' 30''$ (RT)
 $Dc = 0^\circ 26' 00''$
 $R = 13,222.11'$
 $T = 2,199.79'$
 $L = 4,359.64'$
 $E = 181.74'$
 $C = 4,339.92'$
C.B. = N 7° 51' 32" E
 $e_{max} = N.C.$

NOTES:
1.) SEE CROSS SECTION SHEETS FOR ADDITIONAL UTILITY INFORMATION.
2.) SEE SHEETS 450A-450D FOR BMP DETAILS.

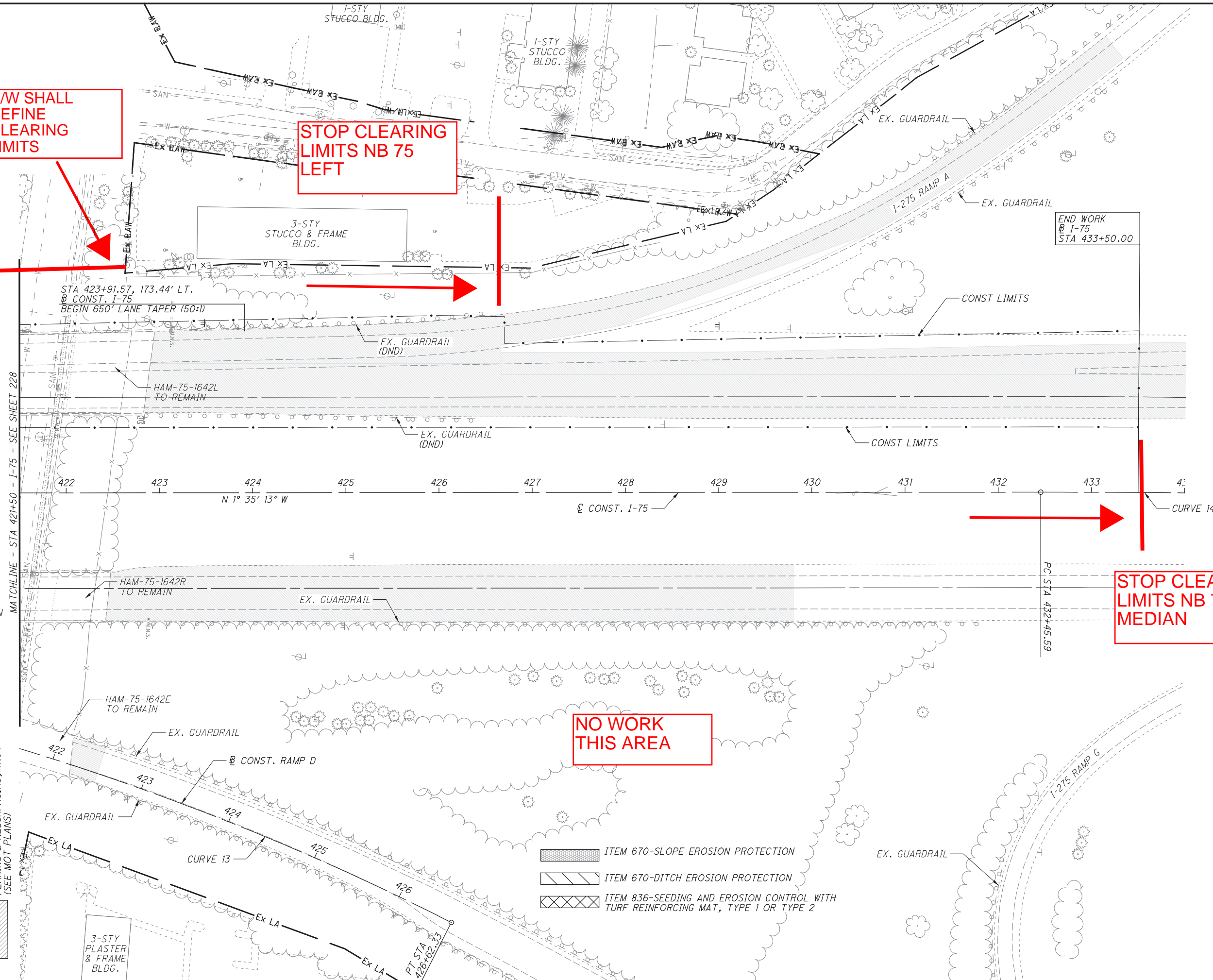
- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANING & RESURFACING, (1.5") (SEE MOT PLANS)

R/W SHALL DEFINE CLEARING LIMITS

STOP CLEARING LIMITS NB 75 LEFT

STOP CLEARING LIMITS NB 75 MEDIAN

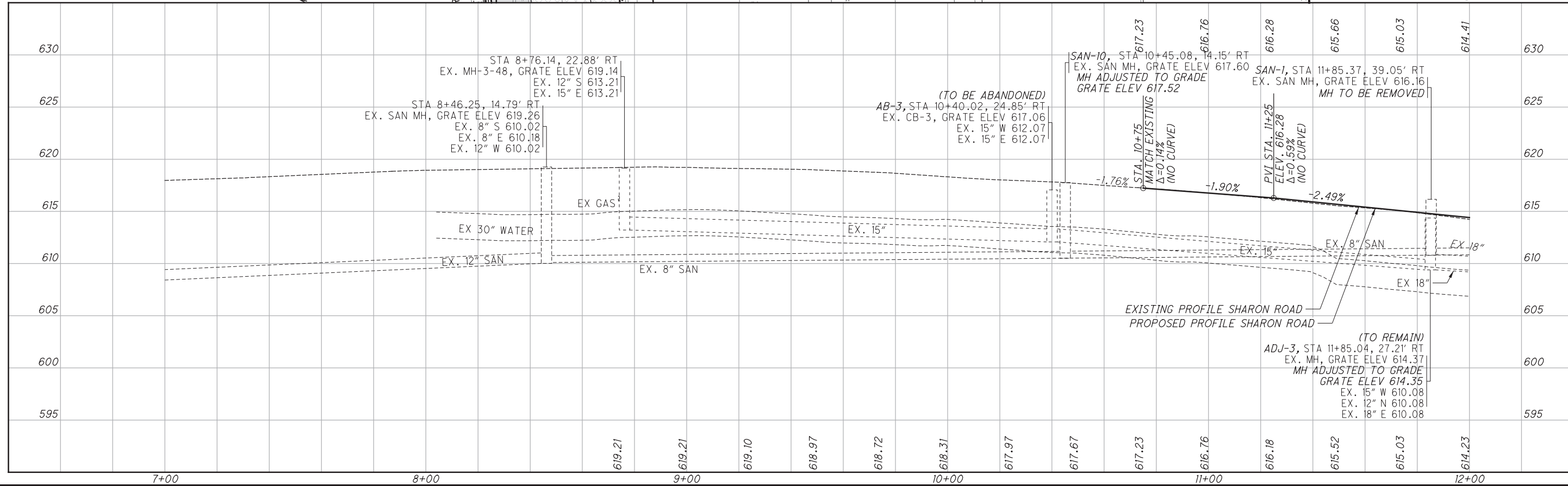
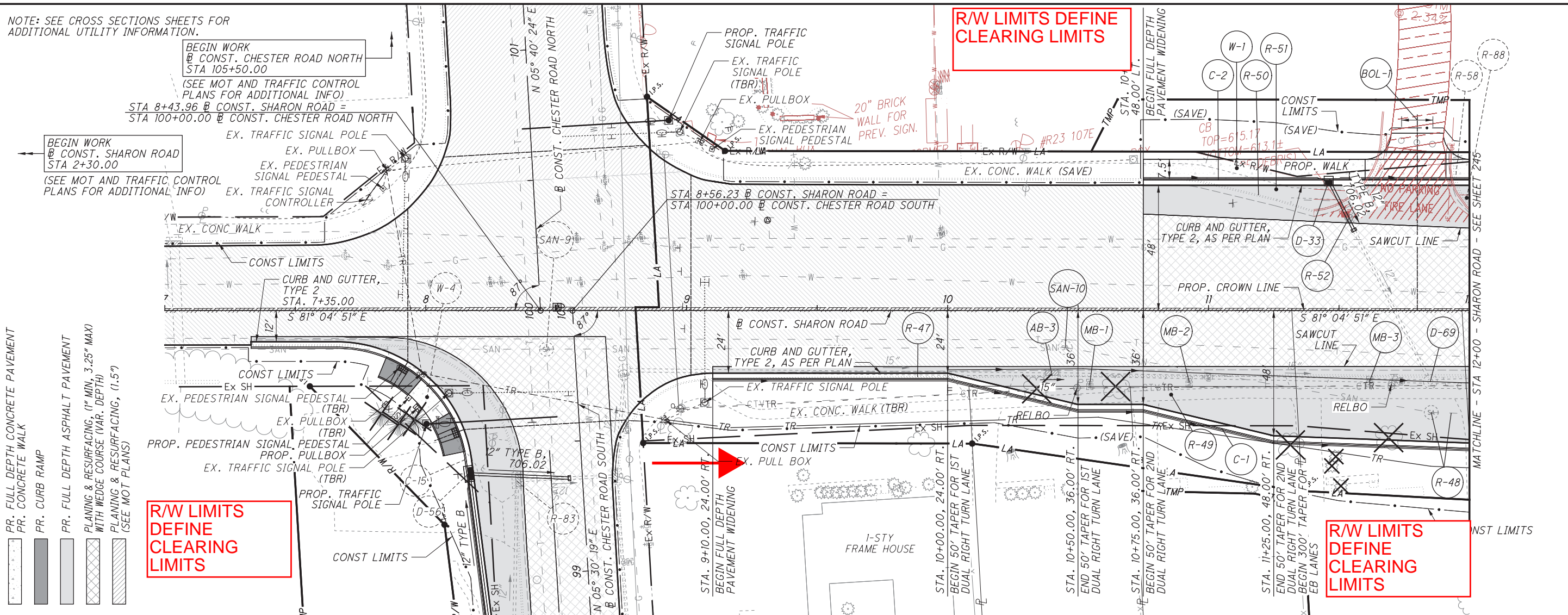
NO WORK THIS AREA



PLAN - I-75
STA 421+50 TO STA 434+00

HAM-75-14.61
229
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24

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- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANING & RESURFACING, (1.5')

NOTE: SEE CROSS SECTIONS SHEETS FOR ADDITIONAL UTILITY INFORMATION.

BEGIN WORK @ CONST. CHESTER ROAD NORTH STA 105+50.00
(SEE MOT AND TRAFFIC CONTROL PLANS FOR ADDITIONAL INFO)

STA 8+43.96 @ CONST. SHARON ROAD = STA 100+00.00 @ CONST. CHESTER ROAD NORTH

BEGIN WORK @ CONST. SHARON ROAD STA 2+30.00
(SEE MOT AND TRAFFIC CONTROL PLANS FOR ADDITIONAL INFO)

R/W LIMITS DEFINE CLEARING LIMITS

R/W LIMITS DEFINE CLEARING LIMITS

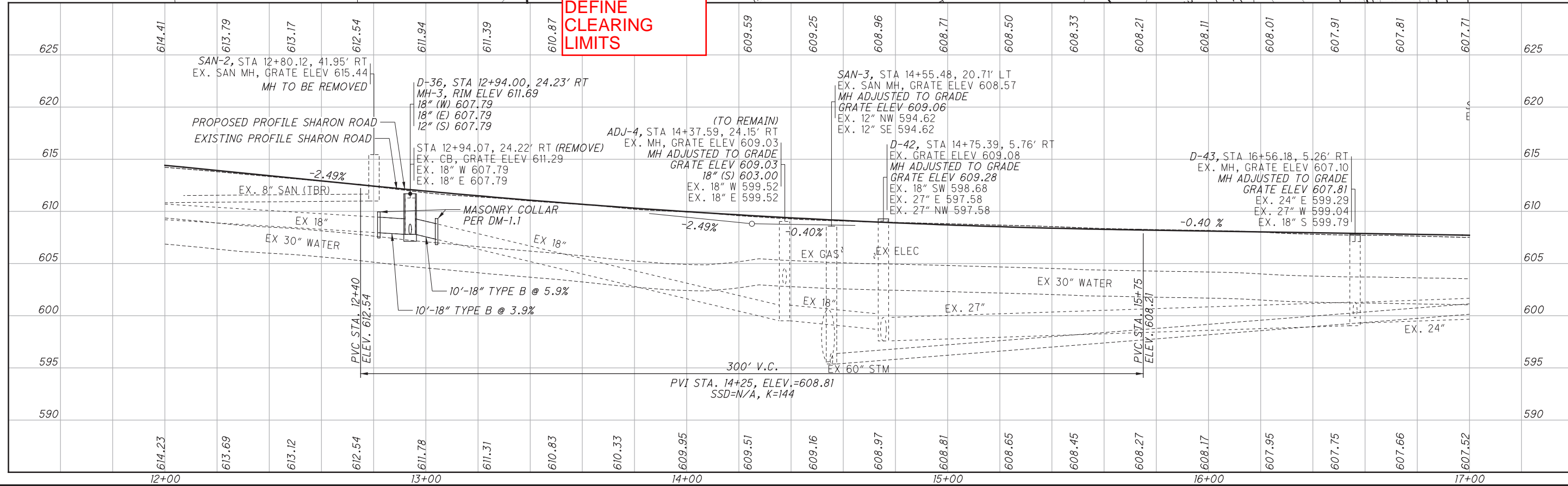
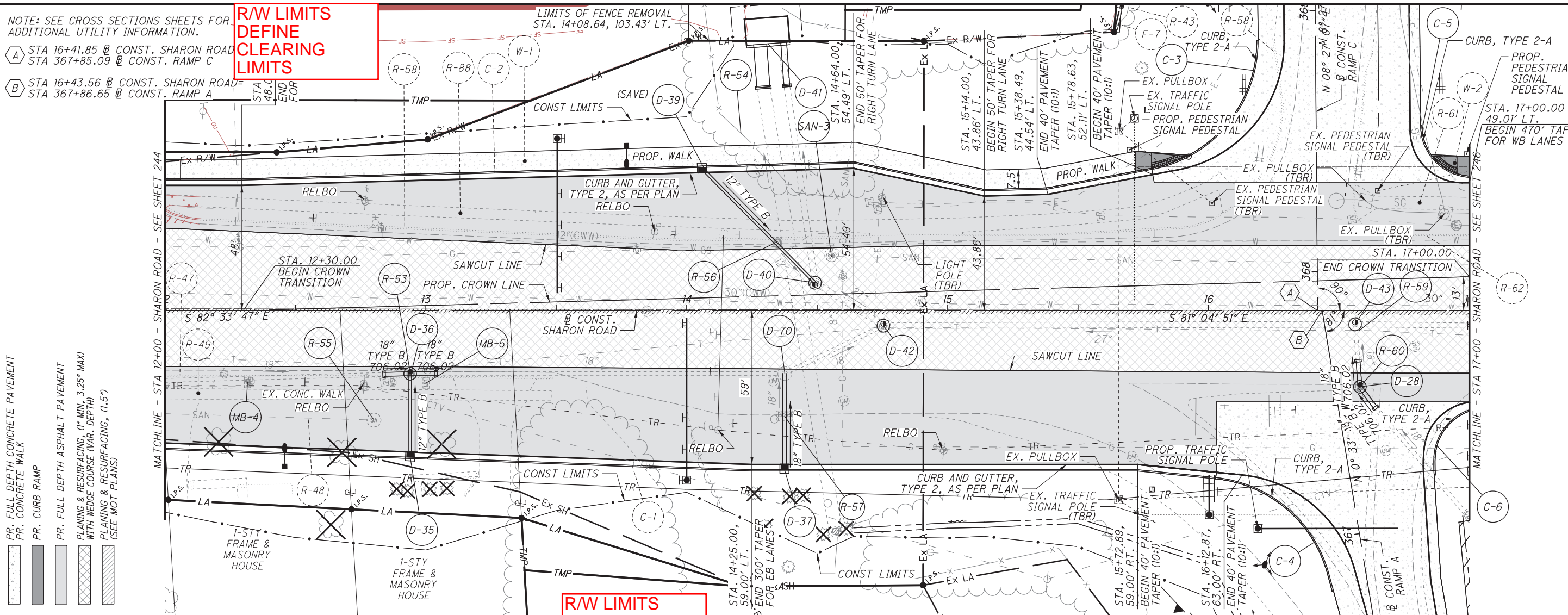
R/W LIMITS DEFINE CLEARING LIMITS



**PLAN AND PROFILE - SHARON ROAD
STA 7+00 TO STA 12+00**

HAM-75-14.61
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NOTE: SEE CROSS SECTIONS SHEETS FOR ADDITIONAL UTILITY INFORMATION.

(A) STA 16+41.85 @ CONST. SHARON ROAD
STA 367+85.09 @ CONST. RAMP C

(B) STA 16+43.56 @ CONST. SHARON ROAD
STA 367+86.65 @ CONST. RAMP A

**R/W LIMITS
DEFINE
CLEARING
LIMITS**

LIMITS OF FENCE REMOVAL
STA. 14+08.64, 103.43' LT.

**R/W LIMITS
DEFINE
CLEARING
LIMITS**

- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANNING & RESURFACING (1" MIN. 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANNING & RESURFACING (1.5") (SEE MOT PLANS)

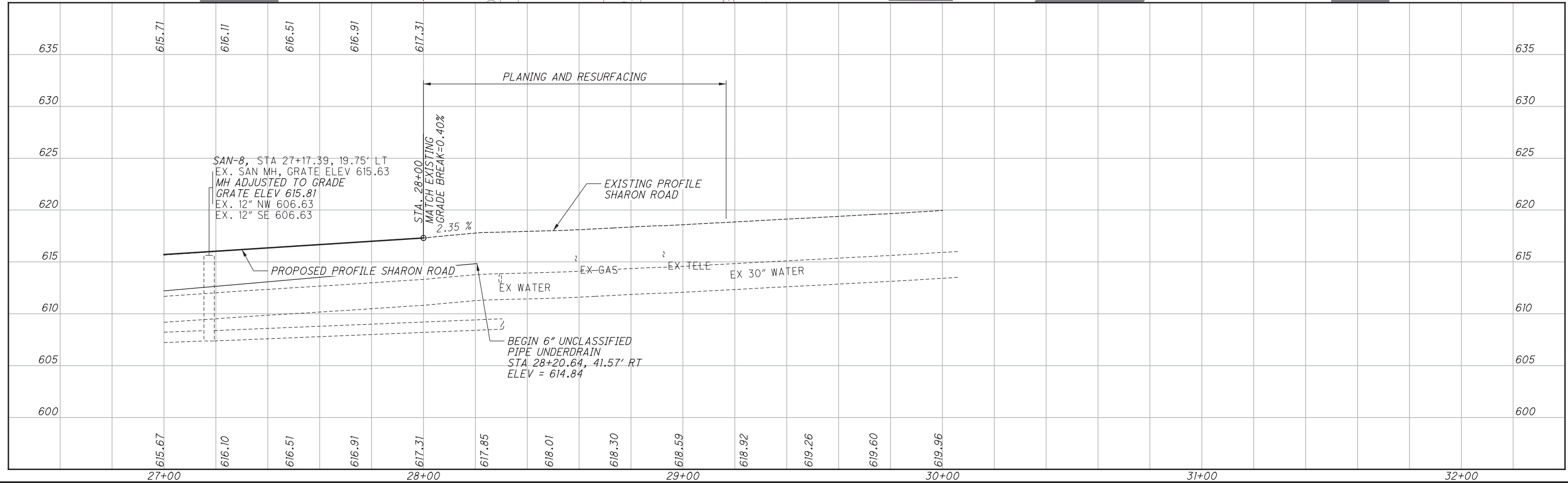
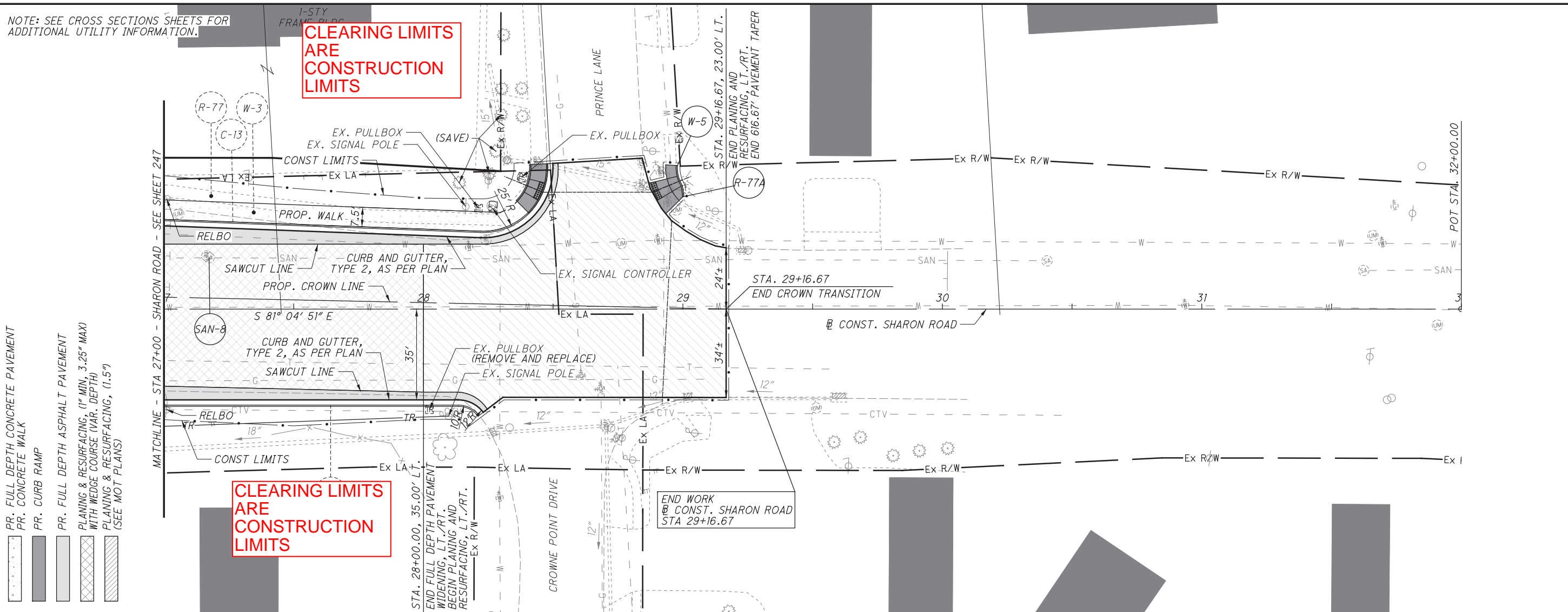
**PLAN AND PROFILE - SHARON ROAD
STA 12+00 TO STA 17+00**

HAM-75-14.61

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- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANING & RESURFACING, (1" MIN., 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANING & RESURFACING, (1.5") (SEE MOT PLANS)

NOTE: SEE CROSS SECTIONS SHEETS FOR ADDITIONAL UTILITY INFORMATION.

CLEARING LIMITS ARE CONSTRUCTION LIMITS

CLEARING LIMITS ARE CONSTRUCTION LIMITS

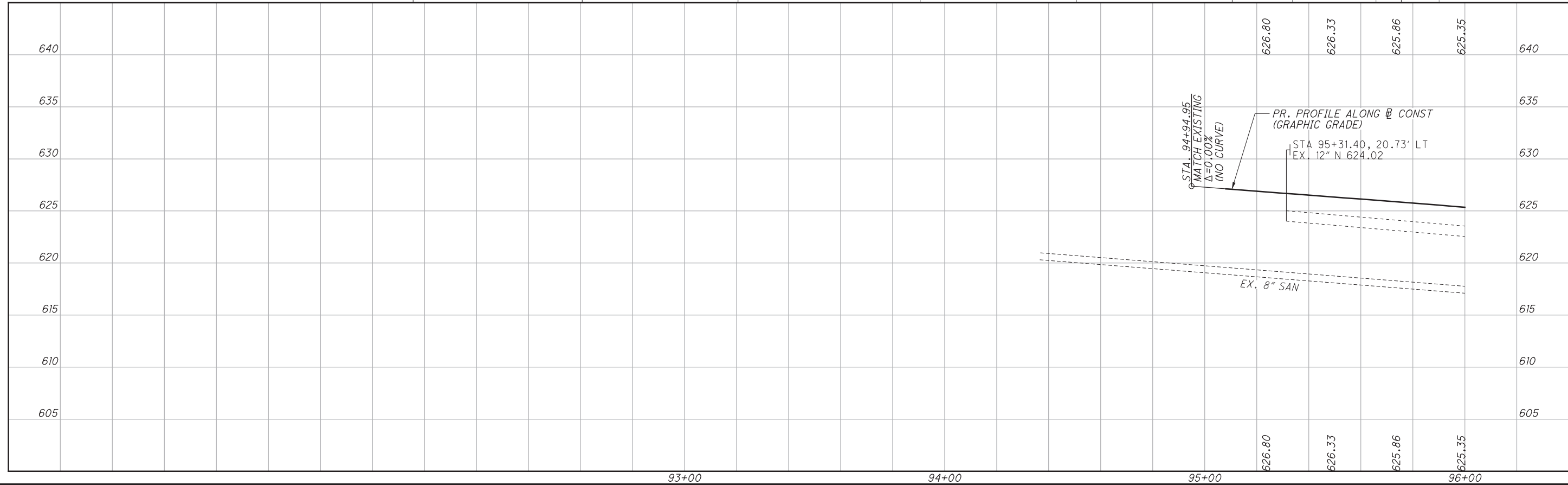
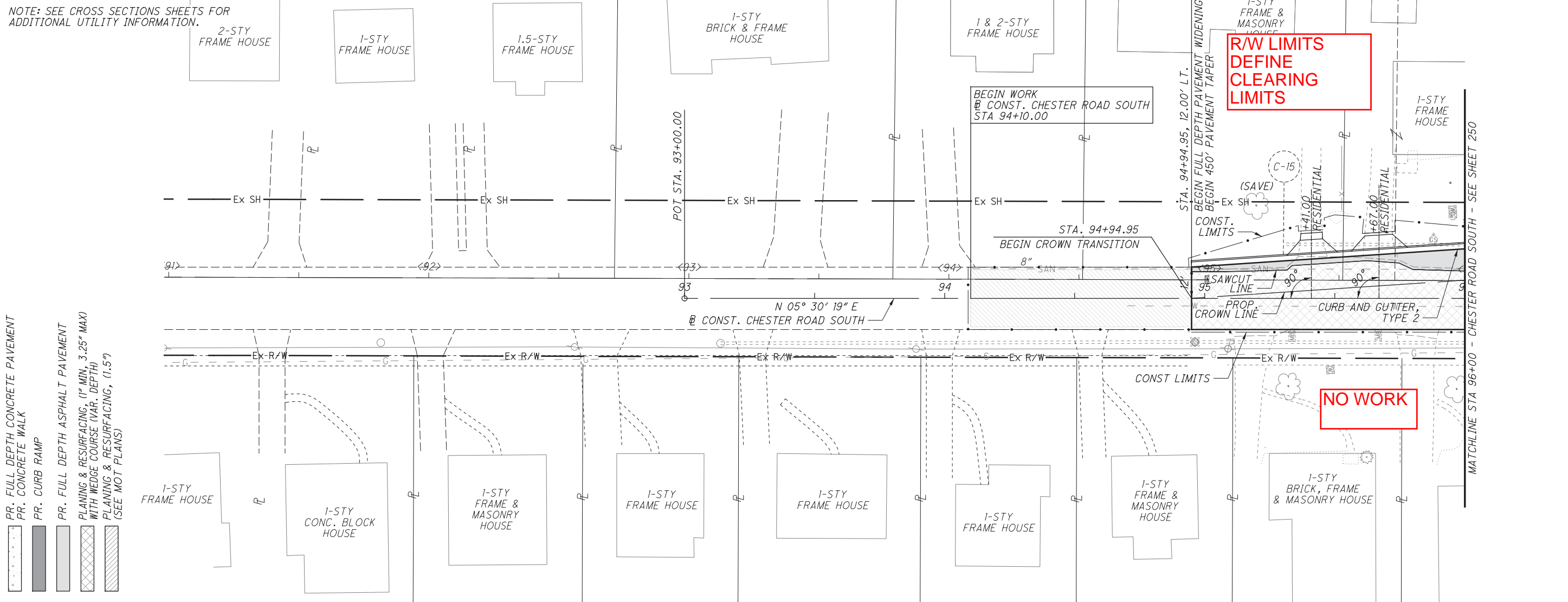
END WORK @ CONST. SHARON ROAD STA 29+16.67

CALCULATED WLC CHECKED JDH

HORIZONTAL SCALE IN FEET

**PLAN AND PROFILE - SHARON ROAD
STA 27+00 TO STA 32+00**

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- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANNING & RESURFACING, (1" MIN, 3.25" MAX) WITH WEDGE COURSE (VAR. DEPTH)
- PLANNING & RESURFACING, (1.5") (SEE MOT PLANS)

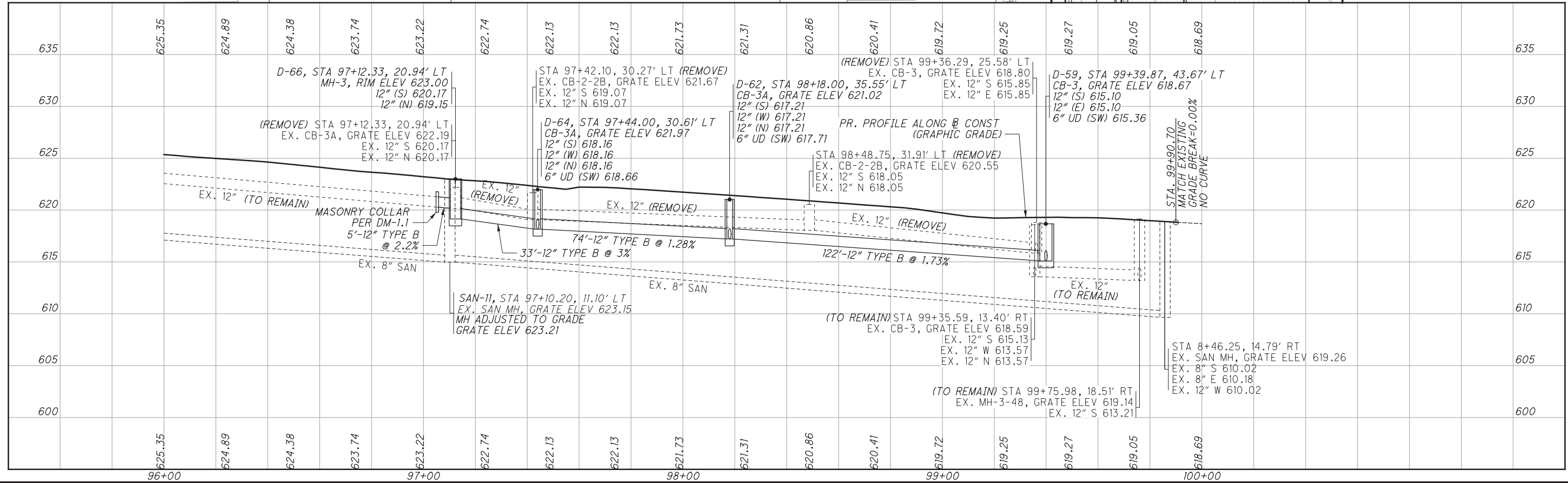
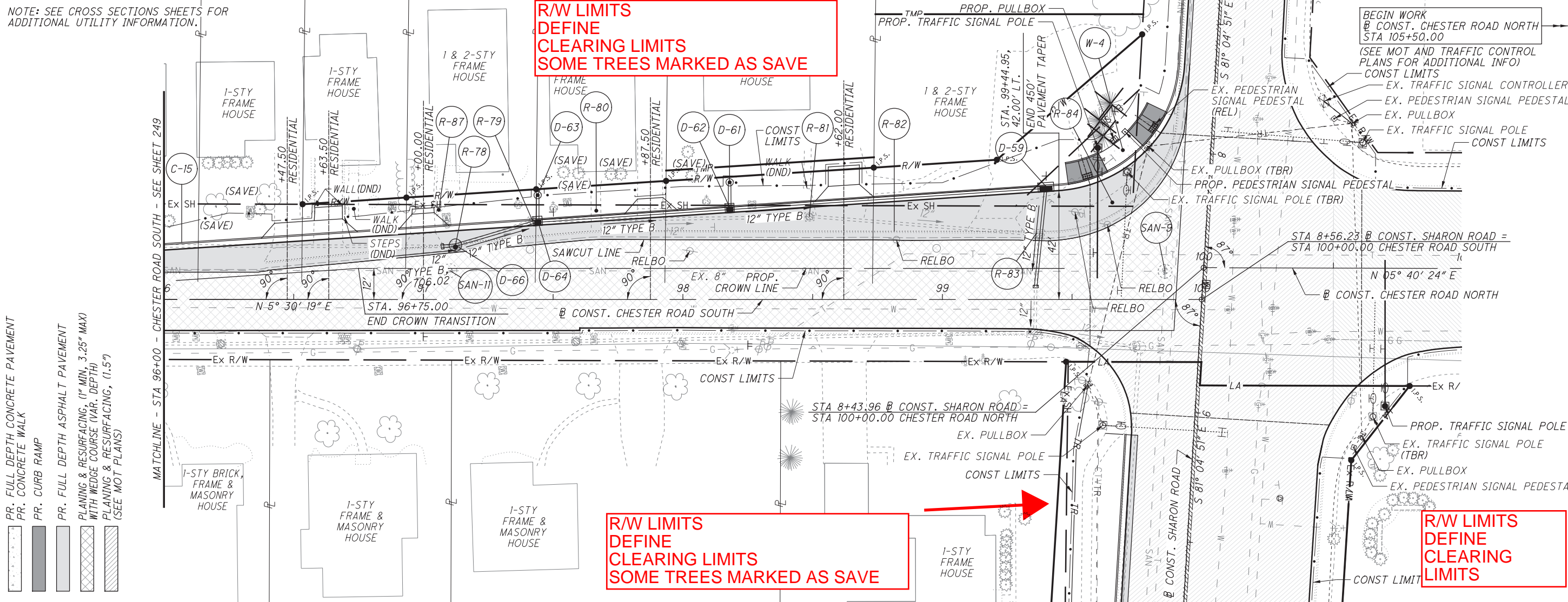
NOTE: SEE CROSS SECTIONS SHEETS FOR ADDITIONAL UTILITY INFORMATION.

CALCULATED W/LC CHECKED JDH

0 20 40
10 HORIZONTAL SCALE IN FEET

PLAN AND PROFILE - CHESTER ROAD SOUTH
STA 93+00 TO STA 96+00

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- PR. FULL DEPTH CONCRETE PAVEMENT
- PR. CONCRETE WALK
- PR. CURB RAMP
- PR. FULL DEPTH ASPHALT PAVEMENT
- PLANING & RESURFACING, (1" MIN, 3.25" MAX) WITH MEDGE COURSE (VAR. DEPTH)
- PLANING & RESURFACING, (1.5") (SEE MOT PLANS)

PLAN AND PROFILE - CHESTER ROAD SOUTH
STA 96+00 TO STA 100+00

HAM-75-14.61

250

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24

CALCULATED: WLC
 CHECKED: JDH

HORIZONTAL SCALE IN FEET